

# Project Development & Environment Study



## Final Comments & Coordination Report

### SR 60 (Courtney Campbell Causeway)

Multi-Use Trail PD&E Study  
from Bayshore Boulevard to West of Ben T. Davis Beach Entrance

Pinellas and Hillsborough Counties, Florida

Florida Department of Transportation  
District 7

Work Program Item Segment Number: 422640 2  
FAP Number: 9045-090-C  
June 2011





# Project Development & Environment (PD&E) Study

**SR 60 (Courtney Campbell Causeway)  
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From Bayshore Boulevard to West of the Ben T. Davis Beach  
Entrance

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WPI Segment No.: 422640 2  
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Pinellas & Hillsborough Counties, Florida

Prepared for the



Florida Department of Transportation  
District Seven

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June 2011

## EXECUTIVE SUMMARY

The Florida Department of Transportation (FDOT), District Seven, is conducting a Project Development and Environment (PD&E) study to evaluate a proposed multi-use trail along approximately 7.4 miles of State Road (SR) 60 (SR 60) (Courtney Campbell Causeway (Causeway) from Bayshore Boulevard in Pinellas County to west of the Ben T. Davis Beach entrance in Hillsborough County, Florida. Design and construction for this project is currently funded in the *FDOT Tentative Work Program 2012-2016*.

The objective of this PD&E study is to assist the FDOT and the Federal Highway Administration (FHWA) reach a decision on the type, location, and conceptual design of the proposed multi-use trail to accommodate recreational users who can experience the scenic qualities of the Causeway, further enhancing tourism and economic development. This study will document the need for the improvements as well as the procedures utilized to develop and evaluate various improvements including elements such as proposed typical sections, preliminary horizontal alignments, and enhancement alternatives. The social, physical, and natural environmental effects and costs of these improvements will be identified. The alternatives identified in the 2008 Feasibility Study (*Project Concept Summary Report*) were evaluated and compared based on a variety of parameters utilizing a matrix format. Based on the evaluation as documented in the 2008 Feasibility Study, the recommended alternative is S2, the South alignment with separate structures over Old Tampa Bay at two locations. The remainder of the project would be constructed on the existing SR 60 causeway fill section.

This PD&E study satisfies all applicable requirements, including the National Environmental Policy Act (NEPA), in order for this project to qualify for further federal-aid funding of subsequent development phases (design and construction).

The project was evaluated through the FDOT's Efficient Transportation Decision Making (ETDM) process. This project is designated as ETDM project #13102. An ETDM *Programming Screen Summary Report* was published on March 29, 2011 and contains comments from the Environmental Technical Advisory Team (ETAT) on the project's effects on various natural, physical and social resources.

This Comments & Coordination Report has been prepared as part of this PD&E study. This report has been prepared in accordance with the FHWA's Technical Advisory 26640.8a, dated October 30, 1987, and the FDOT's *PD&E Manual, Part 2, Chapter 31* (revised May 18, 2010). Based on the ETAT comments, the FHWA has determined that this project qualifies as a Type 2 Categorical Exclusion (CE).

In compliance with state and federal rules, regulations, and policies, a Public Involvement Plan was developed in December 2010 and carried out as an integral part of the SR 60 PD&E Study. Public involvement was accomplished during the PD&E Study to keep appropriate

agencies, public officials, property owners, and interested citizens informed and to ensure project compliance with local and regional transportation plans. The FDOT has conducted an interagency coordination and consultation effort, and public participation process. This *Comments and Coordination Report* is one of several documents that has been prepared as part of this PD&E Study. This report documents the PIP, agency coordination efforts, public involvement activities, and comments received.



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# Section 1 - INTRODUCTION

## 1.1 Project Description

FDOT, District Seven, is conducting a PD&E study to evaluate improvements along approximately 7.4 miles of SR 60 (Courtney Campbell Causeway) from Bayshore Boulevard in Pinellas County to west of the Ben T. Davis Beach entrance in Hillsborough County, Florida. This project is currently funded in the *FDOT Tentative Work Program 2012-2016*. A project location map is shown in **Figure 1-1**. The sections, townships and ranges where the project is located are summarized in **Table 1-1**.

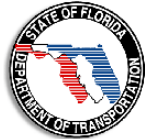
**Table 1-1 Sections, Townships, and Ranges**

Sections	Townships	Ranges
<b>Pinellas County</b>		
13, 14, 15 & 16	29 S	16 E
<b>Hillsborough County</b>		
8, 9, 10 & 11	31 S	19 E

The objective of this PD&E study is to assist the FDOT and the FHWA reach a decision on the type, location, and conceptual design of the proposed multi-use trail to accommodate recreational users who can experience the scenic qualities of the Causeway, further enhancing tourism and economic development. This study will document the need for the improvements as well as the procedures utilized to develop and evaluate various improvements including elements such as proposed typical sections, preliminary horizontal alignments, and enhancement alternatives. The social, physical, and natural environmental effects and costs of these improvements will be identified.

The PD&E study satisfies all applicable requirements, including the National Environmental Policy Act (NEPA), in order for this project to qualify for federal-aid funding of subsequent development phases (design and construction).

The project was evaluated through the FDOT's ETDM process. This project is designated as ETDM project #13102. An ETDM *Programming Screen Summary Report* was published. It contains comments from the ETAT on the project's effects on various natural, physical and social resources. Based on preliminary research and previous studies, it is anticipated that this project will qualify as a Type 2 Categorical Exclusion.



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**Project Location Map**

**Figure 1-1**

Prior to this current PD&E study, FDOT District Seven conducted a feasibility study which was completed in December 2008. The results of that study were documented in a report entitled *Project Concept Summary Report – Final Report, Feasibility Study, SR 60 (Courtney Campbell Causeway) Multi-Use Trail Feasibility Study from McMullen Booth Road to Veterans Expressway*. Note that the limits for the feasibility study were longer than the limits of this current PD&E study. There are several other related ongoing projects, some of which overlap with the current study. All of these related projects are graphically summarized in **Figure 1-2**. The Feasibility Study developed and evaluated alternatives for spanning the Upper Tampa Bay water body at the existing structures by attaching the trail connection to the existing structures or constructing independent structures to complete the connection. The study developed and evaluated any feasible means for the proposed Courtney Campbell Causeway Multi-Use Trail to connect to other trail systems in the future at each end of the proposed trail. Specifically, an evaluation of the trail connections developed by the Tampa Airport Interchange Project Design was reviewed where connections are being made to the Cypress Street Trail at the southeast corner of the feasibility project’s study area.

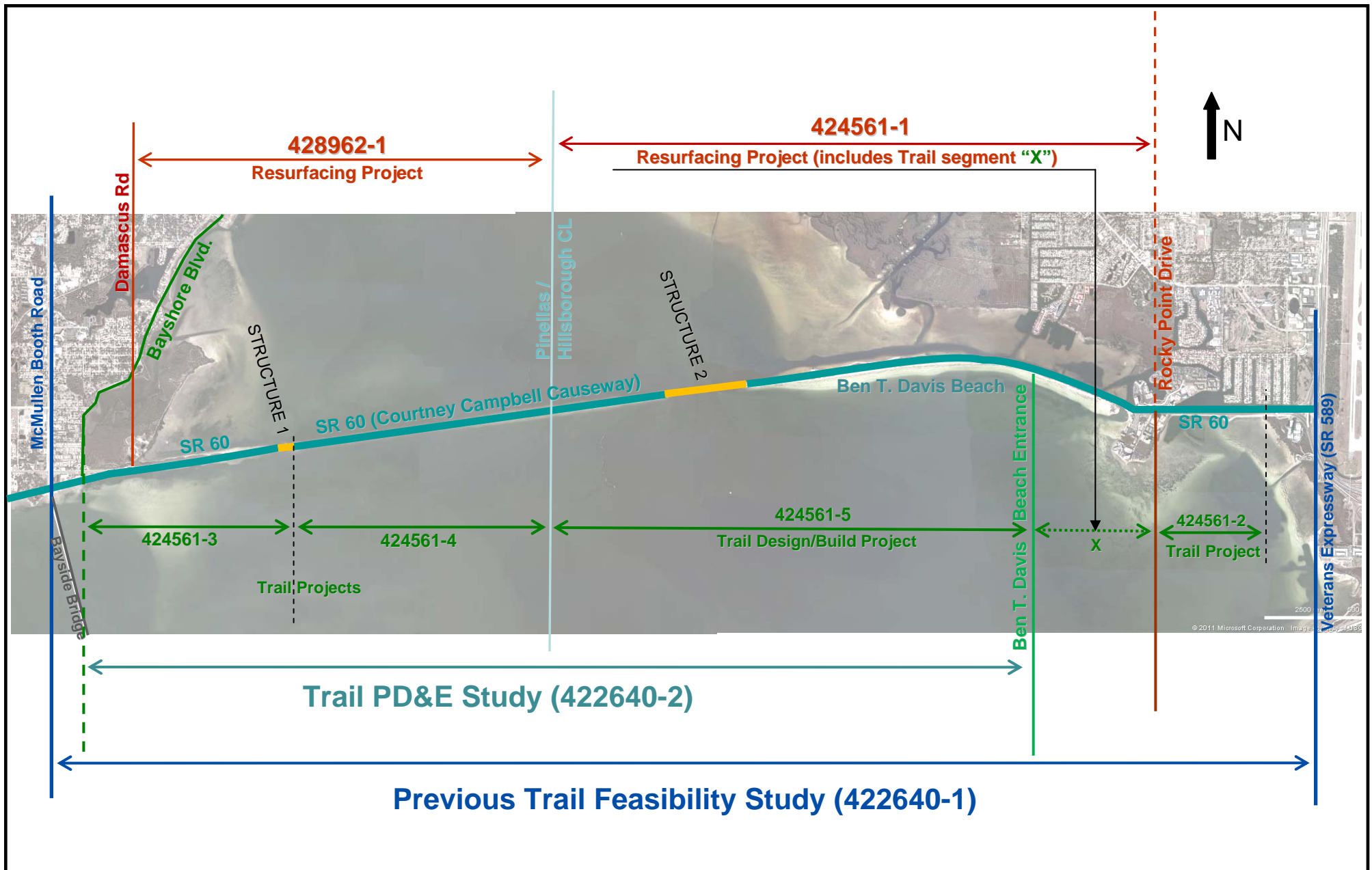
The Feasibility Study included the evaluation of four separate alternatives and one interim staging option. The trail alternatives are located on the *north* or *south* of the causeway and include either the Structural Option ‘W2’ (widening with piles in the water) or Structural Option ‘IS’ (Independent Structure). The intention of the separate bridges is to utilize separate structures to accommodate the trail for non-motorized vehicles and pedestrians. The separate bridges will be designed to accommodate the heaviest required vehicle to perform routine maintenance and inspection.

During the Feasibility Study, two informative newsletters were sent out in October 2007 and April 2008. Also, two open-house Public Workshops were held on May 19, 2008 and May 22, 2008 in Hillsborough and Pinellas Counties, respectively, to present alternative concepts and seek public input.

## **1.2 Existing Facility**

In its entirety, SR 60 is an east-west route that crosses the state of Florida from the Gulf of Mexico (western terminus - Sunsets at Pier 60, Clearwater) to the Atlantic Ocean (eastern terminus - Vero Beach) and is approximately 158.8 miles long. Within the project limits, the Courtney Campbell Causeway is the northernmost bridge crossing over Old Tampa Bay, carrying SR 60 between Clearwater and Tampa, Florida. The Causeway stretches approximately 9.9 miles and is primarily a 4-lane divided rural highway. In 2005, the Courtney Campbell Causeway was designated as an official scenic highway by the state of Florida.





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**Other Related Projects Along  
 SR 60 / Courtney Campbell Causeway**

**Figure 1-2**

The Causeway presently includes intermittent service roads on both sides of SR 60 which are used to provide maintenance access to the existing Causeway and seawall and access to a boat launch along the Causeway. The existing right of way for transportation purposes is 2,640 feet (0.5 mile) in width along SR 60 including submerged lands. Existing SR 60 roadway typical sections are shown in **Figure 1-3**. The two existing SR 60 bridge typical sections are shown in **Figure 1-4**. Structure 1, Bridge No. 150138 is located at the west end of the study in Pinellas County and Structure 2, Bridge No. 100301, is located east of Structure 1 in Hillsborough County. The existing bridges are prestressed concrete girder facilities that were originally built in 1974.

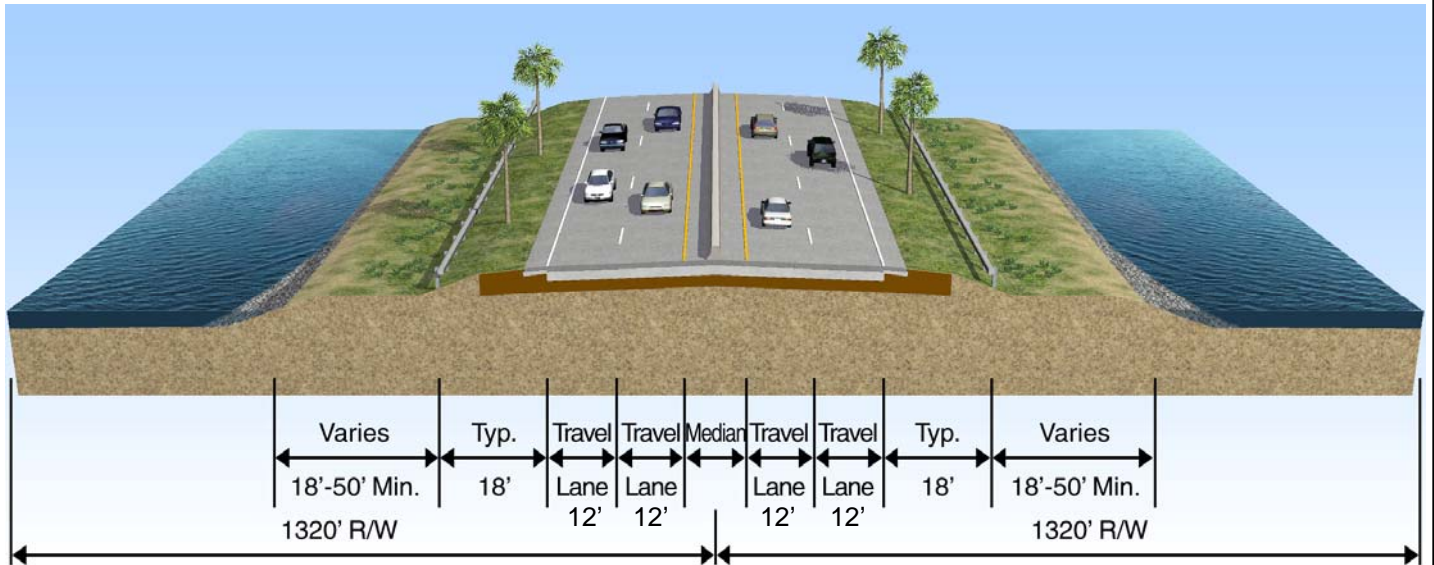
There are four signalized intersections along SR 60 within the study area. They are located at: Bayshore Boulevard (T-Intersection); Beach Entrance/Welcome Center Exit; Damascus Road; and the Boat Ramp Access Road (Mile Post (MP) 2.356). In addition to the SR 60 mainline roadway, there are segments of service roads that run parallel to the Causeway. Not all segments of these service roads allow vehicular access. Those that do, accommodate access for sight-seeing, fishing, and general recreation. The access roads are non-contiguous and do not provide for crossings at channels. In some segments of the Causeway they are non-existent or have been overgrown and have deteriorated. The various segments are found on both the north and south sides of the causeway. As part of the Feasibility Study, options were explored using those segments for the proposed multi-use trail.

### **1.3 Project Purpose and Need**

The proposed multi-use trail along SR 60 from Bayshore Boulevard to west of the Ben T. Davis Beach entrance would accommodate recreational users that can experience the scenic qualities of the Causeway, further enhancing tourism and economic development. The proposed Courtney Campbell Causeway Multi-Use Trail has been identified in the Comprehensive Plans of the following jurisdictions: Hillsborough County; Pinellas County; City of Tampa; and the City of Clearwater.

The trail has also been identified in the City of Tampa *Greenways & Trails Master Plan* (2001), the City of Clearwater *Bikeways and Trails Plan* (1996) and *Shifting Gears: Clearwater's Bicycle and Pedestrian Master Plan* (2007). Design and construction for this project are currently funded in the FDOT's *Tentative Work Program 2012-2016. Transportation Improvement Program (TIP)* amendments are currently being processed to facilitate this segment. The proposed trail will serve as a link in a regional network of trail systems serving the Tampa Bay region (**Figure 1-5**). As a needed west-east link, the trail will provide regional connectivity with the trail networks for the jurisdictions noted above. In providing the west-east link, regional connectivity could be further enhanced offering alternative modes of transportation in the region. The west end of the proposed trail would connect to Clearwater's proposed Bayshore Boulevard Trail, which in turn would connect to numerous other trails in Pinellas County.

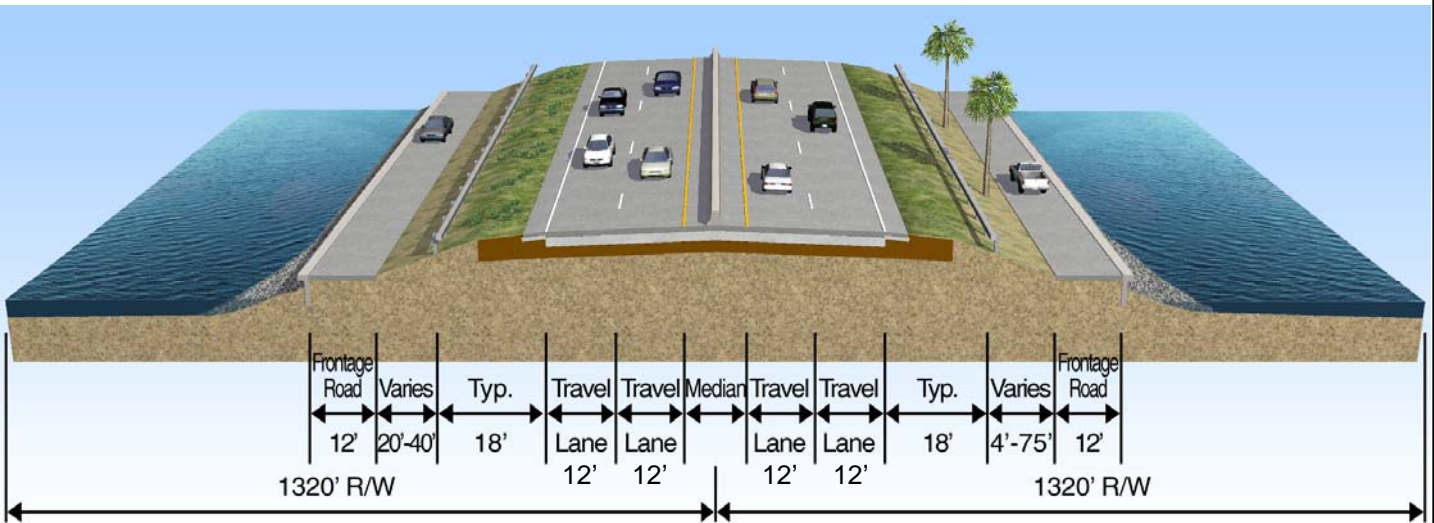
### Existing Typical Section No. 1



(Applies from Begin Project to Structure No. 1\*)

\*Note: Frontage Road and seawall shown in Existing Typical Section 2 below begins at sta 85+00 in eastbound direction, but is no longer accessible to general public

### Existing Typical Section No. 2



(Applies from Structure No. 1\* to End Project)

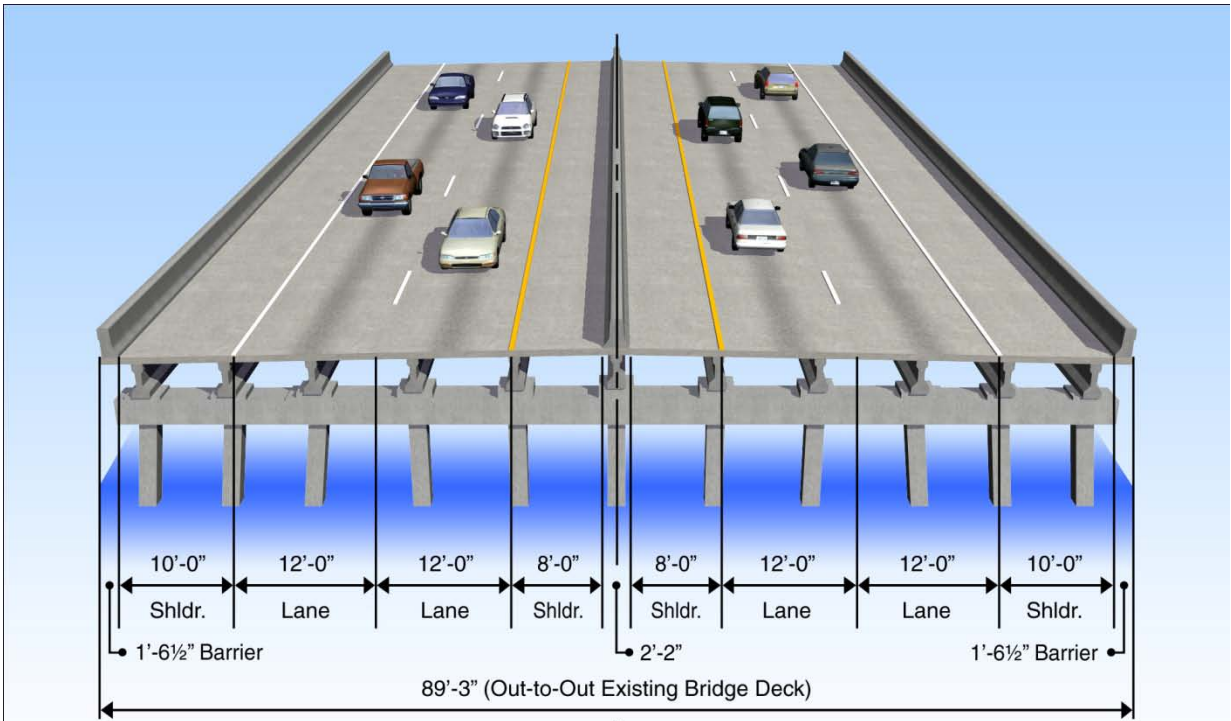


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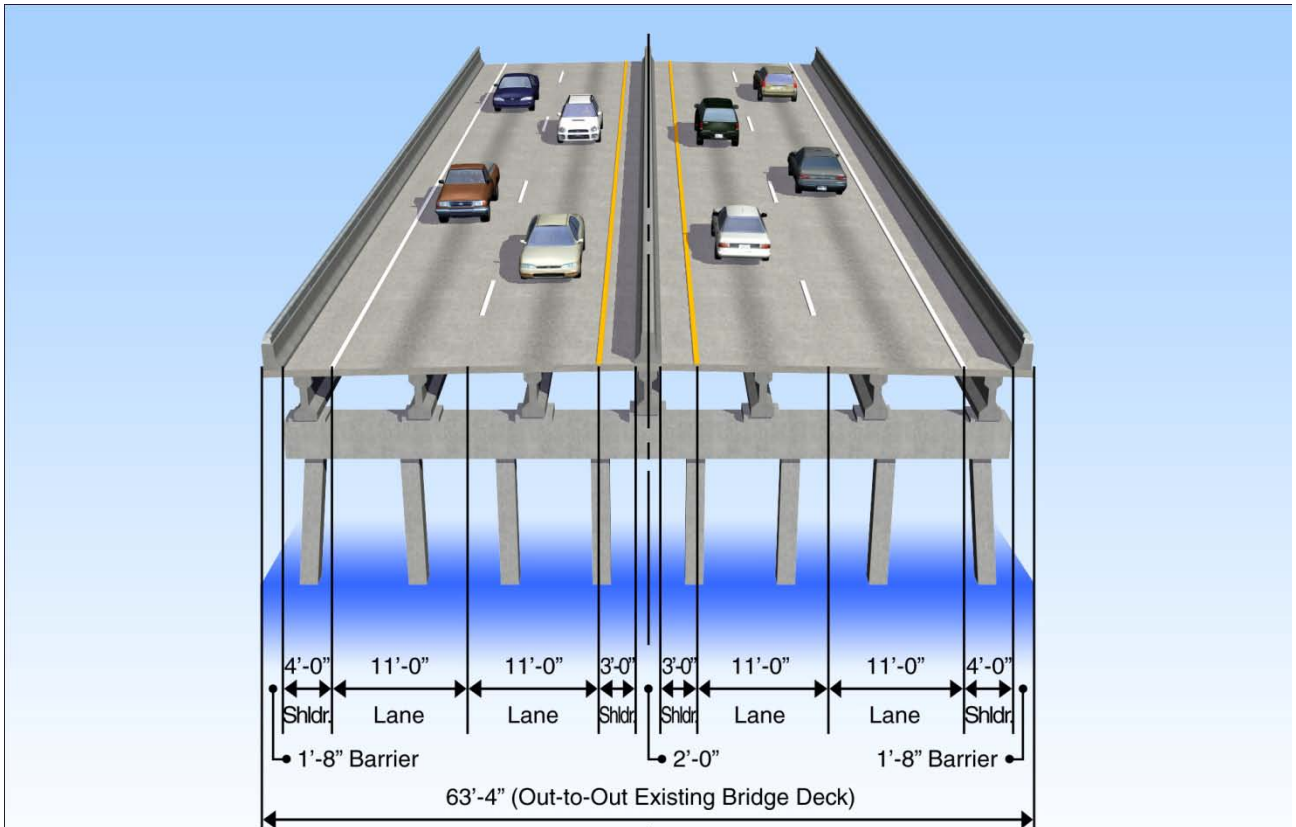
Existing Roadway  
 Typical Sections  
 (facing east)

Figure 1-3





**Structure No. 1 (#150138)**



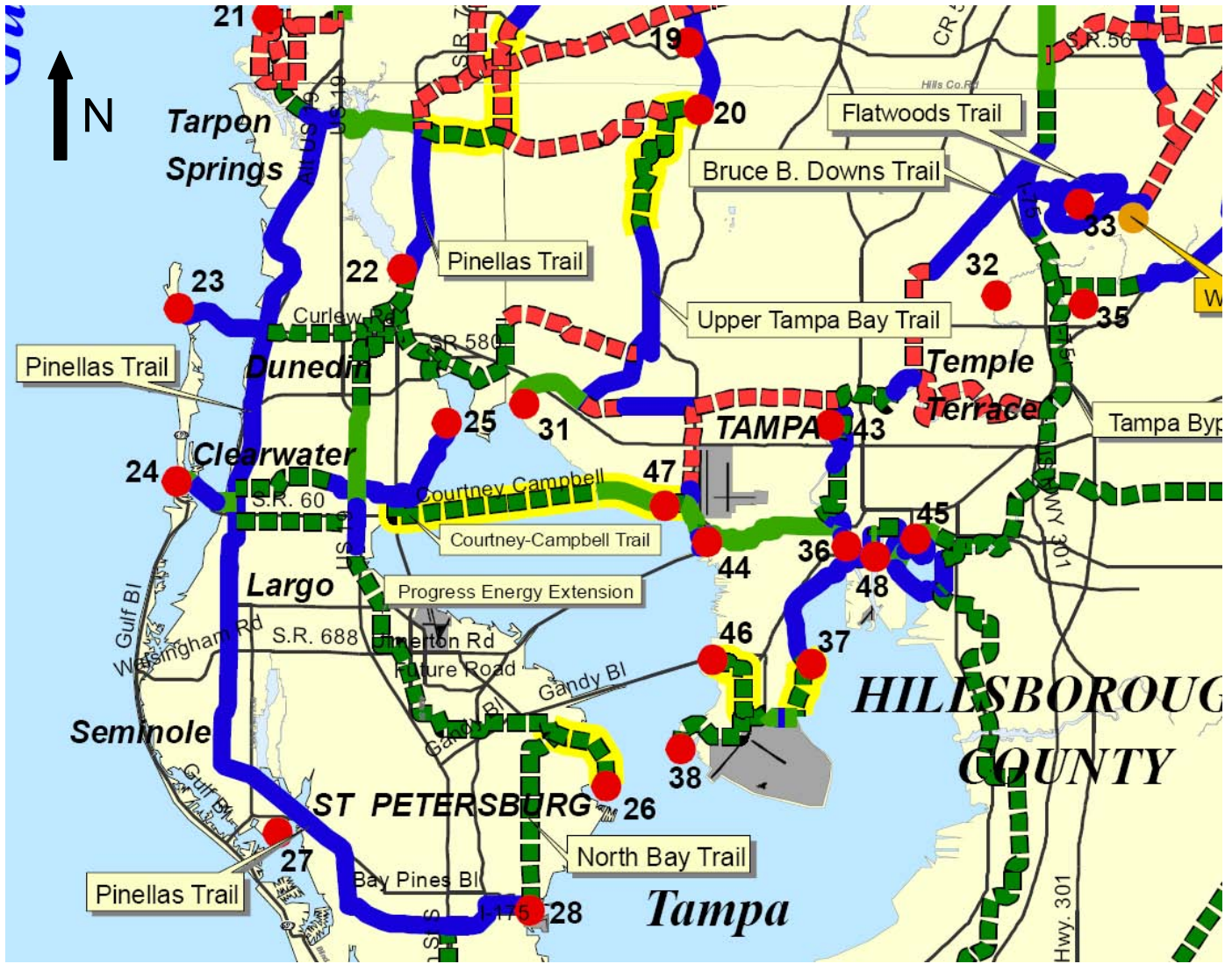
**Structure No. 2 (#100301)**



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**Existing Bridge  
 Typical Sections  
 (facing east)**

**Figure 1-4**



**Legend**

- Regional Attractors & Trailheads
- Hiking/Bicycle Trail (Alignment Not Shown)

**Regional Multi-Use Trails**

- Existing Trail
- Planned - Funded Trail
- - - Planned - Unfunded/Partially Funded Trail
- - - Conceptual Trail
- Regional Priorities
- Regional Roadway Network

Note - the Regional Attractor and Trailhead number refers to the table in the Regional Multi-Use Trail Element

West Central Florida MPO  
Chairs Coordinating Committee



DATA SOURCES: Regional road base layer was created by compiling each county's major road network. Contact respective MPO for specific information.

ACCURACY: It is intended that the accuracy of the base map comply with US national map accuracy standards. However, such accuracy is not guaranteed. The map is for illustrative purposes only.

Prepared Date: April 14, 2010  
Prepared By: Hernando County MPO



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**Excerpt From 2010 Regional  
Multi-Use Trails Map**

**Figure 1-5**



The east end of the proposed trail would eventually connect to the U-Path Trail (**Figure 1-6**) and eventually to additional trails in Hillsborough County. Beyond the trail's transportation benefits, the trail could serve the recreational needs for residents in the area and provide linkage to a series of recreational facilities along the Causeway. It would also recreate a regional recreational opportunity to cross Tampa Bay to link Pinellas and Hillsborough Counties since the existing east-west Friendship Trail Bridge corridor along Gandy Boulevard is no longer available to users within the Tampa Bay area. The Friendship Trail Bridge is permanently closed to users since it is no longer safe to be used and is expected to be demolished once sufficient funds are available to the operating entities for the structure's demolition.

#### **1.4 Report Purpose**

In compliance with state and federal rules, regulations, and policies, a PIP was developed in December 2010 and carried out as an integral part of the SR 60 PD&E Study. Public involvement was accomplished during the PD&E Study to keep appropriate agencies, public officials, property owners, and interested citizens informed and to ensure project compliance with local and regional transportation plans. The FDOT has conducted an interagency coordination and consultation effort, and public participation process. This *Comments and Coordination Report* is one of several documents that has been prepared as part of this PD&E Study. This report documents the PIP, agency coordination efforts, public involvement activities, and comments received.



U-Path Trail - Completed July 1, 2010

Connects Cypress Point Park, Courtney Campbell Causeway and Skyway Park

Source: City of Tampa, Parks & Recreation Department, September 2010



SR 60 Multi-Use Trail PD&E Study

*Bayshore Blvd to West of Ben T. Davis Beach Entrance*

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U-Path Trail

Figure 1-6

## Section 2 - IMPROVEMENT ALTERNATIVES

A traffic analysis of the study area was performed during the Feasibility Study. SR 60 along the Courtney Campbell Causeway is a four-lane, divided highway. Average Annual Daily Traffic (AADT) was approximately 50,500 vehicles per day in 2009 according to the FDOT's DVD-ROM *Florida Traffic Information*. The traffic analysis assumes that no changes will be made to the roadway and that traffic volumes as projected through 2016 are relatively flat for SR 60 across the Causeway. AADT volumes are projected to rise between 1-2% on either end and remain constant over the causeway section.

SR 60 performs at a motor vehicle Level of Service (LOS) "D" for an urbanized, four-lane, divided state highway with interrupted flow bases on current traffic volumes. Because the roadway includes paved shoulders along most of its length, there is generally room for bicyclists in the existing cross-section. Given the traffic characteristics and the roadway geometry, this leads to an existing bicycle LOS "D" based on the FDOT-adopted *Bicycle Level of Service Model*. Pedestrian are not currently accommodated along the roadway, and the *Pedestrian Level of Service Model* indicates a pedestrian LOS "F".

### 2.1 No-Build Alternative

The No-Build Alternative assumed that, other than those improvements already planned and funded, the existing conditions would remain for SR 60 within the project limits and only routine maintenance activities would occur. The advantages to the No-Build Alternative include no new costs for design and construction, no effects to natural resources, and no disruption to the public during construction. However, the No-Build Alternative will not meet the goals of the Comprehensive Plans of Hillsborough and Pinellas Counties and the Cities of Tampa and Clearwater for constructing the SR 60 Multi-Use Trail across Old Tampa Bay, and will not provide alternate modes of transportation on SR 60 for a roadway that is currently at capacity. Furthermore, the No-Build Alternative will not provide the only link in the regional trail network for the Tampa Bay Region and will not meet the stated goals and objectives of this study. The No-Build Alternative will remain under consideration as a viable alternative throughout the PD&E study process.

### 2.2 Build Alternatives

The previous 2008 Feasibility Study included the evaluation of four separate build alternatives and one staging option.

## **2.3 Recommended Build Alternatives**

### **2.3.1 Typical Sections and Trail Concepts**

Recommended trail typical sections are shown in **Figures 2-1A** and **2-1B**. These are generally consistent with the typical sections shown in the Feasibility Study. Trails are shown on the south side only, constructed on the existing SR 60 causeway fill section, although the alternatives studied previously considered a trail on the north side as well; the north side alternatives are essentially a mirror image of the south side trail alternatives. At all locations, due to close proximity of the proposed trail to the existing seawall and vertical drop-off, hand rail is proposed.

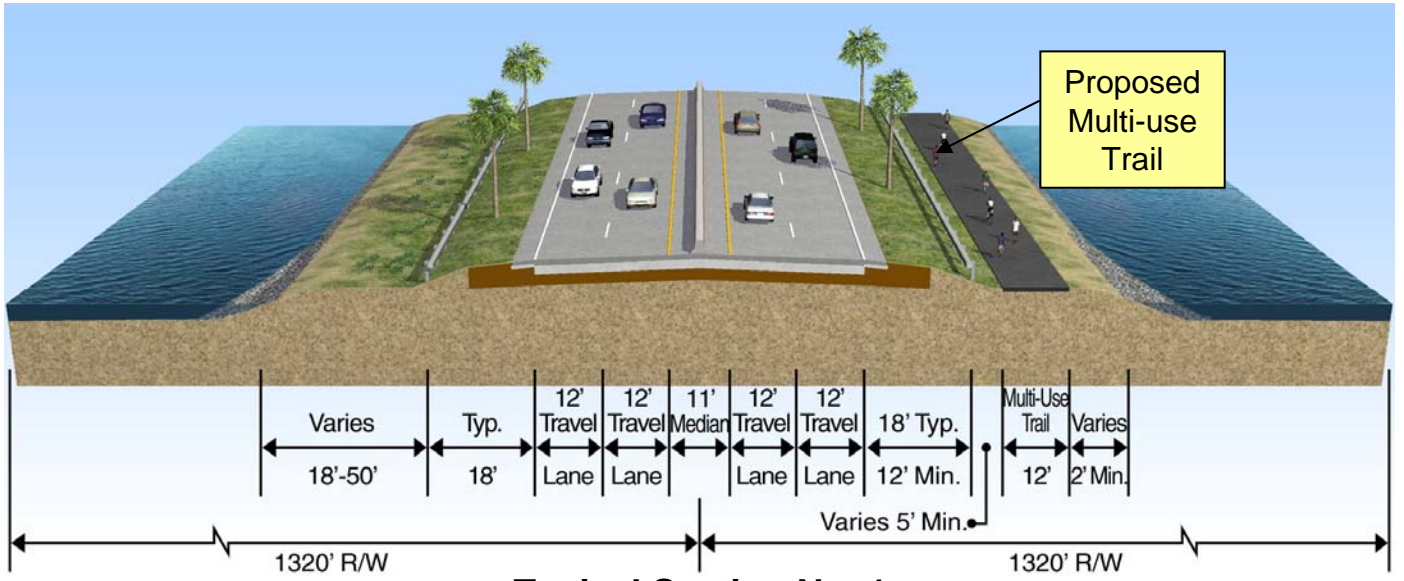
**Typical Section #1** - west portion of study area (approx. Sta 21+00 to 69+00)

This typical section proposes the trail along the south side of SR 60 between the existing guardrail and beach area. The existing guardrail may need to be relocated from the existing 18 foot offset to a minimum offset of 12 feet from the eastbound edge of the travel lane to the face of the guardrail to accommodate the proposed trail typical section. Where the offset between the back of the steel guardrail posts and the trail is less than or equal to 4 feet, a pipe rail will be attached to the back of the steel guardrail posts. A minimum 4 foot separation from the back of the guardrail posts to the inner edge of the trail is preferred. A 2 foot minimum graded separation from the outside edge of the trail to the beach is preferred. This typical extends from Bayshore Boulevard to approximately 4,800 feet to the east.

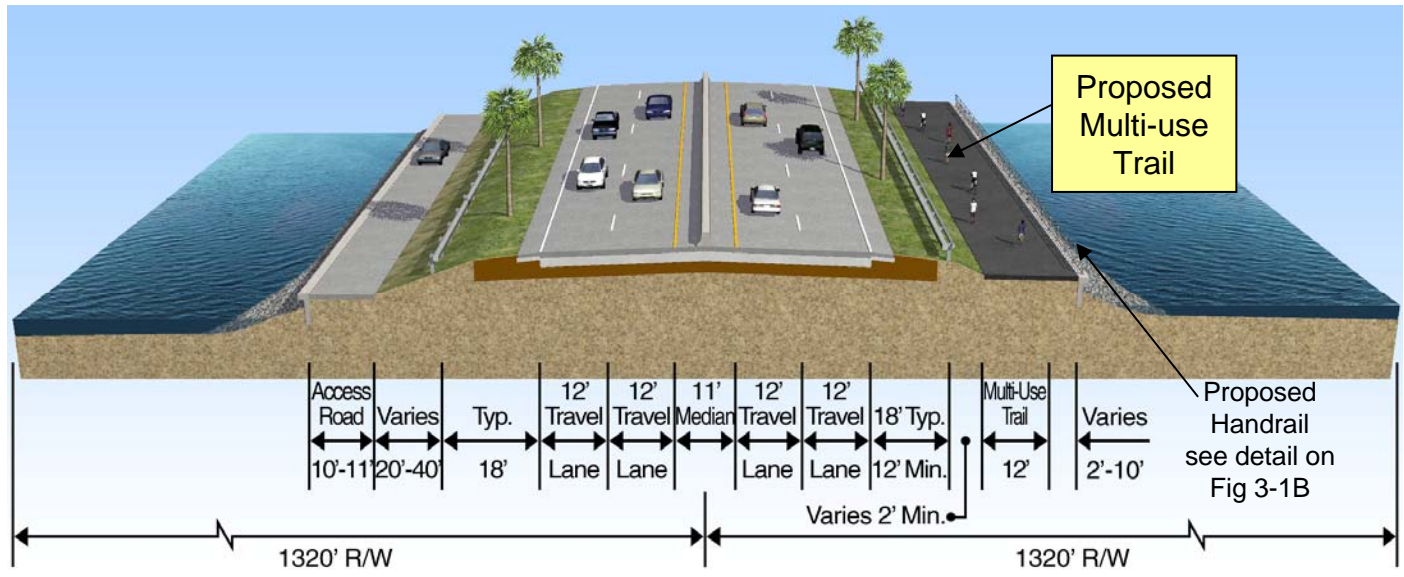
**Typical Section #2** - from approx. Sta 69+00 to 106+00, 111+00 to 256+50 and 394+00 to 412+00

This typical section proposes the trail along the south side of SR 60 between the existing guardrail and sea wall. The existing access road will no longer exist for this section and the proposed trail, instead will be situated in place of the access road. The pavement will be resurfaced and restriped for the trail. The existing guardrail may be relocated from the existing 18 foot offset to a minimum offset of 12 feet from the edge of the travel lane to the face of the guardrail to accommodate the proposed typical section. Where the offset between the back of the steel guardrail posts and the trail is less than or equal to 4 feet a pipe rail will be attached to the back of the steel guardrail posts. A minimum 2 foot separation from the back of the guardrail posts to the inner edge of the trail is required. A 5 foot minimum separation from the outside edge of the trail to the outer edge of the sea wall is preferred. This typical section is used at three locations for an approximate length of 20,050 feet.





**Typical Section No. 1**  
 Station 21+00 to Station 69+00



**Typical Section No. 2**  
 Station 69+00 to Station 106+00 / Station 111+00 to Station 256+50  
 Station 394+00 to Station 412+00

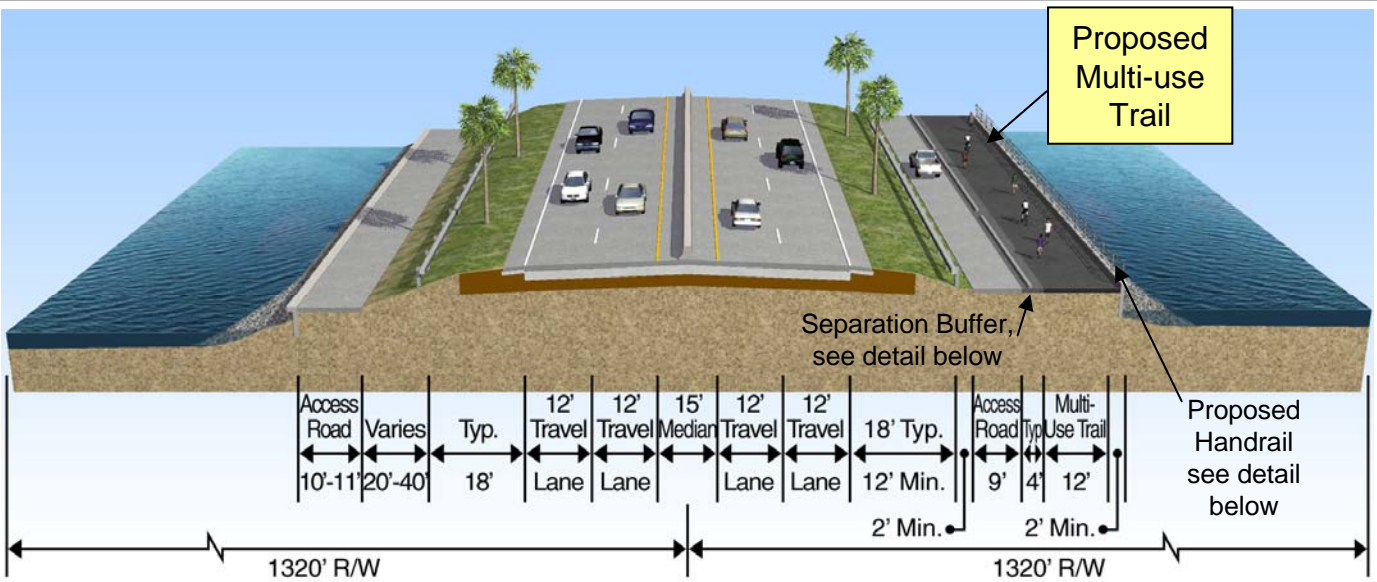


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**Proposed Roadway/Trail  
 Typical Sections  
 (facing east)**

**Figure 2-1A**

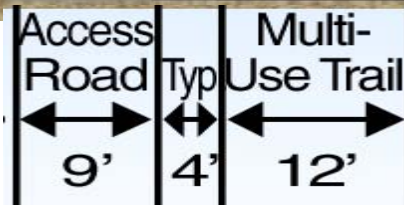
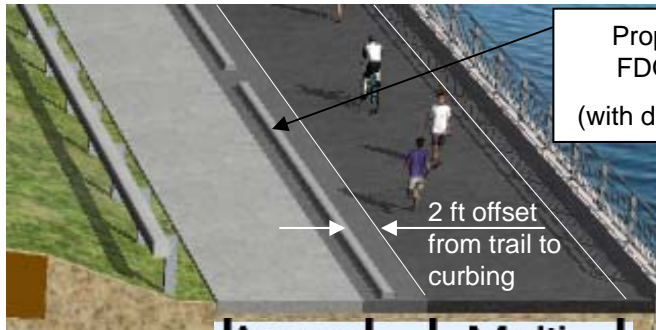




### Typical Section No. 3

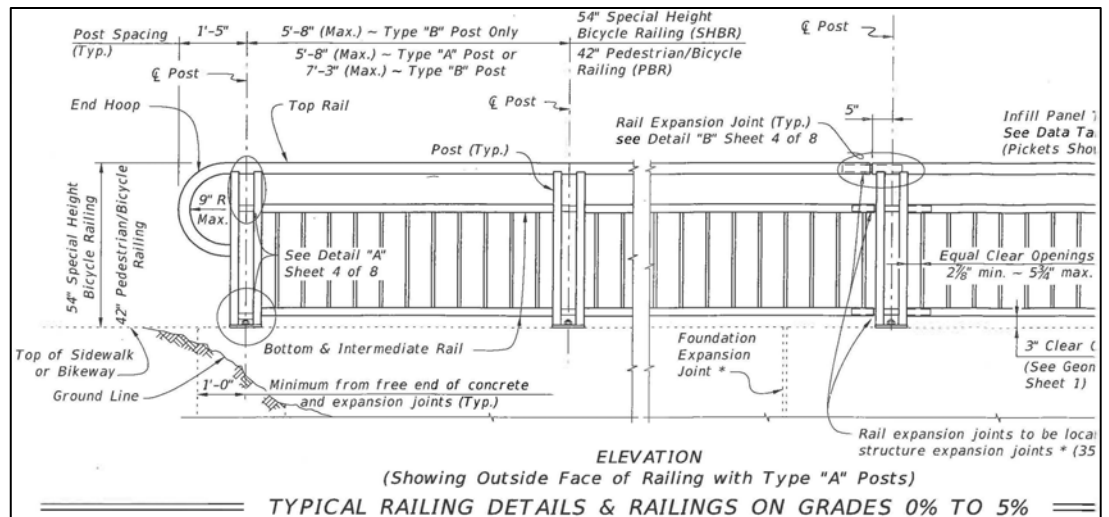
Station 256+50 to Station 265+00 / Station 300+00 to Station 394+00

### Access Road / Trail Separation Buffer Detail



### Proposed Handrail Detail

per FDOT Interim Standard Index 862 – Type 1 Picket Railing (sheet 2 of 8)

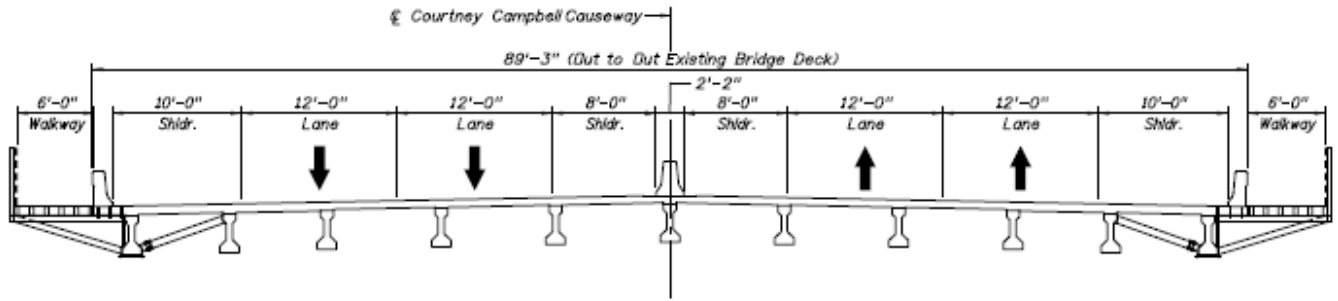


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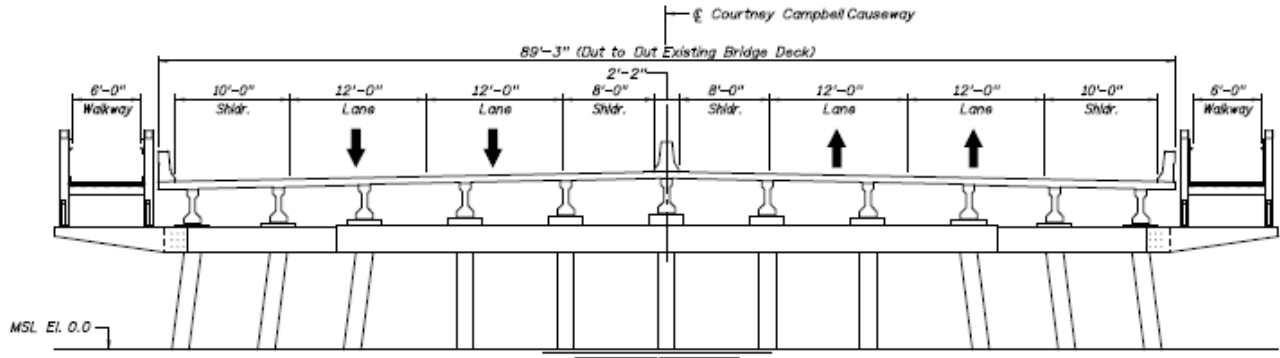
Proposed Roadway/Trail  
 Typical Sections  
 (facing east)

