



**BICYCLE ADVISORY COMMITTEE MEETING**

MONDAY

OCTOBER 22, 2012

8:30 A.M.

PINELLAS COUNTY PLANNING DEPARTMENT  
CONFERENCE ROOM – 1<sup>ST</sup> FLOOR  
310 COURT STREET  
CLEARWATER, FLORIDA 33756

**AGENDA**

1. **CALL TO ORDER AND INTRODUCTIONS** (8:30)
2. **APPROVAL OF MINUTES – September 24, 2012** (8:30-8:35)
3. **MPO ACTIONS – October 10, 2012** (8:35-8:40)
4. **PRESENTATIONS:** (8:40-9:45)
  - A. Bicycle-Pedestrian-Trail Facilities, Mr. Alan Snel, SWFBUD (15 min)
  - B. Dunedin Bicycle Plan - Next Steps (20 min)
  - C. Beckett Bridge Project, Ms. Ann Venables, EC Driver & Associates (20 min)
  - D. Courtney Campbell Trail Draft Amenities Plan (10 min)
5. **ONGOING BAC BUSINESS** (9:45-9:50)
  - A. Pinellas Trail Loop Program Update
  - B. Status Report Chart
  - C. Standing Committees
  - D. Membership
6. **OTHER BUSINESS** (9:50-10:00)
  - A. Remaining Meeting Schedule for 2012 and Beginning of 2013
  - B. Reminder of Election of Officers at December Meeting
  - C. Pinellas Trails, Inc.
  - D. School Transportation and Enhanced Pedestrian Safety (STEPS)
  - E. Correspondence/Publications/Articles of Interest
  - F. Other
7. **ADJOURNMENT** (10:00)

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**NEXT BAC MEETING**  
**NOVEMBER 26, 2012**

APPROVAL OF MINUTES – September 24, 2012

The minutes of the September 24, 2012 BAC meeting are attached for review and approval.

ATTACHMENTS: [BAC Minutes – September 24, 2012](#)

ACTION: Approval of Minutes

MPO ACTIONS

A staff member will report on the actions taken by the MPO Board at the October 10, 2012 MPO meeting. The Newsletter/Action Sheet for that meeting is attached.

At its September 24, 2012 meeting, the Bicycle Advisory Committee (BAC) reviewed proposed amendments to the Pinellas Trailways Plan, which were requested by the cities of Clearwater and St. Petersburg. The BAC recommended MPO approval of the proposed Trailways Plan amendments at the September meeting which are attached. Because the Trailways Plan is part of the Long Range Transportation Plan, proposed amendments are subject to public hearing and review by the MPO. Following a public hearing at the October 10, 2012 MPO meeting, the MPO Board approved the BAC's recommendations.

ATTACHMENTS: [Newsletter/Action Sheet – October 10, 2012](#)

[Proposed Amendments to the Pinellas Trailways Plan 2012 \(Approved by MPO 10-10-12\)](#)

ACTION: None required, information item

PRESENTATIONS

A. Bicycle-Pedestrian-Trail Facilities, Mr. Alan Snel, SWFBUD

In towns and cities across the U.S., there is evidence that the number of adults choosing bicycling as a preferred transportation option has increased. Large cities such as Washington, D.C., Philadelphia, New York City, and Chicago have invested in safe networks of connecting bicycle lanes and off-road paths.

"The Department of Health (DOH) estimates that more than 500,000 adult New Yorkers use a bike at least once a month. According to the Department of Transportation's (DOT) [Commuter Cycling Indicator](#), there was a 13 percent increase in daily commuter bicycling between 2009 and 2010 alone. Our city has seen double-digit growth in bike ridership for four straight years -- effectively doubling the number of regular cyclists on our streets, according to the DOT's [annual counts](#). Since 2006, the City of New York has laid down more than 250 miles of bikes lanes (just over four percent of the city's 6,000 miles of streets) and New Yorkers are flocking to use the lanes as fast as they're opened." (<http://transalt.org/ourwork/bike/bikefaq>)

Mr. Alan Snel, South West Florida Bicycle United Dealers (SWFBUD), has created a presentation to show the amazing bicycle infrastructure of New York City, including bicycle trails, bridge access, bike lanes, sharrows and signage. During a recent visit, he was impressed with the infrastructure system for bicyclists and pedestrians, and agreed to share how similar facilities could benefit the Tampa Bay area.

Please welcome Mr. Snel as he presents a glimpse of a city committed to making bicycle infrastructure design and implementation a priority.

ATTACHMENTS: ["Look Out! DOT Creates Crosswalk Decals, Ad Campaign to Prevent Pedestrian Accidents," The New York Observer, September 19, 2012](#)

[Bicycling in New York City: Know the Facts," Transportation Alternatives, 2012](#)

[SWFBUD 2012 Bicycle Bash flyer](#)

ACTION: None, information item only

PRESENTATIONS

B. Dunedin Bicycle Plan – Next Steps

In late 2011/early 2012, the City of Dunedin developed a Citywide Master Plan highlighting favored routes for future bicycle/pedestrian facilities. Since that time, the city has gathered suggestions from citizens within its community and countywide to improve its bicycle/pedestrian plans. The City of Dunedin has requested guidance from the BAC with regard to proposed bicycle/pedestrian facilities referred to as the “Northern Route” and intended to connect the Pinellas Trail and community activity centers located within or adjacent to the Pinehurst corridor (Dunedin Community Center, Fine Arts Center, the High School, several churches, Hammock Park, and the Pinellas Trail).

The proposed Northern Route bicycle/pedestrian improvements provide a connection from the Pinellas Trail to the Dunedin Fine Arts Center & Community Center by expanding the existing sidewalk located on the south side of Michigan Boulevard. (see attached map) The sidewalk would connect Highlander Park to the south with the northern terminus of Patricia Avenue via an existing easement that runs behind the residential area between the park and the neighborhoods.

After entering the neighborhoods, the facility would transition into a bicycle route (using sharrows) and continue south to the intersection of McCarty Drive, and east to the northwest corner of the Dunedin High School property. The route would then cross Pinehurst Road using a new crosswalk completed in part with the Pinehurst Road improvements, and travel east within a drainage easement that runs along the north side of the high school. Continuing east, the trail would cross Braemoor Drive (a residential subdivision), and travel along an existing sidewalk to the intersection of CR-1 and Solon Avenue where it would transition back into sharrow mode, continue eastward along Solon Avenue to connect with the City's Englebert Sports Complex and Vanech Park, the spring training facilities for the Toronto Blue Jays. The route would then intersect with Belcher Road, which would serve as the eastern terminus of the proposed facility.

Please welcome Mr. Matthew Campbell, Assistant to the City Manager, and Ms. Joan Rice, Transportation and Traffic Engineer, as they present information about the Dunedin Citywide Master Plan to the Committee.

ATTACHMENTS: [Northern Route in Three Phases](#)

ACTION: BAC to review and provide comments

PRESENTATIONS

C. Beckett Bridge Project, Ms. Ann Venable, EC Driver & Associates

Pinellas County, in conjunction with the Florida Department of Transportation (FDOT), is conducting a Project Development and Environment (PD&E) Study to evaluate the removal, rehabilitation or replacement of the existing Beckett Bridge over Whitcomb Bayou in Tarpon Springs. The study is funded through a Local Agency Program (LAP) agreement with FDOT District 7. Mr. Tony Hornnik is the Project Manager for Pinellas County. EC Driver & Assoc., Inc. is under contract with Pinellas County to conduct the study.

The study began in January 2011 and is anticipated to be completed in December, 2013. The following alternatives are currently being evaluated:

- No Build
- No Build with Permanent Removal of the Existing Bridge
- Rehabilitation of the Existing Bridge
- Replacement with a New Low-Level Movable Bridge
- Replacement with a New Mid-Level Fixed Bridge

No additional traffic lanes are proposed. All replacement alternatives evaluated will provide one lane of traffic in each direction. In addition, all replacement alternatives considered will be constructed on the same alignment as the existing bridge.

Public input is an important aspect of the study. An Alternatives Public Workshop is anticipated to be held in January 2013. The project team is meeting with local government organizations and small stakeholder groups prior to the Public Workshop to provide opportunity for input.

Ms. Ann Venables, the consultant project manager and Mr. Jim Phillips, Chief Engineer for EC Driver, will make a brief presentation consisting of a power point presentation about the study and discuss the alternative concepts developed to date. Comments and input from the Advisory Committee concerning the concepts are welcome.

ATTACHMENTS: [Location Map, Beckett Bridge in Tarpon Springs](#)

ACTION: BAC to review and provide comments

PRESENTATIONS

D. Courtney Campbell Trail Conceptual Amenities Plan

The MPO Pinellas Trailways Plan has long identified the Courtney Campbell Causeway as one of the regional connections across Tampa Bay. In June 2010, the West Central Florida Chairs' Coordinating Committee (CCC), the regional MPO agency, recognized the Courtney Campbell Trail as regionally significant in that it would provide a strategic east-west link in a regional network of trail systems serving the Tampa Bay area. The CCC and FDOT provided additional regional support for the Trail project, and the Courtney Campbell Causeway/S.R. 60 trail project was programmed for construction earlier than anticipated. The Hillsborough County segments are under construction, and the Pinellas segments are currently in design, and construction is scheduled in FY 2014.

In 2005, the Courtney Campbell Causeway became the 14th parkway selected to the Florida Scenic Highway Program by FDOT. A group of dedicated individuals meet regularly to further the goals and objectives of the Scenic Highway Program to protect and preserve the Courtney Campbell Scenic Highway Corridor. At its August 17, 2012 meeting, the Corridor Advisory Committee (CAC) reviewed a conceptual trail amenities plan. The amenities plan was drafted to identify potential locations for trailheads, landscaping, benches, shade and other trail amenities.

MPO staff will review the conceptual trail amenities plan for the S.R. 60/Courtney Campbell Causeway with the Committee.

ATTACHMENTS: [S.R. 60 Multi-Use Trail PD&E Study Graphic – Figure 1-2](#)

ACTION: As deemed appropriate based on discussion

ONGOING BAC BUSINESS

A. Pinellas Trail Loop Program Update

The Pinellas Trail Loop Program is a coordinated effort to connect the mainline Pinellas Trail with the Progress Energy Trail and other facilities on the east side of the county to create a continuous trail around the county that traverses nearly every one of its mainland cities. The Trail Loop would provide links and connections to many popular destinations. The completed 75-mile Pinellas Trail Loop will provide non-motorized multi-use trail access countywide.

At this time, MPO staff will provide updated information for the Pinellas Trail Loop Program to the Committee.

ATTACHMENTS: [Pinellas Trail Loop Program Map and Chart](#)

ACTION: None required, information item

B. Status Report Chart

The BAC Status Report Chart is attached for your review.

ATTACHMENTS: [BAC Status Report Chart](#)

ACTION: As deemed appropriate based on discussion

C. Standing Committees

The Facilities, Safety & Education, and Marketing & Public Relations Standing Committees are scheduled to meet on October 22, 2012 at 8:00 a.m., preceding the BAC meeting.

ATTACHMENTS: [2012 BAC Standing Committees List](#)

ACTION: As deemed appropriate based on discussion

D. Membership

Currently there are two At Large Area vacancies and one North County Area vacancy. There are also vacancies for Law Enforcement representatives for Clearwater and St. Petersburg.

ATTACHMENTS: [BAC Membership List](#)

ACTION: None required, information item



OTHER BUSINESS

A. Remaining Meeting Schedule for 2012

As is the typical practice, due to the upcoming holiday season, the BAC will have its next meeting on November 26<sup>th</sup> and the December meeting will be cancelled. The first two meetings of 2013 are scheduled for January 28, 2013 and February 25, 2013. Based on discussion at recent BAC and PTAC meetings, a joint meeting of the two committees may be scheduled for one of these months, especially since the PTAC has conflicts in both January and February.

ATTACHMENTS: [Calendar](#)

ACTION: None required, informational item

B. Reminder of Election of Officers at November Meeting

The BAC Bylaws state that the Election of Officers is to take place at the last committee meeting of the year. Therefore, please be prepared to nominate and elect NEW officers at the November 26, 2012 BAC meeting.

ATTACHMENTS: BAC Membership List (Please see Item 5.D.)

ACTION: None required, information item

C. Pinellas Trails, Inc.

The non-profit Pinellas Trails, Inc. holds meetings at the Largo Library, 120 Central Park Drive in Largo. The meetings are open to ALL residents of Pinellas County. Bring a friend! Additional information regarding Pinellas Trails, Inc. can be found at their website: [www.pinellastrails.org](http://www.pinellastrails.org).

ATTACHMENTS: None

ACTION: None required, information item

D. School Transportation and Enhanced Pedestrian Safety (STEPS) Committee

The August 8, 2012 STEPS Summary is attached.

ATTACHMENTS: [STEPS Meeting Summary – August 8, 2012](#)

ACTION: None required

E. Correspondence/Publications/Articles of Interest

["Florida's Rail-Trail Champion Continues to Give, \*Rails to Trails Conservancy\*, 10-2-2012](#)

["Bayway Trail Dead-End is Part of Larger Project," \*Tampa Bay Times\*, September 23, 2012](#)

["Planners to Hire Consultant to Design Trails, \*Suncoast News\*, September 26, 2012](#)

["Mayor Buckhorn Requests Proposals to Create a Tampa Bicycle Sharing Program; \*City of Tampa\*, October 10, 2012](#)

["12 Hours of St. Pete," \*BicycleTimesMag.com\*, Oct/Nov 2012](#)

["Trail Extension Should Fix Overpass to Nowhere," \*Tampa Bay Times\*, October 7, 2012](#)

["To Encourage Biking, Cities Lose the Helmets," \*NY Times Sunday Review\*, September 29, 2012](#)

["Join BikeMi, It's as Easy as Riding a Bike," \*www.bikemi.com\*](#)

["Bicyclists Get Warning and Free Lights for Riding in the Dark," \*baynews9.com\*, October 10, 2012](#)

F. Other

If any member has other business to discuss, they may address it under this item.



**AGENDA  
CITIZENS ADVISORY COMMITTEE**

**THURSDAY, OCTOBER 25, 2012**  
**7:00 P.M.**

**Pinellas County Planning Department Conference Room  
1<sup>st</sup> Floor  
310 Court Street  
Clearwater, FL 33756**

1. **CALL TO ORDER/PLEDGE TO FLAG/INTRODUCTIONS** (7:00-7:03)
2. **APPROVAL OF MINUTES** (7:03-7:05)
  - September 27, 2012
3. **MPO MEETING ACTIONS** (7:05-7:10)
  - October 10, 2012
4. **FY 2012/13 – FY 2016/17 TRANSPORTATION IMPROVEMENT PROGRAM (TIP) AMENDMENTS** (7:10-7:15) – Presenter: Brian Beaty, FDOT Staff
5. **FALL UPDATE OF THE FISCAL YEARS 2012/13 THROUGH 2016/17 TRANSPORTATION IMPROVEMENT PROGRAM (TIP)** (7:15-7:35) – Presenter: Robert Feigel, MPO Staff
6. **BECKETT BRIDGE PROJECT DEVELOPMENT AND ENVIRONMENTAL (PD&E) STUDY** (7:35-8:10) – Presenter: Anne Venables and Jim Phillips, EC Driver and Associates Staff
7. **PINELLAS SUNCOAST TRANSIT AUTHORITY (PSTA) BUS PRESENTATION** (8:10-8:40) – Presenter: Bob Lasher, PSTA
8. **OTHER BUSINESS** (8:40-9:00)
  - A. Follow-up on the CAC's Request for Information About School Buses
  - B. Comments from Committee Members
9. **ADJOURNMENT**

**NEXT MEETING: December 6, 2012**

**CAC AGENDA ITEM 2.**

**APPROVAL OF MINUTES**

The minutes of the September 27, 2012 CAC meeting is attached for the Committee's review and approval.

**ATTACHMENT:** [September 27, 2012](#)

**ACTION:** CAC to approve minutes

CAC: 10/25/12

**CAC AGENDA ITEM 3.**

**MPO ACTIONS**

A staff member will report on actions taken by the MPO Board at their October 10 meeting. Items of particular interest to the CAC will be highlighted.

**ATTACHMENTS:** [October 10, 2012 MPO Newsletter/Action Sheet](#)

**ACTION:** As deemed appropriate based on discussion

CAC: 10/25/12

**CAC AGENDA ITEM 4.**

**FY 2012/13 – FY 2016/17 TRANSPORTATION IMPROVEMENT PROGRAM (TIP) AMENDMENTS**

There are no proposed TIP amendments scheduled for this meeting; however, if the need for a TIP amendment(s) arises following the mailing of the agenda packet, the Florida Department of Transportation (FDOT) will present the proposed amendment(s) under this agenda item.

**ATTACHMENT:** (Any proposed TIP amendment forms will be provided at the meeting)

**ACTION:** CAC to recommend approval of TIP amendments (if any)

CAC: 10/25/12

## CAC AGENDA ITEM 5.

### FALL UPDATE OF THE FISCAL YEARS 2012/13 THROUGH 2016/17 TRANSPORTATION IMPROVEMENT PROGRAM (TIP)

Each year, the Metropolitan Planning Organization (MPO) updates the Transportation Improvement Program (TIP) to incorporate changes in the County and Municipal Work Programs. The TIP contains project descriptions, schedules, and corresponding funding allocations for the 25 local governments of Pinellas County, as well as the Pinellas Suncoast Transit Authority (PSTA), the local airports, and the Port of St. Petersburg. The projects include new construction, reconstruction, capital purchases, and maintenance work associated with roads, sidewalks, trails, transit services, airports, the Port of St. Petersburg, and the Transportation Disadvantaged Program. The TIP also contains the MPO's priority lists of projects for the Transportation Enhancement Program and the Surface Transportation Program as required by law in order to receive state and federal funding.

The annual fall update incorporates the new locally adopted Pinellas County and municipal transportation work programs into the TIP. The new county and municipal work program tables are attached, along with corresponding project maps. Also attached is a table showing the differences between the Pinellas County transportation improvement projects adopted in 2011 and the latest version adopted in 2012.

**ATTACHMENTS:** [Map of Pinellas County Road, Intersection, and Bridge Improvements](#)  
[Map of Pinellas County Intelligent Transportation System/Advanced Transportation Management Systems and Trail Projects](#)  
[Table Comparing Changes to County Project Schedule to Previous Years](#)  
[Pinellas County Capital Improvement Program for Transportation Projects for FY 2012 – 2017](#)  
[Work Program Table of the St. Petersburg-Clearwater International Airport](#)  
[Work Program Table of the Port of St. Petersburg](#)  
[Work Program Table of the Clearwater Airpark](#)  
[Work Program Table of the Albert Whitted Airport](#)  
[Map of Municipal Work Program Projects](#)  
[Municipal Work Program Tables](#)

**ACTION:** CAC to recommend approval of the Fall Update of the TIP  
CAC: 10/25/12

## CAC AGENDA ITEM 6.

### BECKETT BRIDGE PROJECT DEVELOPMENT AND ENVIRONMENT (PD&E) STUDY

Pinellas County, in conjunction with the Florida Department of Transportation (FDOT), is conducting a Project Development and Environment (PD&E) Study to evaluate the removal, rehabilitation, or replacement of the existing Beckett Bridge over Whitcomb Bayou in Tarpon Springs. The study is funded through a Local Agency Program (LAP) agreement with FDOT District 7. Tony Horrnik is the Project Manager for Pinellas County. EC Driver & Associates, Inc. is under contract with Pinellas County to conduct the study.

The study began in January 2011 and is anticipated to be completed in December, 2013. The following alternatives are currently being evaluated:

- No Build
- No Build with Permanent Removal of the Existing Bridge
- Rehabilitation of the Existing Bridge
- Replacement with a New Low-Level Movable Bridge
- Replacement with a New Mid-Level Fixed Bridge

No additional traffic lanes are proposed. All replacement alternatives evaluated will provide one lane of traffic in each direction. In addition, all replacement alternatives considered will be constructed on the same alignment as the existing bridge.

Public input is an important aspect of the study. An Alternatives Public Workshop is anticipated to be held in January 2013. The project team is meeting with local government organizations and small stakeholder groups prior to the Public Workshop to provide opportunity for input.

Ann Venables, the consultant Project Manager, and Jim Phillips, Chief Engineer for EC Driver, will give a brief presentation about the study and discuss the alternative concepts developed to date. Comments and input from the CAC concerning the concepts are welcome.

**ATTACHMENT:**     [Locator Map: Beckett Bridge in Tarpon Springs](#)

**ACTION:**        As deemed appropriate based on discussion

CAC: 10/25/12

**CAC AGENDA ITEM 7.**

**PINELLAS SUNCOAST TRANSIT AUTHORITY (PSTA) BUS PRESENTATION**

At its August 23, 2012 meeting, the CAC requested that MPO staff invite a representative from PSTA to respond to the CAC's questions about hybrid buses, the cost per person per trip, bus capacity versus ridership, and utilizing smaller buses. The CAC's questions relating to school buses and requests about the possibility of integrating school buses and PSTA buses will be addressed in this agenda under Other Business.

Bob Lasher, Manager of Community Relations for PSTA, will give a brief presentation and answer any questions.

**ATTACHMENT:** None

**ACTION:** None required, informational item

CAC: 10/25/12



**CAC AGENDA ITEM 8.**

**OTHER BUSINESS**

**A. Follow-up on the CAC's Request for Information About School Buses**

At its August 23, 2012 meeting, the CAC requested information about the possibility of integrating school buses and PSTA buses. The CAC also raised questions about school bus size, capacity, and cost per person per trip. Mike Burke is the Route and Safety Auditor for Pinellas County Schools and has agreed to answer questions related to school buses.

**C. Comments From Committee Members**

Members may comment on other business and request future agenda items. Comments should not require presentations or extensive Committee discussion.

CAC: 10/25/12

# Beckett Bridge PD& E Study

## Alternatives Presentation to:

### MPO Board, Technical and Advisory Committees



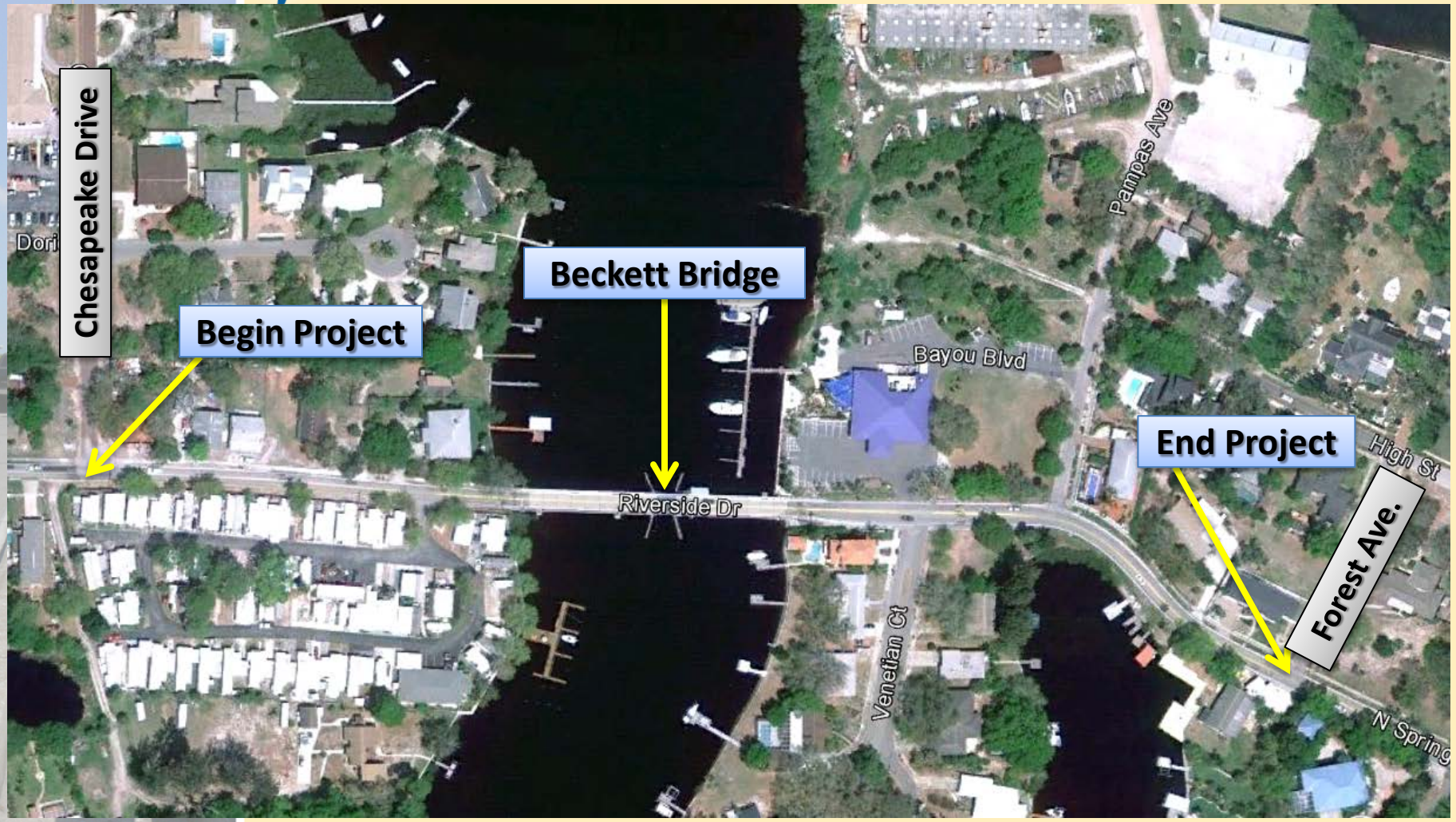
**ECDriver**  
& Associates, Inc.

**URS**

**JANUS**  
RESEARCH

October - November 2012







# Existing Bridge

- Determined to be Eligible for listing in the National Register of Historic Places
- Original Timber Construction – 1924
- Rebuilt Concrete Bridge – 1956
  - Major Repairs in 1979, 1998 and 2011
- Sufficiency Rating - 44.9 (Scale of 1 -100)
  - Structural Concerns
  - Functionally Obsolete

- Vertical Clearance – 6 ft
- Horizontal Clearance – 25 ft
- Opens with 2-hr Notice

**Total Bridge  
Openings**

**2009 - 10**

**2010 - 20**

**2011 - 18**



- **Structural Issues**
  - Posted Weight Restrictions (12 tons)
  - Foundations susceptible to settlement
  - Scour Susceptible



- **Functionally Obsolete**
  - **Narrow Sidewalks**
    - Do Not Meet ADA Requirements



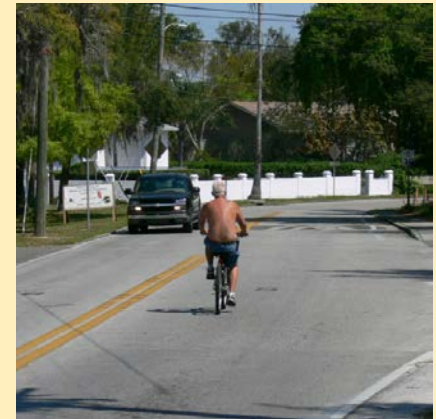
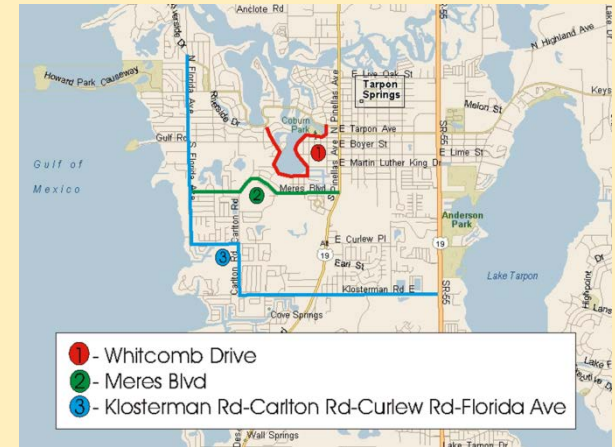
- **Narrow Lanes**
  - No Shoulders
  - No bicycle lanes





- **Engineering, Social & Environmental Studies**
- **Community Involvement**
- **Develop and Evaluate Alternatives**
- **Select Preferred Alternative**
- **Obtain FHWA Approval**

- **Community Concerns**
  - Vertical Clearance
  - Noise
  - Construction Impacts
  - Detour/MOT
  - Aesthetics
  - Bicycle/Pedestrian Facilities



- Project Website
- Coordinate with Local Government
- Meet with Stakeholder Groups
- Alternatives Public Workshop
- Public Hearing





- No-Build
- No-Build with Permanent Removal of Existing Bridge
- Rehabilitation
- Replacement on Existing Alignment
  - Low-Level Movable Bridge
    - 7.8 ft Vertical Clearance
    - 25 ft Horizontal Clearance
  - Mid-Level Fixed Bridge
    - 28 ft Vertical Clearance
    - 25 ft Horizontal Clearance



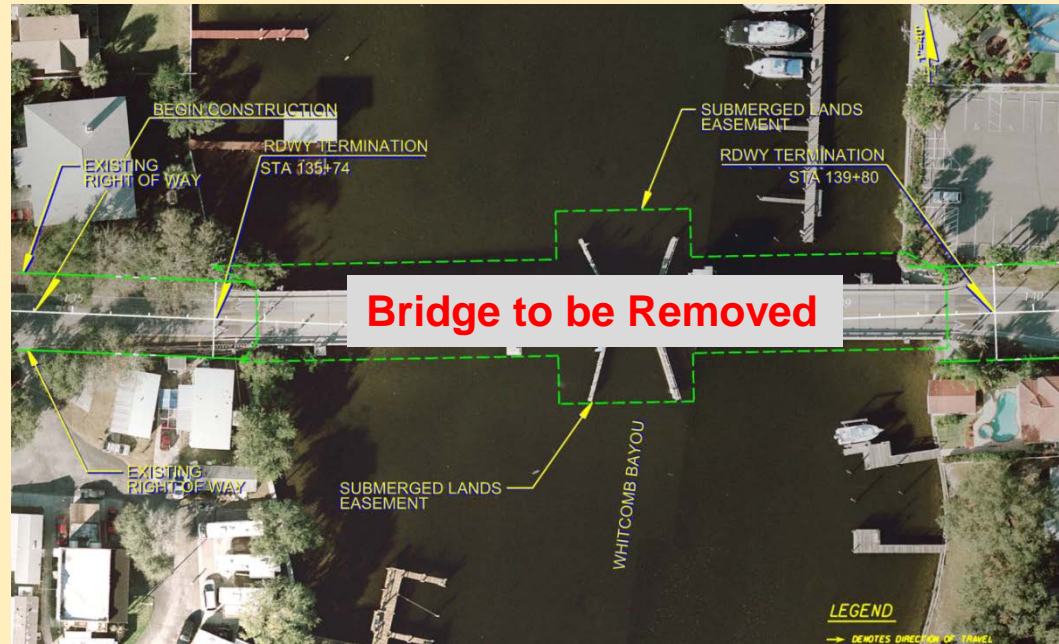
# Pinellas County No Build Alternative

- Existing Bridge Remains Until No Longer Serviceable (approximately 10 years)
- Routine Maintenance Only
- No Major Improvements



# No Build Alternative – Permanent Removal of Existing Bridge

- Routine Maintenance Only
- Existing Bridge Demolished When No Longer Serviceable
- No Replacement Bridge Constructed



- Extensive Repairs
- Correct Structural Deficiencies
- No Widening
  - No Shoulders
  - Narrow Sidewalks would remain
- No Change in Navigational Clearances
- Extend Service Life 25-30 years



# Replacement Alternatives Constraints

- **Limited Right-of-Way**
  - 50 ft ROW East of Bridge
  - 40 ft ROW West of Bridge

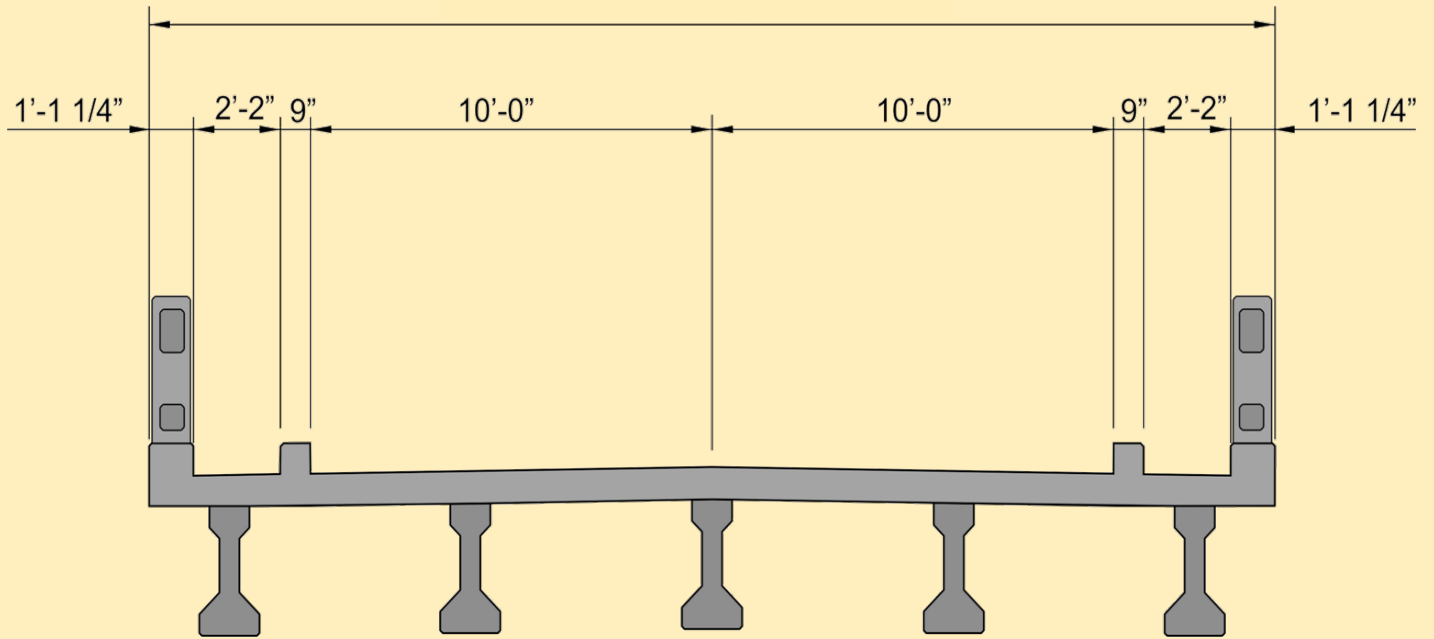


- **Adjacent properties**
- **Driveways**



## Existing Bridge Typical Section

28'-0 1/2" Total Width



**No Shoulders**

**Narrow Sidewalks**

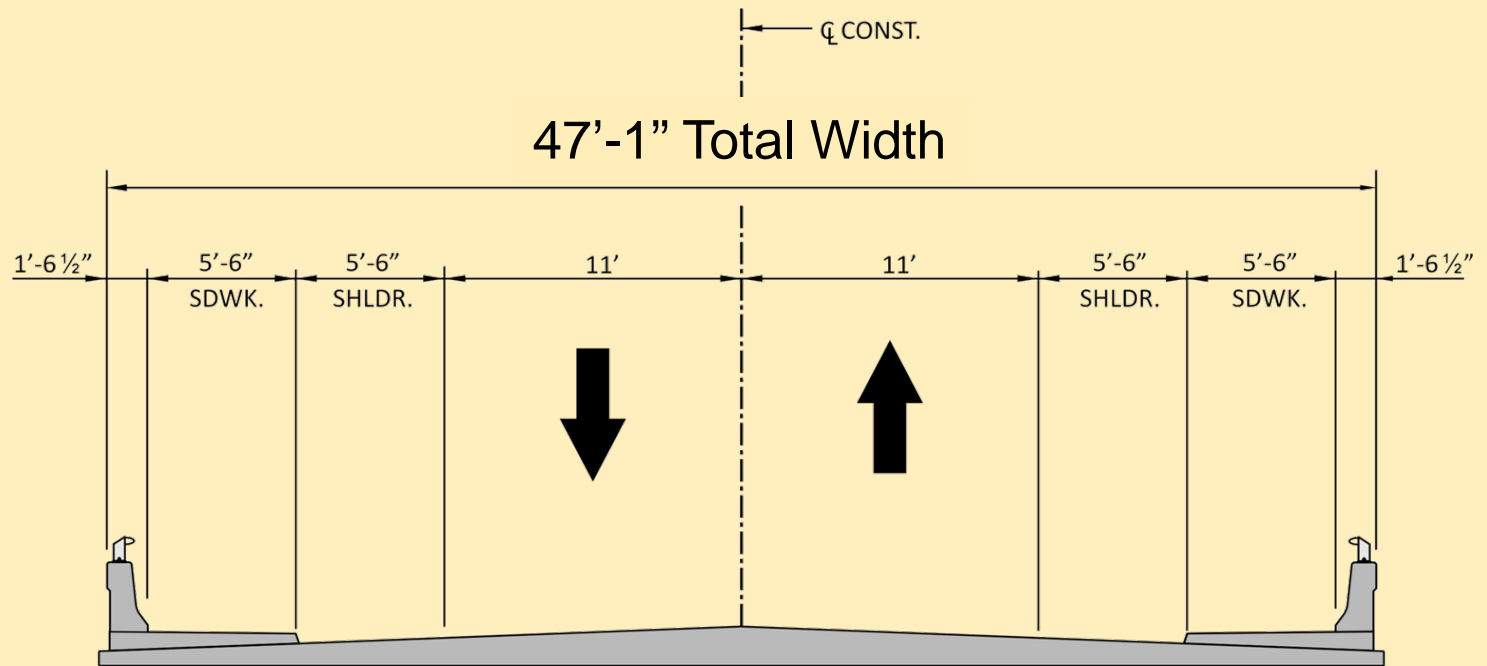


# Proposed Typical Sections

- **Meet Current Safety Standards**
- **Accommodate Future Trails**
- **Minimize ROW Impacts**
- **Lane Widths**  
10 to 11 feet

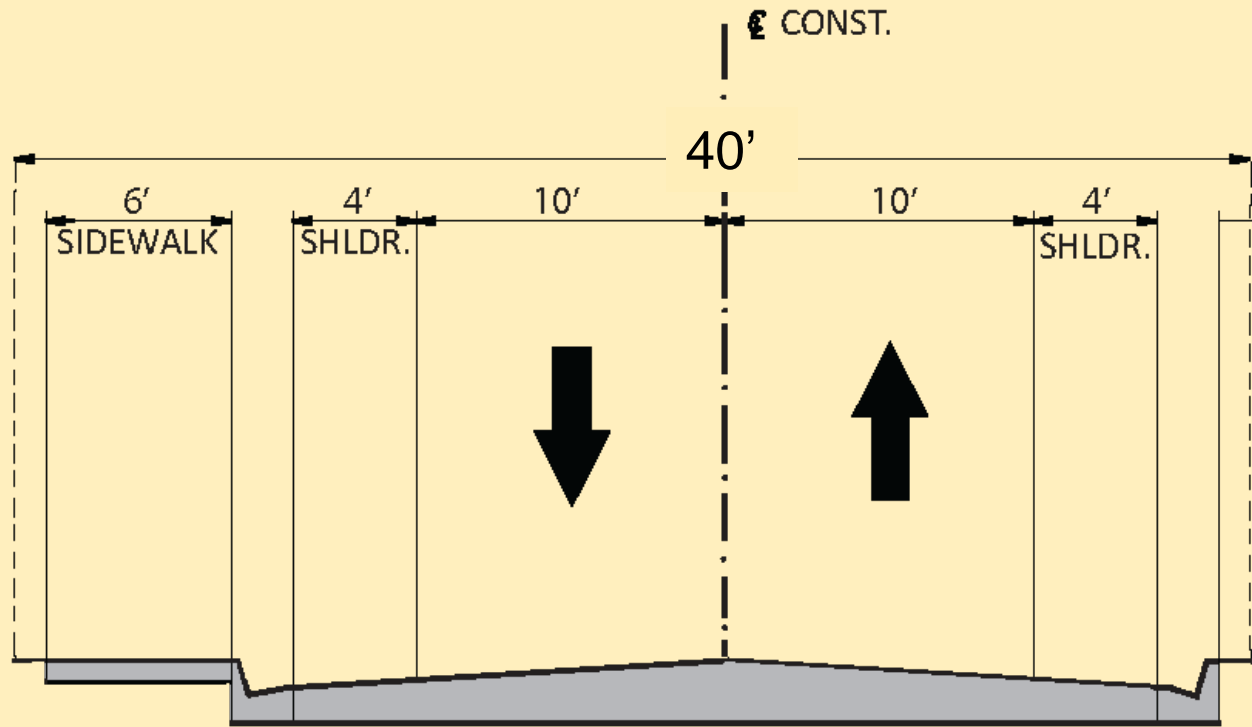


## Proposed Bridge Typical Section



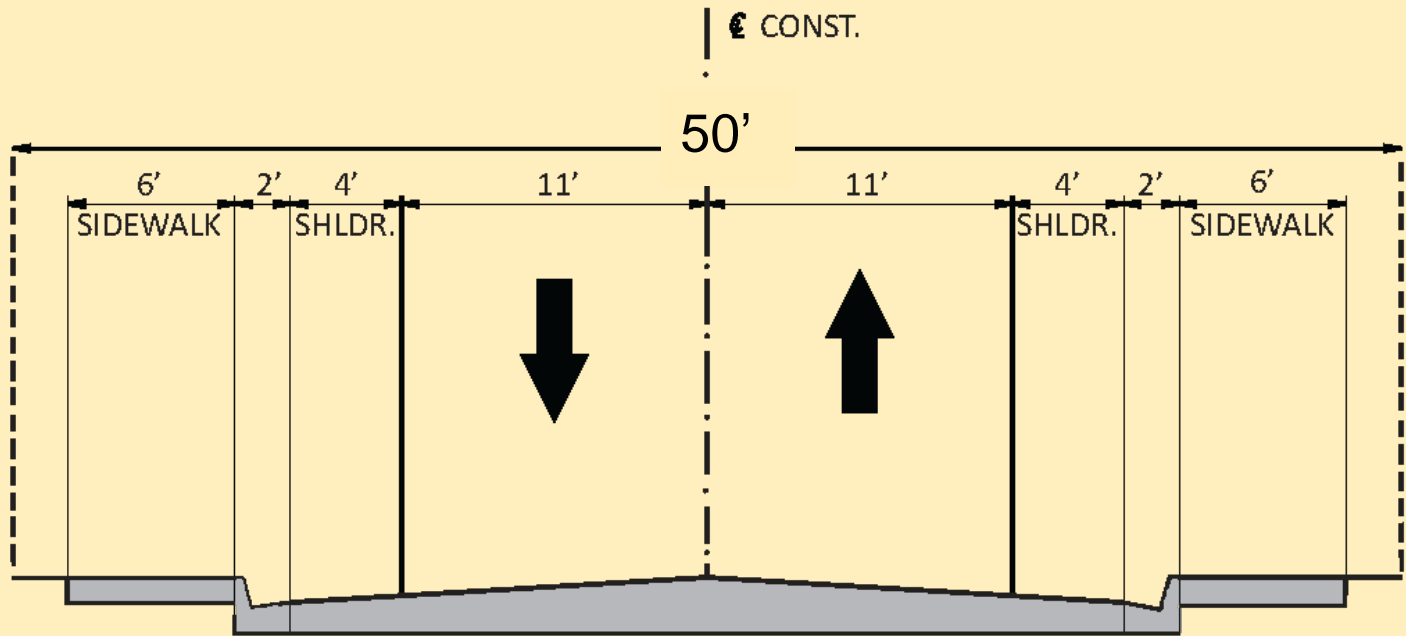
**5'6" Sidewalks and Shoulders – Both Sides**

## Roadway Section – West of Bridge



**6 ft Sidewalk - North Side Only**

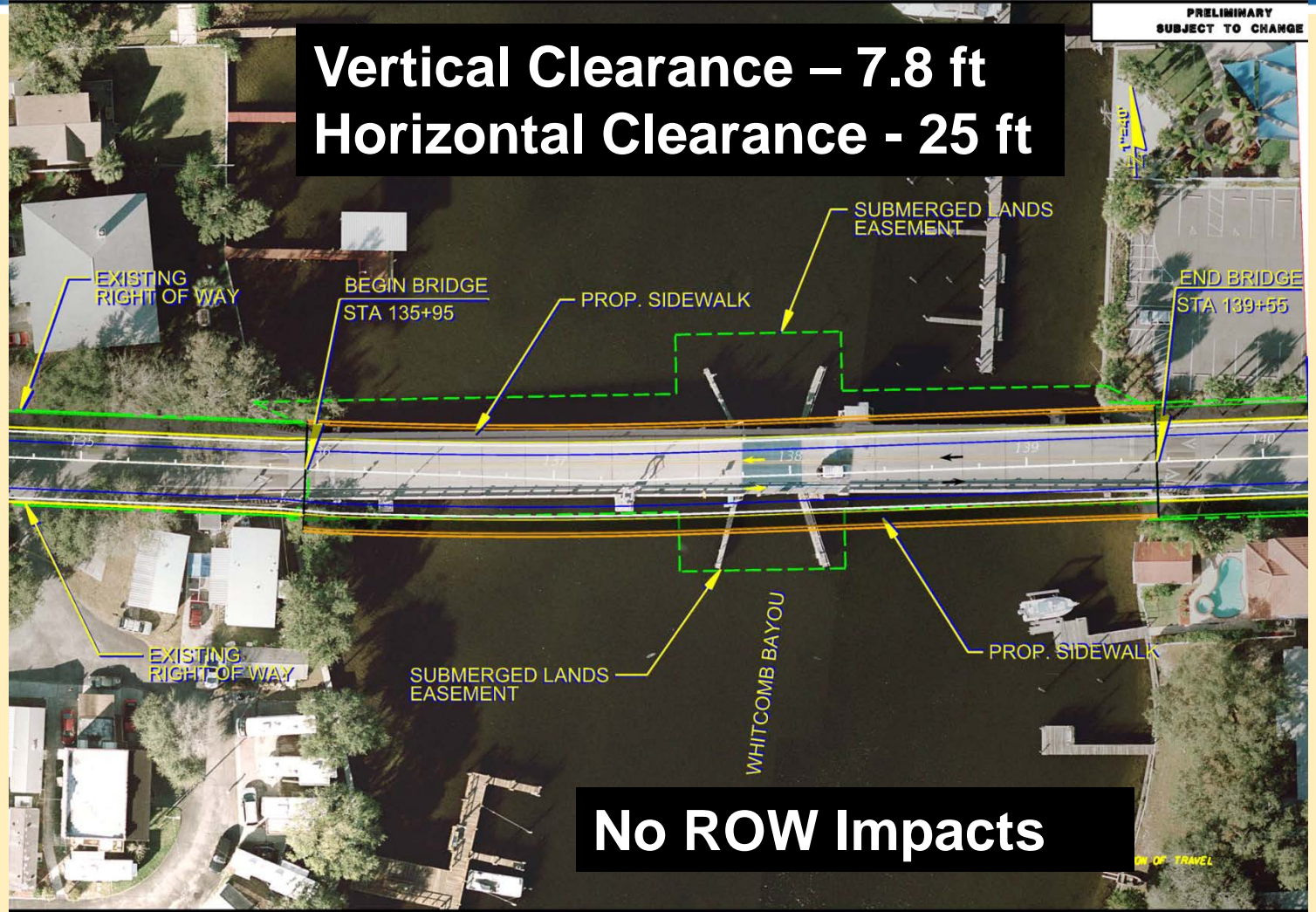
## Roadway Section – East of Bridge



**6 ft Sidewalks - Both Sides**

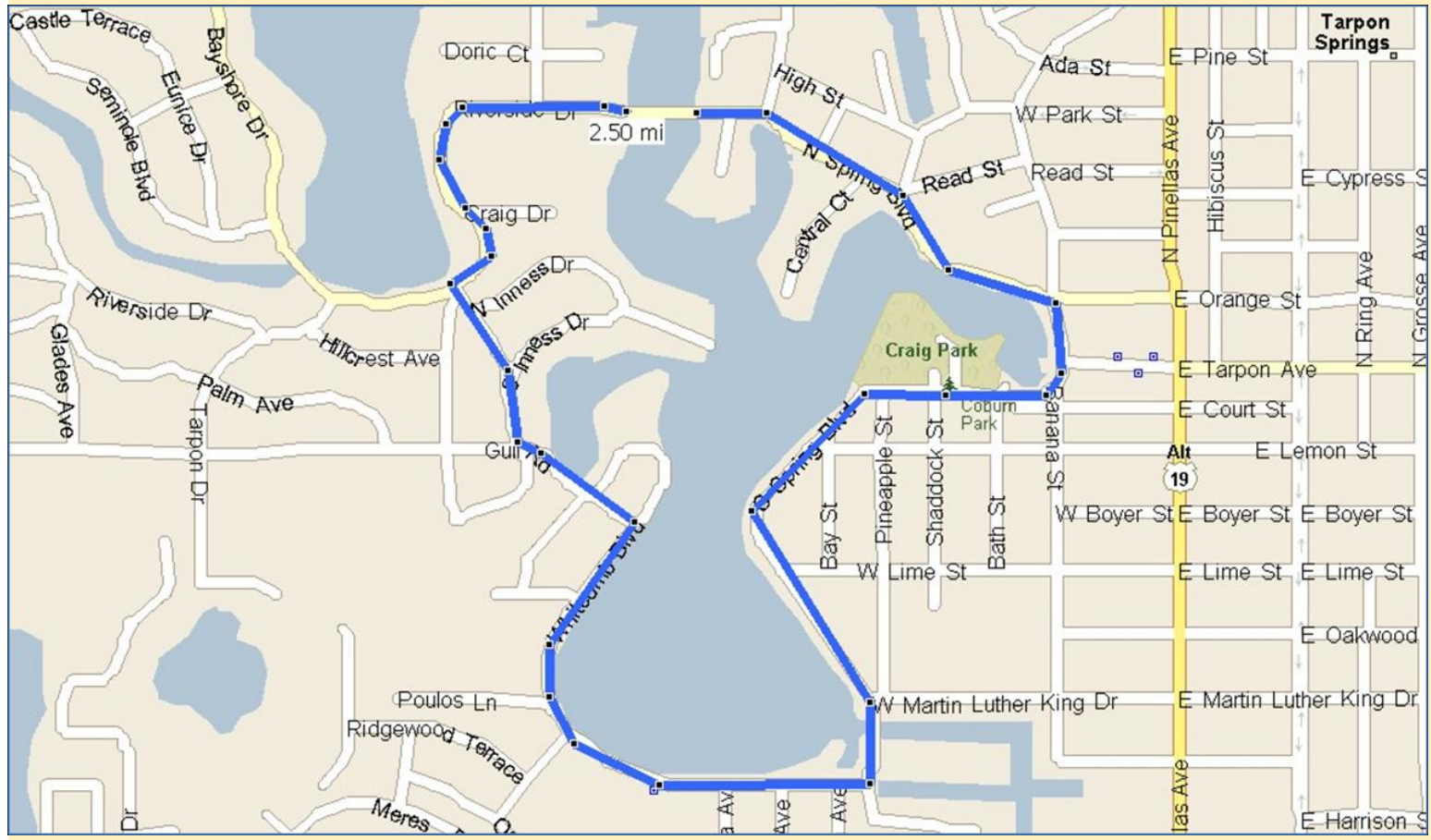


# Low-Level Movable Bridge



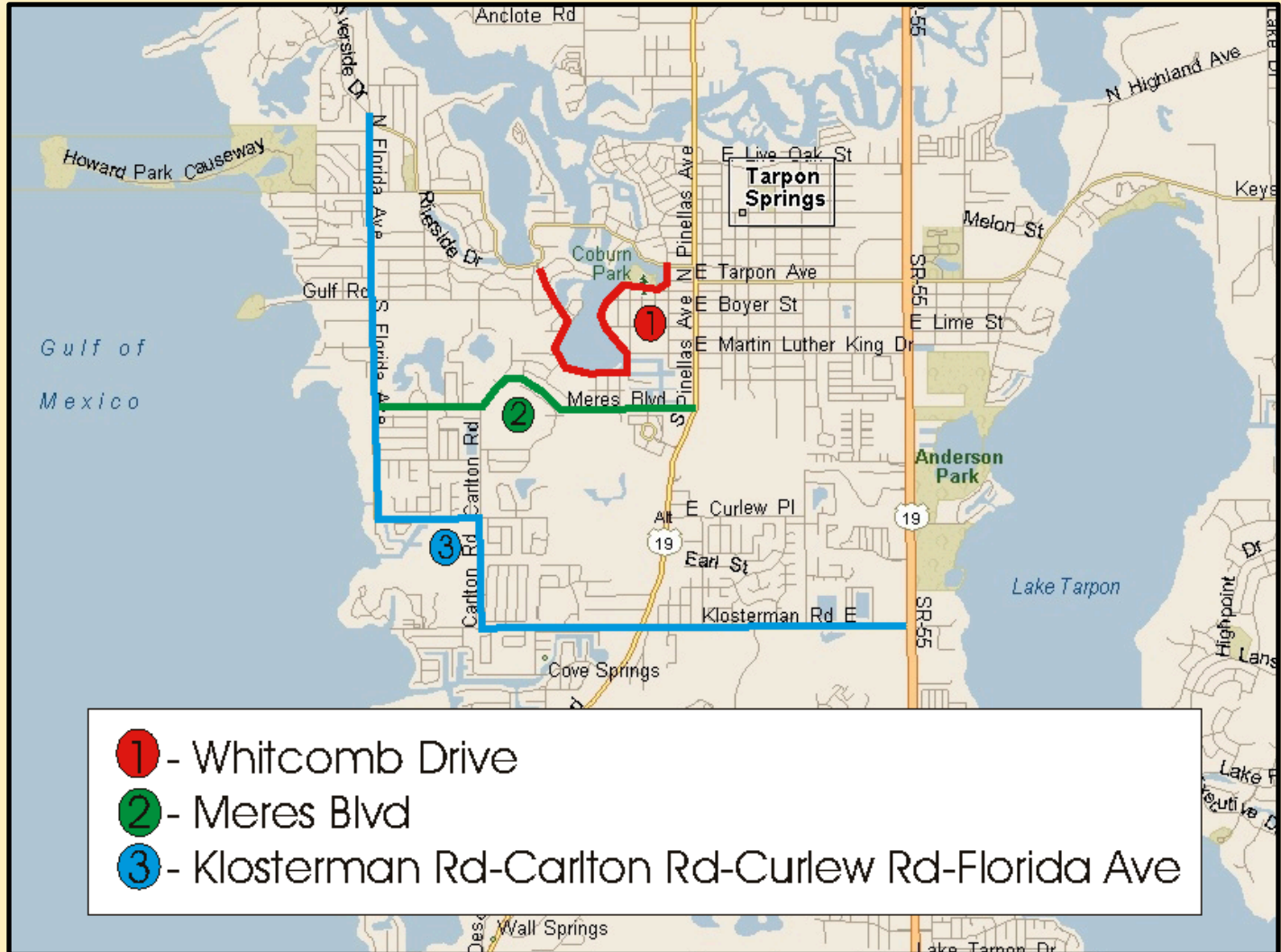


# Previous Detour Route





# Alternate Detour Routes







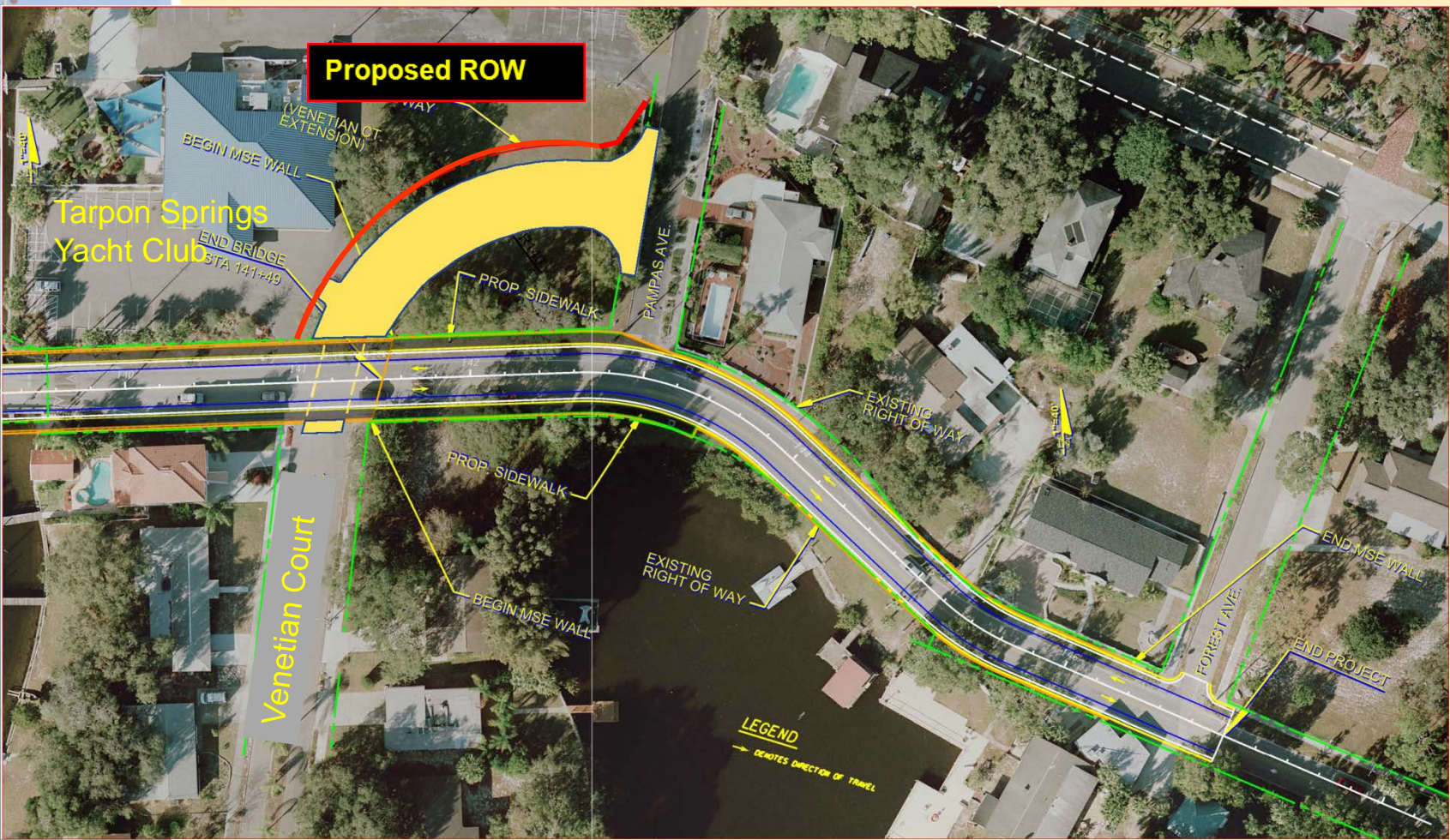
# Mid-Level Fixed Bridge – Option A

- **Vertical Clearance – 28 feet**
- **Horizontal Clearance – 25 feet**
- **Retaining Wall 1 to 19 feet High**
  - **Blocks Driveway Access/Visual Impacts**
- **ROW impacts**
  - **Single Family Residences North Side, West of Bridge**
  - **Yacht Club Property**



# Mid-Level Fixed Bridge – Option A

## Venetian Court Extension





# Mid-Level Fixed Bridge – Option A

## Access Road to Bayshore Mobile Home Park



**Impacts to Residences North of Riverside Drive**



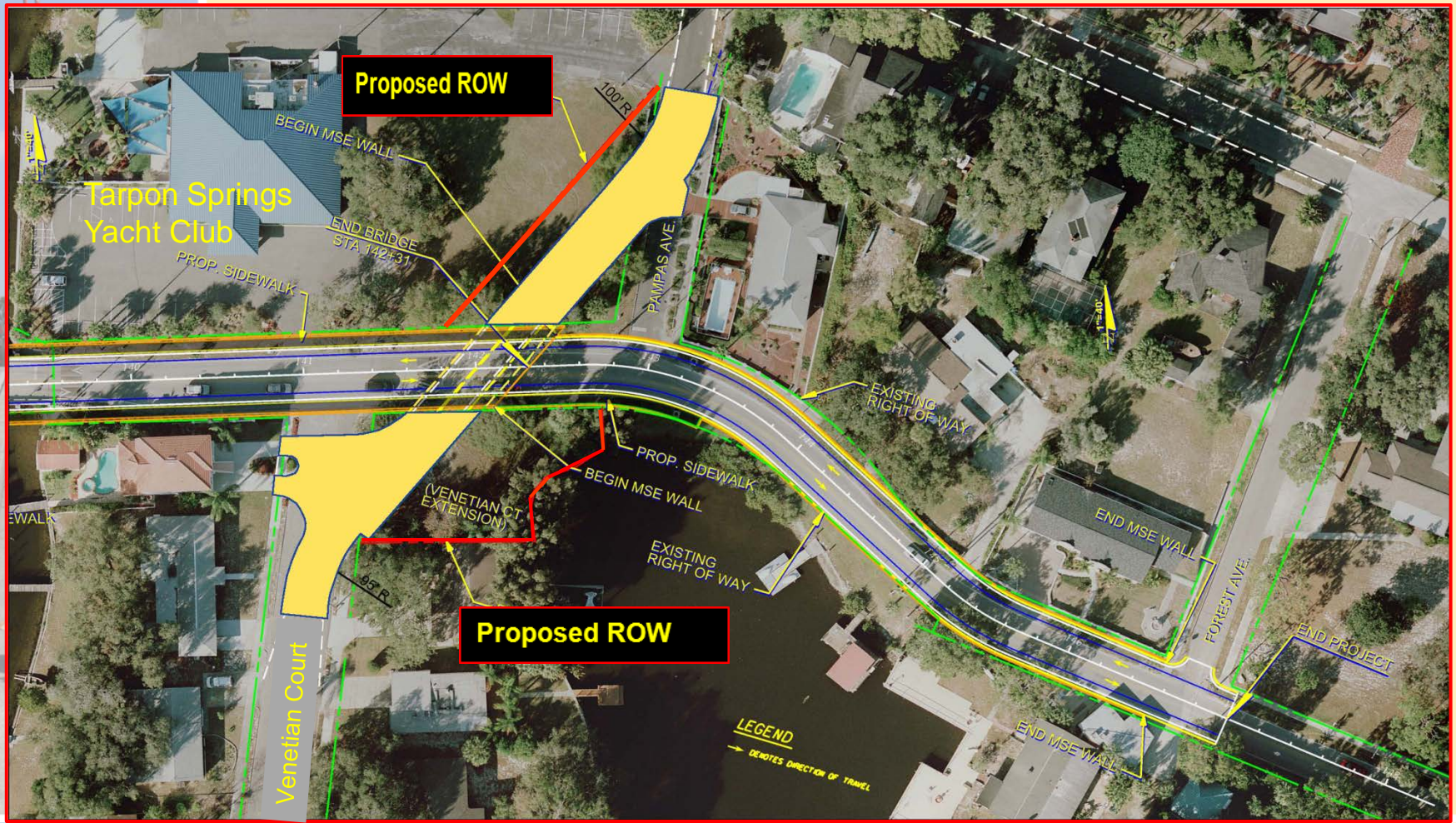
- **Vertical Clearance – 28 feet**
- **Horizontal Clearance – 25 feet**
- **Retaining Wall 1 to 16 feet High**
  - **Blocks Driveway Access/Visual Impacts**
- **ROW impacts**
  - **Single Family Residences North Side,  
West of Bridge**
  - **8 Mobile Homes**
  - **Yacht Club Property**
  - **Vacant Lot East of Bridge, South Side of Roadway**





# Mid-Level Fixed Bridge – Option B

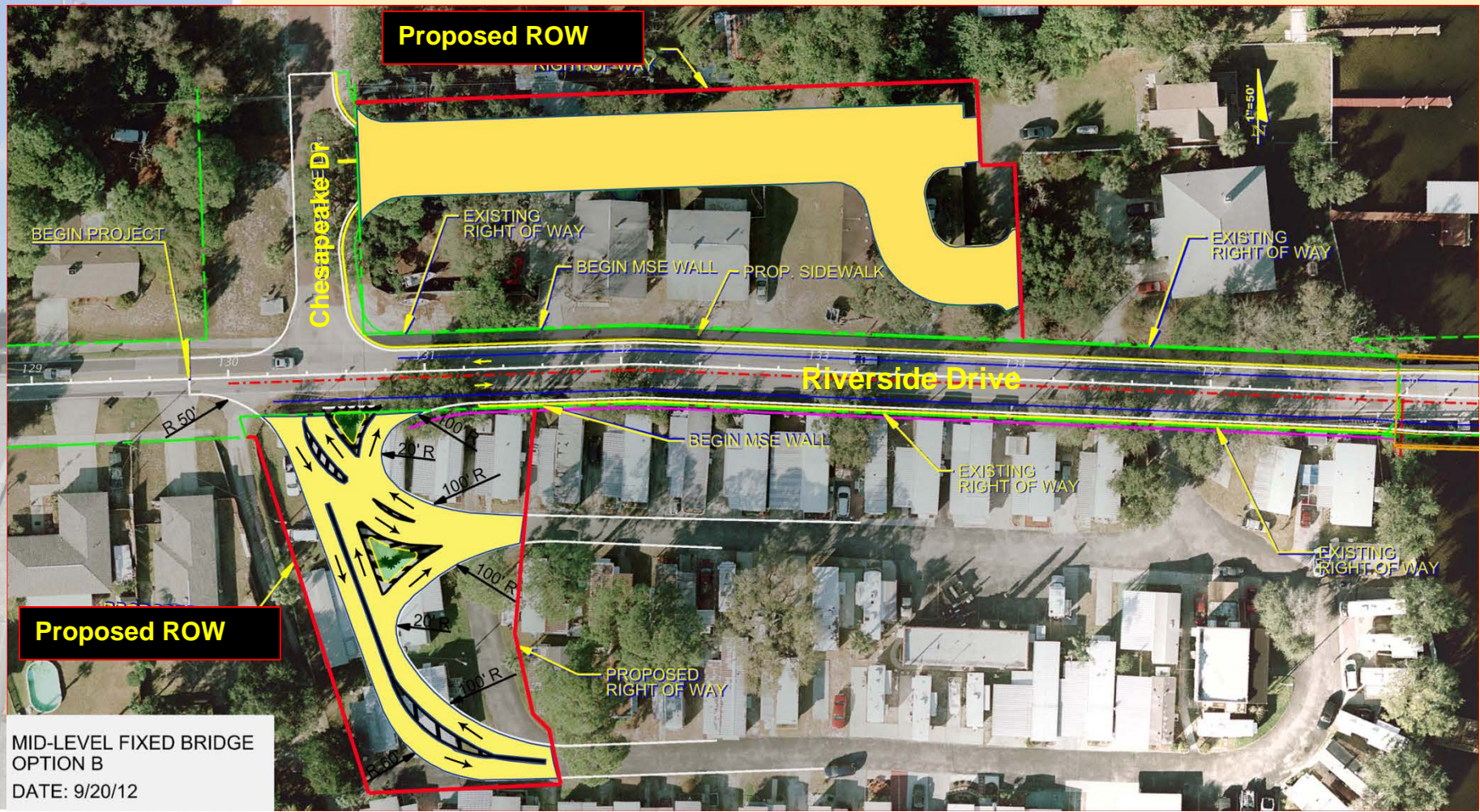
## Venetian Court Extension





# Mid-Level Fixed Bridge – Option B

## Alternate Access to MHP and Waterfront Properties



MID-LEVEL FIXED BRIDGE  
OPTION B  
DATE: 9/20/12



## Boat Survey of Waterfront Property Owners on Whitcomb Bayou

- 289 Surveyed
- 99 Responded



## Results

- Most Boats 22-26 ft Powerboats
  - No Bridge Opening Required
- 4 Sailboats required  $\geq 28$  ft of Clearance
  - Could not Pass Under Mid-Level Option

## Similar for All Build Alternatives

- **Wetlands**
  - 0.02 acre – Low-Level
  - 0.03 acre – Mid-Level
  - Wildlife - Minimal
- **Noise - Minimal**
- **Visual – Minimal to High**
- **Historic Structures**
  - Removal of NRHP Eligible Bridge Requires MOA/Mitigation





# Upcoming Community Involvement Activities

- 
- **Local Government Presentations**
    - City of Tarpon Springs Commission (Nov 20)
    - Pinellas MPO TCC and CAC (Oct 24, 25)
    - Pinellas MPO (Nov 14)
  - **Stakeholder Meetings\***
    - Tarpon Springs Yacht Club
    - Rotary Club
    - Chamber of Commerce
    - Bayshore Mobile Home Park

\*No Date Scheduled
  - **Cultural Resource Committee Meeting** (Oct 29)



# PD&E Schedule

Activity	2012				2013				
	Jan - Mar	Apr - Jun	Jul - Sep	Oct - Dec	Jan - Mar	Apr - Jun	Jul - Aug	Sep - Oct	Nov - Dec
Study Begins	★								
Data Collection	█								
Develop/Evaluate Alternatives	█								
Local Government Coordination			█						
Alternatives Workshop					★				
Select Preferred Alternative					█				
Public Hearing							★		
Final Reports/Agency Coordination					█				
Approval by the Federal Highway Administration									★



**Questions?**





**PEDESTRIAN TRANSPORTATION ADVISORY COMMITTEE MEETING**

**MONDAY  
OCTOBER 15, 2012  
8:30 A.M.**

**PINELLAS COUNTY PLANNING DEPARTMENT  
CONFERENCE ROOM – 1<sup>ST</sup> FLOOR  
310 COURT STREET  
CLEARWATER, FLORIDA 33756**

**AGENDA**

- 1. CALL TO ORDER AND INTRODUCTIONS**
- 2. APPROVAL OF MINUTES – September 17, 2012**
- 3. MPO ACTIONS – October 10, 2012**
- 4. PRESENTATIONS:**
  - A. WalkWise Program Update – Mr. Jason Jackman, CUTR
  - B. Beckett Bridge Presentation – Ms. Ann Venables and Mr. Jim Phillips, EC Driver & Associates
- 5. PEDESTRIAN SAFETY AWARENESS WEEK, OCTOBER 29 through NOVEMBER 2, 2012**
- 6. MADONNA BOULEVARD/PINELLAS BAYWAY INTERSECTION PEDESTRIAN SAFETY**
- 7. COMMITTEE REPORTS**
  - A. Pinellas County Health Department
    - Communities Putting Prevention to Work (CPPW)
    - Florida Injury Prevention Advisory Council (FIPAC)
  - B. School Transportation Safety Committee (STSC) and School Collaborative Meetings
  - C. School Transportation and Enhanced Pedestrian Safety (STEPS)
  - D. Community Traffic Safety Team (CTST)
  - E. Safe Kids Coalition
- 8. OTHER BUSINESS**
  - A. U.S. Highway 19 Resurfacing Project (FPN#429005-1)
  - B. Correspondence/Publications/Articles of Interest
  - C. Membership
  - D. Other
- 9. ADJOURNMENT**

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**NEXT PTAC MEETING**  
**DECEMBER 17, 2012**

**APPROVAL OF MINUTES – September 17, 2012**

The minutes of the September 17, 2012 meeting are attached for review and approval.

ATTACHMENTS: [PTAC Minutes – September 17, 2012](#)

ACTION: Approval of Minutes

**MPO ACTIONS – October 10, 2012**

A staff member will report on action taken by the MPO Board at the October 10, 2012 MPO meeting. Copies of the Newsletter/Action Sheet for that meeting will be distributed at the meeting.

ATTACHMENTS: Newsletter/Action Sheet – October 10, 2012 (to be distributed at meeting)

ACTION: As deemed appropriate based on discussion

**PRESENTATIONS**

A. WalkWise Program Update – Mr. Jason Jackman, CUTR

WalkWise Tampa Bay is a grassroots effort to educate the citizens of Hillsborough, Pinellas, and Pasco Counties regarding the importance of pedestrian safety. This initiative provides innovative pedestrian safety education to the citizens of Tampa Bay.

The WalkWise campaign is funded by the Florida Department of Transportation (FDOT) District 7. The Center for Urban Transportation Research (CUTR) at the University of South Florida manages the program and provides free public safety education to anyone living or working in the Tampa Bay area. A press conference and pedestrian safety event is scheduled for Thursday, October 25, 2012, 9:00 a.m. at Archibald Beach Park Pavilion in Madeira Beach for the beach communities.

Please welcome Mr. Jason Jackman, Program Planner Analyst at CUTR and public educator for WalkWise Tampa Bay, as he provides an update for the Committee.

ATTACHMENTS: None

ACTION: As deemed appropriate based on discussion

**PRESENTATIONS**

B. Beckett Bridge PD&E Study – Ms. Ann Venables and Mr. Jim Phillips, EC Driver & Associates

Pinellas County, in conjunction with the Florida Department of Transportation (FDOT), is conducting a Project Development and Environment (PD&E) Study to evaluate the removal, rehabilitation or replacement of the existing Beckett Bridge over Whitcomb Bayou in Tarpon Springs. The study is funded through a Local Agency Program (LAP) agreement with FDOT District 7. Mr. Tony Hornik is the Project Manager for Pinellas County. EC Driver & Assoc., Inc. is under contract with Pinellas County to conduct the study.

The study began in January 2011 and is anticipated to be completed in December, 2013. The following alternatives are currently being evaluated:

- No Build;
- No Build with Permanent Removal of the Existing Bridge;
- Rehabilitation of the Existing Bridge;
- Replacement with a New Low-Level Movable Bridge; and
- Replacement with a New Mid-Level Fixed Bridge.

No additional traffic lanes are proposed. All replacement alternatives evaluated will provide one lane of traffic in each direction. In addition, all replacement alternatives considered will be constructed on the same alignment as the existing bridge.

Public input is an important aspect of the study. An Alternatives Public Workshop is anticipated to be held in January 2013. The project team is meeting with local government organizations and small stakeholder groups prior to the Public Workshop to provide opportunity for input.

Ms. Ann Venables, the consultant project manager and Mr. Jim Phillips, Chief Engineer for EC Driver, will make a brief presentation consisting of a power point presentation about the study and discuss the alternative concepts developed to date. Comments and input from the Advisory Committee concerning the concepts are welcome.

ATTACHMENTS: [Location Map, Beckett Bridge in Tarpon Springs](#)

ACTION: As deemed appropriate based on discussion



**PEDESTRIAN SAFETY AWARENESS WEEK, OCTOBER 29 through NOVEMBER 2, 2012**

Upon PTAC recommendation over the years, the Pinellas County MPO has established the Monday following Daylight Saving Time as Pedestrian Safety Awareness Day, providing an opportunity to highlight pedestrian safety, education, and awareness. Designed to bring attention to the need for a more conscious effort toward pedestrian safety by both pedestrians and motorists, the MPO launches this annual safety campaign to remind pedestrians and motorists of safety measures. The event is also intended to promote safety for pedestrians at all intersection crosswalks throughout Pinellas County, recognizing that pedestrians have legal right-of-way within crosswalks.

At its September 17, 2012 meeting, the Pedestrian Transportation Advisory Committee (PTAC) recommended that an entire week be devoted to pedestrian safety awareness in order to emphasize the importance of pedestrian safety during this unique time of year. Statistics have shown that the end of October is a particularly challenging time of year for pedestrians in light of both the time change and the occurrence of Halloween.

As part of this safety project, 135,000 pedestrian safety brochures targeting students and their families are distributed across Pinellas County to all public and private schools, libraries, senior centers, and municipalities. This simple but direct safety information was developed to emphasize safety for all modes of transportation and to remind drivers, cyclists, and pedestrians that they can safely share our roadways if they exercise caution and increased safety awareness, especially at intersections, crosswalks, and mid-block crossing areas. Although this material targets school students, the safety information is beneficial for pedestrians, cyclists, and motor-vehicle drivers of all ages.

The PTAC recommended the MPO recognize Pedestrian Safety Awareness Week from Monday, October 29, through Friday, November 2, 2012. The draft 2012 Pedestrian Safety Awareness Week MPO Resolution, sent to the MPO for approval at its October 10, 2012 meeting, is attached for your information.

ATTACHMENTS: [Draft 2012 Pedestrian Safety Awareness Week Resolution](#)

ACTION: As deemed appropriate based on discussion

**MADONNA BOULEVARD /PINELLAS BAYWAY INTERSECTION PEDESTRIAN SAFETY**

At its September 17, 2012 meeting, the Pedestrian Transportation Advisory Committee (PTAC) discussed crashes that have been occurring at the intersection of Madonna Boulevard and the Pinellas Bayway/S.R. 679 in Tierra Verde. The Committee initially reviewed safety concerns in this area in October 2009 because of the high number of motor vehicle crashes as well as safety for pedestrians crossing the Pinellas Bayway/S.R. 679. At that meeting, the PTAC agreed to send a memo to the MPO recommending that FDOT conduct a pedestrian safety review and consider low cost improvements such as striping and signs for a safe pedestrian crossing at this intersection.

The MPO accepted that recommendation, dated November 2, 2009, and FDOT provided an update in August 2010 listing the pavement markings, signs, curb/gutter, curb ramp upgrades, and a directional median opening designed to reduce the vehicular conflicts in the area. Pedestrian crosswalks with curb ramps, truncated domes, sidewalk through the median, signs and push-button flashers across the Pinellas Bayway at two locations were also added.

A request to consider use of a Rectangular Rapid Flashing Beacon (RRFB) at this location was included in the November 2009 PTAC recommendation. As the RRFB continues to exhibit a very high percentage of motorist compliance, and provides pedestrians extra safety while crossing the road, the Committee requested that Madonna Boulevard pedestrian safety be discussed at the October 2012 meeting.

ATTACHMENTS: [Memo dated November 2, 2009 to MPO Chairman Arbutine from PTAC Chairman Michaels](#)

ACTION: As deemed appropriate based on discussion

**COMMITTEE REPORTS**

A. Pinellas County Health Department Programs

Communities Putting Prevention to Work (CPPW) – Across the U.S. the obesity rate has continued to increase over the last several years. In response, the Center for Disease Control (CDC) has created the Communities Putting Prevention to Work (CPPW) Program. The CPPW Program targets policies that encourage unhealthy behaviors that result in obesity and obesity-related illnesses, which ends up costing approximately \$147 billion per year (2008 annual healthcare costs).

The Pinellas County Health Department was awarded a \$4.85 million CPPW grant to reduce obesity, increase physical activity and improve nutrition through policy changes and the built environment. The six target behaviors are increased physical activity, consumption of fruits/vegetables, breastfeeding initiatives and decreased consumption of sugar, nutrient-poor food, and television viewing.

Florida Injury Prevention Advisory Council (FIPAC) – Injuries are the leading cause of death among Floridians between the ages of 1 and 44. After heart disease and cancer, it is the third cause of death for Floridians of any age. According to the Agency for Health Care Administration ([www.fdhc.state.fl.us](http://www.fdhc.state.fl.us)), injury-related hospital charges in 2007 totaled more than \$5 billion, over \$2.6 billion of which was paid by the federal or state government (through Medicare and Medicaid).

The Florida Injury Prevention Advisory Council (FIPAC), comprised of appointed council members and many volunteers, meets a few times per year to develop strategies to achieve the goal of reducing injuries/fatalities. The five goals are: (1) Infrastructure (including: leadership, funding, data, policy, and evaluation), (2) Collaboration (including injury prevention efforts in: traffic safety, poisonings, interpersonal violence, suicide, child maltreatment, and other injuries), (3) Early Childhood Water Safety and Drowning Prevention, (4) Senior Falls Prevention, and (5) Training.

ATTACHMENTS: None

ACTION: None required, informational item

B. School Transportation Safety Committee (STSC) & Pinellas Schools Collaborative

The STSC did not schedule meetings in October or November. The School Collaborative has also not met for a number of months and the November meeting has been cancelled.

ATTACHMENTS: None

ACTION: None required, informational item

C. School Transportation and Enhanced Pedestrian Safety (STEPS)

The STEPS Committee has provided the draft meeting summary for the August 8, 2012 meeting which is attached.

ATTACHMENTS: [STEPS Committee Meeting Summary – August 8, 2012](#)

ACTION: None required, informational item

(Continued)

**PTAC – ITEM 7. A. – E. (Continued)**

D. Community Traffic Safety Team (CTST)

The October 4, 2012 CTST agenda is attached. The September 6, 2012 CTST minutes are also attached.

ATTACHMENTS: [CTST Agenda – October 4, 2012](#)  
[CTST Minutes – September 6, 2012](#)

ACTION: None required, information item

E. Safe Kids Coalition

No Safe Kids Coalition Meeting information was received. It will be provided at the meeting if made available.

**OTHER BUSINESS**

A. U.S. Highway 19 Resurfacing Project (FPN#429005-1)

The design phase has been scheduled for the resurfacing project on U.S. Highway 19 between Countryside Boulevard (just south of S.R. 580) and East Live Oak Street in Tarpon Springs and will include replacement of any damaged sidewalks, and required Americans with Disabilities Act (ADA) upgrades. FDOT has estimated the cost of the design plans to be about \$2.7 million. The PTAC has agreed that this project is an important opportunity to insure that pedestrian accommodations and safety be addressed along this corridor.

Updated information will continue to be provided to the Committee as it becomes available.

ATTACHMENTS: None  
ACTION: None required

B. Correspondence/Publications/Articles of Interest

["Mayor Foster, the Rays and Jabil Join Others to Promote Campbell Park Walking School Bus," City of St. Petersburg, 10-1-12](#)

[Look Out ! DOT creates Crosswalk Decals, Ad Campaign to Prevent Pedestrian Accidents, NY Observer, 9-19-12](#)

[Memo dated October 2, 2012 from Jean Shoemaker Re 2012 International Walk to School Day](#)

[Walk to School Day, Pinellas County Participating Schools 2012](#)

C. Membership

There are currently two vacancies in the At Large Citizen category, as well as vacancies for one Mid-County Citizen Representative, one North County Citizen Representative, and one Pinellas County Government Representative. The Committee is also in need of representatives from the Pinellas County Sheriff's Office and the Clearwater and Largo Police Departments.

ATTACHMENTS: [PTAC Membership List](#)  
ACTION: As deemed appropriate based on discussion

D. Other

If any member has an item of other business, they may address it at this time.



**TECHNICAL COORDINATING COMMITTEE MEETING**

**WEDNESDAY  
OCTOBER 24, 2012  
2:00 P.M.**

**PINELLAS COUNTY PLANNING DEPARTMENT  
CONFERENCE ROOM – FIRST FLOOR  
310 COURT STREET  
CLEARWATER, FLORIDA 33756**

**AGENDA**

1. **CALL TO ORDER**
2. **APPROVAL OF MINUTES – September 26, 2012**
3. **FY 2012/13 – FY 2016/17 TRANSPORTATION IMPROVEMENT PROGRAM (TIP) AMENDMENT(S)**
4. **BECKETT BRIDGE PD&E STUDY – MS. ANN VENABLES AND MR. JIM PHILLIPS, EC DRIVER & ASSOCIATES**
5. **PINELLAS COUNTY SOCIOECONOMIC DATA**
6. **FALL UPDATE OF FISCAL YEARS 2012/13 THROUGH 2016/17 TRANSPORTATION IMPROVEMENT PROGRAM (TIP)**
7. **2040 LONG RANGE TRANSPORTATION PLAN UPDATE – DRAFT PLANNING ASSUMPTIONS TECH MEMO**
8. **TECHNICAL COORDINATING COMMITTEE REVIEW OF ADVISORY COMMITTEE RECOMMENDATIONS**
9. **OTHER BUSINESS**
  - A. MPO Bicycle Pedestrian Master Plan Crash Data Report
  - B. TCC Meeting Schedule for Remainder of 2012
  - C. Reminder of Election of Officers at December 5, 2012 TCC Meeting
  - D. Mobility Plan Update
  - E. Uniform Trail Signage Treatments Update
  - F. Other
10. **ADJOURNMENT**

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**NEXT TCC MEETING – DECEMBER 5, 2012**

**APPROVAL OF MINUTES – September 26, 2012**

The minutes of the September 26, 2012 TCC meeting are attached for your review and approval.

ATTACHMENTS: [TCC Minutes – September 26, 2012](#)

ACTION: Approval of Minutes

**FY 2012/13 – FY 2016/17 TRANSPORTATION IMPROVEMENT PROGRAM (TIP) AMENDMENT(S)**

There are no proposed TIP amendments scheduled for this meeting. However, if the need for a TIP amendment(s) arises following the mailing of the agenda packet, the Florida Department of Transportation (FDOT) will present the proposed amendment(s) under this agenda item.

ATTACHMENTS: (Any proposed TIP amendment forms will be provided at the meeting)

ACTION: As deemed appropriate based on discussion



**BECKETT BRIDGE PD&E STUDY – MS. ANN VENABLES AND MR. JIM PHILLIPS, EC DRIVER & ASSOCIATES**

Pinellas County, in conjunction with the Florida Department of Transportation (FDOT), is conducting a Project Development and Environment (PD&E) Study to evaluate the removal, rehabilitation or replacement of the existing Beckett Bridge over Whitcomb Bayou in Tarpon Springs. The study is funded through a Local Agency Program (LAP) agreement with FDOT District 7. Mr. Tony Hornik is the Project Manager for Pinellas County. EC Driver & Assoc., Inc. is under contract with Pinellas County to conduct the study.

The study began in January 2011 and is anticipated to be completed in December, 2013. The following alternatives are currently being evaluated:

- No Build;
- No Build with Permanent Removal of the Existing Bridge;
- Rehabilitation of the Existing Bridge;
- Replacement with a New Low-Level Movable Bridge; and
- Replacement with a New Mid-Level Fixed Bridge.

No additional traffic lanes are proposed. All replacement alternatives evaluated will provide one lane of traffic in each direction. In addition, all replacement alternatives considered will be constructed on the same alignment as the existing bridge.

Public input is an important aspect of the study. An Alternatives Public Workshop is anticipated to be held in January 2013. The project team is meeting with local government organizations and small stakeholder groups prior to the Public Workshop to provide opportunity for input.

Ms. Ann Venables, the consultant project manager and Mr. Jim Phillips, Chief Engineer for EC Driver, will make a brief presentation consisting of a power point presentation about the study and discuss the alternative concepts developed to date. Comments and input from the Advisory Committee concerning the concepts are welcome.

ATTACHMENT: [Location Map, Beckett Bridge, Tarpon Springs](#)

ACTION: TCC comments requested

**PINELLAS COUNTY SOCIOECONOMIC DATA**

The MPO has initiated the development of the new Long Range Transportation Plan (LRTP) with a horizon year of 2040. As a part of the development of the LRTP, socioeconomic data forecast must be developed through the year 2040. This data will be used for transportation modeling efforts and will also be available for local governments to utilize for their municipal planning activities. The socioeconomic data includes population, employment, school enrollment, and hotel/motel data. Over the course of the month of September, MPO staff has been working with the Pinellas County Planning Department, the municipalities, and the LRTP Consulting Team led by Tindale-Oliver and Associates (TOA), to develop population projections from the base year of 2010 through the year 2040, using a scenario based on a business-as-usual approach to development, assuming no major changes in the land use strategy or in transportation investments. At a series of workshops in November, these partners will analyze the employment, school enrollment, and hotel/motel data and make any adjustments, as necessary, using the same scenario. A separate scenario planning effort is about to begin to analyze two alternative transportation investment scenarios. These two scenarios will build on this business-as-usual scenario, and develop another set of socioeconomic data to analyze population and employment growth potential with increased investments in transportation. Once the preferred scenario is selected from the three developed, the associated socioeconomic data set will be utilized for the 2040 LRTP.

A representative from TOA will review the population methodology, forecast, and allocation results for the business-as-usual scenario with the TCC and receive any comments that members may have. Based on comments received, appropriate modifications will be made and incorporated into the final allocation of socioeconomic data for Pinellas County. TCC members and other representatives from the local jurisdictions are invited to attend the next series of workshops, held on November 7<sup>th</sup> for south county jurisdictions and on November 9<sup>th</sup> for north county jurisdictions, to review the preliminary employment forecast and allocation. All TCC members will also be provided future correspondence regarding the allocations, providing even more opportunities for review and comment.

At this time, TCC members are asked to review the methodology and allocation results and provide MPO staff with any comments, as deemed appropriate.

ATTACHMENTS: [Map of Pinellas County Population Forecast Allocation](#)  
[Map of Pinellas County Dwelling Unit Forecast Allocation](#)

ACTION: Review and comment on Socioeconomic Data Methodology and Allocation Results

**FALL UPDATE OF FISCAL YEARS 2012/13 THROUGH 2016/17 TRANSPORTATION IMPROVEMENT PROGRAM (TIP)**

Each year, the Metropolitan Planning Organization (MPO) updates the Transportation Improvement Program (TIP) to incorporate changes in the County and Municipal Work Programs. The TIP contains project descriptions, schedules, and corresponding funding allocations for the 25 local governments of Pinellas County as well as the Pinellas Suncoast Transit Authority (PSTA), the local airports, and the Port of St. Petersburg. The projects include new construction, reconstruction, capital purchases, and maintenance work associated with roads, sidewalks, trails, transit services, airports, the Port of St. Petersburg, and the Transportation Disadvantaged Program. The TIP also contains the MPO's priority lists of projects for the Transportation Enhancement Program and the Surface Transportation Program as required by law in order to receive state and federal funding.

The annual fall update incorporates the new locally adopted Pinellas County and municipal transportation work programs into the TIP. The new county and municipal work program tables are attached, along with corresponding project maps. Also attached is a table showing the differences between the Pinellas County transportation improvement projects adopted in 2011 and the latest version adopted in 2012.

- ATTACHMENTS: [Map of Pinellas County Road, Intersection, and Bridge Improvements](#)  
[Map of Pinellas County Intelligent Transportation System/Advanced Transportation Management Systems and Trail Projects](#)  
[Table Comparing Changes to County Project Schedule to Previous Years](#)  
[Pinellas County Capital Improvement Program for Transportation Projects for FY 2012 – 2017](#)  
[Work Program Table of the St. Petersburg-Clearwater International Airport](#)  
[Work Program Table of the Port of St. Petersburg](#)  
[Work Program Table of the Clearwater Airpark](#)  
[Work Program Table of the Albert Whitted Airport](#)  
[Map of Municipal Work Program Projects](#)  
[Municipal Work Program Tables](#)

ACTION: TCC to recommend approval of the Fall Update of the Fiscal Years 2012/13 through 2016/17 Transportation Improvement Program (TIP)

**2040 LONG RANGE TRANSPORTATION PLAN UPDATE – DRAFT PLANNING ASSUMPTIONS TECH MEMO**

As one of the first steps in updating the Long Range Transportation Plan (LRTP), MPO staff developed the Planning Assumptions that will serve as the basis for the Plan update. MPO staff has conducted interviews with a number of the local agencies to gauge the transportation issues most important to them and to identify what improvements are necessary to help them achieve their future vision for their communities. Staff has also conducted an extensive document review to help identify any other transportation priorities for the communities and the region as a whole.

Based on this effort, staff has developed a Tech Memo documenting the Planning Assumptions for the LRTP update. The Tech Memo includes an overview of the agency interviews and surveys, covers such topics as public transportation, revenue projections, bicycle and pedestrian, socio-economic data and constrained corridors, as well as basic assumptions for the development of the plan.

MPO staff is seeking comments on the preliminary draft of the Planning Assumptions. A final draft will be brought back to this committee at its November meeting, to allow sufficient time for review and comment. The final draft will be presented to the MPO Board at their December meeting for approval.

ATTACHMENTS: [Draft Pinellas County MPO 2040 Long Range Transportation Plan Planning Assumptions Tech Memo](#)

ACTION: Review and comment on the Pinellas County MPO 2040 Long Range Transportation Plan Update Planning Assumptions Tech Memo

**TECHNICAL COORDINATING COMMITTEE REVIEW OF ADVISORY COMMITTEE RECOMMENDATIONS**

At the October 15, 2012 PTAC meeting the Committee passed a motion recommending that FDOT consider converting the flashing pedestrian beacon at Madonna Boulevard and the Bayway to an RRFB. FDOT has approved the use of RRFBs on multilane roads, specifically Gulf Boulevard, and is currently looking for a way to convert them. In November 2009, the PTAC recommended the installation of an RRFB at this intersection; however, FDOT had not yet approved them at that point. As this is a technical matter, it is being forwarded to the TCC for review and transmittal to the MPO requesting approval to send a letter to FDOT recommending an RRFB at Madonna Boulevard and the Bayway.

ATTACHMENTS: PTAC Motion (provided at meeting)

ACTION: Concur With and Forward the PTAC Recommendation to the MPO

**OTHER BUSINESS**

A. MPO Bicycle Pedestrian Master Plan Crash Data Report

As part of the MPO Bicycle Pedestrian Master Plan update project, URS Corporation prepared a draft technical memorandum containing a report of bicycle and pedestrian crash data countywide and a review and analysis of travel conditions on selected corridors. The report also included recommended countermeasures to address safety issues identified through the corridor studies. The TCC received a presentation of the findings and recommendations contained in the draft report at their meeting on September 26, 2012. Following the meeting, comments regarding the draft report were provided to MPO staff by TCC and local government representatives. Staff is in the process of revising the report and incorporating these comments along with others received from Bicycle Advisory Committee (BAC) members as appropriate. The revised report will be presented to the TCC for review at their December 5th meeting.

ATTACHMENTS: None

ACTION: None required, information item

B. TCC Meeting Schedule for Remainder of 2012

For a variety of reasons, including the Thanksgiving Holidays and also the presentation of the Five-Year Work Program by FDOT, the TCC will have only one meeting in the November-December timeframe. The TCC's last meeting of the year will be on December 5, 2012.

ATTACHMENTS: None

ACTION: None required, information item

C. Reminder of Election of Officers at December 5, 2012 TCC Meeting

As set forth in the TCC Bylaws, the Committee elects a Chairman and Vice Chairman at the last meeting of the year to serve in the upcoming year. This year, the TCC's last meeting will be on December 5, 2012. Please be prepared to nominate and elect both the Chairman and Vice Chairman of the Committee at that time. The current Chairman is Leland Dicus and the Vice Chairman is Paul Bertels. A TCC Membership List is attached.

ATTACHMENTS: [TCC Membership List](#)

ACTION: None required, information item

D. Mobility Plan Update

An update regarding the Mobility Plan will be provided by the MPO Staff.

ATTACHMENTS: None

ACTION: None required, information item

E. Uniform Trail Signage Treatments Update

An update regarding Uniform Trail Signage Treatments will be provided by the MPO Staff.

ATTACHMENTS: None

ACTION: None required, information item

F. Other

If any member has other business to discuss, that may address it under this item.