Figure 2-1B

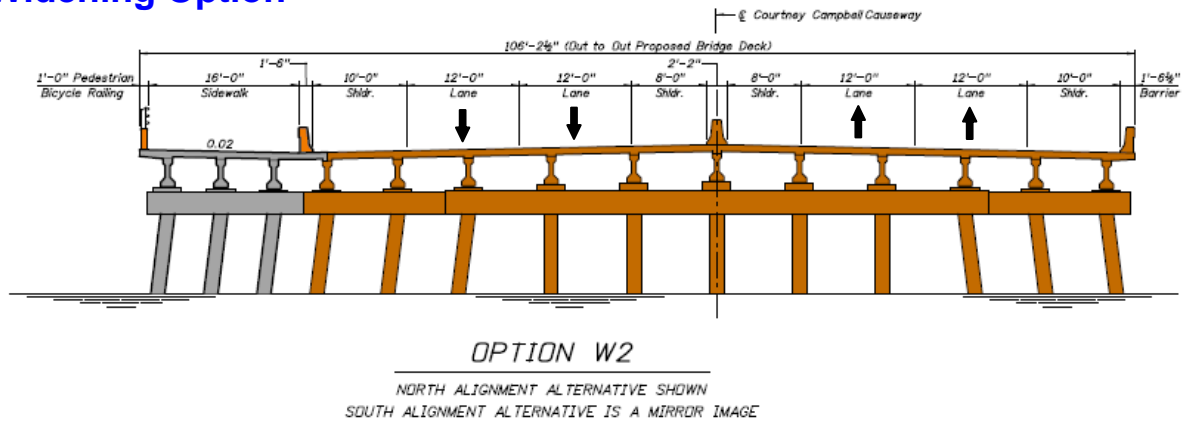
## Cantilevered Walkways Option



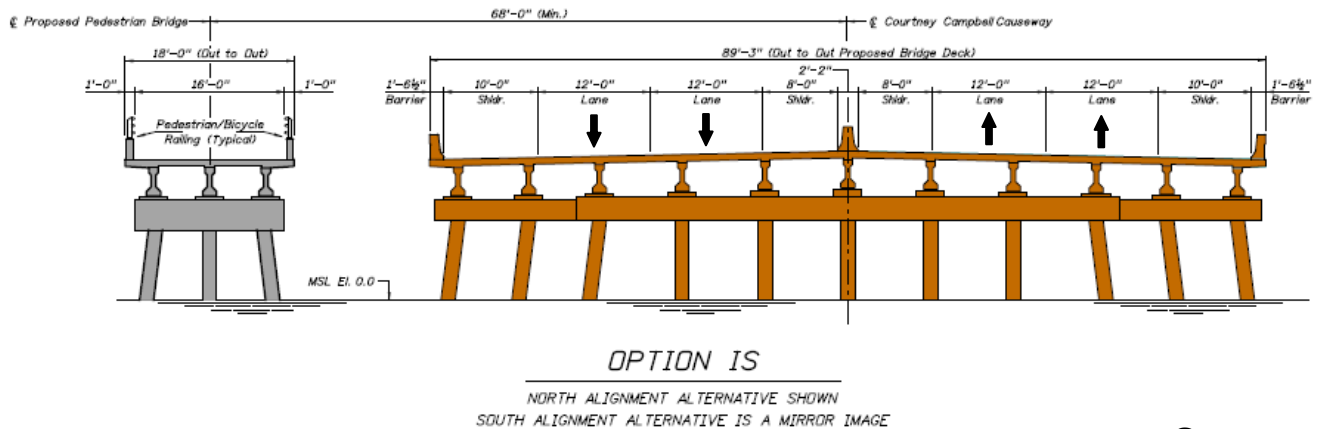
## Cantilevered Prefab Truss Option



## Bridge Widening Option



## Independent Structure Option



Structure 1

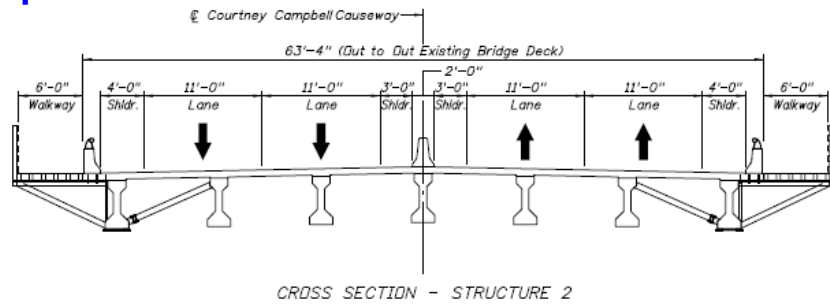


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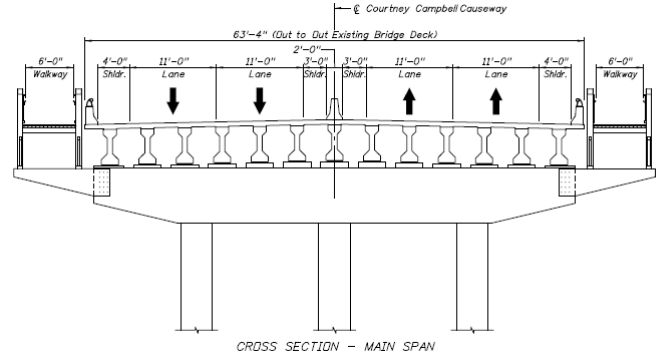
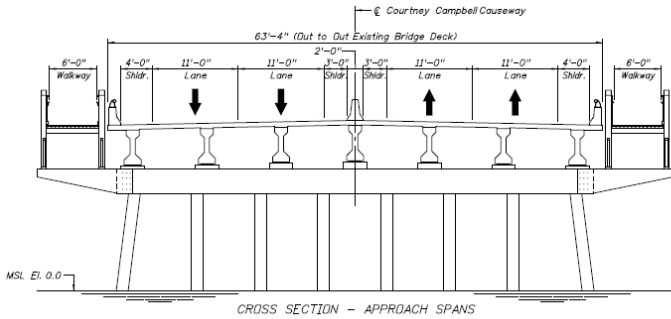
Bridge Alternatives Considered  
 in the Feasibility Study

Figure 2-2A

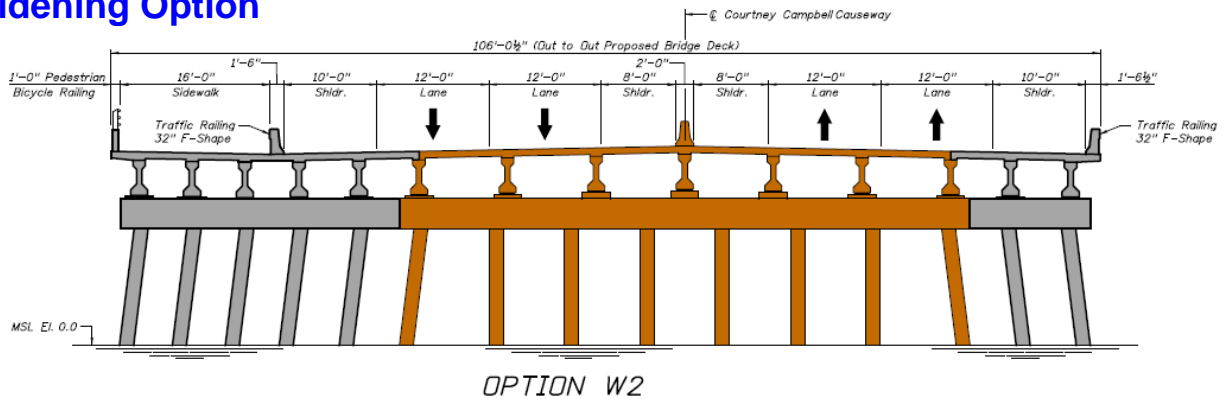
## Cantilevered Walkways Option



## Cantilevered Prefab Truss Option

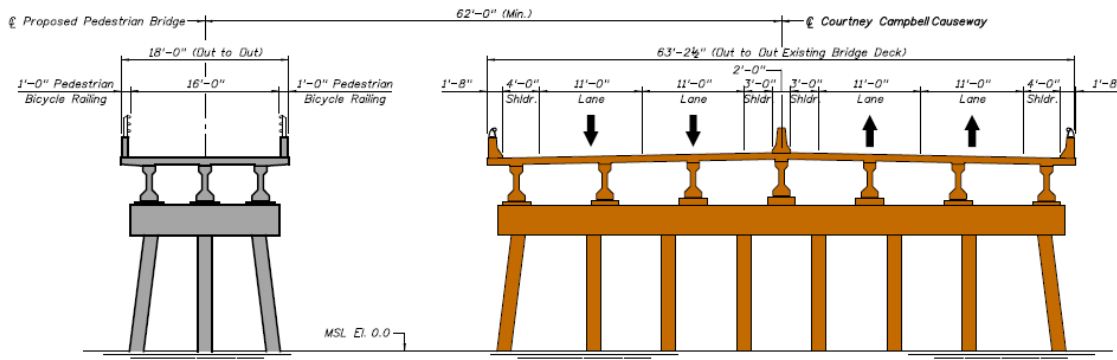


## Bridge Widening Option



NORTH ALIGNMENT ALTERNATIVE SHOWN  
SOUTH ALIGNMENT ALTERNATIVE IS A MIRROR IMAGE

## Independent Structure Option



NORTH ALIGNMENT ALTERNATIVE SHOWN  
SOUTH ALIGNMENT ALTERNATIVE IS A MIRROR IMAGE

Structure 2



SR 60 Multi-Use Trail PD&E Study  
Bayshore Blvd to west of the Ben T. Davis Beach entrance  
WPI Segment No.: 422640 2  
Pinellas & Hillsborough Counties

Bridge Alternatives Considered  
in the Feasibility Study

Figure 2-2B

**Typical Section #3** - from approx. Sta 256+50 to 265+00, 300+00 to 394+00

This typical section proposes an 11 foot frontage road, 5 foot buffer separation (with cable barrier) and a 12 foot multi-use trail along the south side of SR 60 between the existing guardrail and sea wall. The existing guardrail may be relocated from the existing 18 foot offset to a minimum offset of 12 feet from the edge of the travel lane to the face of the guardrail to accommodate the proposed typical section. A minimum 2 foot separation from the back of the guardrail posts to the inner edge of the frontage road is required. A 5 foot minimum separation from the outside edge of the trail to the outer edge of the sea wall is preferred. This typical is used at two locations for an approximate length of 10,250 feet.

According to the Feasibility Study Report, the majority of existing access road pavement that could be incorporated into the proposed trail is located on both sides of the Causeway directly adjacent to the existing revetment system and seawall. This existing surface of the access road was installed between 1978 and 1980 as a part of a revetment project and was not intended to be utilized as a driving surface but instead as part of the permanent erosion control system. The original pavement section of 6-inch soil cement base with a modified surface treatment was resurfaced in 1998. Based on a visual inspection this pavement seems to be performing well; however, additional resurfacing would be needed in order to remove longitudinal undulations and non-Americans with Disabilities Act of 1990 (ADA) compliant cross slopes. Since the existing pavement is performing well under current vehicular loads, trail maintenance vehicles would not pose any problems with the current structure with the added structural enhancement from the resurfacing.

The existing service access road is proposed to be removed from the south side of the causeway at several locations in order to construct the trail and avoid relocating the existing seawall. Maintenance vehicles can utilize the trail or unpaved areas adjacent to the proposed trail to access the causeway areas required to maintain the seawall. The preliminary concept plans depict these locations. The access road pavement will no longer exist from stations 111+00 to 256+00 and 395+00 to 412+00. Accordingly, entry points for the south access road along SR 60 will be closed at approximately station 137+00, 225+00 and 412+00. A new SR 60 entrance is proposed at approximately station 256+00. An additional access point may optionally be closed at approximately station 362+00. This option to keep the access point open at station 362+00 will be presented at the public hearing for public input. No changes will be made to the existing access road or entry points to it on the north side of the causeway.

### **2.3.2 Bridge Alternatives**

The proposed multi-use trail will require bridge crossings over Old Tampa Bay at two locations (within the PD&E study limits) for a continuous pathway. Structures 1 and 2 in their current configuration do not have sufficient deck width to accommodate the required

trail width. These structures would need to be widened or a parallel structure built to provide a multi-use trail.

Two separate alternatives for widening the existing bridges were studied in the Feasibility Study. The first method involved several options for attaching cantilevered structural components to the existing bridges which would not require the driving of additional support piles. Structural analyses of these alternatives showed that these methods were not structurally feasible. The second method of widening involved driving additional support piles alongside the existing bridges. This method is more costly but is structurally viable. The third bridge alternative consisted of constructing independent bridge structures for the trail parallel to the existing highway bridges on the Causeway. These 3 methods are illustrated in **Figure 2-2** for Structure No. 1 only, as an example.

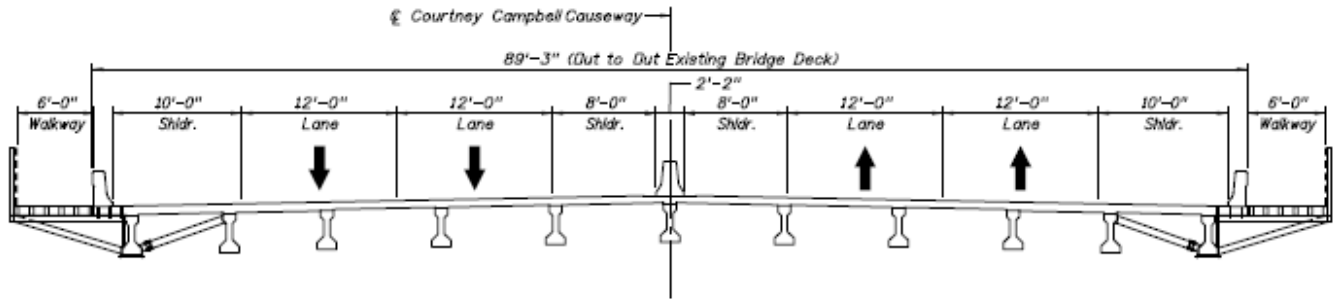
**Structure No. 1** - The existing bridge (Bridge No. 150138) is a prestressed concrete girder facility that was originally built in 1974 and widened in 1992. This bridge is located from MP 7.543 to MP 7.633 in Pinellas County. The superstructure consists of an 89'-3" wide reinforced concrete deck cast over 11 - 43'-0" spans. The deck slab is cast continuously in two separate units. The prestressed concrete girders are American Association of State Highway and Transportation Officials (AASHTO) Type II. The substructure consists of pile bents utilizing 18" square prestressed concrete piles. Joints depend on a compression type seal. The current structure has a vertical clearance of 10.70' above the mean high water elevation and a horizontal clearance of 40'. According to a structural inventory and appraisal performed in March 2010, the existing Causeway Bridge has a structural sufficiency rating of 85.0 percent and was classified as "not deficient, above minimum criteria." The structure has no Load Rating restrictions.

**Structure No. 2** - The existing bridge (Bridge No. 100301) is a prestressed concrete girder facility that was originally built in 1974. This bridge is located from MP 1.758 to MP 2.374 in Hillsborough County. The superstructure consists of a 63'-4" wide reinforced concrete deck cast over 45 spans. There are 12 approach spans on either side of the bridge which are 61'-6" in length and consist of AASHTO Type III girders. The inner spans are made up of ten 83'-6" spans on either side of a 110'-0" navigational span. The inner superstructures consist of Type IV girders. The approach spans are supported on pile bents utilizing 18" (end bents) and 24" (interior bents) square prestressed concrete piles. The 83'-6" inner spans are supported on two column bents grounded on pile footings. The navigational span is supported by three column bents with a 47' x 22' concrete crash walls between the columns. Joints depend on a compression type seal. The navigational span has a vertical clearance of 43.50' above the mean high water elevation and a horizontal clearance of 75.'

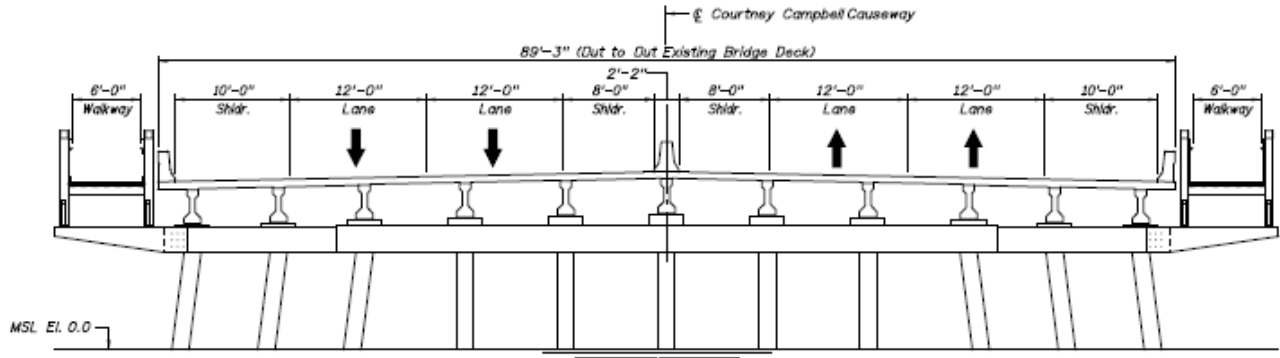
The substructure is protected by a timber fender system. According to a structural inventory and appraisal performed in November 2009, the existing Causeway bridge has a structural sufficiency rating of 70.0 percent and was classified as "not deficient, above minimum tolerable." The structure has no Load Rating restrictions.



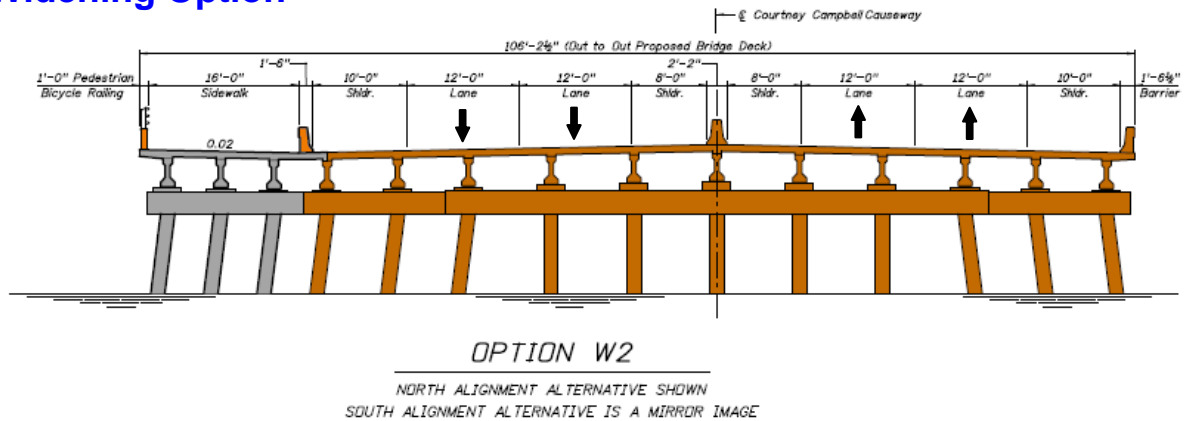
## Cantilevered Walkways Option



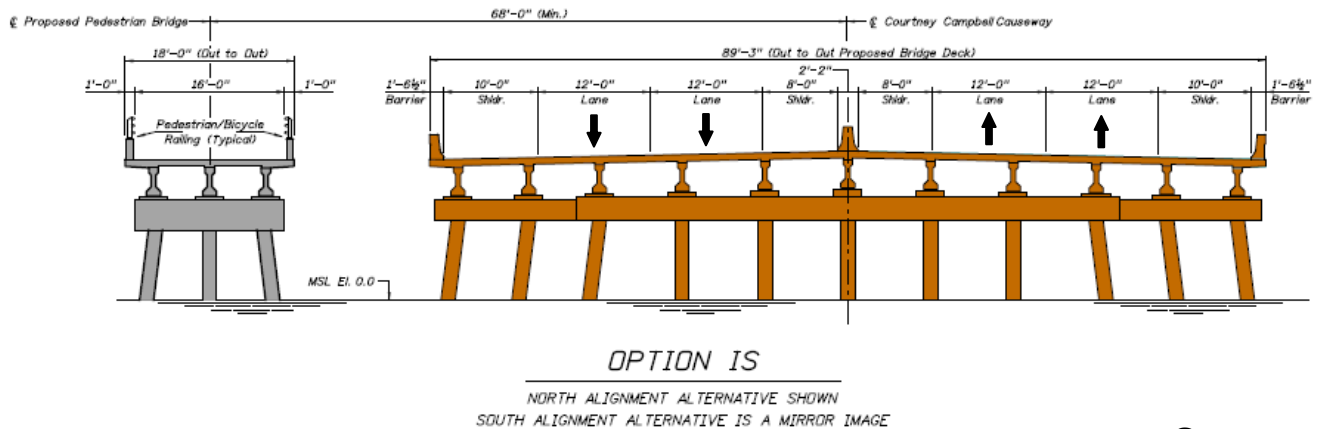
## Cantilevered Prefab Truss Option



## Bridge Widening Option



## Independent Structure Option



Structure 1



SR 60 Multi-Use Trail PD&E Study

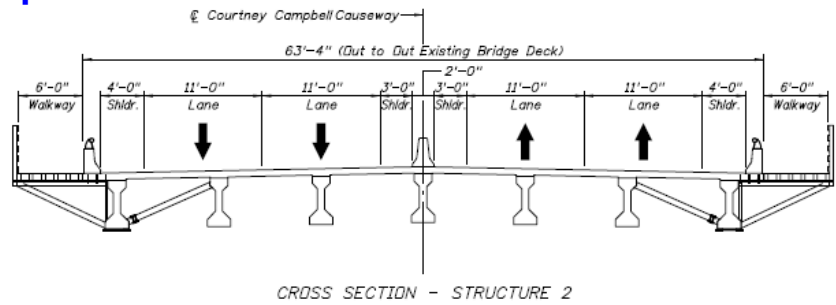
Bayshore Blvd to west of the Ben T. Davis Beach entrance

WPI Segment No.: 422640 2  
Pinellas & Hillsborough Counties

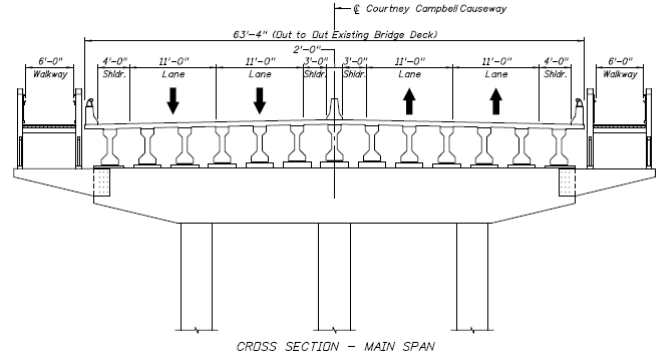
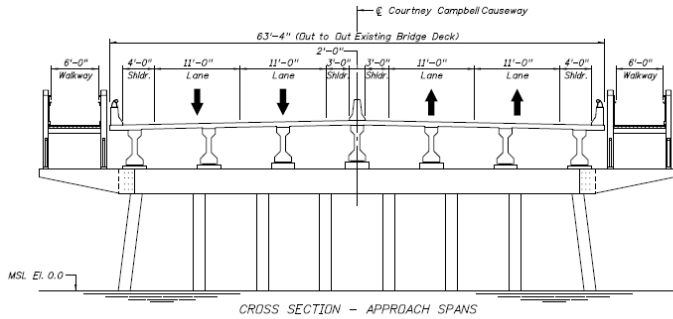
Bridge Alternatives Considered  
in the Feasibility Study

Figure 2-2A

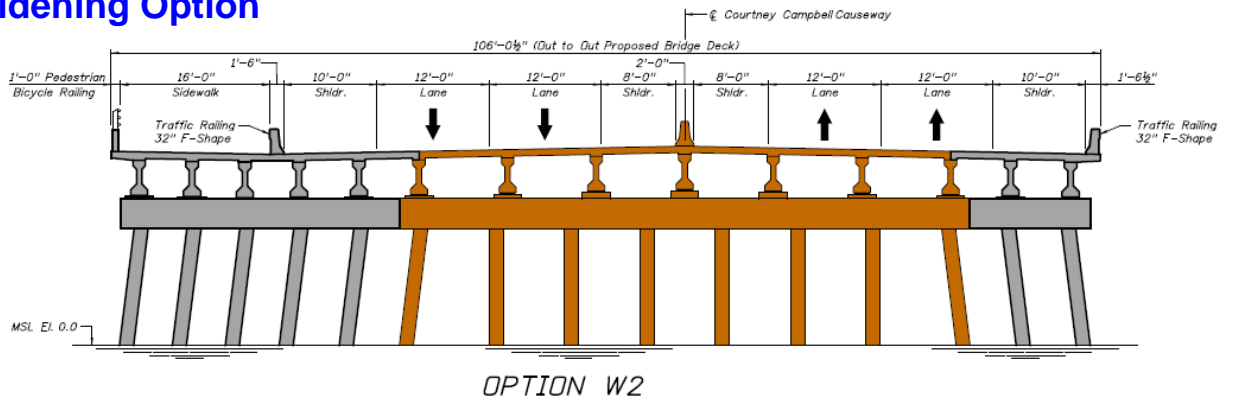
## Cantilevered Walkways Option



## Cantilevered Prefab Truss Option

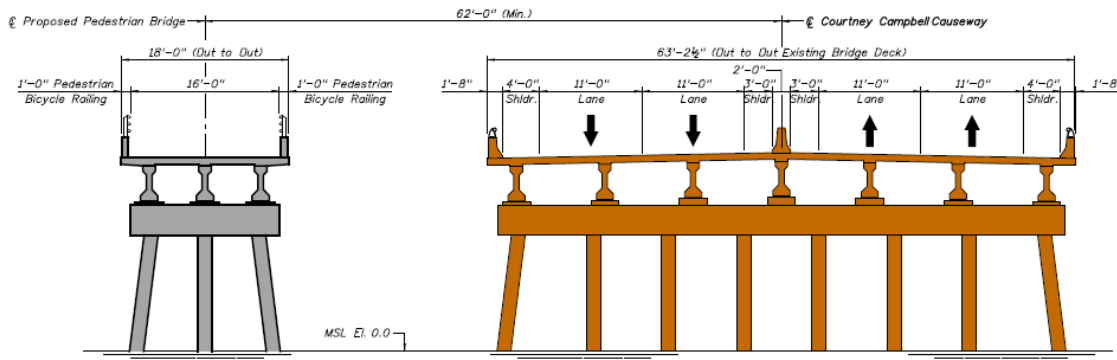


## Bridge Widening Option



NORTH ALIGNMENT ALTERNATIVE SHOWN  
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## Independent Structure Option



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Structure 2



SR 60 Multi-Use Trail PD&E Study  
Bayshore Blvd to west of the  
Ben T. Davis Beach entrance  
WPI Segment No.: 422640 2  
Pinellas & Hillsborough Counties

Bridge Alternatives Considered  
in the Feasibility Study

Figure 2-2B

**Navigational Issues** - The SR 60 bridges cross over the northern regions of Old Tampa Bay. The navigable channel consists of:

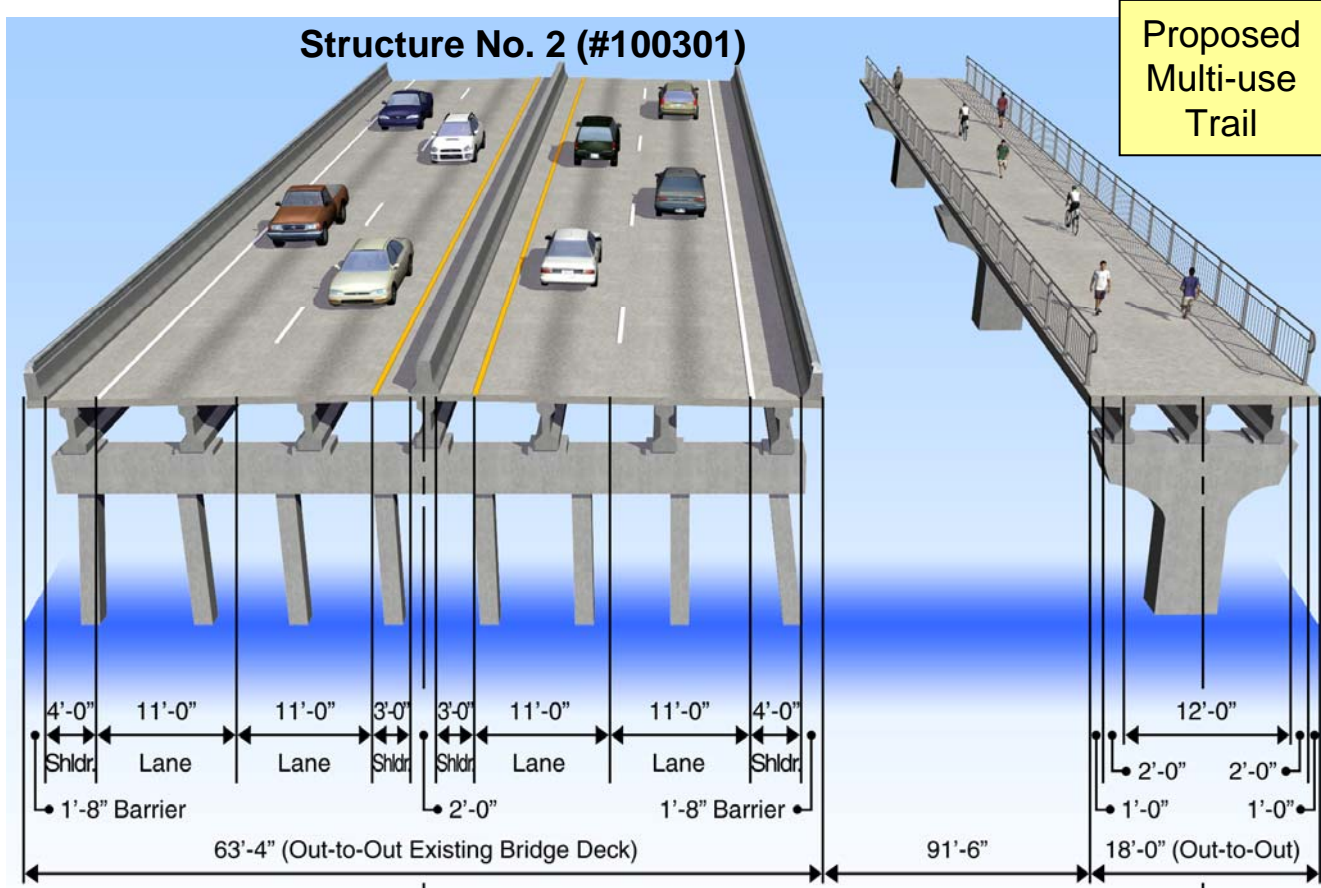
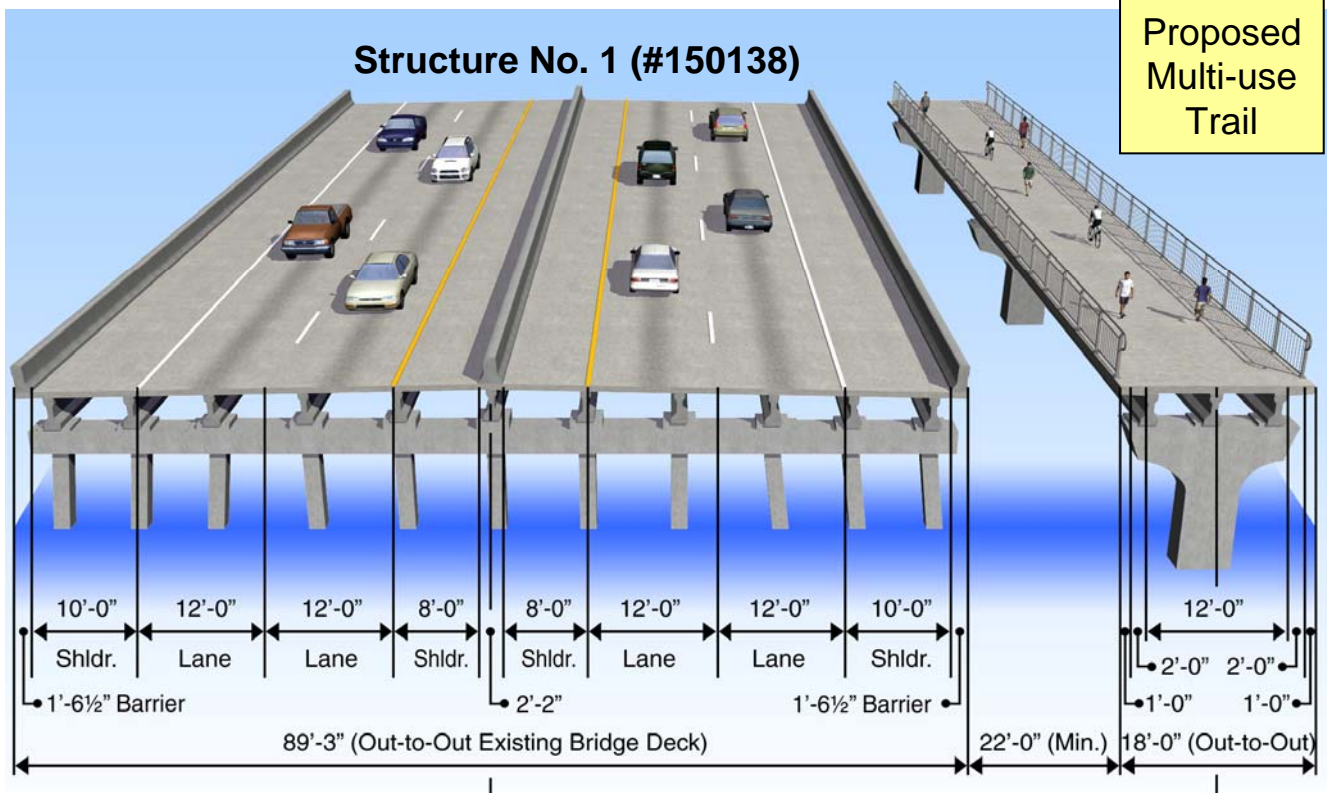
**Structure 1** – The current structure has a vertical clearance of 10.70’ above the mean high water and a horizontal clearance of 40’. Deepest high water depth – 6’ (Based on as-built construction drawings).

**Structure 2** – The current structure has a vertical clearance of 43.50’ above the mean high water elevation and a horizontal clearance of 75’. Vessels are guided thru the channel by a timber fender system at the main span location. Deepest high water depth – 19’ (Based on as-built construction drawings). There have been no significant impacts to the structures on the Causeway since it was constructed.

For the separate trail bridges option, the separate bridges would be designed to accommodate the heaviest required vehicle to perform routine maintenance and inspection. The independent structures option (IS) noted in the Feasibility Study is recommended due to significant cost savings and ease of construction compared to the bridge widening option. The proposed bridges will be built to maintain the existing vertical and horizontal clearances of the existing SR 60 bridges. Also, the proposed span arrangement and substructure elements for the proposed trail bridges are intended to be consistent with the SR 60 roadway bridges and “line-up” to facilitate navigation and tidal flow. The existing tender system under Structure 2 will be extended under the new adjacent trail bridge. The recommended trail bridge typical sections are shown in **Figure 2-3**.

### **2.3.3 Selection of the Recommended Alternative**

The Build Alternative was selected as the Recommended Alternative based on improved connectivity between Pinellas and Hillsborough Counties, enhanced access and pedestrian/bicyclist opportunities for users of the Causeway and Ben T. Davis Beach, and consistency with local government plans.



## Section 3 – Public Involvement Plan

In accordance with Part 1, Chapter 11 of the PD&E Manual, a comprehensive Public Involvement Plan (PIP), approved on February 11, 2011, was developed and implemented for the public involvement program. The purpose of this program was to develop, implement and document the methods that are used to inform and solicit responses from all interested parties including local residents, public officials, agencies and business owners. The PIP helped to identify stakeholders and affected communities and included the following:

- Project background
- Project goals
- Outreach activities, and
- Evaluation of public involvement for the project.

The program included various techniques on how to notify the public of the proposed transportation improvements such as legal display newspaper advertisements, news releases to local media and invitational newsletters. The program included two newsletters; the Public Hearing newsletter and a final newsletter which will be published after the FHWA has issued Location and Design Concept Acceptance (LDCA) for the project. See **Section 7** for more information regarding the project newsletters.

The PIP served as a history and record of commitments made as a result of public involvement activities. These activities included coordination meetings with local officials, an alternatives public workshop, public hearing, unscheduled meetings and presentations (if required) and coordination with adjacent projects. The commitments made through the program included Title VI and Title VII of the Civil Rights Act of 1964, Americans with Disabilities Act (ADA) compliance, SAFETEA-LU compliance and assistance for Limited English Proficiency (LEP) attendees.



## Section 4 – Advance Notification

As part of the FDOT’s ETDM process, this project was evaluated by agencies in the Programming Screen in 2010. Agency comments from the Programming Screen are provided in **Appendix A**. The FDOT continued early project coordination on December 10, 2010 by distribution of an Advance Notification (AN) Package to the Florida State Clearinghouse, Office of the Governor, Tallahassee, Florida, in accordance with Executive Order 83-150. The FDOT received notification that the Clearinghouse received the AN and forwarded the package to the appropriate agencies. **Appendix B** contains a copy of the Advance Notification package.

### 4.1 Agencies That Received Advance Notification

The agencies or government entities listed below received an AN. An asterisk (\*) indicates those agencies that responded to the AN.

#### Federal Agencies

US Department of Transportation – Federal Highway Administration, Division Administrator  
US Department of Transportation, District Transportation Engineer  
US Department of Homeland Security – Federal Emergency Management Agency – Regional Director  
US Department of Transportation -- Federal Aviation Administration – Airports District Office  
US Department of Transportation – Federal Railroad Administration – Office of Economic Analysis, Regional Administrator  
US Department of Health and Human Services – National Center for Environmental Health & Injury Prevention & Control, Director  
US Army Corps of Engineers, Biologist  
US Department of Agriculture – Forest Service, Forest Supervisor  
US Department of Commerce – National Oceanic and Atmospheric Administration, National Marine Fisheries Service, Regional Administrator  
US Department of Commerce – National Oceanic and Atmospheric Administration (NOAA) National Marine Fisheries, Fishery Biologist  
US Environmental Protection Agency – Region IV, Regional Administrator  
US Environmental Protection Agency (EPA), Regional Biosolids Coordinator  
US Department of Interior – US Fish and Wildlife Service, Field Supervisor  
US Department of Interior – Fish & Wildlife Services, Biologist  
US Department of Interior – Bureau of Indian Affairs, Director  
US Department of Interior – Bureau of Land Management, Eastern States Office, Associate State Director  
US Department of Interior – US Geological Survey - Florida Integrated Science Center, Orlando, Director



US Department of Interior – National Park Service – Southeast Regional Office, Acting Regional Director

US Department of Housing and Urban Development – Region IV, Regional Director

US Coast Guard – Commander Office of Aids to Navigation (OAN) – Seventh District, Rear Admiral

#### State Agencies

Florida Department of Transportation – Environmental Management Office, Manager

Florida Department of Transportation – Federal Aid Management, Manager

Florida Department of State – State Historic Preservation Office

Florida Fish and Wildlife Conservation Commission, Exotic Species Lead

Florida Department of Environmental Protection, Environmental Manager

Florida Department of Community Affairs, Senior Planner

#### Regional/Local Agencies

Southwest Florida Water Management District, Executive Director

Southwest Florida Water Management District, District Environmental Technical Advisory Team (ETAT) Representative

Tampa Bay Regional Planning Council, Executive Director

\*Hillsborough County Metropolitan Planning Organization, Executive Director

\*Environmental Protection Commission of Hillsborough County, Executive Director

Tampa-Hillsborough County Expressway Authority, Executive Director

Tampa Port Authority, Port Director/CEO

#### Tribal Officials

Miccosukee Tribe of Indians of Florida, Chairman

Muskogee (Creek) Nation of Oklahoma, Principal Chief

Poarch Band of Creek Indians, Chairman

Seminole Nation of Oklahoma, Principal Chief

Seminole Tribe of Florida, Chairman

Seminole Tribe of Florida, AH-TAH-THI-KI Museum

Mississippi Band of Choctaw Indians, Chairman

## **4.2 Agencies Comments on the Advance Notification**

Agency comments on the AN and responses are provided below. The full comments are provided in **Appendix B**.

The Environmental Protection Commission (EPC) of Hillsborough County provided comments to the AN. The EPC did not object to the PD&E Study and commented that wetlands exist within the project corridor. Wetland impacts would need to be permitted through the EPC pursuant to Chapter 84-446, Laws of Florida. EPC staff recommended that all efforts to avoid or minimize impacts be conducted prior to submittal of project plans. A letter dated January 28, 2011, from EPC can be found in **Appendix B**.

## Section 5 – Coordination Efforts

The FDOT has coordinated with numerous federal and local agencies throughout the study process. This section summarizes the results of these coordination efforts.

### 5.1 Agency Coordination

Throughout the course of the study, coordination was conducted with various state and regional agencies which would be involved with this project whose agreement is required for this project. The following is a list of the federal, state and regional agencies the FDOT coordinated with.

#### 5.1.1 *National Marine Fisheries*

A copy of the Draft WEBAR was sent to National Marine Fisheries Services (NMFS) for review and concurrence on February 25, 2011. A response letter was received on March 9, 2011 (**Appendix B**). The NMFS recommended that a section be included for the smalltooth sawfish, and the NMFS's Sea Turtle and Smalltooth Sawfish Construction Conditions should be implemented as part of the project's commitments. Seagrass surveys would need to be conducted during the prime seagrass growing season (May – September). If seagrass impacts result, appropriate mitigation strategies could be coordinated at that time.

#### 5.1.2 *US Fish & Wildlife Service*

A copy of the Draft Wetland Evaluation and Biological Assessment Report (WEBAR) was sent to U.S. Fish and Wildlife Service (USFWS) for review and concurrence on February 25, 2011. A response letter was received on March 24, 2011 (**Appendix B**). USFWS provided concurrence with degree of effect for sea turtles. Seagrass surveys would need to be conducted during the growing season (June 1 – Sept 30) due to potential impacts to the West Indian manatee. USFWS recommends incorporation of the Construction Special Provisions for the protection of the Gulf Sturgeon. The response letter stated that concurrence for the "No Effect" determination could not be provided if wetland impacts occur. USFWS stated that the Courtney Campbell Causeway is an important area for shorebirds and other migratory birds. The placement of the trail too close to the area where the birds are known to gather may result in flushing and disturbance of shorebirds. USFWS requested that distances between the proposed trail and the known shorebird roosting, feeding, and loafing area be provided.

### **5.1.3 Florida Department of State Historic Preservation Office**

The Cultural Resource Assessment Survey (CRAS) was reviewed by the State Historic Preservation Office in March 2011. The State Historic Preservation Office (SHPO) originally requested additional information regarding the presence of submerged cultural resources within the project area. Numerous meetings were conducted between SHPO and FDOT. A letter was received by SHPO on April 27, 2011, recommending that the project would have no effect on properties listed or eligible for listing on the National Register of Historic Places (NRHP). This letter is attached in **Appendix B**.

## **5.2 Local Government Coordination**

### **5.2.1 Hillsborough County Metropolitan Planning Organization (MPO)**

The project was presented to the MPO on the following dates listed below, to discuss the study process and proposed recommended improvements. Members were shown a condensed and advanced version of the public hearing PowerPoint presentation. Committee member questions and comments involved the proposed height of the bridge structures relative to storm surge heights, and whether parking would be provided along the service road. General project support was conveyed, though no formal motions were raised. The Hillsborough County MPO also provided a letter in response to the AN on January 28, 2011. This letter can be found in **Appendix B**.

- February 21, 2011 – Technical Advisory Committee
- March 9, 2011 – Bicycle/Pedestrian Advisory Committee
- March 16, 2011 – Citizens Advisory Committee
- March 23, 2011 – Livable Roadways Committee
- April 5, 2011 – MPO Board

### **5.2.2 Pinellas County Metropolitan Planning Organization**

The project was presented to the MPO on the following dates to discuss the study process and proposed recommended improvements. Members were shown a condensed and advanced version of the public hearing PowerPoint presentation. General project support was conveyed, though no formal motions were raised.

- February 23, 2011 – Technical Coordinating Committee
- February 24, 2011 – Citizens Advisory Committee
- February 28, 2011 – Bicycle Advisory Committee

- March 9, 2011 – MPO Board
- March 21, 2011 – Pedestrian Transportation Advisory Committee

### ***5.2.3 West Central Florida MPOs Chairs Coordinating Committee (CCC)***

The project was presented to the Joint MPO Citizens Advisory Committee (Pinellas, Hillsborough, Pasco, Hernando, Citrus, Polk and Sarasota/Manatee Counties) on March 1, 2011 and the CCC on March 11, 2011, to discuss the study process and proposed recommended improvements. Members were shown a condensed and advanced version of the public hearing PowerPoint presentation. General project support was conveyed by the CCC, though no formal motions were raised.

## **5.3 Miscellaneous Coordination with Local Groups**

Throughout the course of the study, coordination was conducted with various local or community groups which would be involved with this project. The following is a list of local or community groups with which the FDOT coordinated.

### ***5.3.1 Courtney Campbell Causeway Scenic Highway Committee***

February 9, 2011 - The project was presented to the representatives of the Scenic Highway Committee outside a normal committee meeting to discuss the study process and technical elements as the study team developing proposed improvements. A roll-plot with improvements under consideration was shown and input was provided for FDOT consideration. Items discussed included proposed handrail along the seawall (suggested a photo rendering from passenger's perspective inside a car on SR 60 be developed), positive separation between service road and trail, considerations for eliminating access points to the service road off SR 60, how some motorists have been seen using the service road as a detour around traffic congestion. General project support was conveyed by the committee members present.

### ***5.3.2 Westshore Alliance Transportation Committee***

March 16, 2011 - The project was presented to the Westshore Alliance Transportation Committee to discuss the study process and proposed recommended improvements. Members were shown a condensed and advanced version of the public hearing PowerPoint presentation. General project support was conveyed by the committee, though no formal motions were raised. The Westshore Alliance also provided a letter of support at the public hearing on March 29, 2011. The letter is included with the public hearing transcript in **Appendix D**.

### **5.3.3 Tampa Bay Applications Group**

March 10, 2011 - The project was presented to the Tampa Bay Applications Group to present the project to group members. Attendees were shown a condensed and advanced version of the public hearing PowerPoint presentation. Member questions and comments involved parking, use of the proposed trail for 'iron-man' events, and regional connections. General project support was conveyed by the attendees, though no formal motions were raised.

### **5.3.4 City of Tampa Greenways and Trails Citizens Advisory Committee**

The City of Tampa Greenways and Trails Citizens Advisory Committee provided a letter of support at the public hearing on March 24 2011. The letter is included with the public hearing transcript in **Appendix D**.



## Section 6 – Mailing List

A mailing list was developed for this project. The mailing list was updated throughout the duration of the project to maintain the most current names and addresses. The mailing list contained:

- Those whose property lies, in whole or part, within 500 feet on either side of the centerline of each project alternative. Florida Statutes Section 339.155 states property owners within 500 feet of the centerline of each alternative should be notified about the project. However, FDOT decided to extend the mailing list boundaries to 1,000 feet to ensure proper notification and enhance public involvement activities. In addition to these property owners, current tenants were also included on the mailing list. This portion of the mailing list was based on the Hillsborough County Property Appraiser’s database. Effort was also made to include all residents of a subdivision, if a portion of that subdivision’s residents were to be notified due to their proximity to the centerline.
- Elected and appointed public officials.
- Individuals or groups who requested to be placed on the mailing list for this project.
- Public and private groups, organizations, agencies, and businesses and individuals that have an interest in the project.

The property owner mailing list included over 800 owners and tenants. The agency and interested party mailing list contained approximately 130 people.

The mailing list was used to disseminate project information and announce public hearing. Newsletters (see **Section 7**) were mailed to all those on the mailing list.

## Section 7 – Newsletters

Two newsletters were or will be distributed for this project. Newsletters were mailed to those on the project mailing list as noted in **Section 6**. Newsletters were used to announce the public hearing. A copy of the newsletter is provided in **Appendix C**.

A newsletter was distributed in March 2011. The primary purpose of this newsletter was to promote the public hearing and to invite readers to attend the hearing. The newsletter described the PD&E Study process, discussed the project purpose, presented the recommended build alternatives and the typical sections for each study, and provided a project schedule with the next steps in the study. Additionally, the newsletter included contact information on the project including for those needing special assistance or language support. The newsletter also included a comment form that recipients could use to submit comments on the project.

The second newsletter will be published after the FHWA has issued Location and Design Concept Acceptance for the project.

## Section 8 – Public Hearing

A blended (began with an informal open house format integrated with a time-specified formal hearing) public hearing for this project was held from 5:00 p.m. to 7:00 p.m. in two sessions at two locations. The first session was held in Pinellas County at the Clearwater Christian College, 3400 Gulf-To-Bay Boulevard, Building D - Dambach Hall 101, Clearwater, Florida, 33759 on Thursday March 24, 2011. The second session was held in Hillsborough County at the Westin Tampa Bay, 7627 West Courtney Campbell Causeway, BluVu Room, Tampa, Florida 33607 on Tuesday March 29, 2011. The hearing was held to inform citizens about the project details and schedule, and afford them the opportunity to express their views concerning the proposed improvements. The hearing during both sessions consisted of an open house from 5:00 p.m. to 6:00 p.m. and a formal presentation and public comment period beginning at 6:00 p.m. After the public comment period, the open house resumed until 7:00 p.m.

The study's supporting documents were available for public review from March 1 through April 8, 2011 during normal operating hours at the three locations shown in **Table 8-1**.

**Table 8-1 Locations the Study Documents were Available for Public Review**

Location	FDOT District 7	St. Petersburg College Clearwater Campus Library	HCC – Dale Mabry Learning Resource Center
Address	11201 N. McKinley Dr. Tampa, FL 33612	2465 Drew Street Clearwater, FL 33759	4001 West Tampa Bay Boulevard, Tampa, FL 33614

Newsletters announced the public hearing (see **Section 7**). The newsletters sent via electronic mail to public officials and via direct mail to property owners within 1,000 feet of the project, current tenants, agencies, and interested parties. A Legal display advertising the hearing was published in the Tampa Tribune on March 9 and March 16, 2011 and in the TBT\* (Tampa Bay Times) on March 7 and 17, 2011. An advertisement was also placed in the Florida Administrative Weekly on February 25, 2011. Copies of these advertisements can be found in **Appendix D**.

FDOT staff and its consultants were available at the hearing to discuss the project and answer questions. A continuously-running video (PowerPoint presentation) that described the project and the recommended build alternative was shown during the open house portion of the hearing. Display boards available for review consisted of:

- Aerial photographs depicting the concept plans of the recommended alternative
- Courtney Campbell Causeway Proposed Improvements System Map
- Work Program Schedule

- Existing and recommended typical sections
- Evaluation matrix

The formal portion of the hearing sessions each began at 6:00 p.m. Kirk Bogen, Project Development Engineer for the FDOT, District Seven, presided at both sessions of the hearing. The proceedings were recorded by the court reporter that was on hand throughout the evening. Mr. Bogen welcomed the audience, discussed the purpose of the hearing. The next portion of the Hearing was devoted to oral comments.