# Beckett Bridge PD& E Study

Presentation to:

MPO Advisory Committees



October 2013

**Study Began January 2012**

**Alternatives Presented to Commission October 2013**

**Alternatives Presented to Public January 2013**

### **Alternatives Considered**

- **No-Build**
- **No-Build with Permanent Removal  
of Existing Bridge**
- **Rehabilitation (No Widening)**
- **Replacement**
  - **Fixed Bridge – 28 feet Vertical Clearance**
  - **Movable Bridge - 7.8 feet Vertical Clearance**



## National Environmental Policy Act of 1969

### Federal Highway Administration (FHWA)

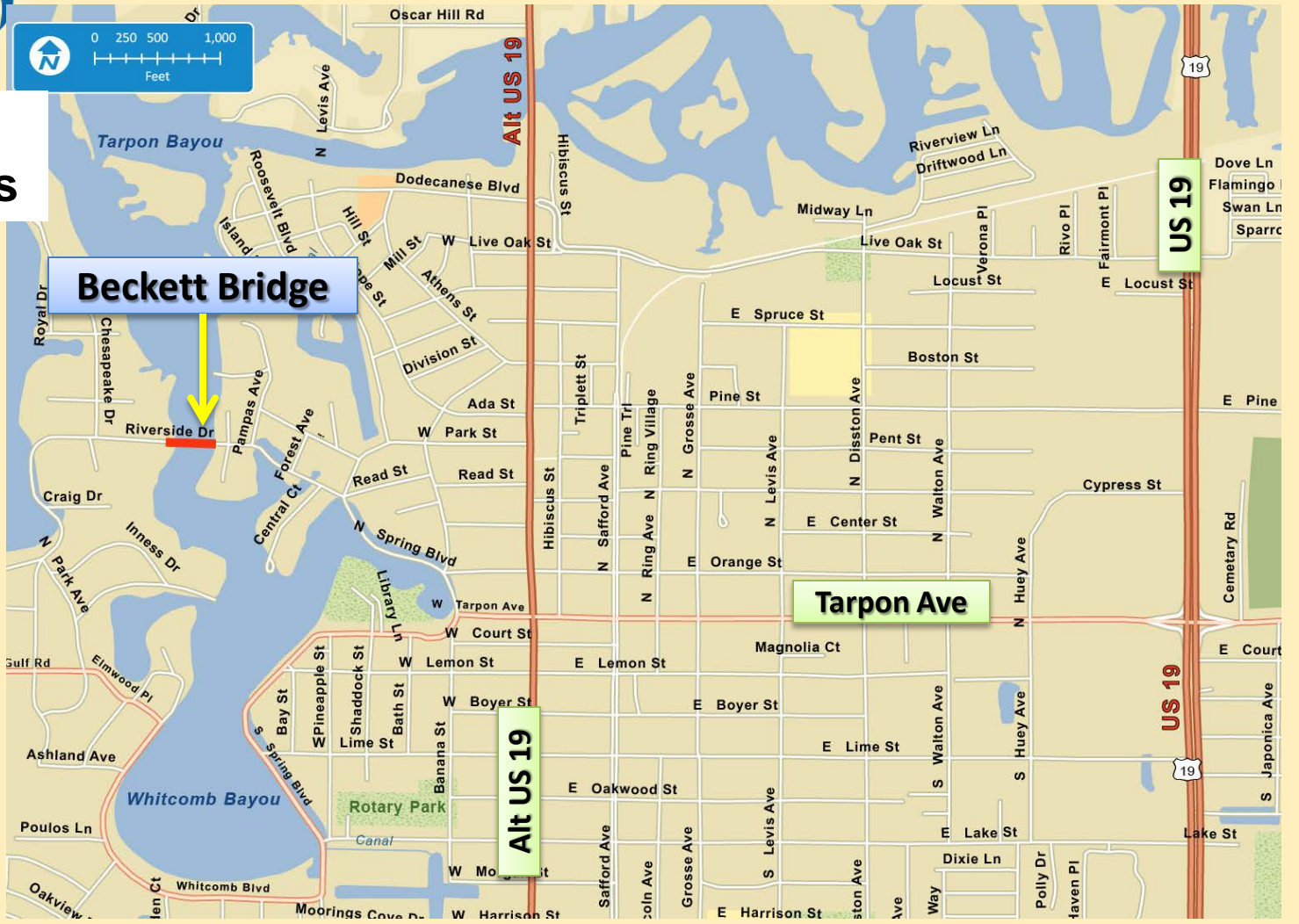
- Assures NEPA Compliance
- Final Authority – Approval of “Recommended Alternative”
- Approval required if federal funds are used
- Approval required to qualify for federal funds



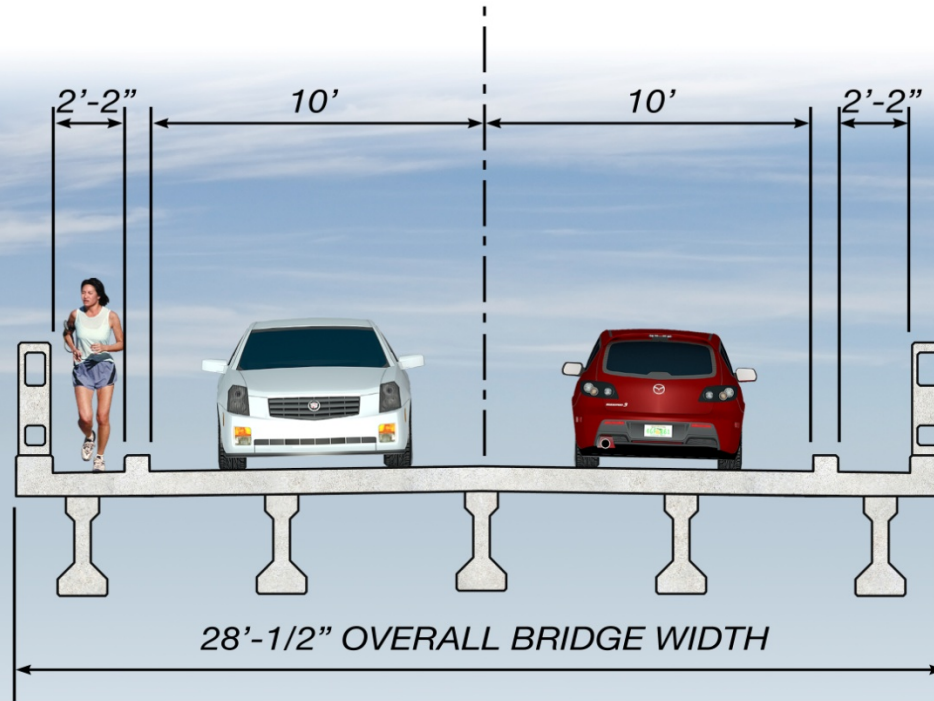


# Project Location

2012 AADT  
7,700 vehicles



## Functionally Obsolete Deck Geometry



No Shoulders

Narrow Sidewalks

## Structural Condition

- Cracked and spalled concrete throughout
- Corrosion of reinforcing steel throughout
- Corroded structural steel
- Distorted steel flanges at tread plates
- Deteriorated timber piles & wales of fender system



- **Mechanical & Electrical Issues**
  - Existing systems are old, worn and no longer reliable



## Stakeholder/Local Government Presentations October – November 2012

- Chamber of Commerce
- Rotary Club
- Tarpon Springs Yacht Club
- MPO Board
- MPO Advisory Committees
- City of Tarpon Springs
- Pinellas County BCC
- Cultural Resource Committee (CRC)





- Alternatives Public Meeting - January 2013  
77 Written Comments Received

## *Preferences for Alternatives*

<b>No-Build</b>	<b>7</b>
<b>No-Build, Remove Bridge</b>	<b>2</b>
<b>Rehabilitation</b>	<b>11</b>
<b>Rehabilitation or New Movable</b>	<b>12</b>
<b>New Movable Bridge</b>	<b>32</b>
<b>New Fixed Bridge</b> (28 ft Vertical Clearance)	<b>4</b>

- **Alternatives Public Meeting - January 2013**

- Community Concerns**

- **Need for safer pedestrian facilities**
    - **Bridge should provide adequate vertical clearance**
    - **Bridge should not adversely affect historic character of the community**
    - **Duration of detour should be minimized**





## Section 106 Process

- Avoid, minimize or mitigate adverse impacts
- Conduct “Good faith consultation” with affected parties
  - Consider affected party concerns
  - Solicit Input on possible mitigation if required
- FHWA is the lead final agency
- SHPO is the concurring agency



## Cultural Resource Committee – CRC

### Affected Parties included:

- **Federal/State agencies**
  - SHPO, USCG, FDOT, FHWA,
- **Stakeholders with special interest in historic preservation**
- **Local government representatives**
- **Local community representatives**

October 2012, March 2013 CRC Meetings

## CRC Meeting – March 2013

SHP0 requested evaluation of two new Rehabilitation Alternatives with Improved Sidewalks

- **Rehabilitation with Widening**
  - Provide sidewalks on both sides
- **Reconfiguration of Existing Bridge (No Widening)**
  - Provide sidewalk on one side



# Evaluation of Rehabilitation Alternatives to Improve Sidewalks

Conclusion of Extensive Engineering Evaluation  
Both Options require:

- Replacement of Bascule (Movable) Span
- Replacement of Bascule Pier

Rehabilitation – Widening to Provide Sidewalks

- **No elements of original bridge will remain**

Rehabilitation – Reconfiguration  
to provide one sidewalk

- **Not Feasible**





# Evaluation of Rehabilitation No Sidewalk Improvements

Rehabilitation – Original Concept - No Widening

Disadvantages:

- No change in roadway geometry
- Narrow sidewalks remain, no shoulders
- Structural concerns – unknown foundations
- Vehicular/pedestrian safety
- Link in future Howard Park Trail
- Life-cycle costs higher compared to replacement
- Existing Service Life – 25 years

**Requires Replacement of Bascule Span**

**Bascule Pier Only Remaining Original Element**

**Original Rehabilitation Concept - \$9.5 M**

No Widening/No Sidewalk Improvements

Remaining Service Life - **25 years**

**Rehabilitation (with Widening) - \$12.5 M**

Provides two 5.5 ft sidewalks

Remaining Service Life - **25 years**

**Reconfiguration of Existing Bridge**

No widening, one 5.5 ft sidewalk

**Not Feasible**

**New Movable Bridge - \$15.8 M**

Provides two 6 ft sidewalks

Service Life - **75 years**

## Costs Compared over a 100 Year Period

- Rehabilitate the bridge in 2020 then replace it with a new movable bridge in 2038  
(25 years from 2013)

Versus

- Replace the bridge in 2020 with a new movable bridge

**Result - More Cost Effective to Replace Bridge in 2020**

## SHPO Evaluation

- Engineering Analysis provides “*ample evidence to support the project team’s opinion that a new bridge would be preferable to the rehabilitation.*”
- Mitigation will be required if existing bridge is demolished



## Sufficient documentation to determine Fixed Bridge alternatives not feasible

- USCG determined that 28 feet of vertical clearance “Does Not Meet the Needs of Navigation”
- Substantial right-of-way impacts
- Substantial visual impacts
- Not consistent with historic character of community
- Requires two-year detour during construction
- Cost **\$14 M - \$15 M** (including Right-of-way) compared to New Movable **\$15.8 M**



# Recommended Alternative

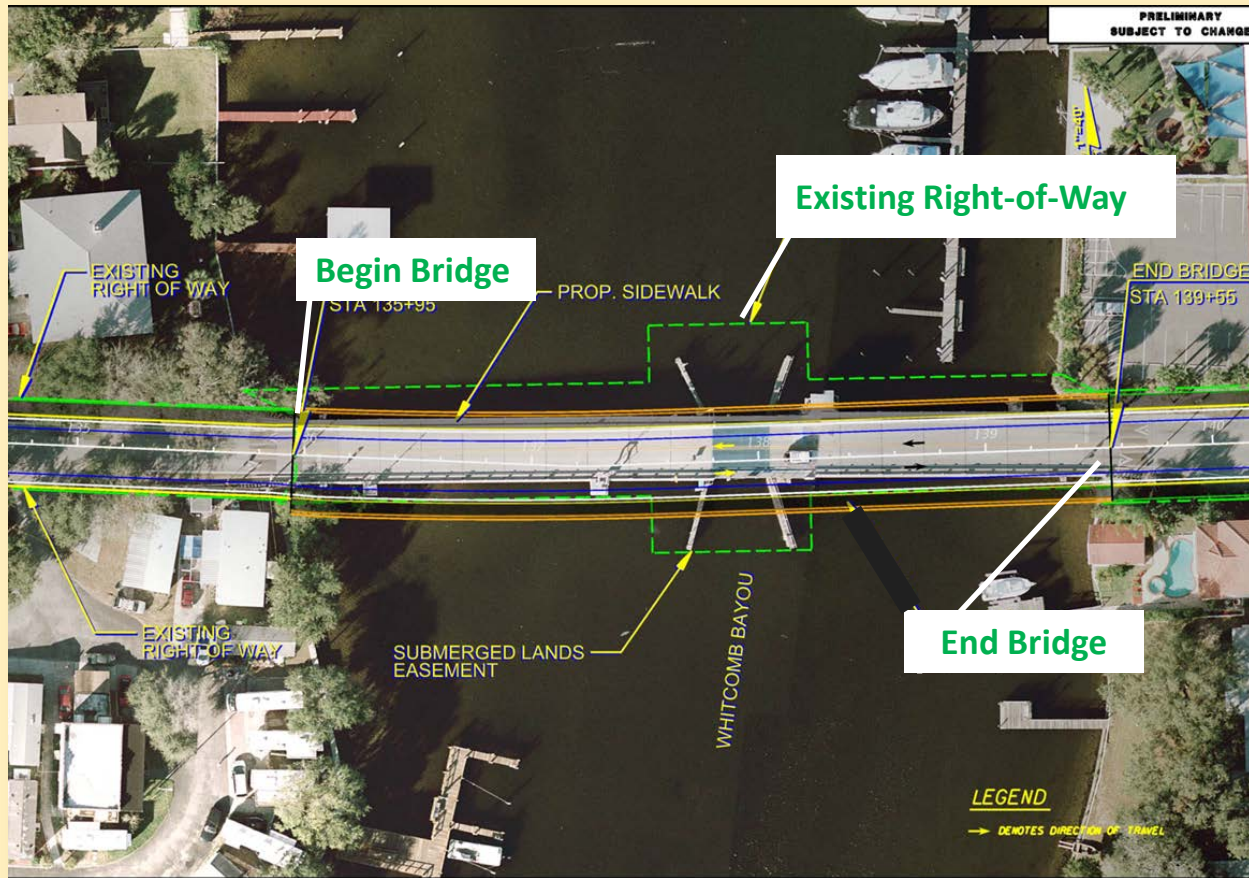
Based on extensive evaluation and consideration of:

- Engineering and Costs
- Safety of vehicles, bicyclists and pedestrians
- Potential socioeconomic and community impacts
- Impacts to the natural and physical environment
- Impacts to cultural resources
- Impacts to adjacent properties
- Impacts to the boating community
- Consideration of public input
- Other potential impacts

Replacement with a New Movable Bridge

“Recommended Alternative” for presentation at  
Public Hearing

## No Impacts to Adjacent Property

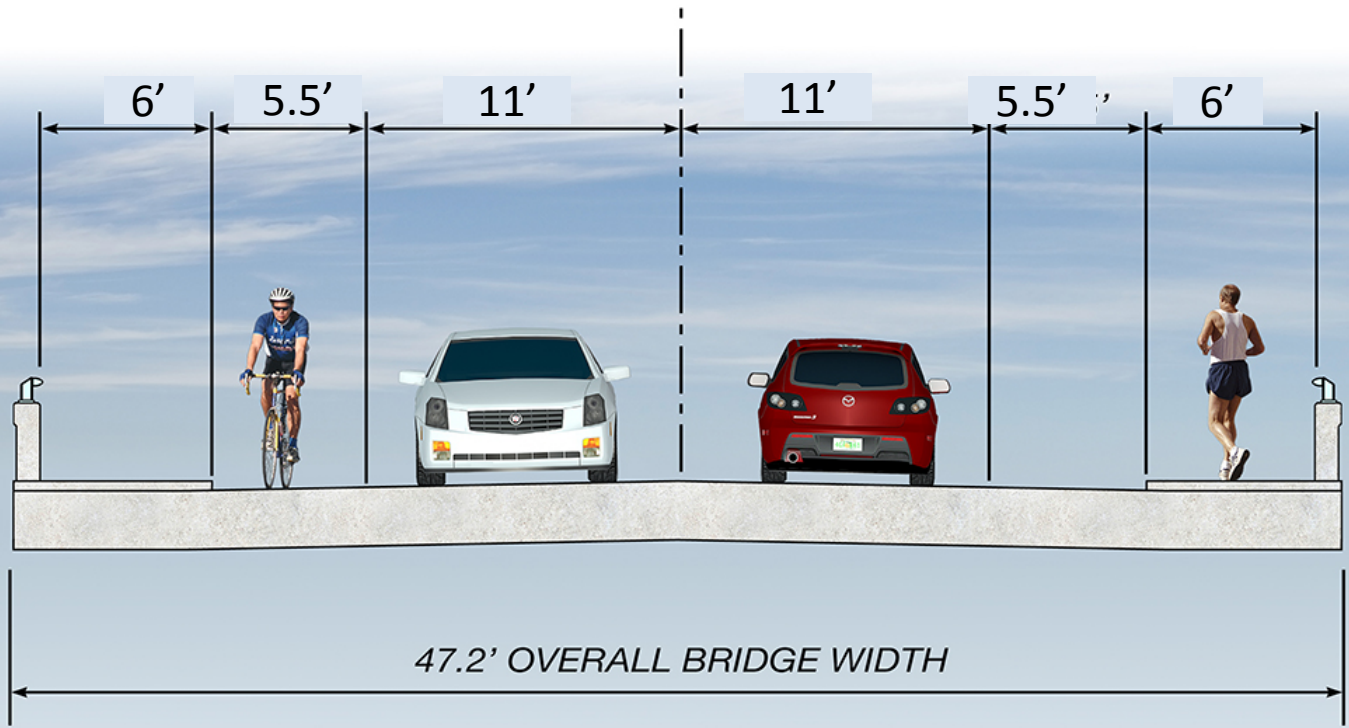


## Description

- No right-of-way impacts
- Vertical Clearance 7.8 feet
  - (existing 6 feet)
- Horizontal Clearance 25 feet
  - (same as existing)
- Total Width 47.2 feet
  - Approximately 19 feet wider than existing
  - 11 ft travel lanes
  - 5.5 ft shoulders and 6 foot sidewalks – both sides

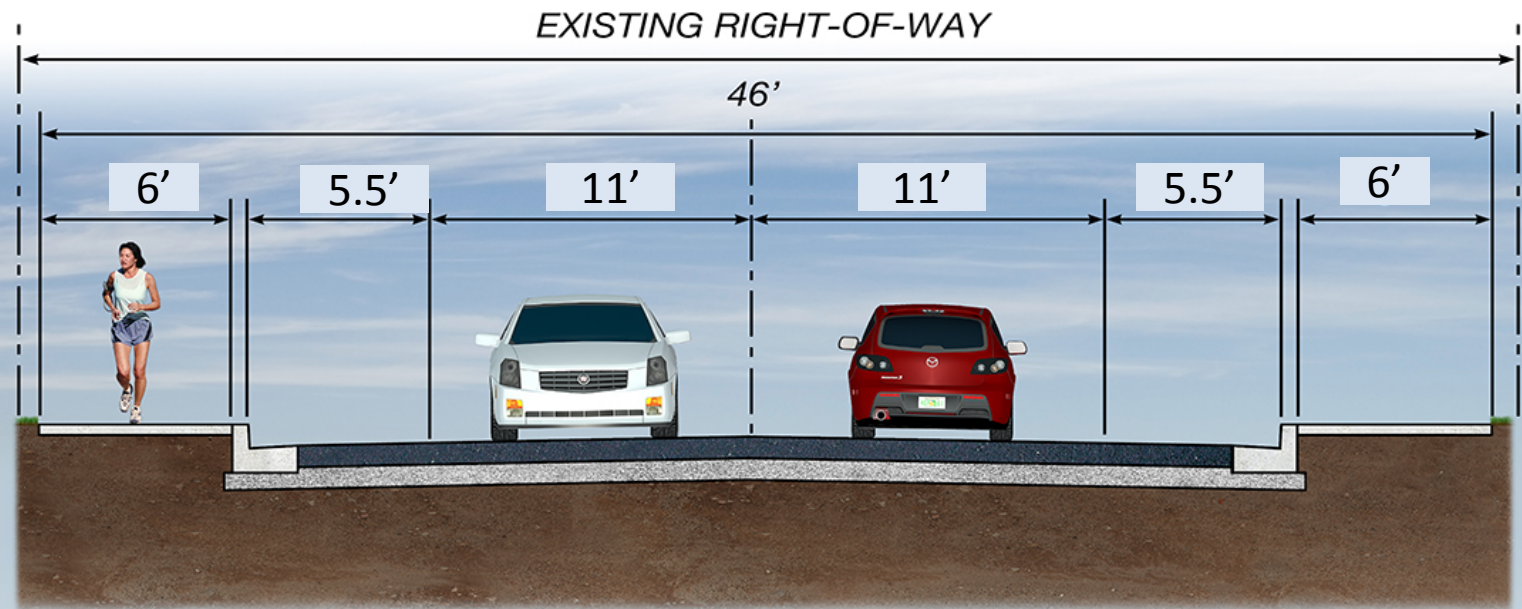
# Movable Bridge Typical Section

Total Bridge Width - 47.2 feet



# Pinellas County Proposed Roadway Typical Section – East of Movable Bridge

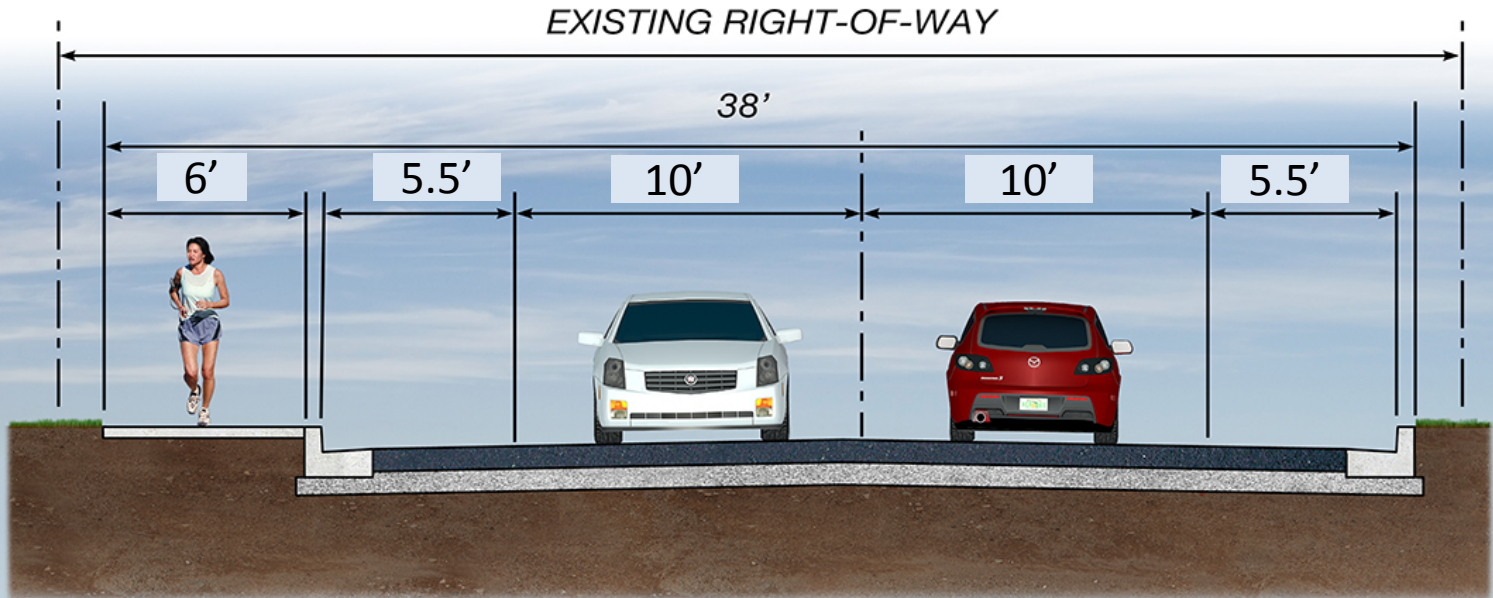
Total Width – 46 feet





# Proposed Roadway Typical Section – West of Movable Bridge

Total Width – 38 feet



**After Approval by FHWA – Project will be eligible to move into the design phase**

**Aesthetics will be determined in Design Phase**  
**Future Opportunities for Public Input**









## “Generic” Movable Bridge





## “Industrial” Style Rolling-Lift Bascule Bridge

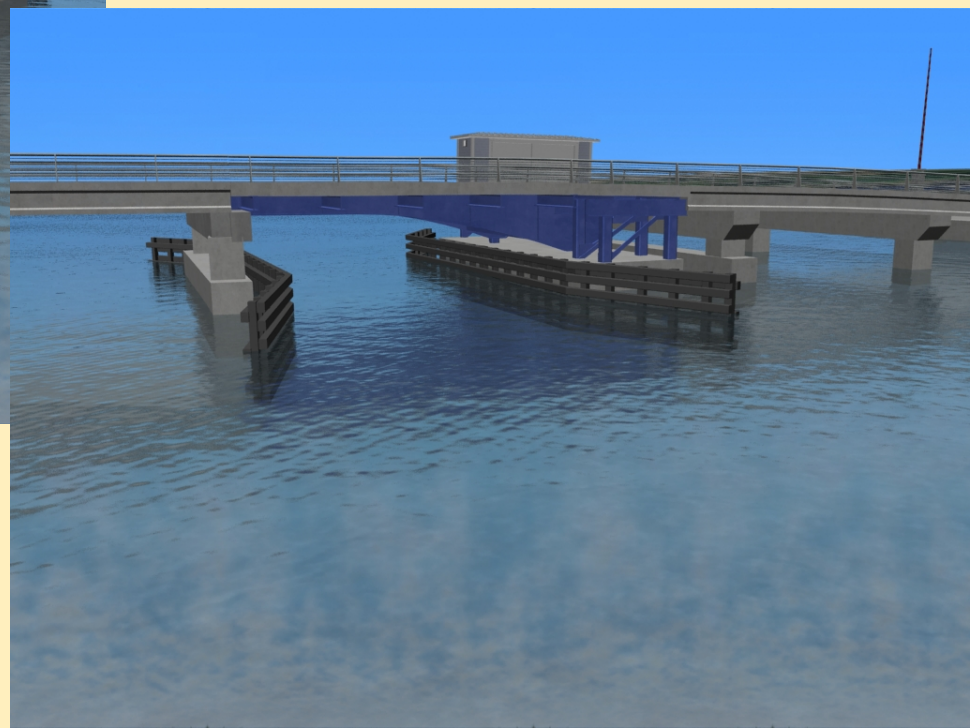
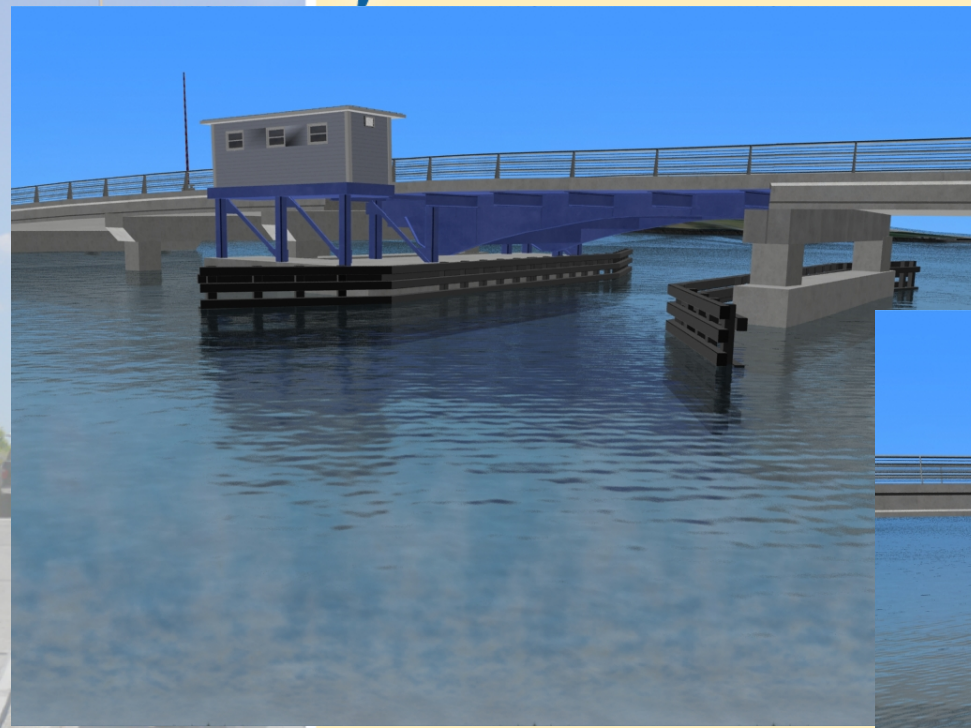




## “Industrial” Style Rolling-Lift Bascule Bridge



**3D Model Views  
Industrial Style**

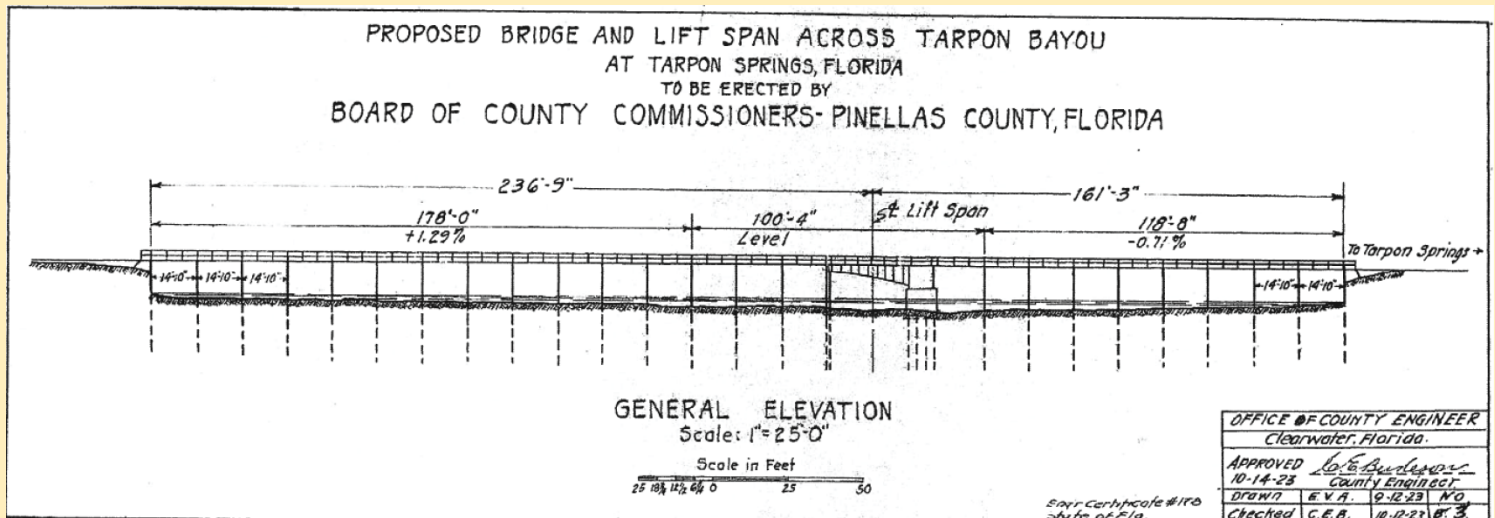




## Required Mitigation

### Historic American Engineering Record (HAER) Documentation

- Large format photographs
- Written history/narrative
- Historic bridge plans copied on archival paper



## Possible Mitigation

- Choose Bridge Rail to Preserve Viewshed from Bridge
- Educational Kiosk/Monument in Public Space
  - On or Near Bridge
  - In City Park or Museum
- Incorporate Monument into Second Control House
- Incorporate Portion of Original Bridge into New Bridge



**Example – Treasure Island**

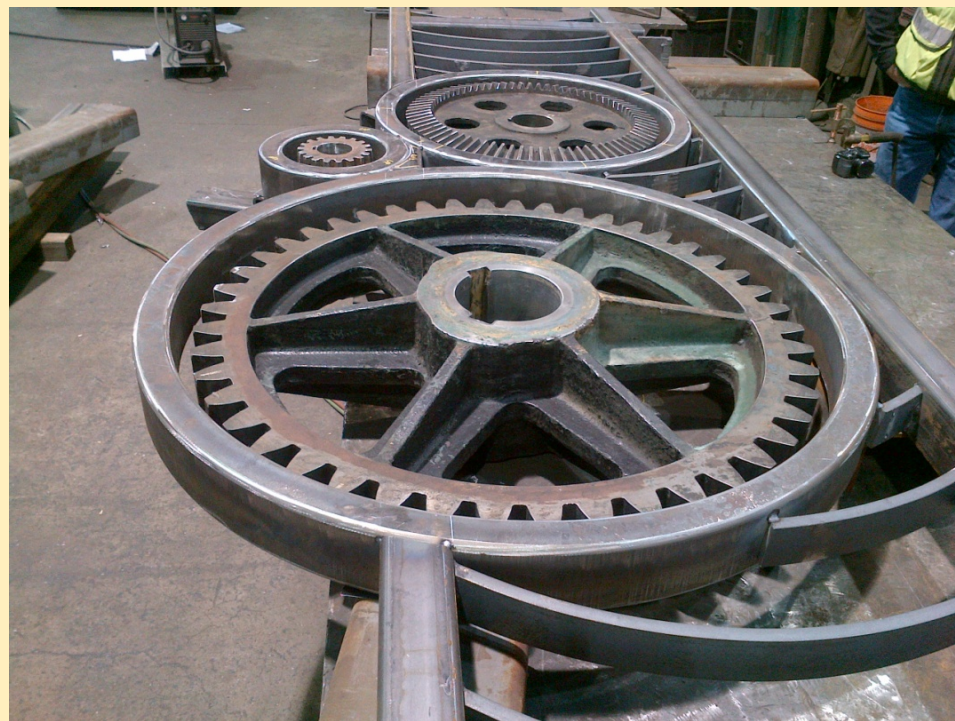
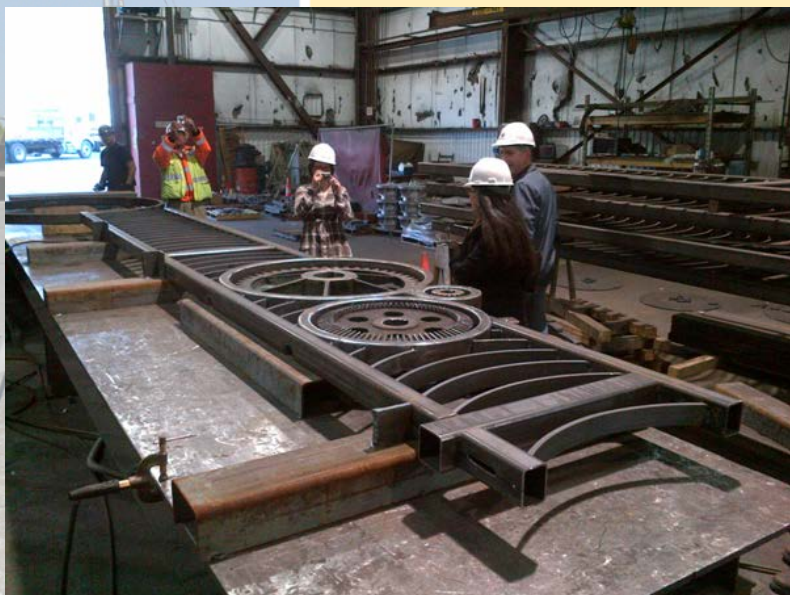
**Monument Bridge in City Park – Treasure Island**





## Example - South Park Bridge, Seattle, WA

Incorporating Part of Existing Bridge into New Bridge



# Pinellas County Minimization/Mitigation Options

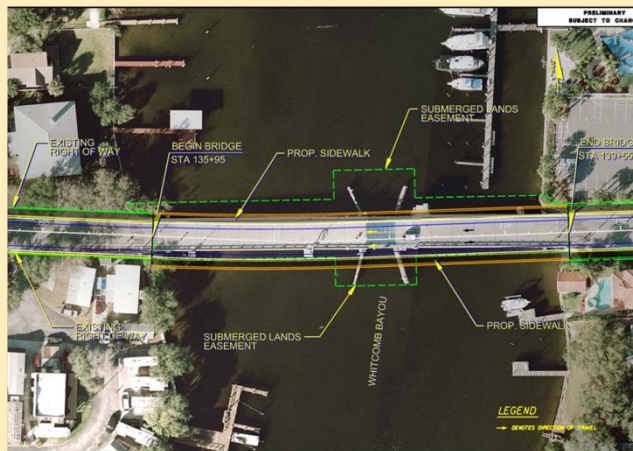


**Incorporating Part  
of Existing Bridge  
into New Bridge**  
**Example:**  
**South Park Bridge**



# Next Steps in PD&E Process

- Present Recommended Alternative to MPO Advisory Committees/Board
- CRC Meeting
  - Continue coordination of Section 106 Issues
  - Solicit input on possible mitigation if Movable Bridge is selected as “Preferred Alternative”



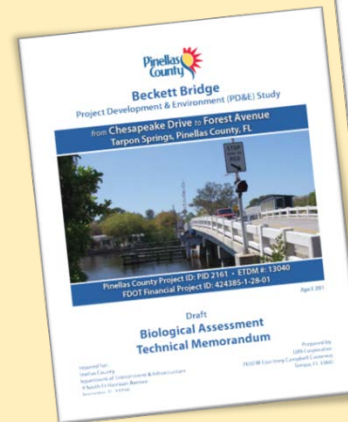
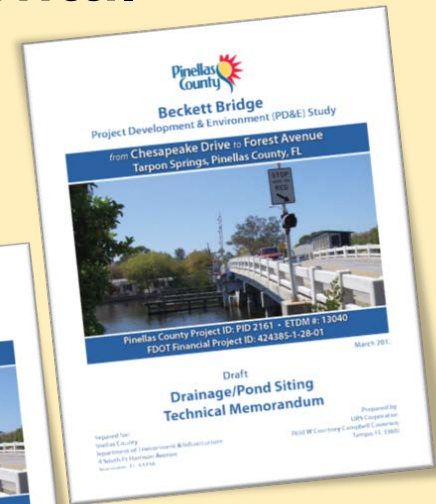
## Present Recommended Alternative at Public Hearing in February 2014

- Presentation will include discussion of all alternatives considered
- Public comments recorded by court reporter
- Comments included in Project Record



- Consider Public Hearing Input
- Finalize Engineering/Environmental Documents
- Continue SHPO Coordination
  - Complete Section 106 documents
  - Develop MOA
    - SHPO, FHWA, FDOT,
    - USCG, County

**Submit Final Documents to FHWA for Approval**



# Pinellas County New Movable Bridge – Animation



# Questions and Discussion





TECHNICAL COORDINATING COMMITTEE MEETING

WEDNESDAY  
OCTOBER 23, 2013  
2:00 P.M.

PINELLAS COUNTY PLANNING DEPARTMENT  
CONFERENCE ROOM – FIRST FLOOR  
310 COURT STREET  
CLEARWATER, FLORIDA 33756

AGENDA

1. CALL TO ORDER
2. [APPROVAL OF MINUTES – September 25, 2013](#)
3. [HOWARD FRANKLAND BRIDGE \(I-275/S.R. 93\) REPLACEMENT UPDATE – FDOT](#)
4. [GANDY BOULEVARD PROJECT, FROM WEST OF I-275 TO EAST OF U.S.19 – FDOT](#)
5. [BECKETT BRIDGE PROJECT DEVELOPMENT AND ENVIRONMENTAL \(PD&E\) STUDY UPDATE](#)
6. [MPO 2040 LONG RANGE TRANSPORTATION PLAN UPDATE](#)
  - A. Congestion Management Project Implementation Plan
  - B. Constrained Roads
7. [CONGESTION MANAGEMENT PROCESS \(CMP\) POLICIES AND PROCEDURES MANUAL](#)
8. [RECOMMENDATION TO REROUTE THE DESIGNATED TRUCK ROUTE IN TARPON SPRINGS](#)
9. [FALL UPDATE OF FY 2013/14 THROUGH FY 2017/18 TRANSPORTATION IMPROVEMENT PROGRAM \(TIP\)](#)
10. [FY 2013/14 – FY 2017/18 TRANSPORTATION IMPROVEMENT PROGRAM \(TIP\) AMENDMENT\(S\)](#)
11. [TCC REVIEW OF ADVISORY COMMITTEE RECOMMENDATIONS](#)
12. [OTHER BUSINESS](#)
  - A. Uniform Trail/Roadway Signage Treatments
  - B. Memorial Causeway Bicycle Speeding Issue
  - C. Multi-Named Roads
13. [MPO ACTIONS – October 9, 2013](#)
14. ADJOURNMENT

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*NEXT TCC MEETING – DECEMBER 4, 2013*



APPROVAL OF MINUTES – September 25, 2013

The minutes of the September 25, 2013 TCC meeting are attached for your review and approval.

ATTACHMENTS: TCC Minutes –September 25, 2013

ACTION: Approval of Minutes

**MINUTES OF THE  
TECHNICAL COORDINATING COMMITTEE MEETING  
SEPTEMBER 25, 2013**

The following is a summary of the Pinellas County Metropolitan Planning Organization's Technical Coordinating Committee meeting held September 25, 2013 in the Planning Department Conference Room, First Floor, 310 Court Street, Clearwater, Florida.

**Members Present**

Leland Dicus, Chairman	City of Largo Community Development – Engineering
Paul Bertels, Vice Chairman	City of Clearwater Traffic Operations
Bob Bray	City of Pinellas Park
Mike Burke	Pinellas County School Board
Bennett Elbo	City of Clearwater Engineering
Linda Fisher	Pinellas Planning Council
Andrew Hayslip	City of Clearwater Planning
George Kinney	City of St. Pete Beach
Ken Jacobs	Pinellas County DEI Transportation
Christine McLachlan	City of Largo Community Development
Michele Parisano	City of Oldsmar
Joan Rice	City of Dunedin Engineering
David Talhouk	Pinellas County DEI Engineering
Danny Taylor	City of Indian Rocks Beach
John Villeneuve	PSTA
Tom Whalen	City of St. Petersburg

**Members Absent**

Gordon Beardslee	Pinellas County Planning Department
Tom Burke	City of Dunedin Engineering
Rodney Chatman	City of Tarpon Springs
Bill Foster	City of St. Petersburg Engineering
Lucy Fuller	City of Dunedin Planning Department
Peter Hessling	Pinellas County Air Quality
John Holt	St. Petersburg/Clearwater International Airport
Bob Klute	City of Largo
Joe Kubicki	City of St. Petersburg – Transportation and Parking
Sheila Martin	TBARTA
Jan Norsoph	City of Seminole
Michael Taylor	City of Gulfport
Avera Wynne	Tampa Bay Regional Planning Council

**Others Present**

Chris Cochran	PSTA
Nick Fritsch	Citizen
Linda Lockhart	URS
Corey Martens	City of Clearwater Traffic Operations
Sri Meka	
Rob Meador	CB&I
Al Bartolotta	Pinellas County MPO Staff
Chelsea Favero	Pinellas County MPO Staff
Gina Harvey	Pinellas County MPO Staff
Rick MacAulay	Pinellas County MPO Staff
Sarah Ward	Interim MPO Executive Director
Anne Funicello, Recorder	Pinellas County MPO Staff

1. CALL TO ORDER

Chairman Dicus called the meeting to order at 2:03 p.m.

2. APPROVAL OF MINUTES – August 28, 2013

A motion was made and seconded to approve the August 28, 2013 TCC meeting minutes as presented. The motion carried unanimously.

3. MPO 2040 LONG RANGE TRANSPORTATION PLAN UPDATE

Ms. Harvey noted the TCC was sent the draft recommendations regarding both the Congestion Management Implementation Plan and the Constrained Roads network requesting their review and comments. She indicated that the order of the items would be changed to first review Item B. Constrained Roads and then present Item A. Congestion Management Implementation Plan.

B. Constrained Roads

Chelsea Favero, MPO staff, reviewed the list of proposed constrained roads that was provided to the TCC at their August meeting, noting that she had received feedback from some of the members since that time. She reviewed that there are a number of planned improvement projects between now and 2035 in the adopted Long Range Transportation Plan (LRTP); however, in 2035, there will still be a number of roadways that are projected to fail despite implementation of the Cost Feasible Plan. Ms. Favero indicated that there are no plans beyond 2035 to widen non-Strategic Intermodal System (NSIS) facilities, so there is a need to look at alternatives. The MPO staff is proposing these particular roadways be labeled as constrained, and to identify alternatives to help mitigate the congestion problems and improve mobility. Ms. Favero noted that one option to improve mobility is increased transit. Although transit might not bring roads out of congestion, it will provide increased mobility and help alleviate congestion along these corridors. It was noted by Ms. Favero that in those areas not served by transit and where no further widening is expected, there will be a need to look at operational and demand management solutions. She noted that the Congestion Management Process (CMP) is federally required to be integrated into the planning processes. Ms. Favero noted that the MPO is currently working on the CMP Implementation Plan, as well as a CMP Procedures and Policies Guidelines document. With few remaining capacity projects, the MPO will be bringing more operational projects into the LRTP to help secure federal funding for congestion solutions. Although a number of corridors have already been studied, Ms. Favero indicated that there are many in residential areas that will not be able to be widened because of impacts on the surrounding land uses and will require a wide range of solutions to help improve mobility throughout the County and improve congestion. She noted that the MPO staff was requesting tentative approval from the TCC regarding the constrained roads, with a caveat they will be working with the County's Department of Environment and Infrastructure (DEI) on several of the roadways, including Starkey Road and 102<sup>nd</sup> Avenue to identify solutions for those corridors.

Mr. Bertels inquired about the section of Belcher Road from Sunset Point Road to Druid Road and the decision that non-SIS facilities will not be widened. Ms. Ward indicated that there are a few additional roads staff has not yet completed. She said that DEI has done a couple of PD&E studies for that stretch of Belcher Road. Ms. Ward noted that the MPO staff is scheduled to meet with the County staff within the next two weeks to have a dialogue on a number of roadways in question that may predominantly serve municipal areas, but are County roads. Ms. Ward said that she did not anticipate that many of those roads would be widened, but there are some that are subject to further discussion, including Belcher Road, Starkey Road, a segment of Forest Lakes Boulevard, 126<sup>th</sup> Avenue, and 102<sup>nd</sup> Avenue. Ms. Ward also noted that they will need to discuss the recommendations with the County with respect to the County's Comprehensive Plan. The MPO staff has made recommendations on a number of these roadways and wants to make sure the plans are consistent. The MPO staff anticipates a few additional capacity projects.

Mr. Bray asked about a segment of 49<sup>th</sup> Street on the list of constrained roadways noting there is a 16 block gap. Ms. Favero said that the methodology used shows that 16 block segment not failing. Ms. Ward noted that the City of Largo staff had also sent some similar comments and she thinks this would be worth a discussion. If there are facilities that are not failing and there are no plans to widen them, they could be shown as constrained. She said this

is the appropriate time to discuss the methodology. Chairman Dicus asked if it would make sense to constrain a more connected network and look for alternatives that would improve congestion.

Following discussion, Ms. Ward suggested prioritizing the operational improvements on those segments that are currently failing, with an option to add the constrained designation on those facilities that are not failing but where there are no plans for further widening. Mr. Bray further suggested looking at those facilities that are currently failing and those that will not be widened due to financial constraints.

In response to Ms. Ward asking if the PPC foresees problems with expanding the list of constrained roads, Ms. Fisher responded that she did not anticipate there would be an impact on the Countywide Plan Update but was not sure on the current regulations and will double check.

Ms. Ward said that after the meeting with County staff the MPO staff will bring back a final map to the TCC connecting the corridors. She asked the TCC members to provide any additional comments.

Nick Fritsch, citizen, stated that he submitted a list of recommendations for the LRTP. He said that his specific concern was the constrained segment of 102<sup>nd</sup> Avenue, which is the only segment on Bryan Dairy Road from I-275 to Oakhurst Road that is not at least four lanes. He said that it is a minor arterial roadway with a level of service "F" and has been on the County's list for improvement for approximately 25 years. Mr. Fritsch said that it is the only two-lane section between two four-lane segments. He also indicated that the existing right-of-way is between 106 and 200 feet wide which is more than sufficient to widen the segment to four lanes. Mr. Fritsch noted that two recent consultant studies have recommended widening the two-lane section to four lanes. The Board of County Commissioners (BCC) reviewed the most recent study at one of their meetings. Several citizens from Walsingham Park spoke against it and the BCC decided not to widen the roadway. Mr. Fritsch asked that the TCC revisit the decision made at the last TCC meeting to accept the recommendation to not widen this segment to four lanes. Ms. Harvey said that it was eliminated as an alternative because it was part of the requirements from the consultant not to look at widening roads, but to look at operational improvements. She said that it is still definitely on the table of recommendations for the TCC.

In this matter, Ms. Ward explained that Mr. Fritsch is a member of the Intelligent Transportation System (ITS) Advisory Committee. At their meeting one week following the August TCC meeting, the MPO staff reported to the ITS Committee the comments made by the TCC representative from the city of Seminole at the TCC meeting who pointed out that the roadway had been identified as constrained based on input from the City and a review by the County Commission as a result of a study in approximately 2008. Ms. Ward noted the question then is whether to revisit decisions regarding policy constrained roadways if there are new circumstances or new data such as increased traffic demand. Mr. Jacobs added that the only action the County took was to not go forward with the widening plans. There was no policy related decision, but was based more on the input from the citizens.

Mr. Whalen commented that he had some changes on a few roads, including right of way constraints on 22<sup>nd</sup> Avenue North, which was not listed. Ms. Favero welcomed any input.

Following discussion, Ms. Ward indicated the MPO staff would bring back a complete product at the next TCC meeting including an updated map. This item was continued until the next TCC meeting.

A. Congestion Management Implementation Plan

Rick MacAulay, MPO staff, presented the updated Congestion Management Process (CMP) Policies and Procedures Manual. He pointed out that this document should not be confused with the URS Congestion Management Plan. The CMP is required by Florida State Statutes, Chapter 339, and states each MPO must develop and implement a traffic congestion management system. This is a coordinated program for monitoring and evaluating the performance of the multimodal transportation system with the goals of identifying specific causes of congestion, identifying appropriate strategies, and evaluating the effectiveness of strategies that are implemented. Mr. MacAulay indicated that there are eight federally-recommended steps encouraged for the CMP. He said that staff is proposing one overall CMP goal

and six congestion specific objectives and has been coordinating with URS, the consultant doing the CMP study. Mr. MacAulay noted that this is still a draft and there could be some changes. He said that the document would be reviewed again at the October TCC meeting and also at the November ITS Committee meeting.

Chairman Dicus noted the recommendations seemed specific to individual segments and, if implemented, he questioned how they would result in improving congestion countywide and multimodal transportation systemwide. He said that it was difficult to pick out any key demand management strategies or operational improvements or strategies that would be applied consistently based on the problems for each segment.

Ms. Ward noted that the CMP Policies and Procedures Manual documents the process for implementing the CMP requirement. The CMP Plan is one part of the operational strategy package and the other part looks at the remaining improvements to the ITS and ATMS Master Plan, which is almost completely implemented. The 2035 LRTP included the ITS and ATMS Master Plan as a systemwide strategy and the updated 2040 LRTP will include the ITS and operational improvements on some of the constrained roadways as systemwide strategies. There is ongoing coordination with TBARTA and the commuter assistance program. Pinellas County has operational issues around the schools and is working with TBARTA to create carpool arrangements. The document will include information about the operational planning that is done through the School Transportation Safety Committee (STSC). The CMP Policies and Procedures Manual will give a better sense of how comprehensive the program is, but will not provide all of the projects. Ms. Ward appreciates the time that the city of Largo spent looking at this report. With regard to the ITS in terms of how benefit can be assessed, there is a model that can look at that systemwide, but is not being used at this time. Mr. Jacobs works with the MPO and does before and after studies on the various corridors. There is a requirement that the effectiveness of the strategies is monitored, and various tools to use for that are being examined. In terms of this update, there has been increased focus on management and operations, transit, and minimizing impacts to neighborhoods. The comments received through the city of Largo were very helpful in identifying areas where staff needs to better communicate how everything is coming together.

Mr. Jacobs noted that they currently do before and after analysis when ITS is implemented, but that does not give information for the entire network. Over the next several years they will be concentrating on implementing data collection devices to gather real time data on the various corridors where ITS has been installed, and on planned corridors that will have installations in the future to have a before picture and an ongoing real-time picture of the transportation system network. As implementation continues, they will be able to determine the benefits to each corridor and the system itself.

Mr. Beaty noted that the CMP requires measures of effectiveness and, through MAP-21, the federal government has shifted to a much greater performance objective emphasis on everything they receive. Ms. Ward added that work continues on how to assess the systemwide benefits. MPOs are struggling with the performance measures and the data needed for the systemwide assessment. Ms. Ward added that FDOT has a task force to look at performance measures for CMPs and there will be more information forthcoming. She said that staff will be bringing this to the TCC over the next few months as work continues on this portion of the Plan. She asked for any input and suggestions. Mr. Beaty noted the MPO staff would be meeting with FDOT's ITS staff soon to discuss the data they are collecting through their ITS and how to share that with the local governments.

In response to Mr. Whalen's question regarding funding, Ms. Ward responded that staff is recommending the MPO set aside funding for operational improvements; however, there is no separate funding for operational improvements unless there are safety-related projects that would qualify for safety funding. FDOT has recommended the MPO combine the CMP priority list with the capacity listing and set aside an amount for funding. Although the MPO is paying for the study, and the MPO staff is working with the local governments to identify the projects, the local governments would need to fund and implement the recommendations.

Ms. Harvey indicated that the MPO staff has received comments on the URS study that included the 14 corridors. She noted this item will be on the next TCC agenda and asked that any additional comments be emailed to the MPO staff. In addition to the 14 corridors, the other roads mentioned including Belcher Road, Starkey Road, 126<sup>th</sup> Avenue, 142<sup>nd</sup> Avenue, will also be included on a list and prioritized for the local governments to assist with implementation.

#### 4. COUNTYWIDE PLAN UPDATE – PINELLAS PLANNING COUNCIL (PPC)

Linda Fisher, Pinellas Planning Council, noted that the PPC's Countywide Plan Update is being coordinated closely with the MPO's Long Range Transportation Plan (LRTP) Update. She provided information on the PPC's membership and noted that they are advisory to the Board of County Commissioners in their role as the Countywide Planning Authority. The PPC maintains a Countywide Plan that coordinates land use across all the jurisdictions and is governed by a Countywide Plan Map and Rules. Ms. Fisher reviewed the process for amending the Land Use Plan Map.

Ms. Fisher reviewed that the PPC will be merging with the MPO to help with integrating land use and transportation. She noted that there will be a single board that will make decisions on both land use and transportation planning. The merger is in the final stages of approval and the MPO and PPC are waiting to hear from the Governor's office that it has been finalized.

In addition to providing input to each other's plans, the MPO and PPC also coordinate with other agencies and jurisdictions including the Joint Land Use Transportation Working Group, PSTA staff and Pinellas County Economic Development.

Ms. Fisher reviewed the timeline of the update, which is about a 3 ½ year process with an anticipated completion date of May 2014. She noted that the Countywide Plan was adopted in 1993 and although there have been incremental changes to the Plan over the years, the Plan needs to be updated to reflect that the County is changing from development to redevelopment mode, in addition to planning for transit-oriented development in appropriate places for high quality transit. Ms. Fisher indicated that the Plan Update will also reduce the number of land use categories from at least three dozen to between 10 and 12. She said that this will limit the number of local changes that will need go through the countywide process and allow more flexibility for the local governments to determine what their communities will look like. They will also be designating targeted employment centers which are areas where the higher wage employers are concentrated. These are companies that bring money in from outside the County and are areas that need to be protected. Transit is encouraged in these areas but would not be required.

Ms. Fisher noted that the PPC and the MPO worked closely and have used the same consultant team, similar models to predict where higher densities and intensities are anticipated or appropriate, used the same land use justification data as inputs, and both coordinated their efforts with PSTA's Community Bus Plan. Ms. Fisher noted that the Countywide Plan currently includes level of service as a criterion that needs to be examined for map amendments. This is being revisited but no final decision has been made at this point. Consideration is being given to not use level of service in the future because it is difficult to get to transit supported densities and intensities when trying to adhere to a roadway level of service. It is believed that congestion will be mitigated in those areas through transit and multimodal improvements other than roadway widening. Ms. Fisher said that level of service could still be used in stable areas, but no decision has been made. Either way they are not encouraging large increases in densities and intensities in the stable areas which will take pressure off the constrained roadways in those areas.

Ms. Fisher noted that one of the next steps includes looking at FDOT's Strategic Freight Plan to make sure there are no conflicts with the land use plan. She also noted that the PPC has been heavily involved in the education effort associated with PSTA's Greenlight Pinellas Program and has created a brochure that is included in the agenda packet. The goals of the Countywide Plan Update are to create greater range of choices for housing, employment, recreation, and transportation; encourage new growth to locate in appropriate centers and cores; encourage high quality job growth; and preserve and enhance the established communities.

In response to Mr. Bertels asking if any other areas have combined the MPO and land use departments, Ms. Fisher said that Atlanta has done so and they have researched other areas around the country that are similar but not exactly the same. Ms. Ward said that the PPC is the only one of its kind in the state and the merged MPO/PPC will be unique in the State of Florida.

Responding to a suggestion that land use possibly be shown on the brochure, Ms. Fisher said that they tried but found that it is difficult to make sense of it visually and suggested possibly showing an excerpt. Following further discussion, Ms. Fisher indicated she will take the comment under consideration.

## 5. FUNCTIONAL CLASSIFICATION

Ms. Favero distributed copies of an updated Functional Classification spreadsheet and map. She noted that every ten years after the Census, the MPO revisits the functional classified roadways, which are grouped by the character of the road. She noted that the spreadsheet adds six roadway corridors to the functional classification system and removes four roadway corridors. At the bottom of the spreadsheet, five roads have been added that are not yet reflected on the map, but those changes will be made. The MPO staff reviewed the roadways against the County's Comprehensive Plan maps and those changes are also reflected. Ms. Favero noted that she had received comments from the City of Dunedin and asked that all comments be sent to her so they can be reflected. The MPO staff is looking for preliminary endorsement from the TCC.

Mr. Bertels questioned the designation of Cleveland Street/Gulf-to-Bay Boulevard from Drew Street to Court Street as a minor arterial. It was his feeling that it should be designated as a collector. Ms. Favero responded that Court and Chestnut Streets had previously been designated as a minor arterial and Cleveland Street was a principal arterial. Mr. Bertels noted that Cleveland Street dead-ends at the marina in downtown Clearwater. Ms. Favero noted that the identification of collectors has changed from urban collector to major collector and minor collector. Major collectors serve more high density neighborhoods and commercial establishments and minor collectors serve lower densities. Mr. Bertels said it should then be designated as a major collector.

Ms. Favero added that Mr. Bartolotta found changes that need to be made to the County's Comprehensive Plan maps: 49<sup>th</sup> Street between Roosevelt Boulevard and U.S. 19 was designated as a principal arterial; however, the MPO had it as a minor arterial. Also, Tarpon Avenue is a minor arterial and the MPO had it as a collector. FDOT asked that U.S. 19 be changed to principal arterial and not expressway.

Mr. Beaty explained the reason for the functional classification is that roads with a category of major collector or above are eligible for federal highway dollars. If the category is less than collector, it is a local road and not eligible for federal aid. In addition, functional classification assists with developing a road network. As agencies move toward a performance-based management approach, functional classification is an important consideration in setting expectations and measuring outcomes for resurfacing, mobility, and safety projects. FDOT is required to go through this exercise after each Decennial Census and, although it does not require a public hearing, FDOT's procedures require it to go through a public forum, which is why it is approved at the MPO meeting. Mr. Beaty said that the final approving authority is the Federal Highway Administration (FHWA) and there are specific guidelines in the federal guidance for each category. He noted FHWA passed new guidance this year and he will make sure staff receives a copy.

Mr. Bertels indicated functional classification is very important and asked everyone to make sure the functional classification in their community is up to date.

Mr. Bertels made a motion that the TCC forward the Functional Classification Network to the MPO, subject to any final changes. The motion was seconded and carried unanimously.

## 6. COMPLETE STREETS PRESENTATION

Ms. Ward reviewed that the MPO Long Range Transportation Plan will be adopted by the MPO in December of 2014. A draft of the Plan will be taken to the MPO in September or October for initial endorsement and to open a 45-day comment period. A large focus has been on transit, as well as work on land use that has progressed for the past year. MPO staff has been bringing individual elements to the TCC for review but they have not brought a complete package. They are in the process of looking at policies for Complete Streets, which is a movement that began about seven years ago by Smart

Growth America. The MPO previously approved a Livable Communities Plan which encompassed more than just transportation. Complete Streets is simpler and conveys the message how to safely accommodate all mobility modes in the transportation network. Ms. Ward said that, over the next several months, staff will show how all the planned improvements will fit together. Staff recognizes they need to look at the various improvements and the adjacent land uses and how they impact each other. MPO staff is working on the Bicycle/Pedestrian Master Plan, which will be reviewed by the TCC once it is completed. Ms. Ward said that with 25 local governments it is often difficult to achieve complete consistency on how this would be implemented but it is hoped that complete streets is a concept that the local governments will develop and support countywide. She showed a series of photographs that were general examples of the Complete Streets concept. She said that when Mr. Bartolotta provides his presentation of the Bicycle/Pedestrian Master Plan there will be photos of existing conditions in Pinellas County as well as things that we would want to change through the program. Ms. Ward indicated that Complete Streets provides accommodation for everyone no matter how they travel to make sure they are safe, comfortable and convenient.

Ms. Ward reviewed information and survey data made available by Smart Growth America and the National Complete Streets Coalition. She noted that survey data showed that people want mobility options and be able to walk or bike short distances, but most feel they do not have a choice. She noted that data also indicates that the younger generation is looking to get around without a vehicle. Ms. Ward said that all of this ties directly to the discussion regarding constraining roadways and providing mobility options but making sure they are safe and convenient. She said that everyone is a pedestrian at some point so it is important to make sure there are safe passageways for pedestrians. Ms. Ward also noted that at the E-Town hall meeting held the previous night regarding transit, someone asked what the community was doing to prepare for the aging population in terms of transportation services. Ms. Ward said that it will not be just looking at roadway infrastructure, but also additional emphasis on what the adjacent land uses are as we look at constrained roadways, transit and bike ped facilities which will be critical to that decision making. Ms. Fisher noted the PPC is looking at the land use part of Complete Streets recommendations with regard to the Countywide Plan Update.

During the presentation, Mr. Bertels asked about a picture showing a yellow crosswalk noting that white is mandated. Ms. Ward responded she would give that information to Mr. MacAulay of MPO staff.

Ms. Ward indicated this will be part of the various elements staff will bring forward as part of the LRTP. The TCC will be asked to assist the MPO staff in developing implementing policies for the MPO Plan, in addition to items they might want to include in their local government plans. MPO staff is looking at a task force of representatives from the TCC and the Bicycle and Pedestrian Committees and others to meet to develop a policy that can be brought to the MPO.

Chairman Dicus noted that several jurisdictions are already implementing Complete Streets concepts to add bike lanes, sidewalks, etc. He said that he anticipated lots of support for the Complete Streets concept; however, many roadways that could accommodate bike lanes, for instance, are County roads within municipalities. The challenge will be how to move them forward for funding, implement and coordinate the designs, and consider the community impact and construction. He said that has been a challenge in Largo. Ms. Ward will include this in the discussions with the County as to how to work with the municipalities if they have a vision for those facilities.

Ms. Ward noted that, at their recent meeting, the BAC decided to have a subcommittee work with staff on the Bicycle/Pedestrian Master Plan. She invited anyone interested to attend the October 28th workshop at 8:30 a.m. in the Planning Department Conference Room. Mr. Bartolotta said he would send a notice to the TCC members and everyone was welcome to attend. Ms. Ward added that there may be other meetings, but this was a good place to start.

## 7. TAMPA BAY TRANSPORTATION MANAGEMENT AREA

Ms. Ward indicated the MPO had received a letter earlier in the year suggesting the three MPOs of Pinellas, Pasco, and Hillsborough consider forming a single MPO or, as an alternative, enhance the process already in place. A meeting was held in May at the TBRPC office and a follow-up meeting was held on September 13th. At the May meeting, it was decided to maintain the single county MPOs. They agreed to form a working group to determine what they wanted to achieve within the urbanized area and whether to establish a formal process, which were the objectives of the September



13 meeting. The group decided they wanted to focus on several things including transit and ways they could better compete for discretionary funds. FDOT indicated that the TMA funds will now be allocated at the TMA level and not the local level. The TMA is the urbanized area of Pinellas, Pasco, and Hillsborough counties. The Transportation Alternatives Program will need to be looked at from a regional perspective and the funds will be allocated on a regional level beginning in 2014. They were also told that they will have to decide how to allocate between \$35 million and \$40 million in highway dollars on a regional level. The consensus was to establish a formal group called the Tampa Bay TMA to be comprised of three representatives from each MPO. Each MPO will be asked to ratify the agreement that was reached. In addition, they are looking at combining the Chairs Coordinating Committee with TBARTA to discuss the larger regional issues. Mr. Dicus added that the group is looking at a near-term project they could support that would be successful.

Ms. Ward noted that she attended a meeting of the beach communities and Paul Steinman, FDOT District 7 Secretary, was in attendance. He discussed the economic benefits that could be realized if the region could come together and speak with one voice regarding major projects. It would be beneficial to FDOT in trying to bring dollars to the area and also to the business community. He was complementary of the number of Pinellas County MPO members who attended the TMA meetings.

8. FY 2013/14 – FY 2017/18 TRANSPORTATION IMPROVEMENT PROGRAM (TIP) AMENDMENT(S)

There were no TIP amendments brought to the TCC this month.

9. TCC REVIEW OF ADVISORY COMMITTEE RECOMMENDATIONS

There were no committee recommendations for TCC review.

10. OTHER BUSINESS

A. Uniform Trail/Roadway Signage Treatments

Ms. Harvey indicated a meeting was scheduled for October 3rd at the DEI Building located at Drew Street and U.S. 19 with Sprinkle Consultants to go over the report regarding the uniform trail treatment.

B. Memorial Causeway Bicycle Speeding Issue

Ms. Harvey noted a draft report was included in the packet and that more detail will be brought forward at a later time.

C. Multi-Named Roads

Ms. Harvey reported the MPO established a subgroup to meet, which included Karen Seel, Doreen Hock-DiPolito, and Jeff Danner, to review the multi-named roadways. They decided to recommend to the MPO that dual signage be used with the top line containing the county or state road number and the bottom line the local road name. This would eliminate trying to rename all the roads countywide. The County will develop cost estimates for the signage. In addition, the subgroup recommended renaming 102<sup>nd</sup> Avenue to Bryan Dairy Road from the bridge to the west.

11. MPO ACTIONS

Ms. Harvey indicated the MPO Newsletter/Action Sheet was included in the agenda packet for information.

12. ADJOURNMENT

The next TCC meeting is scheduled for October 23, 2013. The meeting was adjourned at 4:05 p.m.

HOWARD FRANKLAND BRIDGE (I-275/S.R. 93) REPLACEMENT UPDATE – FDOT

The Florida Department of Transportation (FDOT) plans to replace the aging northbound span of the Howard Frankland Bridge. A Project Development and Environment (PD&E) Study is currently underway to identify the best replacement options and document how the replacement will affect the surrounding environment. The PD&E study limits are from one mile west of to one mile east of the northbound Howard Frankland Bridge. This study is evaluating the costs and benefits of various alternatives for the replacement bridge location, which include consideration of modifications needed to the Causeway approaches and construction sequencing, among other factors.

In addition to the bridge replacement study, a transit corridor evaluation is underway to evaluate options for a transit connection within the bridge corridor that will link Pinellas and Hillsborough counties. The transit study is examining how transit could be included in the bridge replacements construction, and is evaluating two different options. One involves reserving a transit envelope on either side of the northbound or southbound lanes for the construction of a future transit structure. The other option involves adding the premium transit accommodations to the center of the new bridge, replacing two of the four express lanes that are proposed to be added to the northbound bridge structure over time. This item will include a presentation by FDOT staff. Comments and input from the CAC concerning the concepts are welcome.

ATTACHMENTS: [Howard Frankland Bridge Regional Transit Corridor Evaluation, September 2013](#)

ACTION: None required, informational item



# Howard Frankland Bridge Regional Transit Corridor Evaluation

SEPTEMBER 2013

Originally opened as a small segment of Interstate 75 (I-75), present day Interstate 275 (I-275) is now a vital link in the Bay area's transportation network. It is heavily used by commuters and truck traffic and is a critical emergency evacuation route for large portions of Pinellas and Hillsborough Counties. Regionally, I-275 is part of the National Highway System, and locally it is part of Florida's Strategic Intermodal System (SIS), the Department's network that provides for the high-speed, high-volume movement of people and goods.

The Howard Frankland Bridge is the central bridge spanning Old Tampa Bay from Clearwater/St. Petersburg to Tampa, Florida. It is one of three bridges connecting Pinellas County and Hillsborough County; the others being the Gandy Bridge and the Courtney Campbell Causeway. The Howard Frankland carries I-275 and is by far the most traveled of the bay area bridges; carrying an average of 142,000 vehicles per day across Tampa Bay. By 2040 that volume is expected to increase to more than 200,000 vehicles per day. Based on this projected traffic increase, the Florida Department of Transportation is conducting two regional studies: the **Tampa Bay Express Master Plan Study** to evaluate the feasibility of adding express lanes to Bay area interstates and the **Regional Transit Corridor Evaluation** to study the feasibility of adding a future premium transit service within the I-275 corridor.

## Congestion Across the Bay

For many commuters, daily gridlock is a fact of life. Many of us deal with traffic congestion on a daily basis. According to the US Department of Transportation (USDOT), **45% of traffic congestion is caused by preventable, recurring traffic issues.** Recurring traffic congestion occurs when too many vehicles use the same roads at the same time and there isn't enough space on these roads for everyone. Traffic congestion associated with most metropolitan areas can, and often does, have negative environmental, social, and economic effects.

To combat these effects, several congestion management options are being considered along I-275 within the Howard Frankland Bridge corridor. The first and more near-term option is the establishment of tolled express lanes. The addition of express or "managed" lanes is an innovative, low-cost alternative to traditional highway construction and the benefits (reduced congestion and fast, reliable travel times for commuters and buses) can be realized almost immediately.

The second, more long-term, consideration involves reserving or "setting aside" space within the I-275 corridor for premium transit in the future. The addition of a premium transit service will be needed to address our area's growing transportation challenges; however, the exact type of service is still being discussed by local agencies and area officials.

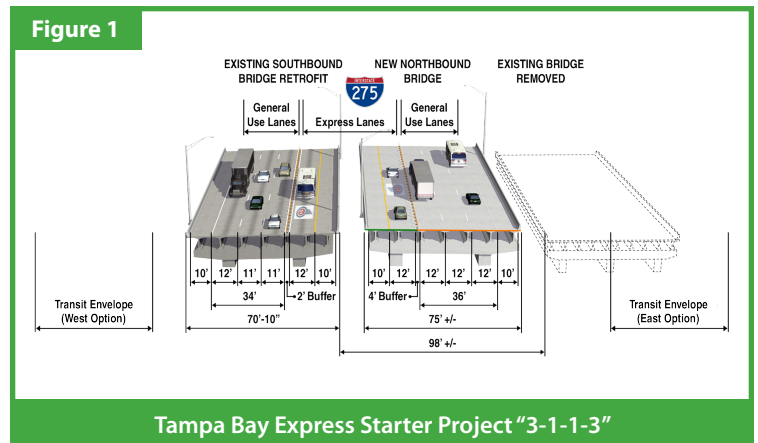
### Express Lanes: Beyond the Bridge

The FDOT is continuously working to improve Florida's transportation network; recognizing that congestion isn't limited to a specific roadway and doesn't end at a county line. This is why the Department is



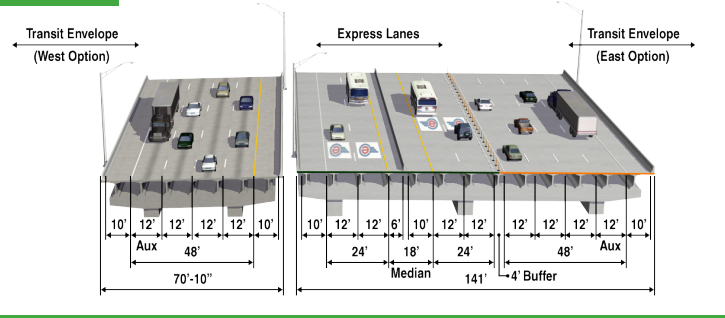
conducting the **Tampa Bay Express Master Plan Study** to evaluate a future **system** of tolled express lanes in order to provide additional capacity for interstate highways in the Tampa Bay area.

This system (Tampa Bay Express) could include more than 90 miles of express lanes along I-275, I-4, and I-75. The master plan study is developing both near-term, low-cost starter projects as well as long-term future investment projects. In regards to the Howard Frankland Bridges, the starter express lane concept consists of converting the auxiliary lane on both bridges to an express lane and leaving the remaining three lanes as general purpose lanes in each direction - commonly referred to as a "3-1-1-3" configuration (**Figure 1**). No additional construction would be required to implement this project along the bridge, except for future restriping and added signage.



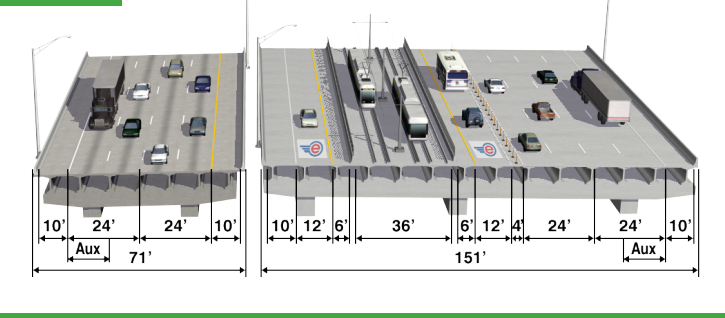
As traffic volumes continue to increase and additional express lanes are needed, the bridge would need to be widened. Since the northbound bridge is currently being evaluated, steps can be taken now to ensure that future expansion costs would be minimal. One of the suggested bridge expansion concepts includes reconfiguring the northbound bridge to carry two northbound and two southbound express lanes plus three general purpose lanes and an auxiliary lane. The southbound bridge would carry three general purpose lanes plus an auxiliary lane. This is commonly referred to as a "4-2-2-4" configuration (**Figure 2**). The new northbound replacement bridge could be constructed so that it could be easily retrofitted and widened to accommodate this option in the future.

Figure 2



Tampa Bay Express Long Term Project "4-2-2-4"

Figure 4



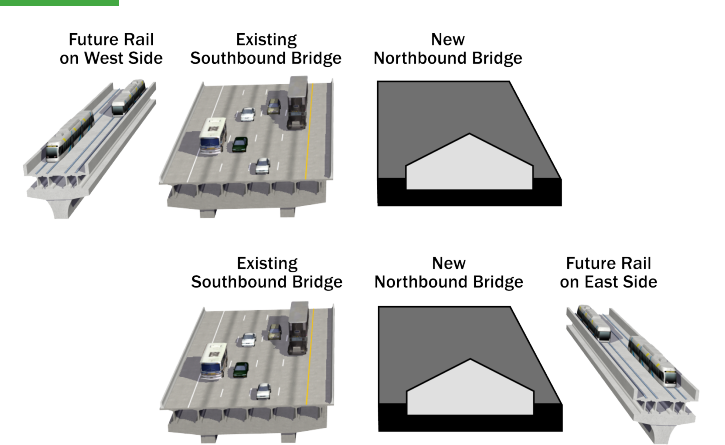
Long Term Express Lanes and Integrated Rail "4-1-R-1-4"

## Express Lanes Plus: A Premium Transit Option

As our region continues to grow, so should our transportation options. To better meet this future demand, the Department is conducting a transit study to evaluate the feasibility of providing a premium transit service within the I-275 corridor.

A key focus area of the **Regional Transit Corridor Evaluation** is the Howard Frankland Bridge and the unique challenges that implementing a premium transit service presents. Implementing a premium transit service requires early planning, community support, and agency cooperation. While the mode, or service type, is still being discussed by local agencies and area officials, the Regional Transit Corridor Evaluation recommends reserving or "setting aside" space within the bridge corridor right of way for **premium transit** service in the future. This space, also known as a **transit envelope**, can be located in one of three areas within the bridge corridor: to the west of the existing bridges, to the east of the existing bridges, or integrated into the center of the new northbound bridge.

Figure 3



Future Transit Options - Separate Rail Guideway

Both the west side and east side transit envelope options would involve the construction of a separate structure and would require additional study to determine the most cost-effective location (Figure 3); however, should the long-term express lane option "4-2-2-4" be implemented, the integrated transit option could be easily incorporated by removing one express lane in each direction. This modification would provide the space necessary to carry a premium transit option, like light rail transit (LRT), on the bridge between the two remaining express lanes. This option is referred to as a "4-1-R-1-4" configuration (Figure 4).

## At a Crossroads: Congestion Management and Transit Options

In order to ensure that we are fulfilling the needs of our transportation infrastructure in the years to come, we will need to look at the "big picture" for the Howard Frankland Bridge. The current PD&E study is only evaluating the replacement of the existing northbound bridge. Beyond considering an extra four feet of bridge width and a possible transit envelope, the study is not considering the environmental impacts of a wider structure or of a separate structure across Tampa Bay. Projects like those discussed above certainly won't come together overnight, but we need to start somewhere.

Our area would benefit from addressing this challenge sooner rather than later. The northbound Howard Frankland Bridge is more than 50 years old and has never been replaced. Since its original design and construction in the 1960s, residential and commercial growth has strained the corridor beyond its capacity, increasing delays and limiting economic activity. Although the bridge structure has been reinforced and repaired over the years, the northbound bridge is nearing the end of its useful life.

However, no single transportation agency can tackle this challenge alone. In addition to the Florida Department of Transportation, other agencies and local governments are involved in developing, implementing and maintaining regional projects, including the Tampa Bay Area Regional Transportation Authority (TBARTA) and the Metropolitan Planning Organizations in Pinellas and Hillsborough Counties. We will need to work together to achieve our shared goals.

This is our opportunity to do something new, while also addressing issues of congestion, pollution, land use and economic development. We must plan for our future now. Together, we can keep Florida at the forefront of the global economy.

For additional information on the **Regional Transit Corridor Evaluation** or the **Tampa Bay Express Master Plan Study**, please contact:

**Kirk Bogen, P.E.**  
Project Development Engineer  
813-975-6448  
kirk.bogen@dot.state.fl.us

You can also visit the project website: [www.mytbi.com/future-projects](http://www.mytbi.com/future-projects), then click on Howard Frankland Bridge.

GANDY BOULEVARD PROJECT, FROM WEST OF I-275 TO EAST OF U.S.19 – FDOT

The FDOT Project Manager will provide the TCC an update on the design plans for Gandy Boulevard (S.R. 694). The design of this segment of Gandy Boulevard (S.R. 694) has been under discussion with FDOT, Pinellas Park, St. Petersburg and the MPO. The project specifics being discussed are the approaches to U.S. Highway 19 and the intersection treatment at Grande Boulevard. FDOT will provide the latest information to the TCC.

ATTACHMENTS: None

ACTION: None required, informational item

**BECKETT BRIDGE PROJECT DEVELOPMENT AND ENVIRONMENTAL (PD&E) STUDY UPDATE**

Pinellas County, in conjunction with the Florida Department of Transportation (FDOT), is conducting a Project Development and Environment (PD&E) Study to evaluate the removal, rehabilitation, or replacement of the existing Beckett Bridge over Whitcomb Bayou in Tarpon Springs. The study was funded through a Local Agency Program (LAP) agreement with FDOT District 7. Tony Hornik is the Project Manager for Pinellas County. EC Driver & Associates, Inc. is under contract with Pinellas County to conduct the study.

The following alternatives were evaluated:

- No Build
- No Build with Permanent Removal of the Existing Bridge
- Rehabilitation of the Existing Bridge
- Replacement with a New Movable Bridge
- Replacement with a New Fixed Bridge (with 28 feet of vertical clearance)

The study began in January 2011. After careful consideration of many engineering, environmental and social factors, including the need for safe and efficient transportation, input from the community and local governments, project costs and consideration of public comments, replacement of the existing bridge with a new two lane movable bridge was selected as the “Recommended Alternative” by the County Staff. The proposed typical section is 47.2 feet wide and includes two 11-foot wide travel lanes, 6-foot wide sidewalks on both sides and 5.5-foot shoulders that could be used as “undesigned” bicycle lanes.

The Recommended Alternative will be presented to the Board of County Commissioners to request their concurrence on October 22, 2013. If the Board concurs with the Recommended Alternative, it will be presented at a public hearing in February 2014. After consideration of all public comments received at the public hearing, the Recommended Alternative will be submitted along with supporting environmental and engineering documents to the Federal Highway Administration (FHWA) for approval.

Public input is an important aspect of the study. Presentations to stakeholder groups were made early in the study to solicit input for the development of alternatives. The alternatives considered were presented at an Alternatives Public Workshop in January 2013. Because the bridge has been determined to be eligible for listing in the National Register of Historic Places, a Cultural Resources Committee was also established to conduct “good faith consultation” with affected parties to address Section 106 requirements.

Ann Venables, URS (previously EC Driver and Associates) consultant, will give a brief presentation about the current status of the study and discuss the Recommend Alternative. Comments and input from the CAC concerning the concepts are welcome.

ATTACHMENT: [Locator Map: Beckett Bridge in Tarpon Springs](#)

ACTION: As deemed appropriate based on discussion



MPO 2040 LONG RANGE TRANSPORTATION PLAN UPDATE

A. Follow-up on the Congestion Management Process Implementation Plan

The MPO is in the process of finalizing the Congestion Management Process Implementation Plan for Pinellas County. Through close coordination with our partner agencies, the Technical Coordinating Committee and other MPO advisory committees, 14 locations were selected to be reviewed through this effort. The Plan identifies small scale capital projects, such as intersection modifications, transit enhancements, and sidewalk, bike lane, trail and signal timing [reviews](#) in order to enhance the operation of the facility. Resulting recommendations could also include identifying the need for a more in-depth analysis/study such as an access management or corridor study.

The initial stage of the Plan's development resulted in the identification of the 14 locations for further analysis. The locations were reviewed, associated studies and previous actions were considered, and recommendations drafted. The drafted recommendations were reviewed through the MPO's technical and advisory committees and all of the comments were considered. The recommendations were finalized, locations were ranked for priority based on the proposed criteria in the Congestion Management Process (CMP) Policy and Procedures Manual (60:40), and cost estimates were developed.

At the last TCC meeting, the MPO staff agreed to present the final Plan recommendations at the October meeting. These final recommendations and other findings will be brought to the other technical and advisory committees for final approval during October and November. The final Congestion Management Process Implementation Plan with recommendations for the 14 locations studied by URS is attached.

As mentioned at the last meeting, there are currently additional corridor evaluations underway through separate efforts. This includes a Pinellas County review of Starkey Road/Keene Road/Park Street (C.R.1). The final priority list for the Congestion Management Process implementation will include all corridors being reviewed that are ranked and prioritized using the same methodology.

The TCC will continue to take the lead role in the prioritization of the CMP efforts as the various parts of the program are developed.

This item will include a presentation by Linda Lockhart, URS staff.

ATTACHMENTS: [Final Congestion Management Process Implementation Plan Recommendations](#)

ACTION: Recommend approval of the Congestion Management Process Implementation Plan

(Continued)



MPO 2040 LONG RANGE TRANSPORTATION PLAN UPDATE

B. Constrained Roads

In Pinellas County, there are a number of roads that are either currently deficient or projected to be deficient by the Long Range Transportation Plan (LRTP) horizon year (based on local government LOS standards) for which no additional through lanes are planned beyond those included in the current LRTP. These roads are proposed to be identified by the MPO as 'constrained' so that these corridors can be evaluated for solutions to relieve congestion, aside from capacity expansions. Concerning state roads, MPO staff sought and received guidance from the FDOT and was advised that state roads, excluding the Strategic Intermodal System (SIS), may be designated as constrained by local government or MPO policies.

The proposed constrained roads were presented to the TCC in September and a number of recommendations were made to MPO staff regarding the proposal. It was noted that a number of gaps existed along roadway corridors where some segments were identified as constrained, but others were not. It was recommended that where these gaps existed, and no capacity expansion is planned, the roadway segment be identified as constrained despite the fact that it wasn't projected to fail. MPO staff has done this and the results are incorporated into the map. Forest Lakes Road was removed from the list of constrained facilities as there is a capacity project planned for that corridor, but not currently in the adopted LRTP. In addition, U.S. Highway 19 from Park Blvd. to 49<sup>th</sup> St. was recommended to be added to the constrained roads list due to congestion issues and the expense of right-of-way necessary to add capacity to the segment. Through the MPO's Congestion Management Process, the feasibility of adding dual left turn lanes from U.S. 19 to Gandy will be considered. This segment was initially excluded due to the fact that it is currently a SIS facility, however the segment is planned to be removed from the SIS upon completion of the CR 296 (Future SR 690) project and there are no plans to widen the segment so it is being proposed to be identified as constrained.

The attached map and table denote the proposed constrained roads and identify the type of constraint on each facility. Those lines that are highlighted denote a change from the September TCC meeting. The TCC is asked to provide comment and recommend approval of the proposed constrained roads map. These roadways will be constrained for the purposes of identifying capacity projects for the 2040 LRTP and will be revisited in the future to address any changes in projected roadway volumes and policy direction at the state and local level.

ATTACHMENTS:     [Proposed Constrained Roads Map](#)  
                          [Proposed Constrained Roads Table](#)

ACTION:             Recommend approval of Proposed Constrained Roads

**CONGESTION MANAGEMENT PROCESS (CMP)**

**IMPLEMENTATION PLAN**

**RECOMMENDATIONS**

**OCTOBER 2013**

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## 1.0 INTRODUCTION

The Pinellas County Metropolitan Planning Organization (MPO) has prepared an implementation plan to address roadway corridors within the County that have congestion and other related issues. This plan approaches congestion with a multi-modal perspective. The roadways studied within this document have had known problems for many years. Multi-modal solutions may be a viable option to address congestion issues when standard capacity improvements have been unsuccessful or are not feasible.

### METHODOLOGY FOR SELECTED CORRIDORS

In order to determine the corridors to be studied, previous data sets and documents were considered. These sources included, but were not limited to:

- Pinellas County 2010 State of System (SOS) Report
  - 10 top congested Strategic Intermodal System (SIS) roadways
  - 25 top congested non-SIS roadways
  - 25 highest crash locations
- Draft Pinellas County 2012 SOS Report
- “Freight Hotspots” identified in Florida Department of Transportation District Seven (FDOT D7) Tampa Bay Regional Goods Movement Study (TBRGM)
- Local Government Priorities
- Pinellas MPO’s 2035 Long Range Transportation Plan (LRTP) Enhancement Projects

### SELECTION OF CORRIDORS FOR FURTHER STUDY

An initial list of corridors was presented to the Technical Coordinating Committee (TCC) in May of 2013 as well as a proposed list of corridors to move forward. These corridors depicted on Figure 1 were the result of the above research and collaboration.

As shown on **Figure 1**, the resulting list of corridors includes:

- 102<sup>nd</sup> Avenue North from 137<sup>th</sup> Street to Seminole Boulevard (Alt 19)
- 22<sup>nd</sup> Avenue North from 34<sup>th</sup> Street N. to I-275
- 62<sup>nd</sup> Avenue North from 66<sup>th</sup> Street N. to 49<sup>th</sup> Street N.
- Alternate 19 (Bayshore Blvd.) from Skinner Boulevard to Curlew Road
- Alternate 19 from Curlew Road to the Pasco County Line
- East Bay Drive (SR 686) from Belcher Road to US 19
- Park Boulevard from 66<sup>th</sup> Street N. to 49<sup>th</sup> Street N.
- Park Boulevard from 113<sup>th</sup> Street N. to Seminole Boulevard (Alt 19)
- Sunset Point Road from Edgewater Drive (Alt. 19) to Keene Road
- US 19 from 54<sup>th</sup> Avenue N. to Bryan Dairy Road
- US 19 from Main Street (SR 580) to Tarpon Avenue
- Nursery Road from Highland Avenue to US 19
- Belleair Road from Keene Road to US 19
- Indian Rocks Road from Walsingham Road to West Bay Drive

Figure 1: Corridor Location Map



The purpose of this study is to recommend multi-modal solutions that could help relieve congestion and improve safety. Interviews with City/County staff and other departments were conducted to identify solutions that have been proposed in the past; issues that need to be addressed; and recommended solutions currently being considered but not yet funded.

**Table 1** depicts each of the 14 corridors that were considered and the sources that went into the selection of these corridors based on the methodology listed above.

**Table 1: Congestion Management Process (CMP) Implementation Plan Corridor List with Source Data**

Roadway Corridor	Top 10 SIS or Top 25 Non-SIS from 2010 State of System Report	Top 25 Crash Locations Per State of System Report	LOS F in Level of Service Report	Freight Hotspots from FDOT TBRGM Study	Local Government Priority Listing	Enhanced Corridor identified in 2035 LRTP
102nd Ave N (137th St to Seminole)			X		X	X
22nd Ave N (34th St N to I-275)	X	X	X		X	
62nd Ave N (66 <sup>th</sup> St N to 49th St N)					X	
Alt 19 (Bayshore Blvd) (Skinner Blvd to Curlew Rd)	X		X		X	X
Alt 19 (Curlew Rd to Pasco County Line)	X		X	X	X	
East Bay Dr (SR 686) (Belcher Rd to US Hwy 19)	X					
Park Blvd (66th St N to 49th St)				X	X	
Park Blvd (113th St N to Seminole Blvd)	X		X	X	X	
Sunset Point Rd (Alt 19 to Keene Rd)						X
US 19 (Bryan Dairy Rd to 54 Ave N)	X	X	X	X		
US 19(SR 580 to Tarpon Ave)	X	X	X	X		
Nursery Rd (Highland Ave to US 19)						X
Belleair Rd (US 19 to Keene)			X			X
Indian Rocks Rd (Walsingham Rd to West Bay Dr)			X	X		X

## **DRAFT RECOMMENDATIONS**

Section 2 of this document lists the original recommendations for each of the 14 selected corridors from a multi-modal perspective. The initial recommendations were developed utilizing a multi-modal approach to corridors that have been reviewed many times in the past. These recommendations were not based on hard data such as turning movements or specific causes of congestion. These are multi-modal recommendations that could improve congestion and/or safety. It is important to balance safety with congestion management to reduce the number of non-recurring congestion incidents.

## **COORDINATION AND COMMENT**

Section 3 of this document outlines the presentations made and comments received on the initial draft document with recommendations on the corridors studied. This section also details the presentations made and comments received regarding the initial multi-modal recommendations.

## **EVALUATION AND RANKING**

Section 4 details the methodology used and resulting ranking of the 14 corridors considering congestion as well as safety. The methodology for evaluating and ranking the corridors was developed by MPO staff. The consultant assisted in the calculations in close coordination with MPO staff. In order to rank the corridors, it was determined that a 60:40 ratio between the congestion factor and the crash factor would be used to rank the corridors.

## **FINAL RECOMMENDATIONS**

Section 5 details the final recommendations of the CMP Implementation Plan. These recommendations will move forward for more detailed cost estimates in the future as funding sources are identified and/or specific projects are included in the 2040 Long Range Transportation Plan (LRTP). Section 5 includes order of magnitude cost estimates. Due to the multi-modal nature of these recommendations, many disciplines were involved in the development of these cost estimates.

## **2.0 INITIAL RECOMMENDATIONS**

This section provides the initial recommendations for multi-modal projects that could improve congestion and related issues along the corridors studied.

### **102<sup>ND</sup> AVENUE NORTH (CR 296): FROM 137<sup>TH</sup> STREET TO SEMINOLE BOULEVARD (ALTERNATE 19)**

#### ***Recommendations***

- **Alternative One:** Design a four-lane suburban parkway in order to continue the existing four-lane typical section from Ridge Road to Seminole Boulevard by widening the segment between 137<sup>th</sup> Street and Ridge Road to four lanes. This would provide continuity along 102<sup>nd</sup> Avenue N. The suburban parkway design uses landscaping and amenities to provide a green space setting that would be consistent with existing aesthetic treatments in the neighborhood.
  - Improve access management with combination of directional and full-median openings.
  - Add 5-foot-wide shoulders marked as bike lanes.
  - Complete the sidewalk connectivity along the entire route.

- **Alternative Two:** Develop a two-lane, suburban parkway concept for the segment between 137<sup>th</sup> Street and Ridge Road with 12-foot-wide lanes and 4-foot-wide shoulders.
  - Improve access management with combination of directional and full-median openings.
  - Add 5-foot-wide shoulders marked as bike lanes.
  - Complete the sidewalk connectivity along the entire route.
- Conduct an intersection study at 137<sup>th</sup> Street and implement an intersection design based on resultant needs of intersection. The study would include:
  - Turning movement counts
  - Signal warrants analysis
- Add an eastbound right-turn lane at Ridge Road.
- Add an eastbound right-turn lane at 113<sup>th</sup> Street intersection.
- Based on the Pinellas County safety audit, upgrade or add pedestrian facilities at 113<sup>th</sup> Street and 125<sup>th</sup> Street.
- Close the median opening at 114<sup>th</sup> Street.
- Construct a 15-foot-wide landscaped multi-use trail connecting the pathway system of east of the Walsingham County Park entrance to the Pinellas Trail crossing at Ashley Drive and eastward to 113<sup>th</sup> Street.
- Add 5-foot-wide shoulders marked as bike lanes from 113<sup>th</sup> Street to Seminole Boulevard and from Seminole Boulevard to the bridge, which includes sidewalks on both sides to the recreation fields at the north end of Lake Seminole Park.

## **22<sup>ND</sup> AVENUE NORTH: FROM 34<sup>TH</sup> STREET NORTH TO I-275**

### ***Recommendations***

- Study signal timing optimization at 34<sup>th</sup> Street intersection.
- Intersection improvements:
  - Extend the southbound left-turn lane on 28th Street and improve intersection to accommodate truck traffic.
  - Extend the 25th Street N. southbound left-turn lane to 22nd Ave to improve truck access from Lowes.
  - Improve the geometry, angle on the south approach, and turn lanes at 25th Street.
- Access Management:
  - Add raised median with adequately spaced full and directional median openings.
  - Identify and consider consolidating access points/driveways where feasible.
  - Make all driveways right in/right out.
- If proposed future light rail station is located at I-275 and 22<sup>nd</sup> Avenue N., consider elevated tracks over 22<sup>nd</sup> Avenue N.

## **62<sup>ND</sup> AVENUE NORTH: FROM 66<sup>TH</sup> STREET NORTH TO 49<sup>TH</sup> STREET NORTH**

### ***Recommendations***

- Upgrade roadway to urban standards:
  - Complete design for four-lane divided typical section with median, build two lanes with drainage and sidewalks located to not prohibit potential future design of four lanes.
  - Include left-turn lanes where needed at intersections.
  - Widen to four lanes if traffic demand warrants.
- For access management, construct a 16-foot-wide local access road on both sides for local residential access between 53<sup>rd</sup> Street N. and 65<sup>th</sup> Street N.



- Close intersections with 64<sup>th</sup> Street N., 62<sup>nd</sup> Way N., 64<sup>th</sup> Terrace, McKee Lake Drive, and 54<sup>th</sup> Street N. on the north side and between 60<sup>th</sup> Avenue N. and 3<sup>rd</sup> Street on the south side. Have connection with local access road, not main roadway.
- Add 8-foot multi-use sidewalks along both sides inside of local access road.
- Include left turn lanes at 62<sup>nd</sup> Street N. (signalized).
- Consider removing as truck route to improve safety. (Trucks can use Park Boulevard to the north or 38<sup>th</sup> Avenue N. to the south. Both are multi-lane facilities that provide good east-west connections to I-275 and US 19.)