Attendees were given the opportunity to provide comments in one of four ways:

- Make an oral statement during the formal portion of the hearing
- Make an oral statement to the court reporter during the informal portion of the hearing
- Complete the written comment form and place it in the drop box at the hearing
- Complete written comments and mail the Comment Form to FDOT - District Seven

The number of attendees at the Hearings totaled 79. A total of 8 written comment forms were received and six oral comments were made during the formal public comment period at Session 1 and a total of 9 written comment forms were received and six oral comments made at Session 2 of the hearing:

Most of the comments expressed support for the project. Some of the comments were concerned about elimination of fishing on the South side of the Causeway, limited access to the water and some concern for wildlife. Other comments expressed concern about the cost of the project and wasting money to build the project.

Copies of the public hearing materials, including the legal display advertisement, the sign-in sheets, the speaker cards, and the public hearing transcript are included in **Appendix D**. Copies of the display graphics, the PowerPoint slides, and attendance rosters are included in the Public Hearing Scrapbook that was prepared for this project and is located in the project files.

A Public Hearing Summary and Comments document was prepared which contains all comments received during and after the public hearing. This document is included in the project file.

## Section 9 – Summary of Public Comments

### 9.1 Comment Summary

This section summarizes and provides responses to those comments that pertain to this project.

The public hearing comment period was advertised to end on April 10, 2011. A total of 285 comments were received. Five (5) official letters of support were received from public groups. A total of 15 written comment forms were received from both public hearing sessions. A total of 12 oral comments were given at both hearing sessions, and 253 comments received were submitted via email after both hearing sessions.

A total of 79 members of the general public attended the two Public Hearing sessions. A total of 8 written comment forms were received and 6 oral comments were made during the formal public comment forum at Session 1 and a total of 7 written comment forms were received and 6 oral comments made at Session 2. Most of the comments provided at the hearing sessions expressed support for the project. Some of the comments expressed concern about elimination of fishing on the South side of the Causeway, limiting access to the water and concern for wildlife. Other comments expressed concern about the cost of the project and a perceived wasteful spending to build the project.

Throughout the course of the study, 19 individuals requested to be placed on the project mailing list. These requests were handled as they were received. One hundred and thirty one (131) comments expressed general support for the project. Seventeen (17) comments were received concerning elimination of access points along the causeway for fishing and swimming. Five (5) comments expressed concerns about potential environmental impacts (excluding noise) such as wildlife, air quality, safety and health. Four (4) comments expressed other concerns such as disliking the idea and a poor notification process. One hundred and fifty (150) comments expressed concerns about the project regarding funding, current economic state and cost of project.

**Appendix E** contains copies of the written comments and a log of the comments. **Table 9-1** summarizes the comments received. Because some individuals submitted several comments in different forms, the total number of comments received, does not equal the total number of individuals in favor or against the project. When duplicate comments were eliminated the total count of project support was one hundred and twenty two (122) individuals and project opposition was one hundred and twenty four (124) individuals. The following table indicates the number and nature of comments received.



**Table 9-1 - Summary of Public Comments**

Comment	Number from Hearing	Number from Email	Total Received
<b>Expressing Concerns Over Various Impacts</b>			
Concern about environmental impacts, wildlife, safety & health	1	4	5
Concern about funding and/or a real need for project	2	96	98
Concerns about eliminating access points for swimming, fishing, etc.		17	17
Other concerns such as, general dislike for the project		4	4
<b>Expressing Support for Project</b>			
Vital trail connectivity for bicyclists and pedestrians	11	111	122

# APPENDIX A

## Efficient Transportation Decision Making Programming Screen Summary Report



# ETDM Summary Report

Project #13102 - SR 60 Trail PD&E Study

Finalized Programming Screen - Published on 03/29/2011

Generated by Wendy Lasher (on behalf of FDOT District 7)

Printed on: 3/29/2011

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## Introduction to Programming Screen Summary Report

The Programming Screen Summary Report shown below is a read-only version of information contained in the Programming Screen Summary Report generated by the ETDM Coordinator for the selected project after completion of the ETAT Programming Screen review. The purpose of the Programming Screen Summary Report is to summarize the results of the ETAT Programming Screen review of the project; provide details concerning agency comments about potential effects to natural, cultural, and community resources; and provide additional documentation of activities related to the Programming Phase for the project. Available information for a Programming Screen Summary Report includes:

- Screening Summary Report chart
- Project Description information (including a summary description of the project, a summary of public comments on the project, and community-desired features identified during public involvement activities)
- Purpose and Need information (including the Purpose and Need Statement and the results of agency reviews of the project Purpose and Need)
- Alternative-specific information, consisting of descriptions of each alternative and associated road segments; an overview of ETAT Programming Screen reviews for each alternative; and agency comments concerning potential effects and degree of effect, by issue, to natural, cultural, and community resources.
- Project Scope information, consisting of general project commitments resulting from the ETAT Programming Screen review, permits, and technical studies required (if any)
- Class of Action determined for the project
- Dispute Resolution Activity Log (if any)

The legend for the Degree of Effect chart is provided in an appendix to the report.

For complete documentation of the project record, also see the GIS Analysis Results Report published on the same date as the Programming Screen Summary Report.



**#13102 SR 60 Trail PD&E Study**

<b>District</b>	District 7	<b>Phase</b>	Programming Screen
<b>County</b>	Pinellas , Hillsborough	<b>From</b>	Bayshore Boulevard
<b>Planning Organization</b>	FDOT District 7	<b>To</b>	W of Ben T. Davis Bch Entrance
<b>Plan ID</b>		<b>Financial Management No.</b>	42264022201
<b>Federal Involvement</b>	Federal Permit Federal Action Federal Funding		
<b>Contact Information</b>	Name: Steve Love Phone: (813) 975-6410 E-mail: steve.love@dot.state.fl.us		

**Snapshot Data From: Programming Screen Summary Report Re-published on 03/29/2011 by Wendy Lasher**

**Overview**

		Evaluation of Direct Effects																				
		Natural									Cultural				Community							
Legend		Air Quality	Coastal and Marine	Contaminated Sites	Farmlands	Floodplains	Infrastructure	Navigation	Special Designations	Water Quality and Quantity	Wetlands	Wildlife and Habitat	Historic and Archaeological Sites	Recreation Areas	Section 4(f) Potential	Aesthetics	Economic	Land Use	Mobility	Relocation	Social	Secondary and Cumulative Effects
N/A	N/A / No Involvement																					
0	None (after 12/5/2005)																					
1	Enhanced																					
2	Minimal (after 12/5/2005)																					
3	Moderate																					
4	Substantial																					
5	Dispute Resolution (Programming)																					
<b>ETAT Review Period: 12/16/2010 - 01/30/2011. Re-Published: 03/29/2011</b>																						
<b>Alternative #1</b>		2	3	2	0	2	0	3	3	3	4	4	3	1	0	2	1	0	1	0	2	3
<b>From Bayshore Boulevard to W of Ben T Davis Bch Entrance</b>																						

## Project Description Data

### Description Statement

The proposed project is a multi-use trail that will be constructed along Courtney Campbell Causeway (SR 60) from the vicinity of the proposed Bayshore Trail extension (Bayshore Blvd. at SR 60) in Pinellas County to West of Ben T. Davis Beach entrance in Hillsborough County. Courtney Campbell Causeway is classified as a scenic highway, and the proposed multi-use trail is consistent with the Local Government Comprehensive Plans (LGCP) for both City of Clearwater and City of Tampa; the Corridor Management Plan (CMP); the Cost Feasible Plan of the Pinellas County 2035 Long Range Transportation Plan (LRTP) adopted December 9, 2009 (Figure 25-Pinellas County Trailways Plan / Page 119, Table 62 - Planned Cost Feasible Trailway Projects / Figure 39 -2009 Regional Multi-Use Trails Network); and the Cost Affordable Plan of the Hillsborough County 2035 LRTP amended August 3, 2010 (Map 10-2 - Bicycle and Trails Cost Affordable / Map 10-3 - Sidewalks Cost Affordable / Appendix B, Page 5, Table B-1 - Cost Affordable Highway, Bicycle, and Pedestrian Projects / Appendix E, Page 4 - Cost Affordable Bicycle and Trails Projects and Unfunded Needs). The proposed facility is intended for bicycle, pedestrian, and other recreational users, thereby providing alternate modes of transportation. The Multi-Use Trail Feasibility Study from McMullen Booth to Veterans Expressway - WPI: 422640 1 and FAP No. 9045-090-C (2008 Feasibility Study) was completed in December 2008 for this project (refer to the project documents section of the project description in the Environmental Screening Tool). The project length is approximately 7.4 miles. The majority of the proposed project is intended to be constructed on the SR 60 fill section and not within the waters of Tampa Bay. The only portions of the proposed project that would be constructed within the waters of Tampa Bay would be the proposed bridges where the main span and the western relief structures are located. These locations are available for viewing on sheet nos. 7, 18, 19, 20 and 21 of Appendix A of the above referenced 2008 Feasibility Study. The study evaluated four (4) separate alternatives and one (1) interim staging option. More details of these alternatives can also be viewed in the Project Concept Summary Report of the project documents section in the Environmental Screening Tool.

The trail alternatives as described in the Project Concept Summary Report are located on the north and south sides of the Causeway and include either the Structural Option 'W2' (widening with piles in the water) or Structural Option 'IS' (Independent Structure). There are three (3) structures within the project limits of the previous 2008 Feasibility Study. The alternatives are described as follows:

Alternative N1 - This alternative includes the trail on the north side of the Causeway and the Structural Widening Option 'W2' for Structures 1 and 2, and the reconfiguration of Structure 3. The associated cost of this alternative based on 2008 estimates is \$60.8M

Alternative N2 - This alternative includes the trail on the north side of the Causeway and the Independent Structural Option 'IS' for Structures 1, 2, and 3. The associated cost of this alternative based on 2008 estimates is \$30.9M

Alternative S1 - This alternative includes the trail on the south side of the Causeway and the Structural Widening Option 'W2' for Structures 1 and 2, and the reconfiguration of Structure 3. The associated cost of this alternative based on 2008 estimates is \$63.2M

Alternative S2 - This alternative includes the trail on the south side of the Causeway and the Independent Structural Option 'IS' for Structures 1, 2, and 3. The associated cost of this alternative based on 2008 estimates is \$33.3M

Staging Option S3 - This is an interim staging option which will provide a shared-use facility on the existing causeway prior to the construction of any new water crossings

There are two bridges within this PD&E study limits. Structure 1, Bridge No. 150138 (Tampa Bay Bridge) is located at the west end of the study and Structure 2, Bridge No. 100301, is located just east of Structure 1. The existing bridges are prestressed concrete girder facilities that were originally built in 1974. The four trail alternatives from the 2008 Feasibility Study considered both widening of the existing bridges and constructing separate trail bridges. The intention of the separate bridges is to utilize separate structures to accommodate the trail for non motorized vehicles and pedestrians. The separate bridges will be designed to accommodate the heaviest required vehicle to perform routine maintenance and inspection.

The trail dimensions vary depending on its location along the project limits (causeway or bridge). The bridge typical section is planned as 16 feet clear width (12' trail plus 2@2' shoulders). Along the causeway, a 12-foot wide multi-use trail is proposed. Improvements are proposed to be constructed within the existing SR 60 Right-of-Way. The trail surfaces proposed for this project include asphalt along the causeway segment and a concrete deck along the bridges.

During the 2008 Feasibility Study, two newsletters were sent out in October 2007 and April 2008. Also, two informal Public Workshops were held on May 19, 2008 and May 22, 2008 in Hillsborough and Pinellas Counties, respectively. Twenty three comments were received with fourteen (14) being in favor, seven (7) offered no opinion and two (2) citizens were against the project. The main concerns of the citizens against the project were "that millions of dollars should not be spent on expanding a road that work perfectly fine but on education and schools." They were also concerned that construction of this project would affect their commute to work.

### Summary of Public Comments

The FDOT completed a Feasibility Study in 2008. During the study, newsletters were distributed to adjacent property owners and interested parties soliciting input. In May 2008, a public workshop was conducted in 2 separate locations (one in Pinellas County and one in Hillsborough County) to provide information to the general public and solicit input. Twenty-three written public comments were received, most of these indicated support of the project or sought additional information about the concepts. Written comments from 2 persons indicated their suggestion to re-allocate public funding necessary for this project to support education as a higher priority. The FDOT coordinated with local agencies, groups and the Courtney Campbell Causeway Scenic Highway xx during the feasibility process to seek input. The 2008 Feasibility Study is posted in the Project Documents portion of this screen, section 8.6 contains the public comment summary with support data located in Appendix E.

### Consistency

- Consistent with Air Quality Conformity.
- CONSISTENT, WITH COMMENTS with Coastal Zone Management Program.
  - Comment: Based on the information contained in the AN and the enclosed state agency comments, the state has no objections to allocation of federal funds for the subject project and, therefore, the funding award is consistent with the Florida Coastal Management Program (FCMP). To ensure the project's continued consistency with the FCMP, the concerns identified by our reviewing agencies must be addressed prior to project implementation. The state's continued concurrence will be based on the activity's compliance with FCMP authorities, including federal and state monitoring of the activity to ensure its continued conformance, and the adequate resolution of issues identified during this and subsequent regulatory reviews. The state's final concurrence of the project's consistency with the FCMP will be determined during the

environmental permitting process in accordance with Section 373.428, Florida Statutes.

- Submitted By: FL Department of Environmental Protection
- Comment Date: 2011-01-26 17:01:43.0
- Consistent with Local Government Comp Plan.
- Consistent with MPO Goals and Objectives.

#### Lead Agency

Federal Highway Administration

#### Exempted Agencies

Agency Name	Justification	Date
Federal Rail Administration	No existing or planned rail lines within project corridor	12/15/2010
US Forest Service	No US Forest land within project corridor.	12/14/2010

#### Community Desired Features

No desired features have been entered into the database. This does not necessarily imply that none have been identified.

#### Purpose and Need

##### Purpose and Need Statement

The purpose of this project is to evaluate a proposed multi-use trail along Courtney Campbell Causeway (SR 60) from Bayshore Blvd. to W. of Ben T. Davis Beach entrance to accommodate recreational users that can experience the scenic qualities of the Causeway, further enhancing tourism and economic development. The proposed Courtney Campbell Causeway Multi-Use Trail has been identified in the Comprehensive Plans of the following jurisdictions: Hillsborough County; Pinellas County; City of Tampa; and the City of Clearwater. The trail has also been identified in the City of Tampa Greenways & Trails Master Plan (2001), the City of Clearwater Bikeways and Trails Plan (1996) and Shifting Gears: Clearwater's Bicycle and Pedestrian Master Plan (2007). A portion of this project is currently funded for design-build in FY 2011/2012 in the FDOT Tentative Work Program 2011-2016. Transportation Improvement Program (TIP) Amendments are currently being processed to facilitate this segment. The proposed trail will serve as a link in a regional network of trail systems serving the Tampa Bay region. As a needed east-west link, the trail will provide regional connectivity with the trail networks for the jurisdictions noted above. In providing the east-west link, regional connectivity could be further enhanced offering alternative modes of transportation in the region.

Beyond the trail's transportation benefits, the trail could serve the recreational needs for residents in the area. The trail could also provide linkage to a series of recreational facilities along the Causeway. It would also recreate a regional recreational opportunity to cross Tampa Bay to link Pinellas and Hillsborough Counties since the existing east-west Friendship Trail Bridge corridor along Gandy Blvd. is no longer available to users within the Tampa Bay area. The Friendship Trail Bridge is permanently closed to users since it is no longer safe to be used. The Friendship Trail Bridge is expected to be demolished once sufficient funds are available to the operating entities for the structure's demolition.

##### Safety

The existing paved shoulders along the causeway portion of the project may be used by avid cyclists, but they do not provide safe access for recreational bikers, walkers, and families to access these amenities. In addition, the absence of shoulders on Structure 2, the main navigable crossing, further exacerbates the safety of cyclists and pedestrians along the corridor. The addition of the multi-use trail will provide for a wider range of non-motorized users.

##### Planned/Programmed Projects in the Project Area

The following are design and construction projects planned or programmed along SR 60 in the project area:

FM No. 424561 3 - SR 60 Trail Project from Bayshore Blvd. to East of Tampa Bay Bridge (Bridge No. 150138), a distance of approximately 1.8 miles - Design is currently planned for FY 2011/2012 and Construction is planned for FY 2015/2016

FM No. 424561 4 - SR 60 Trail Project from East of Tampa Bay Bridge (Bridge No. 150138) to Pinellas/Hillsborough County Line, a distance of approximately 1.7 miles - Design is currently planned for FY 2011/2012 and Construction is planned for FY 2013/2014

FM No. 424561 1 - SR 60 Resurfacing Project from Pinellas/Hillsborough County Line to Rocky Point Drive, a distance of approximately 4.4 miles - Design is ongoing and Construction is planned for FY 2011/2012. This project also includes a small trail segment from the west entrance of Ben T. Davis Beach to Rocky Point Drive

FM No. 424561 2 - SR 60 Trail Project from Rocky Point Drive to East of Bridge # 100064, a distance of approximately 0.4 miles - Design is currently underway and Construction is planned for FY 2011/2012

FM No. 428962 1 - SR 60 Resurfacing Project from West of Damascus Road to Pinellas/Hillsborough County Line, a distance of approximately 3.4 miles - Design is currently programmed for FY 2011/2012 and Construction is planned for FY 2013/2014

##### Area Wide Network/System Linkage

The proposed Courtney Campbell trail will provide regional linkage for non-motorized travel between Pinellas and Hillsborough Counties and, with connection to other facilities, travel into Pasco and Hernando Counties. The project will connect to other existing and planned facilities to the east and west of the Causeway. On the Pinellas (west) side, the project will connect to Pinellas County's extensive trail system (proposed Bayshore Trail extension). On the Hillsborough (east) side, the trail will connect to the West Tampa Greenway (4.6 miles of this 16.6 miles Greenway is completed to date) which will eventually connect via on-street facilities to the Upper Tampa Bay Trail and then from there to the Suncoast Parkway Trail into Pasco and Hernando Counties.

##### Modal Relationships

There are express and local bus routes that operate along SR 60 (Courtney Campbell Causeway) and that intersect SR 60 near the proposed project area. The Hillsborough Area Regional Transit (HART) 200X route is a commuter express route that operates between downtown Tampa and the Eddie Moore Park and Ride Lot in Clearwater. This route only runs during weekday commuter rush hours. Furthermore, HART Route 30 runs near the east end of the proposed trail, and the Pinellas Suncoast Transit Authority (PSTA) Route 60 runs near the west end of the proposed trail. The combination of the existing transit routes and the proposed trail offers additional connections between Pinellas and Hillsborough Counties. The transit routes also provide additional opportunities for use of the proposed trail.

**Social Demands or Economic Development**

There are residential, offices, and commercial land uses located at both ends of the Courtney Campbell Causeway. Rocky Point, located on the east end of the Causeway, has numerous restaurants, office buildings, residences and hotels/resorts. Also located on the east end of the Causeway is the Ben T. Davis Beach. The beaches along the corridor are located within the existing transportation right-of-way and are not considered Section 4(f) protected properties. The shorelines located along the Causeway are popular for fishing, picnicking and use of personal watercraft.

**Purpose and Need Reviews**

Agency	Acknowledgment	Review Date
US Coast Guard	Understood	12/20/2010
FL Department of State	Understood	12/29/2010
Natural Resources Conservation Service	Understood	01/04/2011
Federal Highway Administration	Accepted	01/18/2011

**Comments:** The Purpose and Need Statement is incorrect in that it describes the purpose of the PD&E phase, not the purpose of the project. The purpose for this project is to provide regional connectivity with adjoining trail networks, to offer alternative modes of transportation in the region, to create regional recreational opportunities, and to enhance tourism and economic development.

In the environmental document, please correct the current Purpose and Need Statement so that it describes the purpose of the project.

National Marine Fisheries Service	Understood	01/19/2011
FL Department of Environmental Protection	Understood	01/26/2011
US Fish and Wildlife Service	Understood	01/27/2011
Hillsborough County MPO	Understood	01/27/2011
FL Fish and Wildlife Conservation Commission	Understood	01/27/2011
US Environmental Protection Agency	Understood	01/27/2011
US Army Corps of Engineers	Understood	01/28/2011
Southwest Florida Water Management District	Understood	01/29/2011

**Agencies That Did Not Comment on the Purpose and Need Statement**

**Alternative #1**

**Alternative Description**

<b>From:</b>	Bayshore Boulevard	<b>To:</b>	W of Ben T Davis Bch Entrance
<b>Type:</b>	New Alignment	<b>Status:</b>	ETAT Review Complete
<b>Total Length:</b>	7.473 mi.	<b>Cost:</b>	
<b>Modes:</b>	Bicycle Pedestrian	<b>SIS:</b>	N

**Segment Description(s)**

Location and Length							
Segment No.	Name	Beginning Location	Ending Location	Length (mi.)	Roadway Id	BMP	EMP

Segment #1 7.473 Digitized

**Jurisdiction and Class**

Segment No.	Jurisdiction	Urban Service Area	Functional Class
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Segment #1 FDOT In N/A

**Base Conditions**

Segment No.	Year	AADT	Lanes	Config
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Segment #1

**Interim Plan**

Segment No.	Year	AADT	Lanes	Config
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Segment #1

**Needs Plan**

Segment No.	Year	AADT	Lanes	Config
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Segment #1 2035

**Cost Feasible Plan**

Segment No.	Year	AADT	Lanes	Config
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Segment #1 2035

**Funding Sources**

Segment No.	FEDERAL	Unknown
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Segment #1 \$13,479,950.00

**Project Effects Overview**

Issue	Degree of Effect	Organization	Date Reviewed
<b>Natural</b>			
Air Quality	2 Minimal	US Environmental Protection Agency	01/30/2011
Coastal and Marine	3 Moderate	Southwest Florida Water Management District	01/29/2011
Coastal and Marine	3 Moderate	National Marine Fisheries Service	01/27/2011
Contaminated Sites	2 Minimal	Southwest Florida Water Management District	01/29/2011
Contaminated Sites	0 None	US Environmental Protection Agency	01/27/2011
Contaminated Sites	0 None	FL Department of Environmental Protection	01/26/2011
Farmlands	0 None	Natural Resources Conservation Service	01/04/2011
Floodplains	2 Minimal	US Environmental Protection Agency	01/30/2011
Floodplains	2 Minimal	Southwest Florida Water Management District	01/29/2011
Infrastructure	0 None	Southwest Florida Water Management District	01/29/2011
Navigation	N/A N/A / No Involvement	US Army Corps of Engineers	01/28/2011
Navigation	3 Moderate	US Coast Guard	12/20/2010
Special Designations	3 Moderate	US Environmental Protection Agency	01/30/2011
Special Designations	3 Moderate	Southwest Florida Water Management District	01/29/2011
Water Quality and Quantity	3 Moderate	US Environmental Protection Agency	01/30/2011
Water Quality and Quantity	3 Moderate	Southwest Florida Water Management District	01/29/2011



Water Quality and Quantity	2	Minimal	FL Department of Environmental Protection	01/26/2011
Wetlands	3	Moderate	US Environmental Protection Agency	01/30/2011
Wetlands	3	Moderate	Southwest Florida Water Management District	01/29/2011
Wetlands	N/A	N/A / No Involvement	US Army Corps of Engineers	01/28/2011
Wetlands	4	Substantial	US Fish and Wildlife Service	01/27/2011
Wetlands	3	Moderate	National Marine Fisheries Service	01/27/2011
Wetlands	3	Moderate	FL Department of Environmental Protection	01/26/2011
Wildlife and Habitat	2	Minimal	Southwest Florida Water Management District	01/29/2011
Wildlife and Habitat	4	Substantial	US Fish and Wildlife Service	01/27/2011
Wildlife and Habitat	4	Substantial	FL Fish and Wildlife Conservation Commission	01/27/2011

#### Cultural

Historic and Archaeological Sites	N/A	N/A / No Involvement	Southwest Florida Water Management District	01/29/2011
Historic and Archaeological Sites	2	Minimal	Federal Highway Administration	01/17/2011
Historic and Archaeological Sites	3	Moderate	Seminole Tribe of Florida	01/06/2011
Historic and Archaeological Sites	3	Moderate	FL Department of State	12/29/2010
Historic and Archaeological Sites	2	Minimal	Miccosukee Tribe of Indians of Florida	12/22/2010
Recreation Areas	0	None	US Environmental Protection Agency	01/30/2011
Recreation Areas	0	None	Southwest Florida Water Management District	01/29/2011
Recreation Areas	1	Enhanced	FL Department of Environmental Protection	01/26/2011
Section 4(f) Potential	0	None	Federal Highway Administration	03/16/2011

#### Community

Aesthetics	No reviews recorded.			
Economic	No reviews recorded.			
Land Use	No reviews recorded.			
Mobility	1	Enhanced	Hillsborough County MPO	01/27/2011
Relocation	No reviews recorded.			
Social	2	Minimal	US Environmental Protection Agency	01/30/2011

#### Secondary and Cumulative

Secondary and Cumulative Effects	3	Moderate	Southwest Florida Water Management District	01/29/2011
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#### ETAT Reviews and Coordinator Summary: Natural Issues

##### Coordinator Summary: Air Quality Issue

2 Minimal assigned 02/17/2011 by FDOT District 7

**Comments:** The Florida Department of Transportation (FDOT) has evaluated comments from the US Environmental Protection Agency (USEPA) and recommends a Degree of Effect of Minimal.

The USEPA noted that they do not anticipate any negative air quality impacts related specifically to the project.

The project involves construction of a multi-use recreational trail with no vehicular capacity improvements along SR 60. No impacts to air quality should occur as a result of the project.

No comments were received from the Federal Highway Administration (FHWA).

##### ETAT Reviews: Air Quality Issue: 1 found

2 Minimal assigned 01/30/2011 by Madolyn Dominy, US Environmental Protection Agency

**Coordination Document:** No Selection

**Dispute Information:** N/A

**Identified Resources and Level of Importance:** Resources: Air Quality

**Comments on Effects to Resources:** EPA does not anticipate any negative air quality impacts relating specifically to the project.

**Coordinator Feedback:** None

The following organization(s) were expected to but did not submit a review of the Air Quality issue for this alternative: Federal Highway Administration

**Coordinator Summary: Coastal and Marine Issue**

**3** *Moderate* assigned 02/17/2011 by FDOT District 7

**Comments:** The Florida Department of Transportation (FDOT) has evaluated comments from the Southwest Florida Water Management District (SWFWMD) and National Marine Fisheries Service (NMFS) and recommends a Degree of Effect of Moderate.

The NMFS staff conducted a site inspection of the project area on December 20, 2010, to assess potential concerns to living marine resources within Old Tampa Bay and Safety Harbor and concluded that the project could directly impact NMFS trust resources. Some isolated mangroves occur along the causeway's southern shoreline. Seagrass beds occur adjacent to the shoreline at various points along the south side of the causeway. Certain estuarine habitats within the project area are designated as essential fish habitat (EFH) as identified in the 2005 generic amendment of the Fishery Management Plans for the Gulf of Mexico. Mangroves have been identified as EFH for postlarval/juvenile, subadult, and adult red drum and gray snapper, schoolmaster, cubera snapper, yellowtail snapper, dog snapper, and juvenile goliath grouper by the Gulf of Mexico Fishery Management Council under provisions of the Magnuson-Stevens Act. Seagrasses have been identified as EFH for juvenile and subadult penaeid shrimp, juvenile and adult stone crab, postlarval, juvenile, and subadult and adult red drum, juvenile and adult schoolmaster and mutton snapper, and juvenile gag, goliath grouper, red grouper, black grouper, yellowfin grouper, Nassau grouper, lane snapper, dog snapper, yellowtail snapper, and cubera snapper.

The NMFS requested that an EFH Assessment be prepared for this project. Upon review of the EFH Assessment, the NMFS will determine if it is necessary to provide EFH Conservation Recommendations for the project. The NMFS cannot make a determination between the south side alternatives until potential seagrass impacts for the two bridging options have been assessed. Seagrass surveys should be conducted during the prime seagrass growing season between June 1 and September 30. These surveys can be undertaken as part of the design/build phase.

The SWFWMD noted that the project occupies watersheds that are included in the Tampa Bay Estuary Watershed designated estuary of national significance. The SWFWMD also noted that while it is intended that the project be constructed within the cross section of existing Causeway fill, it may be necessary to add fill to accommodate the proposed facilities. In that case, elimination/disruption of the mangroves and estuarine vegetation now established along much of the project length on the causeway may occur.

The project will be constructed on fill material that was used to construct the existing Causeway and two new bridges will be constructed to span Old Tampa Bay. There are sensitive marine and estuarine resources located near the project corridor. Since the project will be located on the south side of the Causeway and should be located over the existing fill, there should be minimal impacts to these resources. Avoidance and minimize efforts will be implemented during design. The FDOT will commit to using proper best management practices (BMPs) during construction to avoid or minimize any direct or secondary impacts to coastal and marine resources.

The FDOT will prepare a Wetland Evaluation and Biological Assessment Report (WEBAR) during the PD&E study. This report will assess potential species, existing habitat, and potential essential fish habitat (EFH) within the project area. This report and the FDOT's findings will be coordinated with the USFWS and NMFS.

No comments were received from the Federal Highway Administration (FHWA).

**ETAT Reviews: Coastal and Marine Issue: 2 found**

**3** *Moderate* assigned 01/29/2011 by C. Lynn Miller, Southwest Florida Water Management District

**Coordination Document:** Permit Required

**Dispute Information:**N/A

**Identified Resources and Level of Importance:** The project occupies watersheds that are included in the 2200-acre Tampa Bay Estuary Watershed, designated "estuary of national significance" by the US Congress in 1990.

The entire project segment that is located in Pinellas County occupies the Pinellas Aquatic Preserve. Waters within the Preserve, part of Old Tampa Bay, are designated as Outstanding Florida Waters.

The entire project is located in Class II waters designated for Shellfish Propagation or Harvesting; commercial crabbing occurs in Old Tampa Bay.

Designated Environmental Conservation Areas/Bird Nesting Areas containing very dense mangrove forests, closed during the period January to August, are located on the north side of the Causeway.

A total of 95 acres of sovereign submerged lands are present within 100 feet of the project, while 219 acres are within 200 feet of the project.

The final receiving water for the project area is Old Tampa Bay which is the major northwestern embayment of Tampa Bay, a Priority Water Body in the SWFWMD's Surface Water Improvement and Management (SWIM) Program.

Some watersheds in which the project is located are included on the FDEP Verified List of Impaired Waters.

According to 2008-era imagery and mapping, there are seagrass beds located along the project route. Acreage ranges from 22.3 acres to 64 acres within the 100-foot to 200-foot project buffers, respectively.

While indicated otherwise in the EST, there are FWC Manatee Protection Zones (information updated 9/17/09) located adjacent to the Causeway fill near the east project terminus on the north side for a length of approximately 0.94 mile. One zone is restricted to the navigational channel that parallels the Causeway and which requires a speed of no more than 25 mph in the period April 1 through November 15. The second zone requires slow speed in the period April 1 through November 15. There is another Manatee Protection Zone located adjacent to the Causeway fill near the west project terminus on the north side for a length of approximately 0.5 mile.

Alternatives N1, N2: The western portion of the N1 alternative appears to go directly across the adjacent mangrove area. These northern alternatives appear to involve significant mangrove areas.

Alternatives S1, S2: These alternatives appear to involve and possibly affect more seagrass beds, salt flats and shoreline habitats than Mangrove Swamps.

**Comments on Effects to Resources:** While it is intended that the project be constructed within the cross section of existing Causeway fill, it may be necessary to add fill to accommodate the proposed facilities. In that case, the elimination and/or disruption of the mangroves and estuarine vegetation now established along much of the project length on the Causeway may occur.

The project may result in disturbance or the partial elimination of the Designated Environmental Conservation Areas/Bird Nesting Areas on the north side of the Causeway.

The project has the potential to generate increased sedimentation and turbidity during construction that may degrade water quality within Old Tampa Bay, thereby (1) reducing the recovery of important seagrass beds which are particularly vulnerable to sedimentation, (2) adversely affecting the water quality of OFW and Class II waters, and (3) adversely affecting commercially important blue crabs and their habitat.

Impacts to manatees may include direct impingement of animals by in-the-water construction equipment and the disruption of breeding habitat during the period April 1 through November 15.

**Additional Comments (optional):** The SWFWMD has assigned a Degree of Effect based on their opinion of the potential of this project to result in identifiable impacts after fully complying with the SWFWMD's permitting processes or the effort associated with fully complying with the SWFWMD's proprietary interests and obligations.

Adjusting the width of the facility cross section to fit within the varying widths of the existing fill sections along the Causeway would help to reduce or eliminate impacts to mangroves and estuarine vegetation and reduce or eliminate impacts to the Designated Environmental Conservation Areas/Bird Nesting Areas.

Timing of the project construction may help to reduce impact to the Designated Environmental Conservation Areas/Bird Nesting Areas.

It is recommended that updated seagrass maps be prepared or otherwise acquired as the most easily accessible information now is of 2008 vintage.

**Coordinator Feedback:** None

**3** Moderate assigned 01/27/2011 by David A. Rydene, National Marine Fisheries Service

**Coordination Document:** PD&E Support Document As Per PD&E Manual

**Dispute Information:** N/A

**Identified Resources and Level of Importance:** Old Tampa Bay and Safety Harbor, which contain estuarine and marine habitats such as seagrass and mangrove used by federally-managed fish species and their prey.

**Comments on Effects to Resources:** NOAA's National Marine Fisheries Service (NMFS) has reviewed the information contained in the Environmental Screening Tool for ETDM Project # 13102. The Florida Department of Transportation (FDOT) District 7 proposes the construction of a multi-use trail along the SR 60 Courtney Campbell Causeway in Hillsborough County and Pinellas County, Florida. Per a phone conversation with Robin Rhinesmith of FDOT District 7 on January 27, 2011, and a follow-up email, the two alternatives for a trail on the north side of the causeway are no longer under consideration. The remaining two south side trail alternatives would parallel the roadway. The trail would span the water at three points. Crossings would be accomplished by either widening the existing bridge structures or constructing independent bridge structures adjacent to the existing ones. The following comments assess only the two south side trail alternatives.

NMFS staff conducted a site inspection of the project area on December 20, 2010, to assess potential concerns related to living marine resources within Old Tampa Bay and Safety Harbor. The lands adjacent to the proposed project are principally estuarine habitats associated with Tampa Bay, a public beach, and commercial properties at either end of the causeway. It appears that the project could directly impact NMFS trust resources (i.e. mangroves and/or seagrass). Some fringing mangroves occur along the causeway's southern shoreline. Seagrass beds occur adjacent to the shoreline at various points along the south side of the causeway. Certain estuarine habitats within the project area are designated as essential fish habitat (EFH) as identified in the 2005 generic amendment of the Fishery Management Plans for the Gulf of Mexico. The generic amendment was prepared by the Gulf of Mexico Fishery Management Council as required by the 1996 amendment to the Magnuson-Stevens Fishery Conservation and Management Act (Magnuson-Stevens Act). Mangroves have been identified as EFH for juvenile, subadult and adult red drum, gray snapper, schoolmaster, and cubera snapper, and juvenile goliath grouper, yellowtail snapper, and dog snapper by the Gulf of Mexico Fishery Management Council under provisions of the Magnuson-Stevens Act. Seagrasses have been identified as EFH for juvenile and subadult penaeid shrimp, juvenile and adult stone crab, postlarval, juvenile, subadult and adult red drum, juvenile and adult schoolmaster and mutton snapper, and juvenile gag, goliath grouper, red grouper, black grouper, yellowfin grouper, Nassau grouper, lane snapper, dog snapper, yellowtail snapper, and cubera snapper.

Federal agencies which permit, fund, or undertake activities which may adversely impact EFH are required to consult with NMFS and, as a part of the consultation process, an EFH Assessment must be prepared to accompany the consultation request. Regulations require that EFH Assessments include:

1. a description of the proposed action;
2. an analysis of the effects (including cumulative effects) of the proposed action on EFH, the managed fish species, and major prey species;
3. the Federal agency's views regarding the effects of the action on EFH; and
4. proposed mitigation, if applicable.

Provisions of the EFH regulations [50 CFR 600.920(c)] allow consultation responsibility to be formally delegated from federal to state agencies, including FDOT. Whether EFH consultation is undertaken by the federal agency (e.g. Federal Highway Administration) or FDOT, it should be initiated as soon as specific project design and construction impact information is available. EFH consultation can be initiated independent of other project

review tasks or can be incorporated in environmental planning documents. Upon review of the EFH Assessment, NMFS will determine if it is necessary to provide EFH Conservation Recommendations for the project.

Between the two south side alternatives, NMFS cannot make a determination until potential seagrass impacts for the two bridging options have been assessed. Seagrass surveys should be conducted during the prime seagrass growing season between June 1 and September 30. These surveys can be undertaken as part of the design/build phase. NMFS strongly discourages any impacts to seagrass habitat as the success of compensatory mitigation measures for seagrass loss are considered too uncertain given the current state of the art.

NMFS recommends that stormwater treatment systems be upgraded to prevent degraded water from entering estuarine habitats within the system. In addition, best management practices should be employed during trail construction to prevent siltation of estuarine habitats.

**Coordinator Feedback:** None

The following organization(s) were expected to but did not submit a review of the Coastal and Marine issue for this alternative: Federal Highway Administration

### Coordinator Summary: Contaminated Sites Issue

**2** Minimal assigned 02/17/2011 by FDOT District 7

**Comments:** The Florida Department of Transportation (FDOT) has evaluated comments from the Southwest Florida Water Management District (SWFWMD), the Florida Department of Environmental Protection (FDEP), and the US Environmental Protection Agency (USEPA) and recommends a Degree of Effect of Minimal.

The City of Clearwater Advanced Waste Water Treatment Plant (AWWTP) and a Sunoco gas station are located outside the western terminus of the project, and both facilities include petroleum storage facilities on-site. Discharges have been reported at each site. The City of Tampa Rocky Point Pump Station was located to the east of Structure 2. This facility contained an underground storage tank (UST), but has been closed since 1994, and the tank was removed. There should be no impacts to the existing facilities from the proposed construction. The FDOT will prepare a Contamination Screening Evaluation Report (CSER) as part of the PD&E study. Any source identified should be assessed to determine the need for remediation during construction.

No comments were received from the Federal Highway Administration (FHWA).

### ETAT Reviews: Contaminated Sites Issue: 3 found

**2** Minimal assigned 01/29/2011 by C. Lynn Miller, Southwest Florida Water Management District

**Coordination Document:** Permit Required

**Dispute Information:** N/A

**Identified Resources and Level of Importance:** There is one potential contaminated site located near the west project terminus: the City of Clearwater East AWWTP, which also has petroleum storage facilities onsite, is located within 100 feet of the west terminus of the south alternative.

In terms of the possible discharge of toxic or hazardous waste from vehicle damage while on the causeway or its bridges, there appears to be no effective containment and control systems in place or proposed for the project area.

As the precise location for any of the alternatives as well as extensions to the east that will predictably happen if this project is built are not known at this time, it is noteworthy that considerable utilities, including wastewater pumping stations and pipelines may be affected by the proposed construction.

There may be other, as yet unknown, contaminated sites.

**Comments on Effects to Resources:** The construction of the project and associated facilities in areas where there are sources of contamination may mobilize the contamination and cause or contribute to pollution of surface waters. Such pollution may contribute to the degradation of sensitive estuarine waters.

**Additional Comments (optional):** The Degree of Effect is considered "Minimal." It is possible but unlikely that there are other, unknown, sources of contamination within 500 feet of the project. The potential is low for the contamination of estuarine waters as a result of contamination of the surficial aquifer. Even so, it is recommended that FDOT evaluate potential stormwater treatment pond sites for the presence of contamination and eliminate contaminated areas as possible pond sites or steps must be taken (such as use of impermeable liners) to isolate stormwater from contaminated soil or groundwater. If discovered during construction, contaminated soils or waters should be remediated properly so as to eliminate the potential for water resource contamination. Addition of effective containment and control features for the project area may reduce the probability of adverse impact due to uncontrolled releases from vehicle crashes.

**Coordinator Feedback:** None

**0** None assigned 01/27/2011 by Madolyn Dominy, US Environmental Protection Agency

**Coordination Document:** No Selection

**Dispute Information:** N/A

**Identified Resources and Level of Importance:** None found.

**Comments on Effects to Resources:** None found.

**Coordinator Feedback:** None

**0** None assigned 01/26/2011 by Lauren P. Milligan, FL Department of Environmental Protection

**Coordination Document:** No Selection

**Dispute Information:** N/A

**Identified Resources and Level of Importance:** None found.

**Comments on Effects to Resources:** None found.

**Coordinator Feedback:** None

The following organization(s) were expected to but did not submit a review of the Contaminated Sites issue for this alternative: Federal Highway Administration

### Coordinator Summary: Farmlands Issue

0 None assigned 02/17/2011 by FDOT District 7

**Comments:** The Florida Department of Transportation (FDOT) has evaluated comments from the Natural Resource Conservation Service (NRCS) and recommends a Degree of Effect of None.

A review of the Geographical Information Systems (GIS) analysis data and NRCS comments indicates that there are no Prime Farmlands, Farmlands of Unique Importance, or Farmlands of Local Importance are within the 5,280-foot buffer distance. This project will not result in any impacts to farmlands.

No comments were received from the Federal Highway Administration (FHWA).

### ETAT Reviews: Farmlands Issue: 1 found

0 None assigned 01/04/2011 by Rick Allen Robbins, Natural Resources Conservation Service

**Coordination Document:** No Selection

**Dispute Information:** N/A

**Identified Resources and Level of Importance:** The USDA-NRCS considers soil map units with important soil properties for agricultural uses to be Prime Farmland. In addition, the USDA-NRCS considers any soils with important soil properties and have significant acreages that are used in the production of commodity crops (such as, cotton, citrus, row crops, specialty crops, nuts, etc.) to be considered as Farmlands of Unique Importance. Nationally, there has been a reduction in the overall amount of Prime and Unique Farmlands through conversion to non-farm uses. This trend has the possibility of impacting the nation's food supply and exporting capabilities.

**Comments on Effects to Resources:** Conducting GIS analysis of Prime Farmland (using USDA-NRCS data) and Important (Unique) Farmland Analysis (using existing SWFWMD land use data and 2010 SSURGO data) has resulted in the determination that there are no Prime, Unique, or Locally Important Farmland soils within most buffer width within the Project Area. Therefore, no degree of effect to agricultural resources.

**Additional Comments (optional):** It should be noted that Unique Farmlands would be impacted at the 5280 buffer width, but this project will not impact those soil resources.

**CLC Commitments and Recommendations: Coordinator Feedback:** None

The following organization(s) were expected to but did not submit a review of the Farmlands issue for this alternative: Federal Highway Administration

### Coordinator Summary: Floodplains Issue

2 Minimal assigned 02/17/2011 by FDOT District 7

**Comments:** The Florida Department of Transportation (FDOT) has evaluated comments from the US Environmental Protection Agency (USEPA) and Southwest Florida Water Management District (SWFWMD) and recommends a Degree of Effect of Minimal.

A review of the Geographical Information Systems (GIS) analysis data indicates that the project is located within Coastal Flood Zone VE, which is tidally influenced and is a Special Flood Hazard Area. Minimal to no fill will be required for the trail, with the exception of the pilings for the construction of the bridges. Fill will be needed for the construction of the bridge approaches. The FDOT will adhere to SWFWMD criteria and permitting requirements during design and construction.

No comments were received from the Federal Highway Administration (FHWA) or the Florida Department of Environmental Protection (FDEP).

### ETAT Reviews: Floodplains Issue: 2 found

2 Minimal assigned 01/30/2011 by Madolyn Dominy, US Environmental Protection Agency

**Coordination Document:** No Selection

**Dispute Information:** N/A

**Identified Resources and Level of Importance:** Resources: Floodplains

**Level of Importance:** Development within the 100-year floodplain is of a high level of importance. Development and construction may occur within the Special Flood Hazard Area, provided that development complies with floodplain management ordinances and/or local, state, and federal requirements. EPA is assigning a minimal degree of effect for the project (ETDM #13102).

**Comments on Effects to Resources:** A review of GIS analysis data (DFIRM and Special Flood Hazard Areas) in the EST at the programming screen phase of the project indicates that the majority of the project area lies within Coastal Flood Zone VE or Zone AE of the flood hazard zone designation.

The SR 60 Multi-Use Trail project environmental studies should determine what impact the project will have on floodplains. Any proposed action which is located in a floodplain must consider alternatives to avoid effects and incompatible development in the floodplains. If the project will impact floodplains, it should be designed to minimize potential harm to or within the floodplain.

The degree of direct floodplain impacts associated with the project will be dependent upon the amount of right-of-way needed for the project and how much natural environment will be impacted. EPA recommends that any studies for this project should focus on identifying the types of special flood hazard areas to be potentially impacted and what type of additional analyses, if any, will be needed.

**Additional Comments (optional):** General comments relating to floodplains include the fact that any development within the 100-year floodplain has the potential for placing citizens and property at risk of flooding and producing changes in floodplain elevations and plan view extent. Development (such as roadways, housing developments, strip malls and other commercial facilities) within floodplains increases the potential for flooding by limiting flood storage capacity and exposing people and property to flood hazards. Development also reduces vegetated buffers that protect water quality and destroys important habitats for fish and wildlife.

**Coordinator Feedback:** None



**Coordination Document:** Permit Required

**Dispute Information:**N/A

**Identified Resources and Level of Importance:** The project appears to cross a Coastal Emergency Management Flood Area designated as VE. The at-grade segments of the project are located in FEMA FIRM Zones A or AE. The alignment of the west terminal segment of the north alternative as described in the 2008 Feasibility Study now extends over the existing stormwater management facility located on the east side of the FDOT property, which was not in place as of the 2008 study.

**Comments on Effects to Resources:** The FDOT stormwater management facility located along the alignment of the west terminal segment of the north alternative would be rendered ineffective if the trail is built at grade. Consequently, it may be necessary to replace the facility, which will require a modification of the existing ERP. If the trail were built on structure across the stormwater management facility, the facility may still function properly with the replacement of the storage volume occupied by vertical support members; a modification of the existing ERP would still be needed.

At-grade segments of the project within storm surge influence may be damaged due to inundation, return flow, and wave erosion from such events.

**Additional Comments (optional):** The SWFWMD has assigned a Degree of Effect based on their opinion of the potential of this project to result in identifiable impacts after fully complying with the SWFWMD's permitting processes or the effort associated with fully complying with the SWFWMD's proprietary interests and obligations.

The degree of effect may be reduced by: (1) adjusting the alignment of the trail to avoid the existing stormwater management facility on the FDOT property or otherwise ensure proper functioning of the facility; and (2) armoring or protecting constructed stormwater facilities associated with the project.

**Coordinator Feedback:** None

The following organization(s) were expected to but did not submit a review of the Floodplains issue for this alternative: FL Department of Environmental Protection, Federal Highway Administration

#### Coordinator Summary: Infrastructure Issue

0 None assigned 02/17/2011 by FDOT District 7

**Comments:** The Florida Department of Transportation (FDOT) has evaluated comments from the Southwest Florida Water Management District (SWFWMD) and recommends a Degree of Effect of None.

A review of the Geographical Information Systems (GIS) analysis data indicates that no existing infrastructure was identified within the project limits.

No comments were received from the Federal Highway Administration (FHWA).

#### ETAT Reviews: Infrastructure Issue: 1 found

0 None assigned 01/29/2011 by C. Lynn Miller, Southwest Florida Water Management District

**Coordination Document:** No Involvement

**Dispute Information:**N/A

**Identified Resources and Level of Importance:** None found.

**Comments on Effects to Resources:** None found.

**Coordinator Feedback:** None

The following organization(s) were expected to but did not submit a review of the Infrastructure issue for this alternative: Federal Highway Administration

#### Coordinator Summary: Navigation Issue

3 Moderate assigned 02/17/2011 by FDOT District 7

**Comments:** The Florida Department of Transportation (FDOT) has evaluated comments from the US Coast Guard (USCG) and the US Army Corps of Engineers (USACE) and recommends a Degree of Effect of Moderate.

The project is located within waters that are considered to be navigable, tidal, Section 10 waters of the United States. The USACE noted that the USACE does not have regulatory authority over this project. The USGC noted that a Coast Guard Bridge Permit will be acquired during design and permitting of the project. The proposed trail bridges are intended to at least match the existing horizontal and vertical clearances of the adjacent SR 60 highway bridges. The FDOT expects to at least maintain the existing horizontal and vertical clearances of the new bridges.

No comments were received from the Federal Highway Administration (FHWA).

#### ETAT Reviews: Navigation Issue: 2 found

N/A N/A / No Involvement assigned 01/28/2011 by John Fellows, US Army Corps of Engineers

**Coordination Document:** To Be Determined: Further Coordination Required

**Dispute Information:**N/A

**Identified Resources and Level of Importance:** The potentially impacted areas described are considered to be navigable, tidal, Section 10 waters of the United States that are part of Tampa Bay. Based on the project description, the proposed work does not involve the discharge of dredged or fill material into the waters of the United States, per Section 404 of the Clean Water Act.

**Comments on Effects to Resources:** Based on my understanding of the current division of authority over 'bridge' projects between the Corps and the Coast Guard, the Corps does not have regulatory authority over this project.

**Coordinator Feedback:** None

3 Moderate assigned 12/20/2010 by Randy Overton, US Coast Guard

**Coordination Document:** Permit Required

**Dispute Information:** N/A

**Identified Resources and Level of Importance:** Navigation, moderate

**Comments on Effects to Resources:** A Coast Guard Bridge Permit will be required for the construction of an independent structure or the modification of the existing structure.

**Coordinator Feedback:** None

The following organization(s) were expected to but did not submit a review of the Navigation issue for this alternative: Federal Highway Administration

#### Coordinator Summary: Special Designations Issue

3 Moderate assigned 02/17/2011 by FDOT District 7

**Comments:** The Florida Department of Transportation (FDOT) has evaluated comments from the US Environmental Protection Agency (USEPA) and the Southwest Florida Water Management District (SWFWMD) and recommends a Degree of Effect (DOE) of Moderate.

A review of the Geographical Information Systems (GIS) analysis data indicates that Public Land Cooper's Point is located within the 500-foot buffer distance. The western portion of the project is located within the Pinellas County Aquatic Preserve which is an Outstanding Florida Water (OFW), however, the project will be constructed within SR 60's right of way (ROW) that is designated for transportation purposes. No fill material will be placed within the Pinellas County Aquatic Preserve with the exception of the construction of Structure 1. This project is in the public's interest since it provides recreational opportunities for non-motorized users to enjoy this FDOT designated Scenic Highway. Also, please see Special Flood Hazard Areas and Mangroves information in the Floodplain and Coastal and Marine DOEs, respectively.

The SWFWMD stated that Tampa Bay is one of the Priority Waterbodies in the SWFWMD's Surface Water Improvement and Management (SWIM) program. The SWFWMD also noted that estuarine habitats within the project area, including mangroves and seagrass beds, are designated as essential fish habitat for numerous juvenile, sub-adult and adult fish species. The project is located within Class II waters designated for shellfish propagation or harvesting. Designated areas for bird nesting are located on the north side of the Causeway. The project will be located on the south side of the Causeway on existing fill, with the exception of the proposed bridges. The FDOT will use proper best management practices (BMPs) during construction to minimize runoff into the Bay from construction activities and reduce potential turbidity within the waters of Old Tampa Bay.

No comments were received from the Florida Department of Agriculture and Consumer Services (DCA) or the Federal Highway Administration (FHWA).

#### ETAT Reviews: Special Designations Issue: 2 found

3 Moderate assigned 01/30/2011 by Madolyn Dominy, US Environmental Protection Agency

**Coordination Document:** No Selection

**Dispute Information:** N/A

**Identified Resources and Level of Importance:** Resources: Outstanding Florida Waters, Special Flood Hazard Areas (100-year Floodplain), Aquatic Preserves, Mangroves, Public Lands

**Level of Importance:** The resources listed above (identified as special designations) are of a high level of importance in the State of Florida. EPA is assigning a moderate degree of effect to this issue for the proposed project (ETDM #13102).

**Comments on Effects to Resources:** A review of GIS analysis data at the programming screen phase of the project indicates that the following features identified as Special Designations are located within proximity of the project:

Special Flood Hazard Areas (100-year Floodplain) - See Comments under Floodplains issue regarding potential floodplain impacts.

#### Aquatic Preserves - Pinellas County Aquatic Preserve

The Pinellas County Aquatic Preserve was established on March 21, 1972 and was designated as an Outstanding Florida Water on March 1, 1979. The Pinellas County Aquatic Preserve and the Boca Ciega Bay Aquatic Preserve are located on the Gulf coast of west central Florida, and include the state-owned submerged land in Pinellas County waters. The preserves encompass 136,082 hectares (336,265 acres) of state-owned submerged land. The surrounding area is one of the most urbanized areas in Florida, and as such has special management needs. The preserves include nearshore habitats along sandy beaches and mangrove dominated shorelines. Submerged habitats include oyster bars, seagrass beds, coral communities, and springfed caves. Abundant islands, including those formed from dredge spoil material, are also part of the preserve. Approximately 1/3 of Florida's coral species can be found in the Pinellas County Aquatic Preserve.

#### Outstanding Florida Waters - Pinellas County Aquatic Preserve

The Pinellas County Aquatic Preserve is listed as an Outstanding Florida Water (OFW). OFWs are provided the highest level of protection under the Florida Administrative Code (F.A.C.). Degradation of water quality in an OFW is prohibited except under certain circumstances. Pollutant discharges must not lower existing ambient water quality. Any activity within an OFW requiring a Florida Department of Environmental Protection (FDEP) Environmental Resource Permit (ERP) must be deemed to be clearly in the public interest. Additional stormwater retention and treatment requirements may be required. FDOT will need to coordinate and consult with FDEP regarding specific permitting requirements relating to this OFW.

#### Mangroves -

There are mangrove swamps located within proximity of the proposed project. Mangroves serve several important ecosystem functions. They provide nursery habitat for fishes, crustaceans, and shellfish and they provide food for several types of marine species. Both recreational and commercial fisheries in Florida are dependent upon healthy mangrove forests. Mangroves also provide shelter and nesting areas for coastal birds. Protecting mangrove acreage is critical, especially since most of the loss of acreage is due to human impact such as development and construction. As a result of dramatic changes in the Tampa Bay (Pinellas/Hillsborough County) area, a significant amount of coastal wetlands acreage has been lost, including mangroves and salt marshes. Therefore, protection of the coastal wetlands is critical to fish habitat and other marine resources. Regulations to protect mangrove forests have been developed by both state and local agencies. These regulations must be met and consultation with other agencies such as the National Marine Fisheries Service may be required. Avoidance measures should be strongly considered for this project. Also, mitigation to provide

enhanced or increased function should be strongly evaluated within the same general area.

Public Land - Cooper's Point

Direct, indirect, and cumulative impacts to listed special designation features and other natural resources should be evaluated. Opportunities to avoid and or minimize impacts and fragmentation to these types of resources should be considered to the greatest extent practicable.

**Coordinator Feedback:** None

**3** *Moderate* assigned 01/29/2011 by C. Lynn Miller, Southwest Florida Water Management District

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**Coordination Document:** Permit Required

**Dispute Information:** N/A

**Identified Resources and Level of Importance:** The project occupies watersheds that are included in the 2200-acre Tampa Bay Estuary Watershed, designated "estuary of national significance" by the US Congress in 1990 and included in the National Estuary Program.

The project segment located in Pinellas County occupies the Pinellas Aquatic Preserve, a 336,000-acre area that encompasses the sovereign submerged lands in Pinellas County exclusive of those included in the Boca Ciega Aquatic Preserve. Waters within the Preserve, part of Old Tampa Bay, are designated as Outstanding Florida Waters.

The project is located in Class II waters designated for Shellfish Propagation or Harvesting, and commercial crabbing activity occurs in Old Tampa Bay.

Designated Environmental Conservation Areas/Bird Nesting Areas containing very dense mangrove forests are located on the north side of the Causeway. These areas are closed during the period January to August.

A total of 95 acres of sovereign submerged lands are present within 100 feet of the project, while 219 acres are within 200 feet of the project.

According to 2008-era imagery and mapping, there are seagrass beds located along the project route. Acreage ranges from 22.3 acres to 64 acres within the 100-foot to 200-foot project buffers.

The final receiving water for the project area is Old Tampa Bay which is the major northwestern embayment of Tampa Bay, a Priority Water Body in the SWFWMD's Surface Water Improvement and Management (SWIM) Program (Tampa Bay, 1999).

Some watersheds in which the project is located are included on the FDEP Verified List of Impaired Waters.

While indicated otherwise in the EST, there are FWC Manatee Protection Zones (information updated 9/17/09) located adjacent to the Causeway fill near the east project terminus on the north side for a length of approximately 0.94 mile. One zone is restricted to the navigational channel that parallels the Causeway and which requires a speed of no more than 25 mph in the period April 1 through November 15. The second zone requires slow speed in the period April 1 through November 15. There is another Manatee Protection Zone located adjacent to the Causeway fill near the west project terminus on the north side for a length of approximately 0.5 mile.

The project is located in a Special Coastal Flood Hazard Area.

Designated conservation lands, Cooper's Point, are within 500 feet of the proposed project.

**Comments on Effects to Resources:** The project has a potential to result in water quality impacts to Class II Waters and Outstanding Florida Waters and to delay the recovery of Impaired Waters as a result of undertreated or untreated stormwater runoff during and after construction.

It is intended that the project be constructed within the cross section of existing Causeway fill, but it may be necessary to add fill and remove mangroves and fill tidal flats, shoreline areas and saltwater marshes that are established along much of the project length on the Causeway.

Depending on the width of the project cross section in the specific location of the Designated Environmental Conservation Areas/Bird Nesting Areas on the north side of the Causeway, the project may result in disturbance or the partial elimination of these Designated Areas.

The project has the potential to generate increased sedimentation and turbidity during construction that may degrade water quality within Old Tampa Bay, thereby (1) reducing the viability of seagrass beds which are particularly vulnerable to sedimentation, (2) adversely affecting the water quality of OFW and Class II waters, and (3) adversely affecting commercially important blue crabs and their habitat.

Impacts to manatees may include direct impingement of animals by in-the-water construction equipment and the disruption of breeding habitat during the period April 1 through November 15.

**Additional Comments (optional):** The SWFWMD has assigned a Degree of Effect based on their opinion of the potential of this project to result in identifiable impacts after fully complying with the SWFWMD's permitting processes or the effort associated with fully complying with the SWFWMD's proprietary interests and obligations.

It may be necessary to demonstrate a net improvement for the water quality parameters of concern, including parameters for which receiving waterbodies are impaired, by performing a pre/post pollutant loading analysis. This project will discharge to Old Tampa Bay and the SWFWMD will require a demonstration of net reduction of nutrient loading in discharges to the Bay. To minimize pollution potential, it would be useful to collect and treat discharges from the project facilities to a higher standard than the minimum required by rule before discharging to sensitive estuarine areas. Treating those impervious areas that are now untreated also would assist in reducing the sediment load of runoff ultimately reaching the Bay within the project area.

Adjusting the width of the facility cross section to fit within the varying widths of the existing fill sections along the Causeway would help to reduce or eliminate impacts to mangroves and estuarine vegetation and reduce or eliminate impacts to the Designated Environmental Conservation Areas/Bird Nesting Areas.

Timing of the project construction may help to reduce impact to the Designated Environmental Conservation Areas/Bird Nesting Areas.

Strict erosion control measures and turbidity monitoring may help to reduce impacts to seagrass beds, blue crabs and hard bottom habitat preferred by oysters. It is recommended that updated seagrass maps be prepared or otherwise acquired as the most easily accessible information now is of 2008 vintage.

**Coordinator Feedback:** None

The following organization(s) were expected to but did not submit a review of the Special Designations issue for this alternative: FL Department of Agriculture and Consumer Services, Federal Highway Administration

### Coordinator Summary: Water Quality and Quantity Issue

**3** *Moderate* assigned 02/17/2011 by FDOT District 7

**Comments:** The Florida Department of Transportation (FDOT) has evaluated comments from the Florida Department of Environmental Protection (FDEP), the US Environmental Protection Agency (USEPA), and the Southwest Florida Water Management District (SWFWMD) and recommends a Degree of Effect of Moderate.

A review of the Geographical Information Systems (GIS) analysis data indicates that the project is located within portions of the Pinellas County Aquatic Preserve which is an Outstanding Florida Water (OFW). The current list of 303(d) Verified List of Impaired Waters states that surrounding waters are listed for nutrients, fecal coliforms/bacteria, and mercury in fish. The project consists of a non-motorized trail that should not contribute to degradation of the surrounding waters. Trail users, such as bicyclists and pedestrians, would not generate the release of any oils, greases or other pollutants that could enter the Bay from this type of activity. The construction of the proposed project should not contribute to increases in pollutant loads within the Bay.

The SWFWMD noted that the project occupies Old Tampa Bay and Courtney Campbell Beach coastal watersheds and the entire project is located in Class II waters designated for Shellfish Propagation or Harvesting; commercial crabbing occurs in Old Tampa Bay.

The FDEP recommended that the PD&E Study include an evaluation of existing area stormwater treatment adequacy and details on the future stormwater treatment facilities. The FDOT will implement proper best management practice (BMPs) during construction to ensure there are no violations to water quality standards.

No comments were received from the Federal Highway Administration (FHWA).

### ETAT Reviews: Water Quality and Quantity Issue: 3 found

**3** *Moderate* assigned 01/30/2011 by Madolyn Dominy, US Environmental Protection Agency

**Coordination Document:** No Selection

**Dispute Information:** N/A

**Identified Resources and Level of Importance:** Resources: Water quality, surface water

Level of Importance: These resources are of a high level of importance in the State of Florida. A moderate degree of effect is being assigned to this issue for the proposed project (ETDM #13102).

**Comments on Effects to Resources:** According to the project description, the majority of the proposed project is intended to be constructed on the SR 60 fill section and not within the waters of Tampa Bay. The only portions of the proposed project that would be constructed within the waters of Tampa Bay would be the proposed bridges where the main span and the western relief structures are located. The locations are outlined and referenced in the 2008 Feasibility Study. The study evaluated four (4) separate alternatives and one (1) interim staging option.

Old Tampa Bay is listed on the 303(d) list of impaired waters for coliforms, nutrients, mercury (fish consumption). There is also another water (Direct Runoff to Bay) listed for nutrients, total suspended solids, and biochemical oxygen demand (BOD).

Also located within proximity of the project is the Pinellas County Aquatic Preserve. See Special Designations Issue for more detail.

The Pinellas County Aquatic Preserve is listed as an Outstanding Florida Water (OFW). OFWs are provided the highest level of protection under the Florida Administrative Code (F.A.C.). Degradation of water quality in an OFW is prohibited except under certain circumstances. Pollutant discharges must not lower existing ambient water quality. Any activity within an OFW requiring a Florida Department of Environmental Protection (FDEP) Environmental Resource Permit (ERP) must be deemed to be clearly in the public interest. Additional stormwater retention and treatment requirements may be required. The project will need to coordination with FDEP and or the SWFWMD regarding specific permitting requirements relating to this OFW.

There may be special permitting requirements for stormwater management and treatment from project. Stormwater runoff and the increase of pollutants into surface waters as a result of the project and other point and nonpoint sources is a concern from a water quality standpoint. Stormwater runoff from urban sources, including roadways, carries pollutants such as volatile organics, petroleum hydrocarbons, heavy metals, and pesticides/herbicides. Proper stormwater conveyance, containment, and treatment will be required in accordance with state and federal regulations and guidelines. The project will need to coordination with FDEP and or the SWFWMD regarding specific permitting requirements relating to stormwater as well as other water quality issues.

The selection of alternatives and construction of the project should include an evaluation of avoidance and minimization strategies to prevent any further impairment to waters, including sedimentation during construction of the project and bridges. Proper stormwater management facilities will be required.

**Coordinator Feedback:** None

**3** *Moderate* assigned 01/29/2011 by C. Lynn Miller, Southwest Florida Water Management District

**Coordination Document:** Permit Required

**Dispute Information:** N/A

**Identified Resources and Level of Importance:** The project occupies two coastal watersheds: Old Tampa Bay (WBID 1558H) and Courtney Campbell Beach (WBID 1558J). The project is adjacent to three coastal watersheds: Ben T. Davis North (WBID 1558HB), Old Tampa Bay (1558I) and

Direct Runoff to Bay (WBID 1603).

Surface waters consist of Old Tampa Bay which is designated as Outstanding Florida Waters in Pinellas County.

The entire project is located in Class II waters designated for Shellfish Propagation or Harvesting; commercial crabbing occurs in Old Tampa Bay.

Water quality data are available for Old Tampa Bay from: EPA, FDEP, Hillsborough County Environmental Protection Commission and Pinellas County Department of Environmental Management.

The current (May 2009) Verified List of Impaired Waters includes the following TMDL information relevant to the District's permitting interests for this project:

1. Nutrients - The Direct Runoff to Bay watershed (WBID 1603) is impaired for nutrients.
2. Fecal coliform and/or coliform bacteria - The Ben T. Davis watershed (WBID 1558HB), Old Tampa Bay watershed (WBID 1558H) and the Courtney Campbell Beach watershed (WBID 1558J) are impaired for fecal coliform and/or coliform bacteria.
3. Mercury in fish - The two Old Tampa Bay watersheds (WBID 1558H and WBID 1558I) are impaired for mercury in fish.

There is an existing stormwater facility located on the FDOT property at the west terminus that may require relocation, alteration or modification of the ERP-permitted facility due to encroachment from this project.

The City of Clearwater's East AWWTP is located within 100 feet of the west terminus of the south alternative.

**Comments on Effects to Resources:** There are no dedicated stormwater treatment measures now serving most of the existing impervious area on the Causeway. The project will result in additional impervious area, and in the absence of stormwater collection and treatment measures, the project has the potential to generate increased sedimentation during construction and operation that may contribute to a delay in recovery of Impaired Waters and degrade water quality in both Outstanding Florida Waters and Class II waters. A review of available information in the 2008 Feasibility Report and the Advanced Notification did not provide conceptual information or commitments to incorporate stormwater treatment measures into the design of the project.

**Additional Comments (optional):** The SWFWMD has assigned a Degree of Effect based on their opinion of the potential of this project to result in identifiable impacts after fully complying with the SWFWMD's permitting processes or the effort associated with fully complying with the SWFWMD's proprietary interests and obligations.

F.A.C. 40D-4.052(13), Minor Roadway Safety Projects, and 40D-4.051(14), "Recreational Paths," provide for exemptions for the construction of minor roadway safety projects and recreational paths adjacent to roadways. Portions of this project may qualify for exemption provided that:

1. The paths are not located within wetlands or other surface waters as in the case of attaching the paths to existing structures not requiring separate piling supports, and provided that the causeway embankment is not widened to accommodate the recreational trail; and
2. Do not obstruct surface waters; such as the flood and return flows due to storm surge; and
3. Do not exceed 12 feet in width for bidirectional paths, if that were feasible; and
4. "Sidewalks" adjacent to roadways are no wider than six feet.

The SWFWMD strongly recommends a pre-application meeting with the Resource Regulation Department at the District's Tampa Service Office to discuss additional activities in Pinellas County and activities outside of the area covered by Environmental Resource Permit application #642193. A pre-application meeting was held for ERP application #642193 on 10 March 2010. The project area and activities anticipated in ERP application #642193 include:

1. Milling and resurfacing Courtney Campbell Causeway between Rocky Point and the Hillsborough/Pinellas County line,
2. Milling and resurfacing the existing frontage roads and extending the turn lane into the existing boat ramp and parking area on the north side of the Causeway,
3. Adding a shared use recreational path on the south side of the road in the project area,
4. Minor drainage, pedestrian, and bus stop improvements.

Several District projects have generated data that may be useful in the PD&E or design phases of the project. Below are listed the District project number, project title, and District Point of Contact:

1. W020 - SWIM Plan Implementation - Tampa Bay: Kris Kaufman,
2. W027 - Tampa Bay Estuary Program: Lizanne Garcia,
3. W200 - Old Tampa Bay Water Quality and Habitat Assessment: Lizanne Garcia;
4. W201 - Old Tampa Bay Upper Bay Model: Kris Kaufman
5. W239 - Old Tampa Bay Water Quality and Drainage Improvements: Nancy Norton; and
6. W240 - Old Tampa Bay Watershed Improvements: Xinjian Chen.

Other reports are available from the Tampa Bay Estuary Program and FDEP.

Project impacts may be reduced by:

1. Providing treatment of impervious areas that are currently untreated along most of the length of the Causeway;
2. Minimizing new impervious area where feasible by reducing the cross sections of project segments where limited distances are available between the existing guard rail and the cap of the bulkhead and/or the edge of wetland;
3. Using low-impact development strategies in project design; and
4. Retrofitting the existing stormwater treatment facility, if feasible, to increase treatment capacity in order to treat currently untreated impervious areas.

To prevent further degradation of impaired waters and to be consistent with federal and state laws and rules, the District will require stormwater



management systems that discharge directly or indirectly into impaired waters (e.g. Old Tampa Bay) to provide net improvement for the pollutants that contribute to the water body's impairment. To do this, a higher level of treatment is necessary to assure that the permit creates a net improvement in the pollutants that have caused or are contributing to the water body impairment.

If this project will require the acquisition of new right-of-way areas, the current rule for eminent domain noticing is 40D-1.603(9), FAC and requires the applicant to provide the noticing to the affected property owners. Additionally, any issued permit may include special conditions prohibiting construction until the FDOT provides evidence of ownership and control.

For ERP permitting purposes, the project area is located in the Tampa Bay Drainage Basin. The SWFWMD has assigned a pre-application file (PA #397318) for the purpose of tracking its participation in the ETDM review of this project. The pre-application file is maintained at the SWFWMD's Tampa Service Office. Please refer to the pre-application file when contacting SWFWMD regulatory staff regarding this project.

**Coordinator Feedback:** None

**2** *Minimal* assigned 01/26/2011 by Lauren P. Milligan, FL Department of Environmental Protection

**Coordination Document:** Permit Required

**Dispute Information:** N/A

**Identified Resources and Level of Importance:** The recreational, ecological, and commercial impacts of Old Tampa Bay and the Pinellas County Aquatic Preserve make them regionally significant environmental resources. Presently, the watershed within the project area is deemed as good to fair, with Old Tampa Bay being impaired for coliforms, mercury and nutrients. Stormwater treatment should be designed to maintain the natural pre-development hydroperiod and water quality, as well as to protect the natural functions of adjacent surface waters.

**Comments on Effects to Resources:** Every effort should be made to maximize the treatment of stormwater runoff from the proposed project, as stormwater discharges to the Pinellas County Aquatic Preserve, designated Outstanding Florida Waters (OFW) under section 62-302.700(9), F.A.C., and afforded a high level of protection under sections 62-4.242(2) and 62-302.700, F.A.C. Pursuant to section 373.414(1), F.S., direct impacts to these OFW waterbodies and associated wetlands must be demonstrated to be "clearly in the public interest" as part of the ERP permitting process. We recommend that the PD&E study include an evaluation of existing area stormwater treatment adequacy and details on the future stormwater treatment facilities. The permit applicant may be required to demonstrate that the proposed trail/bridge stormwater system meets the design and performance criteria established for the treatment and attenuation of discharges to OFWs, pursuant to rule 40D-4, F.A.C., and the SWFWMD Basis of Review for ERP Applications.

**Coordinator Feedback:** None

The following organization(s) were expected to but did not submit a review of the Water Quality and Quantity issue for this alternative: Federal Highway Administration

#### Coordinator Summary: Wetlands Issue

**4** *Substantial* assigned 02/17/2011 by FDOT District 7

**Comments:** The Florida Department of Transportation (FDOT) has evaluated comments from the US Environmental Protection Agency (USEPA), Florida Department of Environmental Protection (FDEP), the National Marine Fisheries Service (NMFS), the Southwest Florida Water Management District (SWFWMD), the US Army Corps of Engineers (USACE), and the US Fish and Wildlife Service (USFWS) and recommends a Degree of Effect of Substantial.

The USEPA noted that mitigation to provide enhanced or increased function of mangroves should be evaluated within the project area and the PD&E Study should identify wetland areas to be potentially impacted by the project.

The USFWS noted that with proper design and the right materials, the trail could have minimal impacts to wetlands, wildlife, and the natural environment. The FDEP noted that an ERP permit will be required from the SWFWMD for this project.

The entire project, with the exception of the two proposed trail bridges, will be constructed on the existing fill section that was used to construct the Causeway. The proposed recommended build alternative is located on the south side of the Causeway. Isolated mangroves (mainly white mangroves) are located on the south side of the Causeway waterward of the existing seawall in the riprap. The proposed bridges have the potential to impact seagrass within limited areas on the eastern end of each bridge. Mangroves and seagrasses provide habitat for numerous fish and wildlife for feeding, breeding, and nesting. The FDOT will prepare a Wetlands Evaluation and Biological Assessment Report (WEBAR) as part of the PD&E study. The WEBAR will assess existing wetlands and seagrass within the project limits. Permitting will be conducted with the appropriate regulatory agencies during design and prior to construction. The FDOT will take measures to minimize and/or avoid impacts to wetlands.

No comments were received from the Federal Highway Administration (FHWA).

#### ETAT Reviews: Wetlands Issue: 6 found

**3** *Moderate* assigned 01/30/2011 by Madolyn Dominy, US Environmental Protection Agency

**Coordination Document:** No Selection

**Dispute Information:** N/A

**Identified Resources and Level of Importance:** Resources: Wetlands, wetlands habitat, water quality

**Level of Importance:** These resources are of a high level of importance in the State of Florida and within the project area. A moderate degree of effect is being assigned to this issue for the proposed project.

**Comments on Effects to Resources:** A review of GIS analysis data in the EST for wetlands indicates that there are estuarine wetlands along the proposed project length. These include mangrove swamps, saltwater marshes, and seagrass beds.

Mangroves serve several important ecosystem functions. They provide nursery habitat for fishes, crustaceans, and shellfish and they provide food for several types of marine species. Both recreational and commercial fisheries in Florida are dependent upon healthy mangrove forests. Mangroves also provide shelter and nesting areas for coastal birds. Protecting mangrove acreage is critical, especially since most of the loss of acreage is due to human impact such as development and construction. As a result of dramatic changes in the Tampa Bay (Pinellas/Hillsborough County) area, a

significant amount of coastal wetlands acreage has been lost, including mangroves and salt marshes. Therefore, protection of the coastal wetlands is critical to fish habitat and other marine resources. Regulations to protect mangrove forests have been developed by both state and local agencies. These regulations must be met and consultation with other agencies such as the National Marine Fisheries Service may be required. Avoidance measures should be strongly considered for this project. Also, mitigation to provide enhanced or increased function should be strongly evaluated within the same general area.

Seagrass ecosystems fulfill vital ecological functions in the maintenance of estuaries and coastal marine environments. Their structure affects the flow of water locally, dampening the effects of waves and thereby altering erosion and sedimentation rates, nutrient and microorganism fluxes, and recruitment of larval stages of marine animals. Seagrass beds provide refuge from predators for small fish and crustaceans, and act as nurseries for many species.

Potential impacts for the project include, but are not limited to, loss of wetlands function, loss of wildlife habitat, degradation of water quality in wetlands, and reduction in flood storage and capacity. Another issue of concern is increased stormwater runoff and the increase of pollutants into surface waters and wetlands as a result of the project and other point and nonpoint sources.

The PD&E study should focus on identifying wetlands areas to be potentially impacted by the project. The PD&E study should include a delineation of wetlands; functional analysis of wetlands to determine their value and function; an evaluation of stormwater pond sites to determine their impact on wetlands; avoidance and minimization strategies for wetlands; and mitigation plans to compensate for adverse impacts. It is recommended that wetlands be avoided and that impact to these resources is strongly considered when determining project alternatives.

**Coordinator Feedback:** None

**3** Moderate assigned 01/29/2011 by C. Lynn Miller, Southwest Florida Water Management District

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**Coordination Document:** Permit Required

**Dispute Information:** N/A

**Identified Resources and Level of Importance:** Based on the SWFWMD 2008 wetland mapping and onsite inspections, several types of aquatic and wetland habitats occur along the Courtney Campbell Causeway within the expected impact area of the project. The name and the FLUCCS codes of these habitat types include: Bays and Estuaries (541), Tidal Flats (651), Shorelines (652), Mangrove Swamps (612), Saltwater Marshes (642) and Oyster Bars (654).

Tidal Flats and Shorelines provide important foraging and/or nesting and/or resting habitat for over 30 species of birds, including at least seven Listed Species. These habitats occur on both the north and south sides of the Causeway for a total length of approximately 8,550 feet. On the north side, significant Tidal Flats and Shoreline habitat occurs east of the boat ramp for a length of 3,217 feet. On the south side, these habitats occur at two primary locations: west of the east terminus for 1,633 feet, and east of the west terminus for a length of 3,700 feet. This second location includes the City of Clearwater beach area.

Dense Mangrove Swamp is established along the north side of the Causeway at three locations for a total length of approximately 10,445 feet: from Damascus Rd east for 4,278 feet; west of the boat ramp for 1,369 feet; and east of the boat ramp for approximately 4,798 feet. Moderately dense Mangrove Swamp occurs for a length of approximately 1,444 feet east of the first access road on the north side. The Mangrove Swamp, particularly the dense Mangrove Swamp, is important in that it provides flood surge protection, erosion protection and Listed Species habitat. Designated Environmental Conservation Areas/Bird Nesting Areas containing dense Mangrove Swamp are located on the north side of the Causeway. These areas are closed during the period January to August.

The Saltwater Marsh habitat is not common along the Causeway. It occurs slightly waterward of the dense and moderately dense Mangrove Swamp in the locations above mentioned. This habitat is important for wildlife and fish foraging, protection for juvenile fish and erosion protection of the Causeway fill.

Shallow Oyster Bars occur in the areas occupied by Tidal Flats and Shorelines. Oysters also are very prevalent on the hard substrate provided by the rocks present on the slopes of the Causeway and bridge fill areas and on the concrete chunks on the shoreline located just west of the Ben T. Davis Beach on the south side.

According to 2008-era imagery and mapping, seagrass beds are located along the project route. Acreage ranges from 22.3 acres to 64 acres within the 100-foot to 200-foot project buffers. The recovery of seagrass beds in Old Tampa Bay and Tampa Bay has been a major conservation focus since the 1970s and the District, together with municipalities surrounding the Bay and other agencies, have implemented significant conservation efforts since the early 1980s.

The amount of wetland acreage potentially directly affected by the project is difficult to quantify because the cross section of the facility may vary by location along the Causeway. However, it can be said that the North Alternatives (N1 and N2) likely would result in greater impacts to Mangrove Swamp than would the South Alternatives (S1 and S2) simply because there is more acreage of Mangrove Swamp on the north side of the Causeway than on the south side. On the other hand, the South Alternatives would likely result in more impact to Tidal Flats and Shorelines than would the North Alternatives because there is more acreage of Tidal Flats and Shorelines on the south side of the Causeway than on the north side.

Project impacts to the Mangrove Swamp, Tidal Flats and Shoreline habitats have the potential to result in adverse impacts to wildlife including Listed Species. Listed Species (FFWCC, November 2010) known to be present in the wetland and aquatic habitats within the impact zone of the project include: American oystercatcher (SSC), black skimmer (SSC), brown pelican (SSC), least tern (ST or State Threatened), little blue heron (SSC), piping plover (FT or Federally Threatened), reddish egret (SSC), roseate spoonbill (SSC), snowy egret (SSC) and tricolored heron (SSC) and wood stork (FE or Federally Endangered).

The entire project area is within the wood stork Core Foraging Area; habitat for this species is available in the Tidal Flats and Shoreline habitats, while roosting habitat for wood storks is also available in the dense Mangrove Swamp in the three locations above mentioned.

The project area is located within the USFWS Consultation Areas of the piping plover. The piping plover is listed by FWC as Federally Threatened. The species is listed by the USFWS as either Endangered or Threatened, depending upon the specific population involved. Foraging and roosting habitat for wintering piping plovers is available in the Tidal Flats and Shoreline habitats within 100 feet of the project and the species has been observed.

Designated Environmental Conservation Areas/Bird Nesting Areas containing very dense Mangrove Swamp are located on the north side of the Causeway. These areas are closed during the period January to August.

The project area is located within the USFWS Consultation Areas of the West Indian manatee. The West Indian manatee, listed by both USFWS and FWC as Endangered, is known to utilize the habitats in Old Tampa Bay. While indicated otherwise in the EST, there are FWC Manatee Protection Zones (information updated 9/17/09) located adjacent to the Causeway fill near the east project terminus on the north side for a length of approximately 0.94 mile. One zone is restricted to the navigational channel that parallels the Causeway and which requires a speed of no more than 25 mph in the period April 1 through November 15. The second zone requires slow speed in the period April 1 through November 15. There is another Manatee Protection Zone located adjacent to the Causeway fill near the west project terminus on the north side for a length of approximately 0.5 mile

A total of 95 acres of sovereign submerged lands are present within 100 feet of the project.

**Comments on Effects to Resources:** The project's impact on wetlands is highly dependent on the specific alignment and cross sections of the facility and the chosen construction methods and means. Physical impacts could include the elimination and/or significant disturbance of all or part of the Mangrove Swamp, Saltwater Marsh, Tidal Flats, and Shoreline habitats along the Causeway. As a result, there would be a corresponding loss of the functions and values now provided by the impacted wetlands, including flood surge protection, erosion protection of the Causeway fill, and Listed Species habitat. In addition to impacts due to physical disturbance, other impacts could occur to wetlands as a result of the discharge of untreated or under-treated stormwater runoff both during the construction and later operation phases of the project.

Also, if construction equipment is operating from the waterside of the seawall or the erosion protection rock wall along the Causeway, there is a high potential for the destruction of seagrass beds, oyster colonies and Tidal Flats habitat. Further, the fugitive discharge of sediment-containing runoff during construction could result in significant damage to the seagrass beds and oysters located in the immediate vicinity of the project and, depending on the tidal condition, at some distance from the project.

**Additional Comments (optional):** The SWFWMD has assigned a Degree of Effect based on their opinion of the potential of this project to result in identifiable impacts after fully complying with the SWFWMD's permitting processes or the effort associated with fully complying with the SWFWMD's proprietary interests and obligations.

F.A.C. 40D-4.052(13), Minor Roadway Safety Projects, and 40D-4.051(14), "Recreational Paths," provide for exemptions for the construction of minor roadway safety projects and recreational paths adjacent to roadways. Portions of this project may qualify for exemption provided that:

1. The paths are not located within wetlands or other surface waters as in the case of attaching the paths to existing structures not requiring separate piling supports, and provided that the causeway embankment is not widened to accommodate the recreational trail; and
2. Do not obstruct surface waters; such as the flood and return flows due to storm surge; and
3. Do not exceed 12 feet in width for bidirectional paths, if that were feasible; and
4. "Sidewalks" adjacent to roadways are no wider than six feet.

The SWFWMD strongly recommends a pre-application meeting with the Resource Regulation Department at the District's Tampa Service Office to discuss additional activities in Pinellas County and activities outside of the area covered by Environmental Resource Permit application #642193. A pre-application meeting was held for ERP application #642193 on 10 March 2010. The project area and activities anticipated in ERP application #642193 include:

1. Milling and resurfacing Courtney Campbell Causeway between Rocky Point and the Hillsborough/Pinellas County line,
2. Milling and resurfacing the existing frontage roads and extending the turn lane into the existing boat ramp and parking area on the north side of the Causeway,
3. Adding a shared use recreational path on the south side of the road in the project area,
4. Minor drainage, pedestrian, and bus stop improvements.

Wetland impacts can be reduced by the following:

- (1) Adjustment of the alignment to avoid direct impacts to the dense Mangrove Swamps on the north side of the Causeway,
- (2) Adjustment of the alignment to avoid direct impacts to the Tidal Flats and Shoreline habitats prevalent on the south side of the Causeway,
- (3) Implementation of strict controls over sediment transport off site during construction,
- (4) Restriction of vehicles and equipment to only those areas that must be utilized for construction and staging,
- (5) Implementing effective mitigation measures to compensate for wetland impacts;
- (6) Incorporation of stormwater treatment measures into the design of the project,
- (7) Retrofitting the existing stormwater treatment facility near the west project terminus to provide additional treatment capacity,
- (8) Incorporating wildlife-friendly features into stormwater facilities
- (9) Scheduling project activities to avoid the annual closure period (January - August) of the Bird Nesting Area on the north side of the Causeway,
- (10) If Least Terns are determined to nest in areas other than the designated Bird Nesting Area on the north side of the Causeway, scheduling project activities in those areas to avoid the April - May nesting period for that species, and
- (11) If Black Skimmers are determined to nest in areas other than the designated Bird Nesting Area on the north side of the Causeway, scheduling project activities in those areas to avoid the June - July nesting period for that species.

Because the importance of seagrass in Old Tampa Bay and Tampa Bay, it is recommended that particular effort be made to eliminate direct impact to seagrass beds. Impacts from fugitive turbidity and sedimentation should also be eliminated. Further, to assist in eliminating impacts to seagrass, it is recommended that updated, project-specific seagrass maps be prepared and used in the design and construction phases of the project. As of this review, the most easily accessible information is over two years old.

The dense mangroves along the Causeway are important in that they provide flood surge protection, erosion protection and Listed Species habitat. It is recommended that particular effort be made to eliminate impacts to the functions and values associated with mangroves. It is recommended that excessive trimming and total removal of mangroves be avoided.

Adequate and appropriate wetland mitigation activities may be required for unavoidable wetland and surface water impacts associated with the project. The project mitigation needs may be addressed in the FDOT Mitigation Program (Subsection 373.4137, F.S.) which requires the submittal of anticipated wetland and surface water impact information to the SWFWMD. This information is utilized to evaluate mitigation options, followed by nomination and multi-agency approval of the preferred options. These mitigation options typically include enhancement of wetland and upland habitats within existing public lands, public land acquisition followed by habitat improvements, and the purchase of private mitigation bank credits. The SWFWMD may choose to exclude a project in whole or in part if the SWFWMD is unable to identify mitigation that would offset wetland and surface water impacts of the project. Under this scenario, the SWFWMD will coordinate with the FDOT on which impacts can be appropriately mitigated through the program as opposed to separate mitigation conducted independently. Depending on the quantity and quality of the proposed wetland impacts, the SWFWMD may propose purchasing credits from a mitigation bank and/or pursue and propose alternative locations for mitigation. For ERP purposes of mitigating any adverse wetland impacts within the same drainage basin, the project is located within the Tampa Bay Drainage. The SWFWMD requests that the FDOT continue to collaborate on the potential wetland impacts as this project proceeds into future phases, and include the associated impacts on FDOT's annual inventory.

If this project will require the acquisition of new right-of-way areas, the current rule for eminent domain noticing is 40D-1.603(9), FAC and requires the applicant to provide the noticing to the affected property owners. Additionally, any issued permit may include special conditions prohibiting construction until the FDOT provides evidence of ownership and control.

For ERP permitting purposes, the project area is located in the Tampa Bay Drainage. The SWFWMD has assigned a pre-application file (PA #397318) for the purpose of tracking its participation in the ETDM review of this project. The pre-application file is maintained at the SWFWMD's Tampa Service Office. Please refer to the pre-application file when contacting SWFWMD regulatory staff regarding this project.

**Coordinator Feedback:** None

**N/A** N/A / No Involvement assigned 01/28/2011 by John Fellows, US Army Corps of Engineers

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**Coordination Document:** To Be Determined: Further Coordination Required

**Dispute Information:** N/A

**Identified Resources and Level of Importance:** Please see my comments under navigation - they are the same as for wetlands.

**Comments on Effects to Resources:** Please see my comments under navigation - they are the same as for wetlands.

**Coordinator Feedback:** None

**4** Substantial assigned 01/27/2011 by Jane Monaghan, US Fish and Wildlife Service

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**Coordination Document:** No Selection

**Dispute Information:** N/A

**Identified Resources and Level of Importance:** Mangroves, seagrass beds and saltwater marshes are all vitally important to the fish and wildlife that depend on them.

**Comments on Effects to Resources:** The proposed project has the potential to impact seagrass beds and mangroves. If impacts to these natural habitats cannot be avoided the project should be redesigned. Given the purpose and need stated for this new trail, it would not be beneficial to the public to impact these types of habitats in order to have a quality recreational experience. Seagrass beds and mangroves are vitally important nursery and foraging areas for many species of fish, crustaceans, mollusks and at least one marine mammal, the Florida Manatee and five federally listed sea turtle species. The ETDM review screens indicates 15-24 acres of seagrass beds within 100-200 feet of the proposed trail. Depending on the final design, current seagrass surveys and mapping may be required.

With the proper design and the right materials, this trail could have minimal impacts to wetlands, wildlife and the natural environment. Placement of the trail in the wrong area, using the wrong materials and poor design will result in the destruction of seagrass beds, mangrove habitat, shorebird loafing areas and formal consultation with the USFWS on the Florida manatee.

Measures taken to avoid impacts to mangroves, seagrasses and shorebirds could be highlighted along the trail using interpretive signage. Observation areas along the elevated portions of the trail could be incorporated into the design to increase public education about manatees, seagrass beds, mangroves and shorebirds.

It appears that the western half of the project may be within the Pinellas County Aquatic Preserve boundary. If this is the case, interpretive signage could also inform the public about this preserve and the role that preservation serves in our environment.

The USFWS would like to work closely with the project planners as this project moves forward. This trail has the potential to serve the public not only as a place to recreate but also as a place to enjoy watching wildlife without disturbing their feeding, breeding or sheltering needs.

**Coordinator Feedback:** None

**3** Moderate assigned 01/27/2011 by David A. Rydene, National Marine Fisheries Service

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**Coordination Document:** PD&E Support Document As Per PD&E Manual

**Dispute Information:** N/A

**Identified Resources and Level of Importance:** Old Tampa Bay and Safety Harbor, which contain estuarine and marine habitats such as seagrass and mangrove used by federally-managed fish species and their prey.

**Comments on Effects to Resources:** NOAA's National Marine Fisheries Service (NMFS) has reviewed the information contained in the

Environmental Screening Tool for ETDM Project # 13102. The Florida Department of Transportation (FDOT) District 7 proposes the construction of a multi-use trail along the SR 60 Courtney Campbell Causeway in Hillsborough County and Pinellas County, Florida. Per a phone conversation with Robin Rhinesmith of FDOT District 7 on January 27, 2011, and a follow-up email, the two alternatives for a trail on the north side of the causeway are no longer under consideration. The remaining two south side trail alternatives would parallel the roadway. The trail would span the water at three points. Crossings would be accomplished by either widening the existing bridge structures or constructing independent bridge structures adjacent to the existing ones. The following comments assess only the two south side trail alternatives.

NMFS staff conducted a site inspection of the project area on December 20, 2010, to assess potential concerns related to living marine resources within Old Tampa Bay and Safety Harbor. The lands adjacent to the proposed project are principally estuarine habitats associated with Tampa Bay, a public beach, and commercial properties at either end of the causeway. It appears that the project could directly impact NMFS trust resources (i.e. mangroves and/or seagrass). Some fringing mangroves occur along the causeway's southern shoreline. Seagrass beds occur adjacent to the shoreline at various points along the south side of the causeway. Certain estuarine habitats within the project area are designated as essential fish habitat (EFH) as identified in the 2005 generic amendment of the Fishery Management Plans for the Gulf of Mexico. The generic amendment was prepared by the Gulf of Mexico Fishery Management Council as required by the 1996 amendment to the Magnuson-Stevens Fishery Conservation and Management Act (Magnuson-Stevens Act). Mangroves have been identified as EFH for juvenile, subadult and adult red drum, gray snapper, schoolmaster, and cubera snapper, and juvenile goliath grouper, yellowtail snapper, and dog snapper by the Gulf of Mexico Fishery Management Council under provisions of the Magnuson-Stevens Act. Seagrasses have been identified as EFH for juvenile and subadult penaeid shrimp, juvenile and adult stone crab, postlarval, juvenile, subadult and adult red drum, juvenile and adult schoolmaster and mutton snapper, and juvenile gag, goliath grouper, red grouper, black grouper, yellowfin grouper, Nassau grouper, lane snapper, dog snapper, yellowtail snapper, and cubera snapper.

Federal agencies which permit, fund, or undertake activities which may adversely impact EFH are required to consult with NMFS and, as a part of the consultation process, an EFH Assessment must be prepared to accompany the consultation request. Regulations require that EFH Assessments include:

1. a description of the proposed action;
2. an analysis of the effects (including cumulative effects) of the proposed action on EFH, the managed fish species, and major prey species;
3. the Federal agency's views regarding the effects of the action on EFH; and
4. proposed mitigation, if applicable.

Provisions of the EFH regulations [50 CFR 600.920(c)] allow consultation responsibility to be formally delegated from federal to state agencies, including FDOT. Whether EFH consultation is undertaken by the federal agency (e.g. Federal Highway Administration) or FDOT, it should be initiated as soon as specific project design and construction impact information is available. EFH consultation can be initiated independent of other project review tasks or can be incorporated in environmental planning documents. Upon review of the EFH Assessment, NMFS will determine if it is necessary to provide EFH Conservation Recommendations for the project.

Between the two south side alternatives, NMFS cannot make a determination until potential seagrass impacts for the two bridging options have been assessed. Seagrass surveys should be conducted during the prime seagrass growing season between June 1 and September 30. These surveys can be undertaken as part of the design/build phase. NMFS strongly discourages any impacts to seagrass habitat as the success of compensatory mitigation measures for seagrass loss are considered too uncertain given the current state of the art.

NMFS recommends that stormwater treatment systems be upgraded to prevent degraded water from entering estuarine habitats within the system. In addition, best management practices should be employed during trail construction to prevent siltation of estuarine habitats.

**Coordinator Feedback:** None

**3** *Moderate* assigned 01/26/2011 by Lauren P. Milligan, FL Department of Environmental Protection

**Coordination Document:** Permit Required

**Dispute Information:** N/A

**Identified Resources and Level of Importance:** The National Wetlands Inventory GIS report indicates that a total of 665.5 acres (72%) of Old Tampa Bay estuarine wetlands occur within the 500-ft. project buffer zone. Moreover, 38.3 acres of continuous seagrasses, 87.6 acres of discontinuous seagrass beds and 12.9 acres of mangrove swamp occur within the 500-ft. buffer zone. The project will traverse the Pinellas County Aquatic Preserve, designated Outstanding Florida Waters (OFW) under section 62-302.700(9), F.A.C., and afforded a high level of protection under sections 62-4.242(2) and 62-302.700, F.A.C.

**Comments on Effects to Resources:** An Environmental Resource Permit (ERP) will be required from the Southwest Florida Water Management District - the ERP applicant will be required to eliminate or reduce the proposed wetland resource impacts of trail/bridge construction to the greatest extent practicable:

- Minimization should emphasize avoidance-oriented corridor alignments, wetland fill reductions via pile bridging and steep/vertically retained side slopes, and median width reductions within safety limits.
- Wetlands should not be displaced by the installation of stormwater conveyance and treatment swales; compensatory treatment in adjacent uplands is the preferred alternative.
- After avoidance and minimization have been exhausted, mitigation must be proposed to offset the adverse impacts of the project to existing wetland functions and values. Significant attention is given to forested wetland systems and seagrass beds, which are difficult to mitigate.
- The cumulative impacts of concurrent and future transportation improvement projects in the vicinity of the subject project should also be addressed.

**Coordinator Feedback:** None

The following organization(s) were expected to but did not submit a review of the Wetlands issue for this alternative: Federal Highway Administration

**Coordinator Summary: Wildlife and Habitat Issue**

**4** *Substantial* assigned 02/17/2011 by FDOT District 7

**Comments:** The Florida Department of Transportation (FDOT) has evaluated comments from the Southwest Florida Water Management District



(SWFWMD), the Florida Fish and Wildlife Conservation Commission (FFWCC), and the US Fish and Wildlife Service (USFWS) and recommends a Degree of Effect of Substantial.

The project is located within an area that has the potential for protected species involvement that includes manatees, sea turtles, wood storks, and wading and shore bird species.

The USFWS recommended that the trail be constructed of permeable material along the causeway rather than asphalt. The USFWS recommends against using asphalt in natural areas and areas where erosion will be a constant problem. The USFWS noted that with proper design and the right materials, the trail could have minimal impacts to wetlands, wildlife, and the natural environment. The USFWS also noted that the western half of the project is within the Pinellas County Aquatic Preserve and if so, interpretive signage could be added to inform the public about this preserve and the role that preservation serves in our environment. The USFWS also recommended removal of the Friendship Trail Bridge on Gandy Boulevard and any habitat restoration that might be needed as a result of the old bridge and removal of the bridge could be considered a possible mitigation option, if feasible.

The FFWCC recommended land acquisition and restoration of appropriate tracts adjacent to existing public lands near the project area or tracts placed under conservation easement or located adjacent to large areas of jurisdictional wetlands that currently serve as regional core habitat areas.

The recommended build alternative is along the south side of the Causeway where there are minimal to no wetlands with isolated mangroves that are likely to be located within the project's limits of construction. The entire trail, with the exception of the proposed bridges, will be constructed on the existing fill section. The FDOT will commit to use proper best management practices (BMPs) during construction. The FDOT will adhere to the Standard Manatee Conditions for In-Water Work during construction to ensure there is no harm to manatees or other marine species. No USFWS Critical Habitat is documented within the project area. Portions of the Causeway where the proposed trail will be located are currently utilized by motor vehicles, pedestrians and other recreational users. The existing beach areas are susceptible to high pedestrian and vehicular traffic throughout much of the year. There will be no land use changes as a result of the construction of the proposed trail. The project will be constructed within current FDOT transportation right-of-way (ROW). The FDOT will prepare a Wetland Evaluation and Biological Assessment Report (WEBAR) during the PD&E study. This report will assess potential species, existing habitat, and potential essential fish habitat (EFH) within the project area. This report and the FDOT's findings will be coordinated with the USFWS and NMFS.

No comments were received from the Federal Highway Administration (FHWA).

#### ETAT Reviews: Wildlife and Habitat Issue: 3 found

**2** *Minimal* assigned 01/29/2011 by C. Lynn Miller, Southwest Florida Water Management District

**Coordination Document:** Permit Required

**Dispute Information:**N/A

**Identified Resources and Level of Importance:** This section of the report deals with upland habitat and wildlife species as wetland habitats and wildlife are discussed under the "Wetlands" issue.

Upland habitat available for wildlife is limited to a patch of forested area located adjacent to the project alignment extending from the Causeway to Bayshore Dr. The entire patch occupies approximately 10 acres, of which about 4.5 acres are located with 500 feet of the project. Plant communities include remnant pine flatwoods and live oak hammock. The property is adjacent to the Pinellas County Cooper's Point conservation lands which are primarily Mangrove Swamp, Tidal Flats and Shoreline habitats. The interface of the patch of upland habitat with the estuarine wetland habitats increases the wildlife value of both habitats. Listed Species expected in the available upland habitat within 200 feet of the project include Eastern indigo snake (FT), gopher tortoise (ST), and Sherman's fox squirrel (SSC).

**Comments on Effects to Resources:** The 2008 Feasibility Report shows the terminal segment of the project located between the parking lot on the FDOT property and the patch of forested upland to the northeast. It appears that some encroachment on the patch occurred to accommodate the past parking lot expansion. Further encroachment on the forested patch is possible as a result of the project. The potential impacts from the project on wildlife and habitat may include the further elimination of remaining wildlife habitat, resulting in a further decline in urban wildlife populations, including three Listed Species.

**Additional Comments (optional):** The SWFWMD has assigned a Degree of Effect based on their opinion of the potential of this project to result in identifiable impacts after fully complying with the SWFWMD's permitting processes or the effort associated with fully complying with the SWFWMD's proprietary interests and obligations.

Habitat damage and direct impacts to wildlife can be eliminated by re-aligning the project to avoid encroachment on the forested upland patch. Impacts can be reduced by minimizing project cross section in areas where there are remnant patches of native habitat; strictly limiting construction equipment to the actual construction zones and to pre-approved staging areas; and by implementing appropriate upland habitat restoration measures following construction.

**Coordinator Feedback:** None

**4** *Substantial* assigned 01/27/2011 by Jane Monaghan, US Fish and Wildlife Service

**Coordination Document:** No Selection

**Dispute Information:**N/A

**Identified Resources and Level of Importance:** Federally listed species and the ecosystems upon which they depend. Other federal trust resources such as migratory birds and wetlands are also discussed.

**Comments on Effects to Resources:** The project involves the construction of a multi-use trail adjacent to the Courtney Campbell Causeway. The trail would be approximately 7 miles long and 12-16 foot wide. The trail is designed to be used by pedestrians, bicyclists and other recreational users. The portion of the trail along the causeway would be constructed of asphalt and the above water structures would consist of solid concrete. The alternatives include placement of the trail on the North or South side of the causeway and widening the bridge structures or creating new independent structures.

Florida Manatee- This area is heavily utilized by manatees year round. Several manatee sanctuaries and refuges are located within Tampa Bay. No critical habitat for manatees has been designated within the project footprint. On the East end of the proposed trail, an important manatee area has been designated and two special conditions will apply : dedicated manatee observers during project construction and no night-time clamshell dredging. The Standard Manatee In-water Construction Conditions, 2009, for the bridge work will also apply to this project.