## **BAYSHORE BOULEVARD (ALTERNATE 19): FROM SKINNER BOULEVARD TO CURLEW ROAD**

### ***Recommendations***

- Provide exclusive right-turn lanes along corridor where R/W permits.
- For access management, evaluate options for reducing direct access points along the corridor.
- Pedestrian/bike safety improvements:
  - At Michigan Avenue (in the northbound direction) add overhead pedestrian activated “No Right On Red” blank out light. The light would be activated by pedestrian crossing signal. Add special surface treatment to the Pinellas Trail crossing.
  - At Curlew Road (in the eastbound direction) add overhead pedestrian activated “No Right On Red” blank out light. The light would be activated by pedestrian crossing signal. Also improve signage for trail and remove sign clutter.
- Transit:
  - At locations with concrete pads and shelters, extend and taper the shoulder pavement to produce a pull out. This will get the buses out of the traffic flow during loading and unloading. Post “Yield to Bus” signs to allow buses to pull back into traffic.
  - Where right-turn lanes are located, consider locating bus stops at the back of the lane. This will allow buses to get out of the main traffic stream by using the right-turn lane as a pull out. Use surface paint to indicate the bus pull out area.
  - Consider consolidating 10 bus stops (five each direction) on this 0.5-miles segment from Pasadena Drive to San Salvador Drive. Propose removing the following stops.
    - San Jose Drive (one)
    - Cevera Drive (two)
    - Buena Vista Drive S. (one)
    - Buena Vista Drive N, (two)

## **ALTERNATE 19: FROM CURLEW ROAD TO THE PASCO COUNTY LINE**

### ***Recommendations***

- Improve traffic signals by providing mast arms, improve signage, and pavement markings.
- Add a northbound right-turn lane at Meres Boulevard beginning south of the south entrance to the Sweetbay shopping center.
- Complete sidewalks on both sides. Where the road closely parallels to the Pinellas Trail, divert the bike lane to make use of the trail. (For example: From Georgia Avenue to 800 feet south of Maryland Avenue.)
- Add southbound right-turn lane at Dodecanese Boulevard
- Add southbound left-turn lane at Curlew Place
- Transit:

- At locations with concrete pads and shelters, extend and taper the shoulder pavement to produce a pull out. This will get the buses out of the traffic flow during loading and unloading. Post “Yield to Bus” signs to allow buses to pull back into traffic.
- Where right-turn lanes are located, consider locating bus stops at the back of the lane. This will allow buses to get out of the main traffic stream by using the right-turn lane as a pull out. Use surface paint to indicate the bus pull out area.
- At the Helen Ellis Hospital, add a pedestrian bridge over the drainage ditch between the sidewalk and the bus stop pad or add a new sidewalk from the driveways to the bus stop pad in front of the drainage ditch. Widen the paved shoulder between the driveways to facilitate a bus pullout. Add a pedestrian activated crosswalk in front of the northbound bus stop. Add yellow flashers to the overhead signal mast arm.
- Remove two on-street parking spaces northbound south of E. Dr. Martin Luther King Jr. Drive in Tarpon Springs and designate as a bus stop pull out. Similar treatments should be considered at other locations in Tarpon Springs to accommodate bus pull outs.

### **EAST BAY DRIVE (SR 686): FROM BELCHER ROAD TO US 19**

#### ***Recommendations***

- Belcher Road intersection:
  - Consider additional turn lanes at Belcher Road and/or extend left-turn storage by modifying (relocating) the median to allow for additional storage in the inside left-turn lane.
  - Adjust signal phasing between Belcher Road and Bedford Circle for westbound traffic during the PM peak period. This will reduce backups from Belcher Road blocking Bedford Circle. The signal at Bedford Circle should be adjusted to allow westbound traffic to clear the intersection and provide room for vehicles to enter westbound East Bay Drive.
  - Elevate pork chop islands to provide better pedestrian refuge.
- Access Management
  - Improve access management by reducing conflict points. Convert full-median openings at non-signalized intersections to directional openings with longer turn bays on East Bay Drive and right-in and right-out only from the minor side streets and commercial driveways. Consolidate driveways where possible.

### **PARK BOULEVARD (CR 694): FROM 66<sup>TH</sup> STREET NORTH (SR 693) TO 49<sup>TH</sup> STREET NORTH**

#### ***Recommendations***

- Access Management.
  - Convert right lane to buses and right-turns only. This should help reduce congestion due to same side driveway and minor side street ingress and egress. It should also help reduce congestion and potential accidents caused by sudden lane changes when buses stop along the route. It will also improve the bus flow along the corridor.
  - Convert some of the full-median openings to directional openings with longer turn bays to encourage the use of the parallel street on either side of Park Boulevard. Reduce the number of non-signalized minor street intersections with full-median access. Space full access medians a minimum of 0.25 miles apart.
- Provide exclusive eastbound right-turn lane at 66<sup>th</sup> Street.
- Consider consolidating of bus stops.

## **PARK BOULEVARD (CR 694): FROM 113<sup>TH</sup> STREET NORTH TO SEMINOLE BOULEVARD (ALTERNATE 19)**

### ***Recommendations***

- This corridor is approaching the traffic demand for six-lanes. The area has been improved with larger medians to address previous problems, but congestion still exists. Explore the possibility of converting back to a six-lane typical section. The segment to the east of the corridor is already six lanes.
- Consider making the eastbound right lane west of Seminole Boulevard an exclusive right-turn lane only. At the same time, make the northbound right-turn lane to eastbound Park Boulevard a protected movement by installing a raised channelization/pedestrian refuge island at the southeast corner.
- To address truck crash incidents, add separated signal heads for greater visibility. Install overhead “right turn must turn right” sign on westbound approach to Seminole Boulevard. Install advanced warning street signs in all directions.
- Access Management.
  - Reduce median width present at several locations. Alternative median design can be explored to increase capacity.
  - Modify the median opening at 74<sup>th</sup> Avenue right-in/right-out only. Eliminate the eastbound left-turn lane at 74<sup>th</sup> Avenue/111<sup>th</sup> Street N. and make the westbound left-turn lane directional for school bus access only.

## **SUNSET POINT ROAD: FROM EDGEWATER DRIVE (ALTERNATE 19) TO KEENE ROAD**

### ***Recommendations***

- Review right-of-way (ROW) opportunities to support implementing multi-modal solutions in this corridor. R/W varies significantly along the corridor from a minimum of 36 feet west of the Pinellas Trail crossing to 100 feet from west of Kings Highway to Keene Road.
- Improve sidewalk continuity.
- Consider upgrading to a suburban typical section with 10-foot lanes and 4-foot shoulders marked as bike lanes and a 4-foot sidewalk on the south side from Alternate 19 to the Pinellas Trail.
- From the Pinellas Trail to Keene Road continue with the suburban typical section with 11-foot travel lanes and 5-foot bike lanes. Complete sidewalks on both sides along the outer limits of the ROW. This will permit future expansion if or when this becomes necessary. Add right-turn lane eastbound to southbound Highland Avenue.

## **US 19 (34<sup>TH</sup> STREET NORTH): FROM 54<sup>TH</sup> AVENUE NORTH TO BRYAN DAIRY ROAD**

### ***Recommendations***

- Need comprehensive transit study on corridor. Consider consolidation of bus stops. Midblock pedestrian connections at bus stops (i.e. connect with pedestrian activated flashing yellow crossing signals at each side and in the median, consider zigzag crossing/Danish offset in the median at the crosswalk/bus shelter locations.)
- Prioritize project to provide dual lefts for southbound US 19 to eastbound Gandy Boulevard.
- Also consider right lane bus/right-turn only to allow for buses to stop and not impede traffic from 54<sup>th</sup> Avenue N. to Gateway Boulevard. This treatment currently exists between Gateway Boulevard and 46<sup>th</sup> Avenue N.

## **US 19: FROM MAIN STREET (SR 580) TO TARPON AVENUE (CR 582)**

### ***Recommendations***

- Need comprehensive transit study on corridor. Consider consolidation of bus stops. Connect with pedestrian activated flashing yellow crossing signals at each side and in the median, include zigzag fencing in the median at the crosswalk/bus shelter locations.
- Also consider right lane bus/right-turn only to allow for buses to stop and not impede traffic.

## **NURSERY ROAD: FROM HIGHLAND AVENUE TO US 19**

### ***Recommendations***

- Corridor is not currently congested. However, the new interchange at parallel Belleair Road may result in new traffic patterns and need for improvements. (Note, currently traffic on southbound US 19 can access Nursery Road. However, with a new interchange at Belleair Road traffic on Nursery Road could change.)
- Improve to urban typical section with 4-foot shoulders marked as bike lanes and fill in sidewalk gaps on the south side west of Belcher Road (under construction).
- Complete intersection improvements at Belcher Road, Keene Road, and Highland Avenue to include left- and right-turn lanes and pedestrian upgrades.

## **BELLEAIR ROAD: FROM KEENE ROAD TO US 19**

### ***Recommendations***

- Intersection improvements at Belcher Road planned to include pedestrian refuge area and other safety improvements.
- A new interchange at US 19 is expected to be completed spring 2015. Conduct a traffic study within one year after the opening of the US 19 to review new traffic patterns and determine if further improvements are needed.
- Complete Access Management study to determine what specific projects will have the most effect on congestion and safety.
- Complete lighting study on corridor.
- To accommodate retaining the oak canopy east of Sharon Way, the median can be widened to envelope the trees (40 feet) with a single lane constructed on either side. The multi-use trail would shift to the median through this section forming a linear park that connects to the Progress Energy Trail. The existing sidewalk on the north and south side would remain throughout this segment to serve neighborhood pedestrians.
- A 12-foot multi-use path should be constructed on the south side from Eagle Lake Park, (located at the southwest corner of Belleair Road and Keene Road). The path would eliminate the need to add bike lanes to the roadway.

## **INDIAN ROCKS ROAD: FROM WALSINGHAM ROAD (SR 688) TO WEST BAY DRIVE (SR 686)**

### ***Recommendations***

- Intersection improvements at West Bay Drive imperative.
  - These include repaving, pedestrian facility upgrades, new striping, etc.
  - Eliminate the small pedestrian island at the northeast corner of West Bay Drive and realign the crosswalk from the southeast to the northeast corners or make it larger

similar to the one at the southwest corner. Consider a “No Right on Red” signal northbound on Indian Rocks Road at West Bay Drive.

- Mast arm needed.
- Pedestrian facilities need to be upgraded and made simpler.
- For Access Management, evaluate access management to reduce number of direct access points to Indian Rocks Road.
- R/W varies between 60 and 100 feet with most of the corridor at 80 feet or more. Private property encroachment is a problem with some businesses including over half of their parking within the existing ROW.
- With over 15,000 vehicles per day, the roadway is failing and adding sidewalks and bike lanes to a two-lane typical section will not improve the LOS. Since Indian Rocks Road is the only corridor that runs north-south along the west side of the county between two east-west corridors that connect to the barrier island beaches traffic will remain a significant problem. Based on the available ROW, a two-lane suburban typical section could be constructed with 10-foot lanes, a 12- to 16-foot median, and 4-foot shoulders with 4-foot sidewalks where right-of-way is available. Short left-turn lanes could be included at most minor residential intersections. New traffic signals should be considered for Wilcox Road and 8<sup>th</sup> Avenue S.W. with southbound left-turn lanes at 8<sup>th</sup> Avenue S.W., and northbound and southbound left-turn lanes at Wilcox Road. The distance between these two intersections is 1.5 miles and they are located between 0.5 and 0.75 miles from the signals at West Bay Drive and Walsingham Road. The bridge between North Circle and Avocado Drive will have to be widened along with the bridge located immediately south of Oak Manor Lane. The business located at the southwest corner of Jeff Road will lose 15 parking spaces that are currently located within the ROW. Additionally, the entrance signage to the New Atlantis Club will also be taken due to the location within the ROW. No median would be constructed in the short segments where the existing R/W is less than 75 feet.
- A pedestrian activated flashing yellow crosswalk should be included in front of the Suncoast Hospital at the bus stop location on the southbound side. A concrete pad and sidewalk to the driveway should be constructed to allow handicapped passengers to access the hospital.
- Relocate stop bar at Rosemary Lane as identified in the TBRGM Study.

### **3.0 COORDINATION AND COMMENT**

#### **INITIAL INPUT**

As stated in the introduction, the consultant presented to the TCC meeting on May 22, 2013 and received approval to move forward with the 14 selected corridors. An email was sent out by MPO staff to TCC members on June 17, 2013 to solicit comments and identify any historical improvements or long standing issues on these corridors. Comments were received from several members that were considered when developing the draft recommendations.

#### ***Input to Draft Recommendations***

City of Seminole replied on 06/17/2013:

- For 102nd Avenue, consider some improvement to where 102nd and Old Ridge meet, there is a pinch in the roadway that forces west bound traffic from 2 lanes to one lane, unless you

want to turn right to go northbound on Old Ridge. Add sidewalks and or bike lanes along the segment, since there will not be a widening of the roadway from 2 Lane U to 4 Lane D.

- Maybe a longer wait for trail users to cross 102nd Ave during rush hour in the morning and evening who use the pedestrian override light.
- For Park Blvd between Seminole and Park, all you can do is make sure the sidewalk system is complete and ADA compliant. There is not a lot you can do, since the R/W is narrow and the school and the pedestrian override light are located along this stretch.

City of Pinellas Park replied on 06/19/2013:

- US 19 (54 Ave N to Bryan Dairy Rd) The segment south of Mainlands Blvd is the congested area. The choke point is US 19 at Gandy/Park Blvd. Southbound US 19 to eastbound Gandy has historically been problematic; drivers use Gateway Center to cut through to Gandy. This south to east movement is in much need of attention. Northbound US 19 at this point is also problematic. Turning movement or timing modifications would not make sufficient impact on flow to improve the situation. The re-alignment of the overpass at this location would, however, be an improvement.
- When the Wal-Mart store was originally developed, FDOT indicated that there may be a need for an overpass in the vicinity of 82nd Ave. I do not see how this would be feasible with the Wal-Mart, Gateway Centre, Mainlands Blvd., & Golden Gate connections. This segment of US 19 North of Gandy/Park Blvd if changed would negatively impact many businesses and lives in the area. Alternative means of ingress and egress to these properties on this corridor may be something to consider.

City of Belleair Bluffs 06/21/2013:

- The improvements to Indian Rocks Road are a tremendous help, very well done. I will say that a mast arm at the intersection of Indian Rocks Road and West Bay will be a great help in aiding traffic during emergency situations. During the no name storm the lights that fell, blocked the intersection, obviously not allowing residents safe passage to hospitals or the primary exit off the beach for hurricane evacuation. The city of Belleair Bluffs has been trying to get a mast arm at this intersection for some time now, if for nothing else, safety. The reconstruction of the Belleair Bridge is a great asset, but moving people off the beach only to have them stall at the intersection doesn't do any good for anybody. Let's complete the road by installing the mast arm for unincorporated county, beach, and inland residents of the area. Thanks for allowing us to comment.

City of Dunedin 06/28/2013:

- Alt 19 from Skinner to Curlew – This segment's congestion is AM and PM weekday peak hours and weekend issues at Alt US 19/Curlew for the beaches. The segment is mainly residential with commercial at side street nodes. There is limited right-of-way. Other than signal timing I do not know what else could be done. We could look at the Pinellas Trail crossings at Michigan, Palm and Curlew so close to Alt 19. Drivers on Alt 19 do not always look for Trail users when they turn. The blank out sign for northbound-to-eastbound traffic at Alt 19/Curlew has been suggested for the eastbound-to-southbound lane. Also, pedestrian overpasses have been suggested. Designated truck route, PSTA & Jolly Trolley route, only one lane each direction with suicide lane, very large number of residential driveways requiring residents to back into the roadway. Golf cart crossing at Palm Blvd.;

sidewalk terminates at Cedar Creek bridge; frequent residential garbage and recycling collection stops (twice weekly) blocks thru lanes. Uncontrolled and undesignated pedestrian mid-block crossings from west side to reach the east side Trail.

- Alt 19 from Curlew to the north – There are missing segments of sidewalk that FDOT will be adding with the 2014 resurfacing project. I think they will be adding raised islands near Curlew as well. Can the islands be landscaped? Pinellas Trail pedestrian and bicycle traffic congestion is significant with spur to Honeymoon Island. Honeymoon Island draws a large number of tourist unfamiliar with the area, as well as boat and Jet Ski trailers. Any consideration of installing mast arm signals as was once proposed?

City of Largo 06/06/2013:

- Indian Rocks Road and West Bay Drive Intersection. West Bay Drive is the primary evacuation route from the beaches and serves multiple communities.
  - This span wire intersection desperately needs to be replaced with a mast arm.
  - We have multiple pedestrian crossing issues as well.

After the initial comments and coordination with members and MPO staff, the consultant conducted field work and developed an initial draft set of recommendations.

### **DRAFT RECOMMENDATIONS REVIEW AND PRESENTATIONS**

The initial draft set of recommendations (included in Section 2), as well as existing conditions, were documented in a draft Congestion Management Process Implementation Plan that was distributed to the TCC, Intelligent Transportation Systems (ITS) Committee and the Citizens Advisory Committee (CAC) for review and input. These recommendations were presented to the TCC on August 28, 2013, the ITS Committee on September 4, 2013, and the CAC on September 26, 2013. The committees were asked to comment on the document and **Table 3-1** details their comments.

**Table 3-1: Comment Matrix from TCC and ITS Committees**

<b>Corridor</b>	<b>Comment</b>	<b>Agency</b>
102nd Avenue N	102nd Avenue N is constrained by City Resolution to 2 lanes.	City of Seminole
102nd Avenue N	Needs to be four lanes for county through traffic.	ITS Committee Member at Large
Park Boulevard West	Request that you do not reduce the median on Park Boulevard between Seminole and 113th as the City has invested considerable funds in that area.	City of Seminole
Park Boulevard West	Converting the controlled median cuts along Park Blvd is not an acceptable option. They were just converted to a controlled median cut within the last decade and fewer drivers are trying to CROSS the Blvd. I suspect that there are fewer accidents because of the controlled median cuts.	City of Pinellas Park
US 19 and Park Boulevard East	Bus Only Lanes are not likely to be possible.	City of Clearwater
62nd Ave N	Potentially consider building circular driveways on 62nd Avenue.	MPO Staff

**Table 3-1: Comment Matrix from TCC and ITS Committees**

<b>Corridor</b>	<b>Comment</b>	<b>Agency</b>
62nd Ave N	Shifting the truck traffic to Park Blvd needs to be studied further. How many of those 211 trucks that used this facility during the study were not making deliveries to the local area and how many were using the road to go through the area to reach the industrial area between 49th St and US 19?	City of Pinellas Park
62nd Ave N	When upgrading the road it really should be a 2 LD (3lanes) standard. That is what was being discussed for the 49th to US 19 segment and it works elsewhere in the community.	City of Pinellas Park
62nd Ave N	Do not consider the 16' wide local access road for both sides of the mainline! Develop a 3-lane facility (2LD) with trails on the sides of the road.	City of Pinellas Park
62nd Ave N	If the road is operating at a good LOS – Do not close intersections. That only limits access for residents and emergency response vehicles. Closing these access points would only increase the response time and put lives in danger.	City of Pinellas Park
62nd Ave N	No mention of the proximity to Bauder Elementary, Nina Harris School, or Rawlins Elementary. I believe that these schools are within 2 miles of the roadway.	City of Pinellas Park
22nd Avenue N	Concern about reducing access points to existing businesses on 22nd Avenue.	St. Petersburg
Park Boulevard East	In paragraph #3 they refer to Park Station as a historic rail station. Please remove the term historic. There is nothing historic about that station. It is not even on the site of the original station.	City of Pinellas Park
Park Boulevard East	Continuous right-turn /bus lanes. really? Buses usually stay in the right lane. And consolidate bus stops? If only buses and cars turning right are using the right turn lane would there really be a need to reduce the number of bus stops?	City of Pinellas Park
US 19 54th to Bryan Dairy	US 19 from 49th St to Bryan Dairy is better described as a limited access expressway than as it is described in paragraph 3 second sentence - principal arterial.	City of Pinellas Park
US 19 54th to Bryan Dairy	Of the businesses along the corridor, the Expo Center was identified. The Expo Center has not been in operation for over 3 years. After the Expo Center failed it was used by Frito Lay. Now a manufacturing venture is in there - I believe.	City of Pinellas Park
US 19 54th to Bryan Dairy	Two Wal-Marts were identified in this corridor. Be specific and be right. One Wal-Mart Super Center that has everything and the other is the Wal-Mart Neighborhood Market. The latter is a grocery store of normal size with Wal-Mart's buying power.	City of Pinellas Park
US 19 54th to Bryan Dairy	Why must all bus stops be consolidated? People don't want to walk as it is. So you have fewer bus stops at a greater distance?	City of Pinellas Park
US 19 54th to Bryan Dairy	They saw the need for a dual left at the US 19 / Park southbound location, but failed to see the larger picture of the overpass as the north-south choke point.	City of Pinellas Park



**Table 3-1: Comment Matrix from TCC and ITS Committees**

Corridor	Comment	Agency
US 19 54th to Bryan Dairy	In regards to the last bullet. Please explain it to me. I am at a complete loss. They say to consider using a Bus/R-Turn Only lane between 54th Ave North to Gateway Blvd. and indicates that this same configuration is currently in use on US 19 between Gateway Blvd and 46th Ave N. The first road segment is a subset of the second. It doesn't work.	City of Pinellas Park
Alt 19 from Skinner to Curlew	<ol style="list-style-type: none"> <li>1. Page 15 photo is southbound at Curlew.</li> <li>2. Page 15 - The blank out sign should be a no right on green. The pedestrian crosses Michigan when northbound is green. Same thing with Curlew. The sign should be a no right on green for eastbound pedestrians to cross.</li> <li>3. The State and City worked together to remove extra signage on Alt US 19 between Michigan and Curlew, but signs at the Curlew intersection are still excessive.</li> <li>4. The recently completed FDOT resurfacing project eliminated mast arms for the Michigan and Palm signalized intersections.</li> </ol>	City of Dunedin
Alt 19 from Curlew to Pasco County Line	<ol style="list-style-type: none"> <li>1. FDOT is currently in design for the resurfacing north of Curlew. Missing sidewalk sections are in the plans.</li> <li>2. The reason for bike lanes and sidewalks for Alt US 19 right adjacent to the Pinellas Trail is that the Pinellas Trail is not always open.</li> </ol>	City of Dunedin
Sunset Point Road	<p>We suggest that the County consider the use of painted/green bike lanes for this corridor. Green bike lanes have been successfully implemented in several cities including St. Petersburg (along westbound 1st Ave N.) and Tallahassee. Green bike lanes serve dual purposes; they increase safety for cyclists by providing a perceived barrier (this perception of increased safety has implications in increasing bike trips and reducing car trips), and they also serve as a relatively low cost “billboard” highlighting the City’s support for multi-modal transit. Additionally, the Federal Highway Administration has recently offered its support of protected bike lanes and green lanes <a href="http://www.fhwa.dot.gov/environment/bicycle_pedestrian/guidance/design_guidance/design_flexibility.cfm">http://www.fhwa.dot.gov/environment/bicycle_pedestrian/guidance/design_guidance/design_flexibility.cfm</a>).</p>	City of Clearwater Planning
Sunset Point Road	<p>Green bike lanes are wonderful in an ideal world where there is no limitation on maintenance dollars. However, in the City of Clearwater we are charged with maintaining our infrastructure to a high degree and I am just wondering who is going to pay for the maintenance of all these painted or thermoplastic bike lanes.</p>	City of Clearwater Traffic
Sunset Point Road	<p>As transit improvements continue along US 19 in light of the City’s US 19 Corridor Redevelopment Plan, continuous investment in the City’s bike infrastructure should be important moving forward so that infrastructure is in place to accommodate transit users and existing residents. Specifically for the Sunset Point Road corridor, the generous width of the</p>	City of Clearwater Planning

**Table 3-1: Comment Matrix from TCC and ITS Committees**

<b>Corridor</b>	<b>Comment</b>	<b>Agency</b>
	right-of-way for a portion of the corridor might be conducive to protected bike lanes. Additionally, the City’s Community Development Code designates this corridor as a “secondary corridor” under Section 3-1203 “Scenic Corridors,” and provides requirements for landscaping along this corridor.	
US 19 North	The City of Clearwater’s US 19 Corridor Redevelopment Plan emphasizes the City’s support of transit along this corridor, and of course we would be supportive of a transit analysis.	City of Clearwater Planning
Nursery Road	Again, as with the Sunset Point Road corridor, we suggest that the County consider the use of painted/green bike lanes for this corridor for reasons expressed herein.	City of Clearwater Planning
Nursery Road	Green bike lanes are wonderful in an ideal world where there is no limitation on maintenance dollars. However, in the City of Clearwater we are charged with maintaining our infrastructure to a high degree and I am just wondering who is going to pay for the maintenance of all these painted or thermoplastic bike lanes.	City of Clearwater Traffic
Nursery Road	The City’s Comprehensive Plan designates this corridor as a “corridor to preserve”, and the City’s Community Development Code designates this corridor as a “secondary corridor” under Section 3-1203 “Scenic Corridors,” and provides requirements for landscaping along this corridor.	City of Clearwater Planning
Belleair Road	We are supportive of the multiuse lane/linear park concept provided in the recommendations for this corridor. We suggest that the County consider the use of Low Impact Development (LID) when designing this trail, as Pinellas County is currently in the process of updating its code to allow for and incentivize LID. The City of Clearwater has also budgeted to review and update its code to be in accordance with future Pinellas County regulations for LID, and the City’s planning staff is very supportive of the use of LID due to its cost savings, site design benefits, and environmental considerations.	City of Clearwater Planning
Belleair Road	Additionally, this corridor is specified within the City of Clearwater Comprehensive Plan’s objective to “use the city-wide design structure as a guide to preserve the existing character of certain corridors within the City of Clearwater” (Goal A.5.3). Specifically, Policy A.5.3.3 maintains that the City “support the unique character of Belleair Road by maintaining... the unique tree canopy formed over the right-of-way.”	City of Clearwater Planning
Belleair Road	The City’s Comprehensive Plan also designates this corridor as a “corridor to preserve” (Attachment to Map A-14), and the City’s Community Development Code designates this corridor as a “secondary corridor” under Section 3-1203 “Scenic Corridors,” and provides requirements for landscaping along this corridor.	City of Clearwater Planning
US 19 North	1) “Consider consolidation of bus stops. Connect with pedestrian activated flashing yellow crossing signals at each side..... This is	Pinellas County Schools

**Table 3-1: Comment Matrix from TCC and ITS Committees**

Corridor	Comment	Agency
	a recommendation for US 19, what are the chances of 55mph+ traffic stopping for a yellow flashing light? Pedestrian bridges or tunnels seem to me to be the safest way to cross this high speed highway in between controlled intersections.	
Alt 19 Palm Harbor	Section 6, pg 17, Alt 19 Palm Harbor: 1) recommendation to remove bike lanes and use trail on Alt 19 in Palm Harbor, the trail is used for recreational riders, with a posted speed limit of 20 mph, professional bike riders will not use the trail due to the speed limit, number of stop signs, and congestion of walkers and slow moving bicycles. Bike lane on Alt 19 needs to remain. 2) there is no mention of the pedestrian crossing on Alt 19 at Delaware, this crossing has no street lighting, it is very difficult to see pedestrians in the cross walk when it is dark, it is used by students walking/biking to Palm Harbor Univ at 6:30 a.m., upgrade ped crossing with street lighting and ped activated flashers.	Pinellas County Schools
General	The corridor narratives read more like a Road Safety Assessment rather than a congestion management plan. Adding “pork chops” can be counterproductive to pedestrian safety at some intersections. [Traffic Operations]	FDOT Traffic
General	Intent to implement access management practices to reduce conflicts in several cases... generally a good thing, if the focus is to reduce crashes..... but diverting left turns can lead to increased demand at other locations.	FDOT Traffic
General	Keep in mind that safety and congestion mitigation efforts are a compromise. Added safety measures such as crosswalks and signals can often create more delay and congestion. Signal timing changes can sometimes lead to increased delay.	FDOT Traffic
Park Boulevard East	The Park Boulevard corridor is stated as NOT deficient. But is a good candidate for Access Management measures..... OK.	FDOT Traffic
Park Boulevard East and US 19	Disagree with recommendation to convert the right lane to a bus only /right turn lane where this treatment is recommended. Bus pull outs should be constructed even if additional R/W is needed.	FDOT Traffic
General	While the Department can accept the recommendations of the study for consideration, it should be emphasized that they are just recommendations and may be rejected during the design stage.	FDOT Traffic
General	A graphic of each corridor showing the locations of recommended improvements would be helpful.	FDOT Traffic
General	Recommendations for improvements to state roads must be reviewed by FDOT Traffic Operations, and Access Management staff.	FDOT Corridor Management
General	The general recommendations skewed toward road safety rather than a true congestion management analysis and plan.	FDOT Corridor Management

**Table 3-1: Comment Matrix from TCC and ITS Committees**

<b>Corridor</b>	<b>Comment</b>	<b>Agency</b>
General	Implement of access management practices to reduce conflicts; we recommend a traffic and truck percentage analysis be conducted and analyzed prior to any implementation.	FDOT Corridor Management
General	Added crosswalks and signals can often create more delay and congestion. Therefore, a level of service analysis is required.	FDOT Corridor Management
Park Boulevard East/US 19	As stated by Traffic Operations, converting the right lane to a bus only /right turn lane adds congestion and merge issues at intersections. Also this treatment affects adjacent businesses and driveways. Was this issue presented to the community and adjacent businesses? Instead, we recommend "Bus pull outs" be constructed.	FDOT Corridor Management
General	Close coordination with on-going or funded projects along state facilities will facilitate the recommendations made throughout the document	FDOT Traffic
General	Safety should be one major factor in developing CM corridors & CM recommendations. Current methodology mentioned in this report did show "high crash locations" is one of the criteria. However, none of these high crash locations (used by this report) are shown in the FHWA 5%/transparency report.	FDOT Safety
General	If MPO is looking for safety \$ help related to safety recommendations, detailed crash data analysis should be done. Several state corridors did not mention crashes.	FDOT Safety
General	Most of these corridors have heavy transit usage. Has transit factor been considered for impact to congestion & (ped) safety?	FDOT Safety
Alt 19	First recommendation regarding extending and tapering the shoulder pavement to produce a pull out might be constraint by R/W and need to comply with design standards.	FDOT Traffic
Alt 19 Curlew to Pasco county Line	Adding a southbound right turn lane at Dodecanese Blvd. might be prohibitive due to high cost of ROW. Need further analysis before recommendation is made.	FDOT Traffic
Alt 19 Curlew to Pasco county Line	Add southbound left-turn lane at Curlew Place: Need to analyze traffic volumes before this recommendation is adopted. There are no turning movement counts within the report that could support this recommendation.	FDOT Traffic
Alt 19 Curlew to Pasco county Line	At the Helen Ellis Hospital, to add a pedestrian activated crosswalk in front of the northbound bus stop and add yellow flashers to the overhead signal mast arm. This report will need to do a pedestrian crossing study for the crossing at the hospital.	FDOT Safety
East Bay Drive Belcher to US 19	Belcher Rd intersection: Need additional clarification as to what turn lanes should be considered for additional lanes. All the approaches at the intersection have dual left turn lanes currently.	FDOT Traffic
East Bay Drive Belcher to US 19	Access Management: Convert full-median openings at non-signalized intersections to directional openings might not be	FDOT Traffic

**Table 3-1: Comment Matrix from TCC and ITS Committees**

Corridor	Comment	Agency
	justifiable from an operations stand point if there are no crashes that would justify the closure. It might cause issues at signalized intersections.	
East Bay Drive Belcher to US 19	Lengthening the turn bay and the coordinated timing analysis will improve operation. The only big hurdle I see here is all the median modifications and driveway consolidation.	FDOT Safety
US 19 (34th St. N) from 54th Ave. N to Bryan Dairy Rd.	First recommendation regarding mid-block pedestrian connections at bus stops, (i.e. connect with pedestrian activated flashing yellow crossing signals at each side and in the median, consider zigzag crossing/Danish offset in the median at the crosswalk/bus shelter locations.) might not be the safest recommendation for pedestrians crossing a six-lane divided arterial with AADTs ranging from 48,429 to 63,500 vehicles per day. This needs to be closely coordinated with the Safety office.	FDOT Traffic
US 19 (34th St. N) from 54th Ave. N to Bryan Dairy Rd.	Prioritize project to provide dual lefts for southbound US 19 to eastbound Gandy Blvd.: R/W acquisition cost along this section of US 19 is very prohibitive due to presence of commercial retail. This needs to be closely coordinated with R/W acquisition office.	FDOT Traffic
US 19 (34th St. N) from 54th Ave. N to Bryan Dairy Rd.	Comprehensive Transit study – Agreed, on this 5-mile corridor, a full study of transit activity, ped locations, etc. would be key to any improvements.	FDOT Safety
US 19 (34th St. N) from 54th Ave. N to Bryan Dairy Rd.	Dual SB Lefts at 19 & Gandy – This proposed project should be coordinated with the other US-19/Gandy projects that are upcoming to determine need and ability to implement.	FDOT Safety
US 19 (34th St. N) from 54th Ave. N to Bryan Dairy Rd.	Right-turn/bus lanes - This should be looked at with the Comprehensive transit study	FDOT Safety
US 19 from Main St. (SR 580) to Tarpon Ave (CR 582)	The recommendations provided might need to be revisited once construction of FPID 429005-1 is completed. Several comments regarding bicycle facilities along the corridor and consolidation of bus stops will be addressed with that project. Coordination with PSTA was completed during the design stages and the plans were sent to the MPO for review and comments. Construction of FPID 429005-1 is schedule to start September, 2013.	FDOT Traffic
US 19 from Main St. (SR 580) to Tarpon Ave (CR 582)	Many sections within the 5% report for a 2013 resurfacing – recommendations are for the Work Program to be reviewed to include safety enhancements. Road Safety Audit.	FDOT Safety
US 19 from Main St. (SR 580) to Tarpon Ave (CR 582)	Comprehensive transit study – This should be a detailed transit/safety review	FDOT Safety
US 19 from Main St. (SR 580) to	Right lane/shared bus lanes – With the study above could review.	FDOT Safety

**Table 3-1: Comment Matrix from TCC and ITS Committees**

<b>Corridor</b>	<b>Comment</b>	<b>Agency</b>
Tarpon Ave (CR 582)		
Park Boulevard from 66th Street N to 49th Street N	Convert outside lane to buses and right turn only – This would require some capacity analysis. According to the report, the AADT is current 47,492. This is probably LOS F for a 4-lane roadway, which may be okay as traffic may shift to parallel roadways. Would want to avoid neighborhood cut through.	FDOT Safety
Park Boulevard from 66th Street N to 49th Street N	Modifications to access management – On the surface this is a good suggestion. Reducing full access point could reduce friction, access conflicts, and provide for addition of mid-block pedestrian treatments at non full-movement access points.	FDOT Safety
Park Boulevard from 66th Street N to 49th Street N	Exclusive eastbound right turn lane at 66th Street – It is assumed that this is a new lane, not converting the through lane. This would increase pedestrian exposure.	FDOT Safety
Park Boulevard from 66th Street N to 49th Street N	Consider consolidating bus stops – consolidating stops should be accompanied by reviewing the locations for proximity to ped crossings, signalized intersections, and areas that have refuge.	FDOT Safety
102nd Avenue N	Pages 7-9, 102nd Ave N. For this corridor the report gives two alternative recommendations. However, there is no basis for choosing one over the other. If Recommendation 1 is preferred over Recommendation 2, we should say so and indicate why	FDOT Safety
62nd Ave N	Pages 12-13, 62nd Ave N. This corridor is not congested (LOS B). Why is it in the report at all? Recommendations include design for 4-lane section when existing 2-lane is at LOS B. Are we expecting a lot of growth here? R/W acquisition looks like it will be very difficult. Four-laning this roadway will displace many homes. The do-nothing alternative may be a good one here.	FDOT Safety
Alt 19	Pages 14-15, Bayshore Blvd. For the third bullet under “Recommendations,” I suppose that you mean by this a raised median in this three-lane section? Bullet one indicates that R/W issues may preclude widening.	FDOT Safety
Sunset Point Road	Pages 24-25, Sunset Point Rd. It does not indicate what the existing lane widths are, but the third bullet under “Recommendations” indicates 10 foot lanes with a 4 foot shoulder on each side. If the existing lane width is 12’ this modification may reduce the capacity and thus degrade the LOS.	FDOT Safety
Indian Rocks Road	Page 34-35, Indian Rocks Rd. The last sentence indicates that R/W encroachment is an issue. Has this been turned over to code enforcement? Can we (and/or should we) do something about this?	FDOT Safety
General	It is Largo Staff’s understanding that the CMP Document will provide the recommendations used to identify projects and set priorities for the LRTP 2040 Update due in December 2014. With the condition of the Pinellas transportation system and the challenges facing the community it is important that the LRTP	City of Largo

**Table 3-1: Comment Matrix from TCC and ITS Committees**

Corridor	Comment	Agency
	provides a strategic approach to managing congestion and improving mobility on the countywide roadway network.	
General	One concern with the current CMP Document is that it is not clear how the basis for selection of roadway corridors and roadway segments examined in the CMP Document ties back to goals for congestion management and mobility on the countywide roadway network. Additionally, the goals and objectives for the CMP are not clear.	City of Largo
General	Many of the recommended improvements identified in the draft CMP Document appear to be good as stand-alone improvements, however it is not clear how the individual improvements are linked back to achieving identified goals for the countywide roadway network or to the anticipated improvements/impacts to congestion and mobility along the roadway corridor should the recommendations be implemented.	City of Largo
General	It is difficult to understand from the CMP Document how the proposed improvements will mitigate congestion or improve mobility on a system wide basis. Largo Staff makes the following recommendations: a) Identify the CMP objectives used to guide program development. b) A clear methodology needs to be developed to identify constrained corridors and improvement needs. c) The constrained roadway map appears to be a good starting point for identifying the roadway corridors within the countywide roadway network to include in the CMP analysis. d) Based on the built-out condition of Pinellas County, the limited available R/W for additional vehicle travel lanes, and the lack of and deficiencies in non-SOV modes (Transit, Bike and Pedestrian) it is recommended that all roadways (collector and above) other than the SIS roadways be categorized as constrained. This should be explored further with MPO Committees. e) All roadways in the network would then be a part of the CMP analysis including the SIS roadways. Suggest that the roadway corridors and segments selected for further examination be based on multiple factors, in addition to Vehicle V/C, including travel time, reliability, existence/quality of other modes, and existence or lack of advanced operational and demand management strategies. f) It is recommended that CMP Strategies for mitigating congestion and improving mobility on constrained corridors be developed drawing from local, County, and State plans/initiatives already developed or underway (for example the Largo Multi-modal Plan, other local agency multi-modal plans, PSTA Bus Plan, County ITS Plan, FDOT Freight Study, FDOT Managed Lane Study, etc.) g) The strategies, once developed, can then be clearly identified up front in the CMP Document. h) It is recommended that from the strategies, specific techniques	City of Largo

**Table 3-1: Comment Matrix from TCC and ITS Committees**

Corridor	Comment	Agency
	<p>be developed and suited to the characteristics and needs/deficiencies of each roadway corridor. This should be explored further with MPO Committees. Suggest the following techniques be considered:</p> <ul style="list-style-type: none"> <li>• Operational Improvements: managed lanes; bus only lanes; transit signal priority; access management (reduce conflict points and turns on/off road, install median islands and eliminate/consolidate median openings, provide adequate and safe crossings for bike/ped, reduces congestion and improves safety for all modes); ITS – signal optimization; intersection improvements;</li> <li>• Mobility Enhancements – improve safety and mobility for bike and pedestrians; improve level of service for bike and pedestrian on parallel facilities; better pedestrian connectivity; road diets; on-road improvements for bike; improve transit facilities;</li> <li>• Demand Management – Ride share; flexible work hours; transit use promotion or incentives; land use restrictions and incentives;</li> </ul>	
General	<p>Once the corridors are selected, then the developed strategies and techniques should be applied to the applicable deficiencies to develop specific projects and initiatives.</p>	City of Largo
General	<p>A balanced approach should be considered for the process of selecting and prioritizing projects for the LRTP:</p> <ul style="list-style-type: none"> <li>• For each proposed improvement, the impact to all modes should be weighed;</li> <li>• Consider using the measure of person travel time instead of vehicle travel time to account for multiple modes;</li> </ul>	City of Largo
General	<p>The CMP document does not appear to address improvements to support the bus plan:</p> <ul style="list-style-type: none"> <li>• The Greenlight Pinellas Vision includes a grid bus system that appears to depend on more routes and more buses;</li> <li>• The bus plan and the LRTP need to be well integrated and coordinated;</li> <li>• The CMP document needs to identify the necessary improvements to the road network to make buses more efficient and shorten travel time for bus routes; this needs to be a key focus of the LRTP; Travel time for bus routes should be a key CMP performance measure;</li> </ul>	City of Largo
General	<p>The CMP Document should identify inter-agency cooperation points, needs and opportunities.</p>	City of Largo



## 4.0 ANALYSIS AND RANKING OF CORRIDORS

This section details the methodology used and resulting ranking of the 14 corridors, for consideration of congestion and safety. In order to rank the corridors, it was determined that a 60:40 ratio between the congestion factor and the crash factor would be used to rank the corridors. The congestion factor was determined by a formula that multiplied the volume to capacity (v/c) ratio and the duration of congestion (doc):

$$v/c * doc = \text{congestion factor (ConF)}$$

Next, the crash factor (CrashF) was calculated using three years of crash data (2010-2012). The total crashes on each corridor were divided by three to determine the resultant average per year. Then, to normalize the analysis, this number was divided by the number of miles on each segment. The CrashF = crashes per mile per year.

Next the weights were applied and the ConF was multiplied by the CrashF and weighted based on the 60:40 ratio. The resultant formula was:

$$\text{ConF (1.6)} \times \text{CrashF (1.4)} = \text{Weighted Score}$$

Finally, the corridors were ranked based on this weighted score. **Table 4-1** depicts this resultant ranking.

**Table 4-1 Ranked Corridors**

Ranking	Roadway Segment	ConF	CrashF	Weighted Score
1	Park Blvd (113th St N to Seminole Blvd)	13.55	193.57	292.67
2	US 19 (Main St. 580 to Tarpon Ave)	18.95	109.08	183.04
3	22nd Ave N (34th St N to I-275)	6.96	109.17	163.97
4	East Bay Dr (SR 686) (Belcher Rd to US Hwy 19)	10.98	93.33	148.23
5	US 19 (54 Ave N to Bryan Dairy Rd)	13.70	58.72	104.12
6	Park Blvd (66th St N to 49th St)	4.77	66.29	100.44
7	Belleair Rd (US 19 to Keene)	9.92	46.36	80.77
8	102nd Ave N (Seminole to 137th St)	11.20	34.51	66.24
9	Indian Rocks Rd (Walsingham Rd to West Bay Dr)	13.18	22.62	52.75
10	Alt 19 (Curlew Rd to Pasco County Line)	15.91	17.15	49.47
11	Alt 19 (Bayshore Blvd ) (Skinner Blvd to Curlew Rd)	13.27	17.21	45.32
12	62nd Ave N (49th St N to 66th St N)	0.00	24.62	34.47
13	Sunset Point Rd (Alt 19 to Keene Rd)	3.06	19.00	31.49
14	Nursery Rd (Highland Ave to US 19)	0.00	12.97	18.16

## 5.0 FINAL RECOMMENDATIONS FOR APPROVAL

This section details the final recommendations for approval. As detailed in Section 3, the initial list of corridors to study was approved by the TCC in May 2013. Draft recommendations were presented to the TCC in August 2013 and to the ITS Committee and CAC in September 2013. Comments were received and considered when developing the final recommendations. The corridor ranking methodology detailed in Section 4 was then applied to rank the corridors and final recommendations.

Four recommendations were eliminated due to a substantial number of comments that were unfavorable to the proposed multi-modal improvements including the following.

- Right-turn only and bus-only lanes
  - US 19 between 54<sup>th</sup> Avenue N. and Bryan Dairy Road
  - US 19 between SR 580 and Tarpon Avenue
  - Park Boulevard from 66 Street N. to 49<sup>th</sup> Street N
- Proposed local access road on 62<sup>nd</sup> Avenue N

**Table 5-1** details the recommendations, cost estimates, and assumptions supporting the cost estimates. The recommendations are ordered first by corridor rank, and then by cost.

**Table 5-1 CMP Implementation Plan Proposed Recommendations**

Corridor Ranking	Corridor	Location	Proposed Recommendation	Study Cost	Construction Cost	Assumptions for Cost Estimate
1	Park Blvd (113th St N to Seminole Blvd)	Entire Corridor	Detailed corridor study required to determine specific causes of congestion on this corridor. Consider making the eastbound right lane west of Seminole Boulevard an exclusive right-turn lane only. At the same time, make the northbound right-turn lane to eastbound Park Boulevard a protected movement by installing a raised channelized/pedestrian refuge island at the southeast corner. <sup>2</sup>	\$100,000	TBD After Study	Extensive public involvement, coordination, traffic counts, conceptual engineering, access management, detailed crash analysis, Queue Analysis Tech Memo, transit study.
2	US 19 (SR 580/Main Street to Tarpon Ave)	Entire Corridor	Comprehensive Transit Study.	\$75,000	TBD After Study	Bus stop and ridership analyses, transfer study, potential for relocation of stops or consolidation, potential bike/ped mid-block crossings and impacts to traffic. R/W requirements if Bus Pull outs are required.
2	US 19 (SR 580/Main Street to Tarpon Ave)	Tampa Road Intersection	Complete FDOT Freight Quick Fix project (SE corner tight) from TBRGM Study at Tampa Road intersection. This project has been pre-engineered but not yet programmed for construction.		Cost to be Determined by FDOT	
3	22nd Ave N (34th St N to I-275)	34th Street (US 19) intersection	Signal timing study to optimize movements at 34th Street. Study should include special attention to the turning movement from southbound 34th Street to eastbound 22nd Ave N. as mentioned as an issue in TBRGM Study involving traffic signal timing. Consider truck movements specifically, along with bus and auto movements.	\$10,000	\$10,000	Counts, Highway Capacity Software (HCS) Tech Memo.
3	22nd Ave N (34th St N to I-275)	Entire Corridor	Detailed corridor study to determine specific causes of congestion, including turning movements, access/conflict points, freight activity, and potential justification for median(s). <sup>2</sup>	\$120,000	TBD After Study	Public involvement, coordination, traffic counts, conceptual engineering, access management, detailed crash analysis, Queue Analysis Tech Memo, transit study, special events ramps to/from I-275 (but not entire interchange).

Notes: <sup>1</sup> Cost Estimates are preliminary for planning purposes only and not intended for design, right-of-way and construction purposes.

<sup>2</sup> For the purposes of this study, detailed corridor studies are estimated at an average of \$100,000 per mile due to needed public involvement, engineering, and planning activities.

<sup>3</sup> There are no R/W costs included in these estimates. All construction costs include 15% for design, 15% for CEI and 25% contingency.

**Table 5-1 CMP Implementation Plan Proposed Recommendations**

Corridor Ranking	Corridor	Location	Proposed Recommendation	Study Cost	Construction Cost	Assumptions for Cost Estimate
3	22nd Ave N (34th St N to I-275)	28th Street Intersection	Intersection improvements based on TBRGM Study at 28th Street and improve intersection to accommodate truck traffic. (Consider extending the southbound left-turn lane on 28th Street to accommodate truck traffic).		Cost to be Determined by FDOT	
3	22nd Ave N (34th St N to I-275)	25th Street Intersection	Extend 25th Street N. southbound left-turn lane to 22nd Ave N to improve truck access to Lowes. Improve geometry at southeast corner and move stop bar back at 25th Street intersection. Refer to TBRGM Study.		Cost to be Determined by FDOT	
4	East Bay Dr (SR 686) (Belcher Rd to US Hwy 19)	Belcher Road Intersection	Belcher Road intersection improvements. Consider Safety Audit by Pinellas County in 2011 relative to improvements already implemented and other recommendations. Consider extended left-turn storage by modifying the median. Need detailed intersection study with turning movements, signal timing, transit movements and pedestrian improvements. Consider pork chop islands. Consider adjusting the signal timing at Belcher Road and Bedford Circle to allow cars to access East Bay from Bedford Circle in peak periods.	\$50,000	TBD After Study	Traffic counts, Access Management, Modeling Queue Analysis Tech Memo.
4	East Bay Dr (SR 686) (Belcher Rd to US Hwy 19)	Entire Corridor	Detailed Corridor Study to determine specific causes of congestion on this corridor. <sup>2</sup>	\$100,000	TBD After Study	Public involvement, coordination, traffic counts, conceptual engineering, access management, detailed crash analysis, Queue Analysis Tech Memo, transit study.
5	US 19 (54 Ave N to Bryan Dairy Rd)	Gandy Boulevard Intersection	Evaluate future need to provide dual lefts for southbound US 19 to eastbound Gandy Boulevard. Direct connection to I-275 is moving north to 118th in the future. R/W is unknown.	\$50,000	\$500K - \$1M (if dual lefts are warranted)	HCS counts, R/W requirements.
5	US 19 (54 Ave N to Bryan Dairy Rd)	Entire Corridor	Comprehensive Transit Study.	\$75,000	TBD After Study	Bus stop and ridership analyses, transfer study, potential for relocation of stops or consolidation, potential bike/ped mid-block crossings and impacts to traffic. R/W requirements if Bus Pull outs are required.

Notes: <sup>1</sup> Cost Estimates are preliminary for planning purposes only and not intended for design, right-of-way and construction purposes.

<sup>2</sup> For the purposes of this study, detailed corridor studies are estimated at an average of \$100,000 per mile due to needed public involvement, engineering, and planning activities.

<sup>3</sup> There are no R/W costs included in these estimates. All construction costs include 15% for design, 15% for CEI and 25% contingency.

**Table 5-1 CMP Implementation Plan Proposed Recommendations**

Corridor Ranking	Corridor	Location	Proposed Recommendation	Study Cost	Construction Cost	Assumptions for Cost Estimate
5	US 19 (54 Ave N to Bryan Dairy Rd)	54 <sup>th</sup> Ave N. intersection	Evaluators noted corner curb damage on the SW corner. Turns are difficult due to the amount of traffic on 34th Street (US 19). Corner radius requires trucks to make wide turn into center lane of 34th St SB. Identified in TBRGM Study. Consider minor modification of the corner clip.		Cost to be Determined by FDOT	
5	US 19 (54 Ave N to Bryan Dairy Rd)	54 <sup>th</sup> Avenue N. Intersection	Modify the southbound right-turn corner radius. Southbound right turn radius is too tight for large trucks. Identified in TBRGM Study.		Cost to be Determined by FDOT	
5	US 19 (54 Ave N to Bryan Dairy Rd)	64th Ave N.	Extend turning bay to facilitate truck movements northbound @ 64th Ave N. Identified in TBRGM Study.		Cost to be Determined by FDOT	
6	Park Blvd (66th St N to 49th St)	Entire Corridor	Comprehensive Transit Study. A right lane/bus only lane was previously recommended, but is not feasible based on feedback from committee members.	\$75,000	TBD After Study	Bus stop and ridership analyses, transfer study, potential for relocation of stops or consolidation, potential bike/ped mid-block crossings and impacts to traffic. R/W requirements if Bus Pull outs are required.
6	Park Blvd (66th St N to 49th St)	Entire Corridor	Detailed Corridor Study to determine specific causes of congestion on this corridor. Consider exclusive eastbound right turn lane at 66th Street. <sup>2</sup>	\$175,000	TBD After Study	Public involvement, coordination, traffic counts, conceptual engineering, access management, detailed crash analysis, Queue Analysis Tech Memo, transit study.
7	Belleair Rd (US 19 to Keene)	Entire Corridor	Lighting Study	\$50,000	TBD After Study	

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<sup>2</sup> For the purposes of this study, detailed corridor studies are estimated at an average of \$100,000 per mile due to needed public involvement, engineering, and planning activities.

<sup>3</sup> There are no R/W costs included in these estimates. All construction costs include 15% for design, 15% for CEI and 25% contingency.

**Table 5-1 CMP Implementation Plan Proposed Recommendations**

Corridor Ranking	Corridor	Location	Proposed Recommendation	Study Cost	Construction Cost	Assumptions for Cost Estimate
7	Belleair Rd (US 19 to Keene)	Sharon Way to Progress Energy Trail	Between Sharon Way and the Progress Energy Trail, divert the eastbound lane south of the oak trees and develop a linear park in median incorporating oak trees and the community trail. Recommend a divided roadway to allow the separation of vehicles going eastbound from those going westbound. This project would need to involve a certified arborist and a survey to determine exactly how wide the median should be to accommodate the existing oak trees and to ensure their survival during construction. One quarter of a mile R/W 100 feet. Recommend Feasibility Study that includes a survey that includes a tree survey, arborist report, and concept designs. Certified Arborist study to confirm survey of trees, determine condition of trees and develop mitigation strategies needed to protect trees during construction.	\$100,000	TBD After Study	Includes survey and certified arborist analysis of segment as well as concept engineering and technical memorandum.
7	Belleair Rd (US 19 to Keene)	Entire Corridor	Detailed Corridor Study to determine specific causes of congestion and impacts of new interchange at US 19.	\$100,000	TBD After Study	Public involvement, coordination, traffic counts, conceptual engineering, access management, detailed crash analysis, Queue Analysis Tech Memo, transit study.
7	Belleair Rd (US 19 to Keene)	Keene Road to Progress Energy Trail	Add 12- ft. multi-use trail from Eagle Lake Park on the southwest corner of Keene Road to the Progress Energy Trail. This trail would be included in the median within the portion of the new typical section.		\$845,000	Trail from Eagle Lake Park to the Progress Energy Trail. 1.72 miles @ \$316,800 = \$545,000 plus 15% design, 15% CEI, and 25% contingency.
7	Belleair Rd (US 19 to Keene)	Belcher Road Intersection	Complete planned intersection improvements at Belcher Road.		Programmed by Pinellas County	

Notes: <sup>1</sup> Cost Estimates are preliminary for planning purposes only and not intended for design, right-of-way and construction purposes.

<sup>2</sup> For the purposes of this study, detailed corridor studies are estimated at an average of \$100,000 per mile due to needed public involvement, engineering, and planning activities.

<sup>3</sup> There are no R/W costs included in these estimates. All construction costs include 15% for design, 15% for CEI and 25% contingency.

**Table 5-1 CMP Implementation Plan Proposed Recommendations**

Corridor Ranking	Corridor	Location	Proposed Recommendation	Study Cost	Construction Cost	Assumptions for Cost Estimate
8	102nd Ave N (Seminole to 137th St)	113th Street Intersection	Improve pedestrian facilities at the 113th Street Intersection. Study should balance impacts to congestion with improving pedestrian safety.	\$15,000	\$15,300	Check crash data safety audit capacity analysis Traffic Tech Memo. Add crosswalks, add four ped walk signals on existing refuge islands: Includes 356 ft. of new crosswalks @ \$12.54/ft. and four pedestrian crossing signals on poles to be placed in existing raised concrete islands @\$1,350 each plus 15% design, 15% CEI, and 25% contingency.
8	102nd Ave N (Seminole to 137th St)	137th Street Intersection	Conduct an intersection study at 137th Street to eliminate the 4-way stop and implement an intersection design based on resultant needs of intersection. Study should include examination of original intent of the current intersection design, including interaction with school crossing. Review and address existing need for improvements to balance pedestrian safety and improving congestion.	\$15,000	TBD After Study	Check crash data safety audit capacity analysis Traffic Tech Memo.
8	102nd Ave N (Seminole to 137th St)	137th Street to Ridge Road	Public Involvement Program to determine whether a four-lane or two lane configuration from 137th Ave. N to Ridge Road would be considered to address regional and community connectivity. This project would include concept designs, turning movement evaluation, needed access points, and design charettes with local residents and stakeholders. Need to determine where equestrian activities are occurring and propose potential solutions to support crossing of horses. Also, need to consider Pinellas Trail crossing.	\$85,000	TBD After Study	Includes conceptual engineering for structures, drainage, utilities, traffic, landscape architecture and significant public involvement program.
8	102nd Ave N (Seminole to 137th St)	125th Street Intersection	Improve pedestrian facilities at the 125th Street Intersection. These facilities currently exist. Need repainting and restriping.		\$2,100	Three crosswalks totaling 132 ft. @\$12.54/ft and 25% contingency.

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<sup>2</sup> For the purposes of this study, detailed corridor studies are estimated at an average of \$100,000 per mile due to needed public involvement, engineering, and planning activities.

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**Table 5-1 CMP Implementation Plan Proposed Recommendations**

Corridor Ranking	Corridor	Location	Proposed Recommendation	Study Cost	Construction Cost	Assumptions for Cost Estimate
8	102nd Ave N (Seminole to 137th St)	Walsingham County Park entrance (east of 103rd Street) to 113th Street	15-ft.-wide landscaped multi-use trail connecting the path system of Walsingham County Park east of the park entrance to the Pinellas Trail crossing at Ashley Drive and eastward to 113th Street. Add 5-ft.-wide shoulders marked as bike lanes from 113th Street to Seminole Boulevard. Extend bike lanes to the east in order to connect to the north end of Lake Seminole Park.		\$1,742,000	15 ft. multiuse trail x 1.36 miles = \$\$540,000. Add 5-ft shoulders and stripe as bike lanes = \$585,000. Extend bike lane from Seminole Blvd to the existing 8-ft wide shoulder east of Seminole Blvd.= \$1100, Total cost = \$1,240,000, plus 15 % design, 15% CEI, and 25% contingency.
9	Indian Rocks Rd (Walsingham Rd to West Bay Dr)	Largo Hospital	Add pedestrian crossing between bus stop on west side of road and hospital. Improvements of bus stop location in R/W to allow passengers to alight the bus and safely get to the crosswalk. This must be studied with local government and impacts to traffic need to be identified.	\$20,000	TBD After Study	Coordination and traffic impact study.
9	Indian Rocks Rd (Walsingham Rd to West Bay Dr)	West Bay Intersection	Complete Intersection Study at West Bay and prioritize improvements. This intersection needs a study of turning movements and queue lengths to determine viability of additional turn lanes. Need to consider Mast Arms as this is a major evacuation route that has issues with wires during storms.	\$40,000	\$500K - \$1M (if lanes added)	Counts, modeling queue evaluation HCS, coordination with various jurisdictions.
9	Indian Rocks Rd (Walsingham Rd to West Bay Dr)	Entire Corridor	Detailed Corridor Study needed to determine specific causes of congestion. Consider upgrading to urban typical section.	\$280,000	TBD After Study	Public involvement, coordination, traffic counts, conceptual engineering, access management, detailed crash analysis, Queue Analysis Tech Memo, transit study.
9	Indian Rocks Rd (Walsingham Rd to West Bay Dr)	Largo Hospital	Add concrete pad at bus stop across the street from hospital and sidewalk to driveway.		\$6,000	PSTA provided cost.

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<sup>3</sup> There are no R/W costs included in these estimates. All construction costs include 15% for design, 15% for CEI and 25% contingency.



**Table 5-1 CMP Implementation Plan Proposed Recommendations**

Corridor Ranking	Corridor	Location	Proposed Recommendation	Study Cost	Construction Cost	Assumptions for Cost Estimate
9	Indian Rocks Rd (Walsingham Rd to West Bay Dr)	Walsingham Road Intersection	SB right turn and EB right turn are too tight. The SW corner contains a signal strain pole and signal box. There is an open drainage swale. Truck use is mainly to service the shopping center at the SW quadrant. Moving the NB left turn stop bar back should fix the issue. Based on TBRGM Study. Move NB left turn Stop bar back approximately 20 ft. Not included in quick fix because it is on a county road.		\$200 - \$300	Paint to move stop bar back.
9	Indian Rocks Rd (Walsingham Rd to West Bay Dr)	Walsingham Road Intersection	Move SB left turn stop bar back. Modify NE corner. Some issues with drainage ditch. Identified in TBRGM Study. In FDOT Quick fix program.		Cost to be Determined by FDOT	
10	Alt 19 (Curlew Rd to Pasco County Line)	Meres Boulevard	Add northbound right turn lane at Meres Boulevard beginning south of the Sweet bay shopping center. The shopping center has two access points on the approach to Meres Boulevard. An extended right turn lane will allow turning vehicles to clear the through lanes. The right turn lane could also act as a bus pull out for the sheltered bus stop located between the driveways.	\$10,000	\$380,000	Cost assumes relocation of the sidewalk and no ROW needed. Cost for 640 linear ft @ \$383/ft=\$245,120. plus 15% design, 15% CEI, and 25% contingency.
10	Alt 19 (Curlew Rd to Pasco County Line)	Helen Ellis Hospital	At Helen Ellis Hospital add pedestrian crossing to access the hospital from the bus stop on the west side of road. Add crosswalk and pedestrian facilities on existing mast arm.	\$25,000	\$80,000	Traffic counts, modeled impacts, Coordination with stakeholders and traffic impact analysis are included in the study estimate.
10	Alt 19 (Curlew Rd to Pasco County Line)	Entire Corridor	Comprehensive Transit Study	\$75,000	TBD After Study	Bus stop and ridership analyses, transfer study, potential for relocation of stops or consolidation, potential bike/ped mid-block crossings and impacts to traffic. R/W requirements if Bus Pull outs are required.
10	Alt 19 (Curlew Rd to Pasco County Line)	Helen Ellis Hospital	At Helen Ellis Hospital add pedestrian access to sidewalk on eastside of road.		\$750	Adds 50 ft. of sidewalk connecting the bus pad to two driveways.

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<sup>3</sup> There are no R/W costs included in these estimates. All construction costs include 15% for design, 15% for CEI and 25% contingency.

**Table 5-1 CMP Implementation Plan Proposed Recommendations**

Corridor Ranking	Corridor	Location	Proposed Recommendation	Study Cost	Construction Cost	Assumptions for Cost Estimate
10	Alt 19 (Curlew Rd to Pasco County Line)	Entire Corridor	Complete sidewalks on both sides. Remove gaps on entire corridor.		\$386,000	There are 48,785 ft. of gaps on the east side and 49,950 ft. of gaps on the west side. Total of 98,735 linear feet.
10	Alt 19 (Curlew Rd to Pasco County Line)	Dodecanese	Add southbound right turn lane at Dodecanese Boulevard. Also, need from previous CMP.		\$53,600 plus R/W acquisition and potential business damages	
10	Alt 19 (Curlew Rd to Pasco County Line)	Curlew Place	Add southbound left turn lane at Curlew Place. Also, need from previous CMP.		\$70,500 - \$88,000	Assumes a 160 ft. to 200 ft. turn lane.
10	Alt 19 (Curlew Rd to Pasco County Line)	Tampa Road	Ensure that the turn radii issues at the intersection of Alt. US 19 with CR 752/Tampa Road are addressed in the scope of services for the 2011 Work Program resurfacing project 4037251. Identified in TBRGM Study as a Freight Quick Fix project funded by FDOT. Request verification that the issue was remedied in the resurfacing project.		Cost to be Determined by FDOT	
10	Alt 19 (Curlew Rd to Pasco County Line)	Delaware Avenue	Add four Rapid Rectangular Flashing Beacons.		\$12,000	
11	Alt 19 (Bayshore Blvd) (Skinner Blvd to Curlew Rd)	Curlew Road Intersection	Evaluate signage at Curlew and remove sign clutter.	\$8,000	TBD After Study	Road Safety Audit (RSA), Tech Memo.

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<sup>2</sup> For the purposes of this study, detailed corridor studies are estimated at an average of \$100,000 per mile due to needed public involvement, engineering, and planning activities.

<sup>3</sup> There are no R/W costs included in these estimates. All construction costs include 15% for design, 15% for CEI and 25% contingency.

**Table 5-1 CMP Implementation Plan Proposed Recommendations**

<b>Corridor Ranking</b>	<b>Corridor</b>	<b>Location</b>	<b>Proposed Recommendation</b>	<b>Study Cost</b>	<b>Construction Cost</b>	<b>Assumptions for Cost Estimate</b>
11	Alt 19 (Bayshore Blvd) (Skinner Blvd to Curlew Rd)	Entire Corridor	Comprehensive Transit Study.	\$75,000	TBD After Study	Bus stop and ridership analyses, transfer study, potential for relocation of stops or consolidation, potential bike/ped mid-block crossings and impacts to traffic. R/W requirements if Bus Pull outs are required.
11	Alt 19 (Bayshore Blvd) (Skinner Blvd to Curlew Rd)	Entire Corridor	Need detailed corridor study to determine specific causes of congestion on this corridor. Evaluate each intersection on corridor to consider mast arms, improve signage and pavement markings. Study opportunities to provide exclusive turn lanes along entire corridor.	\$246,000	TBD After Study	Public involvement, coordination, traffic counts, conceptual engineering, access management, detailed crash analysis, Queue Analysis Tech Memo, transit study.
11	Alt 19 (Bayshore Blvd) (Skinner Blvd to Curlew Rd)	Curlew Road Intersection	Complete FDOT Freight Quick Fix project from TBRGM Study at Curlew Intersection. This project has been pre-engineered but not yet programmed for construction. Consider impacts to pedestrian and bike movements before implementation and mitigate. Pedestrian and bike safety as well as signage confusion has been identified and needs to be studied further.		Cost to be Determined by FDOT	
11	Alt 19 (Bayshore Blvd) (Skinner Blvd to Curlew Rd)	Michigan Ave Intersection	Add No Right on Red blank out signs at Michigan Avenue for bike/ped crossings.		TBD After Study	
11	Alt 19 (Bayshore Blvd) (Skinner Blvd to Curlew Rd)	Curlew Road Intersection	Complete project of No right on red blank out signs at Curlew by adding in eastbound direction.		Programmed	
12	62nd Ave N (49th St N to 66th St N)	62nd Street Intersection	Intersection study at 62nd Street N. to identify need for additional turn lanes. This intersection needs a study of turning movements and queue lengths to determine viability of additional turn lanes.	\$15,000	TBD After Study	Counts, modeling queue evaluation HCS.

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<sup>3</sup> There are no R/W costs included in these estimates. All construction costs include 15% for design, 15% for CEI and 25% contingency.

**Table 5-1 CMP Implementation Plan Proposed Recommendations**

Corridor Ranking	Corridor	Location	Proposed Recommendation	Study Cost	Construction Cost	Assumptions for Cost Estimate
12	62nd Ave N (49th St N to 66th St N)	Entire Corridor	This roadway is not currently equipped for trucks. Consider removing through trucks from roadway and only allow those trucks making deliveries on corridor to traverse this segment of 62nd Ave. N. This is a county designated truck route not a regionally designated truck route. Need to study impacts to truck movements and determine if this is feasible.	\$10,000	TBD After Study	
12	62nd Ave N (49th St N to 66th St N)	Entire Corridor	Upgrade roadway to 2L Divided as documented in 2035 LRTP. It is recommended to design this segment as four lanes to address drainage and utility concerns and not to prohibit future widening if needed. See Figure 6.		In 2035 LRTP for 2D (Divided) in 2016-2020 for \$17.55 million.	
13	Sunset Point Rd (Alt 19 to Keene Rd)	Entire Corridor	Upgrade roadway to urban standards. Based on previous 2009 recommendation to consider a less extensive solution. Proposed typical sections would allow for drainage and roadway conditions to be addressed with minimal negative impact. It will also be important to consider existing trees when determining where sidewalks will be built to minimize impacts. Consider adding right turn lane eastbound to southbound at Highland Avenue.		\$8.468,000	N Washington to Keene \$4,446,000; Alt US 19 to N Washington: \$842,000; Right turn lane at N Highland: \$72,800. combined Total= \$5,461,000 plus 15% design, 15% CEI, and 25% contingency.
13	Sunset Point Rd (Alt 19 to Keene Rd)	Entire Corridor	Improve sidewalk continuity. County has a project with Safe Routes to Schools. Need exact amount of linear feet of sidewalk that will still be missing after county project is complete. Due to county project, this recommendation is to be estimated after the safe routes to schools detailed designs are complete and remaining sidewalk would be built at \$3.73 per square foot.	\$2,000	TBD After Study	
14	Nursery Rd (Highland Ave to US 19)	Belcher, Keene Rd, Highland Avenue intersections	Intersection studies at Belcher Road, Keene Road and Highland Avenue to determine feasibility of left and right turn lanes and pedestrian upgrades.	\$45,000	TBD After Study	Counts, modeling queue evaluation HCS (Assume \$15,000 per study).

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**Table 5-1 CMP Implementation Plan Proposed Recommendations**

<b>Corridor Ranking</b>	<b>Corridor</b>	<b>Location</b>	<b>Proposed Recommendation</b>	<b>Study Cost</b>	<b>Construction Cost</b>	<b>Assumptions for Cost Estimate</b>
14	Nursery Rd (Highland Ave to US 19)	Entire Corridor	Upgrade roadway to urban standards. To address the enhanced designation in the LRTP, recommend an urban two-lane typical section.		\$11,308,000	Includes urban typical w/ 11 ft. lanes and 4-foot bike lanes and filling in sidewalk gaps. 2.72 miles @\$2,636,000=\$7,170,000; Completing 8,400 ft. of 4-ft.-wide sidewalk @\$3.73/sq. ft.=\$125,400. Combined total=\$7,295,000 plus 15% design, 15% CEI, and 25% contingency.

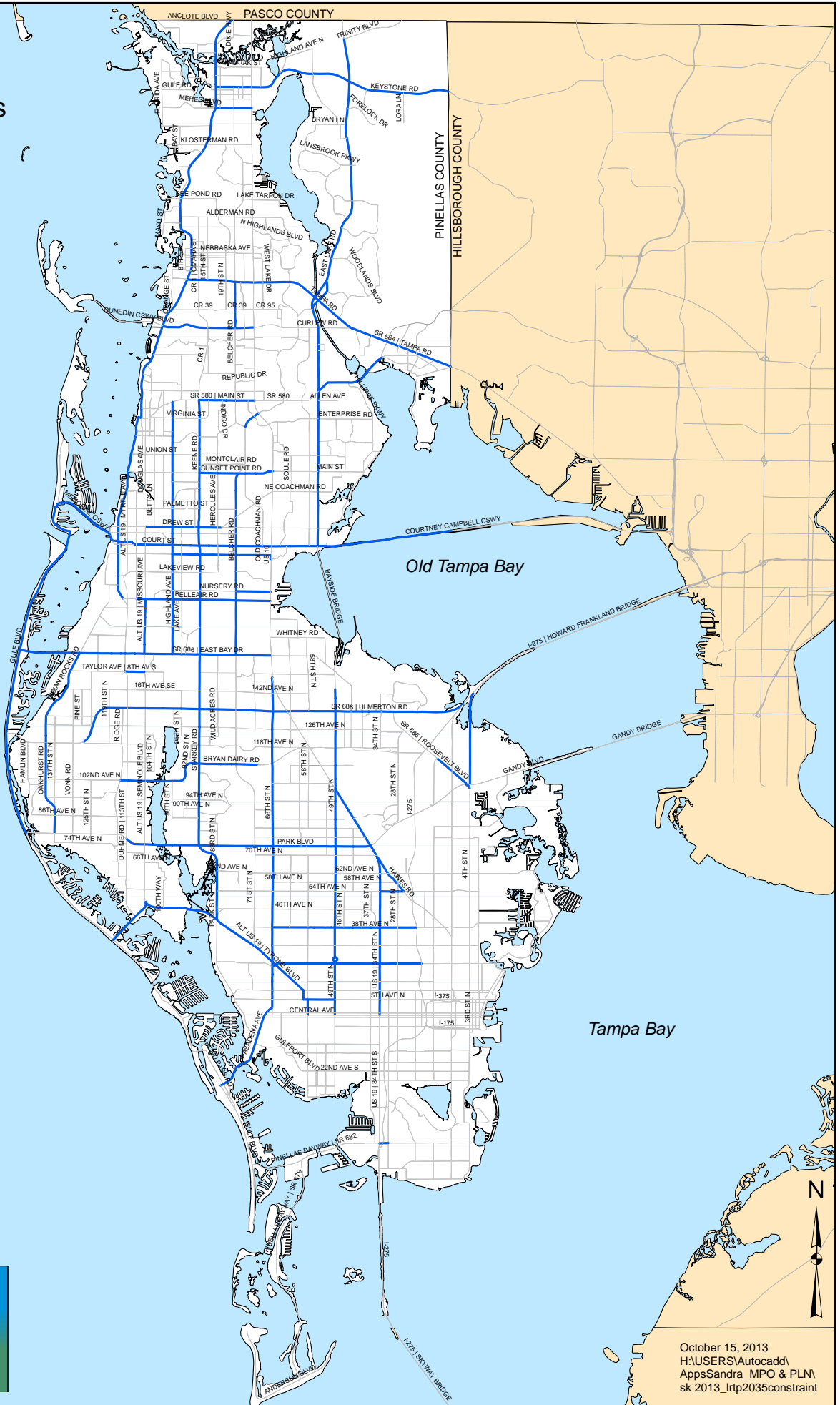
Notes: <sup>1</sup> Cost Estimates are preliminary for planning purposes only and not intended for design, right-of-way and construction purposes.

<sup>2</sup> For the purposes of this study, detailed corridor studies are estimated at an average of \$100,000 per mile due to needed public involvement, engineering, and planning activities.

<sup>3</sup> There are no R/W costs included in these estimates. All construction costs include 15% for design, 15% for CEI and 25% contingency.

# Proposed Constrained Roads

Gulf of Mexico



**Proposed Constrained Roads**

On Street	From	To	Type of Constraint	Existing Land Use	Notes	
102nd Ave.	Ridge Rd.	113th St.	N	SF, PSP		extend
22nd Ave. N.	66th St.	I-275	P, N, R	SF		
38th Ave. N.	66th St.	I-275	P, N, R	SF	County (49th St. to I-275)	
49th St.	54th Ave. N.	144th Ave. N.	P	COMM, IND, PSP		extend
49th St.	Central Ave.	38th Ave. N.	N	SF		extend
4th St.	Gandy Blvd.	I-275	P	MF		
54th Ave. N.	Haines Rd.	31st St.	P, R	MF, MH		
54th Ave. S.	U.S. 19	I-275	R	N/A		
58th St.	5th Ave. N.	Central Ave.	N, R	SF		
5th Ave. N.	Tyrone Blvd.	49th St.	N	SF		
					US 19 to 142nd Constrained by Largo, Ulmerton to 142nd Backlogged by Largo	
66th St.	142nd Ave. N.	54th Ave. N.	P	COMM		extend
66th St.	38th Ave. N.	Pasadena Ave.	P	COMM, COMM OFF		extend
Alt. U.S. 19	Anclote Rd.	Myrtle Ave.	P	COMM, SF	Dunedin, Tarpon Springs	
Alt. U.S. 19	Chestnut St.	Missouri Ave.	P	COMM		
Alt. U.S. 19/Seminole/Miss	East Bay Dr.	Belleair Rd.	P	COMM		
Alt. U.S. 19/Bay Pines	100th Way.	W. End of Bridge	P	PSP		
Alt. U.S. 19/Tyrone	38th Ave. N.	5th Ave. N.	P, N	COMM		
Belcher Rd.	Sunset Point	Gulf to Bay Blvd	P, R	COMM, COMM OFF		
Belcher Rd.	Gulf to Bay Blvd.	East Bay Dr.	P	SF		
Belcher Rd.	Tampa Rd.	Curlew Rd.	P	SF		
Belleair Beach Cswy	Gulf Blvd.	Indian Rocks Rd.	P	COMM, SF		
Belleair Rd.	MLK Jr. Ave.	U.S. 19	P	SF	Clearwater, County, Largo	
Bryan Dairy Rd.	Starkey Rd.	66th St. West Ramp	P	IND, COMM		
Bryan Dairy Rd.	Alt. U.S. 19	98th St. N.	P	ROP/PRES, COMM	County	
Corey Causeway	Gulf Blvd.	Shore Dr. S.	P	COMM, MF		
Coronado Dr.	Gulfview Blvd.	Roundabout	P	COMM		
Countryside Blvd.	Belcher Rd.	U.S. 19	P	COMM OFF,		
Court St.	Missouri Ave.	Highland Ave.	P, R	COMM, COMM OFF		
Courtney Campbell Cswy	Bayshore Blvd.	Hillsborough County	P	ROS		
Curlew Rd.	Alt. U.S. 19	U.S. 19	N	SF		
Drew St.	Highland Ave.	N.E. Coachman Rd.	P	COMM		
Druid Rd.	Highland Ave.	U.S. 19	N	SF, MH		
East Lake Rd.	Trinity Blvd.	North Split	P	SF, MF	County	
Forest Lakes Blvd.	Tampa Rd.	Hillsborough County	P	COMM, SF	County (Pine to Hillsborough), Oldsmar (not constrained)	remove
Ft. Harrison Ave.	Drew St.	Pinellas St.	P	COMM, COMM OFF, PSP		
Gulf Blvd.	Gulfview Blvd.	Park Blvd.	P	MF, SF		
Gulf to Bay Blvd./SR 60	Roundabout	Bayshore Blvd.	P	COMM		extend
Gulfview Blvd.	Coronado Dr.	Clearwater Pass	P	MARINA		
Haines Rd.	U.S. 19	28th St.	P	COMM	County	
Highland Ave.	Union St.	Gulf to Bay Blvd	N	SF		extend
Highland Ave.	Druid Rd.	East Bay Dr.	N	SF, PSP		
Indian Rocks Rd.	West Bay Dr.	Walsingham Rd.	P	SF, COMM	County	
Keene Rd.	SR 580	Drew St.	P	SF, MF		
Keene Rd./Starkey Rd.*#	Belleair Rd.	Park Blvd.	P, N	IND, SF		extend
Keystone Rd.	U.S. 19	Hillsborough County	P	SF	County (East Lake to Hillsborough)	
McMullen Booth Rd.	East Lake Rd.	Gulf to Bay Blvd	P	SF, PSP	County	extend
Meres Blvd.	Alt. U.S. 19	U.S. 19		ROS, COMM, MH		
Nursery Rd.	Belcher Rd.	U.S. 19	P, N	SF, MF	County	
Oakhurst Rd.	Walsingham Rd.	Park Blvd.	P, N	SF		
Park Blvd.	113th St.	U.S. 19	P, R	COMM		
Park St.*#	Park Blvd.	46th Ave. N.		MF, COMM		
Patricia Ave.	Main St.	Union St.	N	COMM		
SR 580	CR 1	U.S. 19	P	COMM		
SR 580	McMullen Booth Rd.	Forest Lakes Blvd.	P	PSP, SF	Oldsmar	
SR 666/Mad Beach Cswy	Gulf Blvd.	Duhme Rd.	P	COMM		
SR 686/East Bay	Seminole Blvd.	U.S. 19	P	COMM, MF		
SR 686/Roosevelt*	16th St.	4th St.	P	COMM, COMM OFF		
SR 688/Ulmerton*	Walsingham Rd.	I-275	P	COMM, IND	MPO Action, Backlogged by Largo	
Sunset Point Rd.	Keene Rd.	U.S. 19	P, N	COMM, COMM OFF		
Tampa Rd./SR 584	Alt. U.S. 19	Hillsborough County	P	COMM, MF	County, Oldsmar	extend
Tarpon Ave.	Alt. U.S. 19	U.S. 19	P	COMM	Tarpon Springs	
Taylor Ave.	Clearwater Largo Rd.	Alt. U.S. 19	N	SW/ MH		
U.S. 19	Central Ave.	Gandy Blvd.	P, R	COMM		
U.S. 19	Gandy Blvd.	49th St.	P	COMM		add
West Bay Dr.	Indian Rocks Rd.	Missouri Ave.	P	COMM, COMM OFF	Backlogged, by Largo	

P = Policy Constraint; N = Neighborhood Constraint; R = Right-of-Way Constraint

Note: List does not include road segments projected to fail in 2035 that have additional, unfunded projects planned for them. SIS facilities have also been removed.

\*Projected to fail after planned projects completed. No additional projects planned beyond the Cost Feasible Plan.

#Corridor currently under reevaluation for mitigating projects.

CONGESTION MANAGEMENT PROCESS (CMP) POLICIES AND PROCEDURES MANUAL

The purpose of Congestion Management Process (CMP) Policies and Procedures Manual is to provide an overview of the federal and state requirements pertaining to the CMP, describe the roles of all of the MPO's transportation partners and the advisory committees, and address the federally recommended eight-step congestion management process: develop CMP objectives; define the CMP network; develop multi-modal performance measures; collect data and measure system performance; analyze congestion problems and needs; identify and assess strategies; implement strategies; and evaluate the effectiveness of the strategies. Perhaps more importantly, the CMP Manual documents the process and procedures the Pinellas County MPO has used over the past many years regarding congestion management, and will serve as a guideline for present and future CMP project selection and plan development.

In addition to presenting the CMP Policies and Procedures Manual, MPO staff will discuss a possible set-aside of funds that will enable both a commitment to congestion management improvements *and* the flexibility to implement such improvements when and where appropriate.

TCC members are asked to review and provide comment on the draft CMP Manual.

ATTACHMENTS: [October 2013 Draft CMP Policies and Procedures Manual](#)

ACTION: TCC review and approval



# Congestion Management Process (CMP)

## Policies and Procedures Manual

(October 2013 – *DRAFT*)



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# Congestion Management Process (CMP) Policies and Procedures Manual

The Congestion Management Process (CMP) Policies and Procedures Manual provides an overview of the federal and state requirements pertaining to the CMP and describes how the Pinellas County MPO will address such requirements through an approach that uses performance measures and coordinates with the policies, plans and processes of State and local governments. This manual serves as a guideline for future CMP project selection and plan development.

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*In Accordance with Title VI of the Civil Rights Act of 1964 and other nondiscrimination laws, public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status.*

*Limited English Proficiency (LEP) – Executive Order of Title VI: Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact the Pinellas County MPO.*

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## ***EXECUTIVE SUMMARY***

*Congestion management is the application of strategies to improve transportation system performance and reliability by reducing the adverse impacts of congestion on the movement of people and goods. A congestion management process (CMP) is a systematic and locally-accepted approach for managing congestion that provides accurate, up-to-date information on transportation system performance and assesses alternative strategies for congestion management that meet state and local needs. The CMP is intended to move these congestion management strategies into the funding and implementation stages.*

*The CMP, as defined in federal regulation, is intended to serve as a systematic process that provides for safe and effective integrated management and operation of the multi-modal transportation system. The process includes:*

- *Development of congestion management objectives*
- *Establishment of measures of multi-modal transportation system performance*
- *Collection of data and system performance monitoring to define the extent and duration of congestion and determine the causes of congestion*
- *Identification of congestion management strategies*
- *Implementation activities, including identification of an implementation schedule and possible funding sources for each strategy*
- *Evaluation of the effectiveness of implemented strategies*
- *Collaboration with transportation partners and public involvement*

*A CMP is required in metropolitan areas with population exceeding 200,000, known as Transportation Management Areas (TMAs). Pinellas County is part of a TMA that includes Hillsborough and Pasco counties. Federal requirements also state that in all TMAs, the CMP shall be developed and implemented as an integrated part of the metropolitan transportation planning process.*

*The purpose of this document is to identify and describe the process used by the Pinellas County MPO to respond to the federal and state CMP requirements. This document is not intended to serve as a congestion management “plan,” but rather a “process” that provides for the safe and effective integrated management and operation of the multi-modal transportation system – countywide. The CMP is intended to use an objectives-driven, performance-based approach to planning for congestion management.*

*The CMP is an on-going process, continuously progressing and adjusting over time as goals and objectives change, new congestion issues arise, new information sources become available, and new strategies are identified and evaluated. The Pinellas County MPO collects transportation system performance field data and archives crash data gathered from law enforcement reports. These data sets are combined with Federal, State and local data to create the MPO’s biennial State of the System Report, which establishes the foundation of the CMP. Project selection and monitoring of the implementation of specific CMP projects is performed by MPO staff with input from federal and state agencies, the county and municipal governments, the MPO’s advisory committees and through public involvement activities.*

*Since 1997, the Pinellas County MPO has implemented a CMP (originally called a “Congestion Management System”). The MPO’s CMP is not a stand-alone process, rather it integrates and is integrated with other plans and studies, including the MPO’s Long Range Transportation Plan (LRTP) and Transportation Improvement Program (TIP), and the Florida Department of Transportation’s Strategic Highway Safety Plan. Additionally, this CMP is integrated with regional plans and programs, including the Regional CMP developed by the West Central Florida MPOs Chairs Coordinating Committee (CCC).*

The U.S. Department of Transportation/Federal Highway Administration studies show that congestion has grown substantially over the past 20 years in cities of every size, particularly in heavily populated areas.

In the context of transportation, the term “congestion” implies stopped or stop-and-go traffic, slow travel speeds and prolonged travel times. Secondary effects may include motorist frustration, elevated crash frequencies, aggressive driving, delays in providing transit and emergency services, reductions in air quality due to an increase in vehicle emissions, and a diminished potential for economic growth in industries directly or indirectly dependent on the movement of people and goods.

The Texas A&M Transportation Institute’s *2012 Urban Mobility Report* states that the annual delay for an auto commuter in the Tampa-St. Petersburg urban area was 38 hours in 2011, which earned a ranking of 30<sup>th</sup> highest in the nation; the average annual congestion cost per auto commuter was \$791, which earned a ranking of 37<sup>th</sup>; and finally, the total peak period travel time was 43 minutes, which earned a ranking of 30<sup>th</sup> highest in the nation. The average annual congestion cost in the Tampa-St. Petersburg urban area associated with truck delay was estimated to be \$246 million, which earned a ranking of 21<sup>st</sup> highest in the nation.

Traditionally, roadway expansion has been considered a primary remedy for congestion. However, in recent years, with rising costs of available land and construction, it has become increasingly apparent that communities can no longer “build their way” out of congestion. In response to these issues, federal requirements were introduced by the *Intermodal Surface Transportation Efficiency Act (ISTEA)* of 1991, and continued under the *Transportation Equity Act for the 21<sup>st</sup> Century (TEA-21)*, which was advanced through the 2005 *Safe Accountable,*

*Congestion can be defined as an excess of vehicles on a portion of roadway at a particular time resulting in speeds that are slower. sometimes much slower – than normal.*

*Flexible, and Efficient Transportation Act: A Legacy for Users* (SAFETEA-LU). As these legislative acts were implemented over the years, the requirements that all MPOs have a “Congestion Management System” (CMS) also evolved, becoming the “Congestion Management Process (CMP).” This change represented a revision in perspective and practice by requiring close investigation into lower cost, less aggressive strategies as primary approaches for alleviating traffic congestion, with road building or expansion reserved as a secondary strategy to be used in situations where more conservative alternatives were determined to be inadequate, inappropriate or impractical. The federal *Moving Ahead for Progress in the 21<sup>st</sup> Century Act* (MAP-21), which became effective in October 2012, makes essentially no change in the requirements for the CMP.

## **The Congestion Management Process (CMP)**

The Federal Highway Administration (FHWA) identifies the Congestion Management Process (CMP) as a systematic, multi-modal, collaboratively developed and coordinated program that provides for the safe and effective management and operation of new and existing transportation facilities.

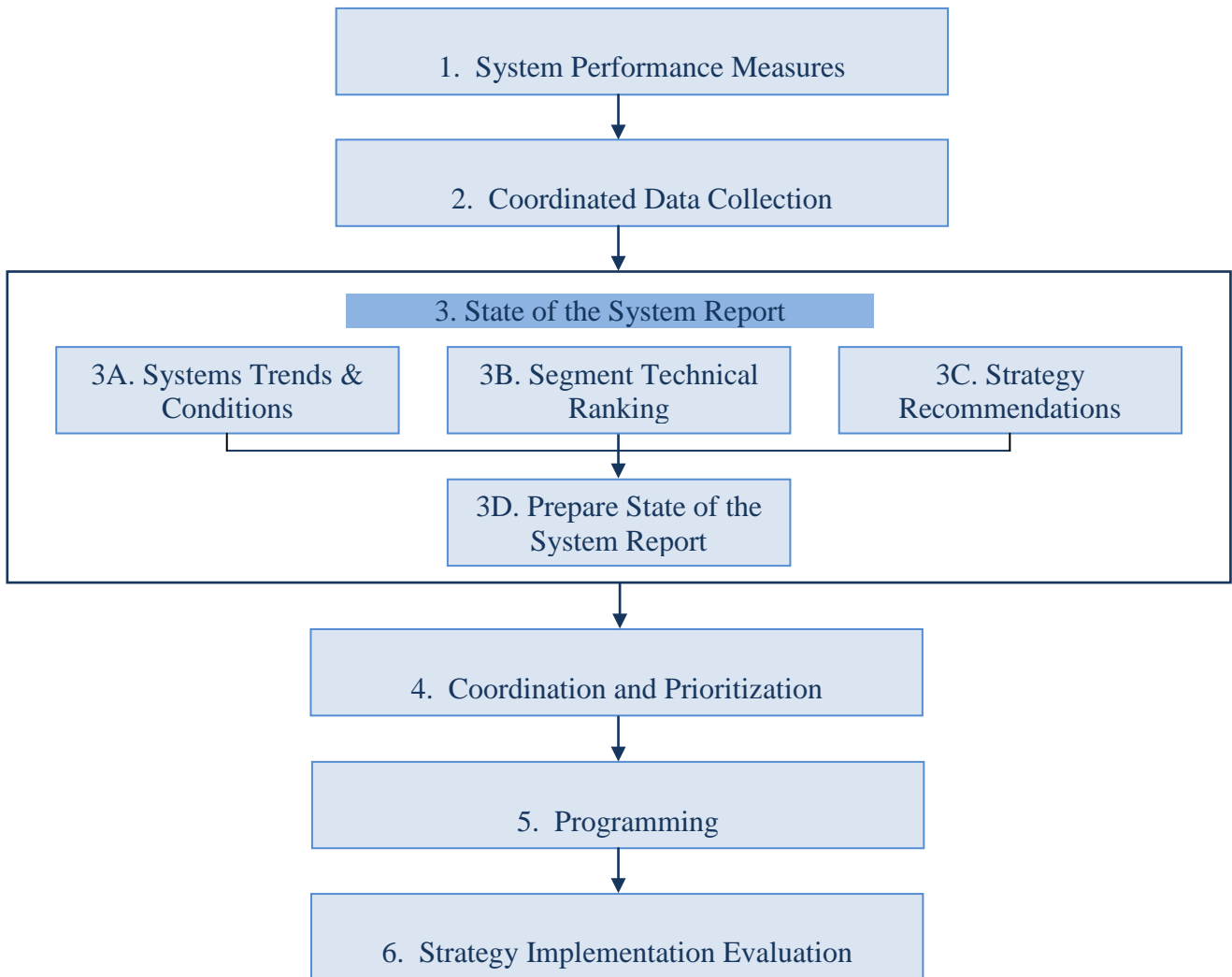
Rather than addressing congestion through a process aimed at identifying needs for road building or expansion – solutions that can be costly and environmentally invasive - the CMP approach emphasizes the need for a broader range of lower cost strategies aimed at enhancing systemwide mobility and regional connectivity (Figure 1). The components of this process include:

- Identification of objectives
- Data collection and system performance monitoring
- Measures of performance
- Identification of strategies (short, medium and long range)



- Implementation activities, including funding and scheduling of strategies
- Evaluation of strategy effectiveness
- Collaboration with agencies and public involvement

**Figure 1 – The Congestion Management Process**



## Congestion Defined by Cause and Occurrence

The root causes of congestion have been identified through studies conducted by the U.S. Department of Transportation (DOT), shown in Figure 2. The causes of congestion include the following:

**Bottlenecks** – Traffic experiences reduced speeds and delays on localized sections of highway where there is narrow or obstructed physical capacity. Examples may include narrowing or converging of lanes and shoulders, interchange merging, grade changes, or severe curves. Some of these are classified as “operational influenced deficiencies,” such as on- and off-ramps, merge areas, weave areas, lane drops, tollbooth areas, and traffic signals; or design constraints, such as curves, climbs, underpasses, or narrow or non-existent shoulders. As shown in Figure 2, the most frequent cause of congestion, nationwide, is bottlenecks (40%), followed by traffic incidents (25%).

**Traffic Incidents** – Includes road incidents such as crashes or obstructions, breakdowns or debris on the road.

**Work Zones** – There is temporary traffic stoppage or slow down due to construction.

**Weather Conditions** – In Florida, this category typically involves heavy rain or fog.

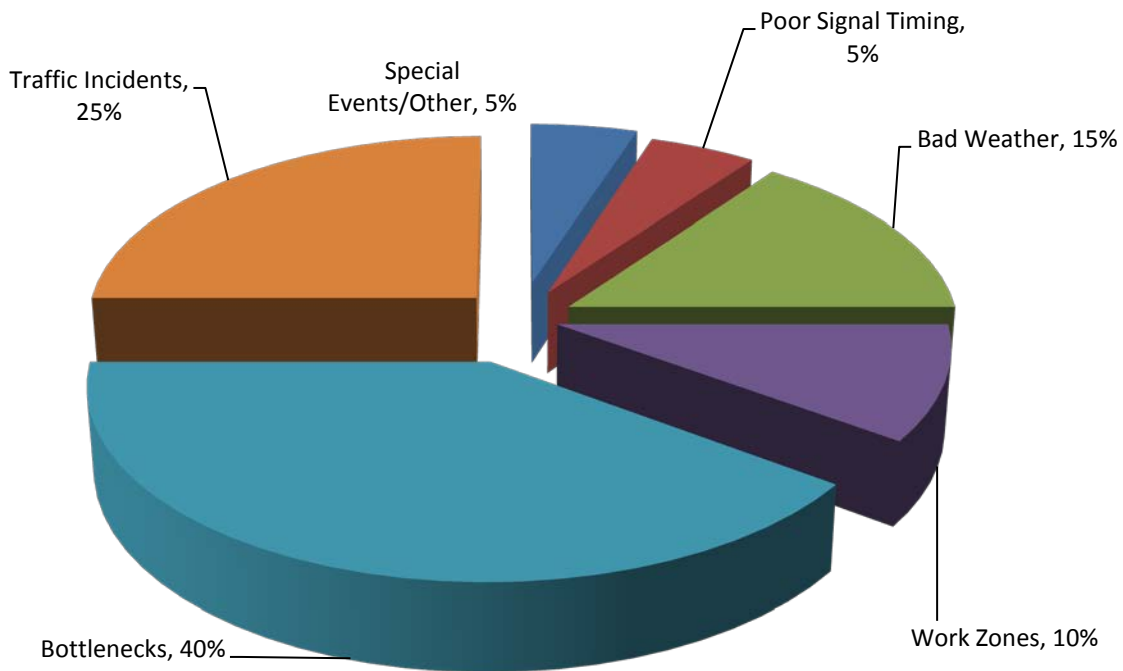
**Special Events** – Congestion may result from temporary “spikes” in volume due to arts & culture and sports events, or seasonal peaks due to winter tourism or holiday shopping.

**Fluctuations in Normal Traffic** – Day-to-day variability results in some days with higher volumes than others for a variety of reasons, which may result in unreliable travel times.

The studies acknowledge that local conditions can vary widely. For example, when compared to northern locations, Pinellas County may be less likely to be affected by extreme weather conditions of longer duration, such as snow or ice. However, it may have a higher percentage of “Special Events/Other” than colder locations, due to seasonal tourism.

It was noted that these root causes can combine to further complicate the identification of a primary cause. For example, a bottleneck can lead to a crash, and the resulting congestion may continue long after the crash has been cleared. Identifying the initial, root causes of congestion on a specific roadway may provide valuable insight into preparing relevant objectives and corrective strategies.