All of the maps and guidelines referenced can be found on [www.MyFWC.com](http://www.MyFWC.com), under imperiled species and manatees. The ETDM review screen indicates the presence of seagrass beds within 100 and 200 feet of the proposed trail. Impacts to seagrass as a result of this project may be avoidable if the design of the structure is done properly. Guidelines dated August 2001 for structures over submerged aquatic vegetation (SAV) or mangroves can be found on our website or the Army COE website and should be followed. Solid concrete should not be used over the shallow portions of the waterway where seagrass is likely to be present and shading may be a problem. Materials that allow sunlight to pass through and into the water column are preferred. Impacts to seagrass or mangrove habitat as a result of this project are not acceptable and would not serve the purpose of this project.

Sea Turtles-Five species of sea turtles can be found within the action area: loggerhead, leatherback, green, Kemp's Ridley and hawksbill. All of these species depend on U.S. coastal waters for foraging and migration during some stage of their life cycle. Mangroves and seagrass beds provide important feeding, breeding and sheltering areas for sea turtles. Impacts to these vitally important habitats should be avoided.

Wood Storks- The project falls within the core foraging areas for at least two active wood stork colonies at this time. Impacts to wetlands within these areas should be avoided. If avoidance is not possible, compensation of suitable foraging habitat will be required.

Piping Plover-No critical habitat for this species has been designated within the project footprint. However, this species may be present within the action area and may utilize the beaches for foraging and loafing. Red knots and Wilson's plovers have been documented along this causeway recently. Many species of shorebirds utilize this area, sometimes in very significant numbers, such as oystercatchers, black skimmers, dunlins, short-billed dowitchers, semi-palmated plovers, willets, sanderlings, ruddy turnstones and many species of terns. Because this area is so important to the shorebirds, the placement of the new trail needs to be coordinated with potential shorebird feeding and loafing areas. Pedestrians, dogs and bicycles will result in the flushing of shorebird flocks if the approach is too close. Dogs should remain on leash if they are allowed on this new trail. Known shorebird nesting areas should be mapped. Surveys during nesting season may also be warranted for shorebirds and wading birds that may utilize the beach or mangrove areas for nesting. Surveys should be done before the final placement of the trail is decided. No take of migratory birds is allowed under the Migratory Bird Treaty Act.

The trail should be constructed of permeable material along the causeway rather than asphalt. There are new materials out on the market being used by State and National Parks in Florida that allow storm water to percolate downward rather than run off the surface. These materials are also suitable for bicycles, wheelchairs, etc. The FWS recommends against using asphalt in natural areas and areas where erosion will be a constant problem.

**Additional Comments (optional):** Removal of the Friendship Trail Bridge on Gandy Blvd and any habitat restoration that might be needed as a result of the old bridge and removal of the bridge could be considered as a possible mitigation option, if feasible.

The Campbell Causeway access road should be evaluated for the placement of this trail. It would seem prudent to co-locate the trail along this access road and impact an area that has already been compromised rather than create a new trail through sensitive habitats that are difficult to mitigate.

**Coordinator Feedback:** None

**4** Substantial assigned 01/27/2011 by Scott Sanders, FL Fish and Wildlife Conservation Commission

**Coordination Document:** To Be Determined: Further Coordination Required

**Dispute Information:** N/A

**Identified Resources and Level of Importance:** The Habitat Conservation Scientific Services Section of the Florida Fish and Wildlife Conservation Commission (FWC) has coordinated an agency review of ETDM #13102, Hillsborough and Pinellas Counties, and provides the following comments related to potential effects to fish and wildlife resources on this Programming Phase project.

The Project Description Summary states that this project involves the construction of a multi-use trail along Courtney Campbell Causeway from the vicinity of the proposed Bayshore Trail extension (Bayshore Boulevard at SR 60) in Pinellas County to west of the Ben T. Davis Beach entrance in Hillsborough County, a distance of 7.473 miles. The proposed trail would be 12 feet wide, paved with asphalt, with 2- to 5-foot-wide shoulders. There are two bridges in the project limits, and both are too narrow to accommodate the trail: Structure 1 (Bridge No. 150138), a 0.1-mile-long bridge near the western end of the causeway, and Structure 2 (Bridge No. 100301), a 0.6-mile-long bridge approximately 3 miles east of Structure 1. Structure 2 includes a navigation span with 43.5 feet of clearance at Mean High Water. To allow the trail to continue across the causeway uninterrupted, new 16-foot-wide bridges would be constructed parallel to the existing bridges. The Project Development and Environment (PD&E) Study will examine the alternatives of constructing the trail and bridges either north or south of SR 60.

The project area was evaluated for potential fish, wildlife, and habitat resources within 500 feet of the proposed alignment. Our assessment reveals that most of the causeway is Right-of-way (ROW) for SR 60, containing the 4-lane highway, cleared areas with planted palm trees, and parallel beach access roads in many locations. However, many areas of the causeway shoreline are fringed with salt marsh and mangrove vegetation, providing intertidal habitat for Tampa Bay's fish and wildlife. There is also 125.9 acres of continuous and discontinuous seagrass beds in the assessment area, mostly occurring immediately adjacent to the shorelines.

Based on range and preferred habitat type, the following species listed by the Federal Endangered Species Act and the State of Florida as Federally Endangered (FE), Federally Threatened (FT), State-Threatened (ST), or State Species of Special Concern (SSC) may occur along the project area: Eastern indigo snake (FT), American alligator (FT), loggerhead sea turtle (FT), green sea turtle (FE), Kemp's ridley sea turtle (FE), leatherback sea turtle (FE), American oystercatcher (SSC), black skimmer (SSC), brown pelican (SSC), least tern (ST), little blue heron (SSC), tri-colored heron (SSC), reddish egret (SSC), snowy egret (SSC), roseate spoonbill (SSC), white ibis (SSC), wood stork (FE), and Florida manatee (FE). An active bald eagle nest (PI037) is located approximately 0.5 miles north of the causeway's west end, on the Cooper's Point tract.

The GIS analysis revealed several specific characteristics associated with lands along the project alignment that provide an indication of potential habitat quality or sensitivity that will require field studies to verify the presence or absence of listed wildlife species and the quality of wildlife habitat resources. The project is within the Pinellas County Aquatic Preserve. On the north side of the causeway's west end is the 84-acre Cooper's Point tract, conservation lands owned and managed by Pinellas County. The project is in U.S. Fish and Wildlife Service Consultation Areas for the Piping Plover and Manatee, and is in the core foraging area of three wood stork rookeries.

Primary wildlife issues associated with this project include: the potential for in-water work associated with bridge construction to adversely impact manatees, sea turtles, seagrass beds, or other aquatic resources, particularly at Structure 1, where seagrasses extend continuously beneath the bridge; potential habitat loss from encroachment of the construction into mangroves, salt marsh, or upland hammock communities; potential adverse

effects to a moderate number of species listed by the Federal Endangered Species Act as Endangered or Threatened, or by the State of Florida as Threatened or Species of Special Concern; and potential water quality degradation as a result of additional stormwater runoff from the expanded impervious surface draining into adjacent wetlands and Tampa Bay.

**Comments on Effects to Resources:** Based on the project information provided, we believe the direct and indirect effects of this project could be moderate to substantial, depending upon the measures taken to avoid and minimize loss of wetland and seagrass habitat.

**Additional Comments (optional):** We recommend that the PD&E Study address natural resources by including the following measures for conserving fish and wildlife and habitat resources that may occur within and adjacent to the project area. Plant community mapping and wildlife surveys for the occurrence of wildlife species listed by the Federal Endangered Species Act as Endangered or Threatened, or by the State of Florida as Threatened or Species of Special Concern should be performed along the ROW. Based on the survey results, a plan should be developed to address direct, indirect, and cumulative effects of the project on wildlife and habitat resources, including listed species. Avoidance, minimization, and mitigation measures should also be formulated and implemented. Equipment staging areas should be located in previously disturbed sites to avoid habitat destruction or degradation. A compensatory mitigation plan should include the replacement of any wetland, upland, or aquatic habitat lost as a result of the project. This could be achieved by purchasing land, or securing conservation easements over lands adjacent to existing public lands, and by habitat restoration. Replacement habitat for mitigation should be type for type, as productive, and equal to or of higher functional value. We recommend land acquisition and restoration of appropriate tracts adjacent to existing public lands near the project area, or tracts placed under conservation easement or located adjacent to large areas of jurisdictional wetlands that currently serve as regional core habitat areas. Please notify us immediately if the design, extent, or footprint of the current project is modified, as we may choose to provide additional comments and/or recommendations.

It will be important to avoid and minimize effects on the Florida manatee and sea turtles during removal of the old bridge structures, construction of the new bridges, or other in-water work. Since no information was provided in terms of seasonality of bridge construction, the duration of project work, methods for constructing the bridge, and any dredging or other in-water work that may be required, it would be premature for us to recommend specific avoidance and minimization measures for the manatee and sea turtles at this time. However, possible manatee protection measures that may be required by our agency include Standard Manatee Conditions for In-Water Work, monitoring of turbidity barriers, manatee entrapment avoidance measures, exclusionary grating on culverts, presence of manatee observers during in-water work, a defined or limited construction window, and no nighttime work. Further coordination with our agency is important, and will be necessary to develop customized or site-specific measures for this project. For technical assistance and coordination on manatees and sea turtles, respectively, please contact Ms. Mary Duncan and Dr. Robbin Trindell of our Imperiled Species Management Section in Tallahassee at (850) 922-4330 very early in the planning process for the PD&E Study.

We appreciate the opportunity to provide input on multi-use trail design and the conservation of fish and wildlife resources. Please contact Brian Barnett at (850) 528-6316 or email [brian\\_barnett@urscorp.com](mailto:brian_barnett@urscorp.com) to initiate the process for further overall coordination on this project.

**Coordinator Feedback:** None

The following organization(s) were expected to but did not submit a review of the Wildlife and Habitat issue for this alternative: Federal Highway Administration

#### ETAT Reviews and Coordinator Summary: Cultural Issues

#### Coordinator Summary: Historic and Archaeological Sites Issue

3 Moderate assigned 02/17/2011 by FDOT District 7

**Comments:** The Florida Department of Transportation (FDOT) has evaluated comments from the Miccosukee Tribe of Indians of Florida, the Federal Highway Administration (FHWA), the Southwest Florida Water Management District (SWFWMD), the Florida Department of State (SHPO), and the Seminole Tribe of Florida and recommends a Degree of Effect of Moderate.

The FHWA, Seminole Tribe of Florida, SHPO, and Miccosukee Tribe of Indians of Florida recommended that a Cultural Resource Assessment Survey (CRAS) for archaeology and historic structures be prepared for this project. The SHPO also recommended that the CRAS include appropriate underwater survey to identify, document, and evaluate any submerged cultural resources. The Seminole Tribe of Florida Tribal Historic Preservation Officer (STOF-THPO) noted that they would like to review a CRAS before commenting on direct effects to archaeological sites in the project area. The Miccosukee Tribe of Indians of Florida commented that there are no recorded archaeological sites, including burial mounds, reported near this project; a CRAS will need to be done to ascertain if there are any archaeological sites within the project boundaries. If no impacts are found, then no further consultation is necessary.

The FDOT has prepared a CRAS as part of the PD&E Study. One archaeological site, the Ben T. Davis Municipal Beach Site (8HI456) and one historic resource, a 1957 Masonry Vernacular style building (8PI11966), are located within the project area of potential effect (APE). The Ben T. Davis Municipal Beach Site is comprised of re-deposited dredge fill and not considered eligible for listing in the National Register of Historic Places. The 1957 Masonry Vernacular style building is also not eligible for listing in the NRHP based on commonality of type, lack of significant historical associations, and alterations.

#### ETAT Reviews: Historic and Archaeological Sites Issue: 5 found

N/A N/A / No Involvement assigned 01/29/2011 by C. Lynn Miller, Southwest Florida Water Management District

**Coordination Document:** No Involvement

**Dispute Information:** N/A

**Identified Resources and Level of Importance:** None found.

**Comments on Effects to Resources:** None found.

**Coordinator Feedback:** None

2 Minimal assigned 01/17/2011 by Linda Anderson, Federal Highway Administration

**Coordination Document:** PD&E Support Document As Per PD&E Manual

**Dispute Information:** N/A

**Identified Resources and Level of Importance:** Per Florida Master Site Files, no FMSF historic cemeteries, bridges, standing structures, archaeological and historical sites, resource groups, or NRHP-eligible structures or sites within the 500' buffer.

**Comments on Effects to Resources:** Two Cultural Resource Assessment Surveys within the 100' buffer have occurred, but it is difficult to tell whether

these surveys covered the entire project APE or whether there are structures adjacent to the APE that may have aged into the historical category since the most recent CRAS.

A CRAS is required.

**Coordinator Feedback:** None

**3** *Moderate* assigned 01/06/2011 by Elliott York, Seminole Tribe of Florida

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**Coordination Document:** No Selection

**Dispute Information:**N/A

**Identified Resources and Level of Importance:** Due to the absence of a systematic Cultural Resources Assessment Survey for the proposed project corridor, the STOF-THPO would like to request a CRAS be conducted in order to determine effects, if any, to archaeological sites within the project area.

**Comments on Effects to Resources:** The STOF-THPO would like to review a CRAS before commenting on possible effects to archaeological sites in the project area.

**Coordinator Feedback:** None

**3** *Moderate* assigned 12/29/2010 by Ginny Leigh Jones, FL Department of State

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**Coordination Document:** PD&E Support Document As Per PD&E Manual

**Dispute Information:**N/A

**Identified Resources and Level of Importance:** FDOT RCI Bridges:

The GIS Analysis revealed that there are 2 bridges within 100 feet of the proposed project area. There are an additional three bridges located within one mile of the project area, but these are not closer than 2,640 feet from the project area. None of the bridges are of historic age.

Florida Site File Historic Standing Structures:

The GIS Analysis of the proposed project area revealed 12 historic standing structures within one mile of the project area (but none are closer than 2,640 feet). None of the structures have been evaluated by the SHPO for their eligibility for the National Register of Historic Places (NRHP).

Florida Site File Archaeological and Historical Sites:

The GIS Analysis revealed that there are 12 archaeological sites recorded within one mile of the proposed project area. None of these sites are closer than 2540 feet of the project area. Nine of the sites have not been evaluated for their eligibility for the NRHP. Two have been evaluated by the SHPO as being not eligible and one was determined to have insufficient information to make an evaluation.

**Comments on Effects to Resources:** Effects on Resources

FDOT RCI Bridges:

There will be no effects on historic bridges within one mile of the project area.

Florida Site File Historic Standing Structures:

Since the recorded historic standing structures are located fair distance from the proposed project area, it is unlikely that they will be affected by the proposed project.

Florida Site File Archaeological and Historical Sites:

The significant distance between the proposed project area and the recorded archaeological sites makes it unlikely that the resources will be affected by the proposed project.

**Additional Comments (optional):** A GIS analysis revealed that there have been 2 cultural resources surveys completed within 100ft of the proposed project area. Both of the surveys were county-wide surveys. Because the project area has not been thoroughly surveyed it is our recommendation that prior to initiating any project-related land clearing or ground disturbing activities within the project area it should be subjected to a systematic archaeological and architectural survey. All historic-age resources, including potential historic districts, within the area of potential effects should be documented and assessed for NRHP eligibility. The resultant survey report shall conform to the specifications set forth in Chapter 1A-46 Florida Administrative Code and need to be forwarded to this agency for review and comment. Also since the project description provides information that there may be some construction of bridges through Tampa Bay, this office recommends that the survey include appropriate underwater survey to identify, document, and evaluate any submerged cultural resources.

**Coordinator Feedback:** None

**2** *Minimal* assigned 12/22/2010 by Steve Terry, Miccosukee Tribe of Indians of Florida

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**Coordination Document:** No Selection

**Dispute Information:**N/A

**Identified Resources and Level of Importance:** There are no recorded archaeological sites reported near this project. However, a Cultural Resources Survey will need to be done to ascertain if there are any archaeological sites within the project boundaries.

**Comments on Effects to Resources:** Once a Cultural Resources Survey has been done, then effects, if any, to archaeological sites can be ascertained.

**Additional Comments (optional):** If the Cultural Resources Survey shows there are no archaeological sites that will be impacted by this project, then no further consultation is necessary. However, if the Cultural Resources Survey does show that archaeological sites will be impacted by this project, then further consultation with the Miccosukee Tribe should be done.

**Coordinator Feedback:** None

#### Coordinator Summary: Recreation Areas Issue

**1** *Enhanced* assigned 02/17/2011 by FDOT District 7

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**Comments:** The Florida Department of Transportation (FDOT) has evaluated comments from the Florida Department of Environmental Protection (FDEP), the US Environmental Protection Agency (USEPA), and the Southwest Florida Water Management District (SWFWMD) and recommends a Degree of Effect of Enhanced.



The project will be constructed within current FDOT transportation right-of-way (ROW). The proposed trail will provide improved recreational opportunities along the Causeway, including fishing, biking, hiking, and observation of wildlife within the area. This project is also a component in connecting already existing trails in Pinellas County to trails in Hillsborough County and throughout the Tampa Bay region. No impacts to any recreation resources would occur due to construction of the trail.

No comments were received from the National Park Service (NPS) or the Federal Highway Administration (FHWA).

#### ETAT Reviews: Recreation Areas Issue: 3 found

0 None assigned 01/30/2011 by Madolyn Dominy, US Environmental Protection Agency

**Coordination Document:** No Selection

**Dispute Information:**N/A

**Identified Resources and Level of Importance:** None found.

**Comments on Effects to Resources:** None found.

**Coordinator Feedback:** None

0 None assigned 01/29/2011 by C. Lynn Miller, Southwest Florida Water Management District

**Coordination Document:** No Involvement

**Dispute Information:**N/A

**Identified Resources and Level of Importance:** None found.

**Comments on Effects to Resources:** None found.

**Coordinator Feedback:** None

1 Enhanced assigned 01/26/2011 by Lauren P. Milligan, FL Department of Environmental Protection

**Coordination Document:** No Selection

**Dispute Information:**N/A

**Identified Resources and Level of Importance:** One Florida Managed Area known as Cooper's Point, and two local recreation areas known as Ben T. Davis Beach and Courtney Campbell Beach, are located within the 500-ft. buffer zone of the project.

**Comments on Effects to Resources:** The project will likely have no adverse impacts on these facilities and should enhance recreational opportunities throughout the area.

**Coordinator Feedback:** None

The following organization(s) were expected to but did not submit a review of the Recreation Areas issue for this alternative: Federal Highway Administration, National Park Service

#### Coordinator Summary: Section 4(f) Potential Issue

0 None assigned 03/29/2011 by FDOT District 7

**Comments:** Based on the Environmental Screening Tool's (EST) GIS screening results, the only potential Section 4(f) resources within the project study limits are the Pinellas County Aquatic Preserve/ Outstanding Florida Waters (AP/OFW) and Ben T. Davis Beach. This recreational trail project would not permanently require nor incorporate any ROW or permanent easement from the AP/OFW or Ben T. Davis Beach resources. The project would be entirely constructed and maintained within the existing transportation ROW that the State of Florida owns and manages for transportation purposes. The project would not cause any proximity impacts that would permanently impair or diminish these resources' attributes which qualify them for protection under the provisions of Section 4(f). With respect to the AP/OFW resource, all construction activities are planned to occur within the existing transportation ROW which is generally mile in width on either side of the SR 60 causeway. No project construction activities are planned to occur within the Ben T. Davis Beach resource either.

Recreational opportunities within these resources will not be temporarily or permanently affected by either the construction of the project or operation of the facility for its intended purpose. There are no water based recreational trails that are officially designated, marked or signed as such either within, along or perpendicular (intersecting) to the project's study limits. Access to navigational activities within the OFW will be maintained during the project's construction as it is expected that this provision would be a condition of the USCG permits that would be required to construct the westernmost SR 60 relief structure which is within the OFW. The construction of the SR 60 main span over Old Tampa Bay will not occur within the OFW since this structure is located in Hillsborough County. It is likely that this recreational project would enhance the use of the resource by improving access to it.

There is an unofficially designated Courtney Campbell Trail that is actually a service road system that is used to maintain the SR 60 transportation ROW. There are only incidental or secondary uses of this service road system for recreational activities.

The ETDM metadata and its use in generating what resources are "found" within the EST GIS buffers indicate that there are statewide (typically land based) Ecological Greenways Critical Linkages and Greenways Ecological Priority Linkages that could be associated with the proposed project. These FDEP designations contain all of the largest areas of ecological and natural resource significance and the landscape linkages necessary to link these areas together in one functional statewide network. This data was created as part of the Florida Statewide Greenways Planning Process. The Florida Ecological Greenways Network identifies the opportunities to protect large, intact landscapes important for conserving Florida's biodiversity and ecosystem services.

There are no FDEP designated Ecological Greenways Critical Linkages and Greenways Ecological Priority Linkages that are officially designated, marked or signed as such either within, along or perpendicular (intersecting) to the project's study limits.

The ETDM metadata and its use in generating what resources are "found" within the EST GIS buffers indicate that there are Paddling Trails Priorities that could be associated with the proposed project. This dataset contains prioritized paddling trail opportunities from the Office of Greenways and Trails Prioritization Project. The areas shown in this layer are intended to identify opportunity corridors of statewide and regional significance. These corridors are 4 kilometers (approx. 2.5 miles) wide to reflect the variability of actual trail location after planning and design is completed. This GIS layer was created by the Office of Greenways of Trails and the UF GeoPlan Center, to support the Florida Statewide Greenways & Trails System.

There are no FDEP designated Paddling Trails Priority locations that are officially designated, marked or signed as such either within, along or perpendicular (intersecting) to the project's study limits. Since the project location is situated within the open waters of Upper Tampa Bay, it would be expected that no officially designated recreational paddling opportunity would be identified for this area's open waters due to the susceptibility of the waters becoming rough due to weather or tidal changes.

Since the construction and maintenance of the proposed project will occur within the existing highway right of way, this project would not involve any Section 4(f) uses. FHWA has reviewed the Section 4(f) Determination of Applicability for ETDM project #13102 and has made the determination that the project will have no Section 4(f) impacts.

#### ETAT Reviews: Section 4(f) Potential Issue: 1 found

0 None assigned 03/16/2011 by Linda Anderson, Federal Highway Administration

**Coordination Document:** PD&E Support Document As Per PD&E Manual

**Dispute Information:**N/A

**Identified Resources and Level of Importance:** Within the 100' buffer:

1. Recreational trail: Old Tampa Bay Courtney Campbell Causeway.
2. 6.7 acres of Pinellas County Aquatic Preserve (Outstanding Florida Water).
3. Ben T. Davis Beach.
4. 182 acres of Ecological Greenways Critical Linkages.
5. 177 acres of Greenways Ecological Priority Linkages (Low Priority).
6. 137 acres of Paddling Trails Priorities (Medium Priority).

Within the 200' buffer:

66 acres of Pinellas County Aquatic Preserve (Outstanding Florida Water).

Within the 500' buffer:

304 acres of Pinellas County Aquatic Preserve (Outstanding Florida Water).

**Comments on Effects to Resources:** Impacts to recreational areas, such as the Old Tampa Bay Courtney Campbell Causeway and the Ben T. Davis Beach may be Section 4(f) impacts.

Impacts, e.g. from the building of the trail bridges, to the Pinellas County Aquatic Preserve may be Section 4(f) impacts because Florida's Aquatic Preserves, per their web page, <http://www.dep.state.fl.us/coastal/programs/aquatic.htm>, have both a recreational and a wildlife preserve function. Quoting from this web page: "Aquatic Preserves are critical nurseries for fish and other aquatic life. . . Florida's Aquatic Preserves protect the living waters of Florida to ensure that they will always be home for bird rookeries and fish nurseries." Permanent impairment of the function of this resource, either for recreation or as a wildlife preserve, as a result of this project may constitute a Section 4(f) Constructive Impact.

With regard to the Ecological Greenways Critical Linkages, the Greenways Ecological Priority Linkages, and the Paddling Trails Priorities, publicly owned properties planned for park, recreation area, wildlife refuge, or waterfowl refuge purposes may be Section 4(f) properties when the public agency that owns the property has formally designated and determined it to be significant for park, recreation area, wildlife and waterfowl refuge purposes. Evidence of formal designation would be the inclusion of the publicly owned land, and its function as a 4(f) resource, into a city or county Master Plan.

A Section 4(f) Determination of Applicability will be needed.

Comment made 3-16-11: FHWA has reviewed the Section 4(f) Determination of Applicability for ETDM project #13102 and has made the determination that the project will have no Section 4(f) impacts. Consequently, the DOE is being changed from "minimal" to "none."

**Coordinator Feedback:** None

#### ETAT Reviews and Coordinator Summary: Community Issues

##### Coordinator Summary: Aesthetics Issue

2 Minimal assigned 02/17/2011 by FDOT District 7

**Comments:** The Florida Department of Transportation (FDOT) recommends a Degree of Effect of Minimal.

The Courtney Campbell Causeway (SR 60) was designated as a Scenic Highway in 2005 by the FDOT. The trail will be designed and constructed to consider safety of trail users while minimizing any impedance to views along the corridor. The construction of the trail is consistent with the Courtney Campbell Causeway Scenic Highway Corridor Management Plan (CMP) as stated in Section 2, Goal 2(b)(i). The objective of this goal is to improve bicycle and pedestrian safety by working with FDOT, MPOs, and local governments to develop a continuous bicycle/pedestrian trail parallel to the main roadway to avoid auto traffic conflicts.

No comments were received from the Federal Highway Administration (FHWA), the Hillsborough County Metropolitan Planning Organization (MPO), or the Pinellas County MPO.

##### ETAT Reviews: Aesthetics Issue: None found

The following organization(s) were expected to but did not submit a review of the Aesthetics issue for this alternative: Federal Highway Administration, Hillsborough County MPO, Pinellas County MPO

##### Coordinator Summary: Economic Issue



**1** Enhanced assigned 02/17/2011 by FDOT District 7

**Comments:** The Florida Department of Transportation (FDOT) recommends a Degree of Effect of Enhanced.

The proposed multi-use trail will increase ecotourism in the Tampa Bay region. The trail also improves access across Old Tampa Bay for non-motorized users traveling to and from Pinellas and Hillsborough counties.

This project should be developed in accordance with the Civil Rights Act of 1964, as amended by the Civil Rights Act of 1968, along with Title VI of the Civil Rights Act, Executive Order 12898 (Environmental Justice), which ensures that minority and/or low-income households are neither disproportionately adversely impacted by major transportation projects, nor denied reasonable access to them by excessive costs or physical barriers (Environmental Protection Agency [EPA], 1994).

No comments were received from the Federal Highway Administration (FHWA), the Hillsborough County Metropolitan Planning Organization (MPO), and the Pinellas County MPO.

**ETAT Reviews: Economic Issue: None found**

The following organization(s) were expected to but did not submit a review of the Economic issue for this alternative: Federal Highway Administration, Hillsborough County MPO, Pinellas County MPO

**Coordinator Summary: Land Use Issue**

**0** None assigned 02/17/2011 by FDOT District 7

**Comments:** The Florida Department of Transportation recommends a Degree of Effect of None.

The trail will be located within right of way (ROW) designated for transportation purposes. The trail, with the exception of the two proposed bridges over Old Tampa Bay, will be constructed on existing fill material used to construct the Causeway. No changes to land use should occur as a result of the construction of the proposed trail.

The trail is consistent with the Comprehensive Plans for Hillsborough County, Pinellas County, City of Tampa, and City of Clearwater. The trail has also been identified in the City of Tampa Greenways & Trails Master Plan (2001), the City of Clearwater Bikeways and Trails Plan (1996) and Shifting Gears: Clearwater's Bicycle and Pedestrian Master Plan (2007). The construction of the trail is consistent with the Courtney Campbell Causeway Scenic Highway Corridor Management Plan (CMP) as stated in Section 2, Goal 2(b)(i). The objective of this goal is to improve bicycle and pedestrian safety by working with FDOT, MPOs, and local governments to develop a continuous bicycle/pedestrian trail parallel to the main roadway to avoid auto traffic conflicts. The trail provides alternative, non-motorized, means of transportation in the region.

No comments were received from the Federal Highway Administration (FHWA), the Hillsborough County Metropolitan Planning Organization (MPO), the Pinellas County MPO, or the Florida Department of Community Affairs (DCA).

**ETAT Reviews: Land Use Issue: None found**

The following organization(s) were expected to but did not submit a review of the Land Use issue for this alternative: FL Department of Community Affairs, Federal Highway Administration, Hillsborough County MPO, Pinellas County MPO

**Coordinator Summary: Mobility Issue**

**1** Enhanced assigned 02/17/2011 by FDOT District 7

**Comments:** The Florida Department of Transportation (FDOT) has evaluated comments from Hillsborough County Metropolitan Planning Organization (MPO) and recommends a Degree of Effect of Enhanced.

As a needed east-west link, the construction of the trail would provide regional connection between Pinellas and Hillsborough counties and other areas within the Tampa Bay region.

The proposed Courtney Campbell trail will provide regional linkage for non-motorized travel between Pinellas and Hillsborough Counties and, with connection to other facilities, travel into Pasco and Hernando Counties. The project will connect to other existing and planned facilities to the east and west of the Causeway. On the Pinellas (west) side, the project will connect to Pinellas County's extensive trail system (proposed Bayshore Trail extension). On the Hillsborough (east) side, the trail will connect to the West Tampa Greenway (4.6 miles of this 16.6 miles Greenway is completed to date) which will eventually connect via on-street facilities to the Upper Tampa Bay Trail and then from there to the Suncoast Parkway Trail into Pasco and Hernando Counties.

There are express and local bus routes that operate along SR 60 (Courtney Campbell Causeway) and that intersect SR 60 near the proposed project area. The Hillsborough Area Regional Transit (HART) 200X route is a commuter express route that operates between downtown Tampa and the Eddie Moore Park and Ride Lot in Clearwater. This route only runs during weekday commuter rush hours. Furthermore, HART Route 30 runs near the east end of the proposed trail, and the Pinellas Suncoast Transit Authority (PSTA) Route 60 runs near the west end of the proposed trail. The combination of the existing transit routes and the proposed trail offers additional connections between Pinellas and Hillsborough Counties. The transit routes also provide additional opportunities for use of the proposed trail.

The Hillsborough County MPO noted that the Courtney Campbell Causeway Trail is the number 2 priority of the Chairs Coordinating Committee for all of West Central Florida.

No comments were received from the Federal Transit Administration (FTA), Federal Highway Administration (FHWA), or the Pinellas County MPO.

**ETAT Reviews: Mobility Issue: 1 found**

**1** Enhanced assigned 01/27/2011 by Wally Blain, Hillsborough County MPO

**Coordination Document:** No Selection

**Dispute Information:** N/A

**Identified Resources and Level of Importance:** The Courtney Campbell Causeway Trail is the number 2 trail priority of the Chairs Coordinating Committee for all of West Central Florida.

**Comments on Effects to Resources:** Construction of this trail is consistent with the Regional Trail Priorities as well as local priorities

**Coordinator Feedback:** None

The following organization(s) were expected to but did not submit a review of the Mobility issue for this alternative: Federal Highway Administration, Federal Transit Administration, Pinellas County MPO

#### Coordinator Summary: Relocation Issue

**0** None assigned 02/17/2011 by FDOT District 7

**Comments:** The Florida Department of Transportation (FDOT) recommends a Degree of Effect of None.

No business or residential relocations are expected with the construction of the proposed multi-use trail.

No comments were received from the Federal Highway Administration (FHWA), Hillsborough County Metropolitan Planning Organization (MPO), or the Pinellas County MPO.

#### ETAT Reviews: Relocation Issue: None found

The following organization(s) were expected to but did not submit a review of the Relocation issue for this alternative: Federal Highway Administration, Hillsborough County MPO, Pinellas County MPO

#### Coordinator Summary: Social Issue

**2** Minimal assigned 02/17/2011 by FDOT District 7

**Comments:** The Florida Department of Transportation (FDOT) has evaluated comments from the US Environmental Protection Agency (USEPA) and recommends a Degree of Effect of Minimal.

The FDEP noted that the FDOT conducted a Feasibility Study for this project in 2008. During a public workshop held in May 2008, 23 public comments were received and 21 of these comments indicated support for the project. The FDOT coordinated with local agencies, groups, and the Courtney Campbell Causeway Scenic Highway during the Feasibility Process to seek input. FDOT commits to continued public coordination throughout the PD&E study and will hold a public hearing as part of this study. The project will provide alternative modes of transportation between Pinellas and Hillsborough counties and throughout the Tampa Bay region.

The USEPA noted support for alternative modes of transportation and recommended that any negative direct or indirect impacts be avoided or minimized to the best extent practicable.

This project should be developed in accordance with the Civil Rights Act of 1964, as amended by the Civil Rights Act of 1968, along with Title VI of the Civil Rights Act, Executive Order 12898 (Environmental Justice), which ensures that minority and/or low-income households are neither disproportionately adversely impacted by major transportation projects, nor denied reasonable access to them by excessive costs or physical barriers (Environmental Protection Agency [EPA], 1994).

No comments were received from the Florida Department of Community Affairs (DCA), Federal Highway Administration (FHWA), Hillsborough County Metropolitan Planning Organization (MPO), or the and Pinellas County MPO.

#### ETAT Reviews: Social Issue: 1 found

**2** Minimal assigned 01/30/2011 by Madolyn Dominy, US Environmental Protection Agency

**Coordination Document:** No Selection

**Dispute Information:** N/A

**Identified Resources and Level of Importance:** Resources: Social impacts, public involvement

**Comments on Effects to Resources:** The FDOT completed a Feasibility Study in 2008. During the study, newsletters were distributed to adjacent property owners and interested parties soliciting input. In May 2008, a public workshop was conducted in 2 separate locations (one in Pinellas County and one in Hillsborough County) to provide information to the general public and solicit input. Twenty-three written public comments were received; most of these indicated support of the project or sought additional information about the concepts. Written comments from 2 persons indicated their suggestion to re-allocate public funding necessary for this project to support education as a higher priority. The FDOT coordinated with local agencies, groups and the Courtney Campbell Causeway Scenic Highway during the feasibility process to seek input. The 2008 Feasibility Study contains the public comment summary with support data.

EPA supports alternative modes of transportation such as provided by this type of project. Overall, EPA does have significant comments regarding social issues for this project. It is recommended that any negative direct and indirect impacts be avoided or minimized to the best extent practicable. Public involvement on this project should be ongoing and continual throughout the project.

**Coordinator Feedback:** None

The following organization(s) were expected to but did not submit a review of the Social issue for this alternative: FL Department of Community Affairs, Federal Highway Administration, Hillsborough County MPO, Pinellas County MPO

#### ETAT Reviews and Coordinator Summary: Secondary and Cumulative Issues

## Coordinator Summary: Secondary and Cumulative Effects Issue

3 Moderate assigned 02/17/2011 by FDOT District 7

**Comments:** The Florida Department of Transportation (FDOT) has evaluated comments from the Southwest Florida Water Management District (SWFWMD) and recommends a Degree of Effect of Moderate.

This project is consistent with the Comprehensive plans for Hillsborough County, Pinellas County, City of Tampa, and City of Clearwater. Minimal environmental impacts are anticipated since construction of the trail will be conducted on existing fill used to construct the Causeway, with the exception of the two proposed bridges. Many of the areas on the existing Causeway are currently paved for the existing access road. The FDOT commits to using proper best management practices to avoid potential secondary impacts during construction. The proposed trail should not contribute to increased pollutant loading in Old Tampa Bay since this facility will be used for non-motorized transportation.

## ETAT Reviews: Secondary and Cumulative Effects Issue: 1 found

3 Moderate assigned 01/29/2011 by C. Lynn Miller, Southwest Florida Water Management District

**Coordination Document:** Permit Required

**Dispute Information:** N/A

**At-Risk Resource:** Wildlife and Habitat

**Comments on Effects:** The project has the potential to result in further reduction of the limited urban wildlife populations in the project vicinity which depend upon the adjacent wetland and surface water features.

**Recommended Avoidance, Minimization, and Mitigation Measures:** Potential upland impacts can be reduced by designing the project to avoid and, to the maximum extent practicable, preserve the existing patch of related, native upland habitat located within 200 - 500 feet of the west project terminus.

**Recommended Actions to Improve At-Risk Resources:** Under most circumstances, it would be helpful to implement upland habitat creation on areas like the Causeway that are virtually devoid of usable upland habitats. However, in the case of the Causeway, the creation of upland habitat in order to attract upland wildlife species likely would result in a significant increase in wildlife fatalities on the roadway. This issue could be investigated further to determine whether upland wildlife can be benefitted while accomplishing the Citizens Advisory Committee's Goals and Objectives (2008 Feasibility Report) "to support a coastal-style, native Florida landscape along the Causeway as additional landscaping is required or needs to be replaced" and "to maintain the natural environment of the Causeway."

**At-Risk Resource:** Water Quality and Quantity

**Comments on Effects:** In the absence of stormwater collection and treatment measures, the project has the potential to generate increased sedimentation during construction and operation that may contribute to a delay in recovery of Impaired Waters and degrade water quality in both Outstanding Florida Waters and Class II waters.

Further degradation of the Class II waters in the project area could threaten both recreational and commercial fishery resources.

**Recommended Avoidance, Minimization, and Mitigation Measures:** Minimize new impervious area where feasible by reducing the cross sections of project segments where limited distances are available between the existing guard rail and the cap of the bulkhead and/or the edge of wetland.

Utilize BMP trains (i.e. BMPs in series) and materials during construction to minimize the conveyance of sediment to OFWs, Class II waters and off-site sensitive habitats such as the extensive dense Mangrove Swamps on the north side of the Causeway and the Tidal Flats/Shoreline habitats on the south side of the Causeway.

Install double lines of staked turbidity barriers or floating turbidity barriers, depending on location, to decrease the potential for damage to seagrass beds, Mangrove Swamps, Tidal Flats and Shoreline habitats from turbidity and sedimentation during construction.

Potential fishery impacts can be reduced by providing treatment for under-treated or untreated runoff to these Class II waters. Retrofit the existing stormwater treatment facility near the FDOT office, if feasible, to increase treatment capacity in order to treat currently untreated impervious areas near the west project terminus.

**Recommended Actions to Improve At-Risk Resources:** Most of the impervious areas along the Causeway have no stormwater runoff treatment measures; therefore, providing treatment of those areas likely would generate significant water quality benefits on the OFW and Class II waters occupied by SR 60 and the proposed project.

**At-Risk Resource:** Wetlands

**Comments on Effects:** Mangrove Swamps, Tidal Flats, Shoreline habitats and seagrass beds may be indirectly affected by the project as a result of inadequate or unmaintained erosion control measures which would allow sediment to settle in and around these sensitive habitats.

Reduction or elimination of the remaining wildlife function of the designated Bird Nesting Areas on the north side of the Causeway and the Tidal Flats/Shoreline habitats on the south side of the Causeway may occur depending on the alternative selected, the construction methods used and the

effectiveness of erosion control measures. The reduction or elimination of the wildlife function of these habitats may result in secondary impacts to the recreational and commercial fishery in Old Tampa Bay.

**Recommended Avoidance, Minimization, and Mitigation Measures:** Install double lines of staked turbidity barriers or floating turbidity barriers, depending on location, to decrease the potential for damage to Mangrove Swamps, Tidal Flats, Shoreline habitats and seagrass beds from turbidity and sedimentation during construction.

Potential fishery impacts can be reduced by protecting and preserving existing wetlands and seagrass beds in the project area. The scheduling of project activity to avoid work during the open seasons for the recreational and commercial taking of crabs would assist in reducing project impacts to Bay fisheries.

**Recommended Actions to Improve At-Risk Resources:** Incorporate stormwater treatment measures into project design.

Add educational and directional signage intended to (1) reduce damage to seagrass beds and Mangrove Swamp, and (2) inform the public about the importance and value of estuarine wetland systems.

**Coordinator Feedback:** None

**Eliminated Alternatives**

No eliminated alternatives present.

## Project Scope

### General Project Commitments

Date	Description
02/24/2011	The Purpose and Need Statement and Alternative Description data was updated to reflect the correct mileage (7.473). This information is now correct and consistent with what is shown in the GIS analysis that the ETAT based their comments on and the Segment Description data.

### Required Permits

Permit Name	Type	Review Date
FDEP NPDES General Permit	Other	12/15/10
<b>Conditions:</b> Unknown		
Environmental Resource Permit	State	12/15/10
<b>Conditions:</b> Unknown		
U.S. Coast Guard Bridge Permit	Federal	12/15/10
<b>Conditions:</b> Bridge Project Questionnaires were submitted to FHWA for 2 bridge crossing locations. Determination of whether permit would be required is pending agency review.		

### Required Technical Studies

Technical Study Name	Type	Review Date
Advance Notification/ICAR Package	ENVIRONMENTAL	12/15/10
<b>Conditions:</b> None at this time		
Public Involvement Plan	ENVIRONMENTAL	12/15/10
<b>Conditions:</b> None at this time		
Contamination Screening Evaluation Report	ENVIRONMENTAL	12/15/10
<b>Conditions:</b> None at this time		
Public Hearing Transcript	ENVIRONMENTAL	12/15/10
<b>Conditions:</b> None at this time		
Endangered Species Biological Assessment	ENVIRONMENTAL	12/15/10
<b>Conditions:</b> Combined with Wetlands Evaluation and Essential Fish Habitat Assessment		
Wetlands Evaluation Report	ENVIRONMENTAL	12/15/10
<b>Conditions:</b> Combined with Endangered Species Biological Assessment and Essential Fish Habitat Assessment		
Cultural Resource Assessment	ENVIRONMENTAL	12/15/10
<b>Conditions:</b> None at this time		
Type 2 CE	ENVIRONMENTAL	12/15/10
<b>Conditions:</b> Assumed as the class of action - combined in the Project Development Summary Report		
Project Development Summary Report (PDSR)	ENGINEERING	12/15/10
<b>Conditions:</b> None at this time		
Essential Fish Habitat Assessment	ENVIRONMENTAL	12/15/10
<b>Conditions:</b> Combined with Endangered Species Biological Assessment and Wetland Evaluation		
Comments and Coordination Report	ENVIRONMENTAL	12/15/10
<b>Conditions:</b> None at this time		

### Class of Action

#### Class of Action Determination

**Class of Action:** Categorical Exclusion with **Lead Agency** Federal Highway Administration  
**Other Actions:** None

#### Class of Action Signatures

ACCEPTED by Steve C. Love, FDOT ETDM Coordinator for FDOT District 7 on 02/24/2011

ACCEPTED by Linda Anderson, Lead Agency ETAT Member for Federal Highway Administration on 03/09/2011

**Comments:** The Federal Highway Administration (FHWA) concurs with the determination of the Florida Department of Transportation (FDOT) that a Type II Categorical Exclusion is a suitable Class of Action for Project #13102, SR 60 Courtney Campbell Causeway Multi-Use Trail. Concurrence is based on the content of reviews and assignments of Degree of Effect in the Programming Summary Report which suggest that there will be no significant impacts associated with the project.

### Dispute Resolution Activity Log

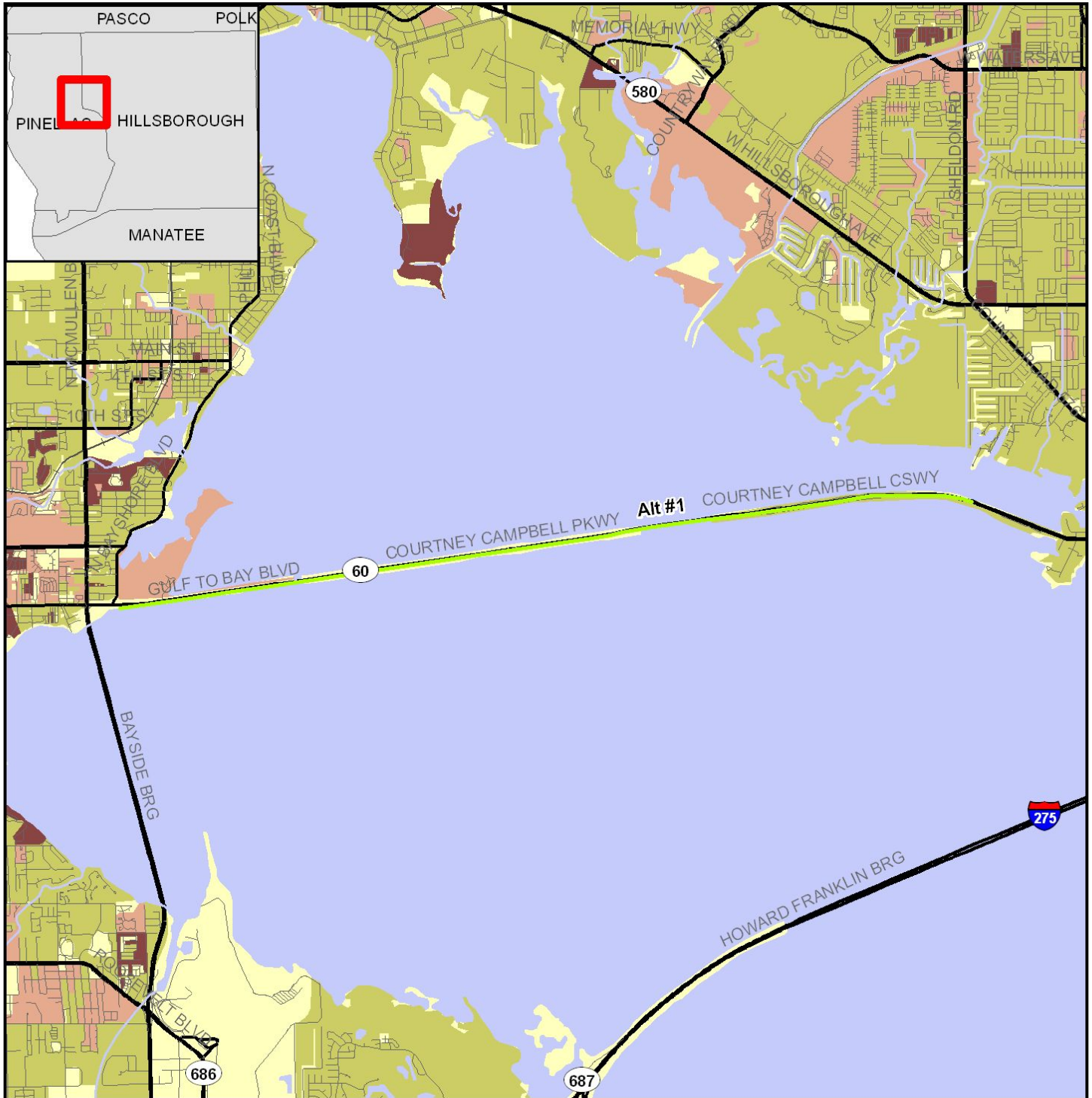
No Dispute Actions Found.



# Project-Level Hardcopy Maps

# 13102 SR 60 Trail PD&E Study

## Bayshore Boulevard to W of Ben T. Davis Bch Entrance



0 4 Miles

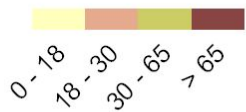
**Population Age Distribution Map**



Data Sources:  
 US Geological Survey  
 FL Department of Transportation  
 Geographic Data Technology, Inc.  
 US Census Bureau

- ETDM Alternative Point
- ETDM Alternative Terminus
- ETDM Alternative Segment
- ETDM Alternative Polygon
- Major Road
- Local Road or Trail
- Railroad
- River, Stream or Canal
- Water Body

Median Age



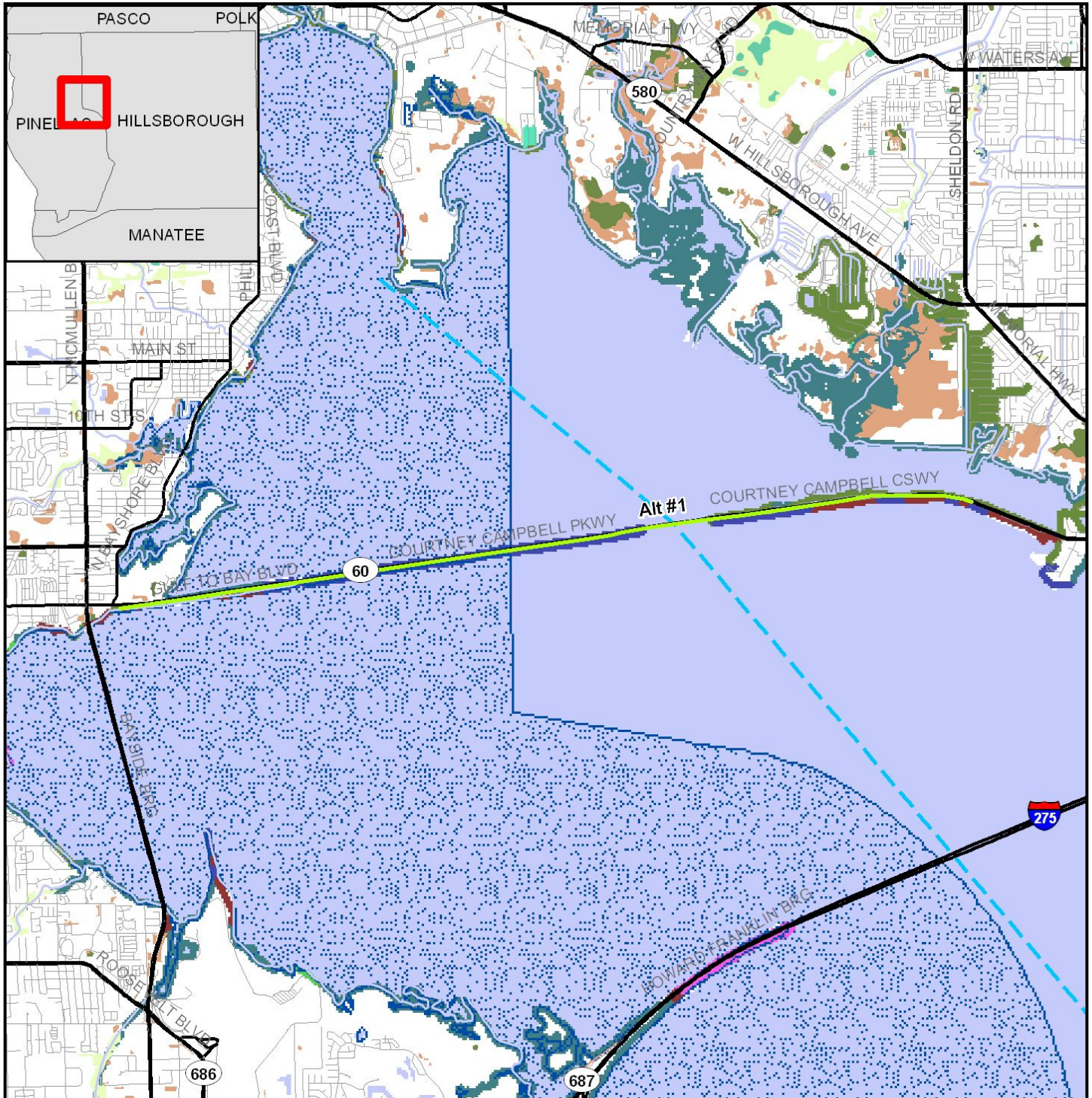
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# 13102 SR 60 Trail PD&E Study

## Bayshore Boulevard to W of Ben T. Davis Bch Entrance



**Coastal and Marine Resource Map**

0	1 Miles	ETDM Alternative Point	River, Stream or Canal	Continuous Seagrass	Gravel Beach/Riprap
W	N	ETDM Alternative Terminus	Water Body	Discontinuous Seagrass	Exposed Tidal Flat
E	S	ETDM Alternative Segment	Aquatic Preserve	Coastal Barrier Resource Area	Sheltered Tidal Flat
		ETDM Alternative Polygon	Navigable Water Way	Swamp or Marsh	Mixed Sand And Gravel Beach
		Major Road		Exposed Rocky Platform	Sheltered Rock/Seawall/Vegetated
		Local Road or Trail		Sand Beach	Exposed Vertical Rocky Shore/Seawall

Data Sources: Geographic Data Technology, Inc.; US Geological Survey; Florida Marine Research Institute; Florida Department of Transportation; Florida Department of Environmental Protection; National Oceanic and Atmospheric Association; Florida Water Management Districts

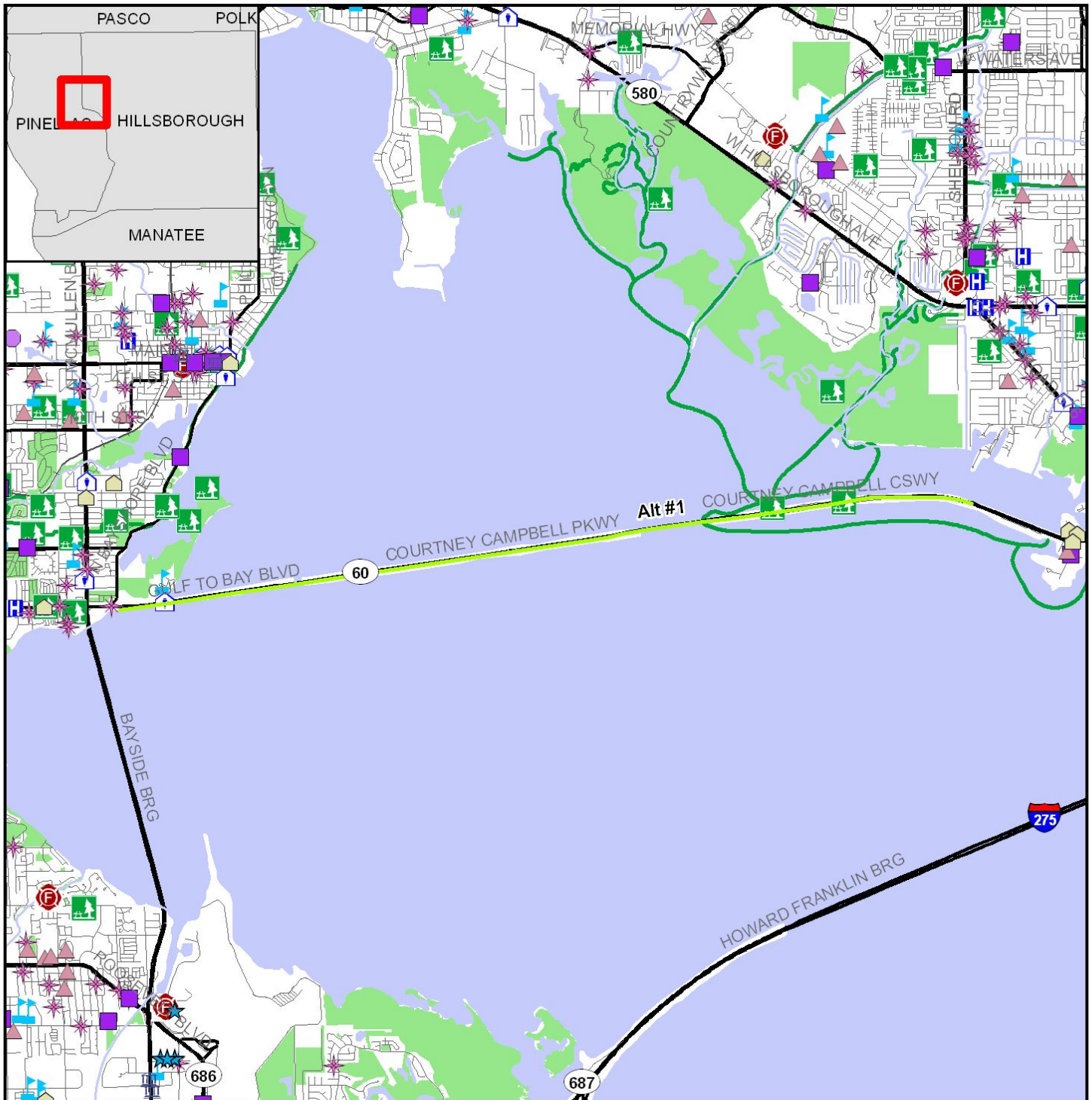
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# 13102 SR 60 Trail PD&E Study

## Bayshore Boulevard to W of Ben T. Davis Bch Entrance



0 0.3 Miles



- |                           |                  |                        |                                 |
|---------------------------|------------------|------------------------|---------------------------------|
| ETDM Alternative Point    | Cemetery         | Fire Station           | Major Road                      |
| ETDM Alternative Terminus | Social Service   | Health Care            | Local Road or Trail             |
| ETDM Alternative Segment  | Community Center | School                 | Railroad                        |
| ETDM Alternative Polygon  | Law Enforcement  | Park                   | Community Boundary              |
| Government                | Place of Worship | River, Stream or Canal | Water Body                      |
| Civic Center              | Cultural Center  | Recreational Trail     | Conservation or Recreation Area |

Data Sources:

US Geological Survey; FL Department of Transportation; Geographic Data Technology, Inc.; FL Property Appraisers; FL Natural Areas Inventory

**etdm**  
Efficient Transportation Decision Making

Environmental Screening Tool **est**

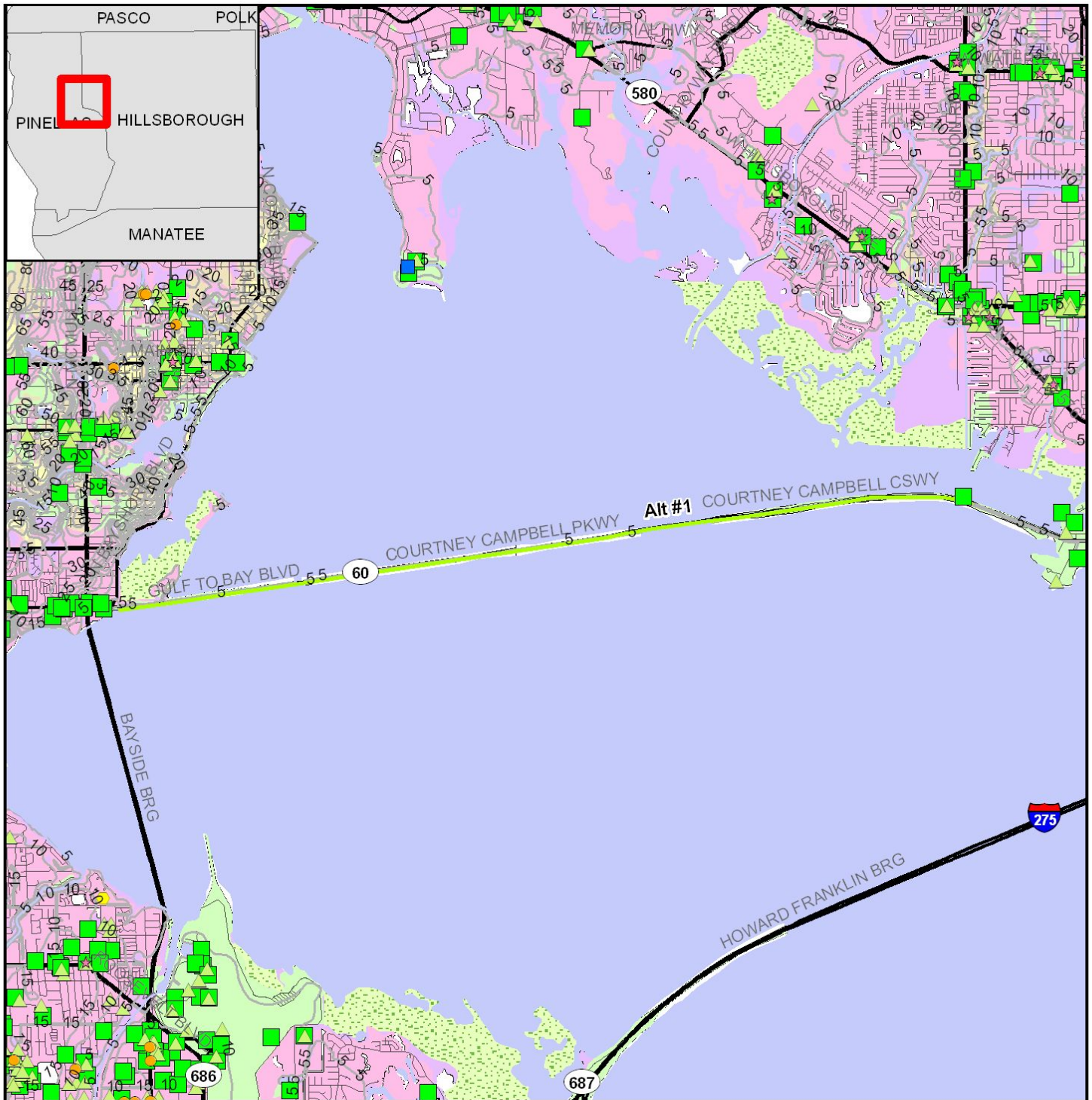
Map Generated on: 12/14/2010





# 13102 SR 60 Trail PD&E Study

## Bayshore Boulevard to W of Ben T. Davis Bch Entrance



**Potential Contamination Assessment Map**

0 1 Miles

<ul style="list-style-type: none"> <li><span style="color: green;">■</span> ETDM Alternative Point</li> <li><span style="color: magenta;">●</span> ETDM Alternative Terminus</li> <li><span style="color: green;">—</span> ETDM Alternative Segment</li> <li><span style="color: green;">▨</span> ETDM Alternative Polygon</li> <li><span style="color: black;">—</span> Major Road</li> <li><span style="color: black;">—</span> Local Road or Trail</li> </ul>	<ul style="list-style-type: none"> <li><span style="color: black;">—</span> Railroad</li> <li><span style="color: blue;">—</span> River, Stream or Canal</li> <li><span style="color: orange;">●</span> Toxic Release Inventory</li> <li><span style="color: red;">★</span> Dry Cleaning Facility</li> <li><span style="color: yellow;">●</span> Solid Waste Facility</li> </ul>	<ul style="list-style-type: none"> <li><span style="color: yellow;">▨</span> NPL Remediation Site</li> <li><span style="color: green;">▲</span> Hazardous Material Site</li> <li><span style="color: blue;">■</span> Power Plant</li> <li><span style="color: black;">●</span> Superfund Site</li> <li><span style="color: blue;">■</span> Nuclear Site</li> <li><span style="color: green;">■</span> FDEP Tanks</li> <li><span style="color: orange;">▨</span> Brownfield Area</li> <li><span style="color: black;">—</span> 5 FT Contour</li> <li><span style="color: blue;">■</span> Water Body</li> <li><span style="color: green;">▨</span> Swamp/Marsh</li> </ul>
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Data Sources:  
Geographic Data Technology, Inc.; US Geological Survey; FL Department of Transportation; FL Department of Environmental Protection;  
FL Water Management Districts; US Environmental Protection Agency; Natural Resource Conservation Service

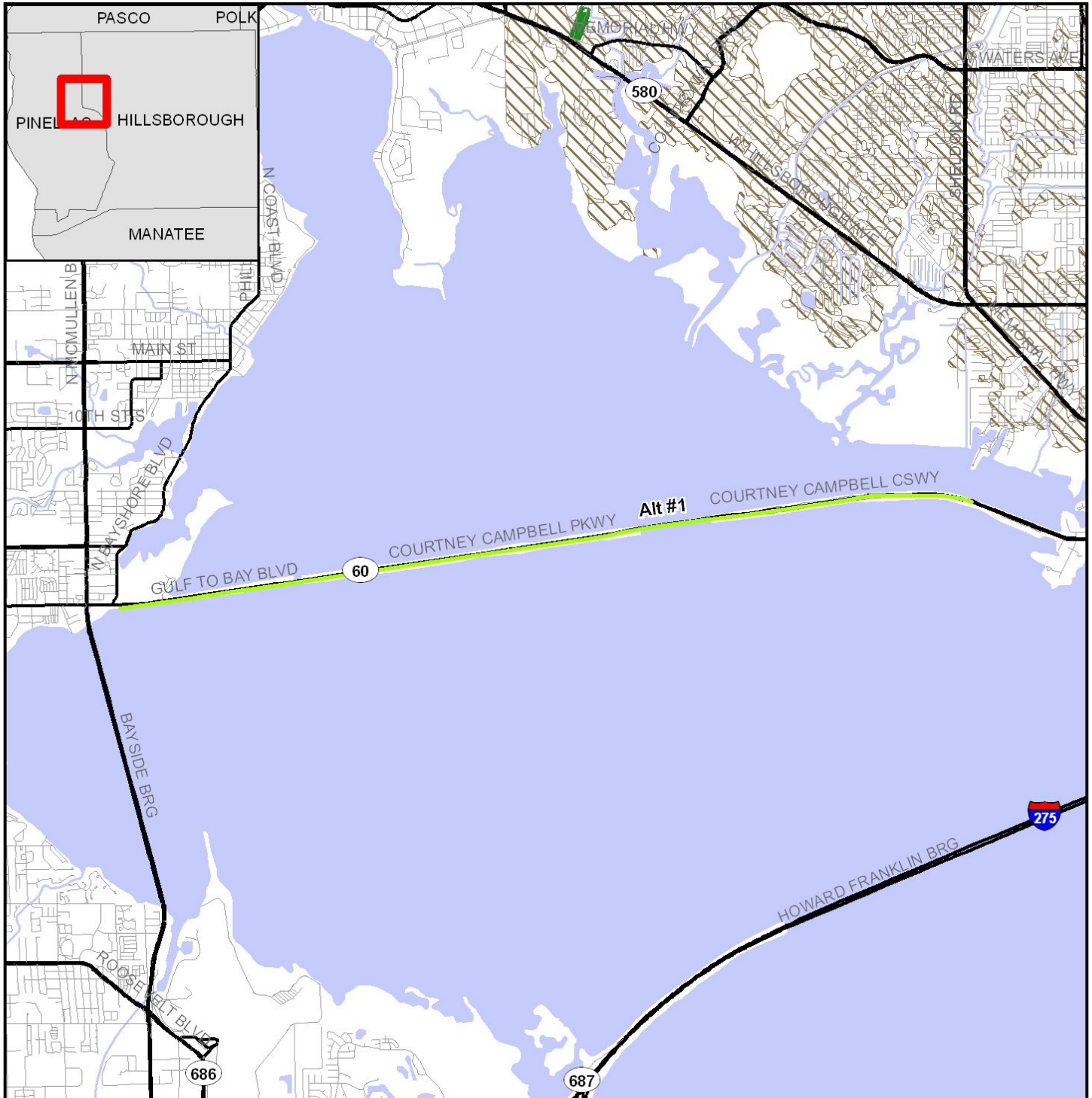
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# 13102 SR 60 Trail PD&E Study

## Bayshore Boulevard to W of Ben T. Davis Bch Entrance



0 2 Miles

**Farmlands Resource Map**



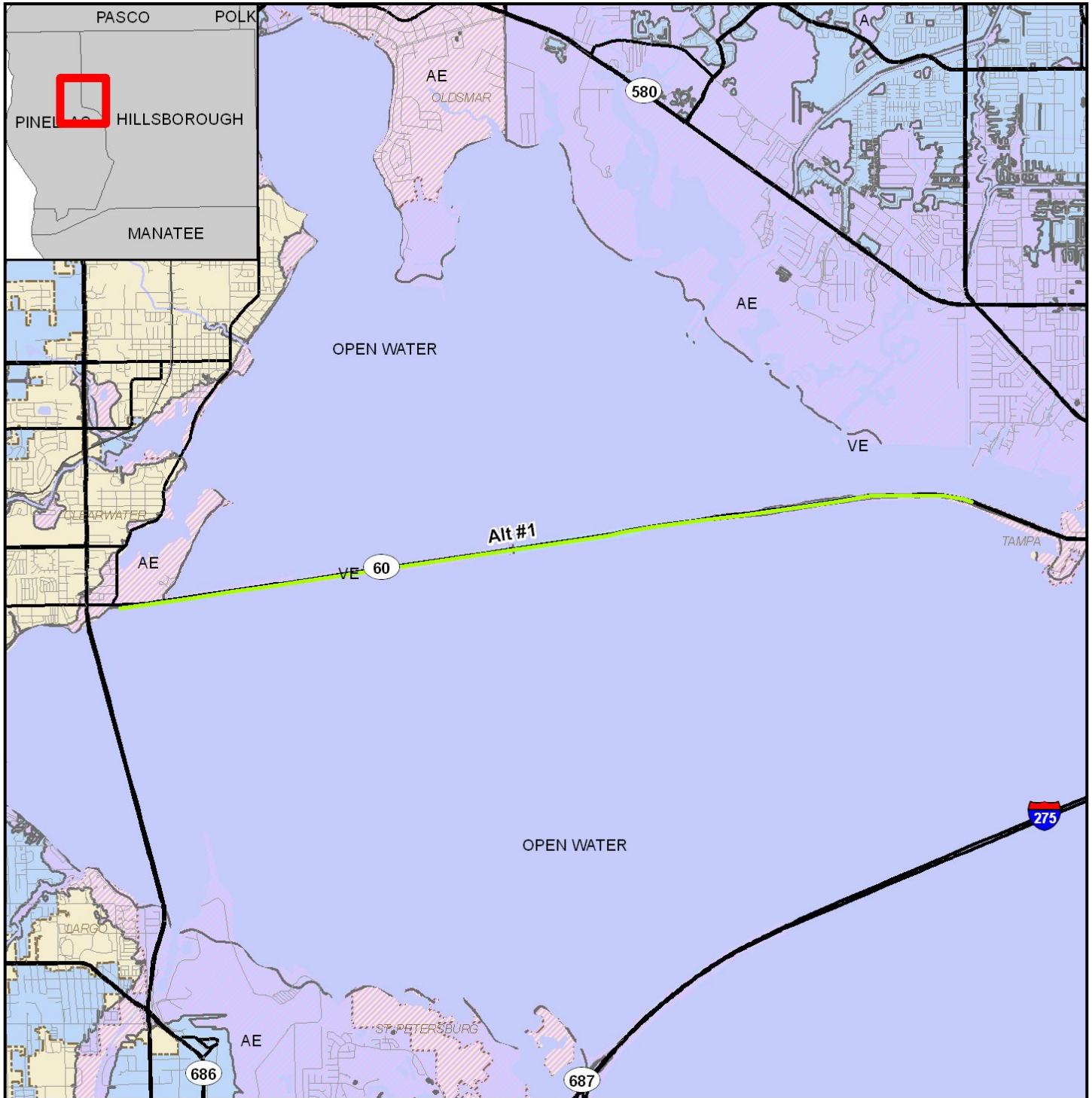
- |   |  |  |
|---|--|--|
| <span style="color: green;">●</span> ETDM Alternative Point                         | <b>Roads</b>   | <span style="background-color: #e0ffe0;">■</span> Cropland/Pastureland |
| <span style="color: magenta;">●</span> ETDM Alternative Terminus – Major Road       |  | <span style="background-color: #90c090;">■</span> Nurseries/Vineyards  |
| <span style="color: green;">—</span> ETDM Alternative Segment – Local Road or Trail | <span style="background-color: #e0e0ff;">■</span> Water Body       | <span style="background-color: #008000;">■</span> Specialty Farms      |
| <span style="background-color: #90c090;">■</span> ETDM Alternative Polygon          | <span style="color: blue;">—</span> River, Stream or Canal         | <span style="background-color: #ffff00;">■</span> Tree Crops           |
| <span style="background-color: #e0e0ff;">■</span> Prime Farmland Soils              | <span style="background-color: #c0c0c0;">■</span> Rural Open Lands |  |

Data Sources: Geographic Data Technology, Inc., Florida Water Management Districts, US Geological Survey, Natural Resources Conservation Services  
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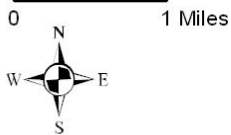


# 13102 SR 60 Trail PD&E Study

## Bayshore Boulevard to W of Ben T. Davis Bch Entrance



**Floodplain Resource Map**



- |                           |                        |                           |
|---------------------------|------------------------|---------------------------|
| ETDM Alternative Point    | Major Road             | City Limits               |
| ETDM Alternative Terminus | Local Road or Trail    | County Boundaries         |
| ETDM Alternative Segment  | Railroad               | Special Flood Hazard Area |
| ETDM Alternative Polygon  | River, Stream or Canal | Water Body                |

Data Sources:  
 Geographic Data Technology, Inc.  
 US Geological Survey  
 Federal Emergency Management Agency

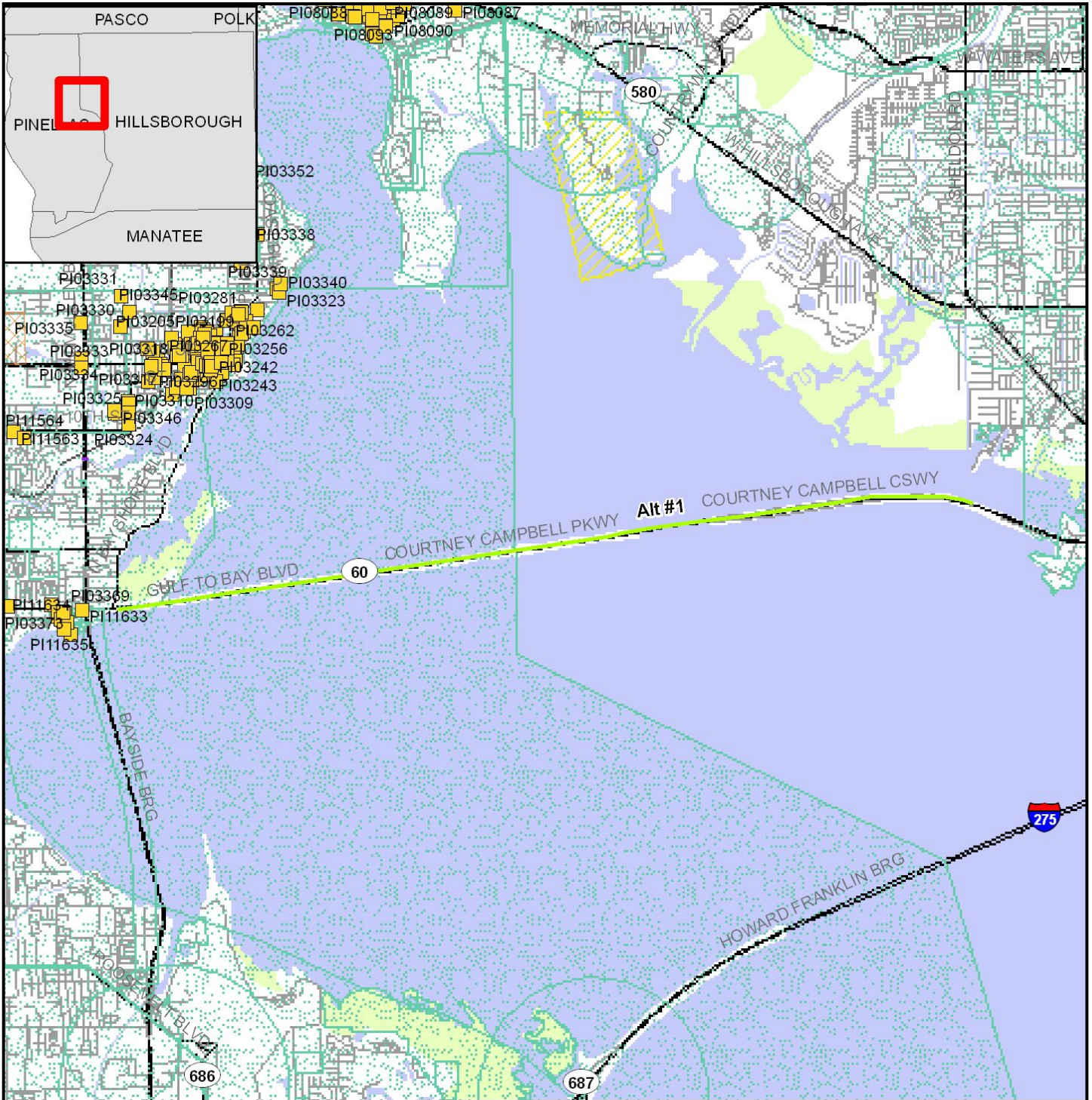
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# 13102 SR 60 Trail PD&E Study

## Bayshore Boulevard to W of Ben T. Davis Bch Entrance



**Historic Resources Map**

0 3 Miles

**Data Sources:**  
 Geographic Data Technology, Inc.  
 US Geological Survey  
 Florida Department of Transportation  
 Florida Department of State,  
 Bureau of Archaeological Research

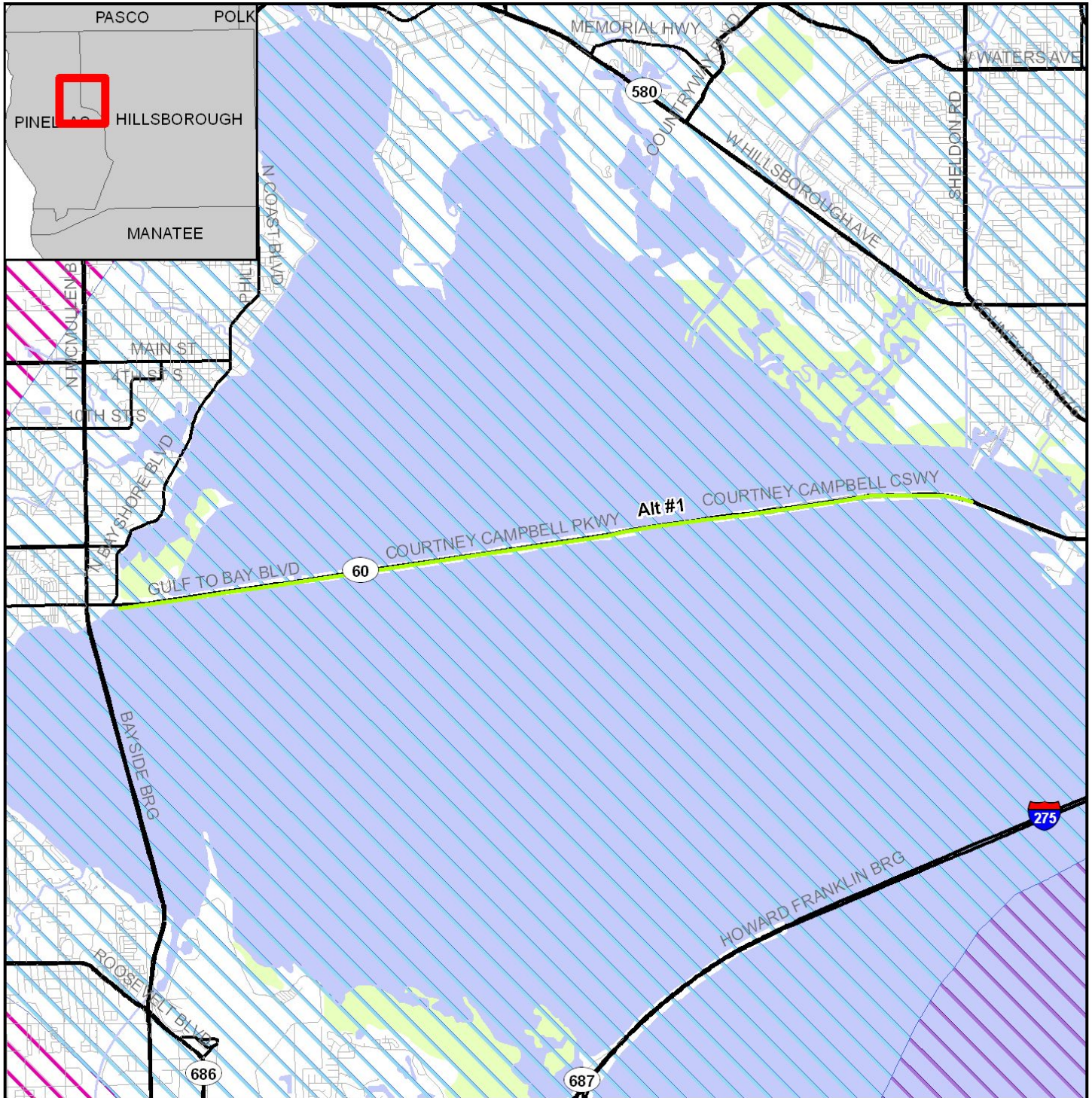
- ETDM Alternative Point
- ETDM Alternative Terminus
- ETDM Alternative Segment
- ETDM Alternative Polygon
- River, Stream or Canal
- Water Body
- Swamp/Marsh
- Major Road
- Local Road or Trail
- Railroad
- Historic Cemetery
- Historic Bridge
- Historic Resource Group
- Cultural Resource Field Survey Area
- State Historic Highway
- Historic Structure

Note: Historic properties depicted on this map represent resources listed in the Florida Master Site File excluding archeological site locations, which, pursuant to Chapter 267.135, Florida Statutes, may be exempt from public record (Chapter 119.07, Florida Statutes). Absence of features on the map does not necessarily indicate an absence of resources in the project vicinity.



# 13102 SR 60 Trail PD&E Study

## Bayshore Boulevard to W of Ben T. Davis Bch Entrance



**Hydrogeology Resource Map**

0 1 Miles

- |  |                           |                        |                  |                         |                        |
|--|---------------------------|------------------------|------------------|-------------------------|------------------------|
|  | ETDM Alternative Point    | Major Road             | Discharge 1 TO 5 | <b>Geological Epoch</b> | Oligocene/Miocene      |
|  | ETDM Alternative Terminus | Local Road or Trail    | Discharge > 5    | Eocene                  | Pleistocene            |
|  | ETDM Alternative Segment  | River, Stream or Canal | Discharge < 1    | Holocene                | Pleistocene & Holocene |
|  | ETDM Alternative Polygon  | Water Body             | Recharge 1 TO 10 | Miocene                 | Pliocene               |
|  |                           | Swamp/Marsh            | Recharge > 10    | Miocene/Pliocene        | Pliocene/Pleistocene   |
|  |                           |                        | Recharge < 1     | Oligocene               |                        |

Data Sources: Geographic Data Technology, Inc.; US Geological Survey; Florida Department of Transportation; South West Florida Water Management District; Florida Geological Survey

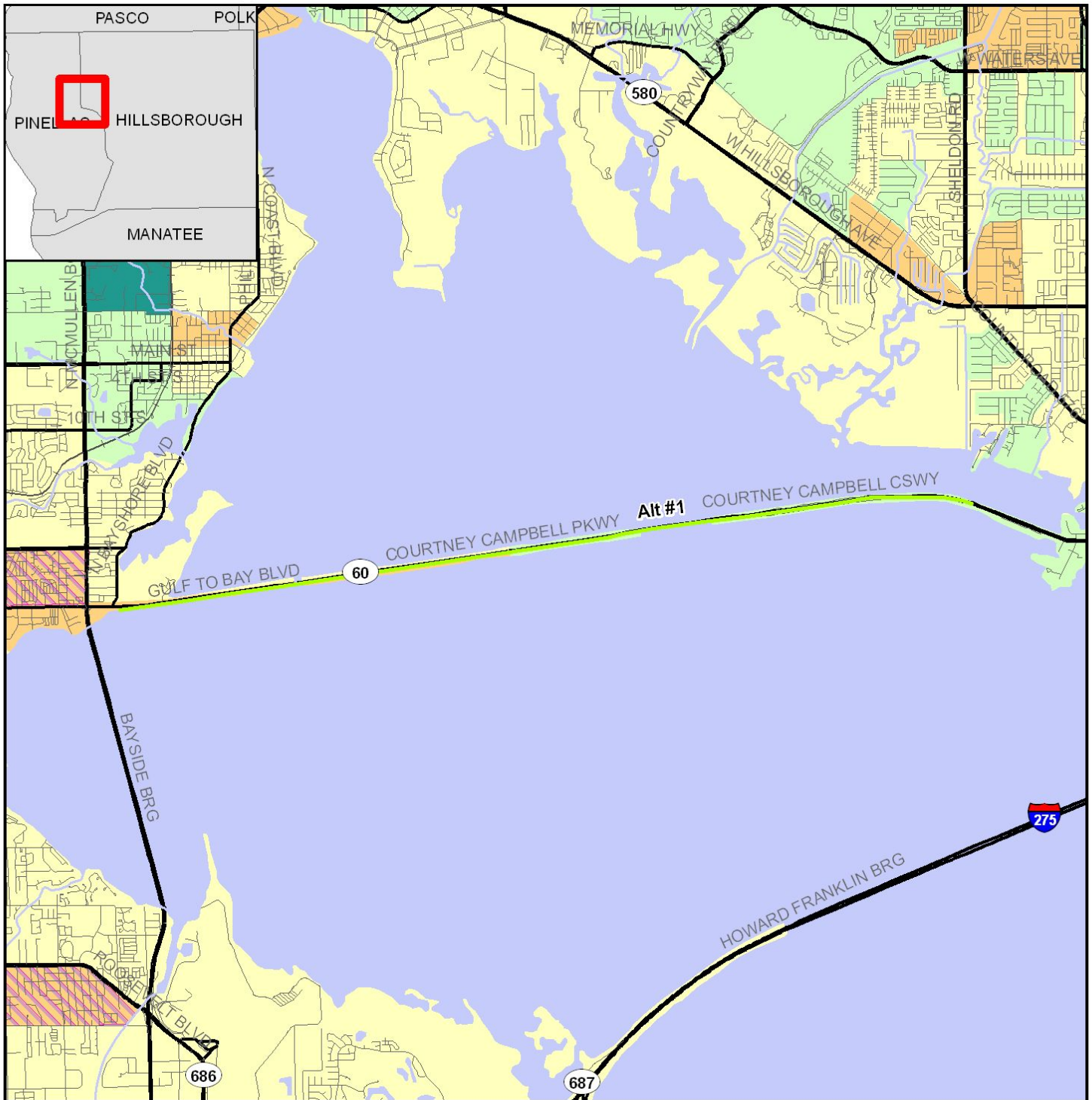
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# 13102 SR 60 Trail PD&E Study

## Bayshore Boulevard to W of Ben T. Davis Bch Entrance

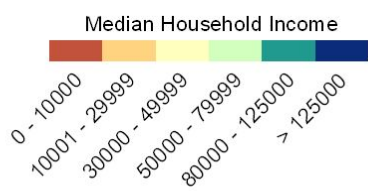


0 2 Miles



Data Sources:  
 US Geological Survey  
 FL Department of Transportation  
 Geographic Data Technology, Inc.  
 US Census Bureau

- ETDM Alternative Point
- ETDM Alternative Terminus
- ETDM Alternative Segment
- ETDM Alternative Polygon
- Major Road
- Local Road or Trail
- Railroad
- River, Stream or Canal
- > 20% Below Poverty
- Water Body



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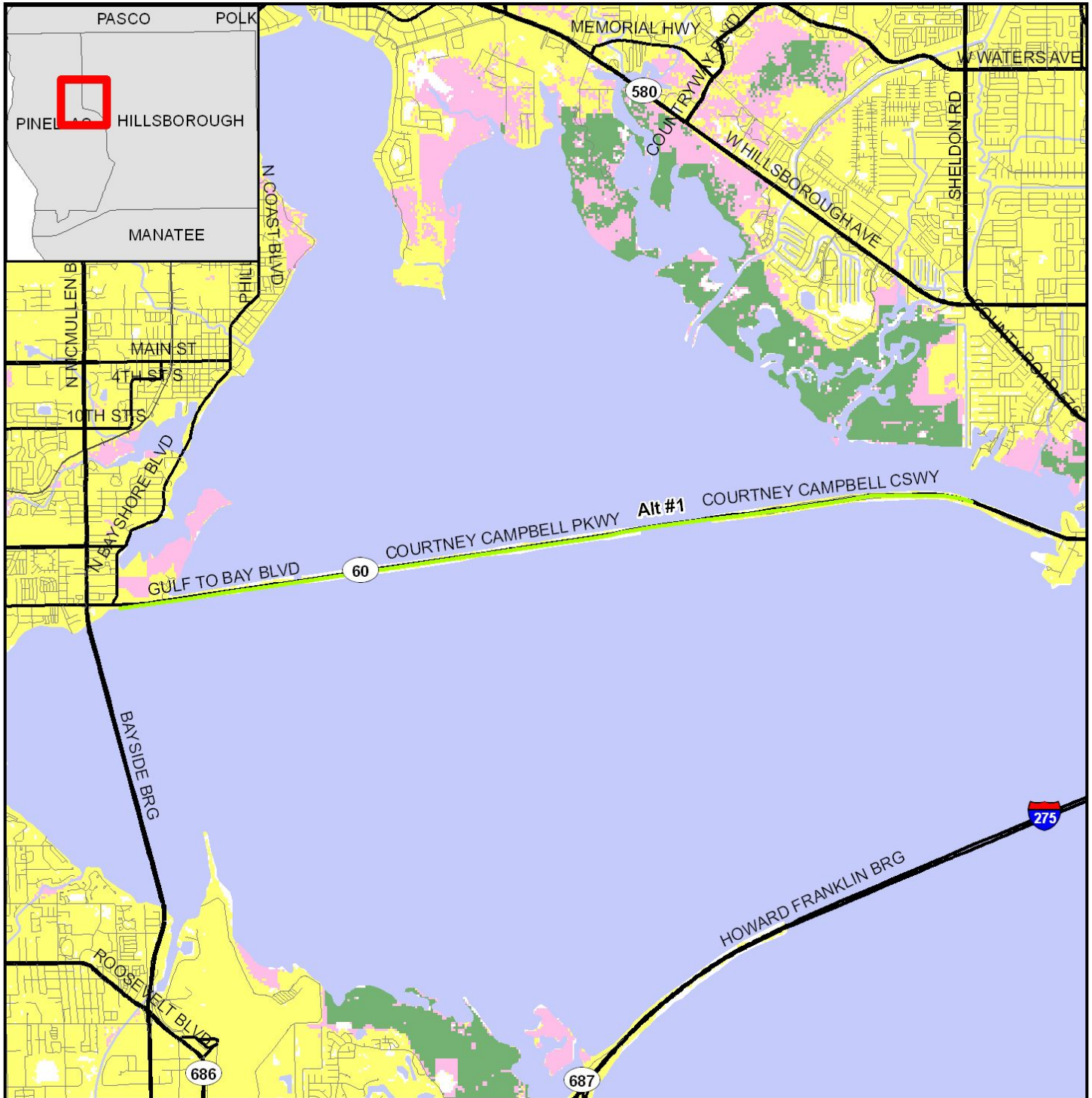
Map Generated on: 12/14/2010





# 13102 SR 60 Trail PD&E Study

## Bayshore Boulevard to W of Ben T. Davis Bch Entrance



### Integrated Wildlife Habitat Ranking System Map

Data Sources:  
 Geographic Data Technology, Inc.  
 US Geological Survey  
 Florida Department of Transportation  
 Florida Fish & Wildlife Conservation Commission

- |                           |                        |                        |
|---------------------------|------------------------|------------------------|
| ETDM Alternative Point    | Major Road             | Low Habitat Quality    |
| ETDM Alternative Terminus | Local Road or Trail    | Medium Habitat Quality |
| ETDM Alternative Segment  | Railroad               | High Habitat Quality   |
| ETDM Alternative Polygon  | River, Stream or Canal | Water Body             |

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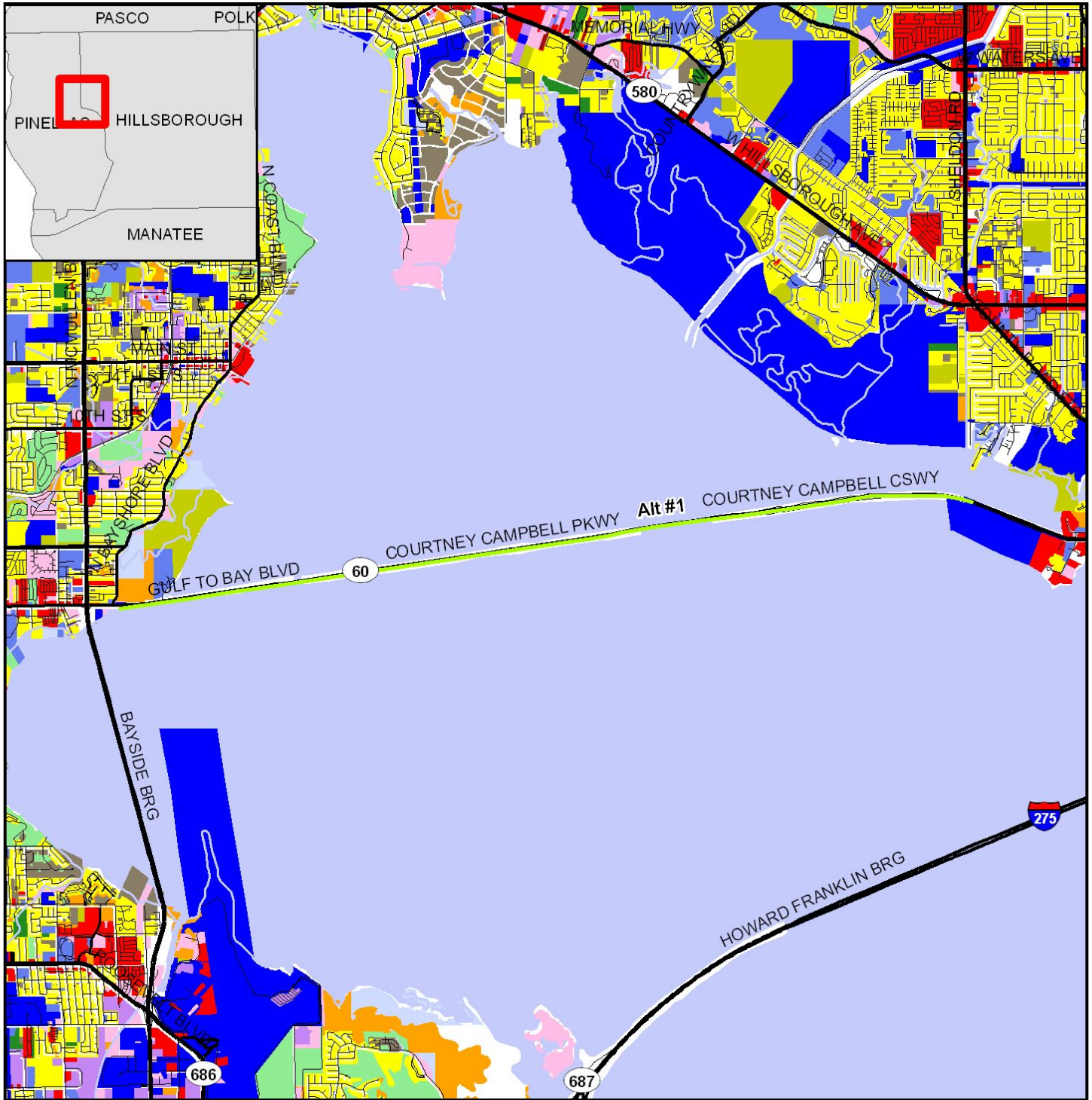
Map Generated on: 12/14/2010





# 13102 SR 60 Trail PD&E Study

## Bayshore Boulevard to W of Ben T. Davis Bch Entrance



0 0.6 Miles



Data Sources:  
 Geographic Data Technology, Inc.  
 US Geological Survey  
 Florida Department of Revenue  
 Florida Department of Transportation  
 Florida County Property Appraiser Offices

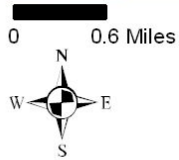
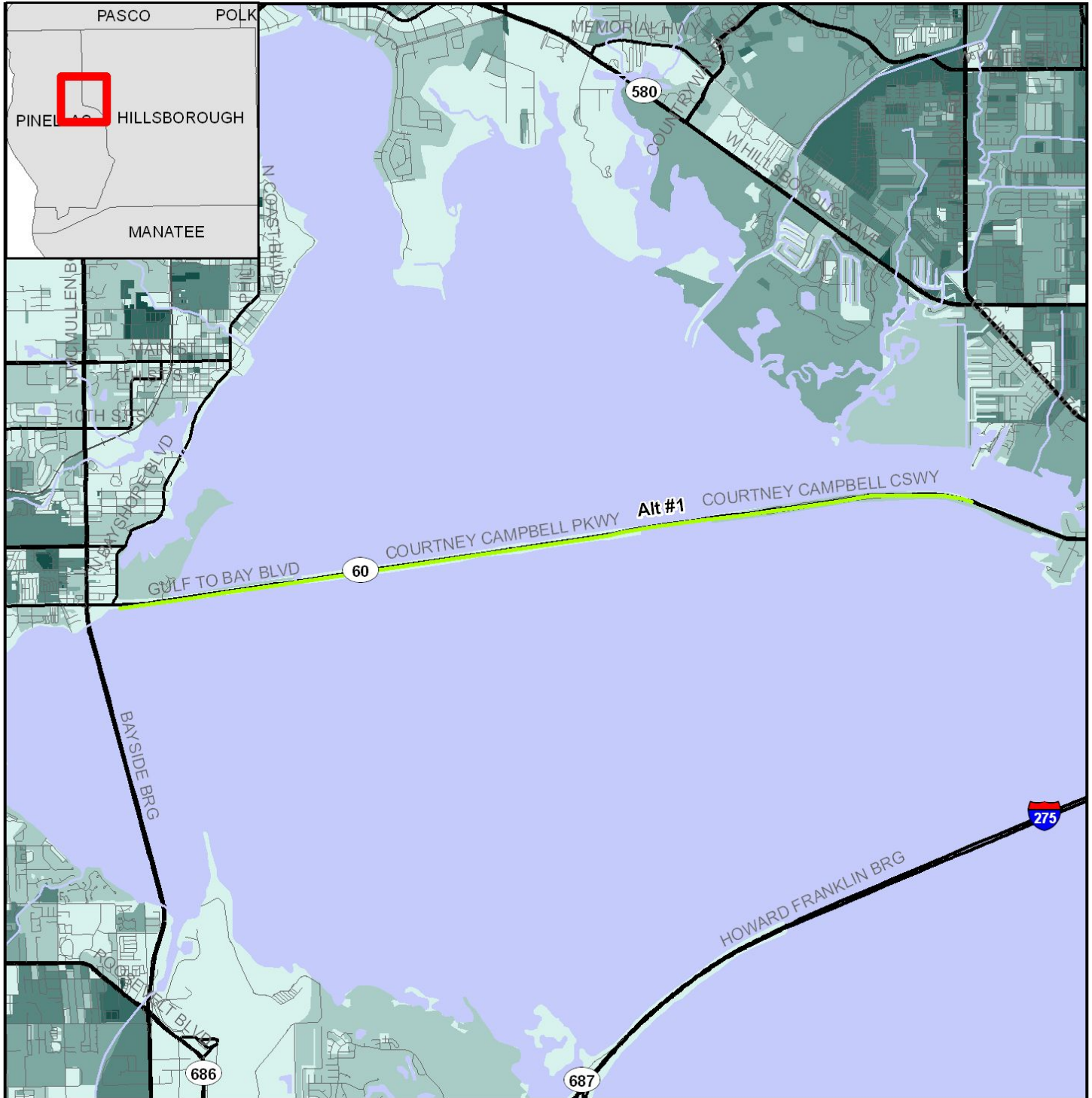
### Land Use Map

- |                           |                        |                         |                         |
|---------------------------|------------------------|-------------------------|-------------------------|
| ETDM Alternative Point    | Railroad               | Open (Not Agricultural) | Retail/Office           |
| ETDM Alternative Terminus | River, Stream or Canal | Other                   | Vacant (Residential)    |
| ETDM Alternative Segment  | Agricultural           | Public                  | Vacant (Nonresidential) |
| ETDM Alternative Polygon  | Industrial             | Right-of-Way            | Water                   |
| Major Road                | Institutional          | Recreational            | No Data                 |
| Local Road or Trail       | Mining                 | Residential             |                         |



# 13102 SR 60 Trail PD&E Study

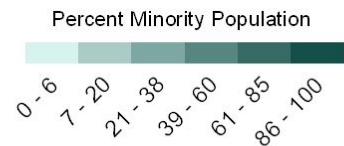
## Bayshore Boulevard to W of Ben T. Davis Bch Entrance



Data Sources:  
 US Geological Survey  
 FL Department of Transportation  
 Geographic Data Technology, Inc.  
 US Census Bureau

### Minority Population Distribution Map

- ETDM Alternative Point
- ETDM Alternative Terminus
- ETDM Alternative Segment
- ETDM Alternative Polygon
- Major Road
- Local Road or Trail
- Railroad
- River, Stream or Canal
- Water Body