**Figure 2: Causes of Congestion, Summary of a Nationwide Study**



Source: US Department of Transportation (DOT)/Federal Highway Administration (FHWA)

### **Recurring and Nonrecurring Congestion**

FHWA subdivides the root causes of congestion into two categories: “recurring” and “nonrecurring.” Recurring congestion implies that the volume of vehicles consistently exceeds the capacity of the road. Two causes of recurring congestion are bottlenecks, the single most frequent cause of congestion (40%), and poor signal timing (5%). With recurring congestion, it is likely that some self-correction may occur as travelers come to anticipate delays and adjust their travel times and routes accordingly. For this reason nonrecurring congestion is considered more complicated to mitigate.

It is estimated that over half of all congestion is nonrecurring. Traffic incidents, including crashes and breakdowns, are the most frequent cause of nonrecurring congestion, accounting for 25% of all road congestion, followed by weather (15%), construction (10%) and special events/other (5%).

### **Travel Time Reliability**

Congestion is never the same every day on a specific roadway. Recent empirical studies suggest that travelers are interested not only in travel time savings, but also in reduction in travel time variability. The term “travel time reliability” applies to travelers’ ability to predict their travel times by including a buffer, an allowance for unanticipated delays. This capability is particularly important to commuters and freight shippers. Nonrecurring congestion is a threat to travel time reliability.

### **Benefit/Cost Analysis**

Due to budgetary constraints and an increasingly competitive fiscal environment, state, regional and local transportation planning organizations around the country are being asked more than ever to justify their programs and expenditures. Transportation System Management and Operations (TSM&O) programs have not escaped this scrutiny and system operators are

routinely asked to rank their projects against traditional capacity expansion projects, as well as conduct other value-related exercises. Use of traditional benefit/cost analysis frameworks for assessing operations projects can face numerous challenges, including: How can new and emerging performance measures (e.g., travel time reliability) be leveraged to provide a more complete picture of the benefits of operations strategies? How can the benefits of integrating various operations strategies be captured? What are the benefits of supporting backbone infrastructure (e.g., communications, traffic management centers)? How can the life-cycle costs of operations strategies be accounted for? How can the benefits of operations strategies targeted at non-typical or non-recurring conditions be estimated?

The Pinellas County MPO's CMP planning presented here will use the *Benefit/Cost Analysis for Operations Desk Reference*, produced by FHWA Office of Operation, to provide guidance on strategies to overcome these unique demands and better estimate benefits that fully capture the impacts of operations strategies.

The CMP is a federal requirement that encourages a single, integrated approach to managing congestion. Successful implementation requires the coordination and balancing of priorities at a variety of levels, i.e. among transportation modalities (roadways, transit, pedestrian and bicycle); between the MPO's CMP and its other programs and plans; and between State, regional and local governments and their implementing agencies. (See Appendix for Title 23, Section 450.320, CFR.)

As noted in the April 2011 *CMP Guidebook*, published by the U.S. Department of Transportation/Federal Highway Administration, regulations for the CMP are not intended to be prescriptive. Instead, MPOs are encouraged to define and address congestion by taking into consideration the needs and values of their respective communities. Mitigation of congestion may not always be possible or, in some cases, even desirable. Instead, the appropriate goal should be one of defining (and periodically redefining) “acceptable levels of congestion,” as appropriate for a location, and setting objectives that take into consideration such factors as mobility, livability, accessibility, multi-modal connectivity, economic vitality and community values.

A CMP is required in metropolitan areas with population exceeding 200,000, known as Transportation Management Areas (TMAs). Pinellas County is part of a TMA that includes

### **23 CFR 450.320**

*The transportation planning process in a TMA shall address congestion management through a process that provides for safe and effective, integrated management and operation of the multi-modal transportation system, based on a cooperatively developed and implemented metropolitan-wide strategy, of new and existing transportation facilities eligible for funding under title 23 U.S.C. and title 49 U.S.C. Chapter 53 through the use of travel demand reduction and operational management strategies.*

Hillsborough and Pasco counties. Federal requirements state that in all TMAs, the CMP shall be developed and implemented as an integrated part of the metropolitan transportation planning process, while Section 339.177, Florida Statutes, indicates that each MPO must develop and implement a traffic congestion management system. CMP requirements for TMAs can be summarized as “a coordinated program for monitoring and evaluating the performance of the multi-modal transportation system,” with the goals of:

- Identifying specific causes of congestion,
- Identifying appropriate remedial strategies, and
- Evaluating the effectiveness of strategies implemented.

Congestion management is one of the MPO's primary responsibilities. Partnerships with state and county agencies, municipal governments, transit and other regional agencies, as well as meaningful relationships with citizens, are essential ingredients for a successful transportation program that includes congestion management.

To this end, the MPO implements a Public Participation Plan (PPP) that is updated and evaluated regularly to remain current and relevant. The following objectives cited in the PPP also pertain to the Congestion Management Process:

- Raise the level of understanding of the transportation planning process in the region and identify how interested citizens can participate.
- Maximize opportunities for public participation in the transportation process.
- Maintain contact with interested citizens and key stakeholders throughout the process of developing MPO plans and projects.
- Be responsive to citizens.
- Involve traditionally underserved persons, including minority, low-income and elderly citizens or those addressed by the Americans with Disabilities Act (ADA) in the development and review of transportation plans and projects.
- Inform and educate incoming MPO Board and advisory committee members regarding the MPO's functions, responsibilities and programs.

The MPO partners with local, county, regional and state agencies and organizations to plan and implement numerous transportation initiatives, including those related to congestion

*MAP-21 calls for the MPO to provide citizens, affected public agencies, representatives of transportation agencies, private providers of transportation and other interested parties with a reasonable opportunity to comment on their transportation plans and programs.*

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*MAP-21 also requires MPOs to consult with affected local, regional, state and federal agencies in the course of planning and program development.*



management. Much of this task is accomplished through the MPO's standing advisory committees, in addition to other focused activities involving informal meetings, work groups and ad hoc committees.

### **Pinellas County MPO and its Advisory Committees**

The Pinellas County MPO is presently governed by an 11-member board of elected officials representing municipal governments, the Pinellas County Board of County Commissioners and the Pinellas Suncoast Transit Authority (PSTA). The Florida Department of Transportation (FDOT) District 7 Secretary, or a designee, serves the board as a non-voting technical advisor. All meetings of the MPO and its committees are open to the public. A reapportionment plan approved by the MPO in July 2013 will add two additional seats, subsequent to the passage of House Bill 869 (Chapter 2012-245, Laws of Florida) by the Florida legislature in March 2012. The purpose and intent of the legislation is to “unify” the MPO board and the Pinellas Planning Council (PPC) board so that a single policymaking body oversees both land use and transportation planning in Pinellas County.

Advisory committees provide input to the MPO to help identify and address transportation planning issues, including congestion. To achieve a broad representation of ideas and perspectives, members are drawn from three categories: citizens, professionals (includes technical, social service, law enforcement, municipal agencies, private industry, etc.), and elected officials. MPO committees include the Technical Coordinating Committee, Intelligent Transportation Systems Advisory Committee, Citizens Advisory Committee, Bicycle Advisory Committee, Pedestrian Transportation Advisory Committee, School Transportation Safety Committee, Local Coordinating Board and the Pinellas Trail Security Task Force.

While each of these contributes periodically to the CMP, as needed, primary oversight for CMP planning is assigned to the MPO's Technical Coordinating Committee (TCC) and the Intelligent Transportation Systems (ITS) Committee.

**Technical Coordinating Committee (TCC)** - The TCC meets monthly and assists the MPO by reviewing and making recommendations on transportation improvement programs and plans; by participating in the TIP prioritization process; and by ensuring that recommended CMP strategies are consistent with local plans and initiatives. Additionally, the TCC plays a key role in the CMP by participating with the ITS Committee in project selection, and by providing updates and reviewing tracking reports on the implementation of CMP projects. Members are mostly planners and engineers, and other professionals representing local governments, the Pinellas Suncoast Transit Authority (PSTA), the Pinellas Planning Council (PPC), the Tampa Bay Regional Planning Council (TBRPC), the Tampa Bay Area Regional Transportation Authority (TBARTA), the Florida Department of Transportation (FDOT), the St. Petersburg-Clearwater International Airport, and the Pinellas County School Board.

**Intelligent Transportation Systems (ITS) Advisory Committee** – The MPO has turned to technology as a way to deal with some of the traffic challenges that create congestion and burden the surface transportation network. In addition to providing prioritization and policy direction for general transportation system management and operations planning, the ITS Committee provides valuable assistance to the MPO by providing direction and guidance for improving efficiency and enhancing safety, planning and evaluating congestion strategies, and CMP project selection. Members include transportation planners, engineers, elected officials and representatives from a variety of transportation-related specialties including traffic management, emergency management, law enforcement, public transit and communications, as well as members involved in tourism and interested citizens. The committee assists in coordinating the planning and deployment of a broad-spectrum ITS program, the components of which include the management of the signal system, incident detection and deployment of emergency and law enforcement teams, coordination of traveler advisory functions, and pedestrian crosswalk and transit applications.

**Citizens Advisory Committee (CAC)** – Members represent a geographic cross-section of the community, including business and civic organizations, senior and minority populations, and the disabled. The CAC meets monthly to evaluate and recommend strategies and generally raise awareness concerning a wide variety of transportation related issues, including ITS projects and the CMP, and it should be noted that a CAC member sits on the ITS Advisory Committee. The CAC also provides input on TIP funding priorities and the development of the LRTP.

**Bicycle Advisory Committee (BAC) and Pedestrian Transportation Advisory Committee (PTAC)** – The BAC and PTAC support the CMP by promoting safety and accommodations for their respective transportation modalities. A primary responsibility for both committees is participating in the development and update of the MPO’s *Bicycle and Pedestrian Master Plan*. Both committees are made up of private citizens, public and business sector representatives, law enforcement officials and others who have experience or simply an interest in bicycle and pedestrian issues.

**School Transportation Safety Committee (STSC)** – The STSC Committee participates in developing transportation initiatives aimed at mitigating congestion in areas surrounding schools and promoting safety for all students, including bicyclists, pedestrians/walkers, school bus riders and vehicle passengers/motorists. The School Pool program currently available to parents at select schools in Pinellas and Hillsborough counties offers rideshare-matching services, which among other things, removes car trips from the surrounding roadway network, thus reducing congestion. STSC members represent the School Board, the Board of County Commissioners, local governments and citizens.

**Local Coordinating Board (LCB)** – The LCB serves as the policy and oversight board for the MPO’s Transportation Disadvantaged Program, which provides non-emergency wheelchair and ambulatory transportation as well as PSTA bus passes for individuals who are low income or physically or mentally impaired.

**Pinellas Trail Security Task Force (PTSTF)** – PTSTF members monitor and address safety issues/concerns specifically related to the Pinellas Trail. Members represent law enforcement agencies, emergency management, Pinellas County Animal Services, and local government planning and parks & recreation personnel. The purpose of the Task Force is to provide a safe environment for trail users, and to encourage biking & walking as a transportation alternative thereby reducing demand on the roadway system.

## **Collaboration with Local and County Partners**

In the performance of its daily responsibilities, the MPO regularly partners with local agencies, and the county and municipal governments in planning programs and strategies that are designed to mitigate congestion and to promote livable communities.

Partnerships are achieved through the participation of Pinellas County and municipal governments on the MPO’s Board and committees, which have been previously described. Additionally, MPO staff participates in numerous planning efforts and activities hosted by county entities and municipal governments and serves on many of their advisory committees, e.g., the PSTA hosted Advisory Committee for Pinellas Transportation (ACPT), MPO/PPC Joint Land Use/Transportation Working Group, and the St. Petersburg Bicycle and Pedestrian Advisory Committee.

The MPO collaborates with all of the local governments within Pinellas County in identifying and prioritizing TIP projects that address congestion, as well as concurrency policies, the countywide transportation impact fee ordinance, and the emerging multi-modal mobility plan. Such collaborations occur between the MPO and individual governments, and through groups such as the Barrier Islands Governmental Council (“BIG-C”) which represents 10 beach communities. The purpose and intent of the BIG-C is to stimulate communications between the

barrier island cities and towns in order to focus on issues and opportunities common to all, including tourism, traffic congestion, safety initiatives associated with pedestrian and bicycle facilities, and public transportation. Ideally, the BIG-C unites and acts as one voice when approaching and addressing various state and county agencies and organizations on matters of mutual concern.

**Pinellas Planning Council (PPC)** – Created by a Special Act of the Florida legislature in 1988, the PPC provides a forum for representatives of the county’s 24 municipalities, the unincorporated area and the Pinellas County School Board to address countywide land use issues. The PPC administers the Countywide Plan, Countywide Future Land Use Map and Countywide Rules to help ensure consistent planning and development across Pinellas County. A PPC staff member serves on the on MPO’s Technical Coordinating Committee (TCC), and as described earlier, subsequent to the passage of House Bill 869 (Chapter 2012-245, Laws of Florida) by the Florida legislature in March 2012, the MPO board and the PPC board are to be “united” so that a single policymaking body oversees both land use and transportation planning in Pinellas County.

The updated Countywide Plan, Rules and Map will encourage compact and mixed-use development, and interconnected streets to accommodate safe and convenient walking, bicycling and public transit use. Higher density transit oriented development will be permitted around the proposed light rail and bus transit hubs, as well as within activity centers and mobility corridors, thereby reducing the number of roadway trips and increasing the likelihood that more residents and visitors will choose transit.

**Pinellas Suncoast Transit Authority (PSTA)** – PSTA presently provides bus service on 42 routes, including two express routes, to 21 of 24 municipalities and the unincorporated area of Pinellas County. A PSTA Board representative serves on the MPO, and there is often considerable overlap between the MPO and PSTA Boards. In addition to bus service, PSTA provides special services to low income persons and to those who qualify as disabled through the Americans with Disabilities Act (ADA). PSTA is advised by its Transit Advisory Committee

(TAC), with membership that includes mostly bus riders. PSTA, the MPO, the Pinellas Planning Council (PPC), the Tampa Bay Area Regional Transportation Authority (TBARTA), and the Florida Department of Transportation (FDOT) work cooperatively to plan premium transit for the region, including bus and light rail services, and to identify local opportunities for transit oriented development. Presently underway is the preparation of a Community Bus Plan, which is an in-depth study of the PSTA bus system that will identify strengths, areas for improvement, and make suggestions to improve efficiency and increase ridership. The Bus Plan will also look at the changing mobility needs of the county's residents, workers and visitors.

**Pinellas County School (PCS) System** – As the provider of public education in Pinellas County, the school system has an interest in supporting safe and efficient transportation in areas surrounding schools for walkers, bicyclists, bus riders, motorists and their passengers. To this end, the MPO and the PCS system partner through the School Transportation Safety Committee (STSC), as well as the Technical Coordinating Committee (TCC).

## **Achieving Regional and State Coordination**

Regional coordination within the greater Tampa Bay area includes the counties comprising the Transportation Management Area (Pinellas, Pasco and Hillsborough), as well as Citrus and Hernando counties. Together, these five counties make up FDOT's District 7. Additionally, the FDOT District 1 counties of Polk, Manatee and Sarasota are also considered part of the region. The West Central Florida MPOs Chairs Coordinating Committee (CCC), described below, is another "umbrella" entity created to achieve regional coordination. Organizations and entities that participate with the MPO in achieving such coordination include the following:

**Florida Department of Transportation (FDOT)** – The MPO and FDOT District 7 are partners in numerous local, regional and statewide initiatives. As previously mentioned, FDOT's District 7 Secretary, or a designee, participates in MPO Board meetings as a non-voting technical advisor, moreover, FDOT representatives routinely attend meetings of the MPO advisory committees and other entities that are also attended by MPO staff, such as the Chair's Coordinating Committee (CCC).

FDOT committees that involve the MPO as members include the Traffic Incident Management (TIM) Committee, the Community Traffic Safety Team (CTST) that meets in Pinellas County and the Regional Goods Movement Advisory Committee (GMAC). FDOT also “hosts” the Technical Review Team (TRT), which is a collaborative group that includes both FDOT and MPO staff. These aforementioned groups provide opportunities for the MPO to share information and perspectives with other agencies, such as law enforcement, transit, emergency management, county and municipal governments, economic development groups and the freight industry. FDOT also partners with the MPO on transit, through the Advisory Committee for Pinellas Transportation (ACPT).

**West Central Florida MPOs Chairs Coordinating Committee (CCC)** – Established by Section 339.175, F.S., the CCC represents eight counties in an effort to address a variety of transportation challenges on a regional, long-range basis. Issues such as personal mobility, access to jobs, goods movement, emergency evacuation, growth management, as well as congestion, are some of the concerns addressed by the CCC, which is made up of the chairpersons (or their designees) from Metropolitan Planning Organizations (MPOs) and Transportation Planning Organizations (TPOs). CCC members represent the Citrus TPO, Hernando MPO, Hillsborough MPO, Pasco MPO, Pinellas MPO, Polk TPO and Sarasota/Manatee MPO.

FDOT Secretaries (District 1 and District 7), Florida's Turnpike Enterprise, four Regional Planning Councils, and the Tampa Bay Area Regional Transportation Authority (TBARTA) are also represented on the CCC in a non-voting, advisory capacity. Among the CCC's responsibilities are the development and coordination of the Regional Congestion Management Process and the Transportation Regional Incentive Program (TRIP). Additionally, members of this MPO's Citizens Advisory Committee represent their MPO as members of the CCC's Joint Citizens Advisory Committee. MPO staff participates in bi-weekly Staff Directors Meetings and meetings of the Regional Multi-Use Trails Committee, the TRIP Working Group, as well as ad hoc committees and work groups.

As a subset of the CCC, Pinellas County is assigned to an urbanized area that includes the counties of Hillsborough and Pasco. The Pinellas County MPO has historically worked cooperatively with its MPO neighbors, as well as with other area MPOs through the CCC. Additionally, the Pinellas County MPO recognizes the need for a coordinated and collaborative regional transportation planning process and is committed to working with the Hillsborough and Pasco MPOs to identify ways to enhance the regional process. The three MPOs have recently agreed to the formation of a working group to develop and evaluate ways to improve coordination and focus on priorities for the urbanized area.

**Tampa Bay Area Regional Transportation Authority (TBARTA)** – In response to rapid development and resulting congestion, the Florida legislature established TBARTA in 2007 to develop and implement a Regional Transportation Master Plan for the purpose of improving mobility and expanding multi-modal transportation options for passengers and freight throughout the seven-county West Central Florida region, consisting of Citrus, Hernando, Hillsborough, Manatee, Pasco, Pinellas and Sarasota counties. The Chairs Coordinating Committee is represented on the TBARTA Board, and MPO staff participates in workgroups, including the Transit Management Committee and the Land Use Working Group, the latter which served as the primary forum for coordination between the Master Plan and local land use planning concerns such as existing land use patterns, long-range land use plans, growth projections, and local community goals. TBARTA is represented on the MPO's Technical Coordinating Committee, Bicycle Advisory Committee and Pedestrian Transportation Advisory Committee.

TBARTA also operates the multi-county transportation demand management (TDM) Commuter Services program, providing a number of simple and easy-to-use tools to help commuters and school children get where they need to go. Using the free online ride-matching program, commuters connect with each other to share the ride to and from work or school via carpool, vanpool or school pool. Other commuter services programs include Emergency Ride Home and Tele-work, in addition to assisting commuters with connecting to the local transit systems.



**Tampa Bay Regional Planning Council (TBRPC)** – TBRPC is responsible for coordinating and conducting a variety of regional planning activities within the Tampa Bay region, which is comprised of Hillsborough, Manatee, Pasco and Pinellas counties. The MPO partners with the TBRPC through membership in the Chairs Coordinating Committee and through support for community visioning and strategic planning initiatives.

**Metropolitan Planning Organization Advisory Council (MPOAC)** – The MPOAC provides a forum for discussion of Federal and State requirements for CMPs, and also facilitates statewide training programs for MPO staff. The Pinellas County MPO participates as a member of the MPOAC governing board and as a member of the Staff Directors Advisory Committee. The Policy and Technical Subcommittee annually prepares legislative policy positions and develops initiatives to be advanced during Florida's legislative session.

**West Central Florida Air Quality Coordinating Committee** – The West Central Florida Air Quality Coordinating Committee was formed in 1992 as a means to coordinate the air quality planning and regulatory activities around the Tampa Bay region and surrounding counties in response to the Clean Air Act Amendments of 1990. Committee membership was comprised of staff from the region's MPOs as well as county environmental management staff, industry representatives and public health groups. The committee convened for several years when the air quality within the region fell below federal standards. Federal and State emission reduction actions resulted in improved air quality and an “attainment” designation for the regional airshed. In 2010 it was decided that the committee will transition from its previous advisory role into a working group that will address, when the time comes, the (anticipated) Environmental Protection Agency’s (EPA) revisions to the ozone standards.

### **Public Participation**

As detailed in the Public Participation Plan, the MPO uses a wide variety of media to provide information about its programs and to collect input from citizens.

**Stakeholder Groups, Workshops and Public Hearings** – The MPO conducts forums, public hearings and workshops in conjunction with many of its planning activities. Community level forums are conducted at transportation accessible locations that comply with standards set by the Americans with Disabilities Act. All hearings and workshops are publicized and considerable efforts are made to connect with potential stakeholders by specifically targeting community associations, libraries, neighborhood newspapers, etc. Opportunities are also provided to individuals who cannot attend through web surveys and by providing other alternatives for MPO contact. Focus groups, charettes and eTownHalls are also conducted by the MPO to better engage the community and obtain input on projects and planning activities.

**MPO Website** – Visitors to the site can read and download this CMP document and other MPO planning documents and publications, including corridor studies and State of the System and Level of Service reports. A “Transportation Survey” web link makes it possible for citizens to report congestion problems as well as recommend solutions.

**Social Media** – The MPO began utilizing social media tools such as Facebook and Twitter in 2011 to provide updates and collect citizen feedback, including comments/complaints about congestion and safety. In addition, the MPO utilizes MindMixer, which is an on-line public engagement platform (branded as *TellUsPinellas*) to gather public input in the development of the 2040 LRTP.

**Printed Materials** – While the MPO still develops printed materials such as brochures, flyers, comment forms, fact sheets, press releases and newsletters for placement in information racks at the MPO office and other facilities including the County Courthouse and libraries, electronic distribution is the primary means for getting the material delivered to the residents of Pinellas County. Community events, neighborhood association meetings, public workshops, conferences and public forums provide additional opportunities for distributing materials and answering questions. Included in these publications are instructions on how to contact the MPO with any questions or comments.

**Government Access Channel** - All MPO meetings are televised live and then rebroadcast on the government access channel, Pinellas County Connection Television (PCC-TV). Additionally, an online web archive is provided to permit citizens to instantly access web videos by agenda topic, including those related to congestion management.

**Speakers Bureau** - The MPO receives requests for speakers directly from community groups and via requests submitted through the Pinellas County Communications Department. Groups that request speakers include local chapters of Rotary and Kiwanis clubs, Chambers of Commerce, business organizations and neighborhood associations. Congestion management issues are among the topics most frequently requested by groups or asked about during public outreach events.

**Surveys** - The MPO also utilizes surveys on occasion to gather public opinion concerning its planning activities and programs and to assess the public's level of awareness and understanding of them. Surveys are typically distributed at public events, workshops, libraries and at the MPO office. They are also posted on the MPO website where respondents can complete and submit them electronically.

**Public Comment at MPO Board Meetings** – Opportunities for the public to comment are provided at the beginning of every MPO board meeting. Citizens may address any item on the MPO's consent agenda, or any issue not already scheduled for a public hearing.

The Pinellas County MPO has had a Congestion Management Process (formerly called a Congestion Management System) in place since September 1997. The process was modified in 2008 to reflect SAFETEA-LU requirements. *Congestion Management Process: A Guidebook*, was published by the U.S. Department of Transportation/ Federal Highway Administration in 2009, and modified in 2011. The guidebook provides information on how to create an objective-driven, performance-based congestion management process (CMP). **The process described herein is consistent with the guidebook.** As stated previously, the federal *Moving Ahead for Progress in the 21<sup>st</sup> Century Act* (MAP-21), which went into effect in October 2012, makes essentially no change in the requirements for the CMP.

The Pinellas County MPO's CMP is intended to be dynamic in that it is subject to on-going reevaluation and adjustment; comprehensive in that it is both multi-modal and systemwide in its analysis and strategies; fiscally conservative in that costly improvements are considered only *after* less expensive alternatives have been determined to be inappropriate, inadequate or impractical; cooperative in that it actively seeks participation from stakeholders; and coordinated with other local and regional plans, studies, reports and processes.

### **Florida Statute, 163.3177**

*...A local government that has all or part of its jurisdiction included within the metropolitan planning area of a metropolitan planning organization (M.P.O.) pursuant to s. 339.175 shall prepare and adopt a transportation element consistent with this subsection. The element shall be coordinated with the plans and programs of any applicable metropolitan planning organization, transportation authority, Florida Transportation Plan, and Department of Transportation adopted work program.*

## Coordination and Integration with Pinellas County MPO Plans and Studies

### Long Range Transportation Plan (LRTP)

The LRTP serves as the basis for the MPO's planning programs and activities. The LRTP provides a systemwide, broad-based approach that defines the goals, objectives and policies to guide transportation planning over the next 25 years. The LRTP is typically updated in five year intervals. In 2009, the MPO adopted the 2035 LRTP. An update to the 2035 LRTP began in 2012, with adoption scheduled for December 2014. Due to the fact that the 2040 LRTP will be adopted in less than 18 months, the CMP policies and procedures presented in this document focus on the *proposed* 2040 LRTP goals and objectives, rather than the 2035 LRTP goals, objectives and policies. Table 1 lists the proposed 2040 LRTP goals and objectives. The MPO's 2040 LRTP will also provide for the following:

- Identification of the multi-modal transportation network;
- Systemwide goals and objectives, strategies and performance measures for the efficient and safe operation of the transportation network;
- An assessment of future demographic and economic viability trends and needs, including transit and goods movement;
- Identification of the concepts and values that provide the underpinning of transportation planning, such as livable communities, complete streets, air quality, environmental protection, and environmental justice;
- Assessment of the linkage between land use planning and transportation planning in Pinellas County; and
- Cost estimates, funding strategies and revenue sources for transportation projects, including those derived from the CMP.

**Table 1: Proposed Goals and Objectives  
for the 2040 Long Range Transportation Plan (LRTP)**

<b>Goal 1: Support and further economic development.</b>
<i>Objective 1.1: Integrate transportation and land use planning to ensure future decisions support keeping Pinellas County a place where people and business want to be.</i>
<i>Objective 1.2: Provide cost effective travel and commute options.</i>
<i>Objective 1.3: Improve access to and from major activity centers.</i>
<i>Objective 1.4: Create and promote opportunities for public-private partnerships.</i>
<i>Objective 1.5: Improve roadway operations for the movement of goods.</i>
<b>Goal 2: Provide a balanced and integrated multi-modal transportation system for local and regional travel.</b>
<i>Objective 2.1: Consider facilities for, and the connectivity between, all modes in the planning, design and construction of transportation projects.</i>
<i>Objective 2.2: Increase transit ridership by providing more frequent and convenient service.</i>
<i>Objective 2.3 Increase bicycle and pedestrian travel by providing sidewalks, bike lanes and multi-use trails throughout the county.</i>
<i>Objective 2.4: Coordinate and collaborate with transportation partners to provide for multi- modal options for local and regional travel.</i>
<b>Goal 3. Provide for a safe and secure transportation system for all users.</b>
<i>Objective 3.1: Reduce the rate and frequency of fatal and incapacitating crashes for all modes of travel.</i>
<i>Objective 3.2.: Provide for efficient emergency evacuation that responds to threats to Pinellas County and the Tampa Bay area.</i>
<i>Objective 3.3: Coordinate safe travel to and from schools.</i>
<i>Objective 3.4: Ensure security plans are in place.</i>
<b>Goal 4: Provide for, manage and operate an efficient transportation system.</b>
<i>Objective 4.1: Improve the performance of the transportation system through intersection modifications, access management strategies, Intelligent Transportation Systems applications, and other management and operational improvements.</i>
<i>Objective 4.2: Achieve consistency among transportation plans and programs.</i>
<i>Objective 4.3: Maintain transportation infrastructure in a state of good repair.</i>
<i>Objective 4.4: Facilitate timely implementation of projects.</i>
<i>Objective 4.5: Provide real-time information to support the efficient movement of people and goods.</i>

<p><b>Goal 5: Encourage public participation and ensure that the transportation plan and other MPO planning activities reflect the needs of the community, particularly those that are traditionally underserved.</b></p>
<p><i>Objective 5.1: Provide opportunities to engage citizens, particularly the traditionally underserved populations, and other public and private sector entities.</i></p>
<p><i>Objective 5.2: Consider and respond, as appropriate, to all comments received.</i></p>
<p><b>Goal 6: Enhance the quality of life and promote sustainability.</b></p>
<p><i>Objective 6.1: Protect the environment from any adverse impacts of the transportation system, and mitigate as appropriate.</i></p>
<p><i>Objective 6.2: Plan for, and adapt to, the potential impacts of rising sea levels and climate change on the transportation system.</i></p>
<p><i>Objective 6.3: Ensure that benefits and impacts of transportation investments are equitably distributed.</i></p>
<p><i>Objective 6.4: Provide better transit access to a greater number of people including those who are transit dependent, minority, low income, and/or disabled.</i></p>

The 2040 LRTP will include a list of CMP projects and identify funding for those projects, where feasible.

Also, an Environmental Justice (EJ) analysis for the Pinellas County MPO is included in the LRTP. EJ builds on Title VI of the Civil Rights Act of 1964, which is designed to ensure non-discrimination in Federal programs. Maps and summary data are spatially analyzed to determine how low income and minority populations are benefitting from the transportation projects being proposed.

**Transportation Improvement Program (TIP)** – The TIP is a federally required five-year program of transportation improvements adopted annually by the MPO that incorporates the work programs of FDOT, PSTA, and the county and local municipalities. The TIP is based on the State’s fiscal year (July 1 to June 30). The TIP’s detailed work program/project descriptions include:

- Location and limits (to/from) of all scheduled projects, including CMP and multi-modal projects;

- Identification of improvements (i.e. road construction, ITS implementation, transit, resurfacing, etc.); and
- Information concerning the cost, funding source and timeline for the completion of each project.

The adopted TIP also includes a list of unfunded CMP projects that is reviewed on an on-going basis for prioritization and implementation (see Appendix). For example, a roadway or intersection that is congested may not receive a CMP priority status if it is already scheduled for capacity improvements. Likewise, resurfacing projects identified in the TIP may provide additional opportunities for enhancements such as sidewalks and bicycle lanes at congested locations.

**Level of Service (LOS) Report** – The MPO monitors approximately 2,280 lane miles, including major freeways, signalized arterials, signalized collectors and non-signalized collectors (State, County and local). Using vTIMAS software, the MPO provides an annual inventory on the performance of these roadways, including volume-to-capacity ratios and AADT (average annual daily trips), in addition to operating conditions, graded on six levels: “A” (best) through “F” (failing). These data are then used in the CMP to help identify “constrained” and “deficient” roads. Constrained roads are roads for which no additional widening (addition of one or more through lanes) beyond what is included in the adopted LRTP is planned. Deficient roads are those roads that operate at a level of service (LOS) “E” or “F,” or a volume-to-capacity ratio of 0.90 or greater. The Pinellas County MPO’s acceptable LOS standard is LOS “D,” or better.

The MPO’s Level of Service Report and Crash Facts Report are used as input to the biennial State of the System (SOS) Report.



### **Long Range Advanced Traffic Management System/Intelligent Transportation Systems (ATMS/ITS) Master Plan (2006) and Intelligent Transportation Systems/ Advanced Traffic Management System Implementation Plan (2009)**

ITS/ATMS is a valuable strategy for addressing congestion management in Pinellas County. ITS is the application of ever emerging technologies that assist agencies in the operation and management of transportation facilities. It has been demonstrated that ITS projects have increased operational capacity, improved efficiency and enhanced safety. ITS covers all transportation modes and is commonly divided into subcategories, based on services, which include Advanced Traffic Management Systems (ATMS) for arterials and freeways. Specifically, the focus is on the arterial road system within Pinellas County with consideration given to the freeway system as necessary to address regional traffic management needs during recurrent congestion, major incidents, evacuations, and other transportation related emergencies. ITS tools utilized by the county's transportation professionals to optimize management of traffic operations, travel demand and roadway capacity include:

- detection system;
- traffic monitoring and surveillance;
- data capture and archiving and information dissemination;
- signal preemption for fire trucks;
- operational strategies, such as fully actuated traffic signals, optimized traffic operations; and
- traffic responsive control, traffic adaptive control, incident management, travelers' information, and special event management.

ITS/ATMS have become a cost-effective, alternative strategy for managing congestion. ITS/ATMS solutions do not replace the traditional capacity building investments such as new lanes or new roads, nor are they substitutes for conventional traffic engineering and traffic operations strategies. Rather, ITS/ATMS offer transportation agencies a broad set of transportation related technologies that encompass more than smart traffic signal systems.

**Crash Facts Report** – This annual report, which is a compilation of motor vehicle crash data from the MPO's Crash Data Management System, provides municipal governments, county and

regional agencies and private sector companies and businesses with information and statistics for future safety planning and project prioritization. Data are drawn from local law enforcement agency crash reports. As shown previously in Figure 2, traffic incidents, including crashes, accounted for approximately 25 percent of nonrecurring congestion in a nationwide survey. The Crash Facts Report assists the MPO in identifying those roadway facilities and segments that have high crash rates, which lead to nonrecurring congestion in Pinellas County. Moreover, as discussed in the next chapter, safety/crash data is used in the overall analysis of congestion problems and the scoring which determines the most congested roadway facilities and segments. Also, the MPO's analysis of crash data directly addresses the FDOT's 2012 Strategic Highway Safety Plan (SHSP), specifically the SHSP's emphasis areas of vulnerable users, lane departure and intersection crashes, as well as traffic data.

**State of the System (SOS) Report** – The MPO's biennial State of the System (SOS) Report contributes to the foundation of the CMP, using data drawn from the MPO's Transportation Planning Inventory (TPI), Crash Data Management System (CDMS), and other sources. The SOS provides an inventory of the transportation network, including roads, trails, sidewalks, bicycle lanes and transit. Trends analyzed include demographics; vehicle miles and hours traveled segmented by planning sector; congested miles segmented by planning sector; duration of congestion hours for SIS/Non-SIS roadways; crash frequencies; bike lane and sidewalk coverage, trail usage; ozone readings, etc. The SOS also reports on the current status of strategies identified through previous CMP studies, and provides data used in the preliminary screening phase for the CMP priority list.

**CMP Corridor, Hot Spot Studies, and Safety Studies** – CMP analysis may indicate a need to study a specific corridor, segment or "hot spot," to be initiated by the MPO, FDOT or a local government. The purpose of the study is to identify the conditions that may be contributing to congestion and crashes, and to identify corrective strategies, some of which may require MPO approval and prioritization for funding. Following this study, progress reports tracking the implementation of strategies are submitted to the Intelligent Transportation Systems (ITS)

Committee and to the Technical Coordinating Committee (TCC). See Appendix for Congestion Management Process (CMP): Corridors and Hot Spots Studies Map.

**Countywide Bicycle & Pedestrian Strategies** – The Pinellas County MPO has long supported non-motorized transportation strategies, including bicycle and pedestrian facility improvements, to encourage modes other than the single-occupant motor vehicle. Specific strategies include increasing sidewalk connectivity and bike lane coverage, improving bicycle facilities at transit stations and other trip destinations, improving the safety of existing bicycle and pedestrian facilities, and supporting a complete streets policy to assist in the creation of a countywide transportation network that gives citizens choices other than the automobile. An update to the MPO’s Bicycle and Pedestrian Master Plan, anticipated for publication in 2013, will include a report and analysis of crash data and trends affecting bicycling and pedestrian activity in the county, while also addressing facility design issues, safety policies, and education and law enforcement measures. With regard to safety, the MPO’s Pedestrian Safety Action Plan identifies strategies and policy initiatives aimed at improving pedestrian safety - countywide.

**Goods Movement** – A concern for the efficiency and safety of goods movement is routinely interwoven through a variety of MPO initiatives and programs. While the County and municipal governments each have individual local ordinances that document and manage the movement of truck traffic, the MPO reviews the operational performance of freight mobility corridors, and makes recommendations for short-term, low-cost improvements and long-term alternative strategies to alleviate congestion. The MPO coordinates with each jurisdiction and maintains a Countywide Truck Route Plan Map. (See Appendix for the Truck Route Plan Map.)

The Tampa Bay Regional Strategic Freight Plan identifies strategic investments and low cost operational improvements needed for better mobility and accessibility for trucks. The MPO considers and refers to the recommendations of this plan, and other studies, during the review of roadway and intersection construction plans submitted by the various implementing agencies. The recommendations from the local and regional freight studies are considered along with high crash and congestion locations in the development of CMP project priorities, moreover, the

recommendations provide the opportunity to include operational enhancements in the projects. The MPO seeks to implement the improvements primarily through construction and resurfacing projects; however, there may be instances when these improvements would need to be advanced as stand-alone projects - the same as other management and operations (e.g. CMP) projects.

### **Coordination and Integration with State, Regional and Local Plans and Studies**

The CMP is coordinated with a broad spectrum of state, regional and local plans and studies, including but not limited to:

**Highway Safety Improvement Program (HSIP)** – Crash reduction is an important strategy for reducing congestion. Section 1401 of the *Safe, Accountable, Flexible, and Efficient Transportation Equity Act: A Legacy for Users* (Public Law 109-59) amended Section 148 of Title 23, United States Code, and created a new Highway Safety Improvement Program (HSIP) as one of the Federal Highway Administration's "core" programs. The goal of the program is to achieve a significant reduction in traffic fatalities and serious injuries on public roads. As part of the new HSIP, states were required to submit an annual report describing not less than five (5) percent of their highway locations exhibiting the most severe safety needs. However, while MAP-21 continued HSIP as a core Federal-aid program, it no longer requires the five (5) percent report.

**Strategic Highway Safety Plan (SHSP)** – The Florida Department of Transportation, in partnership with the Federal Highway Administration, has a Strategic Highway Safety Plan (SHSP), which was updated in 2012. Florida's SHSP is a statewide, data-driven plan that addresses the "4 E's" of safety – engineering, enforcement, education, and emergency response, and the following eight (8) Emphasis Areas:

- Aggressive Driving;
- Intersection Crashes;

- Vulnerable Road Users (pedestrians, bicyclists and motorcyclists);
- Lane Departure Crashes;
- Impaired Driving;
- At-Risk Drivers (aging road users and teens);
- Distracted Driving; and
- Traffic Data

As discussed previously, traffic incidents, including crashes, accounted for approximately 25 percent of nonrecurring congestion in a nationwide survey.

**Regional Congestion Management Process** - The Pinellas County MPO's CMP is coordinated with the Regional Congestion Management Process that is the product of the Chairs Coordinating Committee (CCC). A Regional CMP Steering Committee is made up of representatives from the MPOs, FDOT and other stakeholder agencies within the region. For the most recent update to the Regional CMP, the Regional CMP Steering Committee provided concurrence on the regional goals and objectives, and agreed with producing the State of the Systems Report. With each update of the State of the System Report, a limited number of congested corridors will be selected by the Steering Committee and evaluated in detail to identify strategies for congestion or safety mitigation benefits. The Pinellas County MPO's CMP is also coordinated with the following:

- West Central Florida Multi-Use Trails Plan (WCFMUT) – Non-motorized transportation strategies that can reduce congestion include bicycle, pedestrian and trail facility improvements that encourage non-motorized modes instead of single-occupant motor vehicle trips. The Multi-Use Trails Plan serves as the foundation for regional coordination and inter-jurisdictional review of bicycle and pedestrian issues. All six MPOs in West Central Florida have extensively analyzed bicycle and pedestrian needs as part of their long-range planning programs. These activities include the inventorying and analysis of existing facilities, and studying the location of bicycle/pedestrian trip attractors and generators. The Multi-Use Trails Plan

provides a regional perspective and linkage among MPO bicycle planning and other non-motorized programs.

- **Transportation Regional Incentive Program (TRIP)** - The Transportation Regional Incentive Program (TRIP) was created through Florida's growth management legislation to help improve regionally significant transportation facilities. State funds are made available through the TRIP program to help local governments and other transportation partners pay for transportation projects that benefit regional travel. Examples of TRIP projects that aid in the reduction of congestion include regional vanpool programs, regional bus transfer facilities, ITS trunk line infrastructure, ATMS signal system improvements, intersection improvements, as well as capacity improvements.

**Tampa Bay Regional ITS Architecture** - Provides a roadmap for integrating the intelligent transportation systems in the Tampa Bay/FDOT District 7 region, which includes Citrus, Hernando, Hillsborough, Pasco and Pinellas counties, for a 20 year period. Developed through a cooperative effort by the region's transportation agencies, it represents a shared vision of how agencies' systems will work together, sharing information and resources, to provide a safer, more efficient, and more effective transportation system.

**Locally Preferred Alternative (LPA)** – Following a multi-year Alternatives Analysis study conducted by the MPO, PSTA, FDOT and TBARTA, the Locally Preferred Alternative (LPA) was identified. The LPA includes 24 miles of light rail service connecting Clearwater, Largo, the Greater Gateway Area, Pinellas Park, and St. Petersburg with a connection to Hillsborough County across the Howard Frankland Bridge. The LPA is supported by a premium bus transit network that includes frequent bus with expanded hours of service, trolleys, regional connectors, and community circulators. The adopted LPA will also provide opportunities for transit oriented development in the light rail station areas and along premium bus corridors. Implementation of the LPA is expected to increase transit

ridership, alleviate congestion, encourage economic development and promote and protect the quality of life in Pinellas County.

### Linkage with Local Governments

The MPO has historically played a major role in countywide transportation planning, through a variety of programs and initiatives, described as follows:

**Growth Management and Transportation Concurrency** - In 2006, the MPO developed a model proportionate fair share ordinance for use by local governments. The local government implementing transportation concurrency must provide a means to assess a landowner its proportionate share of the cost of providing the transportation facilities necessary to serve the proposed development, but the landowner shall not be held responsible for mitigation necessary to achieve an adopted level of service standard on an impacted transportation facility.

In 2011, the Florida Legislature made significant changes to Chapter 163, Part II, Florida Statutes, known as the "Community Planning Act" that involved changes to the requirements for transportation and mobility planning in local governments' comprehensive plans. The changes in the law were intended to encourage economic development by providing flexible transportation mobility options to Florida communities. Transportation concurrency is now optional, and if a local government chooses, it can eliminate the transportation concurrency provisions from its comprehensive plan. However, if transportation concurrency is eliminated, level of service standards for roads must still be retained for capital improvement planning; the standards must be appropriate and based on professionally accepted studies, and the capital improvements that are necessary to meet the adopted levels of service standards must be included in the 5-year schedule of capital improvements. The MPO is assisting Pinellas County's 25 local governments through this transition primarily through the preparation of a Multi-modal Mobility Plan, discussed below.

**Multi-modal Mobility Plan** – At the request of the county’s local governments, the MPO is leading the effort to develop a countywide multi-modal Mobility Plan that will be coordinated with the MPOs’ Long Range Transportation Plan. The vision for the Mobility Plan also overlaps with the goals and objectives of the CMP in that both seek to identify low cost operational improvements, transit service enhancements and expansion of facilities for bicyclists and pedestrians. With regard to the LRTP, the Mobility Plan will provide a mechanism for developers to fund CMP projects identified in the LRTP to help mitigate congestion issues. The proposed Mobility Plan/Mobility Fee integrates the existing impact fee ordinance with growth management requirements. The countywide Transportation Impact Fee Ordinance (TIFO) will be maintained, with the fee renamed Multi-modal Impact Fee. The fee would continue to be applied to all development adding new trips to the surrounding road network.

**Access Management Standards** – The MPO works with FDOT to ensure consistent application of access management standards on state roads. The MPO works with local government and state traffic operations engineers to assess access needs, including the need for signalization and median openings on major roads to serve adjacent businesses. The MPO seeks to balance roadway operations, and safety with the access needs of businesses. However, much of this work is the responsibility of, and accomplished through, the respective local government’s site plan review process.

**Coordination of Transportation and Land Use Plans** – The MPO has a number of strategies in place to better integrate transportation and local land use plans. Through its planning for premium transit service in the county, the MPO is working with local governments to develop comprehensive plan policies for Transit-Oriented Development (TOD). Several local governments including Pinellas County and its largest cities have adopted TOD policies. Others are utilizing policies developed by the Pinellas Planning Council (PPC), the agency responsible for administering the Countywide Future Land Use Plan Map and Rules. The PPC is a dependent special district created by special act of the Florida legislature several decades ago for the purpose of undertaking land use planning



functions for all of Pinellas County. As described earlier, in March 2012 the Florida legislature passed House Bill 869, seeking to implement land use and transportation planning functions in Pinellas County in a more integrated manner. The bill combined the leadership of the MPO with the leadership of the PPC so that a single policymaking body oversees both land use planning and transportation planning in Pinellas County.

Simply stated, congestion management is the application of strategies to improve transportation system *performance and reliability* by reducing the adverse impacts of congestion on the movement of people and goods.

As stated previously, the purpose of this document is to identify and describe the process used by the MPO to respond to the federal and state CMP requirements. This document is not intended to serve as a congestion management “plan,” but rather a “process” that provides for the safe and effective integrated management and operation of the multi-modal transportation system – countywide. The CMP *is* intended to use an objectives-driven, performance-based approach to planning for congestion management.

*Congestion Management Process: A Guidebook*, published by the U.S. Department of Transportation/ Federal Highway Administration in 2009 and modified in 2011 provides an Eight Step process to assist MPOs in setting up a CMP. The guidebook was intended to be adapted to meet the unique conditions and requirements of MPOs and their respective communities. It should be noted that these steps are compatible with the process historically used by the MPO.

### **The Eight Step Process:**

- Step One: Develop System-wide Objectives for Congestion Management
- Step Two: Define the CMP Network
- Step Three: Develop Multimodal Performance Measures
- Step Four: Collect Data/Monitor System Performance
- Step Five: Analyze Congestion Problems and Needs
- Step Six: Identify and Assess CMP Strategies
- Step Seven: Program and Implement Strategies
- Step Eight: Evaluate the Effectiveness of Strategies

*Congestion Management Process: A Guidebook*, published by the U.S. Department of Transportation/Federal Highway Administration in 2009 and modified in 2011

### Definitions:

- Goal – A long-term, broad-based, general statement of intention.
- Objective – An intended outcome (systemwide or site specific) that represents a step or an approach to reaching a goal. Objectives may be expressed broadly, (i.e. improve system reliability), or expanded through the application of “SMART” concepts (specific, measurable, agreed upon, realistic, time-bound) for additional definition and commitment.
- Systemwide – Pertains to the transportation network as addressed by the Pinellas County MPO, which includes Pinellas County and 24 local governments.
- Regional – Pertains to the following counties: Citrus, Hernando, Hillsborough, Manatee, Pasco, Pinellas, Polk and Sarasota.
- Community values – The shared ideas or concepts that contribute to the quality of life, or that are commonly regarded as essential to the character, aesthetics or operation of a community or neighborhood.
- Environmental Impact – The National Environmental Policy Act (NEPA) of 1969 requires that all federally funded projects be made with consideration of the impact to the natural and human environment. The MPO extends this consideration to all of its planned improvements.
- Environmental Justice (EJ) – EJ is based on Title VI of the Civil Rights Act of 1964, which is designed to ensure nondiscrimination in Federal programs, including transportation. EJ analysis addresses how low income and minority populations are benefitting from or are adversely affected by transportation projects.
- Transit – (“mass transit”) Refers to public transportation by bus, rail, trolley, etc. In this county, the transit system is operated by the Pinellas Suncoast Transit Authority (PSTA).

## Step One: Develop System-wide Objectives for Congestion Management

Federal regulations require congestion management objectives as part of the CMP (23 CFR 450.320(c) 2). As was previously described, the LRTP serves as the basis for the MPO's planning programs and activities, including congestion management, and it is typically updated in five year intervals. As previously described, an update to the 2035 LRTP began in 2012, with adoption scheduled for December 2014. Due to the fact that the 2040 LRTP will be adopted in less than 18 months, the following CMP goal and objectives have been cross-referenced with the relevant *proposed* 2040 LRTP objectives, rather than the 2035 LRTP goals, objectives and policies. Table 2 lists the cross-referenced (proposed) 2040 LRTP objectives.

### **CMP Goal and Objectives**

**CMP Goal** : To ensure the safe and efficient movement of people and goods by successfully addressing areas of recurring and non-recurring congestion with low cost and cost effective operational and multi-modal improvements, before considering any capital intensive capacity improvements.

**CMP Objective 1**: To increase the number of low cost and cost-effective operational improvements in areas characterized by recurring congestion, i.e., roadways and intersections with bottlenecks and/or poor signal timing. (*Proposed 2040 LRTP Objectives 3.1 and 4.1*)

**CMP Objective 2**: To increase the attractiveness and efficiency of transit service to draw more choice riders and reduce dependency on the single occupant vehicle (SOV). (*Proposed 2040 LRTP Objectives 1.2, 2.2 and 6.4*)

**CMP Objective 3**: To increase or improve the coverage of bike lanes, trails, sidewalks and crosswalks in areas characterized by congestion *and* where shorter automobile trips can be readily converted to foot and bicycle modes. (*Proposed 2040 LRTP Objectives 2.1 and 2.3*)

**CMP Objective 4:** To increase public awareness of, and participation in, transportation demand management programs, including but not limited to carpooling, vanpooling, school pool and telecommuting, in order to reduce dependency on the single occupant vehicle (SOV). *(Proposed 2040 LRTP Objective 1.2)*

**CMP Objective 5:** To effectively manage scheduled and unscheduled traffic incidents associated with non-recurring congestion, including reducing the frequency and severity of accidents in high crash areas. *(Proposed 2040 LRTP Objectives 3.1, 4.1 and 4.6)*

**CMP Objective 6:** To improve the safe and efficient movement of goods. *(Proposed 2040 LRTP Objectives 1.5, 3.1 and 4.6)*

<b>Table 2: Proposed 2040 LRTP Objectives Associated with the CMP Objectives</b>
LRTP Objective 1.2: Provide cost effective travel and commute options.
LRTP Objective 1.5: Improve roadway operations for the movement of goods.
LRTP Objective 2.1: Consider facilities for, and the connectivity between, all modes in the planning, design and construction of transportation projects.
LRTP Objective 2.2: Increase transit ridership by providing more frequent and convenient service.
LRTP Objective 2.3 Increase bicycle and pedestrian travel by providing sidewalks, bike lanes, and multi-use trails throughout the county.
LRTP Objective 3.1: Reduce the rate and frequency of fatal and incapacitating crashes for all modes of travel.
LRTP Objective 4.1: Improve the performance of the transportation system through intersection modifications, Intelligent Transportation Systems applications, and other management and operational improvements.
LRTP Objective 4.6: Provide real-time information to support the efficient movement of people and goods.
LRTP Objective 6.4: Provide better transit access to a greater number of people including those who are transit dependent, minority, low income, and/or disabled.

## Step Two: Define the CMP Network

The CMP covers all of Pinellas County, which is approximately 280 square miles in size, and comprised of 25 local governments and a 2010 population of 916,452 (see Appendix for Pinellas County Urban Boundary Map). Pinellas is bounded by Pasco County to the north, the Gulf of Mexico to the west, Tampa Bay and Hillsborough County to the east, and the Sunshine Skyway Bridge and Manatee County to the south. Local governments include Pinellas County and the following 24 municipalities: Belleair, Belleair Beach, Belleair Bluffs, Belleair Shore, Clearwater, Dunedin, Gulfport, Indian Rocks Beach, Indian Shores, Kenneth City, Largo, Madeira Beach, North Redington Beach, Oldsmar, Pinellas Park, Redington Beach, Redington Shores, Safety Harbor, St. Pete Beach, St. Petersburg, Seminole, South Pasadena, Tarpon Springs and Treasure Island.

Consistent with federal guidelines, the CMP covers a multi-modal transportation network. In addition to evaluating the roadway network, the CMP addresses transit, bike/ped/trail facilities and goods movement.

**CMP Network:** The following describes the CMP network, with the relevant maps provided in the Appendix.

- The MPO partners with State, County and local governments and transportation related agencies, providing planning support and monitoring for slightly less than 587 centerline miles of functionally classified roadway (see Appendix for Major Road Network map). This network includes freeways, arterials and collectors, and some local roads. Functionally classified residential streets are excluded. Strategic Intermodal System (SIS) roadways comprise approximately nine percent of the centerline miles.

### **Definitions:**

The major road network includes:

Freeways – Multilane, divided highways with at least two lanes of traffic in each direction

Arterials – Roadways (excluding freeways) serving thru traffic with average signal spacing of 2 miles or less

Collectors – Roadways providing access and traffic circulation with residential and industrial areas

Segments – Single sections of a roadway, operationally defined

Corridors – Multiple, contiguous segments, operationally defined

Hot Spots – Individual points or intersections on a roadway identified through CMP study as severely congested

Regional Transportation Network – Roadways, trailways and transit routes designated as regionally significant by the West Central Florida Chairs Coordinating Committee (CCC).

In Pinellas County, the State and Federal roadways are managed by the Florida Department of Transportation (FDOT), while the County maintains all county and local roads in unincorporated area. The remaining local roads are maintained by the respective municipality, with the signal systems controlled by Pinellas County, with the exception of St. Petersburg. Sidewalks and bicycle lanes are maintained by the local government responsible for the adjacent roadway.

- Pinellas County operates the Intelligent Transportation Systems/Advanced Traffic Management System (ITS/ATMS) program, under an interlocal agreement with all cities

except St. Petersburg, which controls its own signal system. (See Appendix for ITS Corridors Map.)

- Most of the county is served by one transit system, the Pinellas Suncoast Transit Authority (PSTA), which also operates its own transportation technologies, including ITS applications. Property owners in cities not assessed for PSTA service, thus not served by PSTA, are St. Pete Beach, Treasure Island, Belleair Bluffs, Belleair Shore, and Kenneth City. However, St. Pete Beach and Treasure Island do receive bus service via a special contract. PSTA presently provides bus service along 40 routes, including two express routes. Routes are subject to periodic change and update, as determined by ridership numbers, etc. (See Appendix for Transit Development Plan map.)
- Except within the City of Dunedin, Pinellas County maintains the Pinellas Trail and the Progress Energy Trail, countywide. Local governments construct and maintain trails within their respective boundaries. (See Appendix for Pinellas Trailways Plan map.)
- Roadways within the CMP network that have been identified by the local governments as being appropriate for the routing of trucks are identified on the MPO Countywide Truck Plan Map. The local governments maintain ordinances to impose restrictions on these routes, including restrictions on time of day, vehicle weights, and hazardous materials. The MPO also acts as a forum for matters related to truck routing, seeking to coordinate any changes to the Plan at a countywide level to ensure consistency as trucks move from one jurisdiction to another. Performance of roads that serve as truck routes are included in the major road network, and as a result, are routinely monitored for congestion and safety. (See Appendix for Pinellas County Truck Route Plan with Railroad Corridor Map.)
- The six Metropolitan Planning Organizations (MPOs) that comprise Chairs Coordinating Committee (CCC) are required by federal regulations and Florida statutes to implement a Regional CMP to address congestion of regional significance. The Regional CMP is a



systematic and regionally-accepted approach for managing congestion that provides accurate, current information on transportation system performance and assesses alternative strategies for congestion management that meet state and regional needs. The August 2012 Regional Congestion Management Process, Policy & Procedures Handbook is the working tool that the CCC embraces to effectively integrate both the Regional and each MPO's project prioritization processes, TIPs and LRTPs (see Appendix for CCC Regional Road Map).

## Step Three: Develop Multi-modal Performance Measures

### When Performance Measures are Applied

Performance measures are applied throughout the Congestion Management Process. According to federal regulation, the CMP must include appropriate performance measures to assess the extent of congestion and support the evaluation of the effectiveness of *congestion reduction and mobility enhancement strategies* for the movement of people and goods (23 CFR 450.320 (c) 2). The MPO uses systemwide multi-modal performance measures to evaluate changes on an aggregated basis to the entire transportation system over time, and determines whether the implemented strategies are achieving the desired objectives. The CMP objectives and performance measures are shown in Table 3.

#### **Definitions:**

Multi-modal – Includes all modes of transportation, including automobile, transit, pedestrian, bicycle.

- Performance measures – The use of data and other evidence to determine progress toward specifically defined, organizational objectives. This includes both quantitative evidence (statistical data such as the measurement of travel times) and qualitative evidence (such as the measurement of customer satisfaction, collected citizen comments, etc.).

For the purpose of this report, reference will be made to two types of performance measures:

- Systemwide Performance Measures assess the overall multi-modal availability, safety and efficiency of the transportation network, identifying locations where congestion is a problem.
- Site Specific Performance Measures are used to determine if an implemented strategy has been successful in meeting its predefined objective.

**TABLE 3**

**CMP Goal** : To ensure the safe and efficient movement of people and goods by successfully addressing areas of recurring and non-recurring congestion with low cost and cost effective operational and multi-modal improvements, before considering any capital intensive capacity improvements.

**CMP Objective 1:** To identify and increase the number of low cost and cost-effective operational improvements in areas characterized by recurring congestion, i.e., roadways and intersections with bottlenecks and/or poor signal timing.

**Potential Performance Measures:** Percent of VMT and roadway miles operating below an acceptable level of service; V/C ratios; duration of congestion; travel speed; identification and ranking of high crash intersections and roadways; analysis of crash data, including frequency and severity of crashes, and crashes involving vulnerable users; crashes caused by lane departure and aggressive driving; clearance time per incident; and number of *congestion report forms* entered on the MPO’s web site.

**CMP Objective 2:** To increase the attractiveness and efficiency of transit service to draw more choice riders and reduce dependency on the single occupant vehicle (SOV).

**Potential Performance Measures:** Percent of congested roadway centerline miles with transit service; passenger trips per revenue hour; average service frequency; on-time performance; annual ridership; transit mode share; percentage of population within ¼ mile of a transit stop; real-time bus info; and number of buses and routes.

**CMP Objective 3:** To increase or improve the coverage of bike lanes, trails, sidewalks and crosswalks in areas characterized by congestion *and* where shorter automobile trips can be readily converted to foot and bicycle modes.

**Potential Performance Measures:** Percent of congested roadway centerline miles with bike lane and sidewalk coverage; miles of multi-use trails; number of individuals who walk or bike to work; trail usage; road safety audits; percentage of Pinellas Trail Loop completed; and population and jobs within ¼ mile of the Pinellas trail.

**CMP Objective 4:** To increase public awareness of, and participation in, transportation demand management programs, including but not limited to carpooling, vanpooling, school pool and telecommuting, in order to reduce dependency on the single occupant vehicle (SOV) and thereby reduce congestion.

**Potential Performance Measures:** Number of TDM programs, and participation rates.

**CMP Objective 5:** To effectively manage scheduled and unscheduled traffic incidents associated with non-recurring congestion, including reducing the frequency and severity of accidents in high crash areas.

**Potential Performance Measures:** Identification and ranking of high crash intersections and roadways; analysis of crash data, including frequency and severity of crashes, crashes involving vulnerable users; crashes caused by lane departure and aggressive driving; clearance time per incident; number/percentage of ITS/ATMS projects implemented; and number of variable message signs.

**CMP Objective 6:** To improve the safe and efficient movement of goods.

**Potential Performance Measures:** Truck route V/C ratios; location, frequency and severity of crashes involving heavy/commercial vehicles; number of variable message signs; and number of completed operational improvements for trucks.

## Types of Datasets Used for Performance Measures

Datasets commonly applied to CMP performance measures include the following:

**Roadway Performance Data** – The MPO routinely performs field studies and data gathering activities to monitor roadways for the purpose of obtaining data on the systemwide performance of the transportation network. This activity is intended to identify the location of a congestion problem, or the measures of effectiveness of an implemented strategy. Data collected is supplemented by data received from the Florida Department of Transportation, Pinellas County, and other local governments. Results are published in MPO reports, including the annual *Level of Service Report* and biennial *State of the System Report*. Performance measures generated include vehicle miles of travel and hours of travel, volume to capacity ratios, miles over capacity, level of service, and duration of congestion.

Technologies, including those related to Intelligent Transportation Systems (ITS), are generating increasingly precise data, and advancements such as Bluetooth, GPS and other vehicle onboard technologies hold even greater promise for expanding the list of available performance measures.

Data relating to corridor and hot spot study locations receive additional review and analysis to assess current performance and to track the effectiveness of implemented CMP strategies. These reports are submitted for review to the TCC and ITS advisory committees, responsible for implementing the CMP. (See Appendix for Status Report: Implementation of Recommendations – CMP.)

In addition to roadway data, other data classes used by the MPO for the CMP include:

**Multi-modal Data** – Pinellas Suncoast Transit Authority (PSTA) provides statistics on the number of buses, routes and annual ridership, as well as on-time performance data. The MPO’s *State of the System Report* includes trail, sidewalk and bike lane coverage data.

**Safety Data** – The MPO’s Crash Data Management System (CDMS) archives crash reports, as submitted by local law enforcement agencies. Data can be segmented according to date, location, and by characteristics including aggressive driving, intersection crashes, vulnerable road users, and lane departure crashes.

**Other Types of Quantitative Data** – The CMP approach also requires the design of strategies that are responsive to the needs of the citizens of Pinellas County. Data useful for studying congestion and safety is collected by the MPO from other agencies and organizations, including the U.S. Bureau of the Census (population and other demographic data), Florida Dept. of Highway Safety and Motor Vehicles (crash data, vehicle registrations, licensed drivers), and the Tourist Development Council (tourism data).

**Qualitative Data** – Chapter Three of this document (Partnerships) focuses on how the MPO works with advisory committees, local governments, public and private agencies and organizations, and the general public to obtain qualitative input pertaining to community values and needs, satisfaction with existing CMP initiatives and projects, and suggestions for improvement.

## Step Four: Collect Data/Monitor System Performance

Data collection and system monitoring are needed to provide information to make effective decisions, and are an on-going activity. According to Federal regulation, the CMP must include an established, coordinated program for data collection and system performance monitoring to: 1) define the extent and duration of congestion; 2) contribute in determining the causes of congestion; and 3) evaluate the efficiency and effectiveness of implemented actions (23 CFR 450.320 (c) 3). Table 4 shows the types of data used for monitoring systemwide performance and the agencies responsible for contributing to the system-wide screening.



Type of Data	Monitoring Agency	Notes
<b>Roadway Performance (AADT, DOC, V/C, VMT, VHT, etc.)</b>	MPO, FDOT	Field data collected on an ongoing basis. Analyzed annually for the Level of Service Report, biennially for the State of the System Report, and periodically for CMP updates provided to the TCC and ITS Committees.
<b>Inventory of road lane miles, existing lanes, constrained lanes, etc.</b>	MPO, FDOT	Field data collection is ongoing for MPO.
<b>Inventory of regional roads, SIS/Non-SIS &amp; trails</b>	MPO, CCC, FDOT, TBARTA	Monitored on an ongoing basis with revisions, as needed.
<b>Inventory of bike lanes, sidewalks, trail network</b>	MPO and local governments	MPO coordinates data for the LRTP and the SOS Report. County maintains data for Pinellas Trail/Progress Energy Trail.
<b>Status of current construction projects</b>	MPO, FDOT, Pinellas County & cities	FDOT, County and local governments submit; MPO tracks and posts on the public website.
<b>Status of planned projects</b>	MPO, FDOT and Local Governments	MPO produces the TIP in conjunction with FDOT, County and local governments.
<b>Inventory of ITS/ATMS implementation</b>	MPO	Implementation status is submitted by FDOT, Pinellas County, and local governments. The MPO ITS map and associated table are updated annually and submitted to the ITS and TCC committees for review and approval.
<b>ITS and operations data, studies (travel time, etc.)</b>	Pinellas County	Pinellas County submits reports periodically to the TCC and ITS committees as data become available.
<b>Crash Data</b>	MPO Crash Data Management System (CDMS)	Crash reports from law enforcement agencies are submitted through Florida Department of Highway Safety and Motor Vehicles.
<b>Transit data, including bus routes, on-time studies, ridership, etc.</b>	PSTA	PSTA updates maps as changes occur and reports are issued annually. Analysis is included in the biennial SOS Report.
<b>Truck routes</b>	MPO	The MPO coordinates routes with local governments and FDOT, and maintains Truck Route Maps.
<b>Red light camera installations/ locations</b>	Municipalities report to MPO	Red Light Camera Map on the MPO website is updated as installations occur.
<b>CSX Railroad Corridors and Stops</b>	CSX	Inventory of CSX lines is adjusted as changes occur.
<b>School-related transportation access and safety issues</b>	Pinellas County Schools	MPO’s School Transportation Safety Committee (STSC)
<b>Locations of fire stations and hospitals</b>	Pinellas County/local governments	Municipal governments and County submit updates as needed.
<b>Hurricane evacuation routes</b>	Pinellas County Emergency Management and Tampa Bay Regional Planning Council	Routes are designated and Evacuation Plans are adjusted annually as needed.
<b>Environmental Justice</b>	U.S. Census, MPO (under Federal law)	EJ supports protection from environmental hazard and discrimination due to race, national origin or income.
<b>Qualitative data</b>	MPO	Obtained through interagency coordination, committee input and public involvement activities.
<b>Transportation Demand Management, including school pool, vanpool, carpool, tele-commuting</b>	TBARTA	TBARTA operates the multi-county transportation demand management (TDM) Commuter Services program.

## Step Five: Analyze Congestion Problems and Needs

Before congestion management strategies can be identified, it is necessary to identify what the problems are, where are they located, and what is causing them (e.g., bottlenecks, incidents, work zones, weather, special events). The answers to what, where and why serve as the critical link between data collection and strategy identification. Federal regulations require that the CMP include *methods to monitor and evaluate the performance of the multi-modal transportation system and identify the causes of recurring and nonrecurring congestion* (23 CFR 450.320(c)1).

Annual monitoring efforts are used by the MPO to review LOS on the roadway network to identify recurring congestion. The MPO's Crash Data Management System (CDMS) is also used to identify corridors or intersections with a high frequency of crashes that result in non-recurring congestion. There are several issues taken into account when analyzing data for the purpose of defining or locating congestion problems:

- Locations of major trip generators
- Seasonal traffic variations
- Time-of-day traffic variations
- Work trips vs. non-work trips

Once the data has been translated to allow comparisons of the various levels of congestion in the county, the MPO will begin to apply the SWEEP scoring formula to the individual sections of the transportation system (i.e., certain corridors or roadway segments). The result will be the identification and priority ranking of a set of corridors defined as “congested” based on the scoring and performance measures, and it is these corridors where activities to address congestion are necessary and appropriate. Another result may be a ranking of corridors to determine which are in greater need of congestion relief.



In order to understand which congestion mitigation strategies are appropriate within the context of a specific congested corridor, it is also necessary to understand the causes of congestion, marking an appropriate point for comparison of recurring and non-recurring congestion issues.

Executing the Congestion Management Process requires a periodic review of data to *identify and prioritize* congested corridors, segments and/or hot spots that can benefit from CMP study and strategy implementation. A five-step review process *originated* by the Pinellas County MPO is described as a SWEEP analysis, as follows:

- ✓ **S**creen level of service, traffic count and duration of congestion data, freight “hot spot” data and other State of the System (SOS) Report data; data from FDOT, PSTA and other transportation partners; and local input to determine which segments may be experiencing severe congestion, based on roadway performance.
- ✓ **W**eigh road performance data and safety/crash data for selected facilities/corridors/segments to achieve a single, combined score (rank) for each, based on a 60:40 (congestion: crash) ratio.
- ✓ **E**valuate segments based on the highest combined raw scores and consideration of neighborhood and environmental impacts, economic development needs, and other local input.
- ✓ **E**liminate locations, with MPO advisory committee input, that do not meet established criteria or are already programmed in the TIP for improvement.
- ✓ **P**rioritize remaining locations for programming in the TIP or LRTP or for implementation by local governments.

The MPO’s SWEEP analysis provides the opportunity to identify, evaluate and prioritize congested corridors and locations throughout the County for not only inclusion in the CMP, but also the MPO’s TIP and LRTP. As described above, the congested roadways and intersections are identified based on local input, including a review of county and municipal roadway and intersection projects, freight hot spots, top crash locations, top congested SIS and non-SIS roadways, and the enhanced corridors recognized in the LRTP. Enhancements may include

bicycle and pedestrian features, intersection and safety improvements, or aesthetic improvements.

The locations identified are evaluated to determine the primary cause of congestion and the appropriate multi-modal mitigation strategies. Strategies include the introduction or expansion of transportation demand management programs, public transit improvement as well as bike/ped and trail improvements, access and incident management, and ITS investments. Planning level cost estimates are developed for the recommended CMP strategies or projects.

The evaluation step of the five-step SWEEP process includes the scoring of roadway facilities and segments. The SWEEP scoring formula is based on a 60:40 (congestion:crash) ratio. As shown below, the formula involves adding the congestion factor and the crash factor together to arrive at an overall score. The congestion portion of the 60:40 ratio is comprised of two measures: duration of congestion (DOC) and the volume-to-capacity ratio (V/C). The DOC and V/C ratio are multiplied to determine the congestion factor. In those instances when the subject corridor/facility is comprised of multiple segments, the highest DOC reading and V/C ratio within the overall corridor/facility is used. The crash factor is determined by calculating the average number of crashes in the subject corridor/facility over a three year period, and dividing that number by the length of the corridor. With regard to the crash portion of the 60:40 ratio, crash rates were substituted as performance measures for crash frequencies. In summary:

- Congestion Factor = DOC x V/C
- Crash Factor = Three year crash totals/3, then divide this number by the length of corridor
- Assuming Congestion Factor at 60% weight and Crash Factor at 40% weight:

$$\text{Congestion Factor} \times 1.6 \text{ plus } \text{Crash Factor} \times 1.4 = \text{Score}$$

Table 5 provides examples of the data used in the SWEEP process and the data sources.



**Table 5 – Types of Data Used in the SWEEP Analysis**

Criteria	Purpose	Data Used as Performance Measures	Data Sources
<b>Roadway Performance</b>	Identify where congestion is occurring, severity, and prognosis for future congestion level.	Duration of congestion (DOC); volume to capacity ratio (V/C); modeling data in the Level of Service (LOS) Report	MPO's Transportation Planning Inventory (TPI) Database; State of the System Report; Level of Service Report; Long Range Transportation Plan
<b>Safety</b>	Identify segments that have disproportionate number of crashes, resulting in nonrecurring congestion	Crash frequencies or rates (including types and causes of crashes); citations issued for safety related issues such as red light running, etc.	MPO's CDMS database; FDHSMV crash database; content analysis of various safety studies and reports, including the FDOT 5% high crash report; law enforcement data
<b>Funding Status</b>	Eliminate CMP candidates already scheduled for short term improvements; identify segments scheduled for resurfacing, which may provide opportunities for additional enhancements, e.g., bike lanes.	Funding and implementation schedules	Transportation Improvement Program (TIP) and Long Range Transportation Plan (LRTP), reports from local governments
<b>Value to the Community</b>	Ensure that candidate projects and strategies are compatible with local values and visions, (includes livable communities, Environmental Justice (EJ), economic development, historic landmarks and districts, etc.)	Content analysis of local plans and programs; surveys; qualitative data obtained through individual and committee (group) input	Local government comprehensive plans; LRTP policies; public involvement activities; MPO's Technical Coordinating Committee, Intelligent Transportation Committee and Citizens Advisory Committee; Community Traffic Safety Team; citizen comments
<b>Value to the Transportation System</b>	Assess the significance of impact on the entire transportation network, including the regional network.	Designation as an activity center, Strategic Intermodal System (SIS), regional road, truck route, evacuation route, economic development or tourism site, etc.	Goods Movement Study (Freight Hot Spots); Truck Route designations; Long Range Transportation Plan; State of the System Report; emergency plans; MPO technical committees input
<b>Multi-modal Availability</b>	Promote and support the use of alternative modalities.	Transit travel time and ridership data; sidewalk, crosswalk, trail and bicycle lane coverage and trail usage data	LRTP; State of the System Report; Transit Plans; MPO's TPI Database; Pinellas County database; National Transit Database; PSTA Database; Transit route maps; FDOT and local government reports
<b>Intelligent Transportation Systems</b>	Identify where ITS is already operational, where scheduled, and its effectiveness.	Tentative schedules for ITS implementation; ITS travel time reduction and other follow-up studies	State of the System Report; Transportation Improvement Program; periodic ITS Reports from FDOT and Pinellas County; ITS/ATMS Master Plan

## Step Six: Identify and Assess CMP Strategies

The identification and assessment of appropriate congestion mitigation strategies is a key component of the CMP. At this point in the process, the data and analysis is turned into a recommended set of strategies/ solutions to effectively manage congestion and achieve the CMP objectives. The Federal regulation states that the CMP shall include *“Identification and evaluation of the anticipated performance and expected benefits of congestion management strategies that will contribute to the more effective use and improved safety of the existing and future transportation system. Examples of strategies to consider include: demand management measures; traffic operational improvements; public transit improvements; Information Technology Services (ITS) technologies; and where necessary, additional system capacity.”* (23 CFR 450.320(c)4)

Community context and public involvement play an important role in determining the types of strategies that are appropriate for a specific corridor, roadway segment or intersection. Moreover, the MPO relies on the actions of its “partners” in implementing the strategies, including FDOT, PSTA and the local governments.

A wide range of congestion management strategies is available and can be broadly grouped into the following categories:

- ***Demand Management Strategies*** provide commuters with more options and reduce the numbers of vehicles or trips during congested periods. Strategies include programs that encourage transit use and ridesharing, and employers who permit telecommuting or flexible working hours.
- ***Traffic Operations Strategies*** focus on “getting more out of what we’ve got,” rather than building new infrastructure. Strategies include access management, optimizing traffic signal timing, restricting turns at key intersections, traffic calming, road diets, and traveler information or Intelligent Transportation Systems (ITS). Other operations strategies include improved management of work/construction zones, anticipating and

addressing special events, and FDOT’s Traffic Incident Management (TIM) teams, Rapid Incident Scene Clearance (RISC) program and Road Rangers.

- ***Public Transit Strategies*** largely involve improving transit operations, improving access to transit, and expanding transit service – all which make transit more attractive, leading to increased ridership and thereby reducing the number of vehicles on the roadway network. Public transit strategies include realigning the service schedules and stop locations, providing real-time arrival and departure information, enhanced amenities and safety/security, offering express routes and bus rapid transit (BRT), more frequent service and extended hours of operation, improved bike/ped facilities at transit hubs/stops, and expanding the bikes-on-buses program.
  
- ***Road Capacity Strategies*** involve adding more base capacity to the road network by adding new lanes, building new roads, or redesigning roadway segments or intersections where bottlenecks occur, in order to increase capacity. *It should be noted that management and operational strategies should be considered before additional capacity is considered.*

Table 6 summarizes the variety of congestion management strategies that can be considered. The MPO’s techniques for evaluating and ultimately selecting congestion mitigation strategies include collaboration with its “partners” (FDOT, PSTA, TBARTA and the local governments), and input from primarily two advisory committees: Technical Coordinating Committee (TCC) and the Intelligent Transportation Systems (ITS) Committee.

**Table 6 – Congestion Management Strategies**

<b>Demand Management Strategies</b>	<b>Transportation Demand Management</b>
	Alternative Work Hours/Flexible Hours
	Telecommuting
	Ridesharing/Vanpool/ School Pool
	Emergency Ride Home
	Safe Routes to School
	Park & Ride Lots
	<b>Public Transit Improvements</b>
	Reduced /Discounted Transit Fares
	Increased Route Coverage/Frequency
	Real-Time Data
	Premium Transit/BRT/Passenger Rail
	Exclusive Bus Right-of-Way
	New/Modern Vehicles
	Transit User Amenities
	Park & Ride
	<b>Bike/Pedestrian/Trail</b>
	New Sidewalk Connections/Crosswalks
	Bike Lanes, Paved Shoulders
	Multi-Use Trails
Improved Facilities at Major Attractors/Racks/Lockers	
Improved Safety & Security, Road Safety Audits	
Signing/Striping/Lighting	
<b>Land Use/Growth Management</b>	
Pedestrian and Transit Oriented Development Guidelines	
Mixed-Use Development	
Activity Centers/Overlay Districts	
Complete Streets/Livable Communities/Smart Growth Land Development Regulations	
<b>Operational Management Strategies</b>	<b>Access Management</b>
	Frontage Roads, Consolidated Driveways, Channelized Medians
	<b>Increases in Capacity</b>
	New Road Construction/New Lanes/Road Widening
	<b>Incident Management</b>
	Incident Detection and Management System
	Road Rangers
	Traffic Incident Management (TIM) Team
	RISC (Rapid Incident Scene Clearance) Program
	<b>ITS and Transportation Systems Management</b>
	Traffic Signal Coordination
	Red Light Camera Enforcement
	Intermodal Enhancements
	Dynamic Messaging/Improved Signage
	Transit Signal Priority
Intersection Improvements	
511 Traveler Info	
Road Signage/Pavement Marking	



## Step Seven: Program and Implement Strategies

Federal regulations require that the CMP include “*identification of an implementation schedule, implementation responsibilities, and possible funding sources for each strategy, or combination of strategies proposed for implementation*” (23 CFR 450.320 (c) 5).

The responsibility for programming and funding the operational and demand management strategies identified in Table 6 falls into the purview of federal, state, local and private entities. Because projects are most often implemented by agencies other than the MPO, e.g., various local governments, FDOT or PSTA, oversight by the MPO staff and input from the MPO’s advisory committees help ensure the sharing of information and the coordination requisite to measuring the effectiveness of the strategies being implemented. Such information and coordination is crucial to achieving the full realization of the CMP as a continuous process. The CMP does not over-ride or “trump” existing programming or funding responsibilities, but rather highlights those responsibilities already reflected in the long-range transportation plan (LRTP).

Some Florida MPOs exercise programming authority regarding the menu of operational and demand management strategies outlined in their CMP. For example, the METROPLAN Orlando MPO has set-aside funding for quick response operational improvements, while the Miami-Dade

*The LRTP identifies the long-term transportation system projects and improvements that are to be implemented to provide for the future mobility needs of Pinellas County residents, workers and visitors over the next 25 years. As funding becomes available, the projects become part of the adopted TIP, which contains the five-year schedule of work programs of FDOT, PSTA, Pinellas County and the municipalities. It should be noted, however, that some projects identified in the TIP, such as resurfacing, safety or operational projects, may not be specifically referenced within the LRTP due to their short-term nature.*

\*\*\*\*\*

*Improvements listed in the TIP are consistent with MPO priorities and the FDOT Work Program.*

MPO is expanding an earlier set-aside program to take a more comprehensive corridor-wide approach to funding congestion management improvements, and better integrating them with one another and the adopted LRTP improvements. The Pinellas MPO has a set-aside in the 2035 LRTP for CMP projects starting in 2015. That set aside however is not tied to specific projects. A comprehensive list of management and operations projects is under development. Once completed, the MPO with input from its advisory committees will be asked to establish an annual set-aside of federal funds to be used in combination with state and local funds to pay for CMP operations and management improvements.

*Pinellas County MPO CMP funding policy to be inserted -*

As noted previously, the CMP approach emphasizes the need to consider a broad menu of low cost improvements as primary strategies, reducing the need for, or the cost of, more expensive improvements wherever possible and practical. The agency that bears the cost of such projects is typically the State, County or local government responsible for maintaining the roadway.

Some CMP improvements may require relatively little expense to implement, such as adding signage or pavement markings, or trimming trees and hedges to improve visibility thus reduce crashes at certain intersections. Also, for bicycle or sidewalk enhancement projects, opportunities may become available at a reduced cost when initially included in a TIP resurfacing project. More costly capacity improvements, such as adding turn lanes, may require additional funding through the TIP and the LRTP.

The TIP selection process is a collaborative effort between the Pinellas County MPO, the Florida Department of Transportation (FDOT), Pinellas Suncoast Transit Authority (PSTA), Pinellas County Government, and the municipalities within the urbanized area. The existing TIP prioritization process includes consideration of the following:

- Concurrency management requirements *and levels of traffic congestion (emphasis added)* - Roads operating at peak hour level of service (LOS) E or F; roads with high volume to



capacity ratios or durations of congestion; and roads identified through the Congestion Management Process;

- Safety - Frequency and type of crashes that occur along a particular corridor or intersection, with particular emphasis on school access needs;
- Impact on Strategic Intermodal System (SIS) – Facility improvements necessary to improve the operations of the Strategic Intermodal System (SIS);
- Emergency evacuation – Improvements on roads needed to expedite the process of evacuating people in a hurricane event;
- Connectivity/consistency with Regional Long Range Transportation Plan;
- Intermodal access – Access to airports or seaports, transit terminals;
- Environmental impact – Impacts on the natural environment;
- Goods movement – Accommodation for freight-carrying vehicles, such as heavy trucks and cargo planes in the transportation of goods;
- Access to major trip generator or activity center – The extent to which a transportation improvement would improve access to destination points that attract a high level of traffic activity (e.g., airport, regional shopping center, major employment center, etc.).

## Step Eight: Evaluate the Effectiveness of Strategies

Federal regulations require that the CMP include “*Implementation of a process for periodic assessment of the effectiveness of implemented strategies. Results of this assessment shall be provided to decision-makers and the public to provide guidance on the selection of effective strategies for future implementation*” (23 CFR 450.320 (c) 6). Evaluation of strategy effectiveness can be seen as either a sequential step within the CMP process or as an on-going process. Findings that show improvement in congested conditions due to specific implemented strategies can be used to encourage further implementation of these strategies. Due to the fact that the Pinellas County MPO does not have sufficient resources to conduct many detailed studies to measure the effectiveness of particular congestion strategies or projects, and because “partner agencies” are primarily responsible for the implementation of CMP strategies, the most reasonable approach is to have the local project sponsors conduct the evaluations of their projects and programs, with MPO funding support when available. This information is shared with the MPO staff and then reported to the TCC and the ITS committees, and other advisory committees, as appropriate.

The biennial State of the System (SOS) Report serves as the foundation of the MPO’s CMP. The SOS Report provides a detailed assessment of countywide trends and conditions pertaining to roadway, transit, and bicycle/pedestrian/trail performance in Pinellas County. Arguably, the SOS Report tracks the effectiveness of the congestion mitigation strategies implemented by the various “partners” and the effectiveness of the multi-modal transportation system as a whole. Many of the CMP performance measures identified in Table 3 are tracked in the SOS Report:

- ✓ **Roadway Performance Measures**, including roadway traffic volume to capacity ratios, duration of congestion, and number/location of crashes.
  
- ✓ **Public Transit Performance Measures**, including passenger trips per revenue hour, average peak service frequency, on-time performance, and annual ridership.

- ✓ **Bicycle/Pedestrian/Trail Facility Performance Measures**, including increase in the percent coverage of bike lanes and sidewalks; and increase in the miles of multi-use trails.
- ✓ **TDM Performance Measures**, including the number of carpools, vanpools and school pools, and the participation rates.

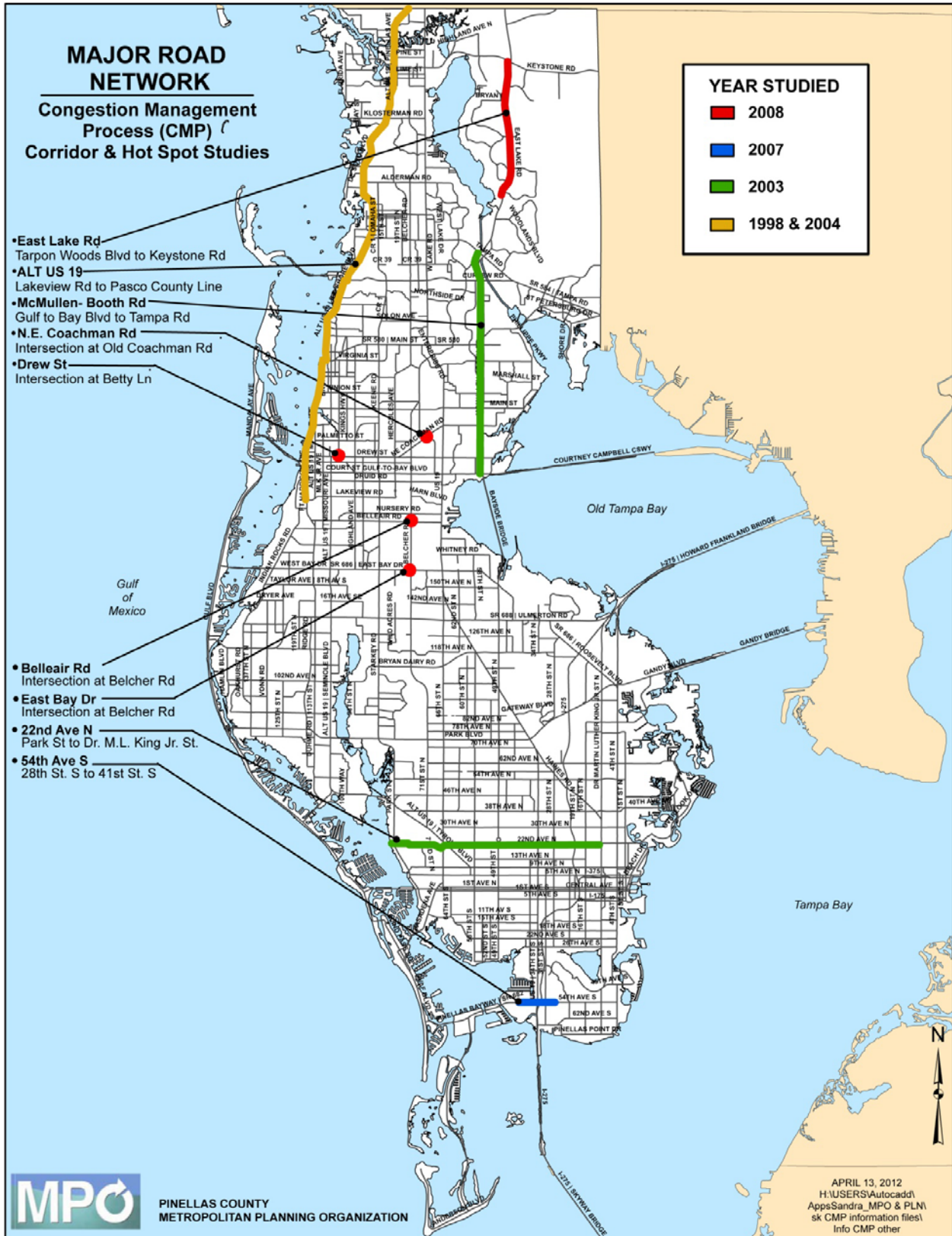
The MPO prepares a strategy tracking report that is updated periodically and presented to the ITS and TCC advisory committees as changes occur. (See Appendix for Status Report: Implementation of Identified Strategies – CMP Corridor and Hot Spot Studies.)