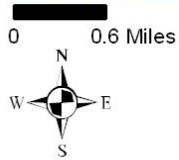
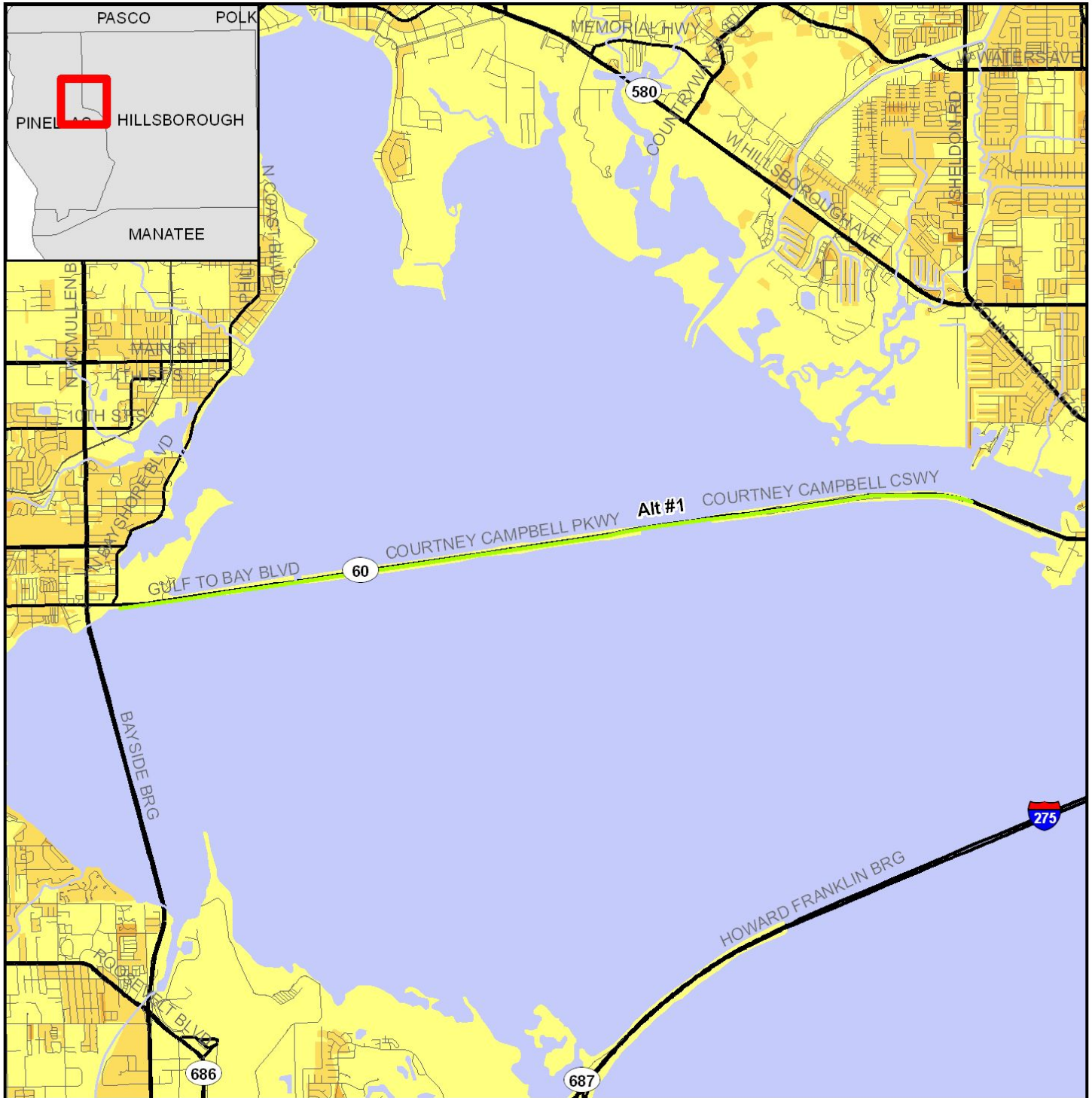
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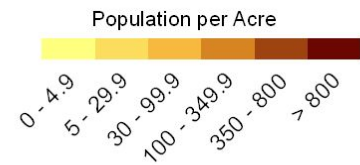
# 13102 SR 60 Trail PD&E Study

## Bayshore Boulevard to W of Ben T. Davis Bch Entrance



Data Sources:  
 US Geological Survey  
 FL Department of Transportation  
 Geographic Data Technology, Inc.  
 US Census Bureau

- ETDM Alternative Point
- ETDM Alternative Terminus
- ETDM Alternative Segment
- ETDM Alternative Polygon
- Major Road
- Local Road or Trail
- Railroad
- River, Stream or Canal
- Water Body

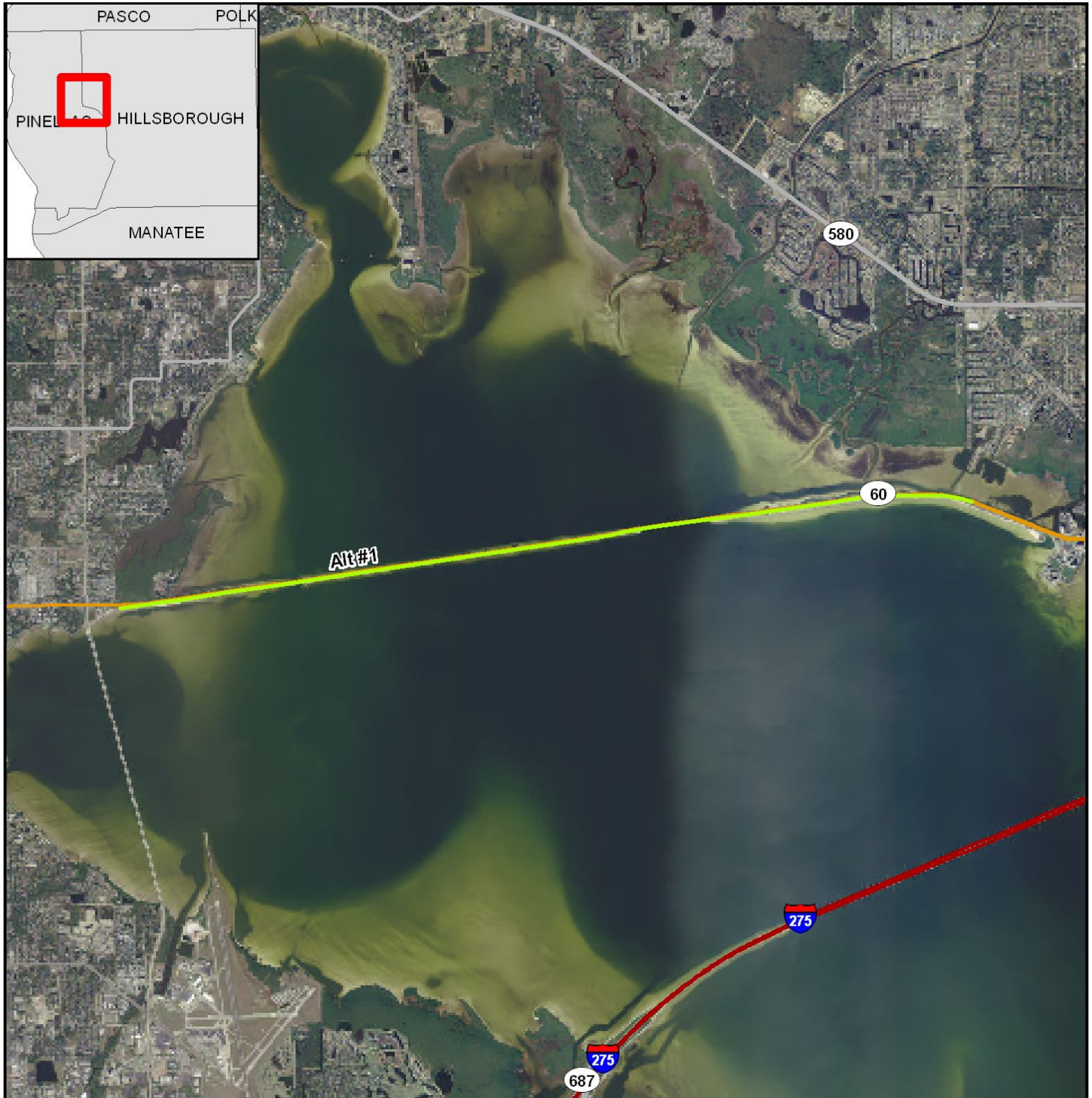


**Population Density Map**



# 13102 SR 60 Trail PD&E Study

## Bayshore Boulevard to W of Ben T. Davis Bch Entrance



**Project Aerial Map**

0 2 Miles



- ETDM Alternative Point     — Primary and Limited Access Highway
- ETDM Alternative Terminus     — Secondary, Unlimited Access Highway
- ETDM Alternative Segment     — Other Highway Feature
- ETDM Alternative Polygon

Data Sources:  
 Highways - Geographic Data Technology, Inc.  
 Digital Orthophotograph - US Geological Survey

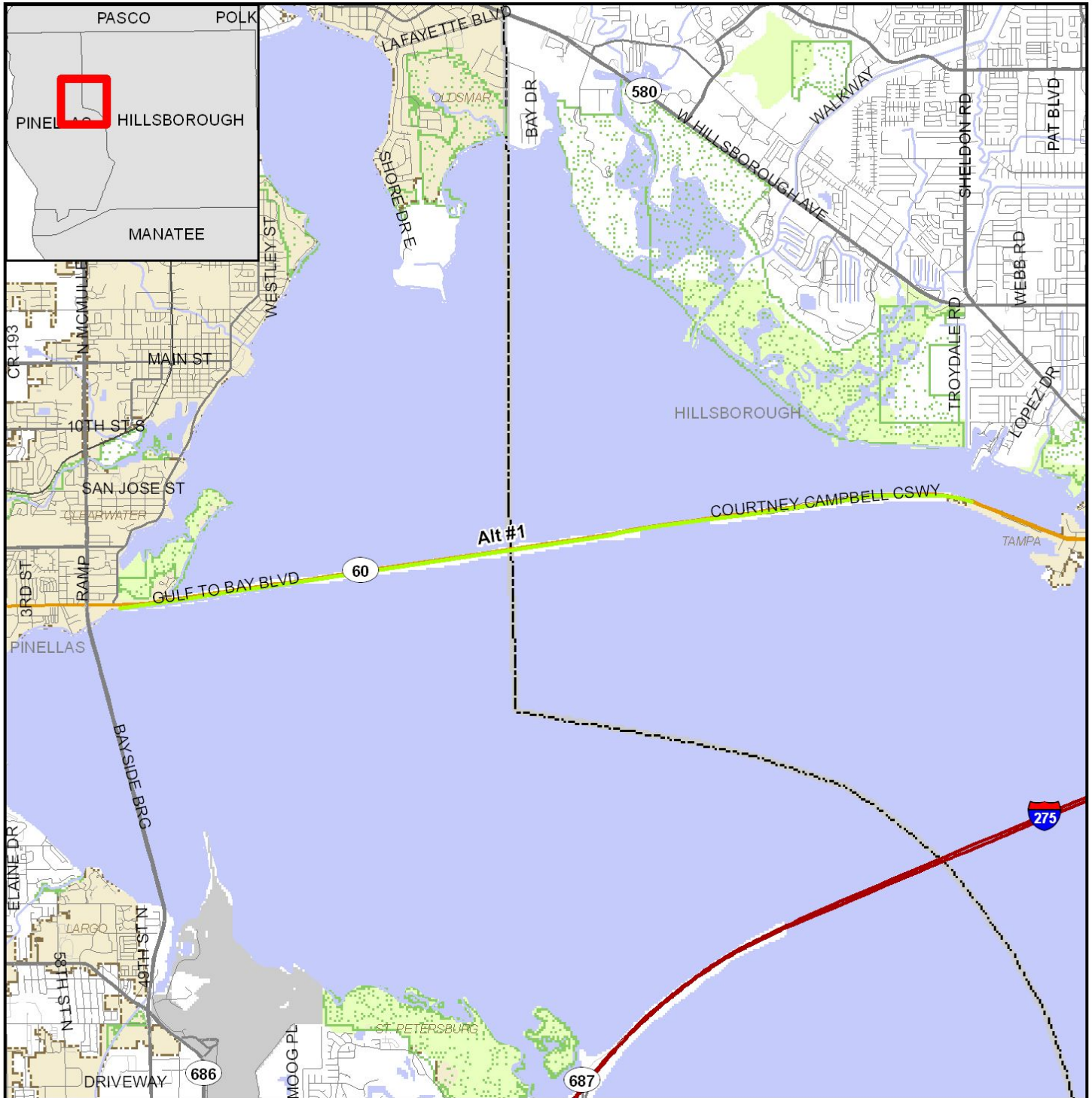
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# 13102 SR 60 Trail PD&E Study

## Bayshore Boulevard to W of Ben T. Davis Bch Entrance



**Project Location Map**

0 1 Miles



**Data Sources:**

Geographic Data Technology, Inc.  
 US Geological Survey  
 US Census Bureau  
 County Property Appraisers  
 Florida Natural Areas Inventory

- |                           |                                     |                   |
|---------------------------|-------------------------------------|-------------------|
| ETDM Alternative Point    | Managed Conservation Lands          | Toll Road         |
| ETDM Alternative Terminus | Primary and Limited Access Highway  | Railroad          |
| ETDM Alternative Segment  | Secondary, Unlimited Access Highway | Airport           |
| ETDM Alternative Polygon  | Connecting Road                     | City Limits       |
| River, Stream or Canal    | Local Road or Trail                 | County Boundaries |
| Water Body                | Other Roadway Feature               |                   |
| Swamp/Marsh               |                                     |                   |

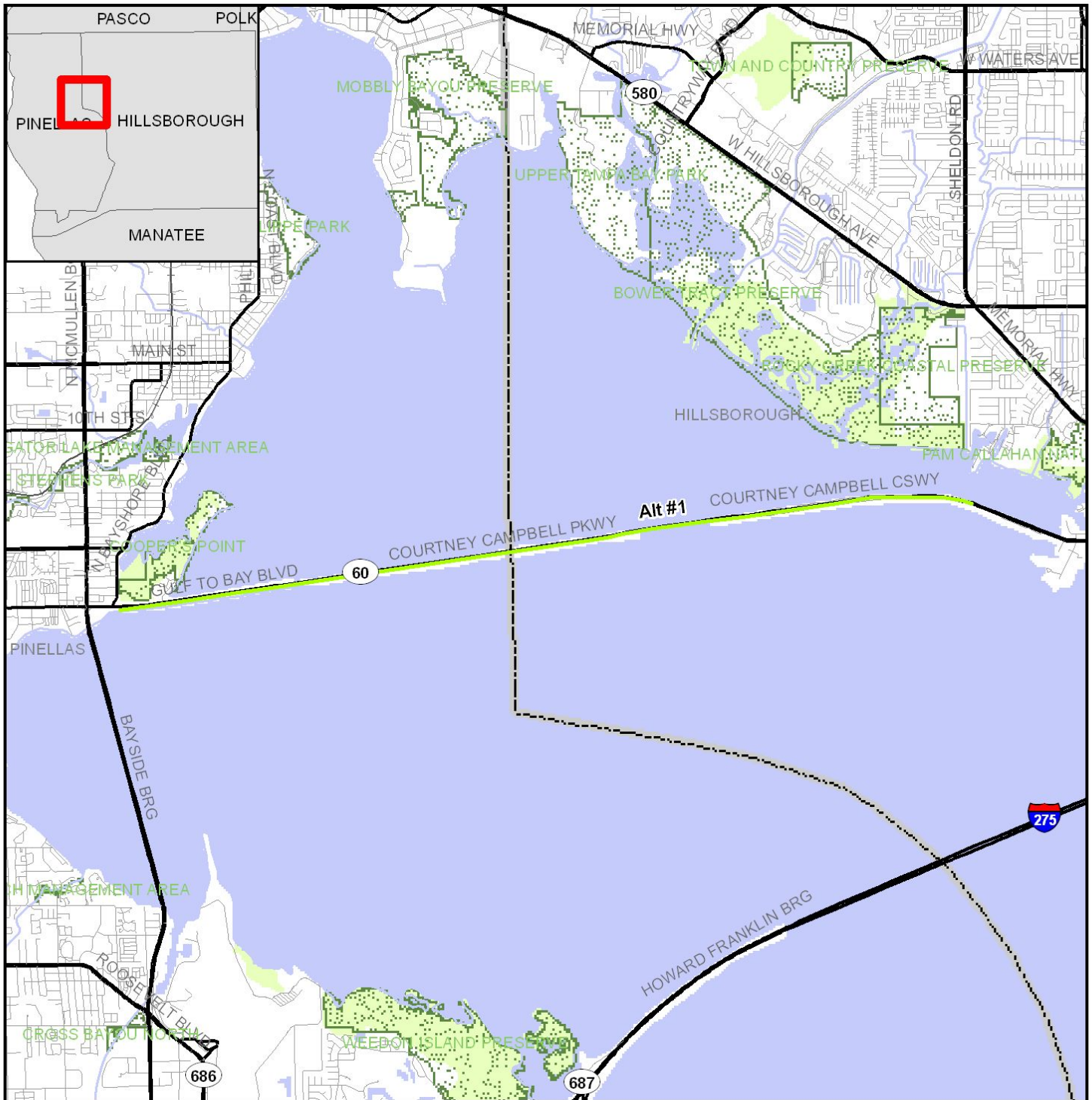
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# 13102 SR 60 Trail PD&E Study

## Bayshore Boulevard to W of Ben T. Davis Bch Entrance



0 2 Miles



Data Sources:  
 Geographic Data Technology, Inc.  
 US Geological Survey  
 Florida Natural Areas Inventory

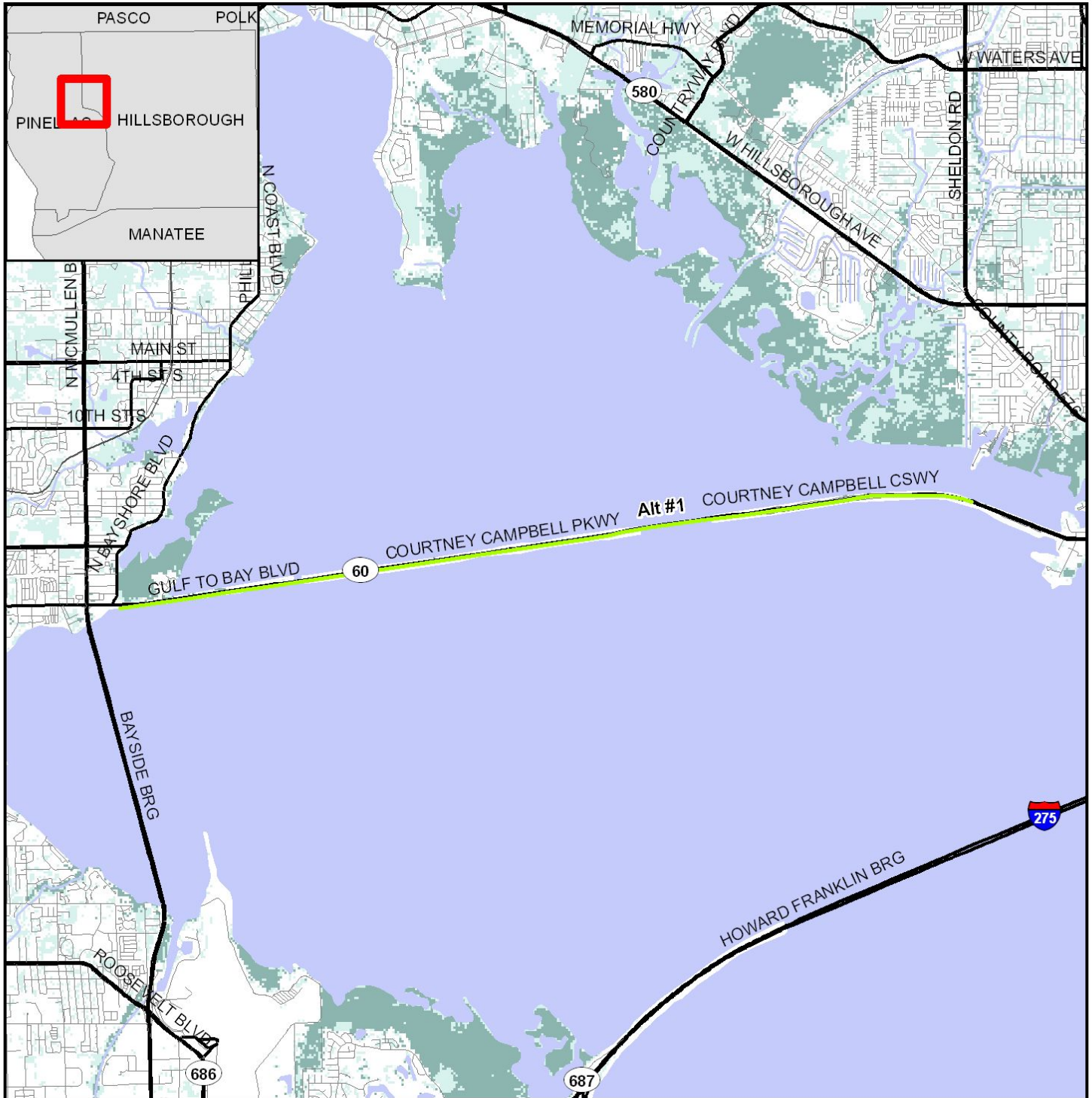
- |                           |                        |                                 |
|---------------------------|------------------------|---------------------------------|
| ETDM Alternative Point    | River, Stream or Canal | Conservation or Recreation Area |
| ETDM Alternative Segment  | Water Body             | Major Road                      |
| ETDM Alternative Polygon  | Swamp/Marsh            | Local Road or Trail             |
| ETDM Alternative Terminus |                        | Railroad                        |
|                           |                        | County Boundary                 |

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# 13102 SR 60 Trail PD&E Study

## Bayshore Boulevard to W of Ben T. Davis Bch Entrance



0 1 Miles



### Species Potential Habitat Model Map

- |  |                          |   |
|--|--------------------------|---|
| <span style="color: green;">■</span> ETDM Alternative Point  | — Major Road             | <span style="background-color: #e0f0e0; border: 1px solid black; display: inline-block; width: 15px; height: 10px;"></span> 1 - 2 Species   |
| <span style="color: magenta;">●</span> ETDM Alternative Terminus   | — Local Road or Trail    | <span style="background-color: #c0d0c0; border: 1px solid black; display: inline-block; width: 15px; height: 10px;"></span> 3 - 5 Species   |
| <span style="color: green; border-bottom: 2px solid green; width: 20px; display: inline-block;"></span> ETDM Alternative Segment                     | — Railroad               | <span style="background-color: #a0b0a0; border: 1px solid black; display: inline-block; width: 15px; height: 10px;"></span> 6 - 8 Species   |
| <span style="background-color: #e0ffe0; border: 1px solid green; width: 15px; height: 10px; display: inline-block;"></span> ETDM Alternative Polygon | — River, Stream or Canal | <span style="background-color: #809080; border: 1px solid black; display: inline-block; width: 15px; height: 10px;"></span> 9 - 10 Species  |
|  | ■ Water Body             | <span style="background-color: #607060; border: 1px solid black; display: inline-block; width: 15px; height: 10px;"></span> 11 - 13 Species |

Data Sources:  
 Geographic Data Technology, Inc.  
 US Geological Survey  
 Florida Department of Transportation  
 Florida Fish & Wildlife Conservation Commission

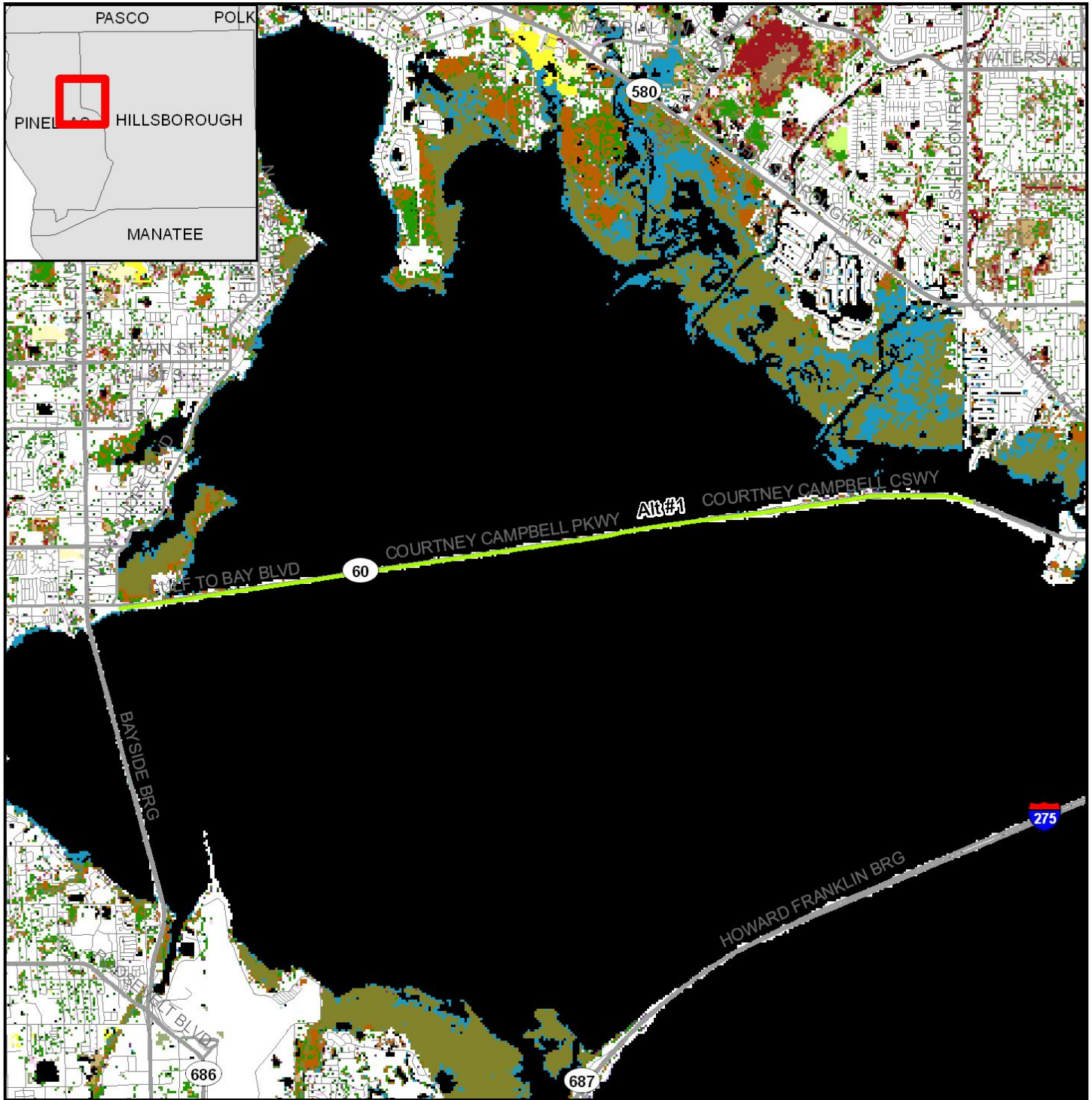
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# 13102 SR 60 Trail PD&E Study

## Bayshore Boulevard to W of Ben T. Davis Bch Entrance



**Vegetation and Land Cover Map**

- |                           |                            |                                  |                            |                     |                     |                   |
|---------------------------|----------------------------|----------------------------------|----------------------------|---------------------|---------------------|-------------------|
| ETDM Alternative Polygon  | Not Classified             | Hardwood Hammocks and Forests    | Bay Swamp                  | Mangrove Swamp      | Unimproved Pasture  | Brazilian Pepper  |
| ETDM Alternative Segment  | Coastal Strand             | Pinelands                        | Cypress Swamp              | Scrub Mangrove      | Sugarcane           | High Impact Urban |
| ETDM Alternative Terminus | Sand/Beach                 | Cabbage Palm-live Oak Hammock    | Cypress/Pine/Cabbage Palm  | Tidal Flats         | Citrus              | Low Impact Urban  |
| ETDM Alternative Point    | Xeric Oak Scrub            | Tropical Hardwood Hammock        | Mixed Wetland Forest       | Open Water          | Row and Field Crops | Extractive        |
| Major Road                | Sand Pine Scrub            | Freshwater Marsh and Wet Prairie | Hardwood Swamp             | Shrub and Brushland | Other Agriculture   |                   |
| Local Road or Trail       | Sandhill                   | Sawgrass Marsh                   | Hydric Hammock             | Grassland           | Exotic Plants       |                   |
|                           | Dry Prairie                | Cattail Marsh                    | Bottomland Hardwood Forest | Bare Soil/Clearcut  | Australian Pine     |                   |
|                           | Mixed Hardwood-pine Forest | Shrub Swamp                      | Salt Marsh                 | Improved Pasture    | Melaleuca           |                   |

**Data Sources:**

Geographic Data Technology, Inc.; Florida Department of Transportation; Florida Fish and Wildlife Conservation Commission

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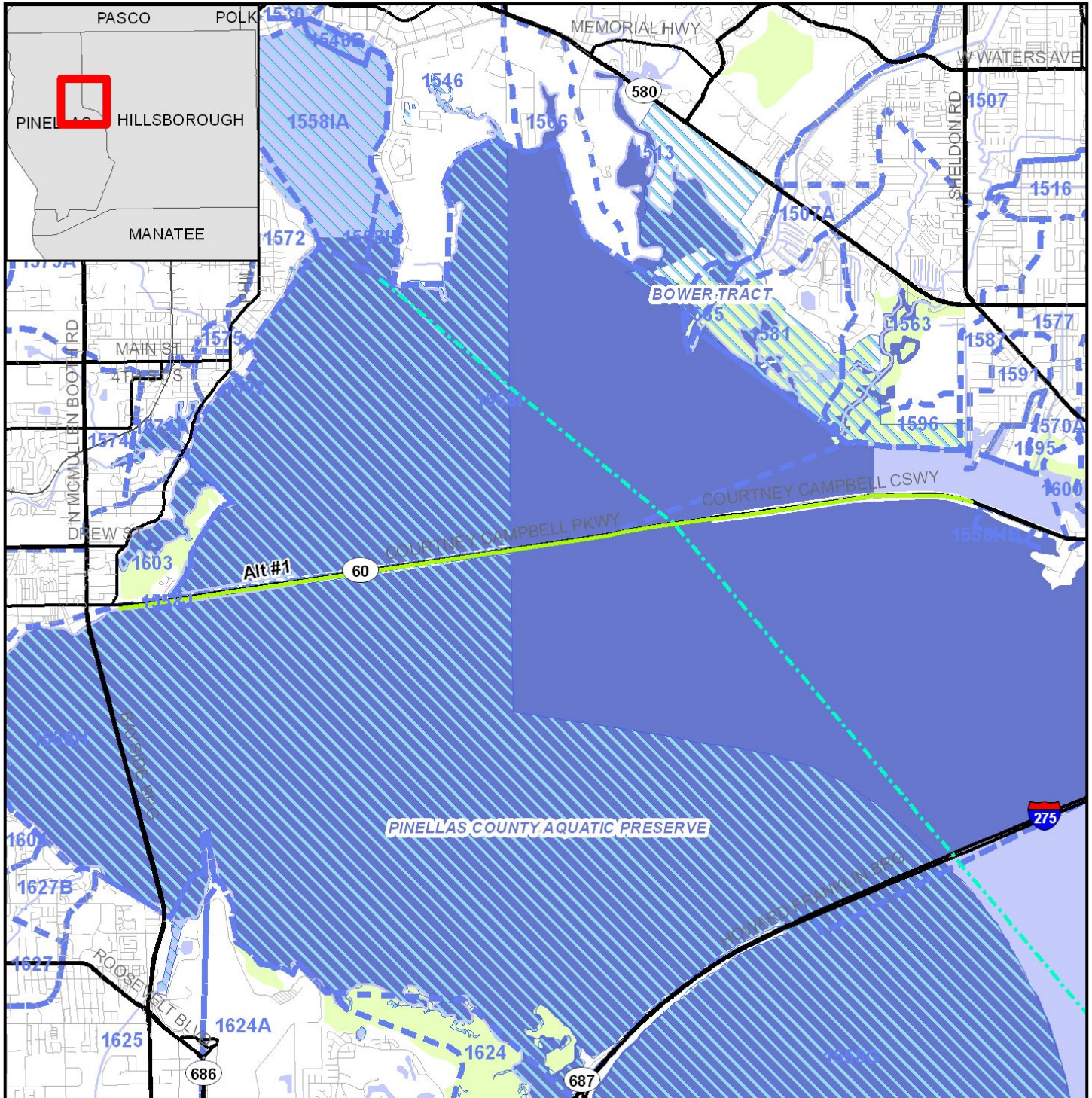
Map Generated on: 12/14/2010





# 13102 SR 60 Trail PD&E Study

## Bayshore Boulevard to W of Ben T. Davis Bch Entrance



0 2 Miles

### Water Resources Map

- |                           |                     |                           |                        |
|---------------------------|---------------------|---------------------------|------------------------|
| ETDM Alternative Point    | Major Road          | 1st Magnitude Spring      | Surface Water Class I  |
| ETDM Alternative Terminus | Local Road or Trail | River, Stream or Canal    | Surface Water Class II |
| ETDM Alternative Segment  | Railroad            | Navigable Water Way       | Water Body             |
| ETDM Alternative Polygon  |                     | Drainage Basin            | Swamp/Marsh            |
|                           |                     | Outstanding Florida Water |                        |

**Data Sources:**

Geographic Data Technology, Inc.      Florida Department of Transportation      Florida Geological Survey  
 US Geological Survey      Florida Department of Environmental Protection      US Bureau of Transportation Statistics

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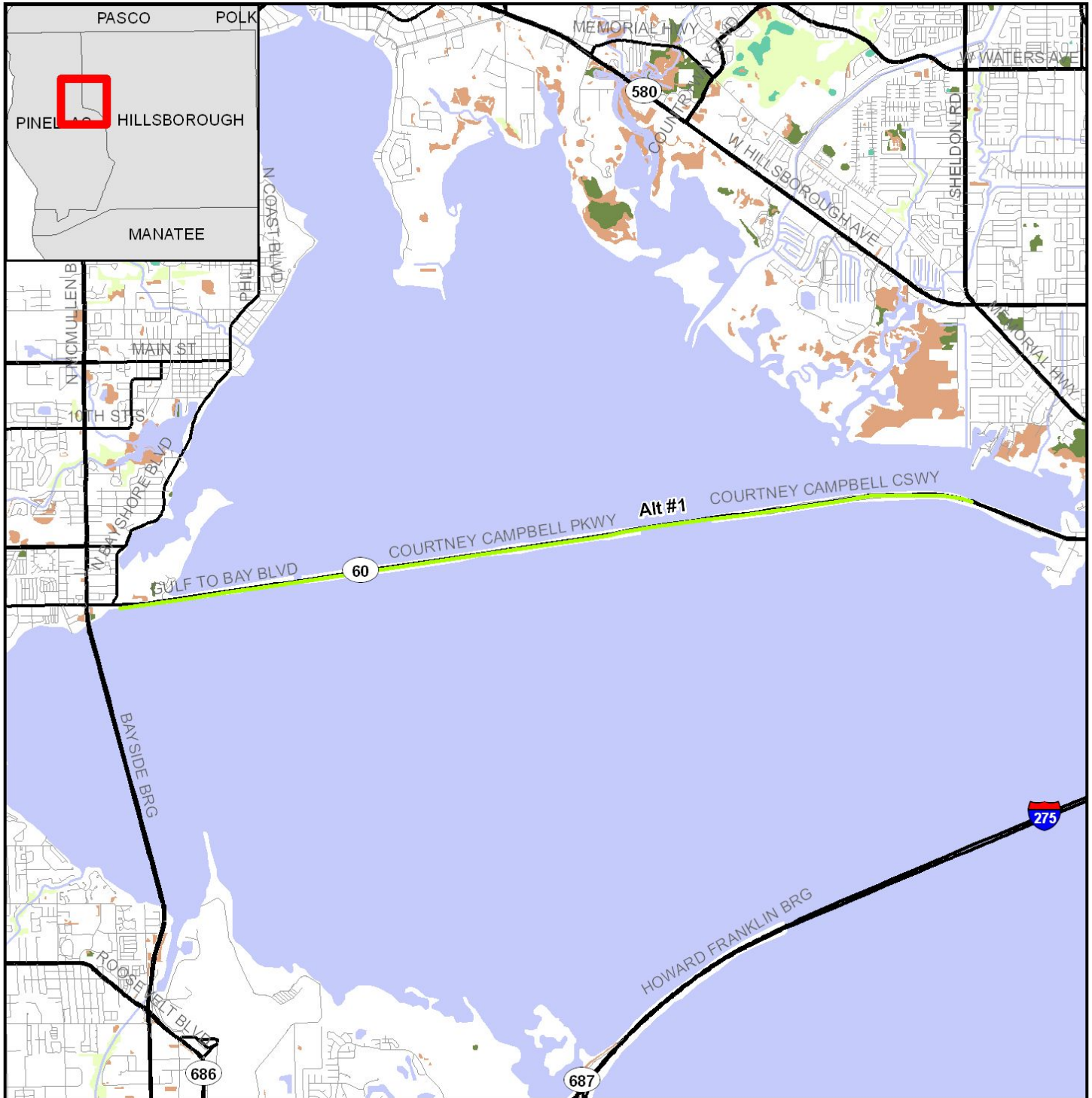
Map Generated on: 12/14/2010





# 13102 SR 60 Trail PD&E Study

## Bayshore Boulevard to W of Ben T. Davis Bch Entrance



**Wetland Resource Map**

- |  |                           |  |                        |  |                                |
|--|---------------------------|--|------------------------|--|--------------------------------|
|  | ETDM Alternative Polygon  |  | River, Stream or Canal |  | Non-vegetated Wetland          |
|  | ETDM Alternative Segment  |  | Water Body             |  | Vegetated Non-forested Wetland |
|  | ETDM Alternative Terminus |  |                        |  | Wetland Forested Mixed         |
|  | ETDM Alternative Point    |  |                        |  | Wetland Coniferous Forest      |
|  | Major Road                |  |                        |  | Wetland Hardwood Forest        |
|  | Local Road or Trail       |  |                        |  |                                |

Data Sources: Geographic Data Technology, Inc.; Florida Water Management Districts; US Geological Survey

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**Appendices**

**Degree of Effect Legend**

Legend			
Color Code	Meaning	ETAT	Public Involvement
N/A	Not Applicable / No Involvement	There is no presence of the issue in relationship to the project, or the issue is irrelevant in relationship to the proposed transportation action.	
0	None (after 12/5/2005)	The issue is present, but the project will have no impact on the issue; project has no adverse effect on ETAT resources; permit issuance or consultation involves routine interaction with the agency. The <i>None</i> degree of effect is new as of 12/5/2005.	No community opposition to the planned project. No adverse effect on the community.
1	Enhanced	Project has positive effect on the ETAT resource or can reverse a previous adverse effect leading to environmental improvement.	Affected community supports the proposed project. Project has positive effect.
2	Minimal	Project has little adverse effect on ETAT resources. Permit issuance or consultation involves routine interaction with the agency. Low cost options are available to address concerns.	Minimum community opposition to the planned project. Minimum adverse effect on the community.
2	Minimal to None (assigned prior to 12/5/2005)	Project has little adverse effect on ETAT resources. Permit issuance or consultation involves routine interaction with the agency. Low cost options are available to address concerns.	Minimum community opposition to the planned project. Minimum adverse effect on the community.
3	Moderate	Agency resources are affected by the proposed project, but avoidance and minimization options are available and can be addressed during development with a moderated amount of agency involvement and moderate cost impact.	Project has adverse effect on elements of the affected community. Public Involvement is needed to seek alternatives more acceptable to the community. Moderate community interaction will be required during project development.
4	Substantial	The project has substantial adverse effects but ETAT understands the project need and will be able to seek avoidance and minimization or mitigation options during project development. Substantial interaction will be required during project development and permitting.	Project has substantial adverse effects on the community and faces substantial community opposition. Intensive community interaction with focused Public Involvement will be required during project development to address community concerns.
5	Potential Dispute (Planning Screen)	Project may not conform to agency statutory requirements and may not be permitted. Project modification or evaluation of alternatives is required before advancing to the LRTP Programming Screen.	Community strongly opposes the project. Project is not in conformity with local comprehensive plan and has severe negative impact on the affected community.
5	Dispute Resolution (Programming Screen)	Project does not conform to agency statutory requirements and will not be permitted. Dispute resolution is required before the project proceeds to programming.	Community strongly opposes the project. Project is not in conformity with local comprehensive plan and has severe negative impact on the affected community.
	No ETAT Consensus	ETAT members from different agencies assigned a different degree of effect to this project, and the ETDM coordinator has not assigned a summary degree of effect.	
	No ETAT Reviews	No ETAT members have reviewed the corresponding issue for this project, and the ETDM coordinator has not assigned a summary degree of effect.	

**GIS Analyses**

Since there are so many GIS Analyses available for Project #13102 - SR 60 Trail PD&E Study, they have not been included in this ETDM Summary Report. GIS Analyses, however, are always available for this project on the Public ETDM Website. Please click on the link below (or copy this link into your Web Browser) in order to view detailed GIS tabular information for this project:

<http://etdmpub.fl-a-etat.org/est/index.jsp?tpID=13102&startPageName=GIS%20Analysis%20Results>

**Special Note:** Please be sure that when the GIS Analysis Results page loads, the **Programming Screen Summary Report Re-published on 03/29/2011 by Wendy Lasher Milestone** is selected. GIS Analyses snapshots have been taken for Project #13102 at various points throughout the project's life-cycle, so it is important that you view the correct snapshot.



# APPENDIX B

Advance Notification and Agency Comment





## Florida Department of Transportation

**CHARLIE CRIST**  
GOVERNOR

11201 N. McKinley Drive  
Tampa, FL 33612-6456

**STEPHANIE KOPELOUSOS**  
SECRETARY

December 17, 2010

Ms. Lauren Milligan  
Environmental Manager  
Florida State Clearinghouse  
Florida Department of Environmental Protection  
3900 Commonwealth Blvd, MS 47  
Tallahassee, FL 32399-3000

Dear Ms. Milligan:

**SUBJECT: Advance Notification  
SR 60 (Courtney Campbell Causeway) Multi-Use Trail PD&E Study/  
From Bayshore Boulevard to West of Ben T. Davis Beach Entrance  
ETDM # 13102  
Financial Project ID Number: 422640-2-22-01  
Pinellas & Hillsborough Counties, Florida**

The Florida Department of Transportation (FDOT) is conducting a Project Development and Environment (PD&E) study for a multi-use trail that will be constructed along the Courtney Campbell Causeway (SR 60) from Bayshore Boulevard in Pinellas County to West of Ben T. Davis Beach entrance in Hillsborough County.

We are sending this Advance Notification (AN) Package to your office for distribution to State agencies that conduct Federal consistency reviews (consistency reviewers) in accordance with the Coastal Zone Management Act and Presidential **Executive Order 12372**. We are also distributing the AN Package to local and federal agencies. Although we will request specific comments during the permitting process, we are asking that permitting and permit reviewing agencies (consistency reviewers) review the attached information and provide us with their comments.

This is a Federal-aid action and the FDOT District Seven, in consultation with the Federal Highway Administration, will determine what type of environmental documentation will be necessary. The determination will be based upon the selected consultant environmental evaluations and comments from other agencies. Please provide a consistency review for this project in accordance with the State's Coastal Zone Management Program.

In addition, please review the project's consistency, to the maximum extent feasible, with the approved Comprehensive Plan of the local government to comply with **Chapter 163 of the Florida Statutes**.

FDOT District Seven is submitting this project through the Programming Screen of the Efficient Transportation Decision Making (ETDM) Environmental Screening Tool (EST) in coordination

Ms. Milligan  
ETDM #13102  
December 16, 2010  
Page 2

with this AN Package. The project is listed as **ETDM #13102 - SR 60 (Courtney Campbell Bridge) Multi-Use Trail PD&E Study**. Environmental Technical Advisory Team (ETAT) members should review this project on the ETDM website. Non-ETAT agencies can review this project at the public access website located at: <http://etdmpub.fl.a-etat.org/est>.

We are looking forward to receiving your comments on the project. Consistency reviewers have 45 days from the Programming Screen Notification to provide their comments. Once you have received their comments, you will supply a summary and consistency determination for your agency within 60 days of the Programming Screen Notification. If you need more review time, send a written request for an extension to our office within the initial 60 days comment period.

Your comments should be addressed to:

Ming Gao, P.E.  
Department Head, Intermodal Systems Development  
Florida Department of Transportation, District 7  
11201 N. McKinley Drive / MS 7-500  
Tampa, FL 33612-6456

Your expeditious handling of this notice will be appreciated.

Sincerely,



Ming Gao, P.E.  
Department Head, Intermodal Systems Development

MG/kb  
Attachment

ADVANCE NOTIFICATION MAILING LIST

CC:

Federal Highway Administration, Division Administrator  
Federal Highway Administration – **ETAT Representative**  
Federal Emergency Management Agency-Mitigation Division, Chief  
Federal Railroad Administration  
Federal Transit Administrator – **ETAT Representative**  
U.S. Department of the Interior-Bureau of Land Management, Eastern States Office  
U.S. Department of Housing and Urban Development, Regional Environmental Officer  
U.S. Department of the Interior-U.S. Geological Survey, Chief  
U.S. Environmental Protection Agency - **ETAT Representative**  
U.S. Department of Interior-U.S. Fish and Wildlife Service - **ETAT Representative**  
U.S. Army Corps of Engineers-Regulatory Branch - **ETAT Representative**  
U.S. Department of Commerce-National Marine Fisheries Service- Southeast  
U.S. Department of Commerce-National Marine Fisheries Service - Southeast  
Regional Superintendent Conservation Division - **ETAT Representative**  
U.S. Department of Agriculture – Southern Region  
U.S. Department of Interior – National Park Service – Southeast Regional Office –  
**ETAT Representative**  
Federal Aviation Administration, Airports District Office  
U.S. Department of Health and Human Services-National Center for Environmental  
Health  
U.S. Department of Interior-Bureau of Indian Affairs-Office of Trust Responsibilities  
U.S. Coast Guard – Seventh District – Commander (oan) – **ETAT Representative**  
Florida Inland Navigation District  
Poarch Band of Creek Indians of Alabama  
Muscogee (Creek) Nation of Oklahoma  
Seminole Tribe of Florida  
Miccosukee Tribe of Indians of Florida  
Seminole Nation of Oklahoma  
Mississippi Band of Choctaw Indians  
Florida Fish and Wildlife Conservation Commission - **ETAT Representative**  
U.S. Forest Service – **ETAT Representative**  
Florida Department of Environmental Protection - **ETAT Representative**  
Florida Department of Environmental Protection – **State Clearinghouse**  
Florida Department of State - **ETAT Representative**  
Florida Department of Community Affairs - **ETAT Representative**  
Florida Department of Agriculture and Consumer Services - **ETAT Representative**  
Federal Transit Administrator - **ETAT Representative**  
Tampa Bay Regional Planning Council  
Southwest Florida Water Management District - **ETAT Representative**  
FDOT Environmental Management Office, Engineer/Manager  
Hillsborough County Metropolitan Planning Organization  
Pinellas County Metropolitan Planning Organization  
Environmental Protection Commission of Hillsborough County



**Advance Notification Package (abridged)**

**Project #13102 - SR 60 Trail PD&E Study  
Programming Screen - Published on 12/16/2010  
Printed on: 12/16/2010**

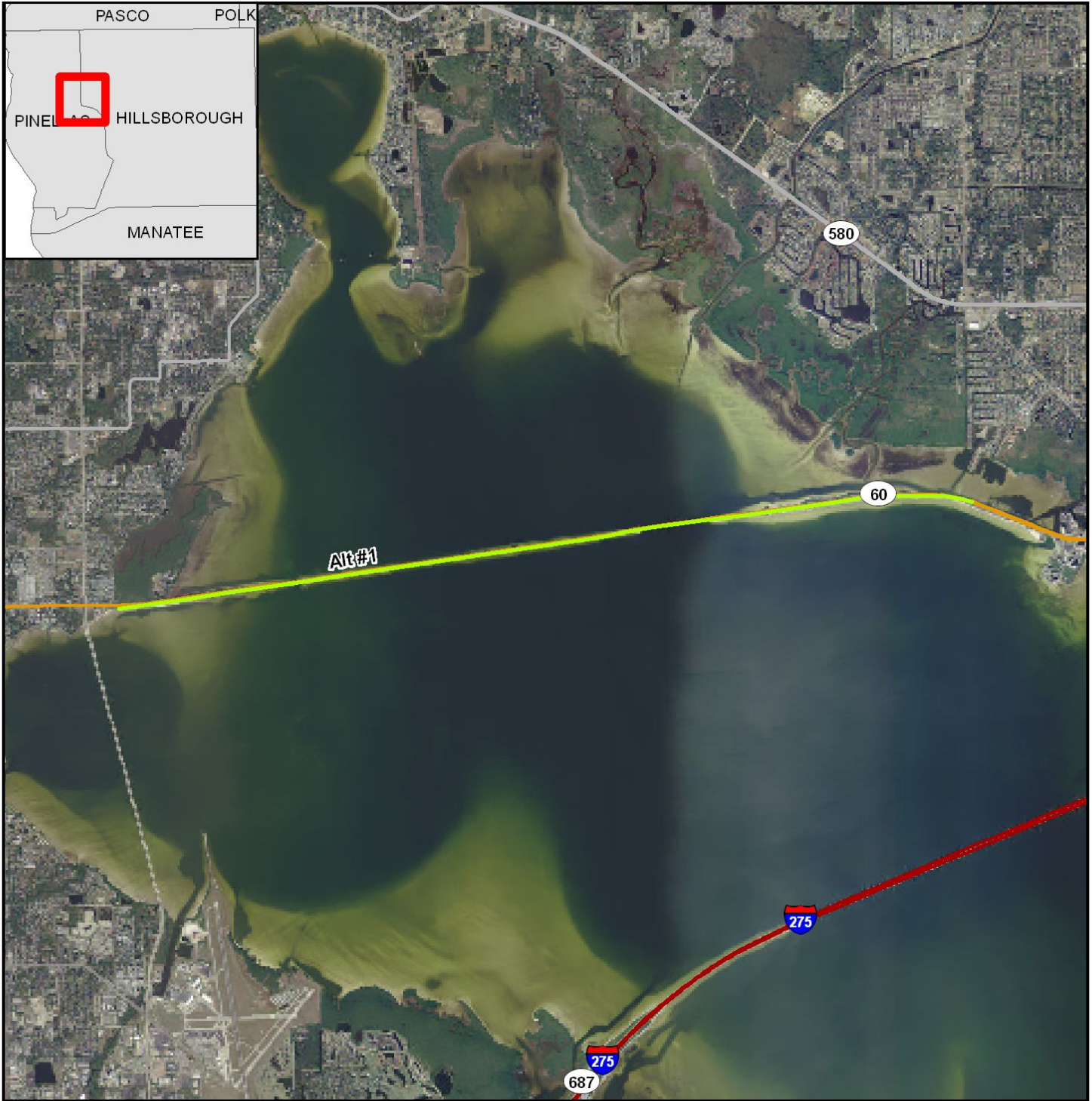
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# Location Maps

# 13102 SR 60 Trail PD&E Study

## Bayshore Boulevard to W of Ben T. Davis Bch Entrance



**Project Aerial Map**

0 2 Miles



- ETDM Alternative Point     — Primary and Limited Access Highway
- ETDM Alternative Terminus     — Secondary, Unlimited Access Highway
- ETDM Alternative Segment     — Other Highway Feature
- ETDM Alternative Polygon

Data Sources:  
 Highways - Geographic Data Technology, Inc.  
 Digital Orthophotograph - US Geological Survey

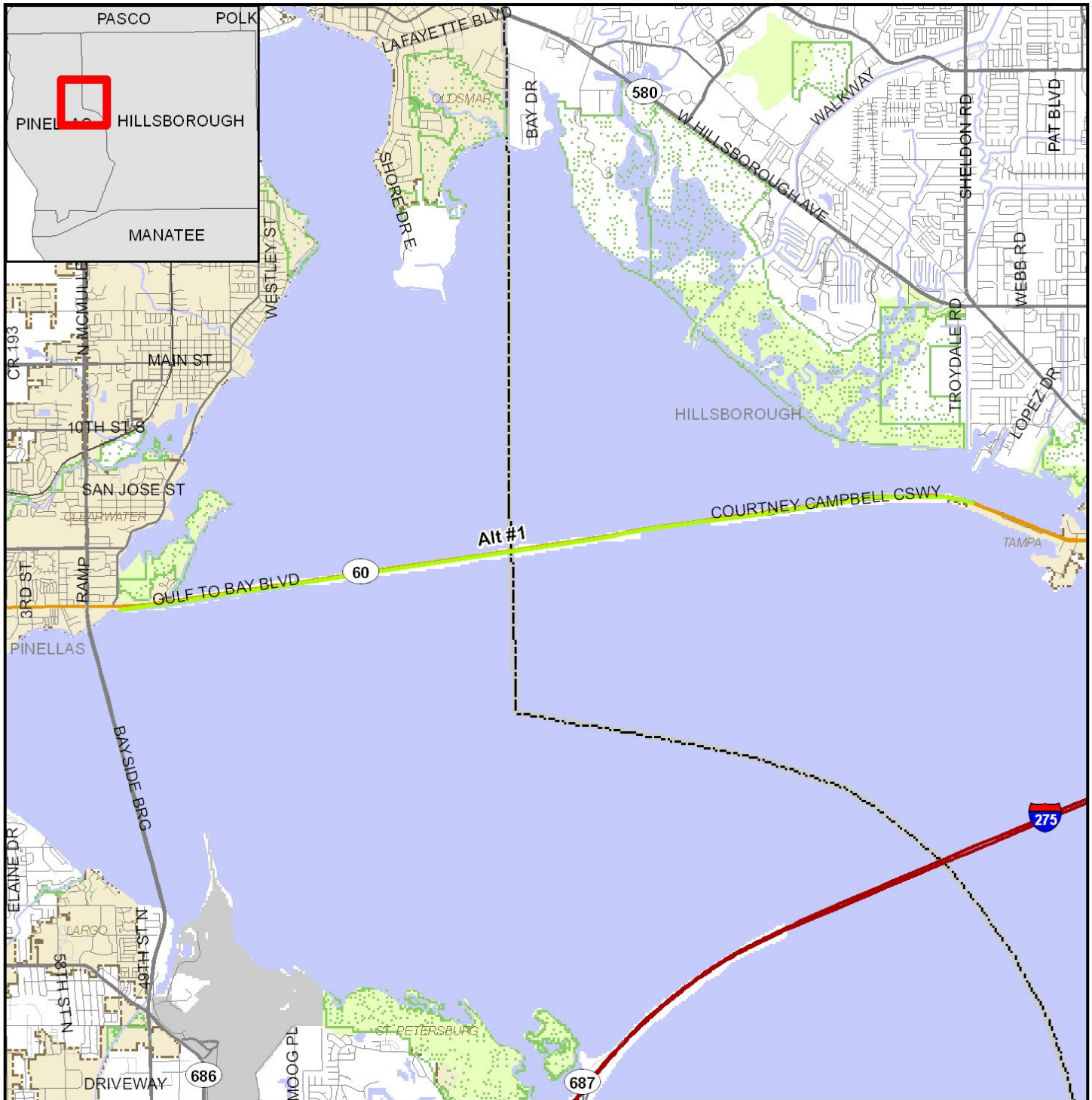
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# 13102 SR 60 Trail PD&E Study

## Bayshore Boulevard to W of Ben T. Davis Bch Entrance



**Project Location Map**

0 1 Miles



**Data Sources:**

- Geographic Data Technology, Inc.
- US Geological Survey
- US Census Bureau
- County Property Appraisers
- Florida Natural Areas Inventory

- |                           |                                     |                   |
|---------------------------|-------------------------------------|-------------------|
| ETDM Alternative Point    | Managed Conservation Lands          | Toll Road         |
| ETDM Alternative Terminus | Primary and Limited Access Highway  | Railroad          |
| ETDM Alternative Segment  | Secondary, Unlimited Access Highway | Airport           |
| ETDM Alternative Polygon  | Connecting Road                     | City Limits       |
| River, Stream or Canal    | Local Road or Trail                 | County Boundaries |
| Water Body                | Other Roadway Feature               |                   |
| Swamp/Marsh               |                                     |                   |

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DISCLAIMER: The Fact Sheet data consists of the most up-to-date information available at the time the Advance Notification Package is published. Updates to this information may be found on the ETDM website at <http://etdmpub.fl.a-etat.org>

**Special Note:** Please be aware of the selected Milestone date when viewing project data on the ETDM website. Snapshots of project and analysis data have been taken for Project #13102 at various points throughout the project's life-cycle. On the website these **Project Milestone Dates** are listed in the the project header immediately after the project contact information. Click on any of the dates listed to view the information available on that date.