## **MAPS**

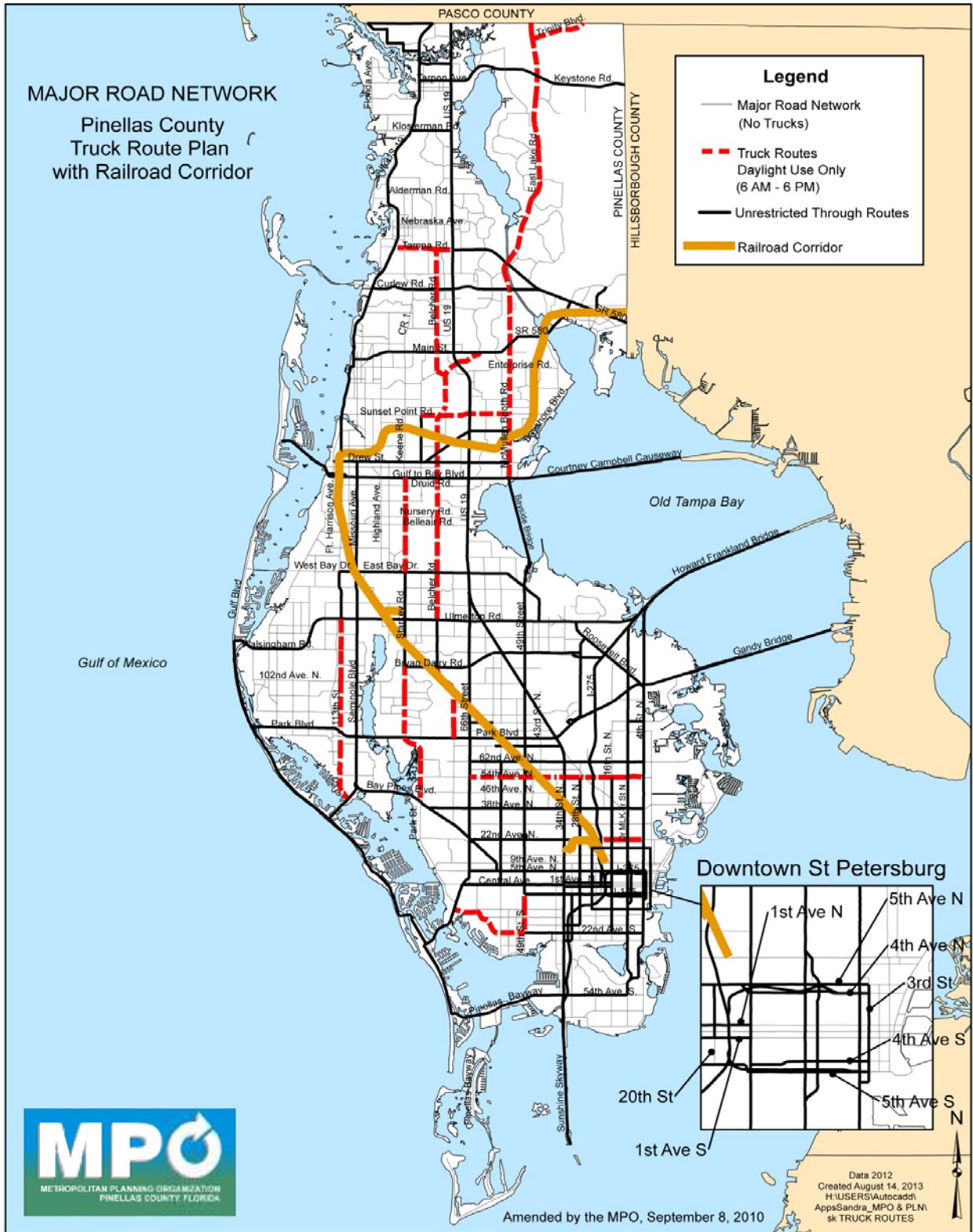
- Major Road Network: Congestion Management Process (CMP) Corridor and Hot Spot Studies
- Major Road Network: Pinellas County Truck Route Plan with Railroad Corridor
- Pinellas County Adjusted Urban Boundary
- Major Road Network by Jurisdiction
- Intelligent Transportation Systems (ITS) Corridors
- PSTA Transit Development Plan FY 2012 – FY 2021
- Pinellas County Trailways Plan
- CCC Regional Road Map

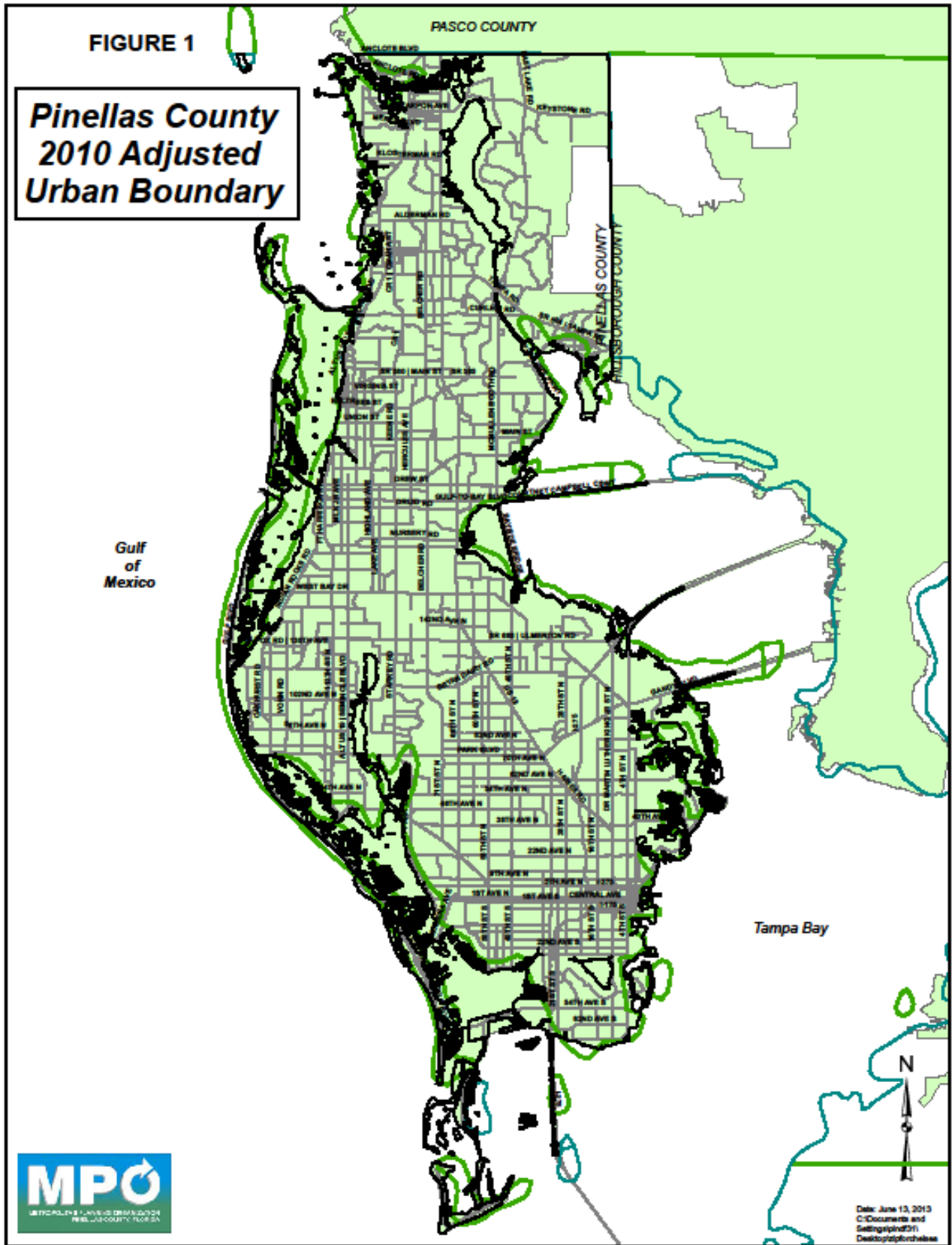
## **Regulations/Reports**

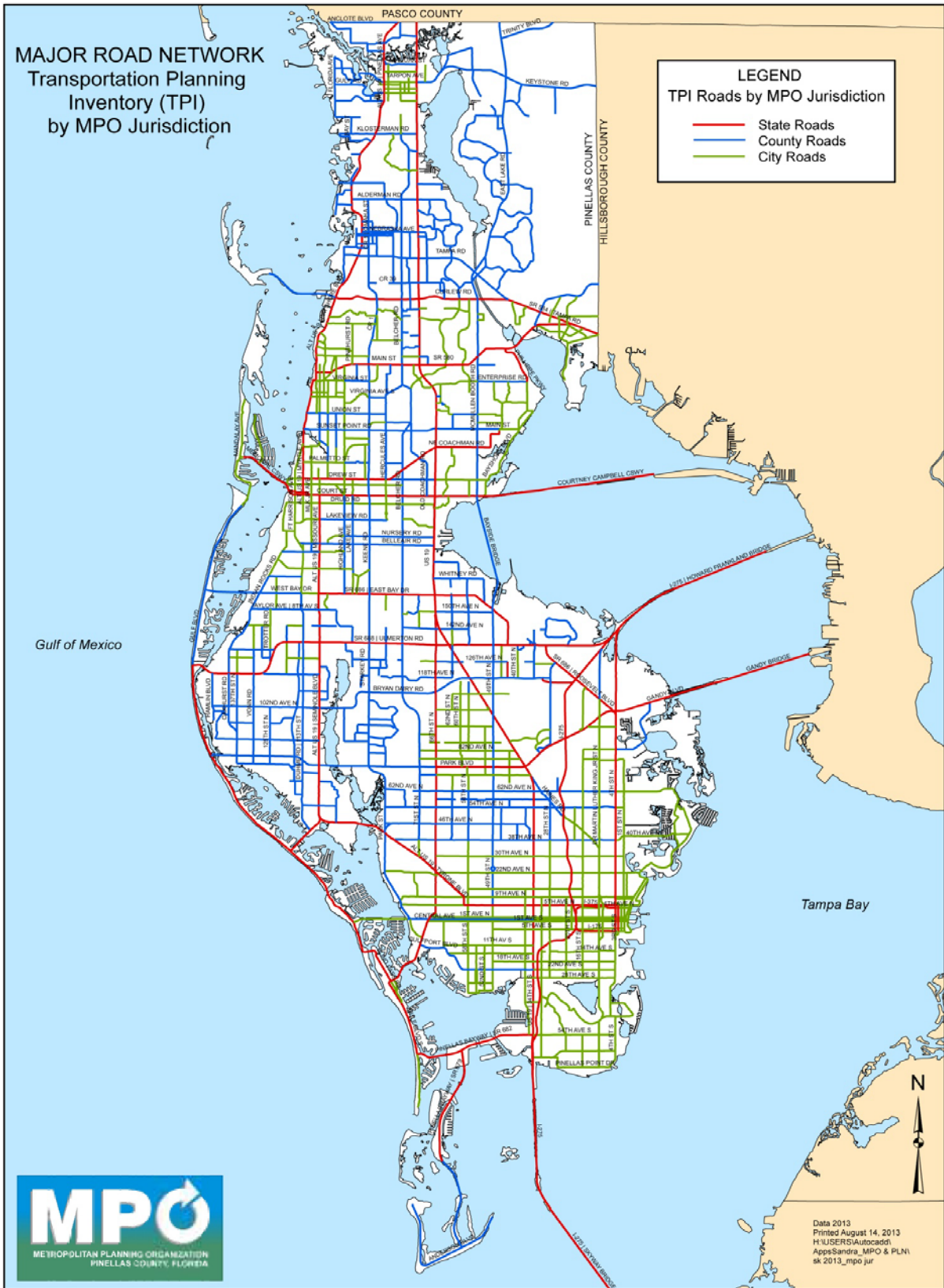
- Title 23, Section 450.320 of the U.S. Code of Federal Regulations: Congestion Management Process in Transportation Management Areas
- Status Report: Implementation of Identified Strategies – CMP Corridor and Hot Spot Studies
- Unfunded CMP Projects from 2013/14 to 2017/18 TIP



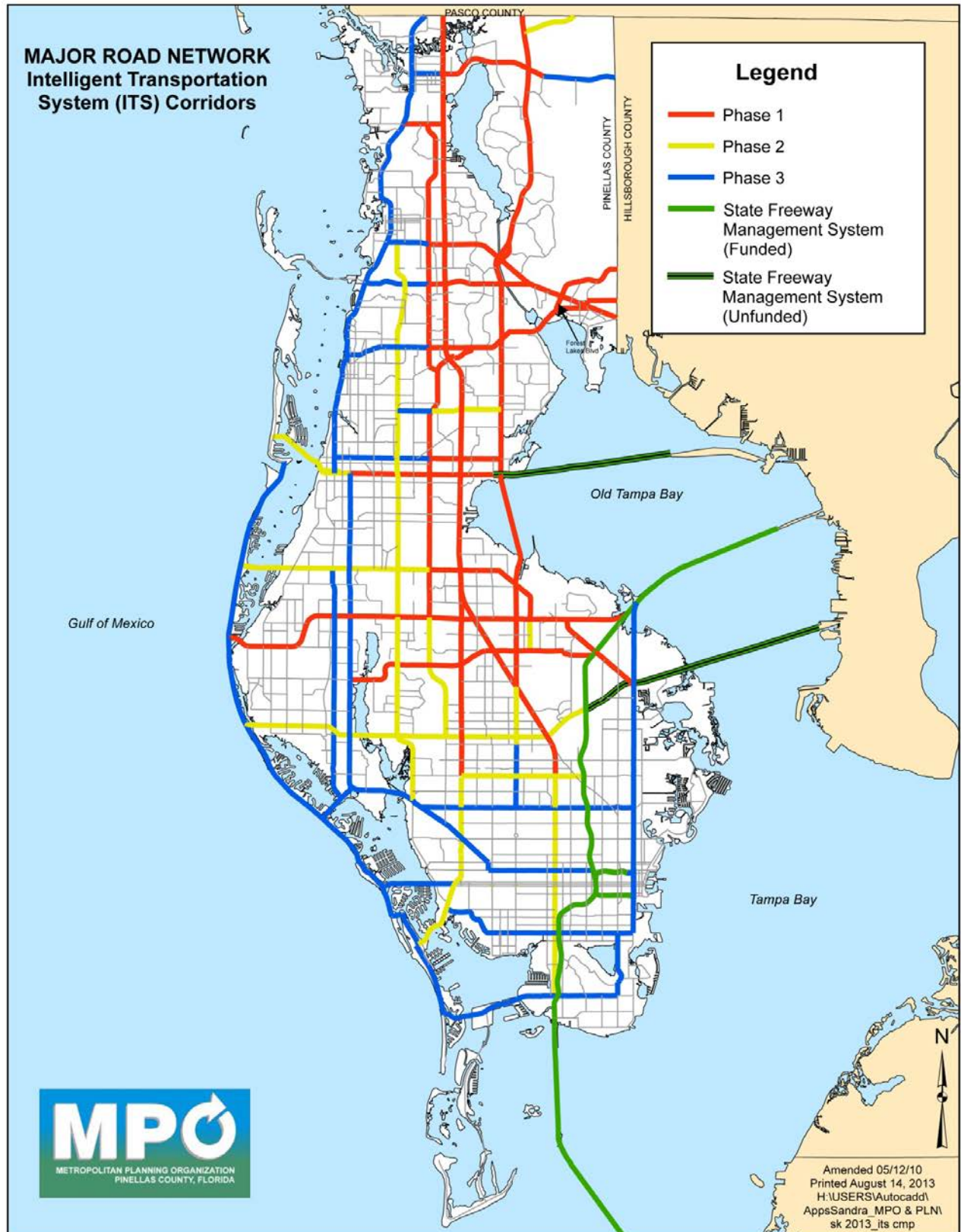
PINELLAS COUNTY  
METROPOLITAN PLANNING ORGANIZATION

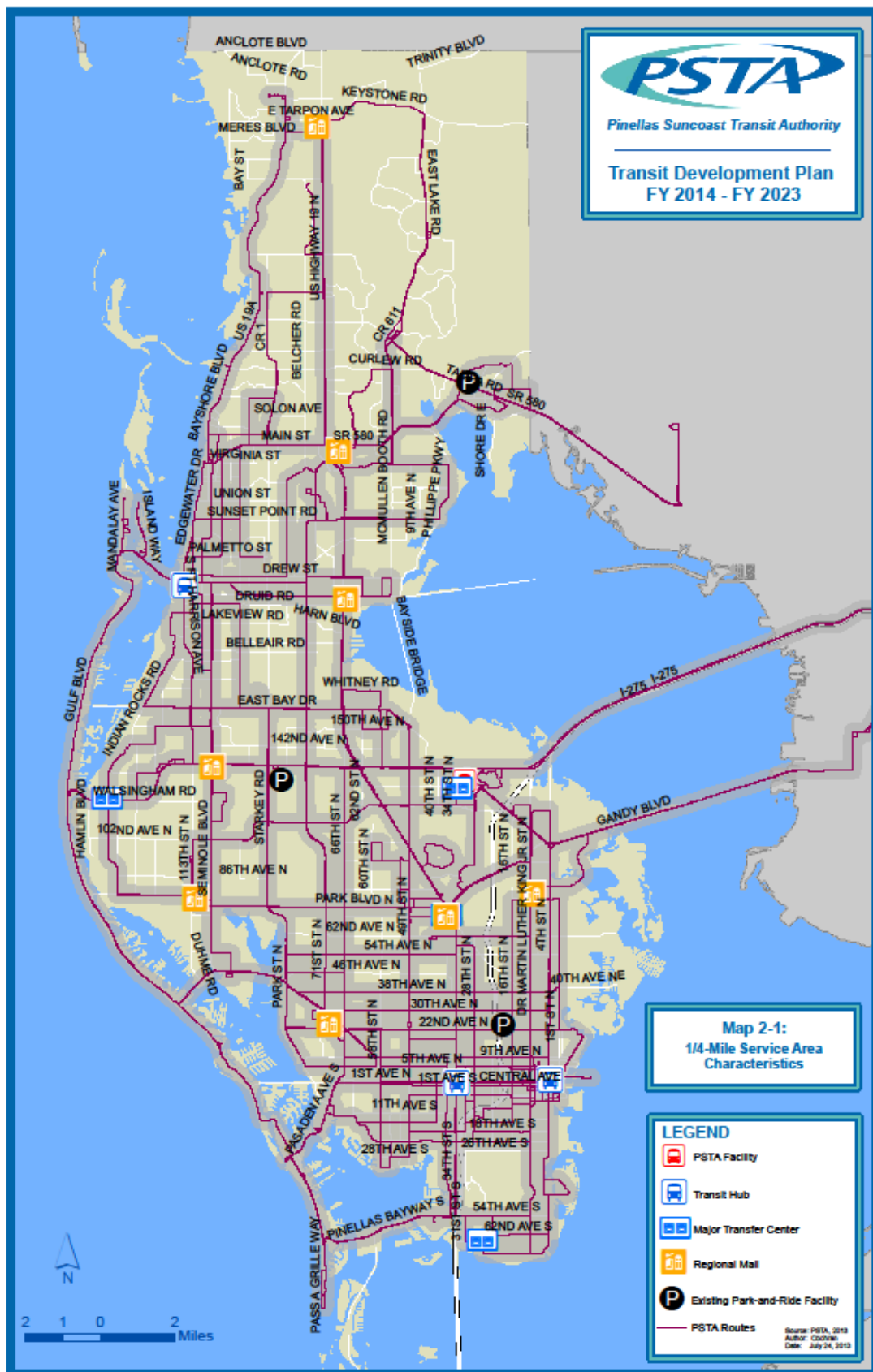




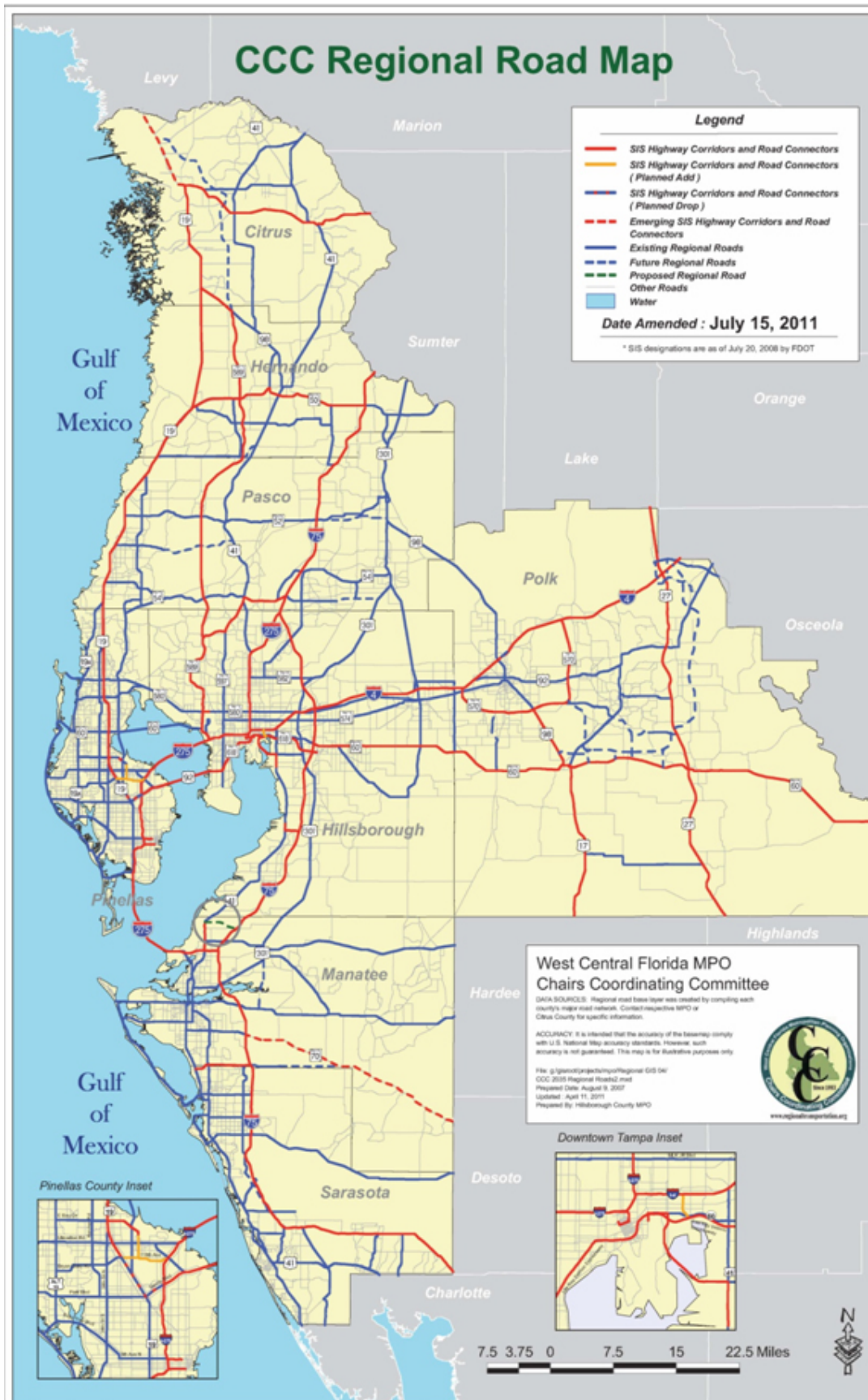












## Title 23, Section 450.320 of the U.S. Code of Federal Regulations: Congestion Management Process in Transportation Management Areas

- a. The transportation planning process in a TMA shall address congestion management through a process that provides for safe and effective integrated management and operation of the multi-modal transportation system.
  - Cooperatively developed and implemented
  - Travel reduction strategies
  - Operational management strategies
- b. The CMP should result in multi-modal system performance measures and strategies.
  - Acceptable levels of service may vary from area to area
  - Consider strategies that:
    - I. Manage demand
    - II. Reduce single occupant vehicle travel
    - III. Improve transportation system management and operations
  - Where general purpose lanes are determined to be appropriate, must give explicit consideration to features that facilitate future demand management strategies.
- c. The CMP shall be developed, established, and implemented in coordination with Transportation Systems Management (TSM) and operations activities. The CMP shall include:
  - Methods to monitor and evaluate the performance of the multi-modal transportation system
    - I. Identify the causes of congestion
    - II. Identify and evaluate alternative strategies
    - III. Provide information supporting the implementation of actions
  - Definitions of congestion management objectives and appropriate performance measures to assess the extent of congestion and support the evaluation of the effectiveness of strategies. Performance measures should be tailored to the specific needs of an area. Establishment of a coordinated program for data collection and system performance monitoring to define the extent and duration of congestion. To the extent possible, this program should be coordinated with existing sources.
  - Identification and evaluation of the anticipated performance and expected benefits of congestion management strategies that will contribute to the more effective use and improved safety of the existing and future transportation system. Examples of strategies to consider include:
    - I. Demand management measures, including growth management and congestion pricing
    - II. Traffic operational improvements
    - III. Public Transit improvements
    - IV. Information Technology Services (ITS) technologies
    - V. Where necessary, additional system capacity
  - Identification of an implementation schedule, implementation responsibilities, and possible funding sources for each strategy.

- Implementation of a process for periodic assessment of the effectiveness of implemented strategies. Results of this assessment shall be provided to decision makers and the public to provide guidance on the selection of effective strategies for future implementation.
  - TMAs designated nonattainment for ozone or carbon monoxide may not program federal funds for any project that will result in a significant increase in the carrying capacity of Single Occupant Vehicles (SOVs), with the exception of safety improvements or the elimination of bottlenecks (within the limits of the appropriate projects that can be implemented).
- d. In TMAs designated nonattainment for ozone or carbon monoxide, the CMP shall provide an appropriate analysis of reasonable (including multi-modal) travel demand reduction and operational management strategies for a corridor in which a project with a significant increase in SOV capacity is proposed to move forward with federal funds.
- e. State laws, rules, and regulations pertaining to congestion management systems or programs may constitute the congestion management process, if FHWA and Federal Transit Administration (FTA) find that these are consistent with the intent of this process.

**Status Report: Implementation of Identified Strategies - CMP Corridor and Hot Spot Studies**

Study Limits	Study Performed	Updates/Notes	Remaining
<p><b>ALT US HWY 19</b> Lakeview RD to Pasco County Line</p>	<p>Oct. 1998 Updated Mar. 2004</p>	<ul style="list-style-type: none"> <li>• Alignment changed in Clearwater and Largo in 2007.</li> <li>• TIP: Resurfacing from Whisper Lake RD to Harry ST CST 2012/2013</li> <li>• Tentative Work Program – Resurfacing from N of Curlew Rd to N of Whisper Lake PE 2012/2013, CST 2014/2015</li> <li>• Gulf Beach Trolley service to Dunedin, Tarpon Springs, etc begun in 2010.</li> <li>• 2035 LRTP: Forecasts severe congestion; transit enhancements have been implemented and segment has been identified for premium transit routes between beaches, Largo, St. Petersburg, Tampa and US HWY 19; roadway enhancements between Anclote BLVD and Live Oak ST and between Klosterman RD and Brevard ST.</li> <li>• Phase III ITS</li> <li>• Truck route (unrestricted)</li> </ul>	<p>Previously identified improvements mostly completed, with the exception of southbound right turn lane at Dodecanese BLVD and southbound left turn lane at Curlew PL. Recent request to look at pedestrian safety and transit access at Florida AVE and Alt 19 in Palm Harbor.</p>
<p><b>22nd AVE N</b> Park ST to Dr. M.L. King Jr. ST</p>	<p>Oct 2003</p>	<ul style="list-style-type: none"> <li>• Implemented at Dr. Martin Luther King, 16<sup>th</sup> ST and 28<sup>th</sup> ST - pedestrian signal heads at trail crossings. Solar powered crossing equipment installed at Pinellas Trail.</li> <li>• Implemented at I-275 - mast arms on both sides with backplate.</li> <li>• Truck route (unrestricted)</li> <li>• Bike lane system expanded in area.</li> <li>• FDOT performed extensive study in summer, 2012</li> </ul>	<p>Provide additional eastbound left turn lane to the northbound on-ramp at I-275. FDOT will complete an Interchange Operations Analysis.</p>
<p><b>54th AVE S</b> 28th ST S to 41st ST S</p>	<p>Mar 2007</p>	<ul style="list-style-type: none"> <li>• Implemented signalization improvements.</li> <li>• Phase III ITS.</li> <li>• Bicycle lanes between 34<sup>th</sup> ST to east of 41<sup>st</sup> ST underway</li> <li>• Truck route (unrestricted)</li> </ul>	<p>Add exclusive eastbound right-turn lane at 31<sup>st</sup>ST, extend westbound lane and modify it to a shared through/right turn lane. At 34<sup>th</sup> ST, modify the southbound approach to two exclusive left-turn lanes, one through lane and one right turn lane.</p>
<p><b>McMullen-Booth RD</b> Gulf-to-Bay BLVD to Tampa RD</p>	<p>Jul 2003</p>	<ul style="list-style-type: none"> <li>• ITS in 2009.</li> <li>• Identified in 2035 LRTP for premium bus</li> <li>• Truck route (daylight)</li> <li>• Safety study of signal at Briar Creek RD complete</li> <li>• Intersection improvements Drew ST, complete</li> <li>• Modifications for Enterprise Rd complete,</li> </ul>	<p>County on-road bike lanes due to be completed by 2012 end.</p>
<p><b>East Lake RD</b> Tarpon Woods BLVD to Keystone RD</p>	<p>Sept 2008</p>	<ul style="list-style-type: none"> <li>• 2035 LRTP: Forecasted for significant congestion; premium bus lines.</li> <li>• Safety Audit - Tampa RD to Trinity RD 2009 (Identified Keystone RD as highest crash. Overall, highest type was rear-end.)</li> <li>• ITS 2009.</li> <li>• Keystone RD widening underway</li> <li>• Truck route (daylight).</li> <li>• Safety related improvements at Tarpon Woods intersection.</li> </ul>	<p>2008 CMP study recommended access management, but 2009 Safety Audit Report concluded that access was “very good,” with a minimum of openings. Implementation of recommendations drawn from 2009 safety audit to be identified by County.</p>

<p><b>Belleair RD</b> Intersection Belcher RD</p>	<p>at Sept 2008</p>	<ul style="list-style-type: none"> <li>• Interchange at US HWY 19 under construction;</li> <li>• 2035 LRTP: intersection improvements and other enhancements planned for Belleair RD; planned Progress Energy Trail extension (Belleair RD to Ulmerton RD).</li> <li>• In Tentative Work Program intersection improvements PE 2012/2013; CST 2014/2015</li> <li>• Eagle Lake Park opened 2010 at Keene RD, making this intersection link between trail and park.</li> <li>• CIP: County will do intersection improvements.</li> </ul>	<p>Bicycle and pedestrian safety improvements and signalization.</p>
<p><b>East Bay DR</b> Intersection Belcher RD</p>	<p>at Sept 2008</p>	<ul style="list-style-type: none"> <li>• ITS 2010-2011.</li> <li>• 2035 LRTP: Forecasted for significant congestion; identified for premium bus network, including to Downtown Tampa.</li> <li>• Truck route (unrestricted)</li> <li>• County performed safety study in 2011</li> </ul>	<p>Pedestrian refuge and other safety improvements. ITS due to be operational in 2012 or slightly later. County performed a road safety audit on July 2011.</p>
<p><b>N.E. Coachman RD</b> Intersection at Old Coachman RD</p>	<p>Sept 2008</p>	<ul style="list-style-type: none"> <li>• Upgraded signal and pavement 2009.</li> <li>• Progress Energy Trail expansion to US HWY 19 at Enterprise RD</li> <li>• Truck route (unrestricted)</li> <li>• Super Walmart provided some modifications to turn lanes west of US Hwy 19.</li> <li>• Tentative Work Program - Resurfacing PE 2012-2013, CST 2014-2015 may provide opportunities</li> </ul>	<p>Intersection improvements including left turn lane, protected turn signal and bicycle facility/safety improvements. (Needs to be included in LRTP prior to FDOT consideration for PD&amp;E study.)</p>
<p><b>Drew ST.</b> Intersection Betty LA</p>	<p>at Sept 2008</p>	<ul style="list-style-type: none"> <li>• Bicycle lane in 2035 LRTP.</li> <li>• ITS Phase III</li> <li>• Truck route (unrestricted)</li> <li>• Resurfacing, Tentative Work Program from Alt US 19/Myrtle to Mariva Ave. may provide opportunity for improvement. CST 2012/2013</li> <li>• Drew St/Betty Lane lot is currently under residential development, but this will not address road improvements.</li> <li>• City permitted use of golf course property for left turn storage, but FDOT was not able to provide full funding for construction, and Clearwater did not have funds available for remainder.</li> <li>• Sidewalk improvements will be made to eliminate gap.</li> <li>• Alternative bike route was established at Cleveland Street. (Road is too narrow for bike lane at Drew St.) Clearwater was asked to consider signage to reroute bicyclists to Cleveland Street.</li> </ul>	<p>Provide left turn storage lane. Eliminate gaps in the sidewalk.</p>

Rev: February, 2013

File: Users/Trans/CMP & State of the System/CMP/Updates/CMP\_Tracking.docx



## Pinellas County MPO FY 2013/14 – 2017/18 TIP: Unfunded Congestion Management Process (CMP) Projects

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- Alt. 19 @ Dodecanese Blvd (Add a southbound right-turn lane)
- Alt. 19 @ Curlew Place (Add a southbound left-turn lane)
- McMullen Booth @ SR 60 (Add a northbound right-turn lane)
- McMullen Booth @ Sunset Point Rd (Add a northbound right-turn lane)
- McMullen Booth @ Curlew Rd (To be determined)
- 54<sup>th</sup> Ave S @ 31<sup>st</sup> St (Add an exclusive eastbound right-turn lane; Extend the westbound right-turn lane and modify it to a shared through/right-turn lane)
- 54<sup>th</sup> Ave S @ 34<sup>th</sup> St (Modify the southbound approach to two exclusive left-turn lanes, one through lane, and one right-turn lane; Eastbound and westbound approach improvements)
- Belleair Rd @ Belcher Rd (Near term - Signalization/signing improvement; Bicycle facility improvement)
- East Bay Dr @ Belcher Rd (Near term - pedestrian refuge area and other safety improvements; Long term – Transportation demand management and access management strategies)
- East Lake Rd from Tarpon Woods Blvd to Keystone Rd (Near term - Access management; Long Term - Implement transportation demand management strategies)
- NE Coachman Rd @ Old Coachman Rd (Long Term – Add left-turn lanes, protected left turn signal and bicycle facility improvement)
- Drew St @ Betty Lane (Long term - Provide exclusive left turn storage lanes pending redevelopment)

*Note: List includes the FDOT five (5) percent High Crash Locations on Local Roads.*

RECOMMENDATION TO REROUTE THE DESIGNATED TRUCK ROUTE IN TARPON SPRINGS

The Pinellas County MPO maintains a Truck Route Plan to identify roadways where heavy trucks must travel and routes that have time-of-day restrictions. The MPO works with each municipality and the County to identify roadways appropriate for through-truck movements and to develop ordinances regulating truck traffic, in an effort to maintain consistency across jurisdictional boundaries. In Pinellas County, trucks are required to use designated truck routes up to the point closest to their destination.

The City of Tarpon Springs is proposing an amendment to the Truck Route Plan that would remove the unrestricted truck route designation from the western section of Tarpon Avenue between Alternate U.S. Highway 19 and Ring Avenue. The proposal would redirect the truck traffic to the south by adding Ring Avenue from Tarpon Avenue to Lemon Street and Lemon Street from Ring Avenue to Alternate U.S. Highway 19.

In 2011, Tarpon Avenue was transferred to the City from the jurisdiction of the State of Florida in order for the City to implement a planned roadway drainage mitigation project, complete downtown redevelopment initiatives and facilitate other economic development opportunities. The intersection of Tarpon Avenue and Alternative U.S. Highway 19 has a very tight turning radii, with a building located directly on the northeast corner of the intersection, with very little setback. This poses a significant constraint to truck traffic that is relieved with this rerouting.

The City has already implemented the truck route signage on the proposed route and since it was been well received, are requesting the route be reflected in the countywide Truck Route Plan.

ATTACHMENTS: [Map of Tarpon Avenue and surrounding area](#)

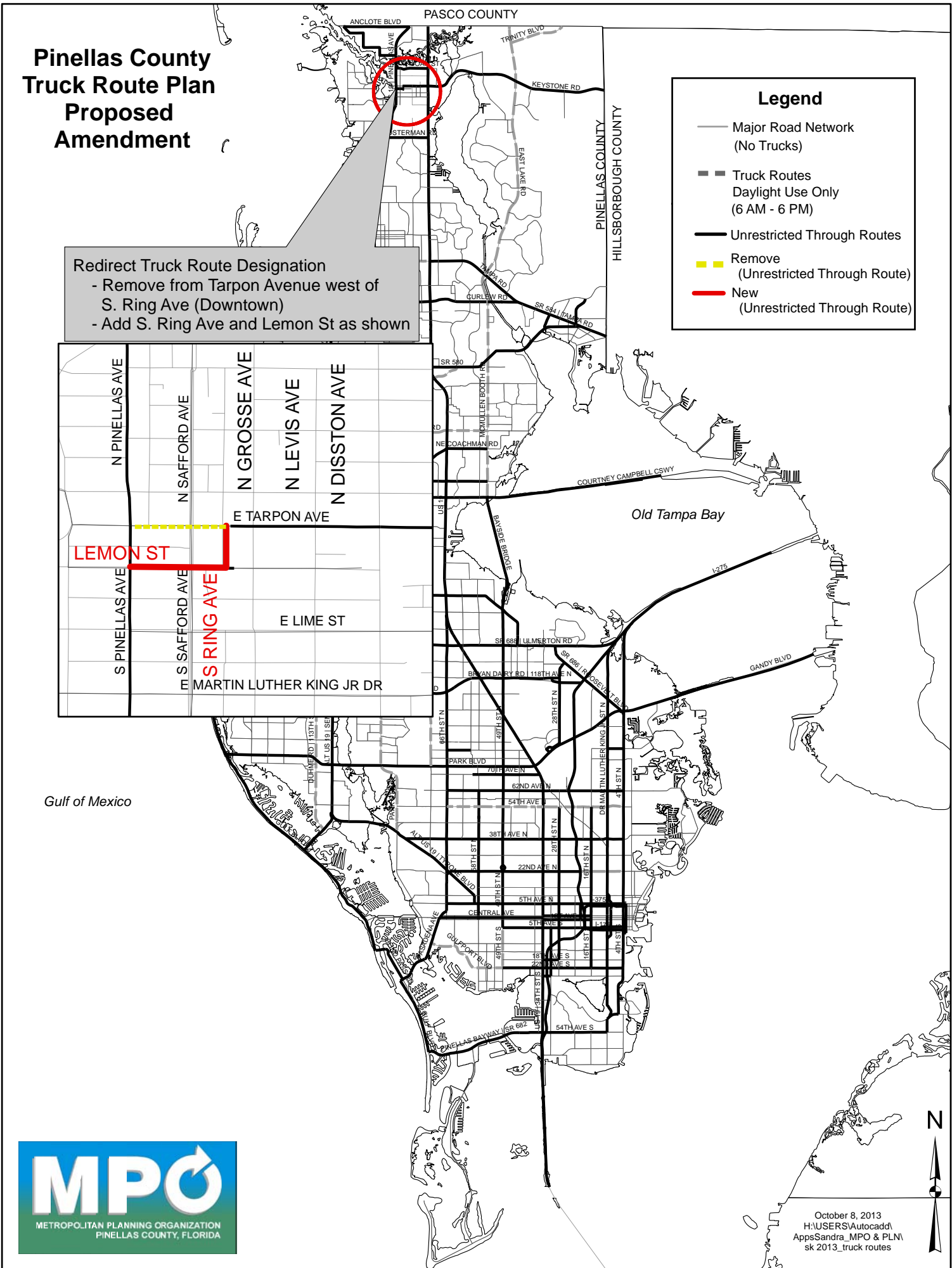
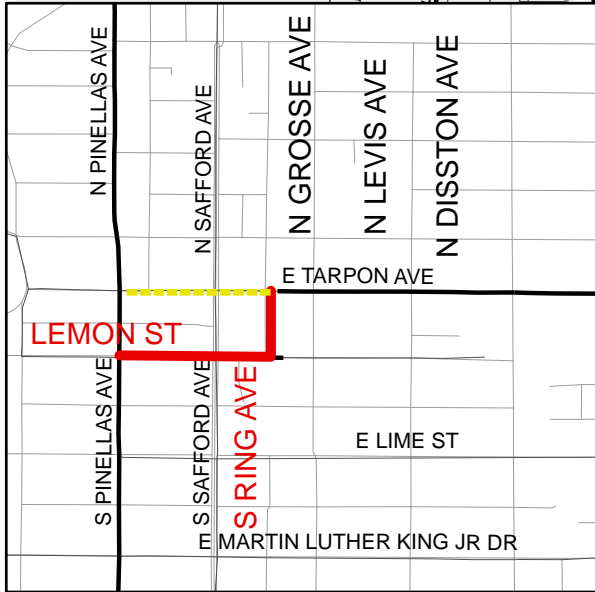
ACTION: TCC to make a recommendation to MPO regarding the rerouting of the truck route in the Tarpon Avenue Area

# Pinellas County Truck Route Plan Proposed Amendment

**Redirect Truck Route Designation**  
 - Remove from Tarpon Avenue west of S. Ring Ave (Downtown)  
 - Add S. Ring Ave and Lemon St as shown

### Legend

- Major Road Network (No Trucks)
- Truck Routes Daylight Use Only (6 AM - 6 PM)
- Unrestricted Through Routes
- Remove (Unrestricted Through Route)
- New (Unrestricted Through Route)



**FALL UPDATE OF FY 2013/14 THROUGH FY 2017/18 TRANSPORTATION IMPROVEMENT PROGRAM (TIP)**

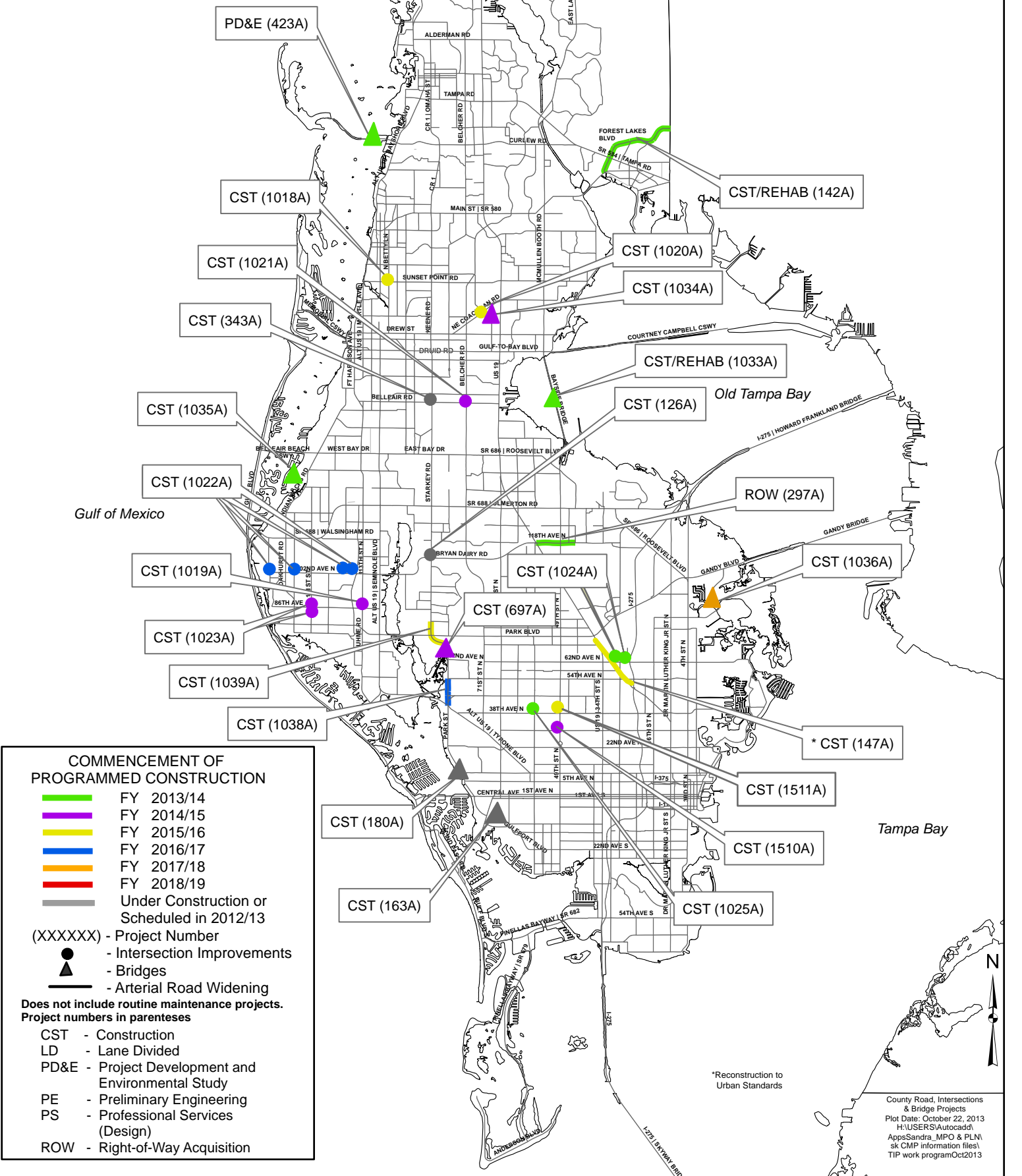
Each year, the Metropolitan Planning Organization (MPO) updates the Transportation Improvement Program (TIP) to incorporate changes in the County and Municipal Work Programs. The TIP contains project descriptions, schedules, and corresponding funding allocations for the 25 local governments of Pinellas County, the Pinellas Suncoast Transit Authority (PSTA), local airports, and the Port of St. Petersburg. The projects include new construction, reconstruction, capital purchases, and maintenance work associated with roads, sidewalks, trails, transit services, airports, the Port of St. Petersburg, and the Transportation Disadvantaged Program. The TIP also identifies the MPO's priority projects for the Transportation Alternatives (TA) Program and Surface Transportation Program funding, as well as those identified through the Congestion Management Process. These priority projects are required by law to be included in the TIP in order to receive state and federal funding.

The annual fall update incorporates the new locally adopted Pinellas County and municipal transportation work programs into the TIP. The new municipal and county work program tables are attached, along with corresponding project maps. Also attached are summary tables intended to provide an abbreviated report of Pinellas County transportation improvement projects. The summary tables include information on the status of the projects and any changes that have occurred from the previous year Work Program. Shaded projects on the table indicate that changes occurred compared to the previous year Work Program. Projects not shaded are unchanged from the previous year. Pending approval by the MPO, the new work programs and accompanying maps will be incorporated into the FY 2013/14-2017/18 TIP.

ATTACHMENTS:        [Map of Pinellas County Road, Intersection, and Bridge Improvements](#)  
[Map of Pinellas County Intelligent Transportation System \(ITS\)/Advanced Transportation Management System \(ATMS\) and Trail Projects](#)  
[Summary Tables of the Pinellas County Work Program for Transportation Projects for FY 2013/14-2018/19](#)  
[Pinellas County Work Program for Transportation Projects for FY 2013/14-2018/19](#)  
[Work Program Table of the St. Petersburg-Clearwater International Airport](#)  
[Work Program Table of the Port of St. Petersburg](#)  
[Work Program Table of the Clearwater Airpark](#)  
[Work Program Table of the Albert Whitted Airport](#)  
[Map of Municipal Work Program Projects](#)  
[Municipal Work Program Tables](#)

ACTION:                TCC to recommend approval of the Fall Update of the TIP

# PINELLAS COUNTY 6 YEAR WORK PROGRAM: ROAD, INTERSECTION & BRIDGE IMPROVEMENTS FY 2013/14 - FY 2018/19



**COMMENCEMENT OF PROGRAMMED CONSTRUCTION**

- █ FY 2013/14
- █ FY 2014/15
- █ FY 2015/16
- █ FY 2016/17
- █ FY 2017/18
- █ FY 2018/19
- █ Under Construction or Scheduled in 2012/13

(XXXXXX) - Project Number

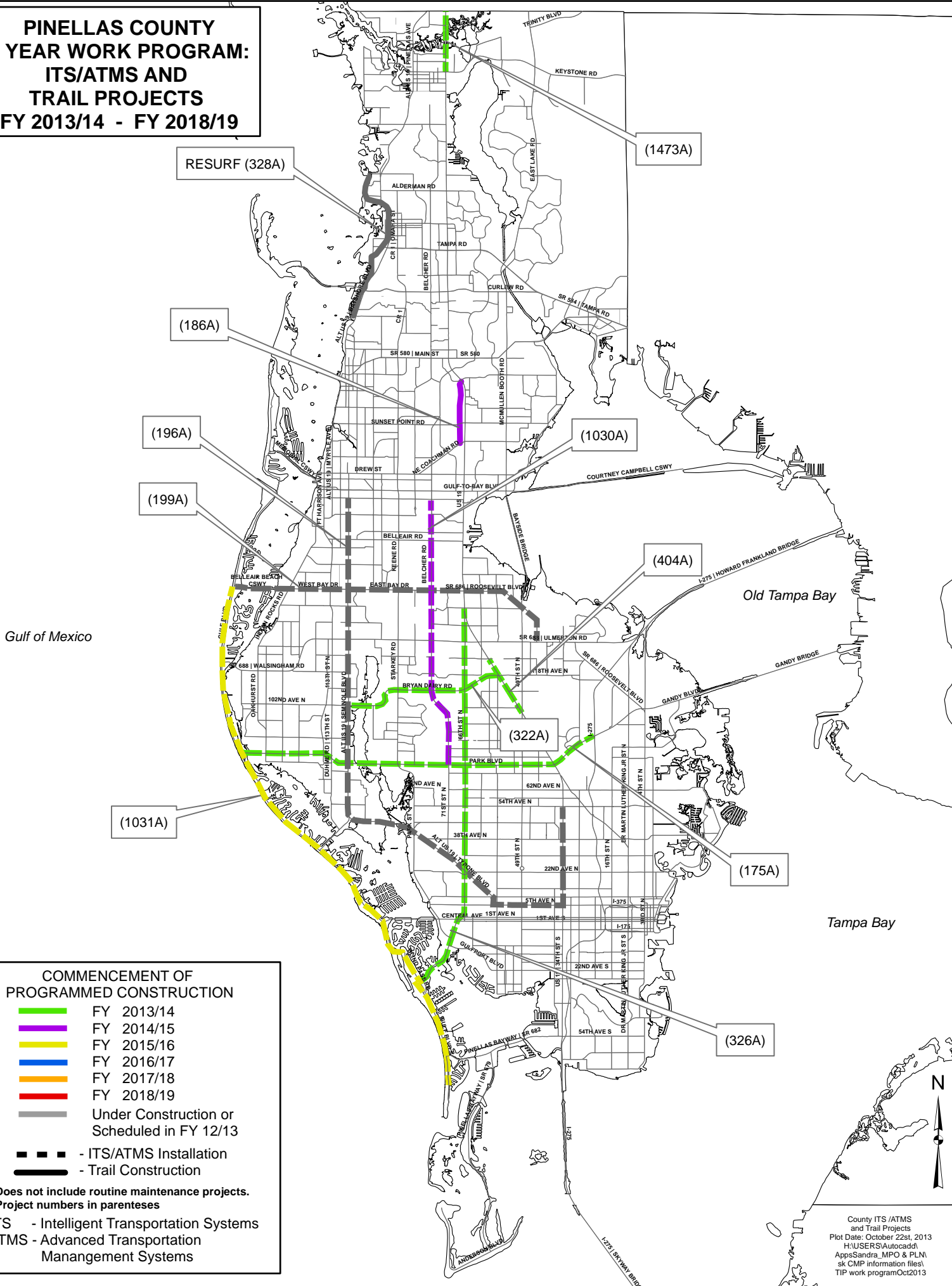
- - Intersection Improvements
- ▲ - Bridges
- - Arterial Road Widening

Does not include routine maintenance projects.  
Project numbers in parentheses

- CST - Construction
- LD - Lane Divided
- PD&E - Project Development and Environmental Study
- PE - Preliminary Engineering
- PS - Professional Services (Design)
- ROW - Right-of-Way Acquisition

\*Reconstruction to Urban Standards

**PINELLAS COUNTY  
6 YEAR WORK PROGRAM:  
ITS/ATMS AND  
TRAIL PROJECTS  
FY 2013/14 - FY 2018/19**



**COMMENCEMENT OF  
PROGRAMMED CONSTRUCTION**

- FY 2013/14
- FY 2014/15
- FY 2015/16
- FY 2016/17
- FY 2017/18
- FY 2018/19
- Under Construction or Scheduled in FY 12/13
- ITS/ATMS Installation
- Trail Construction

Does not include routine maintenance projects.  
Project numbers in parentheses  
ITS - Intelligent Transportation Systems  
ATMS - Advanced Transportation Management Systems

**SUMMARY TABLE OF MAJOR ROAD PROJECTS IN THE  
FY 2013/14 – 2018/19 PINELLAS COUNTY CAPITAL IMPROVEMENT PROGRAM**

PROJECT NUMBER	LOCATION	PROJECT DESCRIPTION	STATUS
142A	Forest Lakes Blvd from 580 to Pinellas/Hillsborough County Line	Resurfacing	CST deferred from 2012/13 to 2013/14
297A	118 <sup>th</sup> Ave Expressway (Future SR 690) from US 19 (SR 55) to East of Roosevelt/CR 296	New Bridge Construction	ROW 2013/14
126A	Bryan Dairy Rd at Starkey Rd	Intersection Improvements	CST 2012/13
147A	Haines Rd from 54 <sup>th</sup> Ave to 28 <sup>th</sup> St	Intersection Improvements	CST 2015/16
343A	Belleair Rd at Keene Rd	Intersection Improvements	CST 2012/13
1018A	Betty Lane at Sunset Point Road	Intersection Improvements	CST 2014/15
1019A	113 <sup>th</sup> St N at 86 <sup>th</sup> Ave N	Intersection Improvements	CST 2014/15
1020A	N.E. Coachman Rd at Coachman Rd	Intersection Improvements	CST Advanced from 2016/17 to 2015/16
1021A	Belcher Rd at Belleair Rd	Intersection Improvements	CST 2014/15
1022A	102 <sup>nd</sup> Ave N at Hamlin Blvd, Antilles Dr, 118 <sup>th</sup> St N, and 119 <sup>th</sup> St N	Intersection Improvements	CST 2016/17
1023A	131 <sup>st</sup> St N at 82 <sup>nd</sup> Ave N and 86 <sup>th</sup> Ave N	Intersection Improvements	CST 2014/15
1024A	62 <sup>nd</sup> Ave N at 25 <sup>th</sup> St N and 28 <sup>th</sup> St N	Intersection Improvements	CST 2013/14
1025A	38 <sup>th</sup> Ave N at 58 <sup>th</sup> St N	Intersection Improvements	CST Advanced from 2015/16 to 2013/14
1510A	30 <sup>th</sup> Ave N at 49 <sup>th</sup> St N	Intersection Improvements	CST added 2014/15
1511A	38 <sup>th</sup> Ave N at 49 <sup>th</sup> St N	Intersection Improvements	CST 2015/16
1038A	Park St from Tyrone Blvd to 54 <sup>th</sup> Ave N	Intersection Improvements	CST 2016/17
1039A	Park St/Starkey Rd from 84 <sup>th</sup> Ln N to Flamevine Ave	Reconstruct	CST Deferred from 2014/15 to 2015/16
*109A	Beckett Bridge PD&E	Bridge Replacement PD&E Study	PD&E 2012/13
163A	LaPlaza Ave Bridge	Reconstruct	CST 2012/13
180A	Park St Bridge	Bridge Replacement	CST 2012/13
423A	Dunedin Causeway Bridge	Bridge Replacement PD&E Study	PD&E Advanced from 2015/16 to 2013/14
697A	Park Street N Bridge Over Cross Bayou Canal	Bridge Widening	CST added 2014/15

**SUMMARY TABLE OF MAJOR ROAD PROJECTS IN THE  
FY 2013/14 – 2018/19 PINELLAS COUNTY CAPITAL IMPROVEMENT PROGRAM**

PROJECT NUMBER	LOCATION	PROJECT DESCRIPTION	STATUS
700A	Westwinds Dr Bridge over Westwind Canal	Bridge Replacement	CST added 2016/17
702A	Crosswinds Dr Bridge over Crosswinds Canal	Bridge Replacement	CST added 2014/15
*971A	Sands Point Dr Bridge between Pinellas Bayway S and 3 <sup>rd</sup> Ave S	Bridge Replacement	Project moved out of work program due to low priority with respect to other bridge projects in the Bridge Rehabilitation Program
1033A	Bayside Bridge	Reconstruct	CST Deferred from 2012/13 to 2013/14
1034A	Old Coachman Rd over Alligator Creek	Bridge Replacement	CST 2014/15
1035A	Oakwood Dr over Stephanie's Channel	Bridge Replacement	CST 2013/14
1036A	San Martin Blvd over Riviera Bay	Bridge Replacement	CST Deferred from 2016/17 to 2017/18
1037A	Beckett Bridge	Bridge Replacement	CST 2018/19

\* Project is not mapped on Pinellas County 6 Year Work Program maps

Notes:

- 1) CST = Construction; ROW = Right-of-Way; PD&E = Project Development & Environment Study; ITS = Intelligent Transportation Systems;
- 2) Shaded projects indicate changes in the FY 2013/14 – 2018/19 Pinellas County Capital Improvement Program (CIP) compared to the adopted FY 2012/13 – 2017/18 CIP. The status column summarizes the changes.
- 3) This project summary table does not include projects such as landscaping, airport improvements, drainage work, railroad crossings and routine maintenance and repairs.



**SUMMARY TABLE OF  
INTELLIGENT TRANSPORTATION SYSTEM (ITS) AND  
TRAIL PROJECTS IN THE FY 2013/14 – 2018/19 PINELLAS COUNTY  
CAPITAL IMPROVEMENT PROGRAM**

PROJECT NUMBER	LOCATION	PROJECT DESCRIPTION	STATUS
175A	Park Blvd (SR 694) from 4 <sup>th</sup> St to Gulf Blvd	ATMS Improvements	CST 2013/14
196A	South Loop Fiber Project on Alt US 19 (SR 595) from Druid Rd to 5 <sup>th</sup> Ave N; 5 <sup>th</sup> Ave N from Alt US 19 to 34 <sup>th</sup> St S; and 34 <sup>th</sup> St S from 5 <sup>th</sup> Ave N to 54 <sup>th</sup> Ave N	ITS Improvements	CST 2012/13
322A	Bryan Dairy Rd/118th Ave N from 28th St N to Alt US 19 (SR 595)	ITS Communication System	CST 2013/14
199A	East Bay Dr (SR 686) from Gulf Blvd to Ulmerton Rd (SR 688)	ATMS/ITS Improvements	CST 2012/13
326A	66 <sup>th</sup> St/Pasadena Ave (SR 693) from US 19 (SR 55) to Gulf Blvd (SR 699)	ATMS/ITS Improvements	CST 2013/14
404A	US 19 (SR 55) from 49 <sup>th</sup> St N to 126 <sup>th</sup> Ave N	ATMS/ITS Improvements	CST 2013/14
1030A	Belcher Rd from Druid Rd to Park Blvd N	ATMS Improvements	CST 2014/15
1473A	US 19 (SR 55) ATMS/ITS from E Tarpon Ave to Pinellas/Pasco County Line	ATMS/ITS Improvements	CST added 2013/14
186A	Pinellas/Progress Energy Trail Extension from Enterprise Rd/ US 19 (SR 55) to NE Coachman Rd (SR 590) on the Progress Energy Florida, Inc. right-of-way	Shared Use Bike Path/Trail	CST deferred from 2012/13 to 2014/15
328A	Fred Marquis Pinellas Trail Rehabilitation (Phase II) from Michigan Ave to Oceanview Ave	Shared Use Bike Path/Trail	CST underway
1031A	Gulf Blvd from Belleair Beach Causeway (SR 686) to S of 35 <sup>th</sup> Ave/Pinellas Bayway	ATMS Improvements	CST 2015/16

**Notes:**

- 1) CST = Construction; ATMS = Arterial Traffic Management Systems ITS = Intelligent Transportation Systems; TE = Transportation Enhancement
- 2) Shaded projects indicate changes in the FY 2013/14 – 2018/19 Pinellas County Capital Improvement Program (CIP) compared to the adopted FY 2012/13 – 2017/18 CIP. The status column summarizes the changes.
- 3) This project summary table does not include projects such as drainage work, railroad crossings and routine maintenance and repairs.

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Other Transportation</b>													
<b>Project: 001817A Municipal Services Taxing Unit - Paving</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3022	Local Streets/Collector Projects								
020.5	Design MSTU	0	70,000	70,000	70,000	70,000	70,000	70,000	70,000	70,000	70,000	70,000	700,000
030.5	Construction MSTU	0	580,000	580,000	580,000	580,000	580,000	580,000	580,000	580,000	580,000	580,000	5,800,000
<b>Project Total for : Fund: 3001 Capital Projects</b>		<b>Center: 414100</b>	<b>CIP-Transportation</b>	<b>Program: 3022</b>	<b>Local Streets/Collector Projects</b>								
		0	650,000	650,000	650,000	650,000	650,000	650,000	650,000	650,000	650,000	650,000	6,500,000
<b>Total for Project: 001817A Municipal Services Taxing Unit - Paving</b>		0	650,000	650,000	650,000	650,000	650,000	650,000	650,000	650,000	650,000	650,000	6,500,000
<b>Funding Source:</b>													
MSTU - General Fund		0	650,000	650,000	650,000	650,000	650,000	650,000	650,000	650,000	650,000	650,000	6,500,000
<b>Funding Total:</b>		0	650,000	650,000	650,000	650,000	650,000	650,000	650,000	650,000	650,000	650,000	6,500,000

**Project Description:** Local paving program to improve residential roadway surfaces and associated drainage serving the unincorporated areas in the County.

**Project Classifications:**

CIP Phase	Design
Location	Countywide
Penny Program	Transportation and Traffic Flow

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000106A 1501 ATMS/ITS Countywide System Program</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects								
020.3	Design-LOGT	400,000	250,000	250,000	250,000	250,000	0	0	0	0	0	0	1,400,000
030.3	Constr-LOGT	0	250,000	0	250,000	0	0	0	0	0	0	0	500,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3021</i>	<i>Intersection Improvements Projects</i>							
		400,000	500,000	250,000	500,000	250,000	0	0	0	0	0	0	1,900,000
<b>Total for Project: 000106A</b>		<b>1501 ATMS/ITS Countywide System Program</b>											
		400,000	500,000	250,000	500,000	250,000	0	0	0	0	0	0	1,900,000
<b>Funding Source:</b>													
Local Option Gas Tax		400,000	500,000	250,000	500,000	250,000	0	0	0	0	0	0	1,900,000
<b>Funding Total:</b>		400,000	500,000	250,000	500,000	250,000	0	0	0	0	0	0	1,900,000

**Project Description:** Project to design and construct the Countywide Advanced Traffic Management System (ATMS)/Intelligent Transportation System (ITS) utilizing the 9th Cent Fuel Tax.

**Project Classifications:**

CIP Phase	Construction
Location	Countywide
Originating Department	DEI Public Works

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000109A 2161 Beckett Bridge Project Development &amp; Environment Study</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3031	Bridges-Repair & Improvement								
020.1	Design-Penny	252,780	0	0	0	0	0	0	0	0	0	0	252,780
020.4	Design-Grant	182,060	0	0	0	0	0	0	0	0	0	0	182,060
020.5	Design-Fed	48,000	0	0	0	0	0	0	0	0	0	0	48,000
<b>Project Total for : Fund: 3001 Capital Projects</b>		<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3031</i>	<i>Bridges-Repair &amp; Improvement</i>								
		482,840	0	0	0	0	0	0	0	0	0	0	482,840
<b>Total for Project: 000109A 2161 Beckett Bridge Project Development &amp; Environment Study</b>		482,840	0	0	0	0	0	0	0	0	0	0	482,840
<b>Funding Source:</b>													
Grant - State		182,060	0	0	0	0	0	0	0	0	0	0	182,060
Penny for Pinellas		252,780	0	0	0	0	0	0	0	0	0	0	252,780
Grant - Federal		48,000	0	0	0	0	0	0	0	0	0	0	48,000
<b>Funding Total:</b>		482,840	0	0	0	0	0	0	0	0	0	0	482,840

**Project Description:** Prepare a Project Development & Environment Study to determine the type of improvements or replacement necessary for the Beckett Bridge.

**Project Classifications:**

CIP Phase	Design
Location	Tarpon Springs
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Various

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation      Budget Type Code: Planning      Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000125A    1646 Bridge Rehabilitation Program</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation		Program: 3031 Bridges-Repair & Improvement								
020.1	Design-Penny	271,080	220,000	330,000	100,000	220,000	220,000	220,000	55,000	0	0	0	1,636,080
020.5	Design-Unfunded	0	0	0	0	0	0	0	165,000	220,000	220,000	220,000	825,000
030.1	Constr-Penny	500,000	500,000	1,000,000	500,000	800,000	1,800,000	1,581,600	745,000	0	0	0	7,426,600
030.5	Constr-Unfunded	0	0	0	0	0	0	0	2,250,000	2,000,000	2,000,000	2,000,000	8,250,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3031</i>	<i>Bridges-Repair &amp; Improvement</i>							
		771,080	720,000	1,330,000	600,000	1,020,000	2,020,000	1,801,600	3,215,000	2,220,000	2,220,000	2,220,000	18,137,680
<b>Total for Project: 000125A</b>		<b>1646 Bridge Rehabilitation Program</b>											
		771,080	720,000	1,330,000	600,000	1,020,000	2,020,000	1,801,600	3,215,000	2,220,000	2,220,000	2,220,000	18,137,680
<b>Funding Source:</b>													
Unfunded		0	0	0	0	0	0	0	2,415,000	2,220,000	2,220,000	2,220,000	9,075,000
Penny for Pinellas		771,080	720,000	1,330,000	600,000	1,020,000	2,020,000	1,801,600	800,000	0	0	0	9,062,680
<b>Funding Total:</b>		771,080	720,000	1,330,000	600,000	1,020,000	2,020,000	1,801,600	3,215,000	2,220,000	2,220,000	2,220,000	18,137,680

**Project Description:** Rehabilitation or replacement work as needed to preserve the integrity of the county's bridge system. Projects to be selected from prioritized list.

**Project Classifications:**

CIP Phase	Construction
Location	Countywide
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

	Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
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**Function: Transportation**

**Activity: Road & Street Facilities**

**Project: 000126A 2182 Bryan Dairy Rd @ Starkey Rd Intersection Improvements**

Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects							
010.1	Acq-Penny	250,000	0	0	0	0	0	0	0	0	0	250,000
020.1	Design-Penny	60,000	60,000	0	0	0	0	0	0	0	0	120,000
030.1	Constr-Penny	600,000	600,000	0	0	0	0	0	0	0	0	1,200,000
030.4	Constr-Grant	600,000	600,000	0	0	0	0	0	0	0	0	1,200,000
040.3	Testing-Penny	20,000	10,000	0	0	0	0	0	0	0	0	30,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3021</i>	<i>Intersection Improvements Projects</i>						
		1,530,000	1,270,000	0	0	0	0	0	0	0	0	2,800,000
<b>Total for Project: 000126A</b>		<b>2182 Bryan Dairy Rd @ Starkey Rd Intersection Improvements</b>										
		1,530,000	1,270,000	0	0	0	0	0	0	0	0	2,800,000
<b>Funding Source:</b>												
	Grant - State	600,000	600,000	0	0	0	0	0	0	0	0	1,200,000
	Penny for Pinellas	930,000	670,000	0	0	0	0	0	0	0	0	1,600,000
<b>Funding Total:</b>		1,530,000	1,270,000	0	0	0	0	0	0	0	0	2,800,000

**Project Description:** Intersection improvements at Bryan Dairy Road and Starkey Road.

**Project Classifications:**

CIP Phase	Design
Location	Countywide
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Greater Seminole Area

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

	Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
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**Function: Transportation**

**Activity: Road & Street Facilities**

**Project: 000127A 920588 Bryan Dairy Road - Starkey to 72nd St**

Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3020	Arterial Roads Projects							
020.1	Design-Penny	100,000	0	0	0	0	0	0	0	0	0	100,000
030.1	Constr-Penny	864,000	0	0	0	0	0	0	0	0	0	864,000
030.4	Constr-Grant	664,000	0	0	0	0	0	0	0	0	0	664,000
040.4	Testing-Grant	100,000	0	0	0	0	0	0	0	0	0	100,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3020</i>	<i>Arterial Roads Projects</i>						
		1,728,000	0	0	0	0	0	0	0	0	0	1,728,000
<b>Total for Project: 000127A</b>		<b>920588 Bryan Dairy Road - Starkey to 72nd St</b>										
		1,728,000	0	0	0	0	0	0	0	0	0	1,728,000
<b>Funding Source:</b>												
	Grant - State	764,000	0	0	0	0	0	0	0	0	0	764,000
	Penny for Pinellas	964,000	0	0	0	0	0	0	0	0	0	964,000
<b>Funding Total:</b>		1,728,000	0	0	0	0	0	0	0	0	0	1,728,000

**Project Description:** Reconstruct & widen Bryan Dairy Rd. from a 4 to 6-lane divided urban arterial roadway. Proj. includes improvements to Belcher Rd. from S of Bryan Dairy Rd. to N of 114th Ave. (Proj. length of 1.48 mi. along Bryan Dairy Rd. & .62 mile along Belcher Rd)

**Project Classifications:**

CIE Elements	Transportation - Traffic Circu
CIP Phase	Design
County Road Corridor	CR 296
Location	Pinellas Park
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Various

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation      Budget Type Code: Planning      Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000130A    104 Contingency Roadway &amp; Right-of-Way Requirements</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3024	Road & Street Support Projects								
030.1	Constr-Penny	10,000	10,000	10,000	10,000	10,000	10,000	10,000	3,000	0	0	0	73,000
030.5	Constr-Unfunded	0	0	0	0	0	0	0	7,000	10,000	10,000	10,000	37,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3024</i>	<i>Road &amp; Street Support Projects</i>							
		10,000	10,000	10,000	10,000	10,000	10,000	10,000	10,000	10,000	10,000	10,000	110,000
<b>Total for Project: 000130A</b>		<b>104 Contingency Roadway &amp; Right-of-Way Requirements</b>											
		10,000	10,000	10,000	10,000	10,000	10,000	10,000	10,000	10,000	10,000	10,000	110,000
<b>Funding Source:</b>													
Unfunded		0	0	0	0	0	0	0	7,000	10,000	10,000	10,000	37,000
Penny for Pinellas		10,000	10,000	10,000	10,000	10,000	10,000	10,000	3,000	0	0	0	73,000
<b>Funding Total:</b>		10,000	10,000	10,000	10,000	10,000	10,000	10,000	10,000	10,000	10,000	10,000	110,000

**Project Description:** Reserve to meet court judgements on condemnation/eminent domain cases, hazardous material evaluations and services, and other unanticipated right of way needs, or other general contingency road repair needs.

**Project Classifications:**

CIP Phase	Construction
Location	Countywide
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Countywide



## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000142A 2177 Forest Lakes Blvd Pavement Rehabilitation</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3020	Arterial Roads Projects								
020.1	Design-Penny	20,000	178,400	107,000	107,000	100,000	0	0	0	0	0	0	512,400
030.1	Constr-Penny	0	500,000	500,000	1,364,000	1,274,000	0	0	0	0	0	0	3,638,000
040.1	Testing-Penny	0	5,000	5,000	10,000	10,000	0	0	0	0	0	0	30,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3020</i>	<i>Arterial Roads Projects</i>							
		20,000	683,400	612,000	1,481,000	1,384,000	0	0	0	0	0	0	4,180,400
<b>Total for Project: 000142A</b>		<b>2177 Forest Lakes Blvd Pavement Rehabilitation</b>											
		20,000	683,400	612,000	1,481,000	1,384,000	0	0	0	0	0	0	4,180,400
<b>Funding Source:</b>													
Penny for Pinellas		20,000	683,400	612,000	1,481,000	1,384,000	0	0	0	0	0	0	4,180,400
<b>Funding Total:</b>		20,000	683,400	612,000	1,481,000	1,384,000	0	0	0	0	0	0	4,180,400

**Project Description:** Design and construction of remediation measures for pavement failures from SR580 to the County line. Phase I in FY14 will be evaluated to determine scope of improvements for future years.

**Project Classifications:**

CIE Elements	Transportation - Traffic Circu
CIP Phase	Design
Location	Countywide
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	East Lake Tarpon Area

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000144A 1096 General Sidewalk and ADA Program</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3026	Sidewalks Projects								
020.1	Design-Penny	157,560	109,500	219,000	218,000	110,000	323,000	320,000	80,000	0	0	0	1,537,060
020.5	Design-Unfunded	0	0	0	0	0	0	0	240,000	320,000	320,000	320,000	1,200,000
030.1	Constr-Penny	800,000	1,000,000	1,792,000	847,000	703,000	1,353,000	1,341,000	335,000	0	0	0	8,171,000
030.5	Constr-Unfunded	0	0	0	0	0	0	0	1,005,000	1,340,000	1,340,000	1,340,000	5,025,000
040.1	Testing-Penny	10,000	10,000	10,000	10,000	10,000	10,000	10,000	10,000	0	0	0	80,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3026</i>	<i>Sidewalks Projects</i>							
		967,560	1,119,500	2,021,000	1,075,000	823,000	1,686,000	1,671,000	1,670,000	1,660,000	1,660,000	1,660,000	16,013,060
<b>Total for Project: 000144A</b>		<b>1096 General Sidewalk and ADA Program</b>											
		967,560	1,119,500	2,021,000	1,075,000	823,000	1,686,000	1,671,000	1,670,000	1,660,000	1,660,000	1,660,000	16,013,060
<b>Funding Source:</b>													
Unfunded		0	0	0	0	0	0	0	1,245,000	1,660,000	1,660,000	1,660,000	6,225,000
Penny for Pinellas		967,560	1,119,500	2,021,000	1,075,000	823,000	1,686,000	1,671,000	425,000	0	0	0	9,788,060
<b>Funding Total:</b>		967,560	1,119,500	2,021,000	1,075,000	823,000	1,686,000	1,671,000	1,670,000	1,660,000	1,660,000	1,660,000	16,013,060

**Project Description:** Funding for construction of sidewalk and ADA improvements countywide. Project locations are chosen from a prioritized list.

**Project Classifications:**

CIP Phase	Construction
Location	Countywide
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Countywide

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total	
<b>Function: Transportation</b>														
<b>Activity: Road &amp; Street Facilities</b>														
<b>Project: 000145A 1219 Gooden Crossing Infrastructure Improvements</b>														
Fund: 1009	Community Developmnt Grnt		Center: 242220	Community Development Block Grant	Program: 1331	Community Vitality & Improvement								
020.4	Design-CDBG	141,000	0	0	0	0	0	0	0	0	0	0	141,000	
030.4	Constr-CDBG	565,800	0	0	0	0	0	0	0	0	0	0	565,800	
040.3	Testing-CDBG	50,000	0	0	0	0	0	0	0	0	0	0	50,000	
110.3	Other-CDBG	16,100	0	0	0	0	0	0	0	0	0	0	16,100	
<b>Project Total for : Fund: 1009 Community Developmnt Grnt</b>			<b>Center: 242220</b>	<b>Community Development Block Grant</b>	<b>Program: 1331</b>	<b>Community Vitality &amp; Improvement</b>								
		772,900	0	0	0	0	0	0	0	0	0	0	772,900	
<b>Total for Project: 000145A 1219 Gooden Crossing Infrastructure Improvements</b>		772,900	0	0	0	0	0	0	0	0	0	0	772,900	
<b>Funding Source:</b>														
Community Development Fund		772,900	0	0	0	0	0	0	0	0	0	0	772,900	
<b>Funding Total:</b>		772,900	0	0	0	0	0	0	0	0	0	0	772,900	

**Project Description:** Drainage improvements, road reconstruction, and sidewalk construction along Gooden Crossing from 119th St to Pinellas County Trail (approximately 1325 LF or 0.25 miles).

**Project Classifications:**

CIP Phase	Design
Location	Largo, Belleair, Belleair Bluffs
Originating Department	DEI Public Works

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation      Budget Type Code: Planning      Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total	
<b>Function: Transportation</b>														
<b>Activity: Road &amp; Street Facilities</b>														
<b>Project: 000146A      875 Gulf Blvd Improvements</b>														
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3024	Road & Street Support Projects									
110.1	Other-Penny	1,560,000	5,440,000	3,500,000	3,500,000	7,000,000	7,000,000	7,000,000	0	0	0	0	35,000,000	
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3024</i>	<i>Road &amp; Street Support Projects</i>								
		1,560,000	5,440,000	3,500,000	3,500,000	7,000,000	7,000,000	7,000,000	0	0	0	0	35,000,000	
<b>Total for Project: 000146A</b>		<b>875 Gulf Blvd Improvements</b>												
		1,560,000	5,440,000	3,500,000	3,500,000	7,000,000	7,000,000	7,000,000	0	0	0	0	35,000,000	
<b>Funding Source:</b>														
	Penny for Pinellas	1,560,000	5,440,000	3,500,000	3,500,000	7,000,000	7,000,000	7,000,000	0	0	0	0	35,000,000	
<b>Funding Total:</b>		1,560,000	5,440,000	3,500,000	3,500,000	7,000,000	7,000,000	7,000,000	0	0	0	0	35,000,000	

**Project Description:** Enhancement of Gulf Blvd. from SR 60 on Clearwater Beach, south to Pass-A-Grille Beach. Enhancements include relocate aerial utility lines underground, construct pedestrian cross-walks, install decorative street lighting, common signage & landscape.

**Project Classifications:**

CIP Phase	Design
Location	Countywide
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Various

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000147A 922265 Haines Rd - 54th Ave to 28th St Intersection Improvements</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects								
010.1	Acq-Penny	50,000	50,000	0	0	0	0	0	0	0	0	0	100,000
020.1	Design-Penny	0	20,000	20,000	11,000	11,000	0	0	0	0	0	0	62,000
030.1	Constr-Penny	0	0	0	746,000	737,000	0	0	0	0	0	0	1,483,000
040.1	Testing-Penny	0	0	0	5,000	5,000	0	0	0	0	0	0	10,000
<b>Project Total for : Fund: 3001 Capital Projects</b>		<b>Center: 414100</b>	<b>CIP-Transportation</b>	<b>Program: 3021</b>	<b>Intersection Improvements Projects</b>								
		50,000	70,000	20,000	762,000	753,000	0	0	0	0	0	0	1,655,000
<b>Total for Project: 000147A 922265 Haines Rd - 54th Ave to 28th St Intersection Improvements</b>													
		50,000	70,000	20,000	762,000	753,000	0	0	0	0	0	0	1,655,000
<b>Funding Source:</b>													
Penny for Pinellas		50,000	70,000	20,000	762,000	753,000	0	0	0	0	0	0	1,655,000
<b>Funding Total:</b>		50,000	70,000	20,000	762,000	753,000	0	0	0	0	0	0	1,655,000

**Project Description:** Right-of-Way acquisition in FY13/FY14 and construction in FY16/FY17 for the reconstruction of Haines Road to a 2-lane urban roadway with sidewalk and drainage improvements.

**Project Classifications:**

CIE Elements	Transportation - Traffic Circu
CIP Phase	Design
Location	St Petersburg
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Greater St. Petersburg Area

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000151A 1659 Indian Rocks Road Sidewalk</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3026	Sidewalks Projects								
020.1	Design-Penny	50,000	54,100	0	0	0	0	0	0	0	0	0	104,100
030.1	Constr-Penny	100,000	700,000	0	0	0	0	0	0	0	0	0	800,000
040.1	Testing-Penny	2,000	2,000	0	0	0	0	0	0	0	0	0	4,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3026</i>	<i>Sidewalks Projects</i>							
		152,000	756,100	0	0	0	0	0	0	0	0	0	908,100
<b>Total for Project: 000151A 1659 Indian Rocks Road Sidewalk</b>		152,000	756,100	0	0	0	0	0	0	0	0	0	908,100
<b>Funding Source:</b>													
Penny for Pinellas		152,000	756,100	0	0	0	0	0	0	0	0	0	908,100
<b>Funding Total:</b>		152,000	756,100	0	0	0	0	0	0	0	0	0	908,100

**Project Description:** Construction of sidewalks on Indian Rocks Road from Walsingham Road to Wilcox Road to improve student access to Anona Elementary School and general pedestrian access within the area.

**Project Classifications:**

CIP Phase	Design
Location	Largo, Belleair, Belleair Bluffs
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Greater Largo Area

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

	Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total	
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000152A 922147 Intersection Improvements</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects								
020.1	Design-Penny	90,000	0	0	0	0	0	0	0	0	0	90,000	
020.5	Design-Unfunded	0	0	0	0	0	0	150,000	200,000	200,000	200,000	750,000	
030.1	Constr-Penny	0	0	1,803,000	0	0	554,000	2,084,000	0	0	0	4,441,000	
030.5	Constr-Unfunded	0	0	0	0	0	0	750,000	1,000,000	1,000,000	1,000,000	3,750,000	
<b>Project Total for : Fund: 3001 Capital Projects</b>		<b>Center: 414100</b>	<b>CIP-Transportation</b>	<b>Program: 3021</b>	<b>Intersection Improvements Projects</b>								
		90,000	0	1,803,000	0	0	554,000	2,084,000	900,000	1,200,000	1,200,000	1,200,000	9,031,000
<b>Total for Project: 000152A 922147 Intersection Improvements</b>		<b>90,000</b>	<b>0</b>	<b>1,803,000</b>	<b>0</b>	<b>0</b>	<b>554,000</b>	<b>2,084,000</b>	<b>900,000</b>	<b>1,200,000</b>	<b>1,200,000</b>	<b>1,200,000</b>	<b>9,031,000</b>
<b>Funding Source:</b>													
	Unfunded	0	0	0	0	0	0	900,000	1,200,000	1,200,000	1,200,000	4,500,000	
	Penny for Pinellas	90,000	0	1,803,000	0	0	554,000	2,084,000	0	0	0	4,531,000	
<b>Funding Total:</b>		<b>90,000</b>	<b>0</b>	<b>1,803,000</b>	<b>0</b>	<b>0</b>	<b>554,000</b>	<b>2,084,000</b>	<b>900,000</b>	<b>1,200,000</b>	<b>1,200,000</b>	<b>1,200,000</b>	<b>9,031,000</b>

**Project Description:** Funding allocation for Countywide intersection safety and capacity modifications and mast arm signalization projects.

**Project Classifications:**

CIP Phase	Construction
Location	Countywide
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Countywide

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000154A 920522 Keystone Road - US19 to East Lake Rd</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3020	Arterial Roads Projects								
020.1	Design-Penny	510,000	0	0	0	0	0	0	0	0	0	0	510,000
030.1	Constr-Penny	4,000,000	0	0	0	0	0	0	0	0	0	0	4,000,000
040.1	Testing-Penny	150,000	0	0	0	0	0	0	0	0	0	0	150,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3020</i>	<i>Arterial Roads Projects</i>							
		4,660,000	0	0	0	0	0	0	0	0	0	0	4,660,000
<b>Total for Project: 000154A</b>		<b>920522 Keystone Road - US19 to East Lake Rd</b>											
		4,660,000	0	0	0	0	0	0	0	0	0	0	4,660,000
<b>Funding Source:</b>													
Penny for Pinellas		4,660,000	0	0	0	0	0	0	0	0	0	0	4,660,000
<b>Funding Total:</b>		4,660,000	0	0	0	0	0	0	0	0	0	0	4,660,000

**Project Description:** Reconstruct and widen Keystone Rd. from US 19 to East Lake Rd from 2 to 4-lane divided urban arterial road including a segment of the Fred E. Marquis Trail. Project cost includes fees for Construction Engineering and Inspection. (Proj. length 3 mi.)

**Project Classifications:**

CIE Elements	Transportation - Traffic Circu
CIP Phase	Construction
County Road Corridor	CR 582
Location	Palm Harbor, East Lake
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Various



## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000163A 2055 LaPlaza Avenue Bridge Reconstruction</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3031	Bridges-Repair & Improvement								
020.1	Design-Penny	72,440	20,000	0	0	0	0	0	0	0	0	0	92,440
030.1	Constr-Penny	900,000	700,000	0	0	0	0	0	0	0	0	0	1,600,000
040.1	Testing-Penny	10,000	5,000	0	0	0	0	0	0	0	0	0	15,000
<b>Project Total for : Fund: 3001 Capital Projects</b>		<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3031</i>	<i>Bridges-Repair &amp; Improvement</i>								
		982,440	725,000	0	0	0	0	0	0	0	0	0	1,707,440
<b>Total for Project: 000163A 2055 LaPlaza Avenue Bridge Reconstruction</b>		982,440	725,000	0	0	0	0	0	0	0	0	0	1,707,440
<b>Funding Source:</b>													
Penny for Pinellas		982,440	725,000	0	0	0	0	0	0	0	0	0	1,707,440
<b>Funding Total:</b>		982,440	725,000	0	0	0	0	0	0	0	0	0	1,707,440

**Project Description:** Reconstruction of the LaPlaza Avenue Bridge. This work will be done in conjunction with the Bear Creek Drainage Improvements (PID 000108A).

**Project Classifications:**

CIE Elements	Drainage Element
CIP Phase	Design
Drainage Basin	39 Bear Creek
Location	St Petersburg
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

	Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>												
<b>Activity: Road &amp; Street Facilities</b>												
<b>Project: 000175A 2159 Park Boulevard ATMS Project</b>												
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects							
020.3	Design-LOGT	200,000	256,000	0	0	0	0	0	0	0	0	456,000
030.3	Constr-LOGT	0	200,000	900,000	100,000	0	0	0	0	0	0	1,200,000
030.4	Constr-Grant	0	200,000	900,000	100,000	0	0	0	0	0	0	1,200,000
110.3	Other-LOGT	0	400,000	100,000	0	0	0	0	0	0	0	500,000
110.4	Other-Grant	0	400,000	100,000	0	0	0	0	0	0	0	500,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3021</i>	<i>Intersection Improvements Projects</i>						
		200,000	1,456,000	2,000,000	200,000	0	0	0	0	0	0	3,856,000
<b>Total for Project: 000175A</b>		<b>2159 Park Boulevard ATMS Project</b>										
		200,000	1,456,000	2,000,000	200,000	0	0	0	0	0	0	3,856,000
<b>Funding Source:</b>												
	Local Option Gas Tax	200,000	856,000	1,000,000	100,000	0	0	0	0	0	0	2,156,000
	Grant - State	0	600,000	1,000,000	100,000	0	0	0	0	0	0	1,700,000
<b>Funding Total:</b>												
		200,000	1,456,000	2,000,000	200,000	0	0	0	0	0	0	3,856,000

**Project Description:** Design and Construct a new ATMS/ITS system on Park Blvd utilizing funds from 9th Cent Fuel Tax and FDOT Transportation Regional Incentive Program.

**Project Classifications:**

CIP Phase	Design
Location	Countywide
Originating Department	DEI Public Works
TIF District	Countywide

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000180A 2162 Park Street Bridge Replacement</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3031	Bridges-Repair & Improvement								
020.1	Design-Penny	62,910	50,000	0	0	0	0	0	0	0	0	0	112,910
030.1	Constr-Penny	400,000	700,000	0	0	0	0	0	0	0	0	0	1,100,000
040.1	Testing-Penny	5,000	5,000	0	0	0	0	0	0	0	0	0	10,000
<b>Project Total for : Fund: 3001 Capital Projects</b>		<b>Center: 414100</b>	<b>CIP-Transportation</b>	<b>Program: 3031</b>	<b>Bridges-Repair &amp; Improvement</b>								
		467,910	755,000	0	0	0	0	0	0	0	0	0	1,222,910
<b>Total for Project: 000180A 2162 Park Street Bridge Replacement</b>		467,910	755,000	0	0	0	0	0	0	0	0	0	1,222,910
<b>Funding Source:</b>													
Penny for Pinellas		467,910	755,000	0	0	0	0	0	0	0	0	0	1,222,910
<b>Funding Total:</b>		467,910	755,000	0	0	0	0	0	0	0	0	0	1,222,910

**Project Description:** Replacement of an existing bridge on Park Street over creek No. 9, between 5th Avenue North and 9th Avenue North.

**Project Classifications:**

CIP Phase	Design
Location	St Petersburg
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Various

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation      Budget Type Code: Planning      Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000181A      621 Paving Assessment Projects</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3025	Special Assessment-Paving								
020.1	Design-SA	0	0	0	0	200,000	0	0	0	0	0	0	200,000
030.5	Constr-Unfunded	0	0	0	0	0	0	0	0	150,000	0	200,000	350,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3025</i>	<i>Special Assessment-Paving</i>							
		0	0	0	0	200,000	0	0	0	150,000	0	200,000	550,000
<b>Total for Project: 000181A</b>		<b>621 Paving Assessment Projects</b>	0	0	0	0	200,000	0	0	150,000	0	200,000	550,000
<b>Funding Source:</b>													
Special Assessment Funds		0	0	0	0	0	200,000	0	0	0	0	0	200,000
Unfunded		0	0	0	0	0	0	0	0	150,000	0	200,000	350,000
<b>Funding Total:</b>		0	0	0	0	0	200,000	0	0	150,000	0	200,000	550,000

**Project Description:** Project reserve of contingency funds for roadway assessment projects.

**Project Classifications:**

CIP Phase	Construction
Location	Countywide
Originating Department	DEI Public Works
TIF District	Countywide

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000182A 921773 Permit Monitoring / Testing Services</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3024	Road & Street Support Projects								
020.1	Design-Penny	150,000	150,000	150,000	150,000	150,000	150,000	150,000	38,000	0	0	0	1,088,000
020.5	Design-Unfunded	0	0	0	0	0	0	0	112,000	150,000	150,000	150,000	562,000
<b>Project Total for : Fund: 3001 Capital Projects</b>		<b>Center: 414100</b>	<b>CIP-Transportation</b>	<b>Program: 3024</b>	<b>Road &amp; Street Support Projects</b>								
		150,000	150,000	150,000	150,000	150,000	150,000	150,000	150,000	150,000	150,000	150,000	1,650,000
<b>Total for Project: 000182A 921773 Permit Monitoring / Testing Services</b>		150,000	150,000	150,000	150,000	150,000	150,000	150,000	150,000	150,000	150,000	150,000	1,650,000
<b>Funding Source:</b>													
Unfunded		0	0	0	0	0	0	0	112,000	150,000	150,000	150,000	562,000
Penny for Pinellas		150,000	150,000	150,000	150,000	150,000	150,000	150,000	38,000	0	0	0	1,088,000
<b>Funding Total:</b>		150,000	150,000	150,000	150,000	150,000	150,000	150,000	150,000	150,000	150,000	150,000	1,650,000

**Project Description:** Funding for planting and monitoring stormwater mgmt facilities to meet permit conditions as required by environmental permitting agencies such as SWFWMD, DEP & ACOE. Project also includes funding for various non-project related test services.

**Project Classifications:**

CIP Phase	Design
Location	Countywide
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Countywide

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000186A 922499 Pinellas/Progress Energy Trail Extension</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation		Program: 3023		Pinellas Trail Projects						
020.1	Design-Penny	20,000	200,000	156,000	130,000	0	0	0	0	0	0	0	506,000
030.1	Constr-Penny	0	0	1,712,000	2,914,000	0	0	0	0	0	0	0	4,626,000
040.1	Testing-Penny	0	0	54,000	30,000	0	0	0	0	0	0	0	84,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3023</i>	<i>Pinellas Trail Projects</i>							
		20,000	200,000	1,922,000	3,074,000	0	0	0	0	0	0	0	5,216,000
<b>Total for Project: 000186A</b>		<b>922499 Pinellas/Progress Energy Trail Extension</b>											
		20,000	200,000	1,922,000	3,074,000	0	0	0	0	0	0	0	5,216,000
<b>Funding Source:</b>													
Penny for Pinellas		20,000	200,000	1,922,000	3,074,000	0	0	0	0	0	0	0	5,216,000
<b>Funding Total:</b>		20,000	200,000	1,922,000	3,074,000	0	0	0	0	0	0	0	5,216,000

**Project Description:** Extension of the Progress Energy Trail from Enterprise Road/US 19 area to SR 590 on Progress Energy / Duke right of way.

**Project Classifications:**

CIP Phase	Design
Location	Countywide
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Countywide

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000189A 921105 Railroad Crossing Improvements (8411104&amp;8414611)</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3024	Road & Street Support Projects								
020.1	Design-Penny	126,000	26,000	53,000	26,000	53,000	103,000	53,000	3,000	0	0	0	443,000
020.5	Design-Unfunded	0	0	0	0	0	0	0	8,000	50,000	20,000	50,000	128,000
030.1	Constr-Penny	214,000	509,000	502,000	0	0	387,000	0	40,000	0	0	0	1,652,000
030.5	Constr-Unfunded	0	0	0	0	0	0	0	120,000	100,000	500,000	200,000	920,000
110.1	Other-Penny	300,000	300,000	350,000	0	0	300,000	0	25,000	0	0	0	1,275,000
110.5	Other-Unfunded	0	0	0	0	0	0	0	75,000	100,000	300,000	100,000	575,000
<b>Project Total for : Fund: 3001 Capital Projects</b>		<b>Center: 414100</b>	<b>CIP-Transportation</b>	<b>Program: 3024</b>	<b>Road &amp; Street Support Projects</b>								
		640,000	835,000	905,000	26,000	53,000	790,000	53,000	271,000	250,000	820,000	350,000	4,993,000
<b>Total for Project: 000189A 921105 Railroad Crossing Improvements (8411104&amp;8414611)</b>		640,000	835,000	905,000	26,000	53,000	790,000	53,000	271,000	250,000	820,000	350,000	4,993,000
<b>Funding Source:</b>													
Unfunded		0	0	0	0	0	0	0	203,000	250,000	820,000	350,000	1,623,000
Penny for Pinellas		640,000	835,000	905,000	26,000	53,000	790,000	53,000	68,000	0	0	0	3,370,000
<b>Funding Total:</b>		640,000	835,000	905,000	26,000	53,000	790,000	53,000	271,000	250,000	820,000	350,000	4,993,000

**Project Description:** Improve railroad crossings in coordination with CSX. "Other" category is for payment to CSX for rails and road crossing panels. FY14 -49th Street. FY15-Hercules Avenue.

**Project Classifications:**

CIP Phase	Design
Location	Countywide
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Countywide

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

	Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total	
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000192A 921544 Road Resurfacing &amp; Rehabilitation Program</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3024	Road & Street Support Projects								
030.1	Constr-Penny	6,000,000	6,900,000	6,982,000	7,027,000	5,083,000	5,500,000	6,728,000	1,750,000	0	0	0	45,970,000
030.5	Constr-Unfunded	0	0	0	0	0	0	0	5,250,000	7,000,000	7,000,000	7,000,000	26,250,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3024</i>	<i>Road &amp; Street Support Projects</i>							
		6,000,000	6,900,000	6,982,000	7,027,000	5,083,000	5,500,000	6,728,000	7,000,000	7,000,000	7,000,000	7,000,000	72,220,000
<b>Total for Project: 000192A</b>		<b>921544 Road Resurfacing &amp; Rehabilitation Program</b>											
		6,000,000	6,900,000	6,982,000	7,027,000	5,083,000	5,500,000	6,728,000	7,000,000	7,000,000	7,000,000	7,000,000	72,220,000
<b>Funding Source:</b>													
	Unfunded	0	0	0	0	0	0	0	5,250,000	7,000,000	7,000,000	7,000,000	26,250,000
	Penny for Pinellas	6,000,000	6,900,000	6,982,000	7,027,000	5,083,000	5,500,000	6,728,000	1,750,000	0	0	0	45,970,000
<b>Funding Total:</b>		6,000,000	6,900,000	6,982,000	7,027,000	5,083,000	5,500,000	6,728,000	7,000,000	7,000,000	7,000,000	7,000,000	72,220,000

**Project Description:** Funding for annual contracts for resurfacing of Countywide arterials, collectors, and local unincorporated area subdivision streets.

**Project Classifications:**

CIP Phase	Construction
Location	Countywide
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Countywide



## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000195A 1145 Signal System Consultant Services</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects								
020.1	Design-Penny	150,000	150,000	150,000	150,000	150,000	150,000	150,000	38,000	0	0	0	1,088,000
020.5	Design-Unfunded	0	0	0	0	0	0	0	112,000	150,000	150,000	150,000	562,000
<b>Project Total for : Fund: 3001 Capital Projects</b>		<b>Center: 414100</b>	<b>CIP-Transportation</b>	<b>Program: 3021</b>	<b>Intersection Improvements Projects</b>								
		150,000	150,000	150,000	150,000	150,000	150,000	150,000	150,000	150,000	150,000	150,000	1,650,000
<b>Total for Project: 000195A 1145 Signal System Consultant Services</b>		150,000	150,000	150,000	150,000	150,000	150,000	150,000	150,000	150,000	150,000	150,000	1,650,000
<b>Funding Source:</b>													
Unfunded		0	0	0	0	0	0	0	112,000	150,000	150,000	150,000	562,000
Penny for Pinellas		150,000	150,000	150,000	150,000	150,000	150,000	150,000	38,000	0	0	0	1,088,000
<b>Funding Total:</b>		150,000	150,000	150,000	150,000	150,000	150,000	150,000	150,000	150,000	150,000	150,000	1,650,000

**Project Description:** Consultant services for capacity and intersection evaluation and improvements.

**Project Classifications:**

CIP Phase	Design
Location	Countywide
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Various

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation      Budget Type Code: Planning      Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000196A    2160 South Loop Fiber Project</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects								
020.3	Design-LOGT	125,000	0	0	0	0	0	0	0	0	0	0	125,000
020.4	Design-Grant	125,000	0	0	0	0	0	0	0	0	0	0	125,000
030.3	Constr-LOGT	200,000	1,100,000	40,000	0	0	0	0	0	0	0	0	1,340,000
030.4	Constr-Grant	200,000	1,100,000	40,000	0	0	0	0	0	0	0	0	1,340,000
110.3	Other-LOGT	125,000	100,000	100,000	0	0	0	0	0	0	0	0	325,000
110.4	Other-Grant	125,000	100,000	100,000	0	0	0	0	0	0	0	0	325,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3021</i>	<i>Intersection Improvements Projects</i>							
		900,000	2,400,000	280,000	0	0	0	0	0	0	0	0	3,580,000
<b>Total for Project: 000196A</b>		<b>2160 South Loop Fiber Project</b>											
		900,000	2,400,000	280,000	0	0	0	0	0	0	0	0	3,580,000
<b>Funding Source:</b>													
Local Option Gas Tax		450,000	1,200,000	140,000	0	0	0	0	0	0	0	0	1,790,000
Grant - State		450,000	1,200,000	140,000	0	0	0	0	0	0	0	0	1,790,000
<b>Funding Total:</b>		900,000	2,400,000	280,000	0	0	0	0	0	0	0	0	3,580,000

**Project Description:** Project will complete the County's Fiber Optic trunkline for the Countywide ATMS/ITS System. ATMS Devices will be installed along primary Alt US 19.

**Project Classifications:**

CIP Phase	Design
Location	Countywide
Originating Department	DEI Public Works
TIF District	Countywide

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation      Budget Type Code: Planning      Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000197A      1809 SR 580 / 584 ATMS</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects								
020.3	Design-Grant	325,000	300,000	0	0	0	0	0	0	0	0	0	625,000
020.4	Design-LOGT	325,000	300,000	0	0	0	0	0	0	0	0	0	625,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3021</i>	<i>Intersection Improvements Projects</i>							
		650,000	600,000	0	0	0	0	0	0	0	0	0	1,250,000
<b>Total for Project: 000197A</b>		<b>1809 SR 580 / 584 ATMS</b>											
		650,000	600,000	0	0	0	0	0	0	0	0	0	1,250,000
<b>Funding Source:</b>													
Local Option Gas Tax		325,000	300,000	0	0	0	0	0	0	0	0	0	625,000
Grant - State		325,000	300,000	0	0	0	0	0	0	0	0	0	625,000
<b>Funding Total:</b>		650,000	600,000	0	0	0	0	0	0	0	0	0	1,250,000

**Project Description:** Design and construct a new ATMS/ITS system on SR 580/SR 584 and SR 586 utilizing funds from 9th Cent Fuel Tax and Transportation Regional Incentive Program.

**Project Classifications:**

CIP Phase	Construction
Location	Countywide
Originating Department	DEI Public Works
TIF District	Palm Harbor Area

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation      Budget Type Code: Planning      Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000198A    1810 SR 60 ATMS / ITS Project - Stage 2</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects								
020.4	Design-Grant	250,000	250,000	0	0	0	0	0	0	0	0	0	500,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3021</i>	<i>Intersection Improvements Projects</i>							
		250,000	250,000	0	0	0	0	0	0	0	0	0	500,000
<b>Total for Project: 000198A</b>		<b>1810 SR 60 ATMS / ITS Project - Stage 2</b>		0	0	0	0	0	0	0	0	0	500,000
<b>Funding Source:</b>													
	Grant - State	250,000	250,000	0	0	0	0	0	0	0	0	0	500,000
<b>Funding Total:</b>		250,000	250,000	0	0	0	0	0	0	0	0	0	500,000

**Project Description:** Installation of ATMS/ITS system on SR 60; funding provided through Federal appropriations.

**Project Classifications:**

CIP Phase	Construction
Location	Countywide
Originating Department	DEI Public Works
TIF District	Greater Clearwater Area

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000199A 2023 SR 686 - East Bay Drive ATMS / ITS</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects								
020.3	Design-Grant	12,500	25,000	0	0	0	0	0	0	0	0	0	37,500
020.4	Design-LOGT	12,500	25,000	0	0	0	0	0	0	0	0	0	37,500
030.3	Constr-Grant	312,500	1,000,000	100,000	0	0	0	0	0	0	0	0	1,412,500
030.4	Constr-LOGT	312,500	1,000,000	100,000	0	0	0	0	0	0	0	0	1,412,500
110.3	Other-Grant	175,000	50,000	200,000	0	0	0	0	0	0	0	0	425,000
110.4	Other-LOGT	175,000	50,000	200,000	0	0	0	0	0	0	0	0	425,000
<b>Project Total for : Fund: 3001 Capital Projects</b>		<b>Center: 414100</b>	<b>CIP-Transportation</b>	<b>Program: 3021</b>	<b>Intersection Improvements Projects</b>								
		1,000,000	2,150,000	600,000	0	0	0	0	0	0	0	0	3,750,000
<b>Total for Project: 000199A 2023 SR 686 - East Bay Drive ATMS / ITS</b>		<b>1,000,000</b>	<b>2,150,000</b>	<b>600,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>3,750,000</b>
<b>Funding Source:</b>													
Local Option Gas Tax		500,000	1,075,000	300,000	0	0	0	0	0	0	0	0	1,875,000
Grant - State		500,000	1,075,000	300,000	0	0	0	0	0	0	0	0	1,875,000
<b>Funding Total:</b>		<b>1,000,000</b>	<b>2,150,000</b>	<b>600,000</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>3,750,000</b>

**Project Description:** Design and installation of ATMS/ITS on SR 686 utilizing funds from 9th Cent Fuel Tax and County Incentive Grant Program.

**Project Classifications:**

CIP Phase	Design
Location	Countywide
Originating Department	DEI Public Works
TIF District	Greater Largo Area

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000213A 922380 Traffic Safety Study / Improvements</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3024	Road & Street Support Projects								
020.1	Design-Penny	20,000	20,000	20,000	20,000	20,000	20,000	20,000	20,000	0	0	0	160,000
030.1	Constr-Penny	80,000	80,000	80,000	80,000	80,000	80,000	80,000	80,000	0	0	0	640,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3024</i>	<i>Road &amp; Street Support Projects</i>							
		100,000	100,000	100,000	100,000	100,000	100,000	100,000	100,000	0	0	0	800,000
<b>Total for Project: 000213A</b>		<b>922380 Traffic Safety Study / Improvements</b>											
		100,000	100,000	100,000	100,000	100,000	100,000	100,000	100,000	0	0	0	800,000
<b>Funding Source:</b>													
	Penny for Pinellas	100,000	100,000	100,000	100,000	100,000	100,000	100,000	100,000	0	0	0	800,000
<b>Funding Total:</b>		100,000	100,000	100,000	100,000	100,000	100,000	100,000	100,000	0	0	0	800,000

**Project Description:** Funding for transportation studies and construction costs for evaluation and implementation of traffic related safety improvements.

**Project Classifications:**

CIP Phase	Design
Location	Countywide
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Countywide

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000216A 921320 Underdrain Annual Contracts</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3024	Road & Street Support Projects								
030.1	Constr-Penny	400,000	500,000	582,000	586,000	579,000	270,000	267,000	125,000	0	0	0	3,309,000
030.5	Constr-Unfunded	0	0	0	0	0	0	0	375,000	500,000	500,000	500,000	1,875,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3024</i>	<i>Road &amp; Street Support Projects</i>							
		400,000	500,000	582,000	586,000	579,000	270,000	267,000	500,000	500,000	500,000	500,000	5,184,000
<b>Total for Project: 000216A</b>		<b>921320 Underdrain Annual Contracts</b>											
		400,000	500,000	582,000	586,000	579,000	270,000	267,000	500,000	500,000	500,000	500,000	5,184,000
<b>Funding Source:</b>													
Unfunded		0	0	0	0	0	0	0	375,000	500,000	500,000	500,000	1,875,000
Penny for Pinellas		400,000	500,000	582,000	586,000	579,000	270,000	267,000	125,000	0	0	0	3,309,000
<b>Funding Total:</b>		400,000	500,000	582,000	586,000	579,000	270,000	267,000	500,000	500,000	500,000	500,000	5,184,000

**Project Description:** Funding for construction of underdrains for County roads at various locations to prevent road failures and extend roadway life.

**Project Classifications:**

CIP Phase	Construction
Location	Countywide
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Countywide

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000297A 1618 118th Avenue Expressway</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3020	Arterial Roads Projects								
101.1	Other-Penny	0	5,000,000	15,000,000	8,960,000	8,960,000	8,960,000	8,960,000	2,240,000	0	0	0	58,080,000
110.5	Other-Unfunded	0	0	0	0	0	0	0	6,720,000	0	0	0	6,720,000
<b>Project Total for : Fund: 3001 Capital Projects</b>		<b>Center: 414100</b>	<b>CIP-Transportation</b>	<b>Program: 3020</b>	<b>Arterial Roads Projects</b>								
		0	5,000,000	15,000,000	8,960,000	8,960,000	8,960,000	8,960,000	8,960,000	0	0	0	64,800,000
<b>Total for Project: 000297A 1618 118th Avenue Expressway</b>		0	5,000,000	15,000,000	8,960,000	8,960,000	8,960,000	8,960,000	8,960,000	0	0	0	64,800,000
<b>Funding Source:</b>													
Unfunded		0	0	0	0	0	0	0	6,720,000	0	0	0	6,720,000
Penny for Pinellas		0	5,000,000	15,000,000	8,960,000	8,960,000	8,960,000	8,960,000	2,240,000	0	0	0	58,080,000
<b>Funding Total:</b>		0	5,000,000	15,000,000	8,960,000	8,960,000	8,960,000	8,960,000	8,960,000	0	0	0	64,800,000

**Project Description:** Funding commitment to FDOT for project design and construction.

**Project Classifications:**

CIP Phase	Planned
Location	Largo, Belleair, Belleair Bluffs
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Various



## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000322A 2294 Bryan Dairy Road ATMS/ITS Improvements</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects								
020.3	Design-LOGT	150,000	100,000	0	0	0	0	0	0	0	0	0	250,000
030.3	Constr-LOGT	0	325,000	850,000	200,000	0	0	0	0	0	0	0	1,375,000
030.4	Constr-Grant	0	325,000	850,000	200,000	0	0	0	0	0	0	0	1,375,000
110.3	Other-LOGT	0	200,000	150,000	200,000	0	0	0	0	0	0	0	550,000
110.4	Other-Grant	0	200,000	150,000	200,000	0	0	0	0	0	0	0	550,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3021</i>	<i>Intersection Improvements Projects</i>							
		150,000	1,150,000	2,000,000	800,000	0	0	0	0	0	0	0	4,100,000
<b>Total for Project: 000322A</b>		<b>2294 Bryan Dairy Road ATMS/ITS Improvements</b>											
		150,000	1,150,000	2,000,000	800,000	0	0	0	0	0	0	0	4,100,000
<b>Funding Source:</b>													
Local Option Gas Tax		150,000	625,000	1,000,000	400,000	0	0	0	0	0	0	0	2,175,000
Grant - State		0	525,000	1,000,000	400,000	0	0	0	0	0	0	0	1,925,000
<b>Funding Total:</b>		150,000	1,150,000	2,000,000	800,000	0	0	0	0	0	0	0	4,100,000

**Project Description:** Design and installation of ATMS/ITS on Bryan Dairy Road utilizing funds from 9th Cent Fuel Tax and FDOT Transportation Regional Incentive Program.

**Project Classifications:**

CIP Phase	Design
Location	Tarpon Springs
Originating Department	DEI Public Works
TIF District	Greater Tarpon Springs

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000325A 2265 Nursery Rd SRTS Sidewalk Improvements-Phase 2</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3026	Sidewalks Projects								
020.3	Design-Grant	20,000	40,000	0	0	0	0	0	0	0	0	0	60,000
030.3	Constr-Grant	0	350,000	0	0	0	0	0	0	0	0	0	350,000
040.3	Testing-Grant	0	10,000	0	0	0	0	0	0	0	0	0	10,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3026</i>	<i>Sidewalks Projects</i>							
		20,000	400,000	0	0	0	0	0	0	0	0	0	420,000
<b>Total for Project: 000325A</b>		<b>2265 Nursery Rd SRTS Sidewalk Improvements-Phase 2</b>											
		20,000	400,000	0	0	0	0	0	0	0	0	0	420,000
<b>Funding Source:</b>													
Grant - Federal		20,000	400,000	0	0	0	0	0	0	0	0	0	420,000
<b>Funding Total:</b>		20,000	400,000	0	0	0	0	0	0	0	0	0	420,000

**Project Description:** Install sidewalk along south side of Nursery Road as part of the Safe Routes to School Program. Project being designed by FDOT consultant and constructed by Pinellas County. Project to be done in conjunction with Phases 1A and 1B.

**Project Classifications:**

CIP Phase	Construction
Location	Largo, Belleair, Belleair Bluffs
Originating Department	DEI Public Works
TIF District	Various

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000326A 2295 SR 693 ATMS/ITS Improvements</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects								
020.3	Design-LOGT	125,000	125,000	0	0	0	0	0	0	0	0	0	250,000
020.4	Design-Grant	125,000	125,000	0	0	0	0	0	0	0	0	0	250,000
030.3	Constr-LOGT	0	400,000	1,050,000	400,000	0	0	0	0	0	0	0	1,850,000
030.4	Constr-Grant	0	400,000	1,050,000	400,000	0	0	0	0	0	0	0	1,850,000
110.3	Other-LOGT	0	400,000	150,000	0	285,000	0	0	0	0	0	0	835,000
110.4	Other-Grant	0	400,000	150,000	0	285,000	0	0	0	0	0	0	835,000
<b>Project Total for : Fund: 3001 Capital Projects</b>		<b>Center: 414100</b>	<b>CIP-Transportation</b>	<b>Program: 3021</b>	<b>Intersection Improvements Projects</b>								
		250,000	1,850,000	2,400,000	800,000	570,000	0	0	0	0	0	0	5,870,000
<b>Total for Project: 000326A 2295 SR 693 ATMS/ITS Improvements</b>		250,000	1,850,000	2,400,000	800,000	570,000	0	0	0	0	0	0	5,870,000
<b>Funding Source:</b>													
Local Option Gas Tax		125,000	925,000	1,200,000	400,000	285,000	0	0	0	0	0	0	2,935,000
Grant - State		125,000	925,000	1,200,000	400,000	285,000	0	0	0	0	0	0	2,935,000
<b>Funding Total:</b>		250,000	1,850,000	2,400,000	800,000	570,000	0	0	0	0	0	0	5,870,000

**Project Description:** Design and construct a new ATMS/ITS system on 66th Street utilizing funds from 9th Cent Fuel Tax and FDOT Transportation Regional Incentive Program TRIP.

**Project Classifications:**

CIP Phase Design  
 Location Tarpon Springs  
 Originating Department DEI Public Works  
 TIF District Greater Tarpon Springs

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000327A 2268 Sunset Point Road SRTS Sidewalk Improvements</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3026	Sidewalks Projects								
020.1	Design-Penny	20,000	25,000	0	0	0	0	0	0	0	0	0	45,000
030.1	Constr-Penny	0	363,300	0	0	0	0	0	0	0	0	0	363,300
030.4	Constr-Grant	0	76,700	0	0	0	0	0	0	0	0	0	76,700
040.1	Testing-Penny	0	5,000	0	0	0	0	0	0	0	0	0	5,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3026</i>	<i>Sidewalks Projects</i>							
		20,000	470,000	0	0	0	0	0	0	0	0	0	490,000
<b>Total for Project: 000327A</b>		<b>2268 Sunset Point Road SRTS Sidewalk Improvements</b>											
		20,000	470,000	0	0	0	0	0	0	0	0	0	490,000
<b>Funding Source:</b>													
Penny for Pinellas		20,000	393,300	0	0	0	0	0	0	0	0	0	413,300
Grant - Federal		0	76,700	0	0	0	0	0	0	0	0	0	76,700
<b>Funding Total:</b>		20,000	470,000	0	0	0	0	0	0	0	0	0	490,000

**Project Description:** Install sidewalk primarily along south side of Sunset Point Road as part of the Safe Routes to School Program. Project will be designed and constructed by Pinellas County.

**Project Classifications:**

CIP Phase	Design
Location	Clearwater
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Various

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000328A 2298 Pinellas Trail Rehabilitation Phase II</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3023	Pinellas Trail Projects								
020.1	Design-Penny	10,000	0	0	0	0	0	0	0	0	0	0	10,000
030.1	Constr-Penny	30,142	0	0	0	0	0	0	0	0	0	0	30,142
030.3	Constr-Grant	469,858	0	0	0	0	0	0	0	0	0	0	469,858
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3023</i>	<i>Pinellas Trail Projects</i>							
		510,000	0	0	0	0	0	0	0	0	0	0	510,000
<b>Total for Project: 000328A</b>		<b>2298 Pinellas Trail Rehabilitation Phase II</b>											
		510,000	0	0	0	0	0	0	0	0	0	0	510,000
<b>Funding Source:</b>													
Penny for Pinellas		40,142	0	0	0	0	0	0	0	0	0	0	40,142
Grant - Federal		469,858	0	0	0	0	0	0	0	0	0	0	469,858
<b>Funding Total:</b>		510,000	0	0	0	0	0	0	0	0	0	0	510,000

**Project Description:** Repair and resurface the Pinellas Trail from Michigan Blvd. to Oceanview Ave. using Transportation Enhancement Program Funding.

**Project Classifications:**

CIP Phase	Construction
Location	Dunedin
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Greater Tarpon Springs

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation      Budget Type Code: Planning      Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000329A    2269 Union St SRTS Sidewalk Improvements</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3026	Sidewalks Projects								
020.1	Design-Penny	20,000	25,000	0	0	0	0	0	0	0	0	0	45,000
030.1	Constr-Penny	0	224,200	0	0	0	0	0	0	0	0	0	224,200
030.3	Constr-Grant	0	232,800	0	0	0	0	0	0	0	0	0	232,800
040.3	Testing-Penny	0	2,000	0	0	0	0	0	0	0	0	0	2,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3026</i>	<i>Sidewalks Projects</i>							
		20,000	484,000	0	0	0	0	0	0	0	0	0	504,000
<b>Total for Project: 000329A</b>		<b>2269 Union St SRTS Sidewalk Improvements</b>											
		20,000	484,000	0	0	0	0	0	0	0	0	0	504,000
<b>Funding Source:</b>													
Penny for Pinellas		20,000	251,200	0	0	0	0	0	0	0	0	0	271,200
Grant - Federal		0	232,800	0	0	0	0	0	0	0	0	0	232,800
<b>Funding Total:</b>		20,000	484,000	0	0	0	0	0	0	0	0	0	504,000

**Project Description:** Install sidewalk along south side of Union Street as part of the Safe Routes to School Program. Project will be designed and constructed by Pinellas County.

**Project Classifications:**

CIP Phase	Design
Location	Dunedin
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Various

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000330A 2267 Nursery Rd SRTS Sidewalk Improvements-Ph 1A</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3026	Sidewalks Projects								
020.3	Design-Grant	60,000	10,000	0	0	0	0	0	0	0	0	0	70,000
030.3	Constr-Grant	550,000	50,000	0	0	0	0	0	0	0	0	0	600,000
040.3	Testing-Grant	5,000	5,000	0	0	0	0	0	0	0	0	0	10,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3026</i>	<i>Sidewalks Projects</i>							
		615,000	65,000	0	0	0	0	0	0	0	0	0	680,000
<b>Total for Project: 000330A</b>		<b>2267 Nursery Rd SRTS Sidewalk Improvements-Ph 1A</b>											
		615,000	65,000	0	0	0	0	0	0	0	0	0	680,000
<b>Funding Source:</b>													
Grant - Federal		615,000	65,000	0	0	0	0	0	0	0	0	0	680,000
<b>Funding Total:</b>		615,000	65,000	0	0	0	0	0	0	0	0	0	680,000

**Project Description:** Install sidewalk along south side of Nursery Road as part of the Safe Route to School Program. Project being designed by FDOT consultant and constructed by Pinellas County. Project to be done in conjunction with Phases 1B and 2.

**Project Classifications:**

CIP Phase	Construction
Location	Largo, Belleair, Belleair Bluffs
Originating Department	DEI Public Works
TIF District	Various

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000332A 2266 Nursery Rd SRTS Sidewalk Improvements-Ph 1B</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3026	Sidewalks Projects								
020.3	Design-Grant	50,000	40,000	0	0	0	0	0	0	0	0	0	90,000
030.3	Constr-Grant	300,000	100,000	0	0	0	0	0	0	0	0	0	400,000
040.3	Testing-Grant	5,000	5,000	0	0	0	0	0	0	0	0	0	10,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3026</i>	<i>Sidewalks Projects</i>							
		355,000	145,000	0	0	0	0	0	0	0	0	0	500,000
<b>Total for Project: 000332A</b>		<b>2266 Nursery Rd SRTS Sidewalk Improvements-Ph 1B</b>											
		355,000	145,000	0	0	0	0	0	0	0	0	0	500,000
<b>Funding Source:</b>													
Grant - Federal		355,000	145,000	0	0	0	0	0	0	0	0	0	500,000
<b>Funding Total:</b>		355,000	145,000	0	0	0	0	0	0	0	0	0	500,000

**Project Description:** Install sidewalk along south side of Nursery Road as part of the Safe Routes to school Program. Project being designed by FDOT consultant and constructed by Pinellas County. Project to be done in conjunction with Phases 1A and 2.

**Project Classifications:**

CIP Phase	Construction
Location	Largo, Belleair, Belleair Bluffs
Originating Department	DEI Public Works
TIF District	Various



## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000343A 1938 Belleair Rd at Keene Rd Intersection Improvements</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects								
020.1	Design-Penny	96,440	0	0	0	0	0	0	0	0	0	0	96,440
030.1	Constr-Penny	350,000	0	0	0	0	0	0	0	0	0	0	350,000
030.2	Constr-TIF	300,000	0	0	0	0	0	0	0	0	0	0	300,000
030.3	Constr-Grant	450,000	0	0	0	0	0	0	0	0	0	0	450,000
040.1	Testing-Penny	5,000	0	0	0	0	0	0	0	0	0	0	5,000
<b>Project Total for : Fund: 3001 Capital Projects</b>		<b>Center: 414100</b>	<b>CIP-Transportation</b>	<b>Program: 3021</b>	<b>Intersection Improvements Projects</b>								
		1,201,440	0	0	0	0	0	0	0	0	0	0	1,201,440
<b>Total for Project: 000343A 1938 Belleair Rd at Keene Rd Intersection Improvements</b>		1,201,440	0	0	0	0	0	0	0	0	0	0	1,201,440
<b>Funding Source:</b>													
Grant - State		450,000	0	0	0	0	0	0	0	0	0	0	450,000
Impact Fees		300,000	0	0	0	0	0	0	0	0	0	0	300,000
Penny for Pinellas		451,440	0	0	0	0	0	0	0	0	0	0	451,440
<b>Funding Total:</b>		1,201,440	0	0	0	0	0	0	0	0	0	0	1,201,440

**Project Description:** Construct left and right turn lanes on Belleair Road at Keene Road.

**Project Classifications:**

CIP Phase	Construction
Location	Largo, Belleair, Belleair Bluffs
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Various

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000404A US 19 Mid-County ATMS/ITS Improvements from 49th St to 126th Ave</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects								
020.4	Design Grant	100,000	80,000	0	0	0	0	0	0	0	0	0	180,000
030.4	Constr-Grant	0	370,000	0	0	0	0	0	0	0	0	0	370,000
110.4	Other-Grant	0	400,000	0	0	0	0	0	0	0	0	0	400,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3021</i>	<i>Intersection Improvements Projects</i>							
		100,000	850,000	0	0	0	0	0	0	0	0	0	950,000
<b>Total for Project: 000404A</b>		<b>US 19 Mid-County ATMS/ITS Improvements from 49th St to 126th Ave</b>											
		100,000	850,000	0	0	0	0	0	0	0	0	0	950,000
<b>Funding Source:</b>													
Grant - State		100,000	850,000	0	0	0	0	0	0	0	0	0	950,000
<b>Funding Total:</b>		100,000	850,000	0	0	0	0	0	0	0	0	0	950,000

**Project Description:** Installation of fiber optic cable and ATMS/ITS equipment, including dynamic message signs, and intergration into the countywide ATMS/ITS system on US 19 from 49th St N to 126th Ave N.

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Construction
County Road Corridor	Not Applicable
Location	Countywide
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Various

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000405A 2093 North Fiber Loop ATMS</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects								
030.3	Constr-LOGT	135,000	50,000	0	0	0	0	0	0	0	0	0	185,000
030.4	Constr-Grant	135,000	50,000	0	0	0	0	0	0	0	0	0	185,000
110.1	Other-LOGT	100,000	0	0	0	0	0	0	0	0	0	0	100,000
110.2	Other-Grant	100,000	0	0	0	0	0	0	0	0	0	0	100,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3021</i>	<i>Intersection Improvements Projects</i>							
		470,000	100,000	0	0	0	0	0	0	0	0	0	570,000
<b>Total for Project: 000405A</b>		<b>2093 North Fiber Loop ATMS</b>											
		470,000	100,000	0	0	0	0	0	0	0	0	0	570,000
<b>Funding Source:</b>													
Local Option Gas Tax		235,000	50,000	0	0	0	0	0	0	0	0	0	285,000
Grant - State		235,000	50,000	0	0	0	0	0	0	0	0	0	285,000
<b>Funding Total:</b>		470,000	100,000	0	0	0	0	0	0	0	0	0	570,000

**Project Description:** Installation of fiber optic cable and ATMS/ITS equipment and integration into the countywide ATMS/ITS system. (Name correction, previously US19 North ATMS/ITS Improvements which is now 001473A)

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Construction
County Road Corridor	Not Applicable
Location	Countywide
Originating Department	DEI Public Works
TIF District	Various

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

	Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total	
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000423A Dunedin Causeway Bridge Project Development &amp; Environment (PD&amp;E) Study</b>													
Fund: 3001 020.1	Capital Projects Design-Penny	Center: 414100 0	CIP-Transportation 100,000	Program: 3031 300,000	Bridges-Repair & Improvement 300,000	300,000	300,000	0	0	0	0	0	1,000,000
<b>Project Total for : Fund: 3001 Capital Projects Center: 414100 CIP-Transportation Program: 3031 Bridges-Repair &amp; Improvement</b>													
		0	100,000	300,000	300,000	300,000	0	0	0	0	0	0	1,000,000
<b>Total for Project: 000423A Dunedin Causeway Bridge Project Development &amp; Environment (PD&amp;E) Study</b>													
		0	100,000	300,000	300,000	300,000	0	0	0	0	0	0	1,000,000
<b>Funding Source:</b>													
	Penny for Pinellas	0	100,000	300,000	300,000	300,000	0	0	0	0	0	0	1,000,000
<b>Funding Total:</b>													
		0	100,000	300,000	300,000	300,000	0	0	0	0	0	0	1,000,000

**Project Description:** Project Development & Environment Study (PD&E) in FY14 - FY17 to determine the type of improvements or replacements necessary.

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Construction
County Road Corridor	Not Applicable
Location	Dunedin
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Greater Dunedin Area

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000697A Park Street (CRI) bridge Widening over Cross Bayou Canal</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3031	Bridges-Repair & Improvement								
020.1	Design-Penny	0	75,000	75,000	50,000	0	0	0	0	0	0	0	200,000
030.1	Constr-Penny	0	0	500,000	500,000	0	0	0	0	0	0	0	1,000,000
040.1	Testing-Penny	0	0	10,000	10,000	0	0	0	0	0	0	0	20,000
<b>Project Total for : Fund: 3001 Capital Projects</b>		<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3031</i>	<i>Bridges-Repair &amp; Improvement</i>								
		0	75,000	585,000	560,000	0	0	0	0	0	0	0	1,220,000
<b>Total for Project: 000697A Park Street (CRI) bridge Widening over Cross Bayou Canal</b>		0	75,000	585,000	560,000	0	0	0	0	0	0	0	1,220,000
<b>Funding Source:</b>													
Penny for Pinellas		0	75,000	585,000	560,000	0	0	0	0	0	0	0	1,220,000
<b>Funding Total:</b>		0	75,000	585,000	560,000	0	0	0	0	0	0	0	1,220,000

**Project Description:** Widening the northbound and southbound existing bridges on Park Street, to include sidewalks, shoulders and bike lanes, in accordance with traffic safety study. (# 150071 & 150139)

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Construction
County Road Corridor	Not Applicable
Location	Seminole
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Various

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000700A Westwinds Drive Bridge Replacement over Westwind Canal</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3031	Bridges-Repair & Improvement								
020.1	Design-Penny	0	25,000	75,000	50,000	50,000	50,000	0	0	0	0	0	250,000
030.1	Constr-Penny	0	0	0	0	400,000	400,000	0	0	0	0	0	800,000
040.1	Testing-Penny	0	0	0	0	5,000	5,000	0	0	0	0	0	10,000
<b>Project Total for : Fund: 3001 Capital Projects</b>		<b>Center: 414100</b>	<b>CIP-Transportation</b>	<b>Program: 3031</b>	<b>Bridges-Repair &amp; Improvement</b>								
		0	25,000	75,000	50,000	455,000	455,000	0	0	0	0	0	1,060,000
<b>Total for Project: 000700A Westwinds Drive Bridge Replacement over Westwind Canal</b>		0	25,000	75,000	50,000	455,000	455,000	0	0	0	0	0	1,060,000
<b>Funding Source:</b>													
Penny for Pinellas		0	25,000	75,000	50,000	455,000	455,000	0	0	0	0	0	1,060,000
<b>Funding Total:</b>		0	25,000	75,000	50,000	455,000	455,000	0	0	0	0	0	1,060,000

**Project Description:** Bridge replacement (# 154003)

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Construction
County Road Corridor	Not Applicable
Location	Tarpon Springs
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Various

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000702A Crosswinds Drive Bridge Replacement over Crosswinds Canal</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3031	Bridges-Repair & Improvement								
020.1	Design-Penny	0	150,000	50,000	50,000	0	0	0	0	0	0	0	250,000
030.1	Constr-Penny	0	0	400,000	400,000	0	0	0	0	0	0	0	800,000
040.1	Testing-Penny	0	0	5,000	5,000	0	0	0	0	0	0	0	10,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3031</i>	<i>Bridges-Repair &amp; Improvement</i>							
		0	150,000	455,000	455,000	0	0	0	0	0	0	0	1,060,000
<b>Total for Project: 000702A</b>		<b>Crosswinds Drive Bridge Replacement over Crosswinds Canal</b>											
		0	150,000	455,000	455,000	0	0	0	0	0	0	0	1,060,000
<b>Funding Source:</b>													
Penny for Pinellas		0	150,000	455,000	455,000	0	0	0	0	0	0	0	1,060,000
<b>Funding Total:</b>		0	150,000	455,000	455,000	0	0	0	0	0	0	0	1,060,000

**Project Description:** Bridge replacement (# 154004)

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Construction
County Road Corridor	Not Applicable
Location	Tarpon Springs
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Various

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000965A 1624 Arterial Road Improvement Program</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3020	Arterial Roads Projects								
030.1	Constr-Penny	0	0	0	0	0	1,054,000	2,012,000	0	0	0	0	3,066,000
030.5	Constr-Unfunded	0	0	0	0	0	0	0	750,000	1,000,000	1,000,000	1,000,000	3,750,000
<b>Project Total for : Fund: 3001 Capital Projects</b>		<b>Center: 414100</b>	<b>CIP-Transportation</b>	<b>Program: 3020</b>	<b>Arterial Roads Projects</b>								
		0	0	0	0	0	1,054,000	2,012,000	750,000	1,000,000	1,000,000	1,000,000	6,816,000
<b>Total for Project: 000965A 1624 Arterial Road Improvement Program</b>		0	0	0	0	0	1,054,000	2,012,000	750,000	1,000,000	1,000,000	1,000,000	6,816,000
<b>Funding Source:</b>													
Unfunded		0	0	0	0	0	0	0	750,000	1,000,000	1,000,000	1,000,000	3,750,000
Penny for Pinellas		0	0	0	0	0	1,054,000	2,012,000	0	0	0	0	3,066,000
<b>Funding Total:</b>		0	0	0	0	0	1,054,000	2,012,000	750,000	1,000,000	1,000,000	1,000,000	6,816,000

**Project Description:** Funding provided for various arterial roadway improvements. Projects to be selected from prioritized list.

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Construction
County Road Corridor	CR 1
Location	Countywide
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Various



## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

	Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total	
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000966A 1533 Countywide Road Improvement Program</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3022	Local Streets/Collector Projects								
030.1	Constr-Penny	400,000	300,000	416,000	400,000	400,000	554,000	2,612,000	100,000	0	0	0	5,182,000
030.5	Constr-Unfunded	0	0	0	0	0	0	400,000	400,000	400,000	400,000	1,600,000	
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3022</i>	<i>Local Streets/Collector Projects</i>							
		400,000	300,000	416,000	400,000	400,000	554,000	2,612,000	500,000	400,000	400,000	400,000	6,782,000
<b>Total for Project: 000966A</b>		<b>1533 Countywide Road Improvement Program</b>											
		400,000	300,000	416,000	400,000	400,000	554,000	2,612,000	500,000	400,000	400,000	400,000	6,782,000
<b>Funding Source:</b>													
Unfunded		0	0	0	0	0	0	400,000	400,000	400,000	400,000	1,600,000	
Penny for Pinellas		400,000	300,000	416,000	400,000	400,000	554,000	2,612,000	100,000	0	0	0	5,182,000
<b>Funding Total:</b>		400,000	300,000	416,000	400,000	400,000	554,000	2,612,000	500,000	400,000	400,000	400,000	6,782,000

**Project Description:** Funding provided for various Countywide road improvements. Projects to be selected from prioritized list.

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Construction
County Road Corridor	Not Applicable
Location	Countywide
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Countywide

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation      Budget Type Code: Planning      Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000967A    2351 Pinellas Trail Extension Program</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3023	Pinellas Trail Projects								
030.1	Constr-Penny	0	0	0	0	3,077,100	0	0	0	0	0	0	3,077,100
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3023</i>	<i>Pinellas Trail Projects</i>							
		0	0	0	0	3,077,100	0	0	0	0	0	0	3,077,100
<b>Total for Project: 000967A    2351 Pinellas Trail Extension Program</b>		0	0	0	0	3,077,100	0	0	0	0	0	0	3,077,100
<b>Funding Source:</b>													
	Penny for Pinellas	0	0	0	0	3,077,100	0	0	0	0	0	0	3,077,100
<b>Funding Total:</b>		0	0	0	0	3,077,100	0	0	0	0	0	0	3,077,100

**Project Description:** Pinellas Trail Extension Program

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Construction
County Road Corridor	Not Applicable
Location	Countywide
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation      Budget Type Code: Planning      Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 000984A    2183 Friendship Trail Bridge Demolition</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3029	Friendship Trail Program								
030.1	Constr-Penny	0	515,000	0	0	0	0	0	0	0	0	0	515,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3029</i>	<i>Friendship Trail Program</i>							
		0	515,000	0	0	0	0	0	0	0	0	0	515,000
<b>Total for Project: 000984A</b>		<b>2183 Friendship Trail Bridge Demolition</b>											
		0	515,000	0	0	0	0	0	0	0	0	0	515,000
<b>Funding Source:</b>													
	Penny for Pinellas	0	515,000	0	0	0	0	0	0	0	0	0	515,000
<b>Funding Total:</b>		0	515,000	0	0	0	0	0	0	0	0	0	515,000

**Project Description:** Pinellas County contributory funds to demolish the Friendship Trail Bridge.

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Construction
County Road Corridor	Not Applicable
Location	St Petersburg
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 001018A Betty Lane at Sunset Point Road - Intersection Improvements</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects								
010.1	Acquis-Penny	50,000	0	0	0	0	0	0	0	0	0	0	50,000
020.1	Design-Penny	0	50,000	200,000	80,000	80,000	0	0	0	0	0	0	410,000
030.1	Constr-Penny	0	0	0	800,000	800,000	0	0	0	0	0	0	1,600,000
040.1	Testing-Penny	0	0	0	5,000	5,000	0	0	0	0	0	0	10,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3021</i>	<i>Intersection Improvements Projects</i>							
		50,000	50,000	200,000	885,000	885,000	0	0	0	0	0	0	2,070,000
<b>Total for Project: 001018A</b>		<b>Betty Lane at Sunset Point Road - Intersection Improvements</b>											
		50,000	50,000	200,000	885,000	885,000	0	0	0	0	0	0	2,070,000
<b>Funding Source:</b>													
Penny for Pinellas		50,000	50,000	200,000	885,000	885,000	0	0	0	0	0	0	2,070,000
<b>Funding Total:</b>		50,000	50,000	200,000	885,000	885,000	0	0	0	0	0	0	2,070,000

**Project Description:** Intersection Improvements including construction of left turn lanes and bridge reconstruction at Sunset Point Road.

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Design
County Road Corridor	Not Applicable
Location	Clearwater
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Countywide

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 001019A 113th Street N at 86th Avenue N Intersection Improvements</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects								
010.1	Acq-Penny	10,000	60,000	0	0	0	0	0	0	0	0	0	70,000
020.1	Design-Penny	0	50,000	50,000	50,000	0	0	0	0	0	0	0	150,000
030.1	Constr-Penny	0	0	500,000	250,000	0	0	0	0	0	0	0	750,000
040.1	Testing-Penny	0	0	5,000	5,000	0	0	0	0	0	0	0	10,000
<b>Project Total for : Fund: 3001 Capital Projects</b>		<b>Center: 414100</b>	<b>CIP-Transportation</b>	<b>Program: 3021</b>	<b>Intersection Improvements Projects</b>								
		10,000	110,000	555,000	305,000	0	0	0	0	0	0	0	980,000
<b>Total for Project: 001019A 113th Street N at 86th Avenue N Intersection Improvements</b>		10,000	110,000	555,000	305,000	0	0	0	0	0	0	0	980,000
<b>Funding Source:</b>													
Penny for Pinellas		10,000	110,000	555,000	305,000	0	0	0	0	0	0	0	980,000
<b>Funding Total:</b>		10,000	110,000	555,000	305,000	0	0	0	0	0	0	0	980,000

**Project Description:** Intersection Improvements including construction of east bound and west bound left turn lanes at 113th Street N and 86th Avenue N.

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Design
County Road Corridor	Not Applicable
Location	Seminole
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Countywide

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 001020A N.E. Coachman Road at Coachman Road Intersection Improvements</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects								
020.1	Design-Penny	0	0	150,000	100,000	0	0	0	0	0	0	0	250,000
030.1	Constr-Penny	0	0	0	900,000	0	0	0	0	0	0	0	900,000
040.1	Testing-Penny	0	0	0	10,000	0	0	0	0	0	0	0	10,000
<b>Project Total for : Fund: 3001 Capital Projects</b>		<b>Center: 414100</b>	<b>CIP-Transportation</b>	<b>Program: 3021</b>	<b>Intersection Improvements Projects</b>								
		0	0	150,000	1,010,000	0	0	0	0	0	0	0	1,160,000
<b>Total for Project: 001020A N.E. Coachman Road at Coachman Road Intersection Improvements</b>		0	0	150,000	1,010,000	0	0	0	0	0	0	0	1,160,000
<b>Funding Source:</b>													
Penny for Pinellas		0	0	150,000	1,010,000	0	0	0	0	0	0	0	1,160,000
<b>Funding Total:</b>		0	0	150,000	1,010,000	0	0	0	0	0	0	0	1,160,000

**Project Description:** Intersection improvements including construction of left turn lanes.

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Design
County Road Corridor	Not Applicable
Location	Clearwater
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Countywide

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation      Budget Type Code: Planning      Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 001021A      Belcher Road at Belleair Road Intersection Improvements</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects								
020.1	Design-Penny	45,000	45,000	0	0	0	0	0	0	0	0	0	90,000
020.4	Design-Grant	45,000	45,000	0	0	0	0	0	0	0	0	0	90,000
030.1	Constr-Penny	0	0	895,000	0	0	0	0	0	0	0	0	895,000
030.4	Constr-Grant	0	0	895,000	0	0	0	0	0	0	0	0	895,000
040.1	Testing-Penny	0	0	5,000	0	0	0	0	0	0	0	0	5,000
040.4	Testing-Grant	0	0	5,000	0	0	0	0	0	0	0	0	5,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3021</i>	<i>Intersection Improvements Projects</i>							
		90,000	90,000	1,800,000	0	0	0	0	0	0	0	0	1,980,000
<b>Total for Project: 001021A</b>		<b>Belcher Road at Belleair Road Intersection Improvements</b>											
		90,000	90,000	1,800,000	0	0	0	0	0	0	0	0	1,980,000
<b>Funding Source:</b>													
Grant - State		45,000	45,000	900,000	0	0	0	0	0	0	0	0	990,000
Penny for Pinellas		45,000	45,000	900,000	0	0	0	0	0	0	0	0	990,000
<b>Funding Total:</b>		90,000	90,000	1,800,000	0	0	0	0	0	0	0	0	1,980,000

**Project Description:** Intersection improvements including right turn lanes on the east and west legs of Belleair Road and extend the left turn lane on the east leg.

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Design
County Road Corridor	Not Applicable
Location	Largo, Belleair, Belleair Bluffs
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Countywide

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 001022A 119th Street at 102nd Avenue N - Antilles Drive, Hamlin Blvd. and 118th Street at 102nd Avenue N - Intersection Improvements</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects								
020.1	Design-Penny	0	20,000	20,000	150,000	70,000	30,000	0	0	0	0	0	290,000
030.1	Constr-Penny	0	0	0	0	700,000	300,000	0	0	0	0	0	1,000,000
040.1	Testing-Penny	0	0	0	0	10,000	5,000	0	0	0	0	0	15,000
<b>Project Total for : Fund: 3001 Capital Projects</b>		<b>Center: 414100</b>	<b>CIP-Transportation</b>	<b>Program: 3021</b>	<b>Intersection Improvements Projects</b>								
		0	20,000	20,000	150,000	780,000	335,000	0	0	0	0	0	1,305,000
<b>Total for Project: 001022A 119th Street at 102nd Avenue N - Antilles Drive, Hamlin Blvd. and 118th Street at 102nd Avenue N - Intersection Improvements</b>													
		0	20,000	20,000	150,000	780,000	335,000	0	0	0	0	0	1,305,000
<b>Funding Source:</b>													
Penny for Pinellas		0	20,000	20,000	150,000	780,000	335,000	0	0	0	0	0	1,305,000
<b>Funding Total:</b>		0	20,000	20,000	150,000	780,000	335,000	0	0	0	0	0	1,305,000

**Project Description:** Intersection improvements including construction of median refuge and intersection widening at four locations at 119th and 118th Streets.

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Design
County Road Corridor	Not Applicable
Location	Seminole
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Countywide



## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 001023A 131st Street N at 82nd Avenue N and 86th Avenue N Intersection Improvements</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects								
020.1	Design-Penny	50,000	150,000	75,000	75,000	0	0	0	0	0	0	0	350,000
030.1	Constr-Penny	0	0	750,000	750,000	0	0	0	0	0	0	0	1,500,000
040.1	Testing-Penny	0	0	5,000	5,000	0	0	0	0	0	0	0	10,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3021</i>	<i>Intersection Improvements Projects</i>							
		50,000	150,000	830,000	830,000	0	0	0	0	0	0	0	1,860,000
<b>Total for Project: 001023A</b>		<b>131st Street N at 82nd Avenue N and 86th Avenue N Intersection Improvements</b>											
		50,000	150,000	830,000	830,000	0	0	0	0	0	0	0	1,860,000
<b>Funding Source:</b>													
Penny for Pinellas		50,000	150,000	830,000	830,000	0	0	0	0	0	0	0	1,860,000
<b>Funding Total:</b>		50,000	150,000	830,000	830,000	0	0	0	0	0	0	0	1,860,000

**Project Description:** Intersection improvements including construction of left turn lanes.

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Design
County Road Corridor	Not Applicable
Location	Seminole
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Countywide

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 001024A 62nd Avenue N at 25th Street N and 28th Street N Intersection Improvements</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects								
020.1	Design-Penny	50,000	80,000	20,000	0	0	0	0	0	0	0	0	150,000
030.1	Constr-Penny	0	800,000	200,000	0	0	0	0	0	0	0	0	1,000,000
040.1	Testing-Penny	0	5,000	5,000	0	0	0	0	0	0	0	0	10,000
<b>Project Total for : Fund: 3001 Capital Projects</b>		<b>Center: 414100</b>	<b>CIP-Transportation</b>	<b>Program: 3021</b>	<b>Intersection Improvements Projects</b>								
		50,000	885,000	225,000	0	0	0	0	0	0	0	0	1,160,000
<b>Total for Project: 001024A 62nd Avenue N at 25th Street N and 28th Street N Intersection Improvements</b>		50,000	885,000	225,000	0	0	0	0	0	0	0	0	1,160,000
<b>Funding Source:</b>													
Penny for Pinellas		50,000	885,000	225,000	0	0	0	0	0	0	0	0	1,160,000
<b>Funding Total:</b>		50,000	885,000	225,000	0	0	0	0	0	0	0	0	1,160,000

**Project Description:** Intersection improvements including construction of mast arms and ADA upgrades.

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Design
County Road Corridor	Not Applicable
Location	Lealman/Kenneth City
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Countywide

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 001025A 38th Avenue N at 58th Street N Intersection Improvements</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects								
020.1	Design-Penny	50,000	40,000	25,000	0	0	0	0	0	0	0	0	115,000
030.1	Constr-Penny	0	425,000	250,000	0	0	0	0	0	0	0	0	675,000
040.1	Testing-Penny	0	5,000	5,000	0	0	0	0	0	0	0	0	10,000
<b>Project Total for : Fund: 3001 Capital Projects</b>		<b>Center: 414100</b>	<b>CIP-Transportation</b>	<b>Program: 3021</b>	<b>Intersection Improvements Projects</b>								
		50,000	470,000	280,000	0	0	0	0	0	0	0	0	800,000
<b>Total for Project: 001025A 38th Avenue N at 58th Street N Intersection Improvements</b>		50,000	470,000	280,000	0	0	0	0	0	0	0	0	800,000
<b>Funding Source:</b>													
Penny for Pinellas		50,000	470,000	280,000	0	0	0	0	0	0	0	0	800,000
<b>Funding Total:</b>		50,000	470,000	280,000	0	0	0	0	0	0	0	0	800,000

**Project Description:** Intersection improvements including construction of mast arms, ADA upgrades and traffic flow improvements.

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Design
County Road Corridor	Not Applicable
Location	St Petersburg
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Countywide

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 001028A CR 1 Sidewalk from SR 580 to Curlew Road</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3026	Sidewalks Projects								
020.4	Design-Grant	50,000	80,000	80,000	0	0	0	0	0	0	0	0	210,000
030.4	Construction-Grant	0	0	540,000	400,000	0	0	0	0	0	0	0	940,000
040.4	Testing-Grant	0	0	2,000	2,000	0	0	0	0	0	0	0	4,000
<b>Project Total for : Fund: 3001 Capital Projects</b>		<b>Center: 414100</b>	<b>CIP-Transportation</b>	<b>Program: 3026</b>	<b>Sidewalks Projects</b>								
		50,000	80,000	622,000	402,000	0	0	0	0	0	0	0	1,154,000
<b>Total for Project: 001028A CR 1 Sidewalk from SR 580 to Curlew Road</b>		50,000	80,000	622,000	402,000	0	0	0	0	0	0	0	1,154,000
<b>Funding Source:</b>													
Grant - State		50,000	80,000	622,000	402,000	0	0	0	0	0	0	0	1,154,000
<b>Funding Total:</b>		50,000	80,000	622,000	402,000	0	0	0	0	0	0	0	1,154,000

**Project Description:** Sidewalk construction - Safe Route to School (SRTS).

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Design
County Road Corridor	Not Applicable
Location	Dunedin
Originating Department	DEI Public Works
TIF District	Countywide

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 001029A Hercules Avenue Sidewalk from Sunset Point Road to Belcher Road</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3026	Sidewalks Projects								
020.4	Design-Grant	50,000	60,000	20,000	0	0	0	0	0	0	0	0	130,000
030.4	Constr-Grant	0	0	750,000	0	0	0	0	0	0	0	0	750,000
040.4	Testing-Grant	0	0	10,000	0	0	0	0	0	0	0	0	10,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3026</i>	<i>Sidewalks Projects</i>							
		50,000	60,000	780,000	0	0	0	0	0	0	0	0	890,000
<b>Total for Project: 001029A</b>		<b>Hercules Avenue Sidewalk from Sunset Point Road to Belcher Road</b>											
		50,000	60,000	780,000	0	0	0	0	0	0	0	0	890,000
<b>Funding Source:</b>													
Grant - State		50,000	60,000	780,000	0	0	0	0	0	0	0	0	890,000
<b>Funding Total:</b>		50,000	60,000	780,000	0	0	0	0	0	0	0	0	890,000

**Project Description:** Sidewalk construction - Safe Route to School (SRTS).

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Design
County Road Corridor	Not Applicable
Location	Clearwater
Originating Department	DEI Public Works
TIF District	Countywide

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 001030A South Belcher Road ATMS Project</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects								
020.3	Design-LOGT	0	175,000	0	0	0	0	0	0	0	0	0	175,000
020.4	Design-Grant	0	175,000	0	0	0	0	0	0	0	0	0	175,000
030.3	Constr-LOGT	0	0	150,000	625,000	225,000	0	0	0	0	0	0	1,000,000
030.4	Constr-Grant	0	0	150,000	625,000	225,000	0	0	0	0	0	0	1,000,000
110.3	Other-LOGT	0	75,000	300,000	200,000	0	0	0	0	0	0	0	575,000
110.4	Other-Grant	0	75,000	300,000	200,000	0	0	0	0	0	0	0	575,000
<b>Project Total for : Fund: 3001 Capital Projects</b>		<b>Center: 414100</b>	<b>CIP-Transportation</b>	<b>Program: 3021</b>	<b>Intersection Improvements Projects</b>								
		0	500,000	900,000	1,650,000	450,000	0	0	0	0	0	0	3,500,000
<b>Total for Project: 001030A South Belcher Road ATMS Project</b>		0	500,000	900,000	1,650,000	450,000	0	0	0	0	0	0	3,500,000
<b>Funding Source:</b>													
Local Option Gas Tax		0	250,000	450,000	825,000	225,000	0	0	0	0	0	0	1,750,000
Grant - State		0	250,000	450,000	825,000	225,000	0	0	0	0	0	0	1,750,000
<b>Funding Total:</b>		0	500,000	900,000	1,650,000	450,000	0	0	0	0	0	0	3,500,000

**Project Description:** Design and construct ATMS/ITS improvements on South Belcher Road from Druid Road to Park Boulevard.

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Design
County Road Corridor	Not Applicable
Location	Pinellas Park
Originating Department	DEI Public Works

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 001031A Gulf Boulevard ATMS</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3033	Advanced Traffic Management System								
020.6	Design-LOGT	0	125,000	100,000	0	0	0	0	0	0	0	0	225,000
020.7	Design-Grant	0	125,000	100,000	0	0	0	0	0	0	0	0	225,000
030.6	Constr-LOGT	0	0	0	400,000	875,000	0	0	0	0	0	0	1,275,000
030.7	Constr-Grant	0	0	0	400,000	875,000	0	0	0	0	0	0	1,275,000
110.6	Other-LOGT	0	0	300,000	200,000	0	0	0	0	0	0	0	500,000
110.7	Other-Grant	0	0	300,000	200,000	0	0	0	0	0	0	0	500,000
<b>Project Total for : Fund: 3001 Capital Projects</b>		<b>Center: 414100</b>	<b>CIP-Transportation</b>	<b>Program: 3033</b>	<b>Advanced Traffic Management System</b>								
		0	250,000	800,000	1,200,000	1,750,000	0	0	0	0	0	0	4,000,000
<b>Total for Project: 001031A Gulf Boulevard ATMS</b>		0	250,000	800,000	1,200,000	1,750,000	0	0	0	0	0	0	4,000,000
<b>Funding Source:</b>													
Local Option Gas Tax		0	125,000	400,000	600,000	875,000	0	0	0	0	0	0	2,000,000
Grant - State		0	125,000	400,000	600,000	875,000	0	0	0	0	0	0	2,000,000
<b>Funding Total:</b>		0	250,000	800,000	1,200,000	1,750,000	0	0	0	0	0	0	4,000,000

**Project Description:** Design and construct ATMS/ITS improvements on Gulf Boulevard.

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Design
County Road Corridor	Not Applicable
Location	Gulf Beaches
Originating Department	DEI Public Works

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 001032A ATMS/ITS Regional Improvements</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects								
020.3	Testing-LOGT	100,000	100,000	100,000	100,000	0	0	0	0	0	0	0	400,000
030.3	Constr-LOGT	400,000	400,000	400,000	400,000	0	0	0	0	0	0	0	1,600,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3021</i>	<i>Intersection Improvements Projects</i>							
		500,000	500,000	500,000	500,000	0	0	0	0	0	0	0	2,000,000
<b>Total for Project: 001032A</b>		<b>ATMS/ITS Regional Improvements</b>											
		500,000	500,000	500,000	500,000	0	0	0	0	0	0	0	2,000,000
<b>Funding Source:</b>													
Local Option Gas Tax		500,000	500,000	500,000	500,000	0	0	0	0	0	0	0	2,000,000
<b>Funding Total:</b>		500,000	500,000	500,000	500,000	0	0	0	0	0	0	0	2,000,000

**Project Description:** Construction funding to install ATMS/ITS improvements at various locations.

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Construction
County Road Corridor	Not Applicable
Location	Countywide
Originating Department	DEI Public Works



## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 001033A Bayside Bridge Rehabilitation</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3031	Bridges-Repair & Improvement								
030.1	Constr-Penny	100,000	320,000	0	0	0	0	0	0	0	0	0	420,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3031</i>	<i>Bridges-Repair &amp; Improvement</i>							
		100,000	320,000	0	0	0	0	0	0	0	0	0	420,000
<b>Total for Project: 001033A</b>		<b>Bayside Bridge Rehabilitation</b>											
		100,000	320,000	0	0	0	0	0	0	0	0	0	420,000
<b>Funding Source:</b>													
	Penny for Pinellas	100,000	320,000	0	0	0	0	0	0	0	0	0	420,000
<b>Funding Total:</b>		100,000	320,000	0	0	0	0	0	0	0	0	0	420,000

**Project Description:** Structural repairs and sealing to preserve integrity of structural steel reinforcement of Bayside Bridge.

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Construction
County Road Corridor	Not Applicable
Location	Clearwater
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Various

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 001034A Old Coachman Road over Alligator Creek Bridge Replacement</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3031	Bridges-Repair & Improvement								
020.1	Design-Penny	50,000	75,000	50,000	50,000	0	0	0	0	0	0	0	225,000
030.1	Constr-Penny	0	0	250,000	250,000	0	0	0	0	0	0	0	500,000
040.1	Testing-Penny	0	0	5,000	5,000	0	0	0	0	0	0	0	10,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3031</i>	<i>Bridges-Repair &amp; Improvement</i>							
		50,000	75,000	305,000	305,000	0	0	0	0	0	0	0	735,000
<b>Total for Project: 001034A</b>		<b>Old Coachman Road over Alligator Creek Bridge Replacement</b>											
		50,000	75,000	305,000	305,000	0	0	0	0	0	0	0	735,000
<b>Funding Source:</b>													
Penny for Pinellas		50,000	75,000	305,000	305,000	0	0	0	0	0	0	0	735,000
<b>Funding Total:</b>		50,000	75,000	305,000	305,000	0	0	0	0	0	0	0	735,000

**Project Description:** Old Coachman Road over Alligator Creek bridge reconstruction/replacement.

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Design
County Road Corridor	Not Applicable
Location	Clearwater
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Various

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 001035A Oakwood Drive over Stephanie's Channel Bridge Replacement</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3031	Bridges-Repair & Improvement								
020.1	Design-Penny	50,000	30,000	60,000	0	0	0	0	0	0	0	0	140,000
030.1	Constr-Penny	0	200,000	600,000	0	0	0	0	0	0	0	0	800,000
040.1	Testing-Penny	0	5,000	5,000	0	0	0	0	0	0	0	0	10,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3031</i>	<i>Bridges-Repair &amp; Improvement</i>							
		50,000	235,000	665,000	0	0	0	0	0	0	0	0	950,000
<b>Total for Project: 001035A</b>		<b>Oakwood Drive over Stephanie's Channel Bridge Replacement</b>											
		50,000	235,000	665,000	0	0	0	0	0	0	0	0	950,000
<b>Funding Source:</b>													
Penny for Pinellas		50,000	235,000	665,000	0	0	0	0	0	0	0	0	950,000
<b>Funding Total:</b>		50,000	235,000	665,000	0	0	0	0	0	0	0	0	950,000

**Project Description:** Oakwood Drive over Stephanie's Channel bridge reconstruction / replacement.

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Design
County Road Corridor	Not Applicable
Location	Largo, Belleair, Belleair Bluffs
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Various

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 001036A San Martin Blvd. over Riviera Bay Bridge Replacement</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation		Program: 3031		Bridges-Repair & Improvement						
020.1	Design-Penny	50,000	300,000	400,000	400,000	300,000	100,000	100,000	0	0	0	0	1,650,000
020.4	Design-Grant	0	0	0	0	0	100,000	100,000	0	0	0	0	200,000
030.1	Constr-Penny	0	0	0	0	0	2,000,000	2,000,000	0	0	0	0	4,000,000
030.4	Constr-Grant	0	0	0	0	0	2,000,000	2,000,000	0	0	0	0	4,000,000
040.1	Testing-Penny	0	0	0	0	0	5,000	5,000	0	0	0	0	10,000
040.4	Testing-Grant	0	0	0	0	0	5,000	5,000	0	0	0	0	10,000
<b>Project Total for : Fund: 3001 Capital Projects</b>		<b>Center: 414100</b>	<b>CIP-Transportation</b>	<b>Program: 3031</b>	<b>Bridges-Repair &amp; Improvement</b>								
		50,000	300,000	400,000	400,000	300,000	4,210,000	4,210,000	0	0	0	0	9,870,000
<b>Total for Project: 001036A San Martin Blvd. over Riviera Bay Bridge Replacement</b>		50,000	300,000	400,000	400,000	300,000	4,210,000	4,210,000	0	0	0	0	9,870,000
<b>Funding Source:</b>													
Penny for Pinellas		50,000	300,000	400,000	400,000	300,000	2,105,000	2,105,000	0	0	0	0	5,660,000
Grant - Federal		0	0	0	0	0	2,105,000	2,105,000	0	0	0	0	4,210,000
<b>Funding Total:</b>		50,000	300,000	400,000	400,000	300,000	4,210,000	4,210,000	0	0	0	0	9,870,000

**Project Description:** San Martin Blvd. over Riviera Bay bridge reconstruction / replacement. Note: This plan anticipates additional funding (i.e, grant) being available starting in FY18.

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Design
County Road Corridor	Not Applicable
Location	St Petersburg
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Various

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation      Budget Type Code: Planning      Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 001037A      Beckett Bridge Replacement</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation		Program: 3031			Bridges-Repair & Improvement					
010.1	Acq-Penny	0	0	0	50,000	100,000	50,000	0	0	0	0	0	200,000
010.4	Acq-Grant	0	0	0	50,000	100,000	50,000	0	0	0	0	0	200,000
020.1	Design-Penny	0	100,000	250,000	250,000	500,000	100,000	50,000	3,000	0	0	0	1,253,000
020.4	Design-Grant	0	0	0	250,000	500,000	100,000	50,000	25,000	0	0	0	925,000
020.5	Design-Unfunded	0	0	0	0	0	0	0	22,000	0	0	0	22,000
030.1	Constr-Penny	0	0	0	0	0	0	8,000,000	210,000	0	0	0	8,210,000
030.4	Constr-Grant	0	0	0	0	0	0	8,000,000	2,500,000	0	0	0	10,500,000
030.5	Constr-Unfunded	0	0	0	0	0	0	0	2,290,000	0	0	0	2,290,000
040.1	Testing-Penny	0	0	0	0	0	0	10,000	1,000	0	0	0	11,000
040.4	Testing-Grant	0	0	0	0	0	0	10,000	5,000	0	0	0	15,000
040.5	Testing-Unfunded	0	0	0	0	0	0	0	4,000	0	0	0	4,000
<b>Project Total for : Fund: 3001      Capital Projects</b>		<b>Center: 414100</b>	<b>CIP-Transportation</b>		<b>Program: 3031</b>			<b>Bridges-Repair &amp; Improvement</b>					
		0	100,000	250,000	600,000	1,200,000	300,000	16,120,000	5,060,000	0	0	0	23,630,000
<b>Total for Project: 001037A      Beckett Bridge Replacement</b>		0	100,000	250,000	600,000	1,200,000	300,000	16,120,000	5,060,000	0	0	0	23,630,000
<b>Funding Source:</b>													
Unfunded		0	0	0	0	0	0	0	2,316,000	0	0	0	2,316,000
Penny for Pinellas		0	100,000	250,000	300,000	600,000	150,000	8,060,000	214,000	0	0	0	9,674,000
Grant - Federal		0	0	0	300,000	600,000	150,000	8,060,000	2,530,000	0	0	0	11,640,000
<b>Funding Total:</b>		0	100,000	250,000	600,000	1,200,000	300,000	16,120,000	5,060,000	0	0	0	23,630,000

**Project Description:** Design and construction of Beckett Bridge replacement after PD&E is completed. This plan anticipates additional funding (i.e., grant) being available starting in FY16.

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Design
County Road Corridor	Not Applicable
Location	Tarpon Springs
Originating Department	DEI Public Works

## Pinellas County Capital Improvement Program Project Budget Detail Report

**Parameters:**    Function: Transportation    Budget Type Code: Planning    Fund Type: Governmental

	Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
Penny Program TIF District	Transportation and Traffic Flow Various											

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 001038A Park Street from Tyrone Blvd. to 54th Avenue N</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3022	Local Streets/Collector Projects								
020.1	Design-Penny	0	20,000	20,000	20,000	500,000	500,000	0	0	0	0	0	1,060,000
030.1	Constr-Penny	0	0	0	0	2,000,000	7,000,000	0	0	0	0	0	9,000,000
040.1	Testing-Penny	0	0	0	0	10,000	10,000	0	0	0	0	0	20,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3022</i>	<i>Local Streets/Collector Projects</i>							
		0	20,000	20,000	20,000	2,510,000	7,510,000	0	0	0	0	0	10,080,000
<b>Total for Project: 001038A</b>		<i>Park Street from Tyrone Blvd. to 54th Avenue N</i>											
		0	20,000	20,000	20,000	2,510,000	7,510,000	0	0	0	0	0	10,080,000
<b>Funding Source:</b>													
Penny for Pinellas		0	20,000	20,000	20,000	2,510,000	7,510,000	0	0	0	0	0	10,080,000
<b>Funding Total:</b>		0	20,000	20,000	20,000	2,510,000	7,510,000	0	0	0	0	0	10,080,000

**Project Description:** Park Street from Tyrone Blvd. to 54th Avenue N intersection improvements including construction of road widening, drainage improvements, sidewalks and mast arms.

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Design
County Road Corridor	Not Applicable
Location	St Petersburg
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Countywide

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 001039A Park Street / Starkey Road from 84th Lane N to Flamevine Avenue Roadway Improvements</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3022	Local Streets/Collector Projects								
020.1	Design-Penny	0	30,000	20,000	500,000	500,000	0	0	0	0	0	0	1,050,000
030.1	Constr-Penny	0	0	0	4,000,000	4,500,000	0	0	0	0	0	0	8,500,000
040.1	Testing-Penny	0	0	0	10,000	10,000	0	0	0	0	0	0	20,000
<b>Project Total for : Fund: 3001 Capital Projects</b>		<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3022</i>	<i>Local Streets/Collector Projects</i>								
		0	30,000	20,000	4,510,000	5,010,000	0	0	0	0	0	0	9,570,000
<b>Total for Project: 001039A Park Street / Starkey Road from 84th Lane N to Flamevine Avenue Roadway Improvements</b>		0	30,000	20,000	4,510,000	5,010,000	0	0	0	0	0	0	9,570,000
<b>Funding Source:</b>													
Penny for Pinellas		0	30,000	20,000	4,510,000	5,010,000	0	0	0	0	0	0	9,570,000
<b>Funding Total:</b>		0	30,000	20,000	4,510,000	5,010,000	0	0	0	0	0	0	9,570,000

**Project Description:** Park Street / Starkey Road from 84th Lane N to Flamevine Avenue intersection improvements including construction of road widening, sidewalks and mast arms.

**Project Classifications:**

CIE Elements	Not Applicable
CIP Phase	Construction
County Road Corridor	Not Applicable
Location	Seminole
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow
TIF District	Countywide



## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 001473A US 19 North ATMS/ITS Improvements</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects								
020.4	Design-Grant	65,000	15,000	0	0	0	0	0	0	0	0	0	80,000
030.4	Constr-Grant	0	200,000	0	0	0	0	0	0	0	0	0	200,000
110.2	Other-Grant	0	160,000	0	0	0	0	0	0	0	0	0	160,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3021</i>	<i>Intersection Improvements Projects</i>							
		65,000	375,000	0	0	0	0	0	0	0	0	0	440,000
<b>Total for Project: 001473A</b>		<b>US 19 North ATMS/ITS Improvements</b>											
		65,000	375,000	0	0	0	0	0	0	0	0	0	440,000
<b>Funding Source:</b>													
Grant - State		65,000	375,000	0	0	0	0	0	0	0	0	0	440,000
<b>Funding Total:</b>		65,000	375,000	0	0	0	0	0	0	0	0	0	440,000

**Project Description:** US 19 North ATMS/ITS Improvements from Beckett Way to Pasco County line

**Project Classifications:**

CIP Phase Construction  
Location Tarpon Springs

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 001510A 30th Avenue N at 49th Street N Intersection Improvements</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects								
020.1	Design-Penny	0	40,000	35,000	30,000	0	0	0	0	0	0	0	105,000
030.1	Constr-Penny	0	0	350,000	300,000	0	0	0	0	0	0	0	650,000
040.1	Testing-Penny	0	0	5,000	5,000	0	0	0	0	0	0	0	10,000
<b>Project Total for : Fund: 3001 Capital Projects</b>		<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3021</i>	<i>Intersection Improvements Projects</i>								
		0	40,000	390,000	335,000	0	0	0	0	0	0	0	765,000
<b>Total for Project: 001510A 30th Avenue N at 49th Street N Intersection Improvements</b>		0	40,000	390,000	335,000	0	0	0	0	0	0	0	765,000
<b>Funding Source:</b>													
Penny for Pinellas		0	40,000	390,000	335,000	0	0	0	0	0	0	0	765,000
<b>Funding Total:</b>		0	40,000	390,000	335,000	0	0	0	0	0	0	0	765,000

**Project Description:** Intersection Improvements including construction of mast arms, ADA ramp upgrades and traffic flow improvements.

**Project Classifications:**

CIP Phase	Construction
Location	St Petersburg
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 001511A 38th Avenue N at 49th Street N Intersection Improvements</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3021	Intersection Improvements Projects								
010.1	Acq-Penny	0	100,000	0	0	0	0	0	0	0	0	0	100,000
020.1	Design-Penny	0	40,000	30,000	40,000	30,000	0	0	0	0	0	0	140,000
030.1	Constr-Penny	0	0	0	400,000	300,000	0	0	0	0	0	0	700,000
040.1	Testing-Penny	0	0	0	5,000	5,000	0	0	0	0	0	0	10,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3021</i>	<i>Intersection Improvements Projects</i>							
		0	140,000	30,000	445,000	335,000	0	0	0	0	0	0	950,000
<b>Total for Project: 001511A</b>		<b>38th Avenue N at 49th Street N Intersection Improvements</b>											
		0	140,000	30,000	445,000	335,000	0	0	0	0	0	0	950,000
<b>Funding Source:</b>													
Penny for Pinellas		0	140,000	30,000	445,000	335,000	0	0	0	0	0	0	950,000
<b>Funding Total:</b>		0	140,000	30,000	445,000	335,000	0	0	0	0	0	0	950,000

**Project Description:** Intersection improvements including construction of mast arms, ADA ramp upgrades and traffic flow improvements.

**Project Classifications:**

CIP Phase	Construction
Location	St Petersburg
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow

## Pinellas County Capital Improvement Program Project Budget Detail Report

Parameters: Function: Transportation Budget Type Code: Planning Fund Type: Governmental

		Current Year Estimate	2014	2015	2016	2017	2018	2019	2020	2021	2022	2023	Total
<b>Function: Transportation</b>													
<b>Activity: Road &amp; Street Facilities</b>													
<b>Project: 001512A Park Blvd SRTS Sidewalk Improvements</b>													
Fund: 3001	Capital Projects	Center: 414100	CIP-Transportation	Program: 3026	Sidewalks Projects								
020.4	Design-Grant	0	25,000	175,000	75,000	75,000	0	0	0	0	0	0	350,000
030.4	Constr-Grant	0	0	0	750,000	750,000	0	0	0	0	0	0	1,500,000
040.4	Testing-Grant	0	0	0	5,000	5,000	0	0	0	0	0	0	10,000
<b>Project Total for : Fund: 3001</b>		<i>Capital Projects</i>	<i>Center: 414100</i>	<i>CIP-Transportation</i>	<i>Program: 3026</i>	<i>Sidewalks Projects</i>							
		0	25,000	175,000	830,000	830,000	0	0	0	0	0	0	1,860,000
<b>Total for Project: 001512A</b>		<b>Park Blvd SRTS Sidewalk Improvements</b>											
		0	25,000	175,000	830,000	830,000	0	0	0	0	0	0	1,860,000
<b>Funding Source:</b>													
Grant - Federal		0	25,000	175,000	830,000	830,000	0	0	0	0	0	0	1,860,000
<b>Funding Total:</b>		0	25,000	175,000	830,000	830,000	0	0	0	0	0	0	1,860,000

**Project Description:** Install sidewalks along Park Blvd from Starkey Road to 66th Street North. Grant funding needs to be approved by FDOT prior to proceeding with design and construction.

**Project Classifications:**

CIP Phase	Design
Location	Pinellas Park
Originating Department	DEI Public Works
Penny Program	Transportation and Traffic Flow

Transportation Total:	30,431,170	44,894,000	55,015,000	46,593,000	45,817,100	42,458,000	54,578,600	29,886,000	15,340,000	15,760,000	15,490,000	396,262,870
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**PINELLAS COUNTY METROPOLITAN PLANNING ORGANIZATION  
TRANSPORTATION IMPROVEMENT PROGRAM  
(PUBLIC TRANSPORTATION)  
FISCAL YEARS 2013/14 – 2017/18**

**ST. PETERSBURG CLEARWATER INTERNATIONAL AIRPORT**

**FUNDS SHOWN IN THOUSANDS**

<b>Proj. #</b>	<b>Project Title</b>	<b>2013/14</b>	<b>2014/15</b>	<b>2015/16</b>	<b>2016/17</b>	<b>2017/18</b>	<b>Total</b>
2133	Construct New Taxiways and Roads				\$4,200		\$4,200
1205	Airfield Drainage	\$1,000	\$1,000				\$2,000
2134	New Airport Maintenance Facility			\$1,500			\$1,500
2280	New Hangars					\$7,000	\$7,000
925	Runway Conversion			\$4,500			\$4,500
2273	Taxiway Rehabilitation (Phase I)	\$2,400	\$10,000				\$12,400
2274	Taxiway Rehabilitation (Phase II)	\$540	\$600	5,700			\$6,840
2132	Airport Terminal Ramp Rehabilitation		\$300	\$2,700			\$3,000
2020	Rehabilitate Runway 18/36				\$6,000		\$6,000
2278	Terminal Improvements - Phase II	\$4,000					\$4,000
2276	Road & Pkg Lot Imprvmnts Airport		\$1,000				\$1,000
1065A	AIRCO Site Development				\$2,000	\$2,000	\$4,000
1543A	Taxiway T Rehabilitation			\$300	\$2,700		\$3,000
1544A	Terminal Improvements Phase III	\$250	\$2,500	\$3,500			\$6,250
1548A	Airport Master Plan Update				\$1,300		\$1,300
1583A	Security System Upgrades			\$1,200			\$1,200
	<b>Totals</b>	\$8,190	\$15,400	\$19,400	\$16,200	\$9,000	\$68,190

**PINELLAS COUNTY METROPOLITAN PLANNING ORGANIZATION  
TRANSPORTATION IMPROVEMENT PROGRAM  
(PUBLIC TRANSPORTATION)  
FISCAL YEARS 2013/14 – 2017/18**

**PORT OF ST. PETERSBURG**

**FUNDS SHOWN ARE IN THOUSANDS**

<b>WORK ITEM #</b>	<b>PROJECT DESCRIPTION</b>	<b>2013/14</b>		<b>2014/15</b>		<b>2015/16</b>		<b>2016/17</b>		<b>2017/18</b>	
FPN# 4225011	Seaport Renovation and Replacement	CST	400	CST	400	CST	400	CST	400	CST	400
			NCIIF		NCIIF		NCIIF		NCIIF		NCIIF
	Port and Wharf Rehabilitation and Utilities		101		101		101		101		101
			PCPF		PCPF		PCPF		PCPF		PCPF

CST=Construction; NCIIF – Neighborhood and Citywide Infrastructure Capital Improvement Fund; PCPF – Port Capital Projects Fund

**PINELLAS COUNTY METROPOLITAN PLANNING ORGANIZATION  
TRANSPORTATION IMPROVEMENT PROGRAM  
(PUBLIC TRANSPORTATION)  
FISCAL YEARS 2013/14 – 2017/18**

**CLEARWATER AIRPARK**

**FUNDS SHOWN IN THOUSANDS**

<b>WORK ITEM #</b>	<b>PROJECT DESCRIPTION</b>	<b>2013/14</b>	<b>2014/15</b>	<b>2015/16</b>	<b>2016/17</b>	<b>2017/18</b>
4259222	Rehabilitate Hangars C and D	CAP 500				
4259221	Rehabilitate Maintenance Hangar	CAP 200				
4317931	Aviation Safety Project			CAP 100		
4317941	New Hangar				CAP 700	
4329771	Aviation Revenue/Operational					CAP 100

CAP = Capital Grant

**PINELLAS COUNTY METROPOLITAN PLANNING ORGANIZATION  
TRANSPORTATION IMPROVEMENT PROGRAM  
(PUBLIC TRANSPORTATION)  
FISCAL YEARS 2013/14 – 2017/18**

**ALBERT WHITTED AIRPORT**

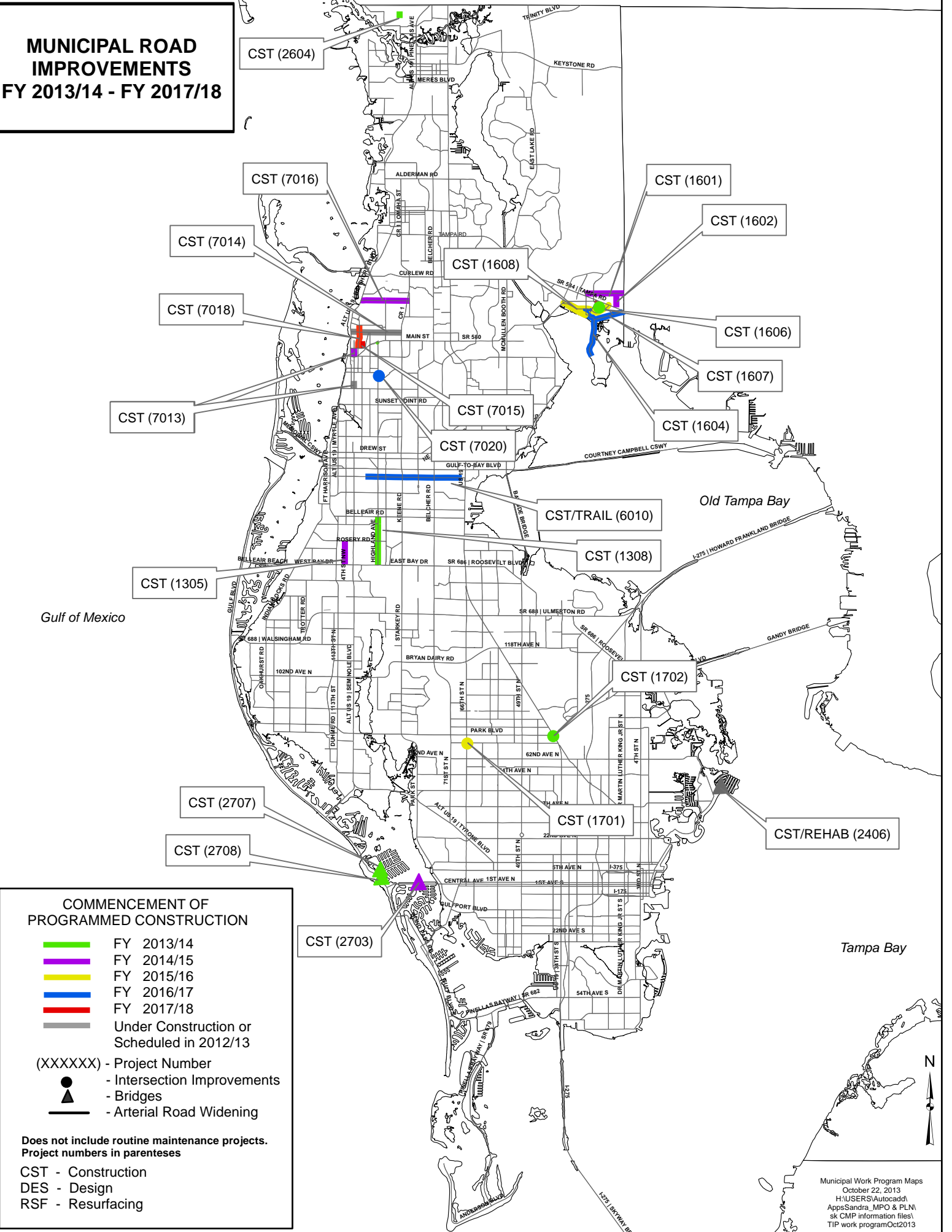
**FUNDS SHOWN ARE IN THOUSANDS**

<b>WORK ITEM #</b>	<b>PROJECT DESCRIPTION</b>	<b>2013/14</b>	<b>2014/15</b>	<b>2015/16</b>	<b>2016/17</b>	<b>2017/18</b>
2821001	Runway 7/25 Rehabilitation Project		CST 1,700 ACPF			
2821002	Airport Hangers	CST 1,200 ACPF	CST 2,000 ACPF			
2821003	Airport Airfield Improvements		CST 166 ACPF	CST 166 ACPF	CST 166 ACPF	
2821004	Airport Hangar #1	CST 300 NCIIF				
2821005	Design and Construct 3 ± 12,000sq/ft corporate hangars			CST 1,500 ACPF		
2821006	Design and Construct 2 <sup>nd</sup> Hangar/ Demolition of Existing Facilities				CST 2,000	
2821007	Design and Construct 3 <sup>rd</sup> Hangar/ Demolition of Existing Facilities					CST 2,000 ACPF
2821008	Airfield Improvements	CST 150	CST 166 ACPF	CST 166 ACPF	CST 166 ACPF	

ACPF = Airport Capital Projects Fund; NCIIF = Neighborhood and Citywide Infrastructure Capital Improvement Fund



# MUNICIPAL ROAD IMPROVEMENTS FY 2013/14 - FY 2017/18



**COMMENCEMENT OF PROGRAMMED CONSTRUCTION**

- █ FY 2013/14
- █ FY 2014/15
- █ FY 2015/16
- █ FY 2016/17
- █ FY 2017/18
- █ Under Construction or Scheduled in 2012/13

(XXXXXX) - Project Number

- - Intersection Improvements
- ▲ - Bridges
- - Arterial Road Widening

Does not include routine maintenance projects.  
Project numbers in parentheses

CST - Construction  
DES - Design  
RSF - Resurfacing

**MUNCIPAL WORK PROGRAMS**  
**FY 2013/14 – 2017/18**

**BELLEAIR**

**FUNDS SHOWN ARE IN THOUSANDS**

WORK ITEM #	PROJECT DESCRIPTION	TYPE WORK	13/14		14/15		15/16		16/17		17/18	
			CST	LF	CST	LF	CST	LF	CST	LF		
2000	Curb/Sidewalk		CST	35 LF	CST	35 LF	CST	35 LF	CST	35 LF		
2001	Roadway Projects				CST	2,500 LF						

CST = Construction; LF = Local Funds

**MUNICIPAL WORK PROGRAMS  
FY 2013/14 – 2017/18**

**BELLEAIR BEACH**

**FUNDS SHOWN IN THOUSANDS**

WORK ITEM #	PROJECT DESCRIPTION	TYPE WORK	13/14		14/15		15/16		16/17		17/18	
			CST		CST		CST		CST		CST	
3000	Resurface/curb work roadways 7 <sup>th</sup> to Harbor Dr. to 8 <sup>th</sup> to Gulf Blvd. 20 <sup>th</sup> and 21 <sup>st</sup> St. from Gulf Blvd. to Bayshore and Bayshore from 19 <sup>th</sup> St., Aleta, Donato Dr.	Resurfacing	CST	45	CST	32	CST	75	CST	40	CST	48
3001	Emergency repairs to bridges/bulkheads at Harrison Ave. and 22 <sup>nd</sup> St. Bridge	Repairs	CST	30	CST	55	CST	5	CST	5	CST	5

CST = Construction

**MUNICIPAL WORK PROGRAMS  
FY 2013/14 – 2017/18**

**BELLEAIR BLUFFS**

**FUNDS SHOWN ARE IN THOUSANDS**

WORK ITEM #	PROJECT DESCRIPTION	TYPE WORK	13/14		14/15		15/16		16/17		17/18	
			Fund	Amount	Fund	Amount	Fund	Amount	Fund	Amount	Fund	Amount
4000	Infrastructure/Paving/ Curbs	Reconstruction	CST	406	CST	651	CST	651	CST	651		

CST = Construction

**MUNCIPAL WORK PROGRAMS  
FY 2013/14 – 2017/18**

**BELLEAIR SHORE**

**FUNDS SHOWN ARE IN THOUSANDS**

<b>WORK ITEM #</b>	<b>PROJECT DESCRIPTION</b>	<b>TYPE WORK</b>	<b>13/14</b>		<b>14/15</b>		<b>15/16</b>		<b>16/17</b>		<b>17/18</b>	
	<p>No transportation improvement projects are scheduled within the five-year time frame of the TIP.</p> <p>NOTE: Belleair Shore has no roads or other transportation facilities within its municipal boundaries</p>											

**MUNICIPAL WORK PROGRAMS  
FY 2013/14 – 2017/18**

**CLEARWATER**

**FUNDS SHOWN ARE IN THOUSANDS**

<b>WORK ITEM #</b>	<b>PROJECT DESCRIPTION</b>	<b>TYPE WORK</b>	<b>13/14</b>		<b>14/15</b>		<b>15/16</b>		<b>16/17</b>		<b>17/18</b>	
6000	Citywide Streets Resurfacing & Maintenance	Resurfacing	CST	3,524	CST	3,538	CST	3,553	CST	5,564	CST	3770
6001	Citywide Intersection Improvements (Minor)	Safety	CST	50 TIF	CST	50	CST	50	CST	50	CST	50
6002	Citywide New Traffic Signals	Traffic Operation	CST	40 TIF	CST	40	CST	40	CST	40	CST	40
6003	Citywide Traffic Signals	Renovation	CST	45	CST	45	CST	45	CST	45	CST	45
6004	Citywide Sidewalks	New Construction	CST	450	CST	472	CST	472	CST	472	CST	472
6005	Paving Unpaved Streets	Paving	CST	1,794	CST	1,794	CST	1,794	CST	1794	CST	1794
6006	Traffic Calming	Traffic Calming	CST	620	CST	595	CST	595	CST	595	CST	570
6007	Intersection Improvements (Major)		CST	200 TIF 100 GT	CST	200 TIF 100 GT	CST	200 TIF 100 GT	CST	200 TIF 55 GT 45 RM	CST	200 TIF 55 GT 45 RM
6008	Parking Lot Resurfacing		CST	50	CST	50	CST	50	CST	50	CST	50

**CLEARWATER**

**FUNDS SHOWN ARE IN THOUSANDS**

WORK ITEM #	PROJECT DESCRIPTION	TYPE WORK	13/14		14/15		15/16		16/17		17/18	
6009	Parking Garage Maintenance & Repair	Parking	CST	75	CST	75	CS T	75	CST	75	CST	75
6010	Druid Trail	Trail							CST	2,011 TAP		
6011	Parking Lot Improvement	Parking	CST	100	CST	100	CST	100	CST	100	CST	100

CST = Construction; TIF = Transportation Impact Fee; GT = Gas Tax; RM = Road Millage, TA = Transportation Alternatives Program

**MUNCIPAL WORK PROGRAMS  
FY 2013/14 – 2017/18**

DUNEDIN			FUNDS SHOWN ARE IN THOUSANDS									
WORK ITEM #	PROJECT DESCRIPTION	TYPE WORK	13/14		14/15		15/16		16/17		17/18	
7000	Street Resurfacing - Citywide	Maintenance	CST	420 GT	CST	420 GT	CST	420 GT	CST	420 GT	CST	420 GT
7001	Sidewalk Rehabilitation - Citywide	Maintenance	CST	52 GT	CST	52 GT	CST	52 GT	CST	52 GT	CST	52 GT
7002	Road Repair Material - Citywide	Maintenance	CST	18 GT	CST	18 GT	CST	26 GT	CST	26 GT	CST	26 GT
7003	Brick Streets - Citywide	Maintenance			CST	50 GT			CST	50 GT		
7004	Pavement Striping - Citywide	Maintenance	CST	40 GT	CST	40 GT	CST	40 GT	CST	40 GT	CST	40 GT
7005	Signage Replacement - Citywide	Maintenance	CST	50 GT	CST	50 GT	CST	50 GT	CST	50 GT	CST	50 GT
7006	Neighborhood Sidewalks - Citywide	Maintenance	CST	42 GT	CST	42 GT	CST	42 GT	CST	42 GT	CST	42 GT
7007	Road Repair - Citywide	Maintenance	CST	17 GT	CST	17 GT	CST	17 GT	CST	17 GT	CST	17 GT
7008	Bridge Repairs - Parks	Maintenance	CST	33 CIP					CST	107 CIP	CST	16 CIP
7009	Battery Replacement - Citywide	Maintenance	CST	5 TIF	CST	5 TIF	CST	5 TIF	CST	5 TIF	CST	5 TIF



DUNEDIN

FUNDS SHOWN ARE IN THOUSANDS

WORK ITEM #	PROJECT DESCRIPTION	TYPE WORK	13/14		14/15		15/16		16/17		17/18	
			CST	TIF	CST	TIF	CST	TIF	CST	TIF	CST	TIF
7010	Traffic Signal Upgrades - Citywide	Maintenance	CST	5 TIF	CST	5 TIF	CST	5 TIF	CST	5 TIF	CST	5 TIF
7011	Speed Feedback Signs –Citywide	Maintenance	CST	21 TIF								
7012	Replace Ravenwood Pedestrian Bridge	Construction	CST	25 OCOST								
7013	North Douglas Corridor Improvement - Main St to Skinner Blvd & South Douglas Streetscape – Union St. to Library	Construction			CST	4 CRA	CST	4 CRA	CST	4 CRA	CST	4 CRA
7014	San Christopher - Alt 19 to CR 1	Construction	CST	731 OCOST	CST	731 OCOST						
7015	Milwaukee north of Skinner	Road Widening to Two Full Lanes									CST	600 OCOST
7016	Michigan Blvd. - Alt 19 to CR 1	Construction			CST	860 OCOST	CST	470 OCOST	CST	540 OCOST		
7017	San Salvador - Alt 19 to CR 1	Construction					CST	840 OCOST	CST	240 OCOST		
7018	MLK - Skinner Blvd to San Salvador Dr	Resurfacing									CST	288 OCOST

**DUNEDIN**

**FUNDS SHOWN ARE IN THOUSANDS**

<b>WORK ITEM #</b>	<b>PROJECT DESCRIPTION</b>	<b>TYPE WORK</b>	<b>13/14</b>		<b>14/15</b>		<b>15/16</b>		<b>16/17</b>		<b>17/18</b>	
7019	Highland Streetscape	Construction			CST	107 CRA	CST	10 CRA	CST	107 CRA	CST	107 CRA
7020	Beltrees St at Patricia Ave	Construction							CST	960 OCOST	CST	648 OCOST
7020	Repair/Replace Infrastructure -Downtown	Maintenance/ Construction	CST	25 CRA	CST	25 CRA	CST	20 CRA	CST	20 CRA	CST	20 CRA
7021	Huntley/ Monroe Enhancements	Construction	CST	161 CRA								

CST=Construction; OCOST = One Cent Optional Sales Tax; GT = Gas Tax; TIF = Transportation Impact Fee; LF = Local Funds

**MUNCIPAL WORK PROGRAMS  
FY 2013/14 – 2017/18**

**GULFPORT**

**FUNDS SHOWN ARE IN THOUSANDS**

WORK ITEM #	PROJECT DESCRIPTION	TYPE WORK	13/14		14/15		15/16		16/17		17/18	
			CST	LF	CST	LF	CST	LF	CST	LF		
8000	Citywide Resurfacing of Local Streets	Resurfacing	CST	150 LF	CST	150 LF	CST	150 LF	CST	150 LF		
8001	City Sidewalk Construction/ Reconstruction	Upgrade	CST	20 LF	CST	20 LF	CST	20 LF	CST	20 LF		

CST = Construction; LF = Local Funds

**MUNICIPAL WORK PROGRAMS  
FY 2013/14 – 2017/18**

**INDIAN ROCKS BEACH**

**FUNDS SHOWN ARE IN THOUSANDS**

WORK ITEM #	PROJECT DESCRIPTION	TYPE WORK	13/14		14/15		15/16		16/17		17/18	
9000	Street Resurfacing	Milling/ Resurfacing					CST	200 LF				

CST = Construction; LF = Local Funds (includes Local Option Sales Tax, Local Option gas Tax, and/or Grant funding)

**MUNCIPAL WORK PROGRAMS  
FY 2013/14 – 2017/18**

**INDIAN SHORES**

**FUNDS SHOWN ARE IN THOUSANDS**

<b>WORK ITEM #</b>	<b>PROJECT DESCRIPTION</b>	<b>TYPE WORK</b>	<b>13/14</b>		<b>14/15</b>		<b>15/16</b>		<b>16/17</b>		<b>17/18</b>	
	No transportation improvement projects are scheduled within the five-year time frame of the TIP.											

**MUNICIPAL WORK PROGRAMS**  
**FY 2013/14 – 2017/18**

KENNETH CITY			FUNDS SHOWN ARE IN THOUSANDS									
WORK ITEM #	PROJECT DESCRIPTION	TYPE WORK	13/14		14/15		15/16		16/17		17/18	
	No transportation improvement projects are scheduled within the five-year time frame of the TIP.											

**MUNICIPAL WORK PROGRAMS  
FY 2013/14 – 2017/18**

**LARGO**

**FUNDS SHOWN ARE IN THOUSANDS**

<b>WORK ITEM #</b>	<b>PROJECT DESCRIPTION</b>	<b>TYPE WORK</b>	<b>13/14</b>		<b>14/15</b>		<b>15/16</b>		<b>16/17</b>		<b>17/18</b>	
1300	Citywide Road and Sidewalk Rehab	Construction /Repair	DGN/ CST	555 CGT	DGN/ CST	745 CGT	DGN/ CST	1070 CGT	DGN/ CST	520 CGT	DGN/ CST	1050 CGT
1301	Citywide Sidewalk Initiative	Construction			PE&E	25 LOST	DGN	50 LOST	CST	590 LOST		
1302	Transit Station Masterplan	Study			PE&E	25 DTIF  75 TIF						
1303	4 <sup>th</sup> St. NW Community Streets	Reconstruction	DGN/ CST	620 TIF  955 CGT  65 DTIF	DGN/ CST	685 TIF  80 CGT  470 DTIF	DGN/ CST	1,720 TIF  355 CGT  470 DTIF	DGN/ CST	1,840 TIF  455 CGT  470 DTIF	DGN/ CST	1,260 TIF  340 CGT  470 DTIF
1304	Downtown Plaza Improvements	Construction	DGN	300			CST	345	CST	530		
1305	East/West Bay Drive Downtown Transportation Safety Improvements	Construction	DGN	65 DTIF	CST	570 DTIF	DGN	100 DTIF	CST	675 DTIF		

**LARGO**

**FUNDS SHOWN ARE IN THOUSANDS**

WORK ITEM #	PROJECT DESCRIPTION	TYPE WORK	13/14		14/15		15/16		16/17		17/18	
1306	Highland Avenue	Reconstruction	CST	3,950 CAP								
1307	Unpaved Roads	Reconstruction			PE&E	25 CGT	DGN	55 CGT	CST	470 CGT		
1308	West Bay Dr. at Pinellas Trail Urban Trails	Construction	CST	500 DTIF								
1309	Unpaved Roads	Resurfacing					DGN	40			CST	235

CST = Construction; DGN = Design; LOST = Local Option Sales Tax; CGT = Community Gas Tax; TIF = Transportation Impact Fee; Gen. = General Fund; PE&E = Preliminary Engineering & Evaluation; DTIF = Downtown Tax Increment Fund



**MUNCIPAL WORK PROGRAMS  
FY 2013/14 – 2017/18**

**MADEIRA BEACH**

**FUNDS SHOWN ARE IN THOUSANDS**

WORK ITEM #	PROJECT DESCRIPTION	TYPE WORK	13/14		14/15		15/16		16/17		17/18	
			CST	LF	CST	LF	CST	LF	CST	LF		
1440	Citywide Maintenance and Resurfacing of Local Streets	Maintenance	CST	100 LF	CST	100 LF	CST	100 LF	CST	100 LF		

CST = Construction; LF = Local Funds

**MUNCIPAL WORK PROGRAMS  
FY 2013/14 – 2017/18**

**NORTH REDINGTON BEACH**

**FUNDS SHOWN ARE IN THOUSANDS**

WORK ITEM #	PROJECT DESCRIPTION	TYPE WORK	13/14		14/15		15/16		16/17		17/18	
1500	Gulf Blvd: Underground Utilities		CST	250 LF	CST	250 LF	CST	250 LF				
1501	Streetscaping		CST	100 LF	CST	100 LF	CST	100 LF				

CST = Construction; LF = Local Funds

**MUNICIPAL WORK PROGRAMS  
FY 2013/14 – 2017/18**

**OLDSMAR**

**FUNDS SHOWN ARE IN THOUSANDS**

<b>WORK ITEM #</b>	<b>PROJECT DESCRIPTION</b>	<b>TYPE WORK</b>	<b>13/14</b>		<b>14/15</b>		<b>15/16</b>		<b>16/17</b>		<b>17/18</b>	
1600	Citywide Resurfacing/Curb Repair	Resurfacing/ Maintenance	CST	150 LF	CST	150 LF	CST	125 LF	CST	125 LF	CST	125 LF
1601	Douglas Rd	Improvement			CST	200 LF	CST	3150 LF				
1602	Burbank Rd.	Extension			CST	200 LF			CST	1250 LF		
1603	Oldsmar Parks Connection	Bike/Pedestrian Trail	CST	996 LF								
1604	Shore Drive East	Improvement							CST	200 LF	CST	1800 LF
1605	Harbor Palms	Sidewalk & Curb Replacement			CST	75 LF	CST	150 LF	CST	150 LF	CST	150 LF
1606	State St/SR580 Intersection	Improvement	CST	170 LF	CST	730 LF						
1607	Lafayette Boulevard	Resurfacing/ Maintenance							CST	275 LF		
1608	St. Petersburg Drive*	Streetscaping					CST	80 LF	CST	600 LF	CST	80 LF
1609	St. Petersburg	Resurfacing/ Maintenance	CST	175 LF								
1610	CRA Intersection.	Traffic Calming	CST	175 LF								
1611	CRA Streets	Resurfacing/ Maintenance	CST	20 LF	CST	20 LF	CST	35 LF	CST	40 LF		
1612	Transit Station/Terminal	Multimodal Station									CST	1400 LF
1613	Arlington Ave	Extension			CST	15 LF	CST	70 LF				

**OLDSMAR**

**FUNDS SHOWN ARE IN THOUSANDS**

WORK ITEM #	PROJECT DESCRIPTION	TYPE WORK	13/14		14/15		15/16		16/17		17/18	
			CST	LF	CST	LF	CST	LF	CST	LF	CST	LF
1614	Bayside Blvd	Resurfacing	CST	125 LF								
1615	Oakleaf Blvd	Resurfacing									CST	250 LF
1616	CityWide	Sidewalk and Curb Replacement	CST	50 LF	CST	50 LF	CST	50 LF	CST	50 LF	CST	50 LF

\*Parking, sidewalks, intersections CST = Construction; LF = Local Funds

**MUNCIPAL WORK PROGRAMS  
FY 2013/14 – 2017/18**

**PINELLAS PARK**

**FUNDS SHOWN ARE IN THOUSANDS**

<b>WORK ITEM #</b>	<b>PROJECT DESCRIPTION</b>	<b>TYPE WORK</b>	<b>13/14</b>		<b>14/15</b>		<b>15/16</b>		<b>16/17</b>		<b>17/18</b>	
1700	Street Resurfacing (5 YR Annual Program – Citywide)	Maintenance	CST	300 IST	CST	300 IST	CST	300 IST	CST	300 IST	CST	300 IST
1701	Left Turn Lane on 70 <sup>th</sup> Ave at 66 <sup>th</sup> St	Construction					CST	180 IST 100 TIF				
1702	Widen 70 <sup>th</sup> Ave at US 19	Construction	CST	22.5 IST 22.5 TIF								
1703	Thermoplastic Road Striping of 86 <sup>th</sup> Ave, 9 <sup>th</sup> Ave and 118 <sup>th</sup> Ave	Construction	CST	50 TIF								
1704	Street Lights in Newly Annexed Neighborhoods	Construction			CST	18 IST						
1705	Mill and Resurface Roads in Gateway Center	Construction							CST	350 IST		
1706	Sidewalk, Ramp and Neighborhood Program	Construction			CST	30 IST	CST	30 IST	CST	30 IST	CST	30 IST
1707	CRA Sidewalk Improvements	Construction	CST	30 CRA/ CDBG	CST	30 CRA/ CDBG	CST	30 CRA/ CDBG	CST	30 CRA/ CDBG	CST	30 CRA/ CDBG

**PINELLAS PARK**

**FUNDS SHOWN ARE IN THOUSANDS**

WORK ITEM #	PROJECT DESCRIPTION	TYPE WORK	13/14		14/15		15/16		16/17		17/18	
1708	60 <sup>th</sup> St. Sidewalks and Related Drainage Improvements	Construction	CST	25 IST	CST	125.5 IST						
1709	90 <sup>th</sup> Ave. Sidewalks and Related Drainage Improvements (70 <sup>th</sup> St. to Belcher Rd.)	Construction							CST	320 IST		
1710	90 <sup>th</sup> Ave. Sidewalks and Related Drainage Improvements (66th St. to 70 <sup>th</sup> St.)	Construction					CST	200 IST				

CST = Construction; CRA = Tax Increment Finance Funds; IST = Infrastructure Sales Tax; TIF=Transportation Impact Fee; CDBG=Community Development Block Grant

**MUNCIPAL WORK PROGRAMS**  
**FY 2013/14 – 2017/18**

**REDINGTON BEACH**

**FUNDS SHOWN ARE IN THOUSANDS**

WORK ITEM #	PROJECT DESCRIPTION	TYPE WORK	13/14		14/15		15/16		16/17		17/18	
	No transportation improvement projects are scheduled within the five-year time frame of the TIP.											

**MUNICIPAL WORK PROGRAMS**  
**FY 2013/14 – 2017/18**

**REDINGTON SHORES**

**FUNDS SHOWN ARE IN THOUSANDS**

<b>WORK ITEM #</b>	<b>PROJECT DESCRIPTION</b>	<b>TYPE WORK</b>	<b>13/14</b>		<b>14/15</b>		<b>15/16</b>		<b>16/17</b>		<b>17/18</b>	
	No transportation improvement projects are scheduled within the five-year time frame of the TIP.											



**MUNCIPAL WORK PROGRAMS  
FY 2013/14 – 2017/18**

**SAFETY HARBOR**

**FUNDS SHOWN ARE IN THOUSANDS**

WORK ITEM #	PROJECT DESCRIPTION	TYPE WORK	13/14		14/15		15/16		16/17		17/18	
			CST	GT, P, W&SR	CST	GT, P, W&SR	CST	GT, P, W&SR	CST	GT, P, W&SR		
2100	Sidewalks (Citywide)	Construct	CST	50 GT	CST	50 GT	CST	50 GT	CST	50 GT		
2101	Resurfacing/ Repair Work	Repair	CST	600 GT,P	CST	600 GT,P	CST	600 GT,P	CST	600 GT,P		
2102	Brick Street Restoration	Construct		50 GT, P, W&SR	PE, CST	50 GT, P, W&SR	PE, CST	50 GT, P, W&SR	PE,C ST	50 GT, P, W&SR		
2103	Citywide Traffic Analysis/ Implementation	Study	CST	20 GT, O	CST	20 GT, O	CST	20 GT, O	CST	20 GT, O		
2104	Bridge Maintenance and Repair	Mainten.	CST	25 GT, O	CST	25 GT, O	CST	25 GT, O	CST	25 GT, O		

CST = Construction; GT = Gas Tax; O= Other (Impact fee, Comm. Dev grants, etc.); P = Penny for Pinellas; W&SR = Water & Sewer Revenues

**MUNICIPAL WORK PROGRAMS**  
**FY 2013/14 – 2017/18**

**SEMINOLE**

**FUNDS SHOWN ARE IN THOUSANDS**

WORK ITEM #	PROJECT DESCRIPTION	TYPE WORK	13/14		14/15		15/16		16/17		17/18	
			CST	LF	CST	LF	CST	LF	CST	LF	CST	LF
2200	Roadway Resurfacing	Maintenance	CST	100 LF	CST	150 LF	CST	150 LF	CST	150 LF	CST	150 LF
2201	Traffic Signal Replacement	Installation	CST	2 LF	CST	2 LF	CST	2 LF	CST	2 LF	CST	2 LF
2202	Street Lights	Installation	CST	5 LF	CST	5 LF	CST	5 LF	CST	5 LF	CST	5 LF
2203	Sidewalk	Installation	CST	10 LF	CST	10 LF	CST	10 LF	CST	10 LF	CST	10 LF

CST = Construction; LF = Local Funds

**MUNICIPAL WORK PROGRAMS**  
**FY 2013/14 – 2017/18**

**SOUTH PASADENA**

**FUNDS SHOWN ARE IN THOUSANDS**

WORK ITEM #	PROJECT DESCRIPTION	TYPE WORK	13/14		14/15		15/16		16/17		17/18	
			CST	LF	CST	LF						
2300	Road Paving	Resurface & Milling	CST	124 LF	CST	20 LF						

CST = Construction; LF = Local Funds

**MUNICIPAL WORK PROGRAMS  
FY 2013/14 – 2017/18**

**ST. PETERSBURG**

**FUNDS ARE SHOWN IN THOUSANDS**

WORK ITEM #	PROJECT DESCRIPTION	TYPE WORK	13/14		14/15		15/16		16/17		17/18	
			CST	NCIIF	CST	NCIIF	CST	NCIIF	CST	NCIIF	CST	NCIIF
2400	Neighborhood Transportation Management Program	Traffic Calming	CST	75 NCIIF	CST	50 NCIIF	CST	50 NCIIF	CST	50 NCIIF	CST	50 NCIIF
2401	Street and Road Improvements	Maintenance	CST	4,000 NCIIF	CST	4,500 NCIIF	CST	4,000 NCIIF	CST	4,500 NCIIF	CST	4,500 NCIIF
2402	Road and Alley Reconstruction	Maintenance	CST	500 NCIIF	CST	1000 NCIIF	CST	750 NCIIF	CST	300 NCIIF	CST	300 NCIIF
2403	Intersections Modifications	Traffic Safety	CST	50 NCIIF	CST	50 NCIIF	CST	50 NCIIF	CST	50 NCIIF	CST	50 NCIIF
2404	Curb Replacement/ Ramps	Maintenance	CST	500 NCIIF	CST	500 NCIIF	CST	500 NCIIF	CST	500 NCIIF	CST	500 NCIIF
2405	Sidewalk Reconstruction/ Expansion	Maintenance	CST	600 NCIIF	CST	600 NCIIF	CST	600 NCIIF	CST	600 NCIIF	CST	600 NCIIF
2406	Bridge Reconstruction/ Replacement, including Overlook Dr. Bridge at Kentucky Ave	Maintenance	CST	2,250 NCIIF	CST	1,150 NCIIF	CST	400 NCIIF	CST	1,700 NCIIF	CST	2,700 NCIIF
2407	Gateway Areawide DRI Mitigation Program	New Construction	CST	100 TIF	CST	100 TIF	CST	100 TIF	CST	100 TIF	CST	100 TIF
2408	Special Assessments for Alley and Street Paving	Maintenance	CST	150 NCIIF	CST	150 NCIIF	CST	150 NCIIF	CST	150 NCIIF	CST	150 NCIIF

**ST. PETERSBURG**

**FUNDS ARE SHOWN IN THOUSANDS**

WORK ITEM #	PROJECT DESCRIPTION	TYPE WORK	13/14		14/15		15/16		16/17		17/18	
2409	Railway Crossing Improvements	Maintenance					CST	50 NCIIF	CST	255 NCIIF		
2410	Bicycle Pedestrian Facilities	New Construction	CST	100 NCIIF	CST	50 NCIIF	CST	100 NCIIF	CST	50 NCIIF	CST	100 NCIIF
2411	Plaza Parkway Improvements	New Construction	CST	200 GCIF	CST	200 GCIF	CST	200 GCIF	CST	200 GCIF	CST	200 GCIF
2412	Sidewalks	New Construction	CST	200 TIF	CST	200 TIF	CST	200 TIF	CST	200 TIF	CST	200 TIF
2413	Downtown Intersection & Pedestrian Facilities	New Construction	CST	125 TIF	CST	125 TIF	CST	125 TIF	CST	125 TIF	CST	125 TIF
2414	Traffic Safety Program	New Construction	CST	250 TIF	CST	250 TIF	CST	250 TIF	CST	250 TIF	CST	250 TIF
2415	City Trails – Bicycle Trails	New Construction	CST	950 TIF	CST	950 TIF						
2416	Comprehensive Streetscaping/ Greenscaping	New Construction			CST	500 NCIIF			CST	500 NCIIF		
2417	BayWalk & SouthCore Garage Improvements	Maintenance	CST	300 DPCPF	CST	150 DPCPF	CST	50 DPCPF	CST	150 DPCPF	CST	50 DPCPF
2418	Traffic Signal Mast Arm Program	Safety Improvement	CST	1,200 TIF								
2419	Parking Meter Expansion	New Construction	CST	200 DPCPF								
2420	I-175 On Ramp/4th St. S Two-Way	New Construction	CST	500 MF	CST	500 MF	CST	800 MF	CST	600 MF	CST	300 MF

CST = Construction; NCIIF = Neighborhood and Citywide Infrastructure Capital Improvement Fund; TIF = Transportation Impact Fees Capital Projects Fund; GCIF = General Capital Improvement Fund; DPCPF = Downtown Parking Capital Projects Fund; MF = Multiple Fund Categories

**MUNICIPAL WORK PROGRAMS  
FY 2013/14 – 2017/18**

**ST. PETE BEACH**

**FUNDS ARE SHOWN IN THOUSANDS**

WORK ITEM #	PROJECT DESCRIPTION	TYPE WORK	13/14		14/15		15/16		16/17		17/18	
			CST	LF	CST	LF	CST	LF				
2500	Citywide Continuing Maintenance and Resurfacing Program	Resurfacing	CST	264 LF	CST	277 LF	CST	277 LF				
2501	Curb and Sidewalk Repairs (citywide)	Repairs	CST	75 LF	CST	79 LF	CST	79 LF				

CST = Construction; LF = Local Funds

**MUNICIPAL WORK PROGRAMS  
FY 2013/14 – 2017/18**

**TARPON SPRINGS**

**FUNDS ARE SHOWN IN THOUSANDS**

WORK ITEM #	PROJECT DESCRIPTION	TYPE WORK	13/14		14/15		15/16		16/17		17/18	
2600	Citywide Sidewalks	Improvements			CST	100			CST	100		
2601	Resurfacing City Streets	Paving			CST	200			CST	200		
2602	Brick Streets	Reconstruction	CST	300			CST	300				
2603	Dodecanese Blvd. (Sponge Docks) Streetscape	Design/ Construction	DGN/ CST	1,172								
2604	L&R Industrial Boulevard Extension	New Roadway Design	CST	2,100								

CST = Construction; DGN = Design

**MUNCIPAL WORK PROGRAMS**  
**FY 2013/14 – 2017/18**

**TREASURE ISLAND**

**FUNDS ARE SHOWN IN THOUSANDS**

WORK ITEM #	PROJECT DESCRIPTION	TYPE WORK	13/14		14/15		15/16		16/17		17/18	
2701	Beach Trail Extension from 119 <sup>th</sup> Ave to John's Pass										CST	1,000
2702	Rehabilitate Major Electrical Components of the Bascule Bridge Generator						CST	80				
2703	Rehabilitate Major Hydraulic Components of the Bascule Bridge Valves				CST	100						
2704	Street Resurfacing		CST	225	CST	305	CST	327	CST	300	CST	277
2705	Upgrade Street Signage for Reflectivity				CST	50						
2706	Install Pedestrian Crosswalk Signal in Downtown Area				CST	35						
2707	Completion of Capri Isle Bridge		CST	1,141								
2708	Completion of Isle of Palms Bridge		CST	883								
2709	Gulf Blvd Undergrounding						CST	3,770				
2710	Mast Arm Maintenance on Gulf Blvd at 112 <sup>th</sup> Ave, 117 <sup>th</sup> Ave and W Gulf Blvd		CST	50								
2711	New Crosswalks on Gulf Blvd				CST	25	CST	25	CST	25	CST	25



**TREASURE ISLAND**

**FUNDS ARE SHOWN IN THOUSANDS**

WORK ITEM #	PROJECT DESCRIPTION	TYPE WORK	13/14		14/15		15/16		16/17		17/18	
2712	Bascule Bridge Painting and Coating/Refurbishing Architectural Features and Concrete								CST	100	CST	150
2713	Bascule Bridge Upgrade Computers and Servers						CST	50				
2714	Treasure Island Causeway Bridge Lighting Replacement		CST	175	CST	100						

CST = Construction

FY 2013/14 – FY 2017/18 TRANSPORTATION IMPROVEMENT PROGRAM (TIP) AMENDMENT(S)

At the time of agenda preparation, there were no requests for TIP Amendments. If the need for a TIP amendment(s) arises following the distribution of the agenda packet, the Florida Department of Transportation (FDOT) will present the proposed amendment(s) under this agenda item.

ATTACHMENTS: Any proposed TIP amendment forms will be provided at the meeting

ACTION: As deemed appropriate based on discussion

TCC REVIEW OF ADVISORY COMMITTEE RECOMMENDATIONS

At the time of Agenda preparation, there were no advisory committee recommendations that would require referral to the TCC for technical consideration. If any recommendations are submitted, they will be presented at the time of the meeting.

ATTACHMENTS: None

ACTION: None required

OTHER BUSINESS

A. Uniform Trail/Roadway Signage Treatments

The kick-off meeting for the Uniform Trail /Roadway Signage effort was held on October 3, 2013 at Pinellas County DEI in Clearwater. An update will be provided at the meeting.

ATTACHMENTS: None

ACTION: None required, informational item

B. Memorial Causeway Bicycle Speeding Issue

The report is currently being revised. The TCC will receive an update regarding the final report and implementation schedule.

ATTACHMENTS: None

ACTION: None required, informational item

C. Multi-Named Roads

The MPO established a subgroup to discuss and review the C.R. 296 naming, as well as other multi-named corridors countywide. The TCC will be provided an update of the meeting.

ATTACHMENTS: [Multi-Named Roads Working Group Meeting Summary](#)

ACTION: None required, information item

D. Other

If any member has other business to discuss, they may address it under this item.

**Meeting Summary**  
**Multi-Named Roads Working Group**  
**September 18, 2013**  
**3:30 p.m.**

**Attendees:**

Karen Seel, County Commission  
Doreen Hock-DiPolito, Clearwater City Council  
Jeff Danner, St. Petersburg City Council  
Sarah Ward, Pinellas MPO  
Gina Harvey, Pinellas MPO  
Tom Whalen, City of St. Petersburg Planning  
Paul Bertels, City of Clearwater Traffic  
Mark Ely, City of Seminole Planning  
Bob Bray, City of Pinellas Park Planning

**Summary:**

Commissioner Seel called the meeting to order and provided a brief overview of the concerns with the multi-named roadways throughout the County. It was noted that the residents and visitors frequently have difficulty traversing the roadway system with multiple names and reference points. With over six million tourists per year, it is confusing to give directions throughout Pinellas County with the inconsistent roadway naming. The focus corridor for the past several years has been Bryan Dairy Road (C.R. 296). For these reasons, Commissioner Seel asked for this subgroup to be formed to discuss the multi-named roadways and the Bryan Dairy Road (C.R. 296) renaming. She asked that the group meet one time and she would report back to the MPO Board on the recommendations.

**Bryan Dairy Road (CR 296)**

The group began their discussion with the effort to provide a single name to the CR 296 Bryan Dairy Road corridor. It was noted that this corridor, (C.R. 296), is named 102<sup>nd</sup> Avenue North, Bryan Dairy Road, and 118<sup>th</sup> Avenue. The Subgroup discussed the background and history of this roadway and the various considerations on the renaming. Several years ago, most of the municipalities agreed to a full renaming of the corridor from Hamlin to the Interstate system. While the City of St. Petersburg was in favor of the naming of the corridor to Bryan Dairy Road to the west of U.S. Highway 19 (S.R.55), the City opposed renaming to Bryan Dairy Road for the section east of U.S. Highway 19 (S.R.55). The City of St. Petersburg determined that, due to their desire for a unique name to reflect the function of the roadway as the connector from the north to the south and east, the corridor should not be named Bryan Dairy Road.

In addition, the State Legislature acted in 2012 to designate the section of 118<sup>th</sup> Avenue North (C.R. 296) east of U.S. Highway 19 as the "St. Pete Crosstown". While not an official naming of the corridor, it created confusion with the uniformity goal. It was noted that this legislative designation requires the approval of the local government agencies (Pinellas County and Pinellas Park) prior to the implementation. This has not been implemented.

It was noted that the section of C.R. 296, east of U.S. Highway 19 (S.R.55), will be reconfigured significantly over the next several years with the addition of the Roosevelt Connector, the S.R. 690 Corridor extension, and the potential exchange of roadway jurisdictions in the area. For this reason, the Subgroup agreed to hold off on any immediate decisions in this area and recommended a meeting with the FDOT to further discuss the long-term plans for road jurisdiction, road numbering, and naming.

After additional discussion, it was noted that the local governments' previous agreements on the renaming of Bryan Dairy Road occurred several years ago and the positions may no longer be accurate. Due to concerns with economic impacts to the agencies as well as the business owners and residents, Pinellas Park would likely recommend a delay in any changes that would create financial issues for the affected properties.

It was noted that the 102<sup>nd</sup> Avenue North segment would be the best segment to start with the renaming to Bryan Dairy Road. 102<sup>nd</sup> Avenue North from Hamlin Boulevard to Lake Seminole Bridge, located just west of Starkey Road (C.R. 1), is primarily residential with most of the homes addressed to the interior roadway networks and not 102<sup>nd</sup> Avenue North. This would initiate the consistent roadway naming effort and provide a test case on proceeding to the rest of this corridor and potentially to other corridors. This would also provide the uniform name to Bryan Dairy Road for the corridor to the west of U.S. Highway 19 (S.R. 55).

The group agreed that the recommendation to the MPO would be to rename 102<sup>nd</sup> Avenue North from west of Starkey Road (C.R.1) to Hamlin Boulevard to Bryan Dairy Road.

**Countywide Multi-Named Roadways**

The Subgroup reviewed the list of the various multi-named roadways throughout Pinellas County. The corridors highlighters were the Alternate U.S. 19 corridor with 14 names, the C.R. 321 corridor, and the C.R. 1 corridor. Due to jurisdictional issues and the community identity with some of the roadway names, the group agreed that an option would be to modify the roadway signs to reflect the County or State Road number first followed by the road name. The Road Number would provide the consistency along the corridor while the name would still provide the community identity and not necessitate a road name changes for the property owners and residents. Sample as below:



Pinellas County representatives agreed to provide an estimate on the cost of changing the existing signs as recommended by the Subgroup.

**Other**

The Subgroup commented on the Advanced Intersection signs being installed throughout the County. This program was initially implemented by the Florida Department of Transportation on the State Road System. Pinellas County followed the recommendation and has implemented the Advanced Intersection signs on the major county roadways.

Commissioner Seel also discussed the desire to solicit public input from out of town visitors and tourists. This is a topic that could be considered through another effort.

MPO ACTIONS – OCTOBER 9, 2013

The October 9, 2013 Newsletter/Action Sheets is attached. A staff member will briefly review the actions taken by the MPO Board at the meeting.

ATTACHMENTS: [MPO Newsletter/Action Sheet – October 9, 2013](#)

ACTION: None required, informational item



**MPO**  
**Newsletter/Action Sheet**  
**October 9, 2013**

**I. CALL TO ORDER**

Chairman Karen Seel called the meeting to order.

**II. INVOCATION AND PLEDGE**

Commissioner Welch performed the invocation and Councilman Danner led the Pledge.

**III. CITIZENS TO BE HEARD**

There were no citizens who came forward.

**IV. CONSENT AGENDA**

The MPO approved the consent agenda, which included the September 11, 2013 MPO meeting minutes; invoices from Tindale-Oliver, URS, Laughlin and Associates, TBARTA (including invoices in the folders), and the MPO's share of the copier with PPC; Disadvantaged Business Enterprise goal; agreement with the City of St. Petersburg for Intermodal Study; and TBARTA as FTA Section 5307 Direct Recipient.

**V. PUBLIC HEARING ITEMS**

**A. Proposed Truck Route Plan Amendment – Sections of Keene Road and Sunset Point Road as Day-Time Only**

Following a public hearing, the MPO approved the Truck Route Plan amendment to designate Sunset Point Road from Hercules Avenue to Keene Road and Keene Road from Sunset Point Road to Gulf-to-Bay Boulevard as day-time only truck routes.

**VI. PRESENTATION AND/OR ACTION ITEMS**

**A. PSTA Items**

**1. Greenlight Pinellas – Financial Analysis**

Brad Miller, PSTA, provided an overview of the financial analysis being performed by Ernst & Young and HNTB for the Greenlight Program. The MPO will consider taking action regarding acceptance of Greenlight Pinellas as the basis for establishing the future transit network for transportation modeling purposes at their November MPO meeting as part of their Long Range Transportation Plan Update.

**2. PSTA Activities Report**

Brad Miller, PSTA, provided a brief report on PSTA activities.

**B. Approval of Functional Classification Map**

The MPO deferred action until their November meeting.

**C. Follow-Up on Multi-Named Roadways Working Group Meeting**

The MPO approved the recommendations from the Working Group to rename 102<sup>nd</sup> Avenue North as Bryan Dairy Road west of Starkey Road; and replace intersection way finding signs to show the County or State road number with the road name below for the multi-named roadways identified by the Working Group. The MPO also approved the Working Group recommendation for staff to meet with FDOT to discuss



the naming and numbering convention of the new C.R. 296/S.R. 690/S.R. 686 corridors. To address concerns about the cost of the sign replacement, the MPO recommended that the signs be replaced in the course of regular maintenance workover a ten-year period and urged staff to explore federal and state grant opportunities to fund the project.

**D. Update on Transportation Management Area Meeting of September 13**

The MPO appointed Karen Seel, Doreen Hock-DiPolito, and Jim Kennedy to serve on the Tampa Bay TMA Committee with Sandra Bradbury and Harriet Crozier as alternates. The next meeting is tentatively scheduled for Friday, November 15, in the morning.

**E. Committee Recommendations – BAC**

• **Endorse Greenlight Program With Inclusion of Bicycle and Pedestrian Projects and Fund Loop Completion**

The MPO deferred action on BAC Motion #13-9 until their November meeting.

• **Endorse the Bicycle Road Safety Audit Recommendation**

The MPO approved BAC Motion #13-10 to endorse the bicycle safety recommendations developed through a bicycle-focused Road Safety Audit for East Bay Drive and to encourage FDOT Traffic Operations and the City of Largo to implement the improvements within their respective responsibilities. In addition, the MPO approved the supplemental recommendation for widening the sidewalks on East Bay Drive to accommodate bicyclists.

• **Approval/Endorse Tri-County Trails Connection Study**

The MPO deferred action on BAC Motion #13-11 until their November meeting, at which time the final study will be presented to the MPO.

**VII. REPORTS/UPDATE**

**A. FDOT Updates**

• **Gandy Boulevard Project**

FDOT provided a brief update on the Gandy Boulevard project between 4<sup>th</sup> Street and Dr MLK Jr Street indicating they have reached a global agreement with Florida Gas Transmission but are still working on agreements with them for specific projects such as this one.

The Gandy Boulevard project between I-275 and U.S. 19 will be on the November MPO agenda.

• **Howard Frankland Bridge**

The MPO will have a presentation on their November agenda.

**B. Long Range Transportation Plan Update**

MPO staff indicated the MPO will be asked to accept the transit network for modeling efforts as part of the MPO's Long Range Transportation Plan Update at their November meeting.

**C. Status of MPO Reapportionment Plan**

MPO staff reported that the Governor still has not approved the Reapportionment Plan.

**D. Information Regarding MPOAC Institute Training**

MPO staff provided information as to the MPOAC Institute Training dates and indicated they will forward the flyer with information about the training to the MPO members once they receive it from CUTR and asked anyone interested in attending to contact MPO staff.

**VIII. INFORMATIONAL ITEMS**

**A. Request for PSTA to Become Designated Recipient Update**

MPO staff is in discussions with PSTA and the other partners regarding PSTA's request to become a Designated Recipient.

**B. Committee Appointments**

The City of Clearwater is seeking an appointment to the Bicycle Advisory Committee.

**C. Advisory Committee for the Pinellas Transportation (ACPT)**

The next ACPT meeting is Monday, October 14.

**D. 2040 Long Range Transportation Plan Stakeholder and Public Outreach Report**

E. An updated list of activities is included in the agenda packet.

**F. Correspondence**

**G. Other**

**PINELLAS COUNTY METROPOLITAN PLANNING ORGANIZATION  
MINUTES – MEETING OF NOVEMBER 13, 2013**

The Pinellas County Metropolitan Planning Organization, created by the State of Florida in accordance with Title 23 United States Code, Section 134 and Chapter 339.175 Florida Statutes, met in regular session on Wednesday, November 13, 2013 in the chambers of the Pinellas County Commission, 315 Court Street, Clearwater, Florida.

**MEMBERS PRESENT**

Karen Seel – *Chairman* – Board of County Commissioners  
Jeff Danner – *Vice Chairman* – Councilman, City of St. Petersburg  
Harriet Crozier – *Secretary/Treasurer* – Commissioner, City of Largo  
David Archie – Mayor, City of Tarpon Springs representing Tarpon Springs/  
Oldsmar/Safety Harbor  
Julie Bujalski – Commissioner, City of Dunedin, representing PSTA  
Doreen Hock-DiPolito – Councilmember, City of Clearwater (arrived at 1:07 p.m.)  
David Eggers – Mayor, City of Dunedin  
Charlie Justice – Board of County Commissioners  
Jim Kennedy – Councilman, City of St. Petersburg  
Ken Welch – Board of County Commissioners  
Debbie Hunt, non-voting advisory – (representing the Secretary, Florida Department of  
Transportation District 7)

**MEMBERS ABSENT**

Sandra Bradbury – Mayor, City of Pinellas Park

**OTHERS PRESENT**

Sarah Ward – MPO Interim Executive Director  
Al Bartolotta – Pinellas County MPO  
Gina Harvey – Pinellas County MPO  
David Sadowsky – County Attorney's Office  
Ming Gao – Florida Department of Transportation (FDOT)  
Brian Beaty – Florida Department of Transportation (FDOT)  
Alicia Parinello – Pinellas County MPO  
Chelsea Favero – Pinellas County MPO  
Joe Kubicki – City of St. Petersburg  
Tom Whalen – City of St. Petersburg  
Leland Dicus – City of Largo  
Bob Bray – City of Pinellas Park  
Patrick Murphy – City of Pinellas Park  
David Chase – City of Pinellas Park  
Paul Bertels – City of Clearwater  
Bill Jonson – City of Clearwater  
Brad Miller – Pinellas Suncoast Transit Authority (PSTA)  
Jim Green – Care Ride, LLC  
Brian Smith – BAC Chairman  
Nick Fritsch – Citizen, TBARTA CAC  
Jim Phillips – URS  
Ann Venables – URS  
Tony Hornik – Pinellas County DEI  
Paul Belhorn – Pinellas County DEI  
Amy Neidgringhaus – FDOT  
Carolyn Kuntz – MPO Recorder

I. **CALL TO ORDER**

Chairman Karen Seel called the meeting to order at 1:02 p.m.

II. **INVOCATION AND PLEDGE**

Mayor Eggers performed the invocation and led the Pledge of Allegiance.

III. **CITIZENS TO BE HEARD**

There were no citizens who came forward to be heard.

IV. **CONSENT AGENDA**

A. **Approval of Minutes – Meeting of October 9, 2013**

B. **Approval of Funds for MPOAC Legislative Advocacy Activities for FY 2013/14**

C. **Approval of Invoice – Tindale-Oliver and Associates**

***Councilman Danner moved, Commissioner Welch seconded, and motion carried to approve the Consent Agenda (Vote 10-0).***

V. **PUBLIC HEARING ITEM**

A. **Proposed Amendments to the FY 2013/14-2017/18 Transportation Improvement Program**

Mr. Beaty reviewed the two proposed amendments to the Transportation Improvement Program (TIP), noting they are both safety projects. The first amendment is for 54<sup>th</sup> Avenue South at 31<sup>st</sup> Street South for the construction of a dedicated eastbound left-turn lane to reduce the queuing of traffic approaching the Interstate. In addition, there will be a dedicated right-turn lane for traffic traveling south on 31<sup>st</sup> Street from 54<sup>th</sup> Avenue South. The construction cost is \$270,700 with funding from the Highway Safety Improvement Program. The second amendment is for 38<sup>th</sup> Avenue North from 4<sup>th</sup> Street to 1<sup>st</sup> Street North for the construction of continuous medians for access control, installation of traffic signals, and realignment of the Northeast Plaza driveway with the new traffic signal at 39<sup>th</sup> Street. The construction cost is \$439,000 with funding from the Highway Safety Improvement Program.

Upon query by Commissioner Welch, Mr. Beaty responded he would provide the information via email as to how far the left-turn lane would be extended on 54<sup>th</sup> Avenue South. Ms. Ward added the project was the result of a Congestion Management study and working with the City of St. Petersburg and FDOT. Joe Kubicki, City of St. Petersburg Director of Transportation, Parking, and Management, appeared and responded the lane was shifted in order to add a right-turn lane and the left-turn lane was extended slightly so additional right-of-way wasn't needed. In response to the time line, Mr. Kubicki indicated probably within the next six months but he would provide that information to him.

Commissioner Justice had some questions regarding the installation of a traffic signal at the 39<sup>th</sup> Street intersection at the shopping center, noting its proximity to an existing traffic signal. Mr. Kubicki explained the reasons for the traffic signal and that it will be coordinated with the existing signal.

Upon call for public comment, no one came forward.

Following the public hearing, ***Councilmember Kennedy moved, Commissioner Welch seconded, and motion carried to approve the two amendments to the TIP by roll call vote (Vote 10-0).***

\*\*Commissioner Welch left at 1:16 p.m. and returned 1:19 p.m.\*\*

**B. Fall Update of the Fiscal Years 2013/14 Through 2016/17/18 Transportation Improvement Program**

Mr. Bartolotta briefly reviewed the updated Transportation Improvement Program (TIP), noting the MPO adopted the TIP in June to incorporate the FDOT Work Program. The TIP is updated in the fall to incorporate the County and municipal work programs. He provided a brief overview of the updated TIP, including the differences.

Councilman Kennedy asked the County to explain the reasons the San Martin Bridge was moved back. Paul Bellhorn, Pinellas County Department of Environment and Infrastructure Design Section, came forward and explained this was originally a bridge replacement project. When they were going through the study, it was determined there was a trail designated from St. Petersburg to the north. In addition, there was a need to move the road to align with the bridge and to take into consideration the boat traffic. The costs increased to over \$2 million. The County needs to meet the federal requirements and guidelines to apply for federal grants to help fund the project. With the additional coordination and complications as well as the necessary requirements, the project date had to be pushed back. The Requests for Proposals have been issued and they are in the process of selecting a consultant and the project is moving forward.

Upon query by Commissioner Bujalski regarding the Dunedin Causeway Bridge and what is included in the Project Development and Environment Study (PD&E), Paul Bellhorn responded the County has already done the Feasibility Study and they are now going through the PD&E stage. They have issued a Request for Proposals and will be selecting a consultant. The PD&E includes reviewing all the alternatives, conducting the public meetings/hearings, and studying the environmental impacts. The County will be applying for federal grants; therefore, they have to follow the federal procedures. Commissioner Bujalski commented on the back up from Honeymoon Island and whether options to alleviate the problem would be included in the process. Mr. Bellhorn responded that will be included in the study.

Upon call for public comment, no one came forward.

Following the public hearing, ***Councilmember Danner moved, Mayor Archie seconded, and motion carried to approve the updated TIP by roll call vote (Vote 10-0).***

**C. Proposed Truck Route Plan Amendment – Reroute Designated Truck Route in Tarpon Springs**

Ms. Ward explained the MPO maintains the Truck Route Map and the regulatory agency is the local government. She summarized the request from the City of Tarpon Springs to amend the Truck Route Plan to remove the unrestricted designation from the western section of Tarpon Avenue between Alternate 19 and South Ring Avenue and redirect the truck traffic to the south by adding South Ring Avenue from Tarpon Avenue to Lemon Street and Lemon Street from South Ring Avenue to Alternate 19.

Ms. Harvey provided additional information regarding the background, the amendment, and reasons for the amendment, noting the City and the advisory committees recommended the amendment.

Ms. Ward added both the Technical and Citizens Committees recommended approval of the amendment.

Upon call for public comment, no one came forward.

Following the public hearing, ***Commissioner Welch moved; Councilmember Hock-DiPolito seconded, and motion carried to approve the amendment to the Truck Route Plan by roll call vote (Vote 10-0).***

**VI. PRESENTATION AND/OR ACTION ITEMS**

**A. ACPT Meetings of October 14 and November 4, 2013**

Ms. Ward provided highlights of the October and November ACPT meetings. The agendas are included in the agenda packet. At their November meeting, the ACPT adopted a resolution endorsing the Greenlight Plan and is requesting partner agencies to take similar action on the Plan.

**B. PSTA Items**

**1. Greenlight Pinellas**

Brad Miller, PSTA, indicated information regarding the financial analysis and other information on the Greenlight Plan are located on the Greenlight Plan website and Ernst and Young will be producing a written report on the information. A resolution to support the Greenlight Plan is included in the MPO member folders and the transit network will be included in the MPO's Long Range Transportation Plan for modeling once acted on by the MPO. He reviewed a PowerPoint presentation, which included the elements and information on the Greenlight Plan. The recommendations to provide accountability include maintaining a detailed website, ongoing oversight councils, and continuous outreach. The two proposed oversight committees are an Implementation Oversight Committee to oversee capital investments, progress on capital projects, and public outreach and a Transit Riders Advisory Committee to recommend operating policies and an operating budget, with both committees reporting directly to the PSTA Board and to the MPO and the County as needed. His presentation included financial assumptions and worst case scenarios, as well as mitigation strategies developed by Ernst and Young.

\*\*Councilman Danner left at 1:45 p.m. and returned 1:49 p.m.\*\*

\*\*Commissioner Justice left at 1:45 p.m.\*\*

***Councilman Danner moved and Councilmember Hock-DiPolito seconded a motion to approve the MPO resolution, including the transit network for modeling as part of the MPO's Long Range Transportation Plan.***

Mayor Archie asked what the plan is to convince the northern Pinellas County voters to support PSTA's long range planning and the transit surtax. Mr. Miller responded there is a private advocacy campaign that is being formulated whose task is to educate and advocate for the voter's support throughout all of Pinellas County. PSTA has been proactive in trying to meet the concerns of northern Pinellas County by increasing transit service and adding connections to Tampa. In addition, PSTA has included in the proposal the elimination of the ad valorem property tax upon a successful referendum. North Pinellas County residents should see a reduction in the amount they pay for their property tax.

Several MPO members provided additional comments such as flex service and enhanced transit service for northern Pinellas County, importance of educating/informing the citizens, invite PSTA to speak at Commission/Council meetings since they are televised and invite PSTA back to provide updates, the potential for increased economic development opportunities that would create additional jobs, embracing the questions people have upfront is important, and that elected officials need to be involved with informing the citizenry as well as PSTA.

Mayor Eggers asked whether the MPO resolution included elimination of PSTA's property tax. Councilman Kennedy responded PSTA's property tax authorization is through the creation of PSTA by Statute and that will not be revoked. PSTA can still levy property taxes by Statute, however, they will choose not to.

There was additional discussion whether there should be two separate votes to approve the resolution and to approve the Greenlight Plan for transit modeling purposes. Ms. Ward responded that both actions are encompassed in the resolution: accepting the transit networks for modeling and accepting the resolution of support since the resolution includes language for inclusion in the Long Range Plan development process.

Additional discussion followed whether the resolution should be modified to include language to eliminate PSTA's property tax upon a successful resolution. The MPO members were in agreement but noted the issue would be addressed with the County Commission and the PSTA Board. Mayor Eggers emphasized the need to make a statement regarding the elimination of PSTA's property tax.

Following discussion, ***the motion passed unanimously (Vote 9-0) to approve the resolution of support and include the transit network for modeling.***

## **2. PSTA Activities Report**

Part of the previous discussion included PSTA activities.

### **C. FDOT Items**

#### **1. Gandy Boulevard Between I-275 and U.S. 19**

Amy Neidgringhaus, FDOT, reviewed a PowerPoint presentation for Gandy Boulevard from east of U.S. Highway 19 to east of I-275. The presentation included the existing conditions and purpose for the improvement, which is to improve safety, reduce congestion, accommodate future traffic demands, and enhance freight and passenger movement along Florida's Strategic Intermodal System. She provided some history; changes to Grand Avenue for an at-grade intersection with a traffic signal; removal of a traffic signal and ramp at I-275; coordinated with the St. Petersburg's City Trails Master Plan where a trail was proposed to 28<sup>th</sup> Street but, based on feedback, that trail will not be implemented. The proposed final recommendation includes four lanes to six lanes, curb and gutter, and median barrier on the west end; six lanes, curb and gutter, grass median, bike lanes, sidewalk, and pedestrian trail (will be eliminated based on feedback) in the center section; and six lanes, grass median, bike lanes, sidewalk, and pedestrian trail \*will be eliminated based on feedback) on the east end. The letting date is anticipated in September of 2021 at a cost estimate of \$38 million.

**\*\*During the presentation, Commissioner Justice returned to the meeting at 2:25 p.m.\*\***

Councilman Kennedy had a concern with not including the bike trail in the project. Joe Kubicki, City of St. Petersburg Director of Transportation, Parking, and Management, appeared and responded there is a concern whether bike lanes or a separated bike path is the best solution for high-volume, high-speed roadways for the safety and convenience of commuter bicyclists. To the east of I-275, the Progress Energy Trail parallels Gandy Boulevard and, to the west, there is a proposal for bike lanes. In discussions with FDOT, they have indicated there are no plans for a bike path to the west of I-275.

Commissioner Welch indicated there was the same type of concern on Ulmerton Road west of 49<sup>th</sup> Street with mixing the speed of vehicle traffic with bicyclists but there doesn't seem to be a problem. He asked whether bicycle crashes on major roadways are tracked, such as Ulmerton Road. Ms. Neidgringhaus responded that she would have to get back with that information since she wasn't sure how it was tracked and whether it was based on roadways.

Upon further query by Commissioner Welch regarding the trail to 28<sup>th</sup> Street no longer being part of the project, Ms. Neidgringhaus responded the feedback she had received was that it was no longer needed; that it is within the County's jurisdiction. Mr. Bartolotta added that he would have to look at the history and get back with that information.

**Councilman Kennedy requested the MPO receive a report on the specifics.**

Chairman Seel thanked FDOT for reworking the plan to make the road more business and driver friendly.

Upon a question by Ms. Neidgringhaus whether the MPO wanted bike lanes along high-speed roadways, Chairman Seel asked her to coordinate with the MPO staff as to the best solution.

**\*\*Commissioner Welch left at 2:37 p.m. and returned at 2:42 p.m.\*\***

Chairman Seel asked about the 78<sup>th</sup> Avenue connector from Gandy Boulevard to U.S. 19. Ms. Neidgringhaus responded she thought that was a local project. Ming Gao, FDOT, added that FDOT has talked with the City of Pinellas Park staff and they hope to coordinate the City's plan with FDOT's plan and to combine the two improvement projects. Chairman Seel felt that would provide some relief for motorists at that intersection.

**2. Update on Gandy Boulevard Project Between 4<sup>th</sup> Street and Dr. MLK Jr. Street**

Ming Gao, FDOT, indicated FDOT is still working with the contractor.

**\*\*Chairman Seel left the meeting at 2:38 p.m. and turned over the gavel to Vice Chairman Danner\*\***

FDOT has established an escrow account and they hope to have a resolution soon.

Upon query by Councilman Kennedy as to an extension of the contract beyond November 15, Mr. Ming responded FDOT has been in constant discussion and working closely with the contractor. They are trying to get the project underway and the design/build team has put in a lot of work.

**3. Howard Frankland Bridge**

Mr. Gao, FDOT, provided an update on the Howard Frankland Bridge Project Development and Environmental (PD&E) Study and public hearings. FDOT held a public hearing in both Pinellas and Hillsborough Counties. The study encompassed two parts: replacement of the bridge and how to accommodate a premium transit connection between the two counties.

**\*\*Chairman Seel returned at 2:43 p.m.\*\***



There were 160 total attendees at the public hearings, with 50% supporting managed/express lanes, 25% supporting transit options/envelope, 38% supported light rail, and 35% not supporting rail. FDOT has completed the PD&E portion for the bridge replacement and will transmit it to the Federal Highway Administration by the end of the year. FDOT will continue to work on the transit portion but will be as flexible as possible to accommodate the technology decision. FDOT Secretary Prasad announced FDOT will spend an additional \$25 million to enhance the bridge structure to accommodate light rail in the future.

Councilman Danner asked whether FDOT had maps available that showed the projects under construction from the east side in Tampa through the S.R. 60 interchange, the Howard Frankland Bridge to the west end in Pinellas County and what those time lines are, where they are in the queue for design, right-of-way, and construction so they can have an overview of the concept and phases. Mr. Gao responded that FDOT could provide that information at a future meeting to show the timeline of funded projects and the concepts moving forward. Councilman Danner wanted to make sure all the projects were tied together. Mr. Gao responded they would show the S.R. 60 concept, the I-275 concept, and how they tie into the Howard Frankland Bridge for both sides of the Bay, tying into the Greenlight Pinellas, the intermodal center, Westshore, and the Tampa International Airport study currently underway.

Chairman Seel asked what the timeline is for selecting the technology. Mr. Gao responded that Friday's meeting will provide a good starting point. There has been activity on the Hillsborough side since they are looking at putting express bus service in the managed lanes.

Ms. Ward noted the advisory committees and MPO staff had a recommendation and asked the MPO to approve the recommendation that the Howard Frankland Bridge be designed to accommodate rail and the Technical Coordinating Committees' recommendation that the matter go to the Transportation Management Area Working Group.

***Councilman Danner moved and Councilmember Hock-DiPolito seconded a motion to approve the advisory committees and staff recommendations as outlined above.***

Commissioner Bujalski thanked FDOT for their support.

Commissioner Welch asked if the \$25 million enhancement would support either transit option and Mr. Gao responded yes.

Commissioner Welch asked if the intermodal center in the Westshore area would serve as a hub to accommodate different modes of technology. Mr. Gao responded the concept is the airport could extend their automated people mover to Westshore to serve as a connecting point for the region as a regional transit service and serve as an extension of the airport. The facility can be designed in such a way for different technologies but not using the same track; however, he felt people would want to stay on one system and not switch from one mode of technology to another. One technology needs to be decided for crossing the bridge.

Following discussion, ***Councilmember Hock-DiPolito moved, Commissioner Welch seconded, and motion carried to approve the advisory committees and staff recommendation (vote 10-0).***

**4. Multi-Use Paths Policy for State Roads**

Mr. Gao indicated FDOT is still working with the City of St. Petersburg. Councilman Kennedy stated the City's intent was not looking at a statewide policy but to construct and maintain a separate path along Roosevelt Boulevard. They don't need sidewalks on both sides of the road but would like a multi-use trail on the other side that could be constructed as part of the resurfacing project.

Joe Kubicki, City of St. Petersburg Director of Transportation, Parking, and Management, indicated they have met with FDOT and should reach closure on this son; however, he requested this item be placed on next month's MPO agenda. FDOT recognizes there is a problem with high-speed, high-volume roadways and mixing bicyclists.

**This item was deferred until the next MPO meeting.**

\*\*Due to the lateness of the hour, Chairman Seel proceeded to the items that required action and deferred the Beckett Bridge presentation until the end of the agenda\*\*

**E. Functional Classification Map**

***Councilman Danner moved, Commissioner Welch seconded, and motion carried to authorize the MPO Chairman to sign the final 2010 urbanized area boundaries and Federal Function Classification Map (Vote 10-0).***

**F. Committee Recommendations**

**1. Bicycle Advisory Committee (BAC)**

- **Endorse Greenlight Program With Inclusion of Bicycle and Pedestrian Projects and Fund Loop Completion**

***Councilman Danner moved and Commissioner Crozier seconded a motion to approve recommendations #1 and #2 of BAC Motion #13-9 and receive and file recommendation #3.***

Brian Smith, BAC Chairman, came forward and indicated the BAC concurred with receiving and filing recommendation #3 with the idea they would seek other funding sources for the trail loop.

Chairman Seel asked whether they received a final determination if the transit sales tax could be used to fund such projects. Dave Sadowsky, MPO attorney, responded there has not yet been a final determination; however, there is a Memorandum of Understanding that implies those types of projects aren't allowed. The transit tax referendum doesn't include a road category so roads would not be viable.

Following discussion, ***the motion passed (Vote 10-0).***

**2. Pedestrian Transportation Advisory Committee (PTAC)**

- **Encourage Acknowledgement of Pedestrian Safety Awareness Week of October 28 Through November 4, 2013 (PTAC Motion #13-3)**

***Councilmember Hock-DiPolito moved, Commissioner Crozier seconded, and motion carried to approve PTAC Motion #13-3 endorsing Pedestrian Safety Awareness Week of October 28 through November 4 (Vote 10-0).***

**3. Technical Coordinating Committee (TCC)**

- **Multi-Named Roadway Signage**

***Mayor Eggers moved, Councilman Danner seconded, and motion carried to defer action on the TCC recommendation until the next MPO meeting (Vote 10-0).***

**G. Transportation Disadvantaged Service Plan (TDSP) Amendment**

***Mayor Eggers moved and Commissioner Crozier seconded a motion to approve the proposed amendments to the Transportation Disadvantaged Service Plan.***

Mr. Miller noted the table showing the provider rates is accurate; however, the footnotes regarding the bus passes have been rescinded and will be going back to the Local Coordinating Board.

***Noting this change, the motion to amend the Plan carried, with the footnotes to be stricken (Vote 10-0).***

**H. Establishment of Nominating Committee for 2014 Election of Officers**

Upon call for volunteers, Jeff Danner, Harriet Crozier, and Doreen Hock-DiPolito volunteered to serve on the Nominating Committee.

***Commissioner Crozier moved, Mayor Archie seconded, and motion carried to approve the appointment of Councilman Danner, Commissioner Crozier, and Councilmember Hock-DiPolito to serve as the Nominating Committee (Vote 10-0).***

It was announced the Nominating Committee will meet immediately after the MPO meeting.

**I. Committee Appointments**

***Councilman Danner moved, Councilman Kennedy seconded, and motion carried to approve the appointments of John Villeneuve as the PSTA representative and Christopher Cochran as the alternate to the Technical Coordinating Committee and Daryl Krumsieg as a St. Petersburg representative to the Citizens Advisory Committee (Vote 10-).***

**VII. REPORTS/UPDATE**

**A. Status of MPO Reapportionment Plan**

Ms. Ward reported the Reapportionment Plan is starting to move through the process. They received some comments that she briefly highlighted, noting they responded to FDOT via email. The MPO staff is recommending the MPO approve the modifications to the MPO Reapportionment Plan.

***Commissioner Crozier moved, Councilmember Hock-DiPolito seconded, and motion carried to approve the modifications to the Reapportionment Plan (Vote 10-0).***

**VIII. INFORMATIONAL ITEMS**

**A. Request for PSTA to Become Designated Recipient Update**

**B. 2040 Long Range Transportation Plan Stakeholder and Public Outreach Report**

**C. MPOAC Meeting of October 31, 2013**

**D. Transportation Management Area (TMA) Meeting of November 15, 2013**

**E. Joint Chairs Coordinating Committee Meeting of December 13, 2013**

**F. Correspondence**

**G. Other**

Due to the lateness of the hour, there was no discussion on these items.

\*\*Councilmember Kennedy left at 3:03 p.m. and returned at 3:06 p.m.\*\*

**D. Beckett Bridge Project Presentation**

Ann Venables and Jim Phillips, URS, provided a brief presentation on the Beckett Bridge Project and the recommended alternative. They noted the location of the bridge in Tarpon Springs, history of the bridge, structure condition, the bridge is functionally obsolete, structurally deficient, the unforeseen conditions including remnants of sink holes under the bridge, support for replacement of a new moveable bridge, the community's concerns, the bridge's historical significance, review of the various alternatives, review of the cost estimates for the alternatives, and discussions with the Federal Highway Administration and SHPO. Based on the extensive evaluation and public comment, the recommended alternative was for the construction of a new moveable bridge with two lanes in the same location and would include shoulders and sidewalks where right-of-way is adequate. Mitigation will be required. They will present the recommended alternative at a public hearing in February but will include information regarding all the options. They will go back to the Board of County Commissioners (BCC) to reconfirm their recommendation, finalize the documents, and seek the Federal Highway Administration approval to receive federal funding.

\*\*During the presentation, Councilmember Danner left at 3:06 p.m. and returned at 3:10 p.m.\*\*

Councilmember Hock-DiPolito asked the consultant if there was a study performed on the sink holes and Mr. Phillips provided that information.

Mayor Archie asked for information regarding the public hearing, Ms. Venables provided that information noting they are working with the Yacht Club and invitations will be sent out to all interested parties once they have a firm date and time. She will also include the date for the follow-up BCC meeting.

Councilmember Hock-DiPolito asked if they could request a report on the sink hole findings or if the BCC received that information. Mr. Phillips responded the County has a copy but he could provide another copy, noting this was done as part of a previous feasibility study.

***Councilman Danner moved, Councilman Kennedy seconded, and motion carried to endorse the recommended alternative to go to public hearing (Vote 10-0).***

**IX. ADJOURNMENT**

There being no further business, the meeting was adjourned at 3:18 p.m.

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Karen Seel, Chairman



November 18, 2013

Mr. Bob LaSala, County Administrator  
Pinellas County Government  
310 Court Street  
Clearwater, FL 33756

Dear Bob,

At the November 13, 2013 meeting, the Pinellas County Metropolitan Planning Organization (MPO) reviewed the alternatives considered for the Beckett Bridge project. Pinellas County, with the Florida Department of Transportation (FDOT), is conducting a Project Development and Environment (PD&E) Study to evaluate the removal, rehabilitation, or replacement of the existing Beckett Bridge over Whitcomb Bayou in Tarpon Springs.

During the evaluation, the following alternatives were evaluated:

- No Build
- No Build with Permanent Removal of the Existing Bridge
- Rehabilitation of the Existing Bridge
- Replacement with a New Movable Bridge
- Replacement with a New Fixed Bridge (with 28 feet of vertical clearance)

The MPO's Pedestrian Transportation Advisory Committee (PTAC), Bicycle Advisory Committee (BAC), Technical Coordinating Committee (TCC), and the Citizen Advisory Committee (CAC) reviewed the evaluation. The TCC and the CAC endorsed the study and the Recommended Alternative. The PTAC and BAC recommended that any alternative recommended for consideration include bicycle and pedestrians accommodations.

Based on the presentation, advisory committee recommendations and a review of the alternatives, the MPO unanimously endorsed the Recommended Alternative to replace the bridge with a two-lane movable structure with bicycle and pedestrian accommodations.

We appreciate the opportunity to provide this additional public review for the Beckett Bridge project and look forward to participating in the public hearings scheduled in February.

Sincerely,

Karen Williams Seel, Chair  
Pinellas County  
Metropolitan Planning Organization

cc: David Scott, DEI Director  
Paul Bellhorn, DEI Project Engineer

H:\users\cendocs\MPOMPO follow-up letters

**PINELLAS COUNTY METROPOLITAN PLANNING ORGANIZATION  
MINUTES – MEETING OF NOVEMBER 14, 2012**

The Pinellas County Metropolitan Planning Organization, created by the State of Florida in accordance with Title 23 United States Code, Section 134 and Chapter 339.175 Florida Statutes, met in regular session on Wednesday, November 14, 2012 in the chambers of the Pinellas County Commission, 315 Court Street, Clearwater, Florida.

**MEMBERS PRESENT**

David Eggers – *Chairman* – Mayor, City of Dunedin Commission  
Karen Seel – *Vice Chairman* – Board of County Commissioners (arrived at 1:08 p.m.)  
Jeff Danner – *Secretary/Treasurer* – Councilman, City of St. Petersburg Council  
Julie Bujalski – Commissioner, City of Dunedin, representing PSTA  
Harriet Crozier – Commissioner, City of Largo  
Jim Kennedy – Councilman, City of St. Petersburg Council  
Jim Ronecker – Mayor, City of Oldsmar representing Oldsmar/Safety Harbor/Tarpon Springs  
Ken Welch – Board of County Commissioners (arrived at 1:02 p.m.)  
Ming Gao, non-voting advisory – (representing Secretary, Florida Department of Transportation District 7)

**MEMBERS ABSENT**

Sandra Bradbury – Mayor, City of Pinellas Park  
Neil Brickfield – Board of County Commissioners  
Doreen Hock-DiPolito – Councilmember, City of Clearwater

**OTHERS PRESENT**

Sarah Ward – MPO Interim Executive Director  
Al Bartolotta – Pinellas County MPO  
Gina Harvey – Pinellas County MPO  
David Sadowsky – County Attorney's Office  
Lee Royal – Florida Department of Transportation (FDOT)  
Brian Beaty – Florida Department of Transportation (FDOT)  
Gary Thompson – Florida Department of Transportation (FDOT)  
Heather Sobush – Pinellas County MPO  
Alicia Parinello – Pinellas County MPO  
Chelsea Favero – Pinellas County MPO  
Don Eastman – Eckerd College  
Mary Gregory – Eckerd College/ASPEC  
Seena Baker – Eckerd College/ASPEC  
Martha Sweeny – Eckerd College/ASPEC  
Jim Sweeny – Eckerd College/ASPEC  
Edward Herman – Eckerd College/ASPEC  
Halina Herman – Eckerd College/ASPEC  
Bill Stickley – Eckerd College/ASPEC  
Laila Petrou – Eckerd College/ASPEC  
Lisa A. Mets – Eckerd College President's office  
Bill Baker – Eckerd College  
Julia Lewis – Eckerd College/ASPEC  
William J McKenna, Jr – Eckerd College  
Stanley Reimer – Eckerd College/ASPEC/Dolphin Cay  
Jane McBride – Eckerd College/ASPEC  
Ryan Jarrett – Eckerd College/ASPEC  
Jesalyn Darling – Eckerd College/ASPEC  
Ashoh Kalro – Eckerd College/ASPEC  
Jim Horner – Eckerd College/ASPEC  
Kathleen Peters – Florida House of Representatives  
Jared Schneider – Tindale-Oliver and Associates

Ann Venables – EC Driver & Associates  
Jim Phillips – EC Driver & Associates  
Vivian Peters – Transit Advisory Committee (TAC)  
Tom Whalen – City of St. Petersburg  
Joe Kubicki – City of St. Petersburg  
Bob Bray – City of Pinellas Park  
David M. Chase – City of Pinellas Park  
Jim Green – CareRide  
Brad Miller – Pinellas Suncoast Transit Authority (PSTA)  
Cassandra Borchers – Pinellas Suncoast Transit Authority (PSTA)  
Smith Howard – Citizen  
Carolyn Kuntz – MPO Recorder

**I. CALL TO ORDER**

Chairman Eggers called the meeting to order at 1:01 p.m.

**II. INVOCATION AND PLEDGE**

Commissioner Bujalski performed the invocation and led the Pledge of Allegiance.

\*\*Commissioner Welch arrived at 1:02 p.m.\*\*

**III. CITIZENS TO BE HEARD**

Upon call for public comment, Don Eastman (Eckerd College President), Bill Stickley (Eckerd College/ASPEC/Dolphin Cay), Ryan Jarrett (Eckerd College senior representing the student body and Eckerd College Ethics Bowl), and Representative Elect Kathleen Peters (House of Representatives District 69) came forward to speak on behalf of the numerous attendees, Eckerd College Board of Trustees, students, and faculties; nearby residents; and the Academy for Senior Professionals at Eckerd College (ASPEC – a senior volunteer organization) requesting the MPO's support and FDOT's reconsideration of the need for the installation of a traffic signal at the entrance to Eckerd College. They spoke regarding the recent death of Bob Shepherd as the result of a tragic car accident at that entrance, who had been a strong advocate for a traffic signal. They emphasized the number of students and adults who attend that facility, as well as the number of faculty and others who are employed there. FDOT had performed 3 studies within 11 years regarding the need for a traffic signal at this location and denied a signal each time. The City of St. Petersburg is supportive of a traffic signal. It's a complicated entrance with bike paths, bike trails, and the entrance/exit is onto a busy multi-lane highway that leads to St. Pete Beach. Once the Bayway Bridge is complete, there will be less opportunities to exit the college and, in addition, there will be an increase in traffic. They requested the MPO's support in asking FDOT to re-evaluate the situation and look at all the issues and not just the numbers for the installation of a traffic signal.

\*\*During public comment, Commissioner Seel arrived at 1:08 p.m.\*\*

The MPO expressed their condolences to the Shepherd family and Eckerd College community for their loss.

***Councilman Kennedy moved and Commissioner Welch seconded a motion that the MPO supports and requests FDOT re-evaluate the installation of a traffic signal at the entrance to Eckerd College.***

The MPO noted this is a problem and dangerous intersection and they support the motion.

Mr. Gao, FDOT, expressed his condolences to the Shepherd's family and friends and the Eckerd College family on behalf of FDOT. He indicated that, immediately after FDOT heard about the tragic accident, they scheduled a re-evaluation of the intersection for a traffic signal and staff is out there today to look at the intersection, including looking at recent crash data. FDOT evaluated the intersection in 2009 and it did not meet warrants. At that time, FDOT looked at crashes that could be corrected with the installation of a traffic signal and found there was only one. FDOT looks at the

type of crashes to justify installation of a traffic signal and not just the warrants. FDOT will re-evaluate the intersection and bring the results of the study back as quickly as possible.

Commissioner Welch noted there are a number of trail projects in this vicinity that will increase the number of pedestrians and bicyclists. Mr. Gao responded that FDOT is committed to ensuring the safety of pedestrians and bicyclists and this will be part of the re-evaluation.

Following discussion, ***the motion was approved (Vote 8-0).***

**IV. CONSENT AGENDA**

- A. Approval of Minutes – Meeting of October 10 , 2012**
- B. Approval of Invoices – Tindale-Oliver and Associates**
- C. Approval of Funds for MPOAC Legislative Advocacy Activities for FY 2012/13**
- D. Approval of Funds for the Tampa Bay Area Regional Transportation Authority (TBARTA)**
- E. Approval of Job Access Reverse Commute (JARC) Contract Amendments with Hillsborough – Extension of End date**
- F. Approval of Committee Appointments (Bicycle Advisory Committee)**
- G. Approval of Traffic Counter Repairs**
- H. Approval of Interlocal Agreement With PSTA (in folders)**

***Mayor Ronecker moved, Commissioner Welch seconded, and motion carried to approve the Consent Agenda as presented (Vote 8-0), which includes the October 10, 2012 MPO meeting minutes; invoices from Tindale-Oliver and Associates; funds for the MPOAC and TBARTA, contract amendments with Hillsborough for JARC funds, appointment of Barbara Hoffman as North County and Dr. Lynn Bosco and Steve Lasky as At Large representatives on the Bicycle Advisory Committee; traffic counter repairs; and an Interlocal Agreement with PSTA (included in member folders).***

**V. PUBLIC HEARING ITEMS**

**A. Transportation Improvement Program Amendment**

Brian Beaty, FDOT, reviewed the proposed Transportation Improvement Program (TIP) amendment, which will add \$50,000 from Section 5312 Veterans Transportation and Community Living Initiative grant that will be used by TBARTA for marketing, outreach efforts, and monitoring the effectiveness of the One-Call/One-Click Transportation Information Service.

Upon call for public comment, no one came forward to speak.

***Councilman Danner moved, Commissioner Welch seconded, and motion carried to approve the TIP amendment by roll call vote (Vote 8-0).***

**B. Fall Update to the Transportation Improvement Program**

Al Bartolotta reviewed the fall update of the Transportation Improvement Program (TIP) that rolls in the recently-adopted Pinellas County and municipal Work Programs. He reviewed the differences table that indicates the changes from the last TIP. The Keystone Road and Bryan Dairy Road projects are under construction and will be completed soon. There are 14 intersection projects at a cost of \$11.2 million and 7 bridge projects at a cost of \$15.3 million. The Starkey Road six-lane and 102<sup>nd</sup> Avenue four-lane widening projects have been included but MPO staff is working with the County's Department of Environmental and Infrastructure to look at other types of improvements that could be accomplished instead of adding lanes. The Citizens Advisory Committee (CAC) had a concern with the potential impacts of improvements to the Bayside Bridge at the same time as U.S. 19 is under construction. In discussions with the County's Department of Environmental and Infrastructure (DEI) staff, they have indicated there will only be lane closures during off-peak hours. The CAC also had a concern with the



continued slippage of the Progress Energy Trail and endorsed continuing to move forward with this project.

Commissioner Bujalski requested information regarding the Dunedin Causeway Bridge project moving out of the Work Program but adding in the bridge repairs. The study is to be done in 2016/17. Tony Hornick, County DEI, responded that repairs are necessary before replacement of the bridge occurs, which will be necessary in 10 to 12 years. The drawbridge needs frequent maintenance, the bridge is 50 years old, and there is contamination of the concrete. Regarding the determination that several years ago the bridge was considered functionally obsolete, Mr. Hornick responded the bridge has narrow lanes and other safety features that don't meet current design standards. Commissioner Bujalski asked for information regarding the decision-making process. Mr. Hornick added routine bridge inspections are performed and each element is rated and, based on those ratings, the bridge is given a structural efficiency rating based on deficiency, safety, and essentiality. Paul Belhorn, County DEI, provided additional comments that the cost to replace the bridge is very expensive; therefore from a programming standpoint, it was determined to replace a large number of smaller bridges and structures that are in need of replacement with funds from the current "Penny" sales tax and move forward with a Preliminary Engineering study for the Dunedin Causeway Bridge so that the bridge replacement could be funded with the next "Penny" sales tax. Revenues from the "Penny" sales tax continues to be reduced and adjustments are made accordingly. The bridge is functionally obsolete but it is not in bad condition compared to other structures. The County will continue to make repairs for the next several years until it can be replaced. Commissioner Bujalski requested additional information regarding the original study date for bridge replacement, as well as the problems and plans. Ms. Ward responded staff will get that information. Commissioner Bujalski stated she would like additional information regarding the process, original study date, and other such information before approving the two projects as part of the updated TIP.

Additional discussion ensued.

***Commissioner Bujalski moved and Commissioner Welch seconded a motion to approve the updated TIP minus the two items related to the Dunedin Causeway project and bring those two items back for action at the next MPO meeting with additional information.***

Councilman Kennedy asked how organizations can provide input to projects, noting there were two organizations interested in the San Martin Bridge (Page 245) project. Mr. Bartolotta responded a workshop will be held in the area and he will make sure interested groups are invited to participate.

Following a call for public comment (no one came forward to speak), ***the motion was approved by roll call vote (Vote 8-0).***

**C. Job Access Reverse Commute (JARC)/New Freedom Program of Projects**

Ms. Ward indicated the proposed projects for the 2012 Job Access Reverse Commute (JARC) and New Freedom Programs are included in the backup material. There is approximately \$1.2 million available for JARC and \$900,000 for New Freedom. Both of these programs have been eliminated under the new federal legislation, MAP-21, and rolled into other programs (this will be covered later on the agenda). The Local Coordinating Boards (LCB) in Hillsborough, Pasco, and Pinellas have reviewed the projects and approved the priority ranking. In addition, both the Hillsborough and Pasco MPOs have approved the projects. The projects are now before the Pinellas MPO for approval.

Upon call for public comment, no one came forward to speak.

***Commissioner Welch moved, Commissioner Seel seconded, and motion carried to approve the JARC and New Freedom Program of Projects for FY 2012 by roll call vote (Vote 8-0).***

## VI. ACTION ITEMS

### A. Approval of New Freedom Contract Amendment With Quality of Life – Extension of End Date (Including Financial Management Oversight Update)

Ms. Ward indicated, as a result of the Federal Transit Administration's (FTA) Financial Management Oversight (FMO) review of Quality of Life and the MPO and the MPO's response to FTA regarding the recommendations, MPO staff is requesting a release of the 2011 funds (approximately \$309,000) to Quality of Life that had been held in abeyance awaiting the outcome of the FMO review, with those funds to be amended into the 2011 agreement. MPO staff will request a budget revision from FTA to move the \$309,000 from contingency so that Quality of Life can access those funds to continue to provide services. There are a few other minor amendments, including the addition of "e-verify" to the contract.

*Councilman Danner moved, Councilman Kennedy seconded, and motion carried to approve the Amendment No 1 to the 2010 agreement between the Pinellas County MPO and Quality of Life Community Services, Inc. (Vote 8-0).*

### B. Committee Recommendations

#### 1. Bicycle Advisory Committee

##### a. Motion Supporting Dunedin Bicycle Trail Plans

The Bicycle Advisory Committee (BAC), at their last meeting, received a presentation from the City of Dunedin regarding its Citywide Master Plan, which includes a connection to the Pinellas Trail. After the presentation, the BAC passed a motion expressing their support of Dunedin's bicycle trails plan.

*Commissioner Bujalski moved, Councilman Kennedy seconded, and motion carried to approve BAC Motion #12-4 (Vote 8-0).*

##### b. Motion Supporting Conceptual Amenities Plan for Courtney Campbell Causeway

The Bicycle Advisory Committee (BAC), at their last meeting, received a presentation regarding conceptual enhancements to the Courtney Campbell Causeway Trail that will include landscaping, benches, and shade trees. The BAC passed a motion supporting the conceptual amenities to the trail.

Commissioner Seel indicated her support but expressed concern regarding the high costs for the amenities.

*Councilman Kennedy moved, Commissioner Welch seconded, and motion carried to approve BAC motion #12-5 (Vote 8-0).*

#### 2. Madonna Boulevard/Pinellas Bayway – Pedestrian Transportation Advisory Committee and Technical Coordinating Committee Recommendations

The Pedestrian Transportation Advisory Committee (PTAC), at their last meeting, reviewed pedestrian issues regarding the intersection of Madonna Boulevard and the Pinellas Bayway and recommended FDOT install rectangular rapid flashing beacons at this location to improve pedestrian safety. This recommendation was forwarded to the Technical Coordinating Committee (TCC) for their review. Mr. Bartolotta showed the location on a map and indicated the improvements already completed by FDOT. Even with the improvements, the PTAC felt there were still issues. The TCC reviewed the PTAC recommendation at their last meeting and, after discussion, recommended the MPO request FDOT to conduct a traffic study to perform an overall assessment of the entire intersection to address issues.

Mr. Gao responded FDOT is coordinating with their Traffic Operations Office to look at the intersection and will provide preliminary results at the next meeting.

*Councilman Kennedy moved, Commissioner Bujalski seconded, and motion carried to approve the TCC recommendation (Vote 8-0).*

**C. Establishment of Nominating Committee for 2013 Election of Officers**

Chairman Eggers asked for volunteers to serve on the Nominating Committee and Commissioner Welch, Commissioner Crozier, and Councilman Kennedy volunteered. Chairman Eggers announced the Nominating Committee would meet immediately upon adjournment of the MPO meeting.

*Councilman Danner moved, Commissioner Bujalski seconded, and motion carried to approve Commissioner Welch, Commissioner Crozier, and Councilman Kennedy as serving on the Nominating Committee (Vote 8-0).*

**VII. PRESENTATION ITEMS**

**A. Beckett Bridge Project Presentation**

Ms. Ward noted the advisory committees have received the Beckett Bridge presentation.

Ann Venables, EC Driver, indicated a public workshop will be held on this project in January. She then reviewed a PowerPoint presentation regarding the Beckett Bridge PD&E Study, which included the project limits, information and issues with the existing bridge, community concerns, opportunities for community input, the four alternatives and associated constraints or issues, proposed typical sections, possible detour routes, boat survey, environmental impacts, upcoming community involvement activities and PD&E Schedule. The consultant will be presenting this project to the stakeholders prior to holding a public meeting in January. The next step is to select a preferred alternative and hold a public hearing in the summer of 2013, deal with the issues related to the selected alternative, and anticipate receiving approval in December of 2013 from the Federal Highway Administration.

\*\*During the presentation, at 202 p.m., Commissioner Welch left the meeting\*\*

**VIII. REPORTS**

**A. FTA Section 5307 Program Funding Allocation**

Ms. Ward indicated there are changes to the FTA Section 5307 formula as a result of the 2010 Census. In several cases, federal funds are allocated to urbanized areas and not individual counties. The MPO and HART serve as Designated Recipients for Section 5307 funds. The current formula allocates 45% of the funds to HART and 55% split between PSTA (89%) and Pasco County Public Transit (11%). Based on the 2010 Census, the agencies will be looking at a new funding agreement and, in addition, TBARTA will receive a share of the funds. MPO staff will be meeting in coordination with PSTA, HART, Pasco County Transit, and TBARTA and will bring back the results of those meetings at a later date. It was clarified that the funding is allocated based on population.

**B. Upcoming Changes to the FTA 5316 (JARC), 53417 (New Freedom), and 5310 (Elderly & Disabled) Programs**

Ms. Ward indicated there have been changes to the Job Access Reverse Commute (JARC) and New Freedom programs as a result of MAP-21. JARC will be eliminated and, instead, flow through the transit agencies as part of Section 5307. The New Freedom Program is to be combined with Section 5310 that is administered by FDOT. They are still looking at guidance regarding the designation of recipients for the funding as to who would administer the funds, whether it's HART, the MPO, or FDOT. FTA has requested they begin discussions as to who would be the designated recipient. They are looking at FDOT since they currently administer the Section 5310 funds.

**C. Legislative Matters**

Ms. Ward noted there is nothing new on legislative matters.

**D. PSTA Activities Report**

Brad Miller, PSTA, indicated the federal formulas are complex and will have impacts on the funding amounts to PSTA and HART since the MAP-21 bill did not increase the funding amounts. He will be bringing this information to the PSTA Board. The MPO members will

receive an invitation to a ribbon cutting event on December 4, at 11:00 a.m., at the Shops at Booth Ranch on East Lake Road for the North County Flex Route service that will begin the following Monday. Last Thursday, PSTA staff met with FTA in Atlanta to update them on current PSTA activities, such as the Alternatives Analysis, the PSTA bus study, PSTA messaging and branding, partnership with the MPO regarding the Long Range Transportation Plan, and activities associated with the PSTA/HART consolidation study. The Advisory Committee for Pinellas Transportation will meet on Monday, December 10, at 3:00 p.m. at the PSTA office. Also on December 10, at 9:30 a.m., the PSTA Board will meet with the HART Board regarding the Consolidation Study. The subcommittee comprised of the two Boards met this past Monday to review the final draft report from the consultant that looked at efficiencies and how a consolidation might work. This was a preliminary analysis and more analysis still needs to be done; however, the consultant identified 22 overhead managerial positions that were duplicative of the two agencies. If there were just one of each position, it would save approximately \$2.4 million. There was discussion at the subcommittee meeting whether that savings was realistic. The consultant indicated that further analysis was needed. There are a number of one-time costs that need further study but there are ways to work together for efficiencies. Senator Latvala has been invited to the December 10 meeting of the two Boards. There will be a meeting of the two Boards in early January to vote on the recommendations before they are submitted to the Legislature. October had record ridership for any one given month. In response, Mr. Miller indicated the consolidation study is available on the PSTA website, as well as the “Tampa Bay Times” website.

## **IX. INFORMATIONAL ITEMS**

### **A. Update on Regional Transportation Priority List**

Ms. Ward indicated a chart was included in the folders as to the boards that have taken action to date on the Regional Transportation Priority List. Some of the agencies will not have a chance to review the list prior to the December 14 Joint Chairs Coordinating Committee meeting; therefore, there will be an update at that meeting but no action will be taken at that time.

### **B. MPOAC Meeting of October 25, 2012**

Ms. Ward noted the MPOAC held its meeting on October 25 and the agenda was included in the meeting packet. A governance workshop was held the day before the MPOAC meeting where there is an ongoing discussion on regional agencies to see whether there are opportunities to better align boundaries. At the workshop, they discussed options for MPO structures in Florida. FDOT indicated that the MPOs should expect a letter by the end of this year regarding MPO redesignations. The MPO should still be on schedule for the spring of 2013 for the Reapportionment Plan going forward.

### **C. Joint Chairs Coordinating Committee Meeting of December 14, 2012**

Ms. Ward indicated this item was to announce the Chairs Coordinating Committee meeting for December 14. The TBARTA meeting will also be held that same day so the schedules and location of the two meetings will be coordinated to make it convenient for those members who serve on both groups. .

### **D. Correspondence**

### **E. Other**

## **X. ADJOURNMENT**

There being no further business, the meeting was adjourned at 2:27 p.m.

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Dave Eggers, Chairman

**AGENDA**  
**PINELLAS COUNTY**  
**METROPOLITAN PLANNING ORGANIZATION**  
**WEDNESDAY, NOVEMBER 13, 2013**  
**1:00 P.M.**  
**BOARD ASSEMBLY ROOM – 5<sup>th</sup> FLOOR**  
**CLEARWATER COURTHOUSE**  
**315 COURT STREET, CLEARWATER, FLORIDA**

**I. CALL TO ORDER**

**II. INVOCATION AND PLEDGE**

**III. CITIZENS TO BE HEARD**

Citizen comments to the MPO are invited on items or concerns not already scheduled for public hearing on today's agenda. Please limit comments to three minutes.

**IV. CONSENT AGENDA**

- A. Approval of Minutes – Meeting of October 9, 2013
- B. Approval of Funds for MPOAC Legislative Advocacy Activities for FY 2013/14
- C. Approval of Invoice – Tindale-Oliver and Associates

**V. PUBLIC HEARING ITEM**

- A. Proposed Amendments to the FY 2012/13-2016/17 Transportation Improvement Program
- B. Fall Update of the Fiscal Years 2013/14 Through 2017/18 Transportation Improvement Program
- C. Proposed Truck Route Plan Amendment – Reroute Designated Truck Route in Tarpon Springs

**VI. PRESENTATION AND/OR ACTION ITEMS**

- A. ACPT Meetings of October 14 and November 4, 2013
- B. PSTA Items
  - 1. Greenlight Pinellas
  - 2. PSTA Activities Report
- C. FDOT Items
  - 1. Gandy Boulevard Between I-275 and U.S. 19
  - 2. Update on Gandy Boulevard Project Between 4<sup>th</sup> Street and Dr. MLK, Jr. Street
  - 3. Howard Frankland Bridge
  - 4. Multi-Use Paths Policy for State Roads
- D. Beckett Bridge Project Presentation
- E. Functional Classification Map
- F. Committee Recommendations
  - 1. Bicycle Advisory Committee (BAC)
    - Endorse Greenlight Program With Inclusion of Bicycle and Pedestrian Projects and Fund Loop Completion
  - 2. Pedestrian Transportation Advisory Committee (PTAC)
    - Encourage Acknowledgement of Pedestrian Safety Awareness Week of October 28 Through November 4, 2013 (PTAC Motion #13-3)
  - 3. Technical Coordinating Committee (TCC)
    - Multi-Named Roadway Signage
- G. Transportation Disadvantaged Service Plan (TDSP) Amendment
- H. Establishment of Nominating Committee for 2014 Election of Officers
- I. Committee Appointments

**VII. REPORTS/UPDATE**

- A. Status of MPO Reapportionment Plan

**VIII. INFORMATIONAL ITEMS**

- A. Request for PSTA to Become Designated Recipient Update
- B. 2040 Long Range Transportation Plan Stakeholder and Public Outreach Report
- C. MPOAC Meeting of October 31, 2013
- D. Transportation Management Area (TMA) Meeting of November 15, 2013
- E. Joint Chairs Coordinating Committee Meeting of December 12, 2013
- F. Correspondence
- G. Other

**IX. ADJOURNMENT**

*Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability, or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation services (free of charge) should contact the Office of Human Rights, 400 South Fort Harrison Avenue, Suite 300, Clearwater, Florida 33756; [(727) 464-4062 (V/TDD)] at least seven days prior to the meeting.*

CONSENT AGENDA

**A. Approval of Minutes – Meeting Of October 9, 2013**

**ATTACHMENT:** [Minutes of October 9, 2013](#)

**B. Approval of Funds for MPOAC Legislative Advocacy Activities for FY 2013/14**

Annually, the MPO contributes \$500 to the Metropolitan Planning Organization Advisory Council (MPOAC) to support legislative advocacy activities. The MPOAC prepares legislative policy positions and initiates legislative actions that are advanced during the session. The MPOAC also monitors federal and state legislation as it pertains to MPO interests in Florida. The MPOAC's advocacy on the part of the MPOs reduces staff travel to Tallahassee and Washington and provides an ongoing presence during legislative committee meetings and throughout the legislative session. **MPO staff recommends payment of the \$500 for legislative advocacy activities.**

**ATTACHMENT:** [Memo Dated September 17, 2013](#)

**C. Approval of Invoice – Tindale-Oliver and Associates**

Attached is an invoice for professional services provided by Tindale-Oliver and Associates for work associated with the crash data in the amount of \$1,549.58. **MPO staff recommends payment in the amount of \$1,549.58.**

**ATTACHMENT:** [Tindale-Oliver Invoice #4055 for Period as of October 31, 2013](#)

Pinellas MPO: 11/13/13

PUBLIC HEARING ITEM

**A. Proposed Amendments to the FY 2012/13-2016/17 Transportation Improvement Program**

The Florida Department of Transportation (FDOT) is proposing two amendments to the Pinellas County FY 2013/14 – FY 2017/18 Transportation Improvement Program (TIP). These are described below.

**1. FPN# 433787-1 – 54<sup>th</sup> Avenue South at 31<sup>st</sup> Street South**

This proposed intersection improvement project amendment adds \$270,700 in FY 2013/14 for the construction of an exclusive right-turn lane on the eastbound side of 54<sup>th</sup> Avenue South. The funding source for this project is FDOT's Highway Safety Program (HSP) fund.

**2. FPN# 433786-1 – 38<sup>th</sup>/40<sup>th</sup> Avenue North from 4<sup>th</sup> Street North to 1<sup>st</sup> Street North**

This proposed road project amendment adds \$439,000 in FY 2013/14 for the construction phase of new medians, signals and pedestrian improvements on 38<sup>th</sup>/40<sup>th</sup> Avenue North from 4<sup>th</sup> Street North to 1<sup>st</sup> Street North. The funding source for this project is FDOT's Highway Safety Program (HSP) fund.

The Technical Coordinating Committee and Citizens Advisory Committee reviewed the proposed TIP amendments at their October meetings. Both committees recommended that the MPO approve the amendments

**ATTACHMENTS:** [TIP Amendment Forms](#)  
[TIP Amendment Maps](#)

**ACTION:** MPO to conduct public hearing and approve TIP amendment by roll call vote

Pinellas MPO: 11/13/13

**PUBLIC HEARING ITEM**

**B. Fall Update of the Fiscal Years 2013/14 Through 2017/18 Transportation Improvement Program**

In the fall of each year, the Metropolitan Planning Organization (MPO) updates the Transportation Improvement Program (TIP) to incorporate changes in the County and Municipal Work Programs. The TIP contains project descriptions, schedules, and corresponding funding allocations for the 25 local governments of Pinellas County, as well as the Pinellas Suncoast Transit Authority (PSTA), the local airports, and the Port of St. Petersburg. The projects include new construction, reconstruction, capital purchases, and maintenance work associated with roads, sidewalks, trails, transit services, airports, the Port of St. Petersburg, and the Transportation Disadvantaged Program. The TIP also contains the MPO's priority lists of projects for the Transportation Alternatives Program, the Surface Transportation Program, and a list of Congestion Management Process projects as required by law in order to receive state and federal funding.

The new work program tables are attached, along with corresponding project maps. Also attached are summary tables intended to provide an abbreviated report of Pinellas County transportation improvement projects. The summary tables include information on the status of the projects and any changes that have occurred from the previous year Work Program. Shaded projects on the table indicate that changes occurred compared to the previous year Work Program. Projects not shaded are unchanged from the previous year. The Technical Coordinating Committee (TCC) and Citizens Advisory Committee (CAC) reviewed the updated TIP and recommended approval at their respective meetings in October.

- ATTACHMENTS:** [Map of Pinellas County Road, Intersection, and Bridge Improvements](#)  
[Map of Pinellas County Intelligent Transportation System/Advanced Traffic Management Systems and Trail Projects](#)  
  
[Summary Table of the Pinellas County CIP – Major Road Projects](#)  
[Summary Table of the Pinellas County CIP – ITS and Trail Projects](#)  
[Pinellas County Capital Improvement Program for Transportation Projects for FY 2013/14 – 2022/23](#)  
  
[Work Program Table of the St. Petersburg-Clearwater International Airport](#)  
[Work Program Table of the Port of St. Petersburg](#)  
[Work Program Table of the Clearwater Airpark](#)  
[Work Program Table of the Albert Whitted Airport](#)  
[Map of Municipal Work Program Projects](#)  
[Municipal Work Program Tables](#)

**ACTION:** MPO to conduct public hearing and approve Fall Update to the TIP by roll call vote

Pinellas MPO: 11/13/13



PUBLIC HEARING ITEM

**C. Proposed Truck Route Plan Amendment – Reroute Designated Truck Route in Tarpon Springs**

The Pinellas County MPO maintains a Truck Route Plan to identify roadways where heavy trucks must travel and where time-of-day restrictions should apply. The MPO works with each municipality and the County to identify roadways appropriate for through-truck movements and to develop ordinances regulating truck traffic in an effort to maintain consistency across jurisdictional boundaries. In Pinellas County, trucks are required to use designated truck routes up to the point closest to their destination.

The City of Tarpon Springs is proposing an amendment to the Truck Route Plan that would remove the unrestricted truck route designation from the western section of Tarpon Avenue between Alternate U.S. Highway 19 and Ring Avenue. The proposal would redirect the truck traffic to the south by adding Ring Avenue from Tarpon Avenue to Lemon Street and Lemon Street from Ring Avenue to Alternate U.S. Highway 19.

In 2011, Tarpon Avenue was transferred to the City from the jurisdiction of the State of Florida in order for the City to implement a planned roadway drainage mitigation project, complete downtown development initiatives, and facilitate other economic development opportunities. The intersection of Tarpon Avenue and Alternate U.S. Highway 19 has a very tight turning radii, with a building located directly on the northeast corner of the intersection, with very little setback. This poses a significant constraint to the truck traffic that would be relieved with the proposed amendment.

At their regularly-scheduled meeting of October 23, 2013, the MPO's Technical Coordinating Committee (TCC) recommended approval of the proposed amendment to the Truck Route Plan. At the October 24, 2013 meeting, the MPO's Citizens Advisory Committee also recommended approval of the proposed amendment. Once approved, the Truck Route Plan Map in the adopted Long Range Transportation Plan will be updated to reflect this amendment, as well as the amendments approved at the October MPO meeting to Keene Road and Sunset Point Road.

**ATTACHMENT:** [Map of Tarpon Avenue and Surrounding Area](#)

**ACTION:** Following a public hearing, MPO to approve the proposed amendment to the Countywide Truck Route Plan

Pinellas MPO: 11/13/13

**PRESENTATION AND/OR ACTION ITEMS**

**A. ACPT Meetings of October 14, 2013 and November 4, 2013**

The Advisory Committee for the Pinellas Transportation (ACPT) met on Monday, October 14, and Monday, November 4. Staff will provide a brief summary of the meetings.

**ATTACHMENTS:**     [October 14](#) and [November 4 ACPT Agendas](#)

**ACTION:**         As deemed appropriate based on discussion

**B. PSTA Items**

**1. Greenlight Pinellas**

The GreenLight Pinellas Plan, which is comprised of several elements including Bus, Rail, Station Area Design Concepts, Access Facilities (e.g. sidewalks, trails, bicycle racks, park and ride lots, etc), a Financial Plan, a Phasing Strategy and Delivery Plans, was developed through a coordinated effort involving the Pinellas Suncoast Transit Authority, the Metropolitan Planning Organization, the Pinellas Planning Council, the Tampa Bay area Regional Transportation Authority, the Florida Department of Transportation, local governments, and various community stakeholders.

The Advisory Committee for Pinellas Transportation (ACPT) endorsed the plan at its November 4 meeting and recommended that it be transmitted to the partner agencies for action and that it be wholly incorporated into the plans of agencies represented on the ACPT, including the MPO's Long Range Transportation Plan (LRTP).

At last month's meeting, staff provided an update on the LRTP noting the adoption is scheduled for December 2014. Staff indicated the board could not approve the GreenLight Plan for inclusion in its LRTP at this time as the MPO's plan development and adoption process must follow several prescribed steps. As an example, updated socioeconomic (SE) data (e.g. population and employment) must be incorporated. The LRTP presently under development is for the year 2040. The MPO approved the new SE data set, which was developed in partnership with the local governments in May of this year. The 2040 SE data will be utilized in the travel demand forecast model to determine future needs. Alternative transportation improvements (highway and transit) will be modeled to determine which best respond to the LRTP goals and objectives. The transit network identified in the GreenLight Plan will be modeled utilizing the new SE data set. A no new revenue alternative also will be modeled and included in the LRTP if new revenue for transit is not approved in 2014, as the MPO must demonstrate that its plan is cost feasible.

In light of the remaining steps in the LRTP development process, staff recommends that the MPO accept for modeling purposes the bus and rail elements of the Green Light Plan. Pending a successful referendum, the GreenLight Plan will be wholly incorporated into the MPO's 2040 Cost Feasible LRTP.

This item will include a presentation by PSTA staff.

**ATTACHMENT:**     [ACPT Resolution](#)

**ACTION:**         MPO to accept bus and rail elements for modeling

**2. PSTA Activities Report**

In accordance with the usual practice, this item will be a report as to the recent PSTA activities as they relate to the MPO. This is also an opportunity for any MPO member to take up business as it relates to the PSTA and the MPO Board.

**ATTACHMENT:**     October 23, 2013 PSTA Board Summary (if available)

**ACTION:**         As deemed appropriate based on discussion

**PRESENTATION AND/OR ACTION ITEMS**

**C. FDOT Items**

**1. Gandy Boulevard Between I-275 and U.S. 19**

The Florida Department of Transportation (FDOT) has scheduled design for the segment of Gandy Boulevard from east of I-275 to east of U.S. 19 in the current year's work program, with right-of-way acquisition in 2015/16-2016/17. Responding to concerns from affected businesses and the City of Pinellas Park, the MPO at its April meeting asked FDOT to consider alternatives to the proposed partially-controlled access design for that segment of Gandy. Owners of adjacent businesses expressed concerns about a proposed interchange at Gandy and Grand Avenue and its impact on access to those businesses. The City expressed concerns about safety issues associated with higher speed traffic if the signalized intersection is removed. Over the last several months, FDOT has worked with the property owners and the local officials to address their concerns and has developed an alternative design concept.

This item will include a presentation of the proposed design by FDOT staff.

**ATTACHMENT:** None

**ACTION:** As deemed appropriate based on discussion

**2. Update on Gandy Boulevard Project Between 4<sup>th</sup> Street and Dr. MLK, Jr. Street**

As the MPO is aware, the construction project on Gandy Boulevard between 4<sup>th</sup> Street and MLK Jr. Street in St. Petersburg has been delayed due to issues with the Florida Gas Transmission (FGT). At the last meeting, FDOT reported that negotiations with FGT were underway and that resolution of the issues appeared imminent.

This item will include an update from FDOT.

**ATTACHMENT:** None

**ACTION:** None required

Pinellas MPO: 11/13/13

**PRESENTATION AND/OR ACTION ITEMS**

**C. FDOT Items (Continued)**

**3. Howard Frankland Bridge**

The Florida Department of Transportation is conducting a Project Development and Environment (PD&E) Study for the replacement of the northbound span of the Howard Frankland Bridge. This span of the bridge was opened to traffic in 1959 and is approaching the end of its serviceable life. The PD&E will identify the best replacement options and document how the replacement will affect the surrounding environment. In addition to the bridge replacement study, a transit corridor evaluation is underway to look at options for a transit connection that will link Pinellas and Hillsborough within the bridge corridor. The study is evaluating options for a stand-alone transit structure, as well as options for including transit on the roadway bridge. The new bridge will have a design life of 75 years.

At their October meetings, both the Citizens Advisory and the Technical Coordinating Committees took action recommending the MPO recommend to FDOT that the preferred alternative include provisions in its design to accommodate rail in the future. During the TCC meeting, it was noted that, in order for FDOT to design for rail, it needed to know what the rail technology on the bridge would be. Therefore, the TCC also recommends that the Transportation Management Area Working Group that includes representatives of the Hillsborough, Pasco and Pinellas MPO boards include this topic on its next agenda for discussion of the rail technology issue. The PSTA Legislative Committee also recommends that the bridge be designed to accommodate rail. Staff concurs with the advisory committee recommendations.

Subsequent to the advisory committee meetings, FDOT Secretary Ananth Prasad announced that the bridge would be designed to support rail in the future.

This item will include a presentation by FDOT staff or its consultant.

**ATTACHMENTS:** [TCC Memo Dated October 23, 2013](#)  
[Tampa Bay Times Article Dated October 31, 2013](#)  
[Tampa Bay Business Journal Article Dated October 31, 2013](#)

**ACTION:** MPO to approve committee and staff recommendations

Pinellas MPO: 11/13/13

**PRESENTATION AND/OR ACTION ITEMS**

**C. FDOT Items (Continued)**

**4. Multi-Use Paths Policy for State Roads**

At both its July and September meetings, the MPO took action to concur with recommendations from its advisory committees that the Florida Department of Transportation (FDOT) construct a multi-use trail on the north side of Roosevelt Boulevard from 28<sup>th</sup> Street to 4<sup>th</sup> Street during an upcoming resurfacing project. FDOT policies require the Department to provide accommodations for bicyclists and pedestrians when resurfacing and/or constructing new roads. The FDOT plans for the Roosevelt Boulevard resurfacing project include provisions for sidewalks. The City of St. Petersburg requested that the sidewalks on the south side be eliminated and that those funds be applied to the cost of constructing a multi-use trail on the north side. The MPO's Bicycle Advisory Committee and Technical Coordinating Committee supported the City's request. The TCC recommended the FDOT not only construct the trail but should also maintain it.

In response to the City's request and the MPO's recommendation, FDOT advised the City that it would have to cover the additional cost of constructing a trail instead of a sidewalk and, further, that the City would be required to execute a maintenance agreement with the Department before the state would agree to build the trail. The City is concerned not only about the application of the policy to Roosevelt Boulevard, it is also concerned about the Pinellas Bayway bridges. The City requested this matter be placed on the MPO agenda for discussion of FDOT's policies.

**ATTACHMENTS:** [MPO Letter Dated September 13, 2013](#)  
[FDOT Letter Dated September 30, 2013](#)  
[Table From FDOT Plans and Preparation Manual for Bicycle Facilities](#)

**ACTION:** As deemed appropriate based on discussion

Pinellas MPO: 11/13/13

**PRESENTATION AND/OR ACTION ITEMS**

**D. Beckett Bridge Project Presentation**

Pinellas County, in conjunction with the Florida Department of Transportation (FDOT), is conducting a Project Development and Environment (PD&E) Study to evaluate the removal, rehabilitation, or replacement of the existing Beckett Bridge over Whitcomb Bayou in Tarpon Springs. URS (formerly EC Driver & Associates, Inc.) is under contract with Pinellas County to conduct the study.

The following alternatives were evaluated:

- No Build
- No Build with Permanent Removal of the Existing Bridge
- Rehabilitation of the Existing Bridge
- Replacement with a New Movable Bridge
- Replacement with a New Fixed Bridge (with 28 feet of vertical clearance)

The study began in January 2011. After careful consideration of many engineering, environmental and social factors, including the need for safe and efficient transportation, input from the community and local governments, project costs and consideration of all public comments received, replacement of the existing bridge with a new two lane movable bridge was selected as the “Recommended Alternative” by the County staff. The proposed typical section is 47.2 feet wide and includes two 11 foot wide travel lanes, 6-foot sidewalks on both sides of the bridge, and 5.5-foot outside shoulders that could be used as “undesignated” bicycle lanes.

The Board of County Commissioners (BCC) approved the Recommended Alternative as proposed by staff on October 22, 2103. Accordingly, the Recommended Alternative will be presented at a (NEPA) Public Hearing in February 2014. Information about all alternatives considered during the study will also be presented at the Public Hearing. Invitations to the Public Hearing will be mailed to all those invited to the January 2013 Alternatives Meeting and all those who have expressed interest and provided contact information since the Study began.

In April 2014, the results of the Public Hearing will be presented to the BCC. The BCC will decide whether to ratify and confirm their approval of the Recommended/Preferred Alternative for submittal to the Federal Highway Administration (FHWA).

Public input is an important aspect of the study. Presentations to stakeholder groups were made early in the study to solicit input for development of alternatives. Because the bridge has been determined to be eligible for listing in the National Register of Historic Places, a Cultural Resources Committee was also established to conduct “good faith consultation” with affected parties to address Section 106 requirements.

The project’s alternatives were presented to the MPO’s Pedestrian Transportation Advisory Committee (PTAC), Bicycle Advisory Committee (BAC), Technical Coordinating Committee (TCC), and the Citizen Advisory Committee (CAC). The TCC and the CAC endorsed the study and the recommended alternative. The PTAC and BAC recommended that any alternative recommended for consideration should include bicycle and pedestrians accommodations.

Ann Venables and Jim Phillips of URS (previously EC Driver and Associates) will give a brief presentation about the current status of the study and discuss the rationale for the Recommended Alternative.

**ATTACHMENT:**    [Location and Typical Section Graphic](#)

**ACTION:**    MPO to endorse the Recommended Alternative

Pinellas MPO: 11/13/13

**PRESENTATION AND/OR ACTION ITEMS**

**E. Functional Classification Map – Authorize Chairman to Sign**

Federal functional classification is the process when streets and highways are grouped into classes, or systems, according to the character of service they provide. This process occurs at least once every ten years, following the Decennial Census. Federal functional classification is utilized at the state and federal level for planning, budgeting, programming, and for fiscal management. It is also used to determine eligibility for funding under the Federal-aid program, to determine roads with billboard restrictions, and to assist with describing roadway system performance. At the local level, Pinellas County and the municipalities maintain local functional classification designations in their comprehensive plans for regulatory purposes, determining guidelines for roadway design, including speed, capacity and relationship to existing and future land use development.

MPO staff has been working with the Florida Department of Transportation (FDOT) and the MPO's Technical Coordinating Committee (TCC) to identify proposed changes to the Federal Functional Classification. While the Proposed Federal Functional Classifications are still under review by FDOT and FHWA, MPO staff and the TCC have completed their review and any recommended changes have been incorporated into the latest draft. Per federal regulations, the Chair of the MPO Board must sign the final map that includes both the Urbanized Area Boundary and the Federal Functional Classification. At the MPO meeting in December 2012, the MPO received an overview on some minor changes being proposed to the Urbanized Area Boundary and had no objections on those changes. The MPO Board is asked to authorize the MPO Chair to sign the final map, upon completion of the review by FDOT and FHWA. MPO staff will give a brief overview of the changes being proposed.

**ATTACHMENTS:** [Draft 2010 Urbanized Area Boundaries and Federal Functional Classification Map](#)  
[Table of Proposed Changes to the Federal Functional Classification](#)

**ACTION:** MPO to authorize MPO Chairman to sign final Federal 2010 Urbanized Area Boundaries and Federal Functional Classification Map

Pinellas MPO: 11/13/13

**PRESENTATION AND/OR ACTION ITEMS**

**F. Committee Recommendations**

**1. Bicycle Advisory Committee**

- **Endorse Greelight Program With Inclusion of Bicycle and Pedestrian Projects and Fund loop Completion (BAC Motion #13-9)**

At their meeting on September 23, the Bicycle Advisory Committee (BAC) approved Motion #13-9 that included three recommendations. The recommendations related to the planning of bicycle and pedestrian facilities, particularly in relation to the transit system proposed in Greenlight Pinellas. The MPO considered the motion at their October 9 meeting but deferred action until the November meeting after the Proposed Facilities Element of the Bicycle Pedestrian Master Plan had been developed and reviewed by a subcommittee comprised of various advisory committee members. The recommendations included in BAC Motion #13-9 are shown below.

- 1) The BAC supports refining the bicycle/pedestrian provisions in the current update of the MPO LRTP and establishing a subcommittee to work with the MPO staff on that initiative.
- 2) The BAC supports the inclusion of bicycle/pedestrian provisions in the Greenlight Pinellas Program as those facilities are an integral part of an alternative transportation program.
- 3) The BAC supports the completion of the Pinellas Trail Loop and recommends that construction of the facility within the remaining gaps be included for funding in the Greenlight Pinellas Program. The BAC recognizes the project is a key element in the development of a multi-modal transportation system that allows people to travel around Pinellas County without the use of an automobile.

Attached is a staff summary responding to the BAC motion, as well an explanation of current planning efforts addressing bicycle and pedestrian access to the planned transit system called for in Greenlight Pinellas.

**ATTACHMENT:** [MPO Staff Summary on BAC Motion #13-9 and Bicycle/Pedestrian Access to Greenlight Pinellas Transit Plan](#)

**ACTION:** MPO to approve recommendations #1 and #2 and receive and file #3 of BAC Motion #13-9

Pinellas MPO: 11/13/13



**PRESENTATION AND/OR ACTION ITEMS**

**F. Committee Recommendations (Continued)**

**2. Pedestrian Transportation Advisory Committee**

- **Encourage Acknowledgement of Pedestrian Safety Awareness Week of October 28 through November 4, 2013 (PTAC Motion #13-3)**

At their October 21 meeting, the Pedestrian Transportation Advisory Committee (PTAC) approved Motion #13-3, encouraging all jurisdictions and law enforcement agencies to recognize and promote Pedestrian Safety Awareness Week from October 28 through November 4, 2013. The purpose of the event is to remind pedestrians and motorists of safety measures and to promote safety for pedestrians at all intersection crosswalks throughout Pinellas County, recognizing that pedestrians have the legal right-of-way within crosswalks. As part of this event, MPO pedestrian safety brochures (see web link below) targeting students and their families were produced and distributed to every student enrolled in Pinellas County Public Schools. In addition, information about the event was provided through a press release, Pinellas County TV and social media.

**ATTACHMENT:** [PTAC Motion #13-3  
www.pinellascounty.org/MPO/bikeped/PedBrochure.pdf](http://www.pinellascounty.org/MPO/bikeped/PedBrochure.pdf)

**ACTION:** MPO to approve the recommendation or take other action based on discussion

**3. Technical Coordinating Committee**

- **Multi-Named Roadway Signage**

At their meeting on October 23, the Technical Coordinating Committee (TCC) reviewed the MPO action concerning the recommendation from the Multi-Named Roadway Subgroup. The TCC concurs with the concept that consistent signing of street names would be beneficial to residents and tourists but, respectfully, requests the MPO provide them with the opportunity to review this as a technical issue prior to recommending action to the local jurisdictions.

Attached is a memo from the TCC Chairman regarding their recommendation.

**ATTACHMENT:** [Memo Dated October 23, 2013 From TCC Chairman](#)

**ACTION:** MPO to approve the TCC recommendation

Pinellas MPO: 11/13/13

**PRESENTATION AND/OR ACTION ITEMS**

**G. Transportation Disadvantaged Service Plan (TDSP) Amendment**

The Pinellas Suncoast Transit Authority (PSTA) recently renewed its service agreements with Clearwater Yellow Cab and Care Ride to provide ambulatory and wheelchair transportation for DART, their demand response service, and Transportation Disadvantaged Program passengers in fiscal year 2013/14. The agreements took effect October 1. Included with the new agreements was a four percent rate increase for ambulatory and wheelchair trips. These rate increases require an amendment to Table 8 of the TDSP. The TDSP establishes the objectives, strategies, and quality assurance standards for the TD Program, as well as its operational elements. To implement the rate changes, Table 8 of the TDSP needs to be amended. The table is attached showing the rate changes in strike-through/underline. This amendment was approved by the Local Coordinated Board (LCB) at their September meeting.

**ATTACHMENT:** [Transportation Disadvantaged Service Plan Table 8, Showing Proposed Changes to Service Rates Based on New Provider Contracts for 2013/14](#)

**ACTION:** MPO to approve proposed amendment of Table 8 in the TDSP

**H. Establishment of Nominating Committee for 2014 Election of Officers**

As is the usual practice, it is time for the MPO to establish a Nominating Committee for a slate of officers to be acted on at the December MPO meeting. Those officers will take office as of January 1, 2014. Attached is a membership listing, as well as the past officers.

**ATTACHMENT:** [MPO Membership Listing, Including Past Officers](#)

**ACTION:** MPO to appoint members to the Nominating Committee and to announce when they are meeting

Pinellas MPO: 11/13/13

**PRESENTATION AND/OR ACTION ITEMS**

**I. Committee Appointments**

• **Technical Coordinating Committee**

PSTA is requesting John Villeneuve be appointed as the PSTA representative and Christopher Cochran be appointed as the alternate on the TCC.

• **Citizens Advisory Committee**

The Citizens Advisory Committee has vacancies in the St. Petersburg, Clearwater, Dunedin, and At Large areas. The CAC meets on the 4<sup>th</sup> Thursday at 7:00 p.m.

• **Bicycle Advisory Committees**

The Bicycle Advisory Committee has vacancies in the Clearwater and At Large areas and usually meets on the 4<sup>th</sup> Monday at 8:30 a.m.

• **Pedestrian Transportation Advisory Committees**

The Pedestrian Transportation Advisory Committee has vacancies in the Mid-County and North County areas and two in the At Large areas and usually meets on the 3<sup>rd</sup> Monday at 8:30 a.m.

**ATTACHMENTS:** [TCC Membership Listing](#)  
[Email With PSTA's Request](#)  
[CAC Membership Listing](#)  
[Letter Dated October 25, 2013 to Neil McMullen](#)  
[BAC Membership Listing](#)  
[PTAC Membership Listing](#)

**ACTION:** MPO to approve the appointments of John Villeneuve as the PSTA representative and Christopher Cochran as the alternate to the TCC

Pinellas MPO: 11/13/13

**REPORTS/UPDATE**

**A. Status of MPO Reapportionment Plan**

As of the time of the agenda packet mailout, the Plan is still under review by the Florida Department of Transportation's Central Office staff and has not been approved by the Governor.

**ATTACHMENT:** None

**ACTION:** None required

Pinellas MPO: 11/13/13

**INFORMATIONAL ITEMS**

**A. Request for PSTA to Become Designated Recipient (Update)**

The Pinellas Suncoast Transit Authority (PSTA) has requested to become a Designated Recipient for Federal Transit Administration (FTA) funds. This would allow the agency to receive Section 5307 grant funds directly from FTA. The Pinellas Suncoast Transit Authority and Pasco County Public Transportation (PCPT) are currently designated as Direct Recipients for these funds. As such, they receive their funding through an agreement with the MPO, which administers the funds as the Designated Recipient for the urbanized area.

Securing Designated Recipient status for PSTA is subject to approval by the Governor and requires coordination with the Florida Department of Transportation (FDOT), PCPT, Hillsborough Area Regional Transit (HART), and the Transportation Management Area MPOs. Staff is in the process of working with these partner agencies to assist PSTA with their request.

**ATTACHMENT:** None

**B. 2040 Long Range Transportation Plan Stakeholder and Public Outreach Report**

Attached is a list of current and upcoming stakeholder and public outreach activities for the 2040 Long Range Transportation Plan.

**ATTACHMENT:** [Stakeholder and Public Outreach Event Listing](#)

**C. MPOAC Meeting of October 31, 2013**

MPO staff and/or Councilman Danner will provide a brief summary of the October 31 MPOAC meeting.

**ATTACHMENTS:** [MPOAC Agendas](#)

**D. Transportation Management Area (TMA) Working Group Meeting of November 15, 2013**

The Transportation Management Area (TMA) Working Group will meet on Friday, November 15, 2013.

**ATTACHMENT:** None

**E. Joint Chairs Coordinating Committee Meeting of December 13, 2013**

The Joint Chairs Coordinating Committee will meet on Friday, December 13, 2013.

**ATTACHMENT:** None

**F. Correspondence**

**ATTACHMENT:** [Letter Dated October 31, 2013 From MPO Chairman Karen Seel to US DOT Regarding Heavy Trucks on Transportation Infrastructure](#)

**G. Other**

At this time, the MPO may take up other matters that might be identified by the members.

Pinellas MPO: 11/13/13

# Beckett Bridge PD& E Study

Presentation to:

Pinellas MPO Board



**URS**

**JANUS  
RESEARCH**

November 13, 2013

**Study Began January 2012**

**Alternatives Presented to Commission October 2013**

**Alternatives Presented to Public January 2013**

**Alternatives Considered**

- **No-Build**
- **No-Build with Permanent Removal  
of Existing Bridge**
- **Rehabilitation (No Widening)**
- **Replacement**
  - **Fixed Bridge – 28 feet Vertical Clearance**
  - **Movable Bridge - 7.8 feet Vertical Clearance**

## National Environmental Policy Act of 1969

### Federal Highway Administration (FHWA)

- Assures NEPA Compliance
- Final Authority – Approval of “Recommended Alternative”
- Approval required if federal funds are used
- Approval required to qualify for federal funds





## FHWA Policy:

*Alternatives are to be evaluated and decisions are to be made in the best overall public interest based on balanced consideration of:*

- Need for safe and efficient transportation
- Social, economic and environmental impacts
- National, state and local environmental protection laws

PD&E Process – Assures Compliance with NEPA

## Public Input – Important Component

- Decisions not made by a public vote
- Many other factors also considered

## Input from Federal and State Agencies

- Policies, laws and procedures that govern how FHWA considers agency input
- USFWS, NMFS, USCG
- State Historic Preservation Officer (SHPO)

Concurring agency on decisions regarding historic resources

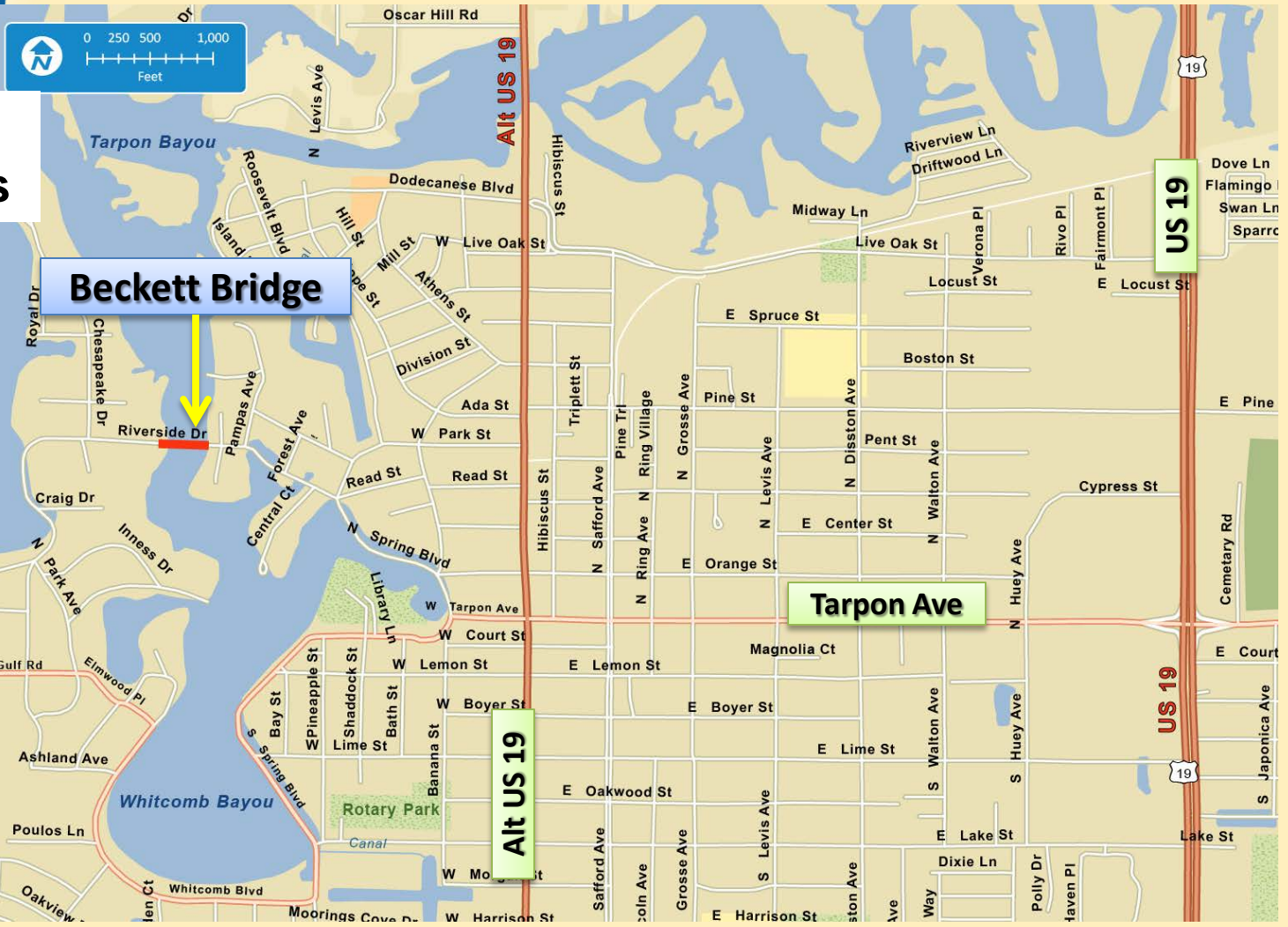
- **Property Owners/ Residents**
- **Boaters**
- **Commuters**
- **County and City Emergency Services**
- **School Board**
- **Local Governments**
- **Bicyclists**
- **Special Interest Groups**

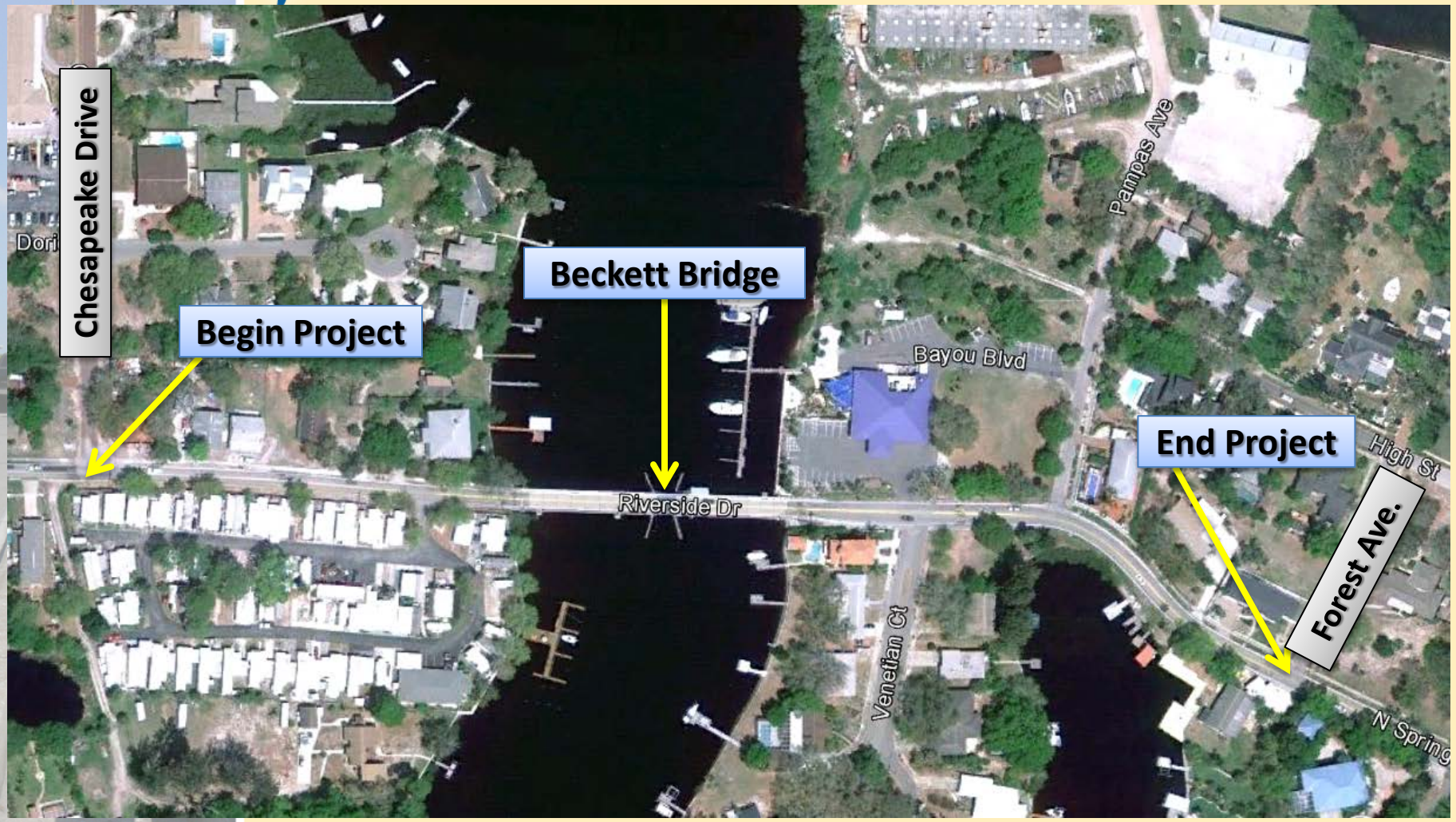




# Project Location

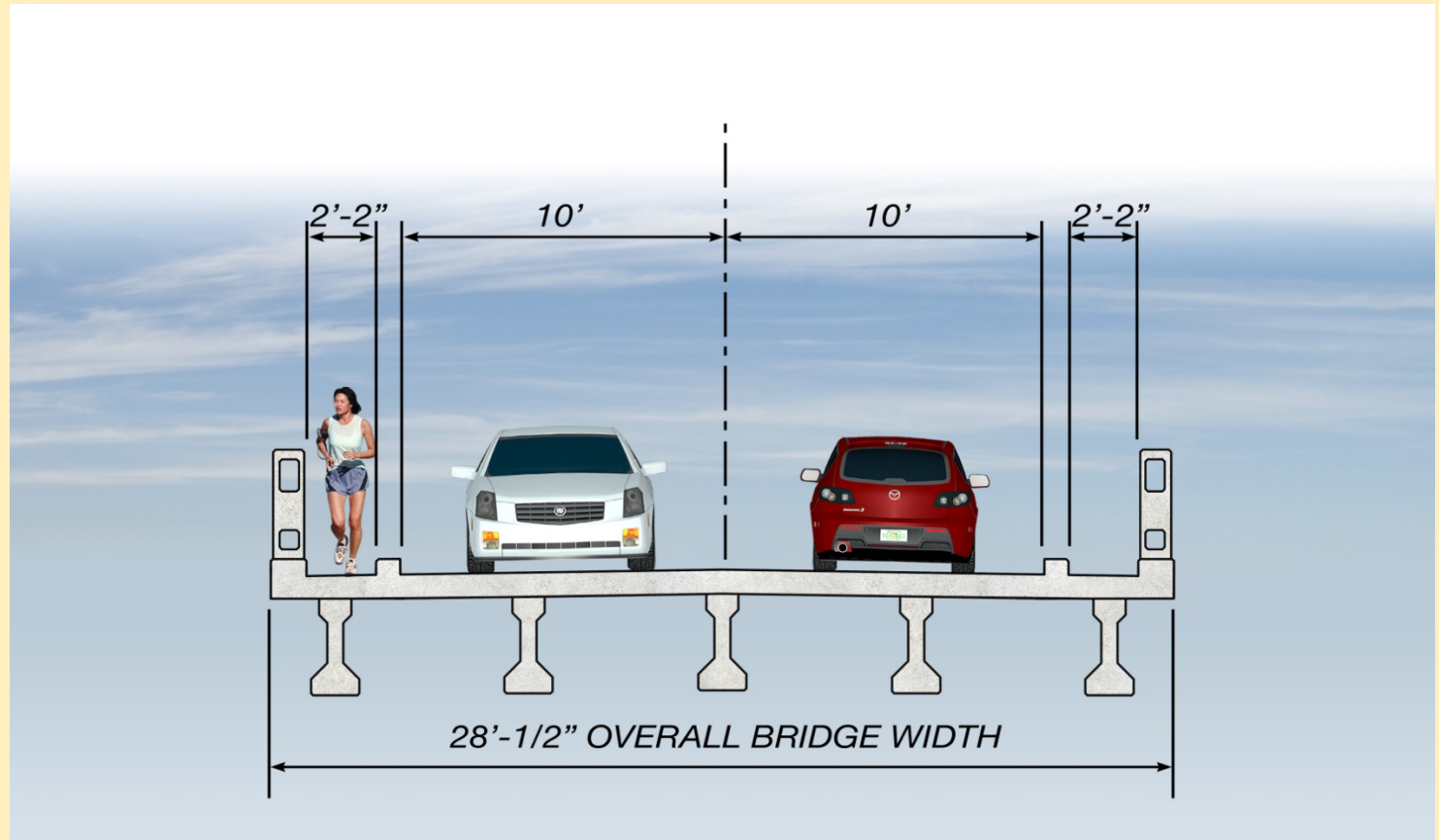
2012 AADT  
7,700 vehicles





- **Constructed 1924**
  - Original timber construction
- **Substantially Rehabilitated 1956**
  - Original steel bascule span and machinery retained
- **Major Repairs in 1979, 1998 and 2011**
  - Machinery replaced “in-kind”
- **Sufficiency Rating 44.7**





**No Shoulders**

**Narrow Sidewalks**

- **Determined Eligible for listing in the National Register of Historic Places**
  - One of a few remaining pre-1965, Single-Leaf Rolling-Lift Bascule Highway Bridges in Florida
  - Eligible in Areas of Community Planning and Development, Transportation and Engineering
  - Contributed to Westward Expansion of the City of Tarpon Springs



- Vertical Clearance – 6 ft
- Horizontal Clearance – 25 ft
- Opens with 2-hr Notice

**Total # Bridge Openings**

**2009 - 10**

**2010 - 20**

**2011 - 18**

**2012 - 14**



## Condition Assessment

- Health & Sufficiency
  - Deterioration
  - Wear
  - Corrosion
  - Damage
- Shortcomings of original design and/or construction
- Unforeseen conditions



## Structural Condition

- Cracked and spalled concrete throughout
- Corrosion of reinforcing steel throughout
- Corroded structural steel
- Distorted steel flanges at tread plates
- Deteriorated timber piles & wales of fender system



- **Mechanical & Electrical Issues**
  - Existing systems are old, worn and no longer reliable



- **Functionally Obsolete**

- **Narrow Lanes**

- No Shoulders
- No bicycle lanes

- **Narrow Sidewalks**

- Do Not Meet ADA Requirements

- **Structural Deficiencies**

- **Load Posted**

- **Not designed for current heavier vehicles**



- **Unforeseen Conditions**
  - **Foundations susceptible to settlement**
  - **Scour susceptible**



**Existing Crutch Bents**

## Stakeholder/Local Government Presentations October – November 2012

- Chamber of Commerce
- Rotary Club
- Tarpon Springs Yacht Club
- MPO Board
- MPO Advisory Committees
- City of Tarpon Springs
- Pinellas County BCC
- Cultural Resource Committee (CRC)





- Alternatives Public Meeting - January 2013  
77 Written Comments Received

## *Preferences for Alternatives*

No-Build	7
No-Build, Remove Bridge	2
Rehabilitation	11
Rehabilitation or New Movable	12
New Movable Bridge	32
New Fixed Bridge (28 ft Vertical Clearance)	4



- Alternatives Public Meeting - January 2013

## Community Concerns

- Need for safer pedestrian facilities
- Bridge should provide adequate vertical clearance
- Bridge should not adversely affect historic character of the community
- Duration of detour should be minimized



## Section 106 Process

- Avoid, minimize or mitigate adverse impacts
- Conduct “Good faith consultation” with affected parties
  - Consider affected party concerns
  - Solicit Input on possible mitigation if required
- FHWA is the lead final agency
- SHPO is the concurring agency

## Cultural Resource Committee – CRC

### Affected Parties included:

- **Federal/State agencies**
  - SHPO, USCG, FDOT, FHWA,
- **Stakeholders with special interest in historic preservation**
- **Local government representatives**
- **Local community representatives**

October 2012, March 2013 CRC Meetings



## CRC Meeting – March 2013

SHPO requested evaluation of two new Rehabilitation Alternatives with Improved Sidewalks

- **Rehabilitation with Widening**
  - Provide sidewalks on both sides
- **Reconfiguration of Existing Bridge (No Widening)**
  - Provide sidewalk on one side

Rehabilitation – Sidewalk Improvements

Conclusion of Extensive Engineering Evaluation

- Replacement of Bascule (Movable) Span
- Replacement of Bascule Pier

**No elements of original bridge will remain**





## Rehabilitation – No Widening

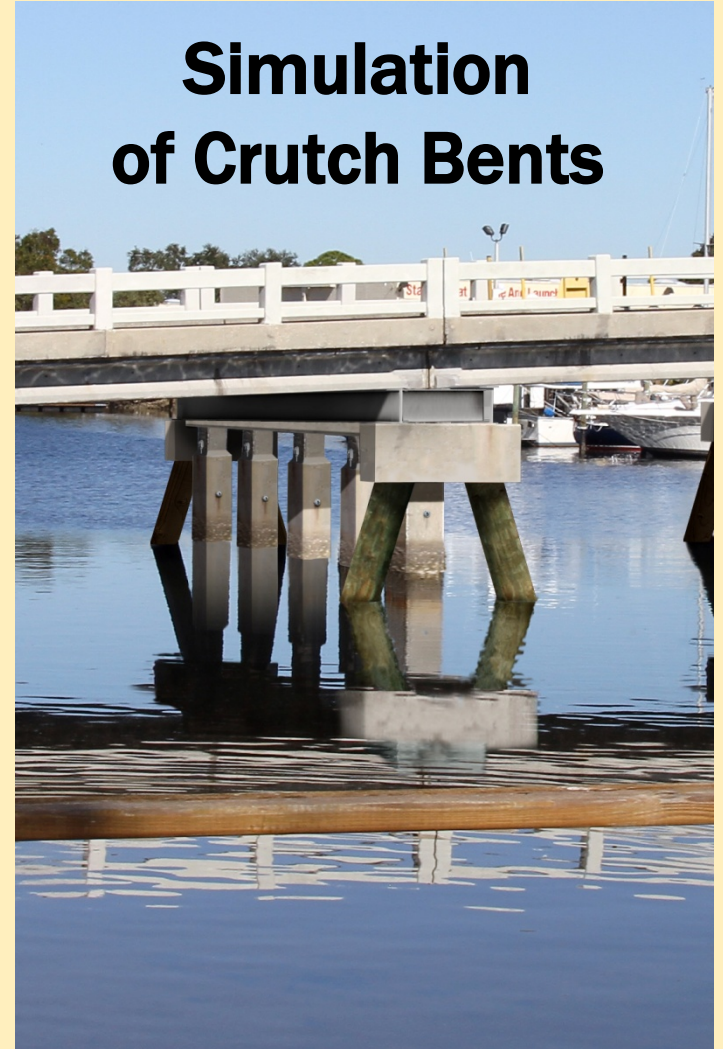
### Major Issues

- Structural concerns – unknown foundations
- Vehicular/pedestrian safety
- Link in future Howard Park Trail
- Life-cycle costs higher compared to replacement
- **Bascule Span and Pier Only Remaining Original Elements**
- **Crutch Bents and Pile Jackets Required**

**Existing Bridge**



**Simulation  
of Crutch Bents**



## Original Rehabilitation Concept - **\$9.5 M**

No Widening/No Sidewalk Improvements

Remaining Service Life - **25 years**

## Rehabilitation (with Widening) - **\$12.5 M**

Provides two 5.5 ft sidewalks

Remaining Service Life - **25 years**

## Reconfiguration of Existing Bridge

No widening, one 5.5 ft sidewalk

**Not Feasible**

## New Movable Bridge - **\$15.8 M**

Provides two 6 ft sidewalks

Service Life - **75 years**



## Costs Compared over a 100 Year Period

- Rehabilitate the bridge in 2020 then replace it with a new movable bridge in 2038  
(25 years from 2013)

Versus

- Replace the bridge in 2020 with a new movable bridge

**Result - More Cost Effective to Replace Bridge in 2020**

## SHPO Evaluation

- Engineering Analysis provides “*ample evidence to support the project team’s opinion that a new bridge would be preferable to the rehabilitation.*”
- Mitigation will be required if existing bridge is demolished

## Sufficient documentation to determine Fixed Bridge alternatives not feasible

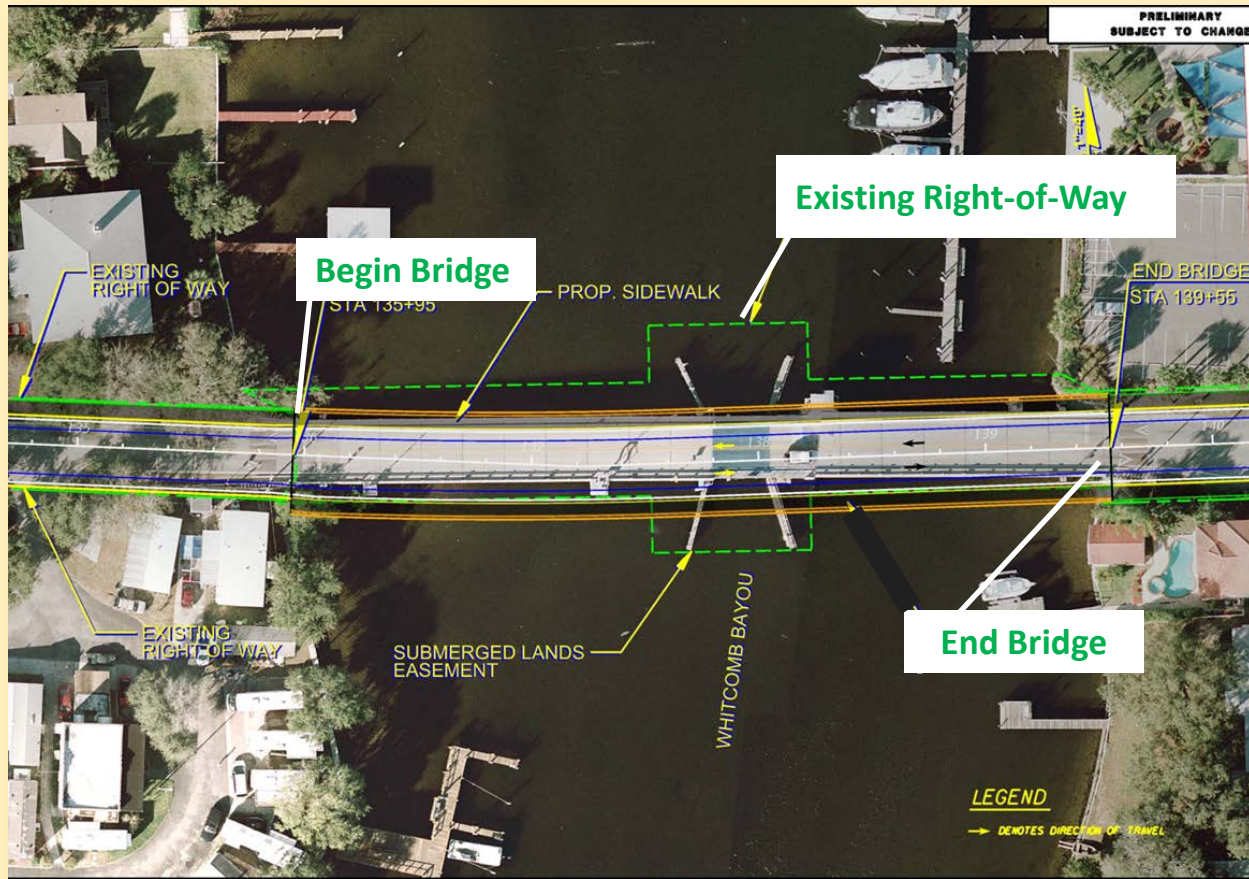
- USCG determined that 28 feet of vertical clearance “Does Not Meet the Needs of Navigation”
- Substantial right-of-way impacts
- Substantial visual impacts
- Not consistent with historic character of community
- Requires two-year detour during construction
- Cost **\$14 M - \$15 M** (including Right-of-way) compared to New Movable **\$15.8 M**

Based on extensive evaluation and consideration of:

- Engineering and Costs
- Safety of vehicles, bicyclists and pedestrians
- Potential socioeconomic and community impacts
- Impacts to the natural and physical environment
- Impacts to cultural resources
- Impacts to adjacent properties
- Impacts to the boating community
- Consideration of public input
- Other potential impacts

Replacement with a New Movable Bridge  
“Recommended Alternative” for presentation at  
Public Hearing

## No Impacts to Adjacent Property

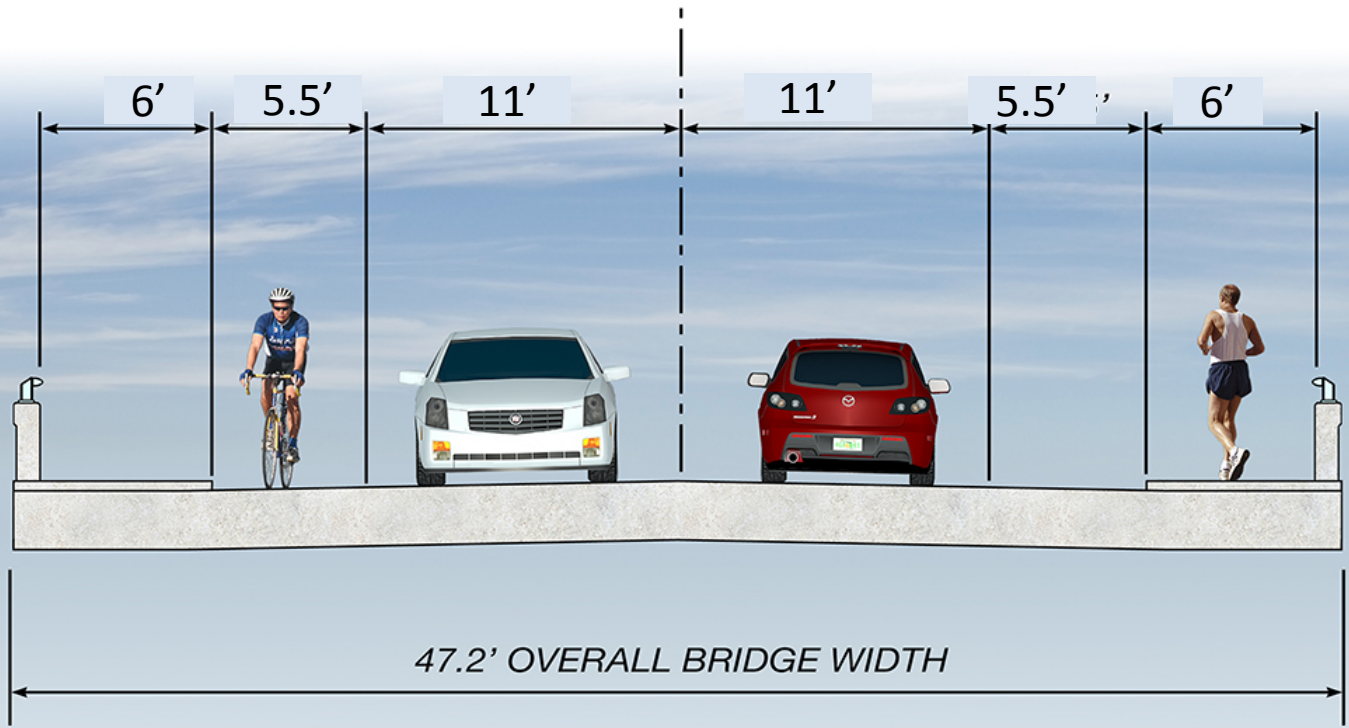


## Description

- No right-of-way impacts
- Vertical Clearance 7.8 feet
  - (existing 6 feet)
- Horizontal Clearance 25 feet
  - (same as existing)
- Total Width 47.2 feet
  - Approximately 19 feet wider than existing
  - 11 ft travel lanes
  - 5.5 ft shoulders and 6 foot sidewalks – both sides

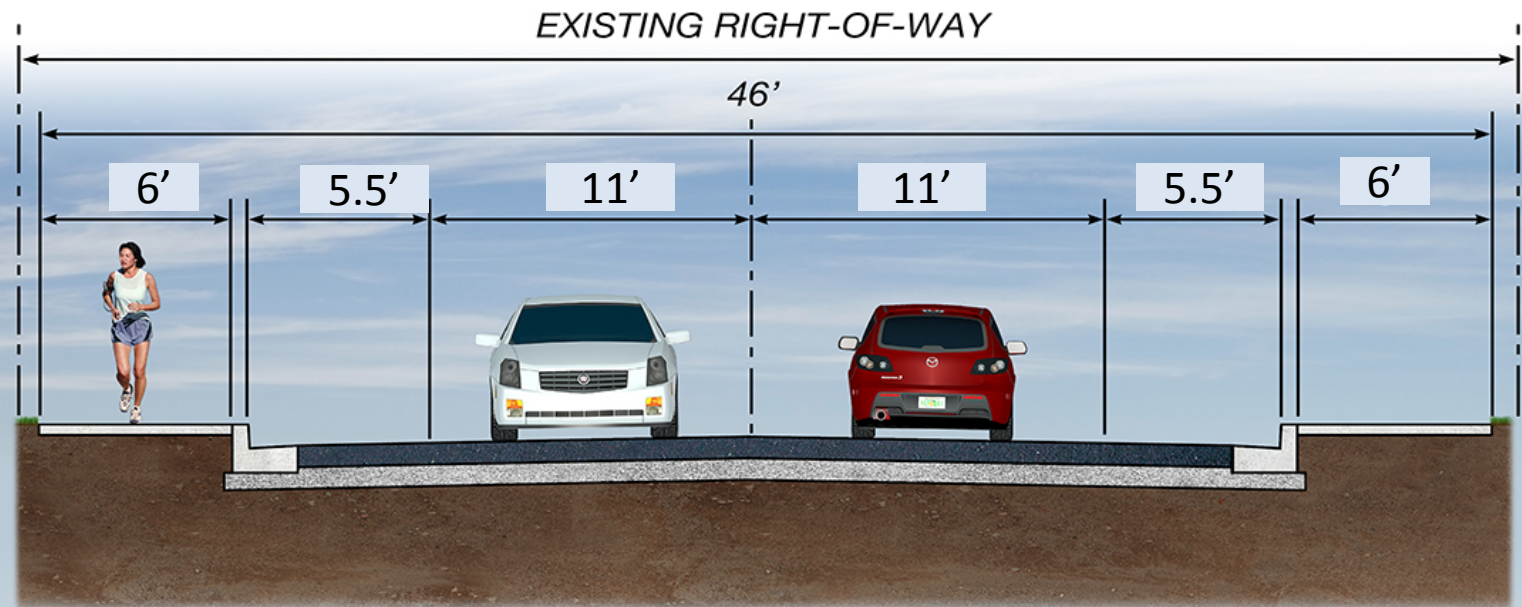
# Movable Bridge Typical Section

**Total Bridge Width - 47.2 feet**



# Pinellas County Proposed Roadway Typical Section – East of Movable Bridge

Total Width – 46 feet

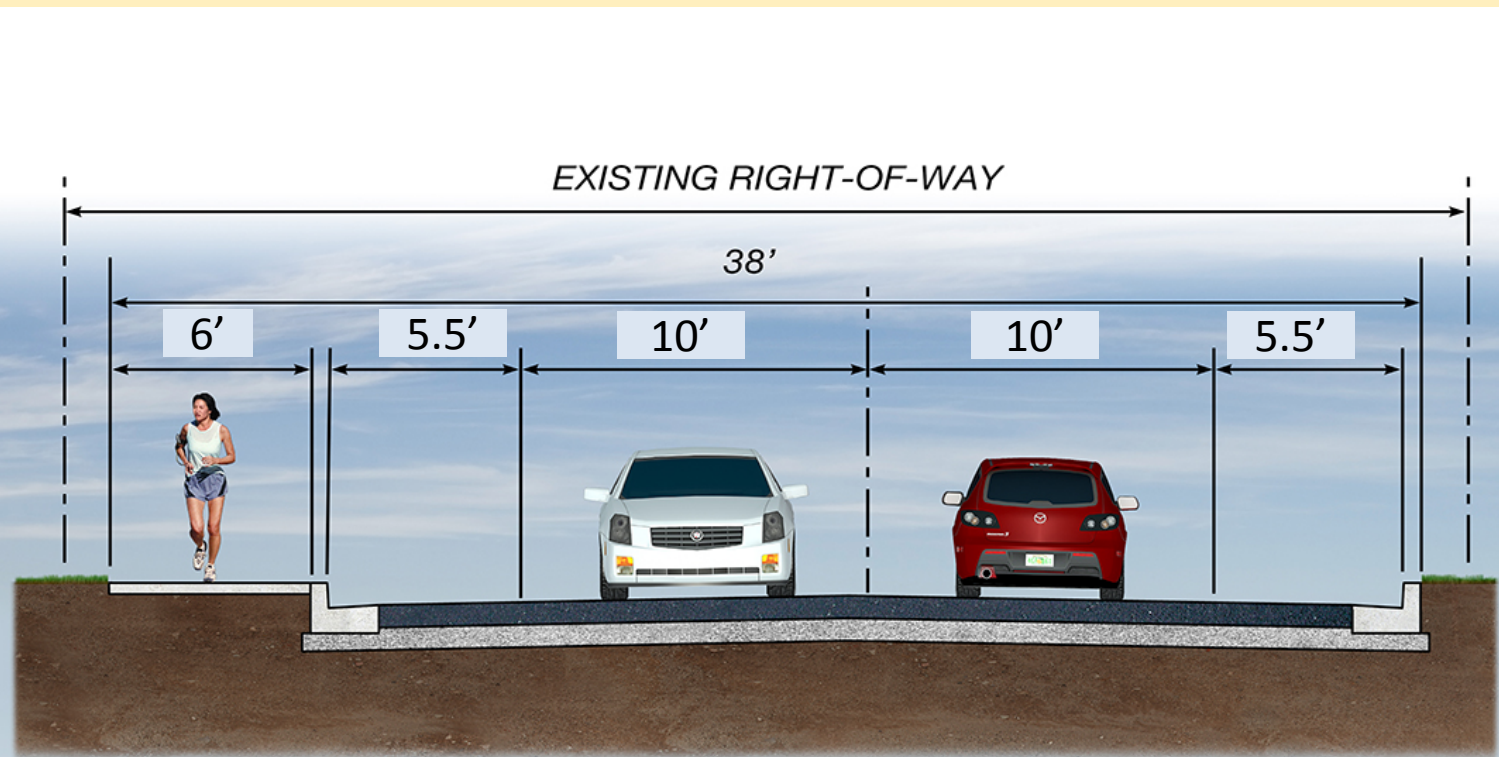






# Proposed Roadway Typical Section – West of Movable Bridge

Total Width – 38 feet



# Pinellas County Existing Bridge





## “Generic” Movable Bridge





# New Movable Bridge

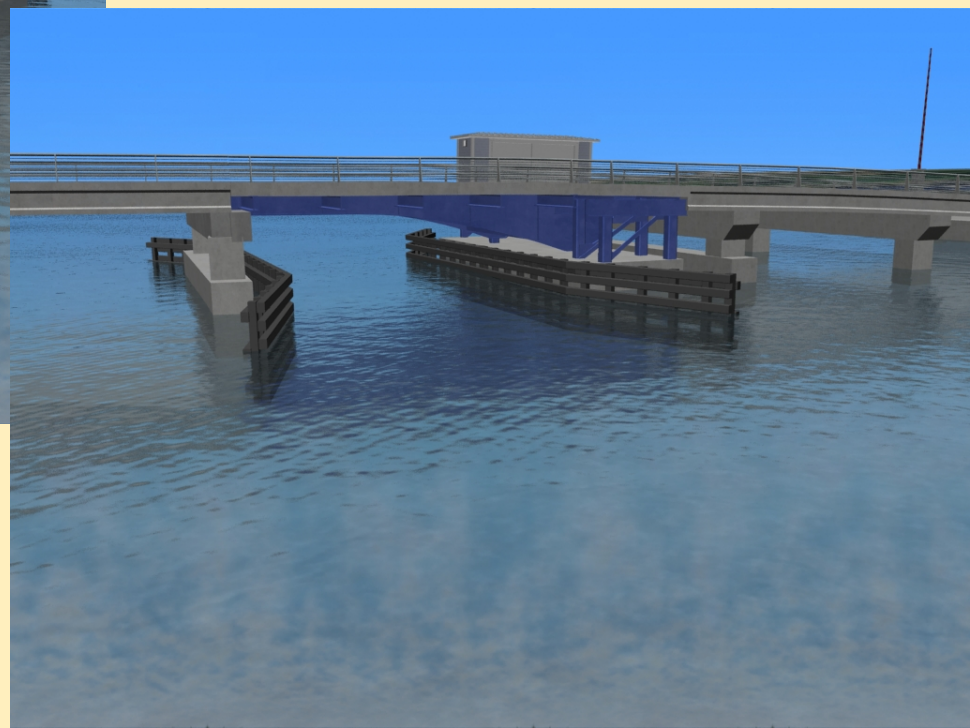
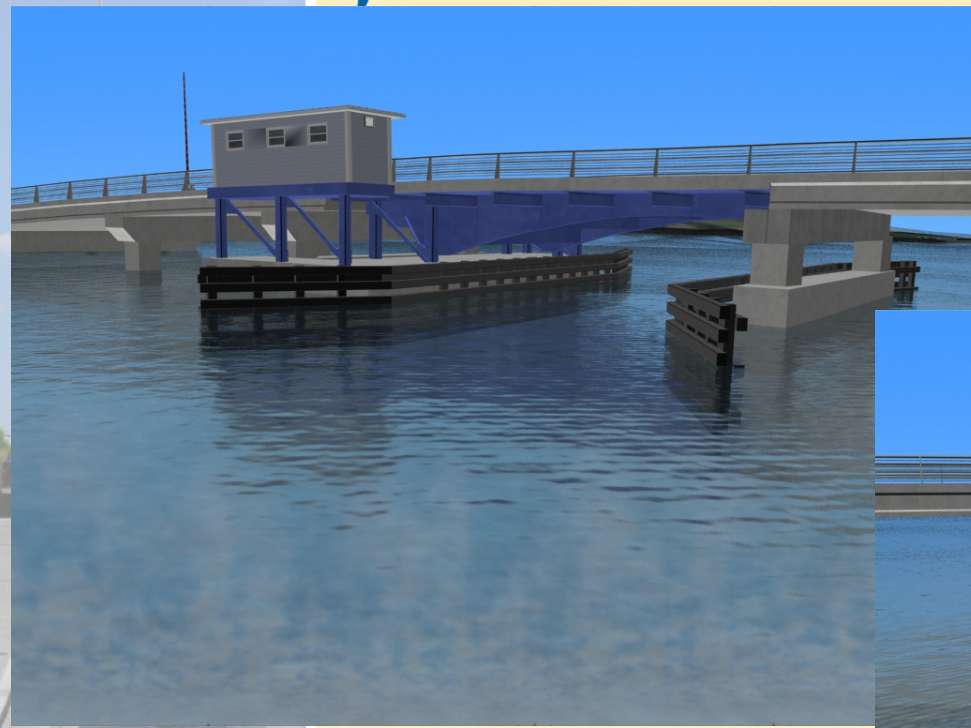
## “Industrial” Style Rolling-Lift Bascule Bridge



## “Industrial” Style Rolling-Lift Bascule Bridge



**3D Model Views  
Industrial Style**



If Conceptual Design for the Movable Bridge is

- Selected as “Preferred Alternative” after the Public Hearing

and

- Approved by FHWA

Aesthetics will be determined in Design Phase  
Future Opportunities for Public Input

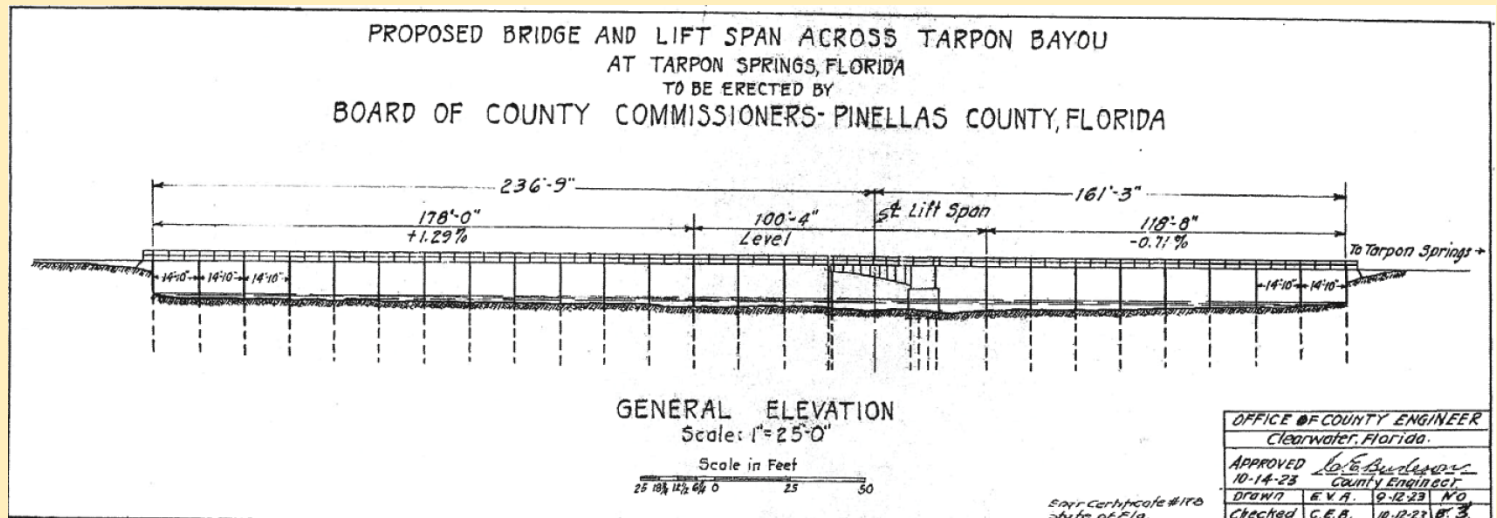




## Required Mitigation

### Historic American Engineering Record (HAER) Documentation

- Large format photographs
- Written history/narrative
- Historic bridge plans copied on archival paper





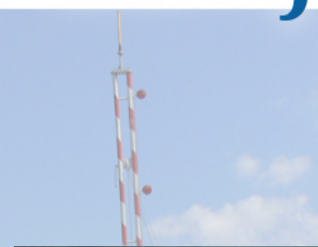
## Possible Mitigation

- Choose Bridge Rail to Preserve Viewshed from Bridge
- Educational Kiosk/Monument in Public Space
  - On or Near Bridge
  - In City Park or Museum
- Incorporate Monument into Second Control House
- Incorporate Portion of Original Bridge into New Bridge



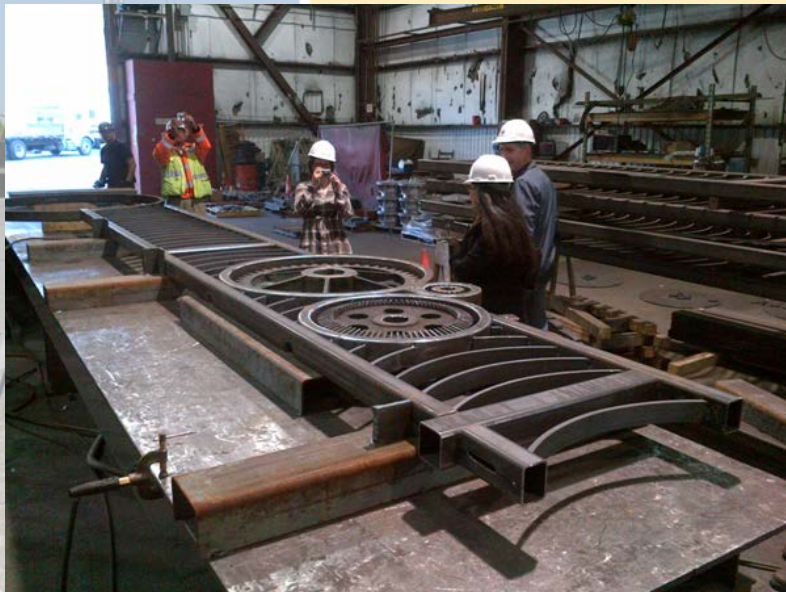
**Example – Treasure Island**

**Monument Bridge in City Park – Treasure Island**



**Example - South Park Bridge, Seattle, WA**

**Incorporating Part of Existing Bridge into New Bridge**





**Incorporating Part  
of Existing Bridge  
into New Bridge**  
**Example:**  
**South Park Bridge**

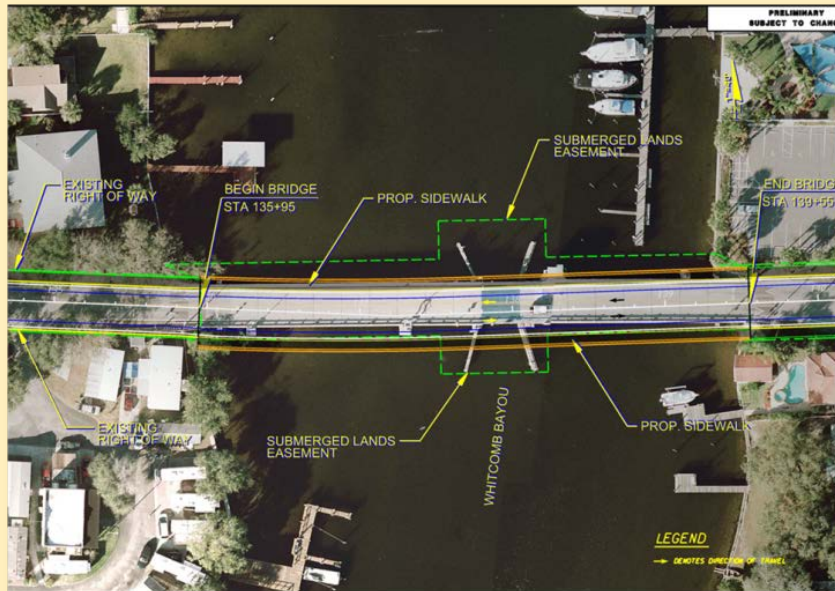


## Present Recommended Alternative at Public Hearing in February 2014

- Presentation will include discussion of all alternatives considered
- Public comments recorded by court reporter
- Comments included in Project Record

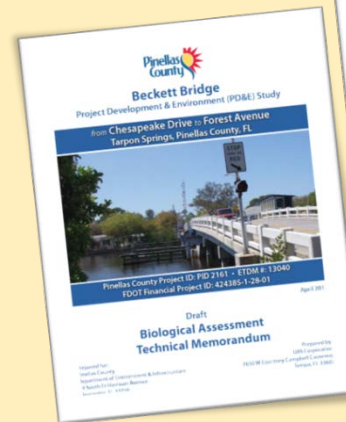


- **CRC Meeting**
  - Continue coordination of Section 106 Issues
  - Solicit input on possible mitigation if Movable Bridge is selected as “Preferred Alternative”



- Consider Public Hearing Input
- Finalize Engineering/Environmental Documents
- Continue SHPO Coordination
  - Complete Section 106 documents
  - Develop MOA
    - SHPO, FHWA, FDOT,
    - USCG, County

**Submit Final Documents to FHWA for Approval**





# Questions and Discussion



# Beckett Bridge PD& E Study

## Alternatives Presentation to:

### MPO Board, Technical and Advisory Committees



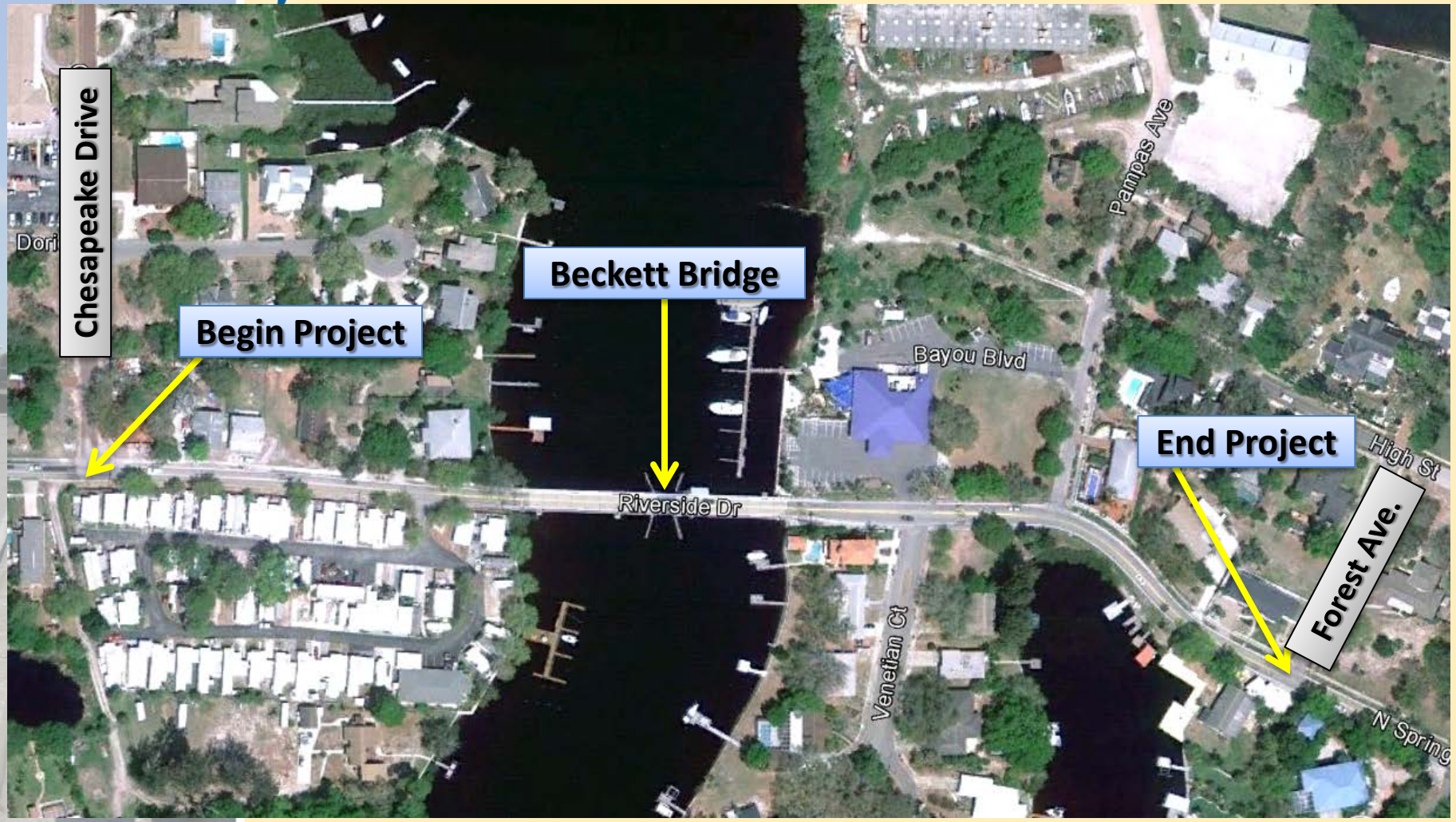
**ECDriver**  
& Associates, Inc.

**URS**

**JANUS**  
RESEARCH

October - November 2012







# Existing Bridge

- Determined to be Eligible for listing in the National Register of Historic Places
- Original Timber Construction – 1924
- Rebuilt Concrete Bridge – 1956
  - Major Repairs in 1979, 1998 and 2011
- Sufficiency Rating - 44.9 (Scale of 1 -100)
  - Structural Concerns
  - Functionally Obsolete

- Vertical Clearance – 6 ft
- Horizontal Clearance – 25 ft
- Opens with 2-hr Notice

**Total Bridge  
Openings**

**2009 - 10**

**2010 - 20**

**2011 - 18**



- **Structural Issues**
  - Posted Weight Restrictions (12 tons)
  - Foundations susceptible to settlement
  - Scour Susceptible



- **Functionally Obsolete**
  - **Narrow Sidewalks**
    - Do Not Meet ADA Requirements



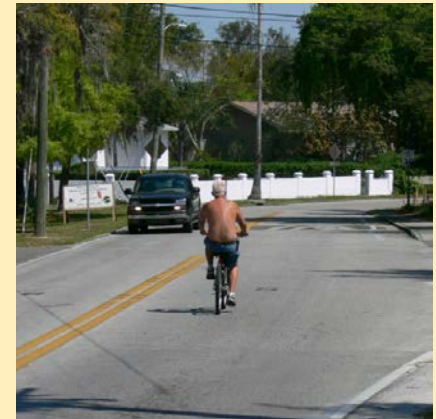
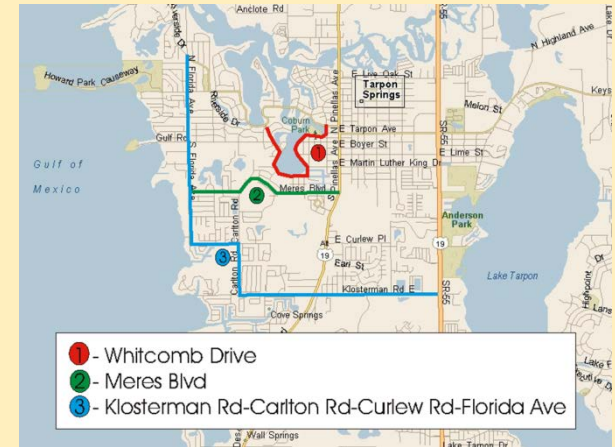
- **Narrow Lanes**
  - No Shoulders
  - No bicycle lanes



- **Engineering, Social & Environmental Studies**
- **Community Involvement**
- **Develop and Evaluate Alternatives**
- **Select Preferred Alternative**
- **Obtain FHWA Approval**



- **Community Concerns**
  - Vertical Clearance
  - Noise
  - Construction Impacts
  - Detour/MOT
  - Aesthetics
  - Bicycle/Pedestrian Facilities





- Project Website
- Coordinate with Local Government
- Meet with Stakeholder Groups
- Alternatives Public Workshop
- Public Hearing





- No-Build
- No-Build with Permanent Removal of Existing Bridge
- Rehabilitation
- Replacement on Existing Alignment
  - Low-Level Movable Bridge
    - 7.8 ft Vertical Clearance
    - 25 ft Horizontal Clearance
  - Mid-Level Fixed Bridge
    - 28 ft Vertical Clearance
    - 25 ft Horizontal Clearance

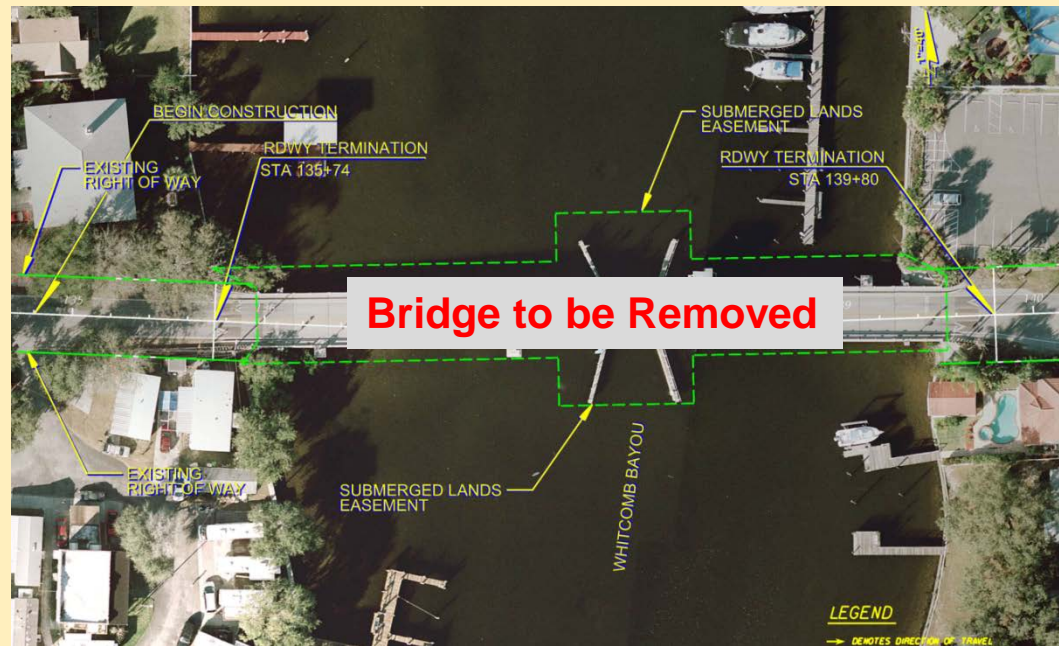


- Existing Bridge Remains Until No Longer Serviceable (approximately 10 years)
- Routine Maintenance Only
- No Major Improvements



# No Build Alternative – Permanent Removal of Existing Bridge

- Routine Maintenance Only
- Existing Bridge Demolished When No Longer Serviceable
- No Replacement Bridge Constructed



- Extensive Repairs
- Correct Structural Deficiencies
- No Widening
  - No Shoulders
  - Narrow Sidewalks would remain
- No Change in Navigational Clearances
- Extend Service Life 25-30 years



# Replacement Alternatives Constraints

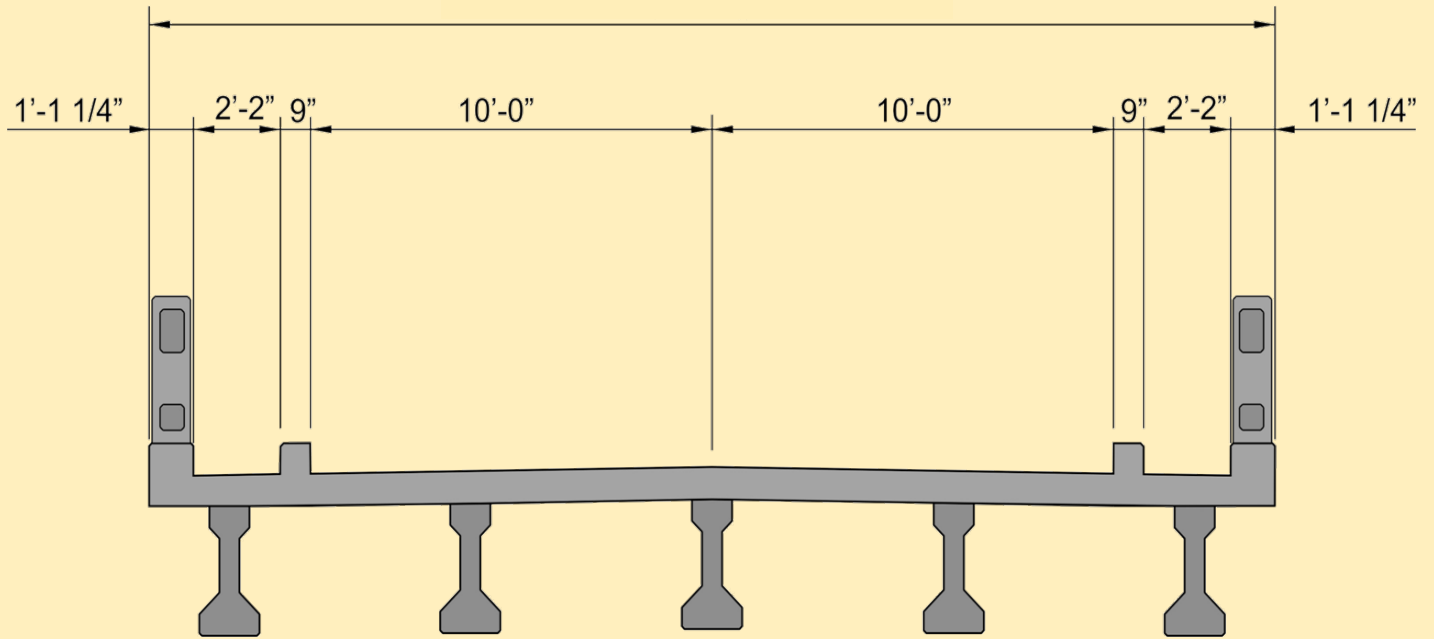
- **Limited Right-of-Way**
  - 50 ft ROW East of Bridge
  - 40 ft ROW West of Bridge



- **Adjacent properties**
- **Driveways**

## Existing Bridge Typical Section

28'-0 1/2" Total Width



**No Shoulders**

**Narrow Sidewalks**



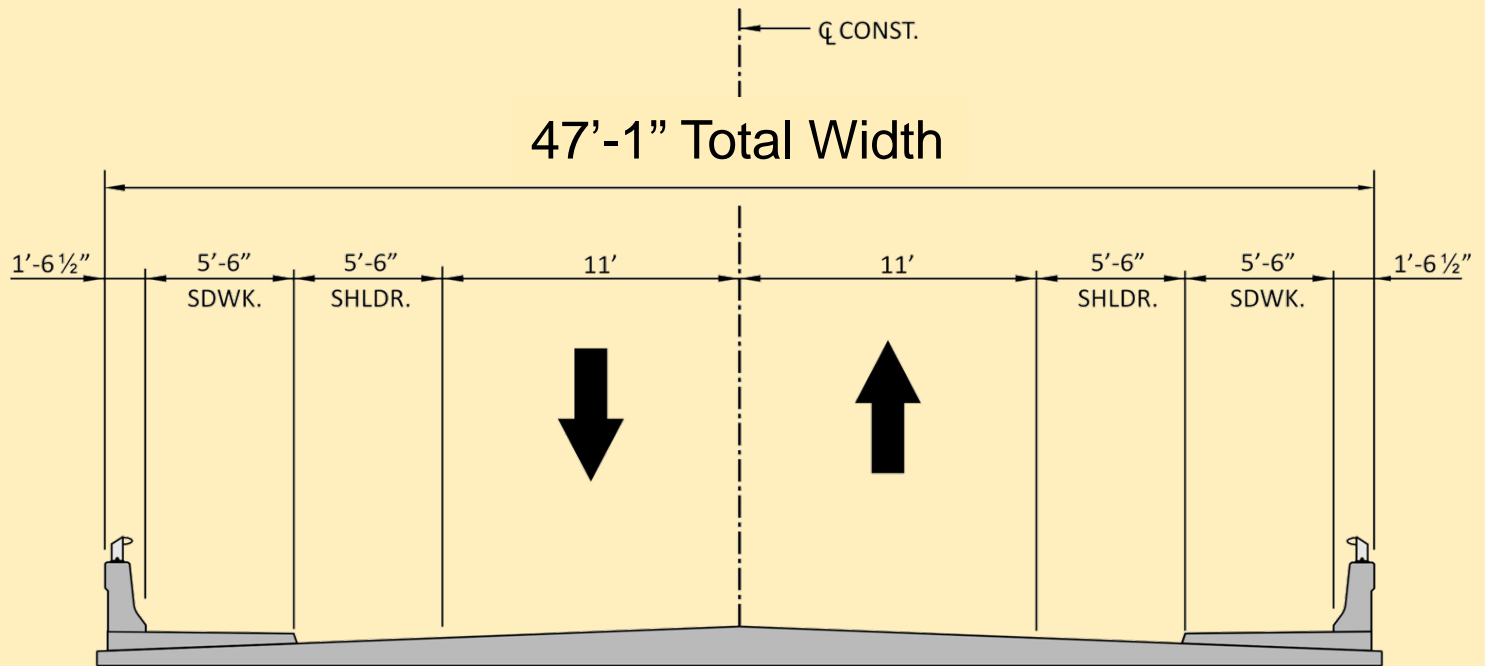


# Proposed Typical Sections

- **Meet Current Safety Standards**
- **Accommodate Future Trails**
- **Minimize ROW Impacts**
- **Lane Widths**  
**10 to 11 feet**

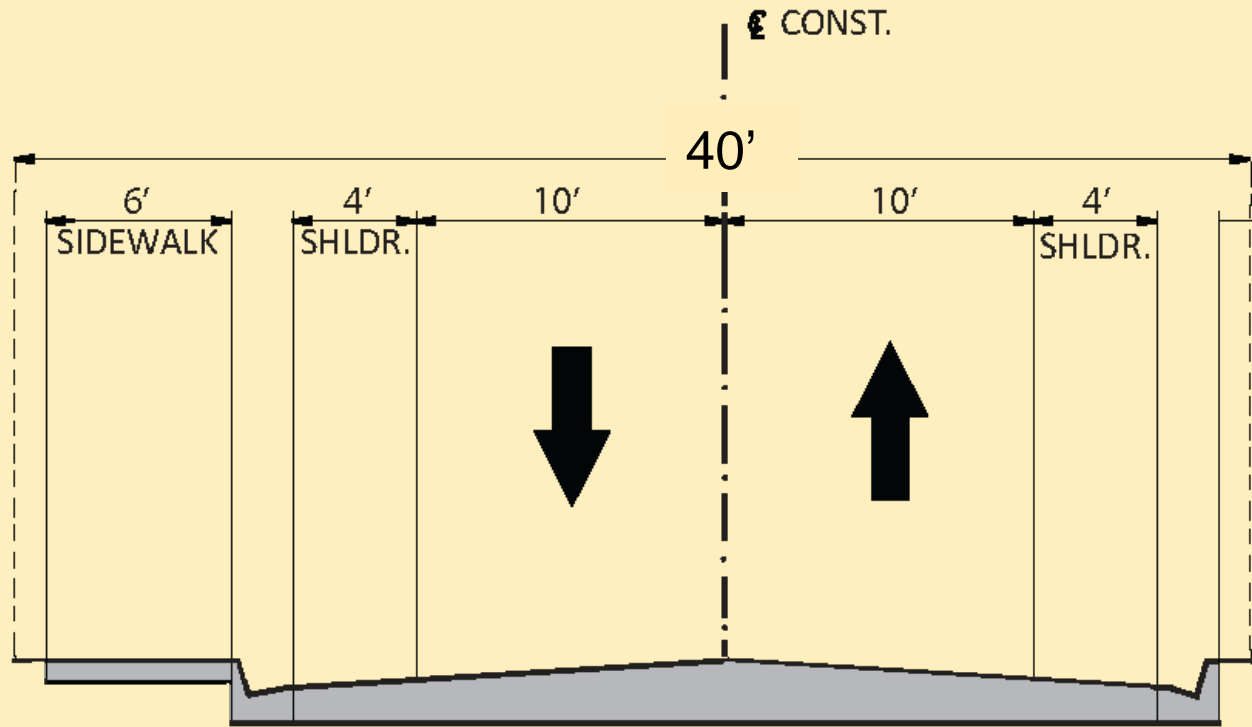


## Proposed Bridge Typical Section

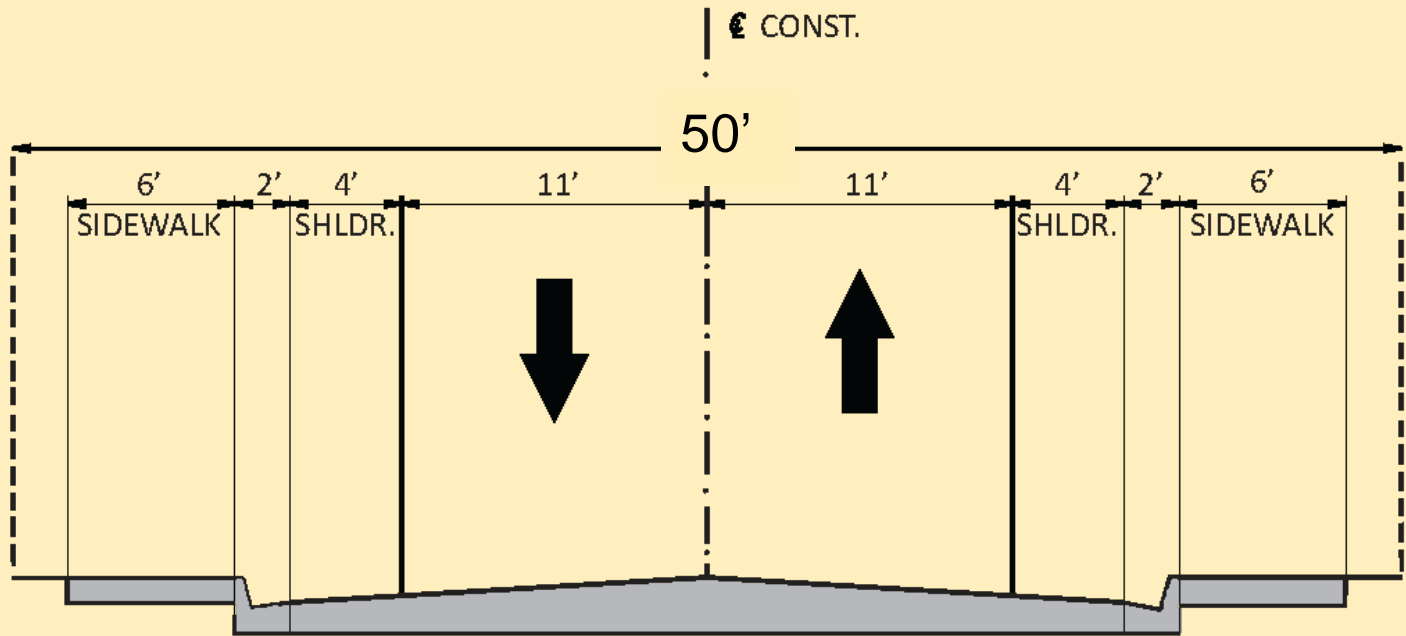


**5'6" Sidewalks and Shoulders – Both Sides**

## Roadway Section – West of Bridge



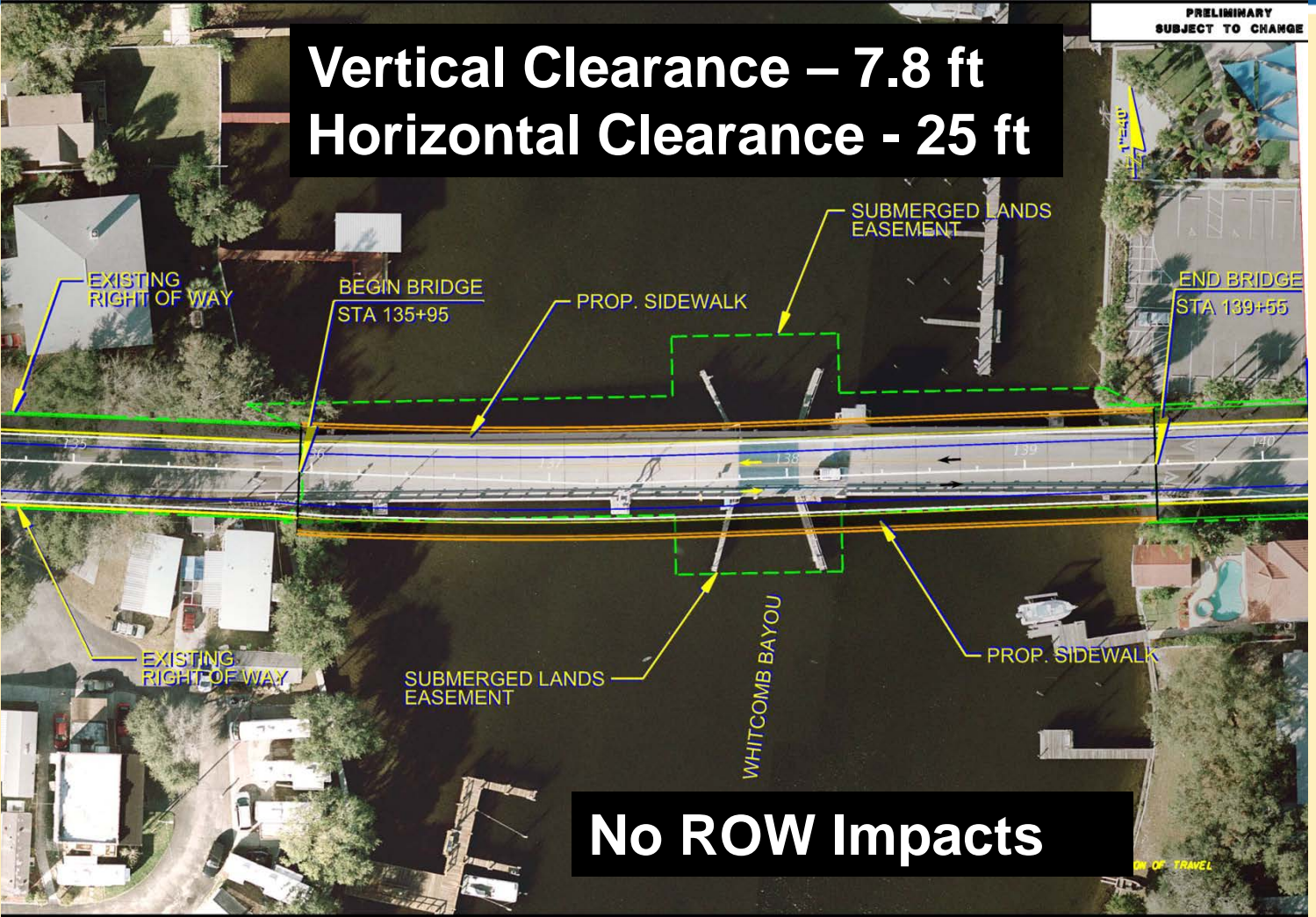
## Roadway Section – East of Bridge



**6 ft Sidewalks - Both Sides**



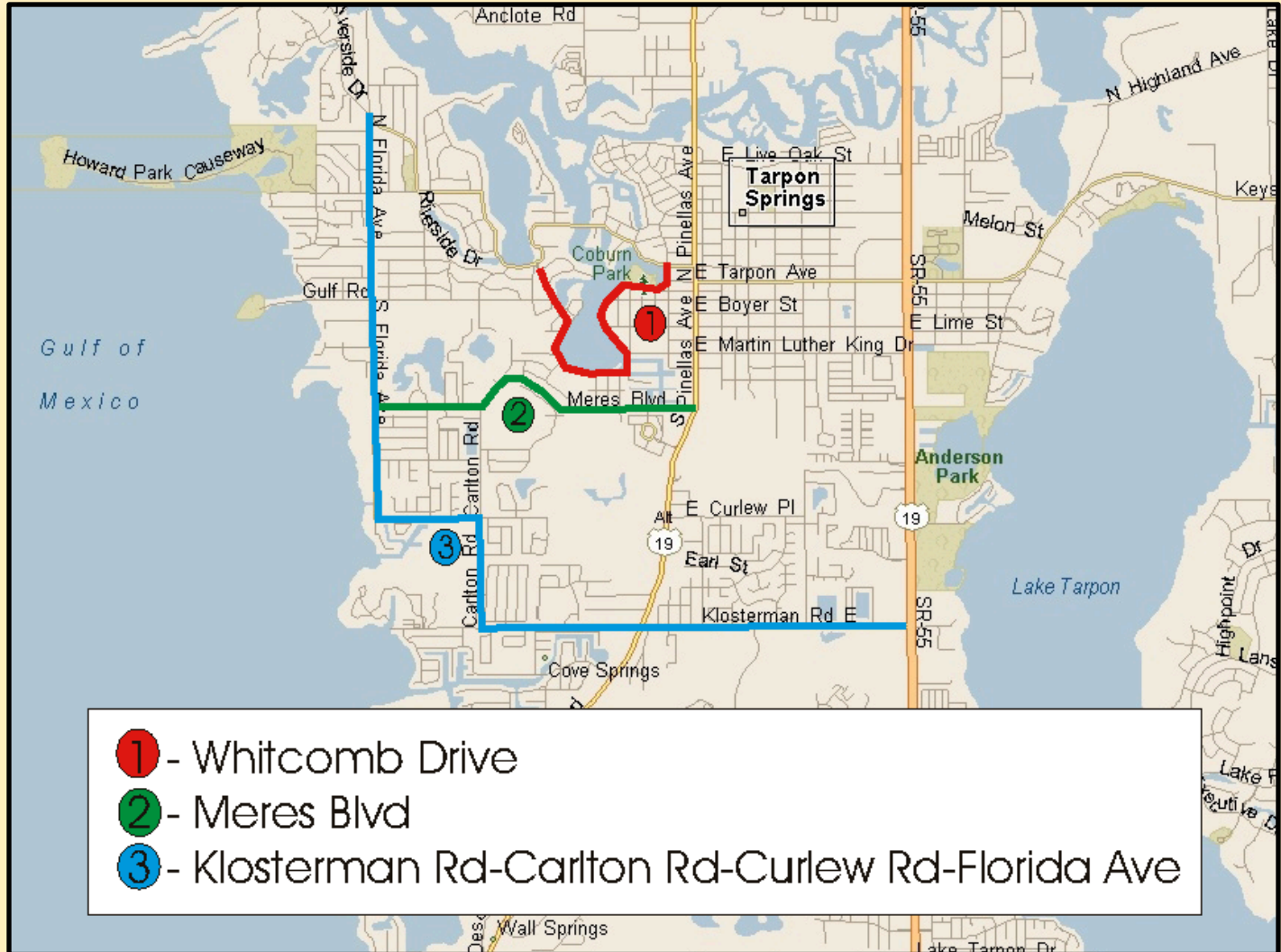
# Low-Level Movable Bridge







# Alternate Detour Routes





- **Vertical Clearance – 28 feet**
- **Horizontal Clearance – 25 feet**
- **Retaining Wall 1 to 19 feet High**
  - **Blocks Driveway Access/Visual Impacts**
- **ROW impacts**
  - **Single Family Residences North Side, West of Bridge**
  - **Yacht Club Property**

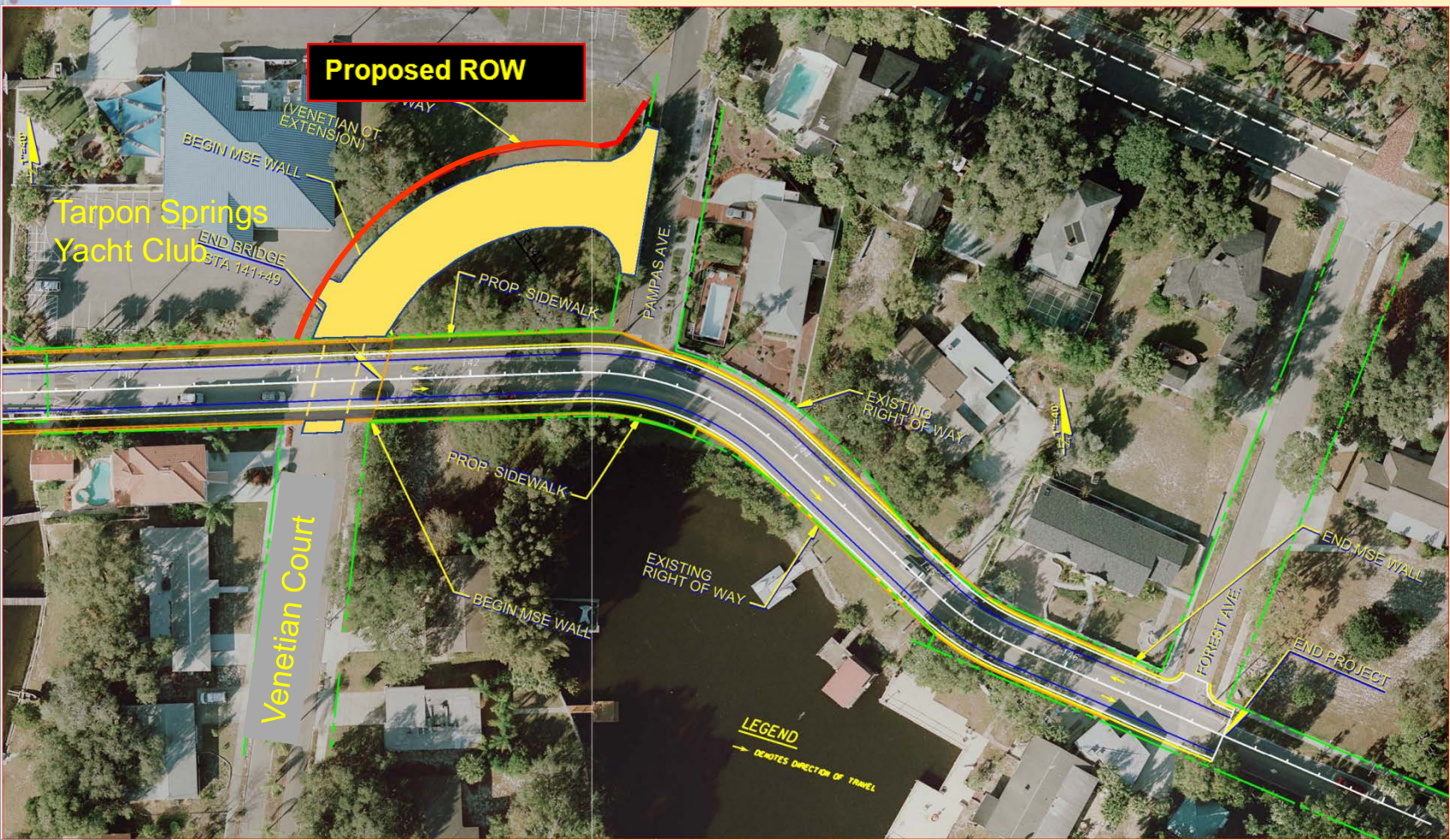






# Mid-Level Fixed Bridge – Option A

## Venetian Court Extension







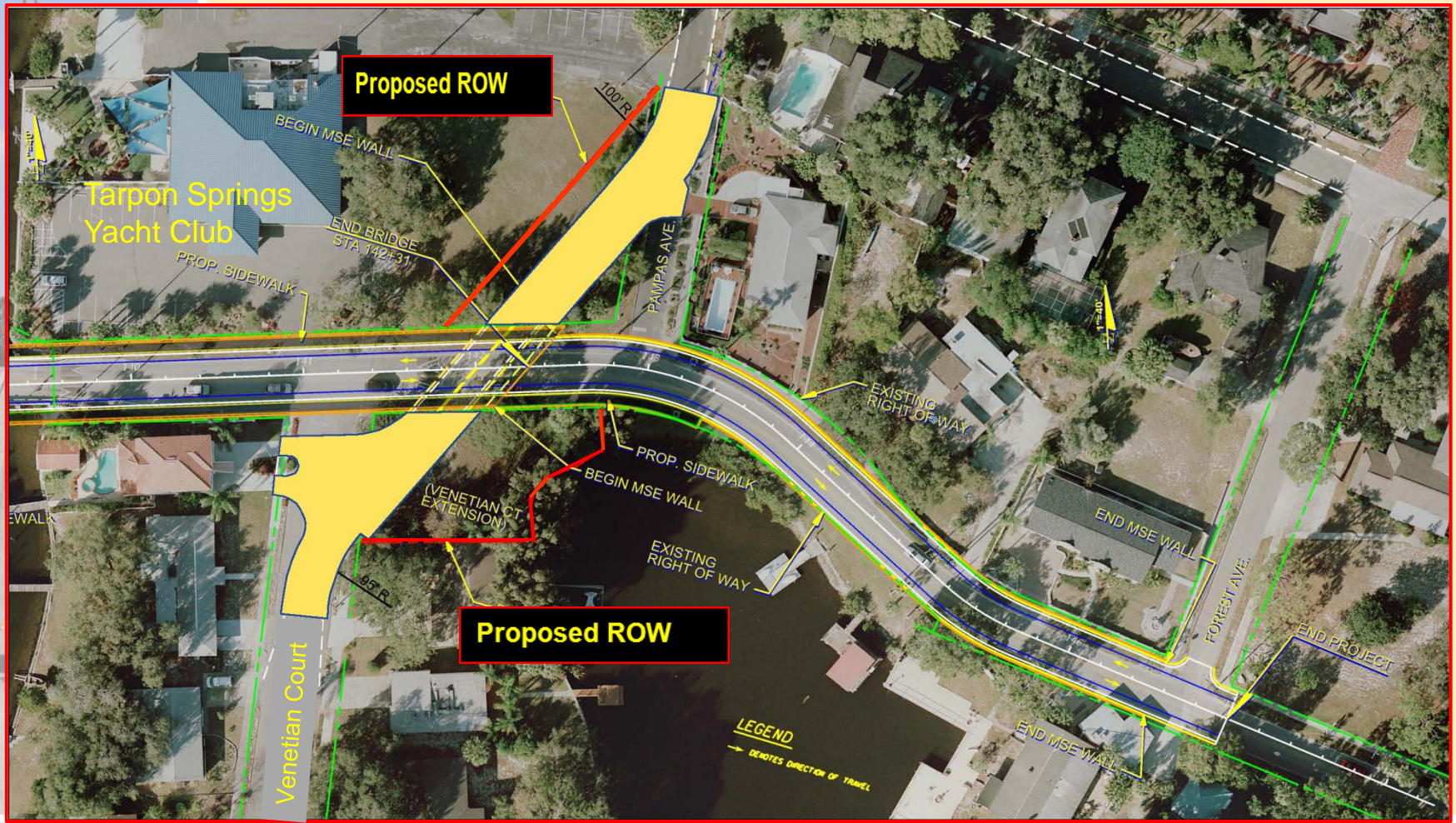
- **Vertical Clearance – 28 feet**
- **Horizontal Clearance – 25 feet**
- **Retaining Wall 1 to 16 feet High**
  - Blocks Driveway Access/Visual Impacts
- **ROW impacts**
  - Single Family Residences North Side,  
West of Bridge
  - 8 Mobile Homes
  - Yacht Club Property
  - Vacant Lot East of Bridge, South Side of Roadway





# Mid-Level Fixed Bridge – Option B

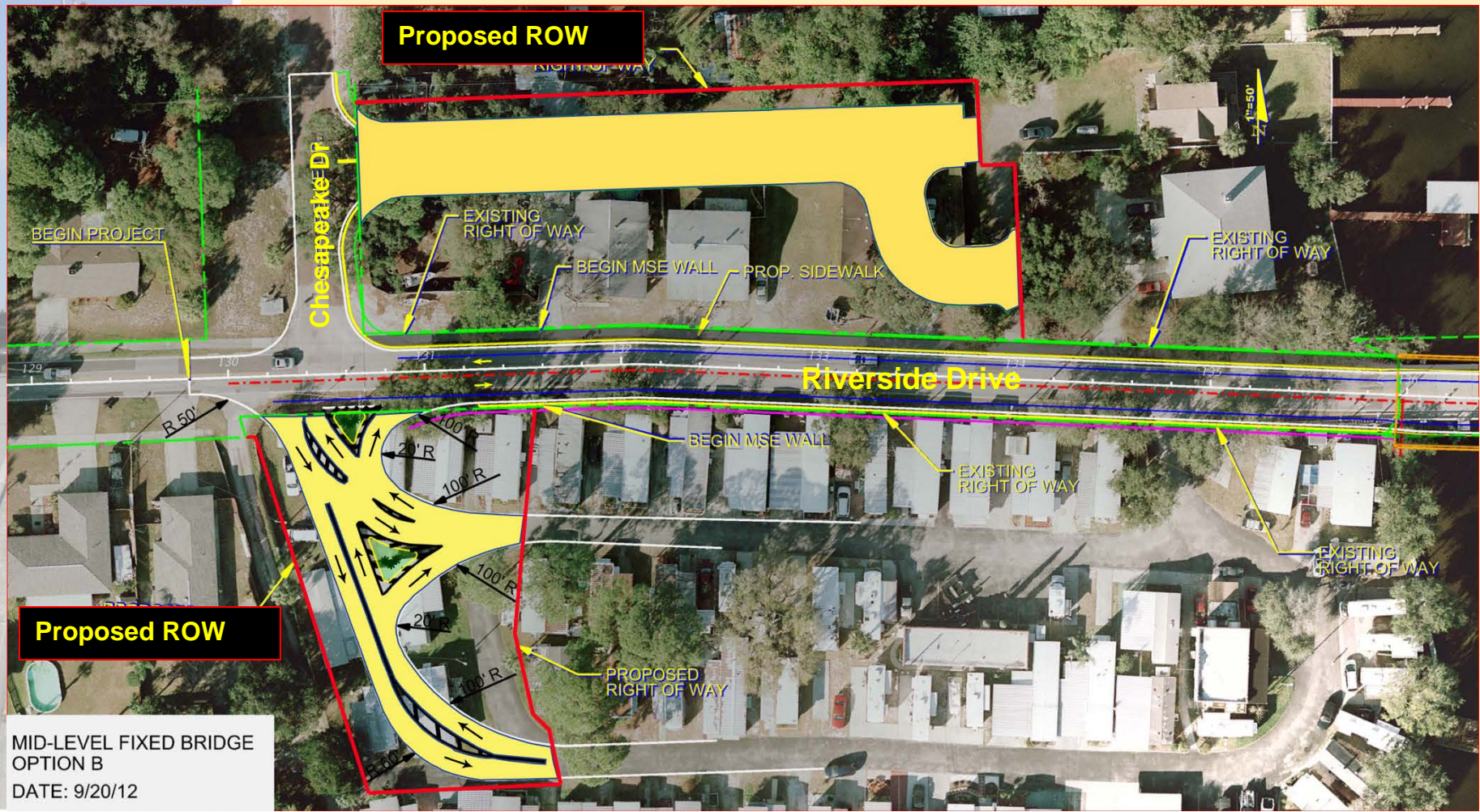
## Venetian Court Extension





# Mid-Level Fixed Bridge – Option B

## Alternate Access to MHP and Waterfront Properties



MID-LEVEL FIXED BRIDGE  
OPTION B  
DATE: 9/20/12

## Boat Survey of Waterfront Property Owners on Whitcomb Bayou

- 289 Surveyed
- 99 Responded



## Results

- Most Boats 22-26 ft Powerboats
  - No Bridge Opening Required
- 4 Sailboats required  $\geq 28$  ft of Clearance
  - Could not Pass Under Mid-Level Option

## Similar for All Build Alternatives

- **Wetlands**
  - 0.02 acre – Low-Level
  - 0.03 acre – Mid-Level
  - Wildlife - Minimal
- **Noise - Minimal**
- **Visual – Minimal to High**
- **Historic Structures**
  - Removal of NRHP Eligible Bridge Requires MOA/Mitigation



# Upcoming Community Involvement Activities

- 
- **Local Government Presentations**
    - City of Tarpon Springs Commission (Nov 20)
    - Pinellas MPO TCC and CAC (Oct 24, 25)
    - Pinellas MPO (Nov 14)
  - **Stakeholder Meetings\***
    - Tarpon Springs Yacht Club
    - Rotary Club
    - Chamber of Commerce
    - Bayshore Mobile Home Park

\*No Date Scheduled
  - **Cultural Resource Committee Meeting** (Oct 29)





# PD&E Schedule

Activity	2012				2013				
	Jan - Mar	Apr - Jun	Jul - Sep	Oct - Dec	Jan - Mar	Apr - Jun	Jul - Aug	Sep - Oct	Nov - Dec
Study Begins	★								
Data Collection	█								
Develop/Evaluate Alternatives	█								
Local Government Coordination			█						
Alternatives Workshop					★				
Select Preferred Alternative					█				
Public Hearing							★		
Final Reports/Agency Coordination					█				
Approval by the Federal Highway Administration									★



Questions?