Project Description			
#13102 SR 60 Trail PD&E Study			
District	District 7	Phase	Programming Screen
County	Hillsborough , Pinellas	From	Bayshore Boulevard
Planning Organization	FDOT District 7	To	W of Ben T. Davis Bch Entrance
Plan ID		Financial Management No.	42264022201
Federal Involvement	Federal Permit Federal Action Federal Funding		
Contact Information	Name: Steve Love Phone: (813) 975-6410 E-mail: steve.love@dot.state.fl.us		

### Project Description Data

#### Description Statement

The proposed project is a multi-use trail that will be constructed along Courtney Campbell Causeway (SR 60) from the vicinity of the proposed Bayshore Trail extension (Bayshore Blvd. at SR 60) in Pinellas County to West of Ben T. Davis Beach entrance in Hillsborough County. Courtney Campbell Causeway is classified as a scenic highway, and the proposed multi-use trail is consistent with the Local Government Comprehensive Plans (LGCP) for both City of Clearwater and City of Tampa; the Corridor Management Plan (CMP); the Cost Feasible Plan of the Pinellas County 2035 Long Range Transportation Plan (LRTP) adopted December 9, 2009 (Figure 25-Pinellas County Trailways Plan / Page 119, Table 62 - Planned Cost Feasible Trailway Projects / Figure 39 -2009 Regional Multi-Use Trails Network); and the Cost Affordable Plan of the Hillsborough County 2035 LRTP amended August 3, 2010 (Map 10-2 - Bicycle and Trails Cost Affordable / Map 10-3 - Sidewalks Cost Affordable / Appendix B, Page 5, Table B-1 - Cost Affordable Highway, Bicycle, and Pedestrian Projects / Appendix E, Page 4 - Cost Affordable Bicycle and Trails Projects and Unfunded Needs). The proposed facility is intended for bicycle, pedestrian, and other recreational users, thereby providing alternate modes of transportation. The Multi-Use Trail Feasibility Study from McMullen Booth to Veterans Expressway - WPI: 422640 1 and FAP No. 9045-090-C (2008 Feasibility Study) was completed in December 2008 for this project (refer to the project documents section of the project description in the Environmental Screening Tool). The project length is approximately four (4) miles. The majority of the proposed project is intended to be constructed on the SR 60 fill section and not within the waters of Tampa Bay. The only portions of the proposed project that would be constructed within the waters of Tampa Bay would be the proposed bridges where the main span and the western relief structures are located. These locations are available for viewing on sheet nos. 7, 18, 19, 20 and 21 of Appendix A of the above referenced 2008 Feasibility Study. The study evaluated four (4) separate alternatives and one (1) interim staging option. More details of these alternatives can also be viewed in the Project Concept Summary Report of the project documents section in the Environmental Screening Tool.

The trail alternatives as described in the Project Concept Summary Report are located on the north and south sides of the Causeway and include either the Structural Option 'W2' (widening with piles in the water) or Structural Option 'IS' (Independent Structure). There are three (3) structures within the project limits of the previous 2008 Feasibility Study. The alternatives are described as follows:

Alternative N1 - This alternative includes the trail on the north side of the Causeway and the Structural Widening Option 'W2' for Structures 1 and 2, and the reconfiguration of Structure 3. The associated cost of this alternative based on 2008 estimates is \$60.8M

Alternative N2 - This alternative includes the trail on the north side of the Causeway and the Independent Structural Option 'IS' for Structures 1, 2, and 3. The associated cost of this alternative based on 2008 estimates is \$30.9M

Alternative S1 - This alternative includes the trail on the south side of the Causeway and the Structural Widening Option 'W2' for Structures 1 and 2, and the reconfiguration of Structure 3. The associated cost of this alternative based on 2008 estimates is \$63.2M

Alternative S2 - This alternative includes the trail on the south side of the Causeway and the Independent Structural Option 'IS' for Structures 1, 2, and 3. The associated cost of this alternative based on 2008 estimates is \$33.3M

Staging Option S3 - This is an interim staging option which will provide a shared-use facility on the existing causeway prior to the construction of any new water crossings

There are two bridges within this PD&E study limits. Structure 1, Bridge No. 150138 (Tampa Bay Bridge) is located at the west end of the study and Structure 2, Bridge No. 100301, is located just east of Structure 1. The existing bridges are prestressed concrete girder facilities that were originally built in 1974. The four trail alternatives from the 2008 Feasibility Study considered both widening of the existing bridges and constructing separate trail bridges. The intention of the separate bridges is to utilize separate structures to accommodate the trail for non motorized vehicles and pedestrians. The separate bridges will be designed to accommodate the heaviest required vehicle to perform routine maintenance and inspection.

The trail dimensions vary depending on its location along the project limits (causeway or bridge). The bridge typical section is planned as 16 feet clear width (12' trail plus 2@2' shoulders). Along the causeway, a 12-foot wide multi-use trail is proposed. Improvements are proposed to be constructed within the existing SR 60 Right-of-Way. The trail surfaces proposed for this project include asphalt along the causeway segment and a concrete deck along the bridges.

During the 2008 Feasibility Study, two newsletters were sent out in October 2007 and April 2008. Also, two informal Public Workshops were held on May 19, 2008 and May 22, 2008 in Hillsborough and Pinellas Counties, respectively. Twenty three comments were received with fourteen (14) being in favor, seven (7) offered no opinion and two (2) citizens were against the project. The main concerns of the citizens against the project were "that millions of dollars should not be spent on expanding a road that work perfectly fine but on education and schools." They were also concerned that construction of this project would affect their commute to work.

### **Purpose and Need Statement**

The purpose of this project is to evaluate a proposed multi-use trail along Courtney Campbell Causeway (SR 60) from Bayshore Blvd. to W. of Ben T. Davis Beach entrance to accommodate recreational users that can experience the scenic qualities of the Causeway, further enhancing tourism and economic development. The proposed Courtney Campbell Causeway Multi-Use Trail has been identified in the Comprehensive Plans of the following jurisdictions: Hillsborough County; Pinellas County; City of Tampa; and the City of Clearwater. The trail has also been identified in the City of Tampa Greenways & Trails Master Plan (2001), the City of Clearwater Bikeways and Trails Plan (1996) and Shifting Gears: Clearwater's Bicycle and Pedestrian Master Plan (2007). A portion of this project is currently funded for design-build in FY 2011/2012 in the FDOT Tentative Work Program 2011-2016. Transportation Improvement Program (TIP) Amendments are currently being processed to facilitate this segment. The proposed trail will serve as a link in a regional network of trail systems serving the Tampa Bay region. As a needed east-west link, the trail will provide regional connectivity with the trail networks for the jurisdictions noted above. In providing the east-west link, regional connectivity could be further enhanced offering alternative modes of transportation in the region.

Beyond the trail's transportation benefits, the trail could serve the recreational needs for residents in the area. The trail could also provide linkage to a series of recreational facilities along the Causeway. It would also recreate a regional recreational opportunity to cross Tampa Bay to link Pinellas and Hillsborough Counties since the existing east-west Friendship Trail Bridge corridor along Gandy Blvd. is no longer available to users within the Tampa Bay area. The Friendship Trail Bridge is permanently closed to users since it is no longer safe to be used. The Friendship Trail Bridge is expected to be demolished once sufficient funds are available to the operating entities for the structure's demolition.

### **Safety**

The existing paved shoulders along the causeway portion of the project may be used by avid cyclists, but they do not provide safe access for recreational bikers, walkers, and families to access these amenities. In addition, the absence of shoulders on Structure 2, the main navigable crossing, further exacerbates the safety of cyclists and pedestrians along the corridor. The addition of the multi-use trail will provide for a wider range of non-motorized users.

### **Planned/Programmed Projects in the Project Area**

The following are design and construction projects planned or programmed along SR 60 in the project area:

FM No. 424561 3 - SR 60 Trail Project from Bayshore Blvd. to East of Tampa Bay Bridge (Bridge No. 150138), a distance of approximately 1.8 miles - Design is currently planned for FY 2011/2012 and Construction is planned for FY 2015/2016

FM No. 424561 4 - SR 60 Trail Project from East of Tampa Bay Bridge (Bridge No. 150138) to Pinellas/Hillsborough County Line, a distance of approximately 1.7 miles - Design is currently planned for FY 2011/2012 and Construction is planned for FY 2013/2014

FM No. 424561 1 - SR 60 Resurfacing Project from Pinellas/Hillsborough County Line to Rocky Point Drive, a distance of approximately 4.4 miles - Design is ongoing and Construction is planned for FY 2011/2012. This project also includes a small trail segment from the west entrance of Ben T. Davis Beach to Rocky Point Drive

FM No. 424561 2 - SR 60 Trail Project from Rocky Point Drive to East of Bridge # 100064, a distance of approximately 0.4 miles - Design is currently underway and Construction is planned for FY 2011/2012

FM No. 428962 1 - SR 60 Resurfacing Project from West of Damascus Road to Pinellas/Hillsborough County Line, a distance of approximately 3.4 miles - Design is currently programmed for FY 2011/2012 and Construction is planned for FY 2013/2014

### **Area Wide Network/System Linkage**

The proposed Courtney Campbell trail will provide regional linkage for non-motorized travel between Pinellas and Hillsborough Counties and, with connection to other facilities, travel into Pasco and Hernando Counties. The project will connect to other existing and planned facilities to the east and west of the Causeway. On the Pinellas (west) side, the project will connect to Pinellas County's extensive trail system (proposed Bayshore Trail extension). On the Hillsborough (east) side, the trail will connect to the West Tampa Greenway (4.6 miles of this 16.6 miles Greenway is completed to date) which will eventually connect via on-street facilities to the Upper Tampa Bay Trail and then from there to the Suncoast Parkway Trail into Pasco and Hernando Counties.

### **Modal Relationships**

There are express and local bus routes that operate along SR 60 (Courtney Campbell Causeway) and that intersect SR 60 near the proposed project area. The Hillsborough Area Regional Transit (HART) 200X route is a commuter express route that operates between downtown Tampa and the Eddie Moore Park and Ride Lot in Clearwater. This route only runs during weekday commuter rush hours. Furthermore, HART Route 30 runs near the east end of the proposed trail, and the Pinellas Suncoast Transit Authority (PSTA) Route 60 runs near the west end of the proposed trail. The combination of the existing transit routes and the proposed trail offers additional connections between Pinellas and Hillsborough Counties. The transit routes also provide additional opportunities for use of the proposed trail.

### **Social Demands or Economic Development**

There are residential, offices, and commercial land uses located at both ends of the Courtney Campbell Causeway. Rocky Point, located on the east end of the Causeway, has numerous restaurants, office buildings, residences and hotels/resorts. Also located on the east end of the Causeway is the Ben T. Davis Beach. The beaches along the corridor are located within the existing transportation right-of-way and are not considered Section 4(f) protected properties. The shorelines located along the Causeway are popular for fishing, picnicking and use of personal watercraft.

### **Summary of Public Comments**

The FDOT completed a Feasibility Study in 2008. During the study, newsletters were distributed to adjacent property owners and interested parties soliciting input. In May 2008, a public workshop was conducted in 2 separate locations (one in Pinellas County and one in Hillsborough County) to provide information to the general public and solicit input. Twenty-three written public comments were received, most of these indicated support of the project or sought additional information about the concepts. Written comments from 2 persons indicated their suggestion to re-allocate public funding necessary for this project to support education as a higher priority. The FDOT coordinated with local agencies, groups and the Courtney Campbell Causeway Scenic Highway xx during the feasibility process to seek input. The 2008 Feasibility Study is posted in the Project Documents portion of this screen, section 8.6 contains the public comment summary with support data located in Appendix E.

**Consistency**

- Consistent with Air Quality Conformity.
- Consistency information for Coastal Zone Management Program is not available.
- Consistent with Local Government Comp Plan.
- Consistent with MPO Goals and Objectives.

**Potential Lead Agencies**

- Federal Highway Administration

**Exempted Agencies**

Agency Name	Justification	Date
Federal Rail Administration	No existing or planned rail lines within project corridor	12/15/2010
US Forest Service	No US Forest land within project corridor.	12/14/2010

**Project Attachments**

Date	Type	Size	Link / Description
12/16/2010	Form SF-424: Application for Federal Assistance	125 KB	<a href="http://etdmpub.fla-etat.org/est/servlet/blobViewer?blobID=10539">http://etdmpub.fla-etat.org/est/servlet/blobViewer?blobID=10539</a> Form SF-424: Application for Federal Assistance: Form SF-424: Application for Federal Assistance
12/15/2010	Hardcopy Map (from Attach Document Tool)	235 KB	<a href="http://etdmpub.fla-etat.org/est/servlet/blobViewer?blobID=10537">http://etdmpub.fla-etat.org/est/servlet/blobViewer?blobID=10537</a> Project Location Map: Project Location Map
12/07/2010	Feasibility Study	2.38 MB	<a href="http://etdmpub.fla-etat.org/est/servlet/blobViewer?blobID=10519">http://etdmpub.fla-etat.org/est/servlet/blobViewer?blobID=10519</a> Bridge Project Questionnaire: Structure 2 - Located at Existing Bridge Number 100301
12/07/2010	Feasibility Study	2.09 MB	<a href="http://etdmpub.fla-etat.org/est/servlet/blobViewer?blobID=10518">http://etdmpub.fla-etat.org/est/servlet/blobViewer?blobID=10518</a> Bridge Project Questionnaire: Structure 1 - Located at Existing Bridge Number 150138
12/07/2010	Feasibility Study	108.94 MB	<a href="http://etdmpub.fla-etat.org/est/servlet/blobViewer?blobID=10517">http://etdmpub.fla-etat.org/est/servlet/blobViewer?blobID=10517</a> Project Concept Summary Report - Feasibility Study SR 60 (Courtney Campell Causeway): Multi-Use Trail Feasibility Study from McMullen Booth Road to Veterans Expressway

**Alternative #1**

**Alternative Description**

<b>From:</b>	Bayshore Boulevard	<b>To:</b>	W of Ben T Davis Bch Entrance
<b>Type:</b>	New Alignment	<b>Status:</b>	ETDM QA/QC
<b>Total Length:</b>	9.808 mi.	<b>Cost:</b>	
<b>Modes:</b>	Bicycle Pedestrian	<b>SIS:</b>	N

**Segment Description(s)**

Location and Length							
Segment No.	Name	Beginning Location	Ending Location	Length (mi.)	Roadway Id	BMP	EMP
Segment #1				7.473	Digitized		
Jurisdiction and Class							
Segment No.	Jurisdiction	Urban Service Area		Functional Class			
Segment #1	FDOT	In		N/A			
Base Conditions							
Segment No.	Year	AADT	Lanes	Config			
Segment #1							
Interim Plan							
Segment No.	Year	AADT	Lanes	Config			
Segment #1							
Needs Plan							
Segment No.	Year	AADT	Lanes	Config			
Segment #1	2035						
Cost Feasible Plan							
Segment No.	Year	AADT	Lanes	Config			
Segment #1	2035						
Funding Sources							
Segment No.	FEDERAL	Unknown					
Segment #1	\$13,479,950.00						

## Eliminated Alternatives

No eliminated alternatives present.

## Community-Desired Features

No Data Available

## Screening Summary Overview

Not Applicable

## Environmental Information

The following tables show results of standard data analyses that compare the locations of the project alternatives with locations of various environmental resources, as recorded in the ETDM Geographic Information System database. This report provides results for various resources within 500 feet from the center of the planned corridor. Results for additional types of resources and buffer distances may be viewed on the ETDM Environmental Screening Tool web site, or may be requested from the project contact as indicated on the Advance Notification cover letter. Public access to the ETDM Environmental Screening Tool is provided by the Florida Department of Transportation at the following web address: <http://etdmpub.fl.a-etat.org>

### Coastal Zone Consistency Review Is Required?

YES

### Potential Navigable Waterway Crossing Features Found?

YES

## Alternative #1

### Alternative #1 Summary

Analysis Type	Date Run	0 ft.	500 ft.		1320 ft.	
		Count	Count	Acres	Count	Acres
<b>Land Uses</b>						
District 7 Generalized Landuse	--	--	--	--	--	--
<b>Wetlands</b>						
National Wetlands Inventory	12/14/2010	--	20	665.46	--	--
SWFWMD Wetlands 2008	12/14/2010	--	3	16.02	--	--
<b>Floodplains</b>						
DFIRM FLOOD HAZARD ZONES	12/14/2010	--	7	928.53	--	--
FEMA Flood Insurance Rate Maps 1996	12/14/2010	--	14	923.68	--	--
<b>Wildlife and Habitat</b>						
2003 FFWCC Habitat and Landcover GRID	12/14/2010	--	--	923.68	--	--
2008 SWFWMD FL Land Use and Land Cover	12/14/2010	--	14	921.83	--	--
Florida Managed Areas	12/14/2010	--	1	1.29	--	--
Florida Natural Areas Inventory Managed Lands	--	--	--	--	--	--
Strategic Habitat and Conservation Areas 2000	--	--	--	--	--	--
<b>Outstanding Florida Waters</b>						
Other Outstanding Florida Waters	12/14/2010	--	1	303.34	--	--
<b>Aquatic Preserves</b>						
List of Aquatic Preserves	12/14/2010	--	1	303.38	--	--
<b>Cultural Resources</b>						
Field Survey Project Boundaries	12/14/2010	--	2	474.16	--	--
Florida Site File Cemeteries	12/14/2010	--	0	0.0	--	--
Florida Site File Historic Bridges	12/14/2010	--	0	0.0	--	--
Florida Site File Historic Standing Structures	12/14/2010	--	0	0.0	--	--
Resource Groups	12/14/2010	--	0	0.0	--	--
<b>Coastal Barrier Resources</b>						
Coastal Barrier Resource System	12/14/2010	--	0	0.0	--	--
<b>Contamination</b>						
Brownfield Location Boundaries	12/14/2010	--	0	0.0	--	--
FDEP Off Site Contamination Notices	12/14/2010	--	0	0.0	--	--



National Priority List Sites	12/14/2010	--	0	0.0	--
Solid Waste Facilities	12/14/2010	--	0	0.0	--
Superfund Hazardous Waste Sites	12/14/2010	--	0	0.0	--
Toxic Release Inventory Sites	12/14/2010	--	0	0.0	--
<b>Sole Source Aquifer</b>					
Sole Source Aquifers	12/14/2010	--	0	0.0	--
<b>Noise Sensitive Facilities</b>					
Geocoded Health Care Facilities	12/14/2010	--	0	0.0	--
Geocoded Laser Facilities	12/14/2010	--	0	0.0	--
Geocoded Schools	12/14/2010	--	0	0.0	--
<b>Essential Fish Habitat Potential</b>					
Environmentally Sensitive Shorelines	12/14/2010	--	17	0.0	--
Florida Artificial Reefs	12/14/2010	--	0	0.0	--
Florida Reef Locations and Names	12/14/2010	--	0	0.0	--
Florida Sea Grass Bed Scar Damage	12/14/2010	--	1	0.08	--
Mangroves	12/14/2010	--	1	12.89	--
Seagrass Beds (Showing Continuous/Discontinuous)	12/14/2010	--	15	125.88	--
Submerged Lands Act	12/14/2010	--	1	717.12	--
<b>Farmlands</b>					
Generalized Agricultural Land Use	12/14/2010	--	0	0.0	--
Prime Farm Land	12/14/2010	--	0	0.0	--
<b>Communities</b>					
Census Data	12/14/2010	--	14	923.68	--
Census data Block Groups - Indicators	12/14/2010	--	3	923.68	--
County Demographics	12/14/2010	--	2	214.28	--
<b>Recreation Areas</b>					
Existing Recreational Trails 2005	12/14/2010	--	2	0.0	--
Florida State Parks	12/14/2010	--	0	0.0	--
Geocoded Parks	12/14/2010	--	2	0.0	--
Parcel Derived Parks	12/14/2010	--	1	0.01	--
<b>Wild and Scenic Rivers</b>					
Wild and Scenic Rivers	12/14/2010	--	--	0	0.0
<b>Navigable Waterway Crossing?</b>					
Potential Navigable Waterway Crossings	12/14/2010	1	--	--	--

**National Wetlands Inventory** <http://www.fla-etat.org/est/metadata/nwip.htm>

Wetland areas from the National Wetlands Inventory summarized by wetland system type. - analysis run on 12/14/2010

System	100 Ft.		200 Ft.		500 Ft.	
	Acr	Pct	Acr	Pct	Acr	Pct
ESTUARINE	20.1	11.08%	128.0	35.07%	665.5	72.04%

**SWFWMD Wetlands 2008** [http://www.fla-etat.org/est/metadata/lu\\_sw\\_wtlnds\\_08.htm](http://www.fla-etat.org/est/metadata/lu_sw_wtlnds_08.htm)

SWFWMD Wetlands 2008 - analysis run on 12/14/2010

Land Use Classification	100 Ft.		200 Ft.		500 Ft.	
	Acr	Pct	Acr	Pct	Acr	Pct
MANGROVE SWAMPS	0.1	0.03%	6.9	1.89%	12.9	1.4%
SALTWATER MARSHES			0.1	0.04%	2.6	0.28%
WETLAND FORESTED MIXED					0.5	0.06%

**DFIRM FLOOD HAZARD ZONES** [http://www.fla-etat.org/est/metadata/dfirm\\_fidhaz.htm](http://www.fla-etat.org/est/metadata/dfirm_fidhaz.htm)

FLOOD HAZARD ZONES OF THE DIGITAL FLOOD INSURANCE RATE MAP (DFIRM) - analysis run on 12/14/2010

	100 Ft.	200 Ft.	500 Ft.

Flood Zone	Acr	Pct	Acr	Pct	Acr	Pct
AE	50.9	27.97%	77.7	21.29%	137.7	14.9%
OPEN WATER	39.6	21.8%	79.4	21.74%	237.8	25.74%
VE	92.3	50.76%	210.0	57.51%	553.1	59.88%

**FEMA Flood Insurance Rate Maps 1996** <http://www.fla-etat.org/est/metadata/fema96.htm>

FEMA Flood Insurance Rate Maps 1996 summarized by zone. See metadata for descriptions of zones. - analysis run on 12/14/2010

Zone	100 Ft.		200 Ft.		500 Ft.	
	Acr	Pct	Acr	Pct	Acr	Pct
AE	53.4	29.35%	81.1	22.21%	140.3	15.19%
UNDES					22.1	2.39%
VE	128.5	70.65%	284.0	77.79%	761.4	82.43%

**2003 FFWCC Habitat and Landcover GRID** [http://www.fla-etat.org/est/metadata/gfchab\\_03.htm](http://www.fla-etat.org/est/metadata/gfchab_03.htm)

2003 Habitat and Landcover Grid from the Florida Fish and Wildlife Conservation Commission summarized by type. Data is currently not displayed in maps. - analysis run on 12/14/2010

Description	100 Ft.		200 Ft.		500 Ft.	
	Acr	Pct	Acr	Pct	Acr	Pct
DRY PRAIRIES			0.7	0.18%	0.7	0.07%
EXOTIC PLANTS			2.5	0.67%	3.8	0.41%
HARDWOOD HAMMOCKS AND FORESTS	0.9	0.49%	4.7	1.29%	10.2	1.11%
HIGH IMPACT URBAN	162.4	89.33%	225.5	61.75%	247.2	26.76%
LOW IMPACT URBAN	1.8	0.98%	5.6	1.53%	8.2	0.89%
MANGROVE SWAMP	0.2	0.12%	0.9	0.24%	5.1	0.55%
MIXED HARDWOOD-PINE FORESTS	0.5	0.25%	1.6	0.43%	1.8	0.19%
OPEN WATER	15.2	8.34%	119.3	32.68%	635.1	68.76%
PINELANDS	0.2	0.12%	1.1	0.31%	4.7	0.51%
SALT MARSH	0.7	0.37%	2.0	0.55%	5.3	0.58%
SAND - BEACH			1.1	0.31%	1.1	0.12%
SHRUB AND BRUSHLAND			0.2	0.06%	0.4	0.05%

**2008 SWFWMD FL Land Use and Land Cover** [http://www.fla-etat.org/est/metadata/lu\\_swfwmd\\_2008.htm](http://www.fla-etat.org/est/metadata/lu_swfwmd_2008.htm)

2008 SWFWMD FL Land Use and Land Cover - analysis run on 12/14/2010

Land Use Classification	100 Ft.		200 Ft.		500 Ft.	
	Acr	Pct	Acr	Pct	Acr	Pct
BAYS AND ESTUARIES	43.8	24.1%	175.7	48.12%	702.8	76.09%
COMMERCIAL AND SERVICES					2.1	0.23%
HARDWOOD CONIFER MIXED			0.0	0.01%	4.9	0.53%
INSTITUTIONAL					1.6	0.17%
MANGROVE SWAMPS	0.1	0.03%	6.9	1.89%	12.9	1.4%
RECREATIONAL	0.5	0.27%	1.0	0.28%	2.1	0.22%
RESERVOIRS					0.4	0.04%
RESIDENTIAL HIGH DENSITY					0.0	0%
SALTWATER MARSHES			0.1	0.04%	2.6	0.28%
TRANSPORTATION	136.8	75.23%	180.3	49.39%	190.0	20.57%
UTILITIES	0.7	0.37%	1.0	0.28%	1.9	0.21%
WETLAND FORESTED MIXED					0.5	0.06%

**Florida Managed Areas** <http://www.fla-etat.org/est/metadata/flma.htm>

Florida Managed Areas - analysis run on 12/14/2010

Name	100 Ft.	200 Ft.	500 Ft.
COOPER'S POINT			✓

**Other Outstanding Florida Waters** [http://www.fla-etat.org/est/metadata/ofw\\_other.htm](http://www.fla-etat.org/est/metadata/ofw_other.htm)

Other Outstanding Florida Waters - analysis run on 12/14/2010

Name	100 Ft.	200 Ft.	500 Ft.
PINELLAS COUNTY AQUATIC PRESERVE	✓	✓	✓

List of Aquatic Preserves

<http://www.fla-etat.org/est/metadata/aquap.htm>

Aquatic preserves listed by Name. - analysis run on 12/14/2010

Name	100 Ft.	200 Ft.	500 Ft.
PINELLAS COUNTY AQUATIC PRESERVE	✓	✓	✓

Field Survey Project Boundaries

[http://www.fla-etat.org/est/metadata/shpo\\_surveys.htm](http://www.fla-etat.org/est/metadata/shpo_surveys.htm)

Field Survey Project Boundaries - analysis run on 12/14/2010

Title	100 Ft.	200 Ft.	500 Ft.
AN ARCHAEOLOGICAL AND HISTORICAL SURVEY OF THE UNINCORPORATED AREAS OF PINELLAS COUNTY, FLORIDA	✓	✓	✓
COUNTYWIDE CULTURAL RESOURCES SURVEY, PINELLAS COUNTY, FLORIDA	✓	✓	✓

Environmentally Sensitive Shorelines

<http://www.fla-etat.org/est/metadata/senshr.htm>

Environmentally Sensitive Shorelines from FWRI, summarized by type. - analysis run on 12/14/2010

Type	100 Ft.	200 Ft.	500 Ft.
10D: SCRUB-SHRUB WETLANDS		2217.5504	5552.0553
3A: FINE- TO MEDIUM- GRAINED SAND BEACHES	10115.6239	11451.1699	11485.2392
6B: RIPRAP	19472.9319	42576.9986	42928.1442
8B: SHELTERED SOLID MAN-MADE STRUCTURES			515.1875
8C: SHELTERED RIPRAP	961.0794	7229.3188	12219.5822

Florida Sea Grass Bed Scar Damage

<http://www.fla-etat.org/est/metadata/sgscar.htm>

Florida Sea Grass Bed Scar Damage - analysis run on 12/14/2010

Description	500 Ft.	
	Acr	Pct
LIGHT	0.1	0.01%

Mangroves

<http://www.fla-etat.org/est/metadata/mangroves.htm>

Environmental Sensitivity Index Coastal Mangroves - analysis run on 12/14/2010

Type	100 Ft.		200 Ft.		500 Ft.	
	Acr	Pct	Acr	Pct	Acr	Pct
MANGROVE SWAMP	0.1	0.03%	6.9	1.89%	12.9	1.4%

Seagrass Beds (Showing Continuous/Discontinuous)

<http://www.fla-etat.org/est/metadata/seagr.htm>

Seagrass beds broken down by whether the bed is continuous or discontinuous - analysis run on 12/14/2010

Description	100 Ft.		200 Ft.		500 Ft.	
	Acr	Pct	Acr	Pct	Acr	Pct
CONTINUOUS	14.9	8.2%	24.3	6.66%	38.3	4.14%
DISCONTINUOUS	7.4	4.06%	40.2	11.02%	87.6	9.49%

Submerged Lands Act

<http://www.fla-etat.org/est/metadata/sla.htm>

Submerged Lands Act - analysis run on 12/14/2010

Description	100 Ft.		200 Ft.		500 Ft.	
	Acr	Pct	Acr	Pct	Acr	Pct
SUBMERGED LANDS ACT	95.0	52.23%	219.1	60%	717.1	77.64%

**Census Data** <http://www.fla-etat.org/est/metadata/cenblk.htm>

US Census Bureau data by block. Detailed information is for each of the entire blocks that intersect an analysis area. - analysis run on 12/14/2010

	Males	Females	Native Hawaiian and Other Pacific Islander Alone	2000 Population	# Households	# White	# Black	# Native American	# Asian	# Hispanic	# Other Race
<b>Totals</b>	380	401	0	781	261	724	16	2	16	54	11

**Census data Block Groups - Indicators** <http://www.fla-etat.org/est/metadata/blkgp.htm>

Census data Block Groups - Indicators - analysis run on 12/14/2010

	Speak English "Not At All"	Housing Units With No Vehicle Available	Housing Units With 1 Vehicle Available	Housing Units With 2 Vehicles Available	Housing Units With 3 Vehicles Available	Housing Units With 4 vehicles Available	Housing Units With 5 or More Vehicles Available
<b>Totals</b>	0	245	967	571	90	0	0

**County Demographics** <http://www.fla-etat.org/est/metadata/cntdem.htm>

2000 Census General Demographic Profile by County - analysis run on 12/14/2010

Description	# Male	# Female	Median Age	# White	# Black or African American	# American Indian, Eskimo, or ...	# Asian	# Native Hawaiian and Other P...	# Some Other Race	# Hispanic or Latino (of any r...	Total Number of Households	Average Household Size	100 Ft.	200 Ft.	500 Ft.
998948	4887 72	5101 76	35.1	7509 03	1494 23	3879	2194 7	727	4653 9	1796 92	3913 57	2.51			✓
921482	4389 59	4825 23	43	7911 11	8255 6	2719	1898 4	484	1048 2	4276 0	4149 68	2.17			✓

**Existing Recreational Trails 2005** [http://www.fla-etat.org/est/metadata/existing\\_trails.htm](http://www.fla-etat.org/est/metadata/existing_trails.htm)

Existing Recreational Trails 2005 - analysis run on 12/14/2010

Trail Name	100 Ft.	200 Ft.	500 Ft.
OLD TAMPA BAY-COURNEY CAMPBELL CSWY	86.6137	203.3634	2875.0005
TAMPA BAY-COURTNEY CAMPBELL CSWY	125.4277	312.3844	1111.6524

**Geocoded Parks** [http://www.fla-etat.org/est/metadata/gc\\_parks.htm](http://www.fla-etat.org/est/metadata/gc_parks.htm)

Geocoded Parks - analysis run on 12/14/2010

Name	Description	100 Ft.	200 Ft.	500 Ft.
COURTNEY CAMPBELL	COURTNEY CAMPBELL			✓
BEN T DAVIS BEACH	BEN T DAVIS BEACH	✓	✓	✓

**Parcel Derived Parks** [http://www.fla-etat.org/est/metadata/par\\_parks.htm](http://www.fla-etat.org/est/metadata/par_parks.htm)

Parcel Derived Parks - analysis run on 12/14/2010

Name	Use type	100 Ft.	200 Ft.	500 Ft.
TRADEWINDS CONDO ASSN INC	OUTDOOR RECREATIONAL			✓



*Listing of Potential Navigable Waterways which intersect the project*

**This analysis is based on feature intersection instead of a buffer distance - analysis run on 12/14/2010**

**Description**

OLD TAMPA BAY

## Additional Information

More information about this project can be found on the Public ETDM website at <http://etdmpub.fl-a-etat.org>

A hardcopy map series for this project is available on the Public ETDM Website. Please click on the link below (or copy this link into your Web Browser) in order to view a listing of the hardcopy maps available for this project:

<http://etdmpub.fl-a-etat.org/est/index.jsp?tpID=13102&startPageName=Hardcopy%20Maps>

**Special Note:** Please be sure that when the Hardcopy Maps page loads, the **Project Milestone Date** corresponding to this Advance Notification is selected. Hardcopy map snapshots have been taken for Project #13102 at various points throughout the project's life-cycle, so it is important that you view the correct snapshot.

## Transmittal List

### Official Transmittal List

	Organization	Name
1.	Bureau of Indian Affairs	* Office of Trust Responsibilities - Environmental Services Staff
2.	FDOT District 7	Gonzalez, Roberto
3.	Federal Aviation Administration	* Airports District Office
4.	Federal Highway Administration	Anderson, Linda
5.	Federal Highway Administration	Kendall, Cathy
6.	Federal Highway Administration	Williams, Marvin L.
7.	Federal Transit Administration	Youngkin, Dale
8.	FIHS Central Office	Powell, Dusty
9.	FL Department of Agriculture and Consumer Services	Hardin, Dennis
10.	FL Department of Agriculture and Consumer Services	Morris, Vince
11.	FL Department of Community Affairs	Donaldson, Gary
12.	FL Department of Community Affairs	Penrose, Jo
13.	FL Department of Environmental Protection	Milligan, Lauren P.
14.	FL Department of Environmental Protection	Schatzman, Jillian
15.	FL Department of Environmental Protection	Stahl, Chris
16.	FL Department of State	Jones, Ginny L.
17.	FL Department of State	Kammerer, Laura
18.	FL Department of State	McManus, Alyssa
19.	FL Department of Transportation	Bixby, Marjorie
20.	FL Fish and Wildlife Conservation Commission	Gilbert, Terry
21.	FL Fish and Wildlife Conservation Commission	Poole, MaryAnn
22.	FL Fish and Wildlife Conservation Commission	Sanders, Scott
23.	Florida Inland Navigation District	* Mr. David Roach
24.	Hillsborough County MPO	Blain, Wally
25.	Miccosukee Tribe of Indians of Florida	Terry, Steve
26.	Miccosukee Tribe of Indians of Florida	* The Honorable Mr. Colley Billie, Chairman
27.	Mississippi Band of Choctaw Indians	* The Honorable Miko Mr. Beasley Denson

27.	Mississippi Band of Choctaw Indians	* The Honorable Miko Mr. Beasley Denson
28.	Muscogee (Creek) Nation	* The Honorable Mr. A.D. Ellis, Principal Chief
29.	National Marine Fisheries Service	Rydene, David A.
30.	National Marine Fisheries Service	Sramek, Mark
31.	National Park Service	Barnett, Anita
32.	Natural Resources Conservation Service	Robbins, Rick A.
33.	Pinellas County MPO	Bartolotta, Al
34.	Pinellas County MPO	Brinson, Ryan
35.	Poarch Band of Creek Indians	* The Honorable Mr. Buford Rolin, Chairman
36.	Seminole Nation of Oklahoma	* The Honorable Mr. Leonard M. Harjo, Principal Chief
37.	Seminole Tribe of Florida	Steele, Willard S.
38.	Seminole Tribe of Florida	* The Honorable Mr. Mitchell Cypress, Chairman
39.	Seminole Tribe of Florida	York, Elliott
40.	Southwest Florida Water Management District	Miller, C. L.
41.	Southwest Florida Water Management District	O'Neil, Paul W.
42.	Tampa Bay Regional Planning Council	Cooper, Suzanne T.
43.	Tampa Bay Regional Planning Council	Meyer, John M.
44.	US Army Corps of Engineers	Barron, Robert B.
45.	US Army Corps of Engineers	Fellows, John
46.	US Coast Guard	Overton, Randy
47.	US Department of Health and Human Services	* National Center for Environmental Health Centers for Disease Control and Prevention
48.	US Department of Housing and Urban Development	* Regional Environmental Officer
49.	US Department of Interior	* Bureau of Land Management, Eastern States Office
50.	US Department of Interior	Director, USGS-FISC
51.	US Environmental Protection Agency	Dominy, Madolyn
52.	US Fish and Wildlife Service	Mecklenborg, Todd S.
53.	US Fish and Wildlife Service	Monaghan, Jane

\* Hardcopy recipient

Application for Federal Assistance SF-424		Version 02
*1. Type of Submission: <input type="checkbox"/> Preapplication <input checked="" type="checkbox"/> Application <input type="checkbox"/> Changed/Corrected Application	*2. Type of Application    * If Revision, select appropriate letter(s) <input checked="" type="checkbox"/> New <input type="checkbox"/> Continuation                    *Other (Specify) _____ <input type="checkbox"/> Revision	
3. Date Received:	4. Applicant Identifier: FPID No. 422640-2-22-01	
5a. Federal Entity Identifier:	*5b. Federal Award Identifier:	
<b>State Use Only:</b>		
6. Date Received by State:	7. State Application Identifier:	
<b>8. APPLICANT INFORMATION:</b>		
*a. Legal Name: FLORIDA DEPARTMENT OF TRANSPORTATION		
*b. Employer/Taxpayer Identification Number (EIN/TIN): 59-6001874	*c. Organizational DUNS: 8093971020000	
<b>d. Address:</b>		
*Street 1:	<u>11201 N. McKinley Drive</u>	
Street 2:	<u>FDOT, District Seven</u>	
*City:	<u>Tampa</u>	
County:	<u>Hillsborough</u>	
*State:	<u>Florida</u>	
Province:	_____	
*Country:	_____	
*Zip / Postal Code	<u>33612-6456</u>	
<b>e. Organizational Unit:</b>		
Department Name: FDOT Intermodal Systems Development Department	Division Name: FDOT District Seven	
<b>f. Name and contact information of person to be contacted on matters involving this application:</b>		
Prefix: <u>Mr.</u>	*First Name: <u>Ming</u>	
Middle Name: _____		
*Last Name: <u>Gao</u>		
Suffix: _____		
Title:	<u>Department Head, Intermodal Systems Development</u>	
Organizational Affiliation:		
*Telephone Number: 813-975-6454	Fax Number:	
*Email: <u>ming.gao@dot.state.fl.us</u>		

**Application for Federal Assistance SF-424**

Version 02

**\*9. Type of Applicant 1: Select Applicant Type:**

A.State Government

Type of Applicant 2: Select Applicant Type:

Type of Applicant 3: Select Applicant Type:

\*Other (Specify)

**\*10 Name of Federal Agency:**

U.S. Department of Transportation

**11. Catalog of Federal Domestic Assistance Number:**

\_\_\_\_\_

CFDA Title:

\_\_\_\_\_

**\*12 Funding Opportunity Number:**

\_\_\_\_\_

\*Title:

\_\_\_\_\_

**13. Competition Identification Number:**

\_\_\_\_\_

Title:

\_\_\_\_\_

**14. Areas Affected by Project (Cities, Counties, States, etc.):**

Pinellas County, Hillsborough County, City of Tampa &amp; City of Clearwater

**\*15. Descriptive Title of Applicant's Project:**

SR 60 (Courtney Campbell Causeway) Multi-Use Trail Project: Construct a multi-use trail along SR 60 from Bayshore Blvd. to West of Ben T. Davis Beach Entrance, a distance of approximately four miles. Proposed improvements also include constructing separate bridges at Structure 1 and Structure 2 just east and west of the Pinellas/Hillsborough County Line.



**Application for Federal Assistance SF-424**

Version 02

**16. Congressional Districts Of:**

\*a. Applicant: FL-11

\*b. Program/Project: FL-10 &amp; FL-11

**17. Proposed Project:**

\*a. Start Date: 12/8/10

\*b. End Date: 06/8/11

**18. Estimated Funding (\$):**

*a. Federal	<u>12,250,000.00</u>
*b. Applicant	<u>0.00</u>
*c. State	<u>0.00</u>
*d. Local	<u>0.00</u>
*e. Other	<u>0.00</u>
*f. Program Income	<u>0.00</u>
*g. TOTAL	<u>12,250,000.00</u>

**\*19. Is Application Subject to Review By State Under Executive Order 12372 Process?**

- a. This application was made available to the State under the Executive Order 12372 Process for review on Dec. 16, 2010
- b. Program is subject to E.O. 12372 but has not been selected by the State for review.
- c. Program is not covered by E. O. 12372

**\*20. Is the Applicant Delinquent On Any Federal Debt? (If "Yes", provide explanation.)**

Yes       No

21. \*By signing this application, I certify (1) to the statements contained in the list of certifications\*\* and (2) that the statements herein are true, complete and accurate to the best of my knowledge. I also provide the required assurances\*\* and agree to comply with any resulting terms if I accept an award. I am aware that any false, fictitious, or fraudulent statements or claims may subject me to criminal, civil, or administrative penalties. (U. S. Code, Title 218, Section 1001)

\*\* I AGREE

\*\* The list of certifications and assurances, or an internet site where you may obtain this list, is contained in the announcement or agency specific instructions

**Authorized Representative:**

Prefix: Mr.      \*First Name: Ming

Middle Name: \_\_\_\_\_

\*Last Name: Gao

Suffix: \_\_\_\_\_

\*Title: Department Head, Intermodal Systems Development Department

\*Telephone Number: 813-975-6454

Fax Number: \_\_\_\_\_

\* Email: ming.gao@dot.state.fl.us

\*Signature of Authorized Representative: 

\*Date Signed: 12-16-10

## Comments on AN Package and Federal Consistency

### #13102 SR 60 Trail PD&E Study

<b>District</b>	District 7	<b>Phase</b>	Programming Screen
<b>County</b>	Hillsborough , Pinellas	<b>From</b>	Bayshore Boulevard
<b>Planning Organization</b>	FDOT District 7	<b>To</b>	W of Ben T. Davis Bch Entrance
<b>Plan ID</b>		<b>Financial Management No.</b>	42264022201
<b>Federal Involvement</b>	Federal Permit Federal Action Federal Funding		
<b>Contact Information</b>	Name: Steve Love Phone: (813) 975-6410 E-mail: steve.love@dot.state.fl.us		

### Snapshot Data From: Current Project Data

### Comments on AN Package and Federal Consistency

#### Comments on AN Package and Federal Consistency

There are not comments for this project.

### Purpose and Need Reviews

Agency	Acknowledgment	Review Date
US Coast Guard	Understood	12/20/2010
FL Department of State	Understood	12/29/2010
Natural Resources Conservation Service	Understood	01/04/2011
Federal Highway Administration	Accepted	01/18/2011
<p><b>Comments:</b> The Purpose and Need Statement is incorrect in that it describes the purpose of the PD&amp;E phase, not the purpose of the project. The purpose for this project is to provide regional connectivity with adjoining trail networks, to offer alternative modes of transportation in the region, to create regional recreational opportunities, and to enhance tourism and economic development.</p> <p>In the environmental document, please correct the current Purpose and Need Statement so that it describes the purpose of the project.</p>		
National Marine Fisheries Service	Understood	01/19/2011
FL Department of Environmental Protection	Understood	01/26/2011
US Fish and Wildlife Service	Understood	01/27/2011
Hillsborough County MPO	Understood	01/27/2011
FL Fish and Wildlife Conservation Commission	Understood	01/27/2011
US Environmental Protection Agency	Understood	01/27/2011
US Army Corps of Engineers	Understood	01/28/2011
Southwest Florida Water Management District	Understood	01/29/2011
FL Department of Community Affairs	Understood	05/04/2011

### Purpose and Need Reviews

### ETAT Review Overview

#### Alternative #1

#insertETATReview\_pdf(org.etdm.model.TranProjAlternative@6b557d [org.etdm.model.IssueGroup@193bb5f, org.etdm.model.IssueGroup@1f25cd5, org.etdm.model.IssueGroup@acb769, org.etdm.model.IssueGroup@13d74cc])

#### ETAT Review Overview

COMMISSION  
Kevin Beckner – Chairman  
Lesley "Les" Miller – Vice Chairman  
Victor Crist  
Ken Hagan  
Al Higginbotham  
Sandy Murman  
Mark Sharpe



Executive Director  
Richard D. Garrity, Ph.D.

Roger P. Stewart Center  
3629 Queen Palm Dr. Tampa, FL 33619  
Ph: (813) 627-2600

Fax Numbers (813):  
Admin 627-2620 Waste 627-2640  
Legal 627-2602 Wetlands 627-2630  
Water 627-2670 Air 627-2660  
Lab 635-8061

VIA EMAIL

January 28, 2011

Mr. Ming Gao, P.E.

[ming.gao@dot.state.fl.us](mailto:ming.gao@dot.state.fl.us)

Department Head, Intermodal Systems Development

Florida Department of Transportation, District 7

11201 N. McKinley Drive / MS-7-500

Tampa, FL 33612-6456

**Subject: EPC Comments – Courtney Campbell Causeway Multi-Use Trail PD&E Study**

Staff from the Environmental Protection Commission of Hillsborough County (EPC) has conducted a review of the subject Courtney Campbell Causeway Multi-Use Trail PD&E Study and does not object to the study as proposed subject to the following comments/conditions:

1. Wetlands exist within the project boundaries as indicated through review of aerial photography and Soil Conservation Service soils maps.
2. Review of this PD&E by EPC staff does not constitute a guarantee that the Environmental Protection Commission approvals/permits necessary for the development as proposed will be issued, does not itself serve to justify any impact to wetlands, and does not grant any implied or vested right to environmental approvals.
3. Development within wetlands of Hillsborough County which destroys, reduces, or impairs the wetland or which contributes to the present or potential future destruction, reduction, or impairment of the environmental benefits provided by the wetland, or a portion thereof, constitutes pollution as defined by Chapter 84-446, Laws of Florida, as amended. Impacts to wetlands are prohibited except unless specifically authorized in writing by the EPC Executive Director or his authorized agent. Pursuant to EPC Wetland Rule Section 1-11.07(1), Rules of the EPC, "written authorization may be given to conduct proposed development affecting wetlands only if reasonable use of the land cannot be accomplished without affecting the wetland."
4. EPC staff requires that all efforts be taken to avoid or reduce wetland impacts prior to submittal of any site development plans. The size, location, and configuration of the wetlands may result in requirements to reduce or reconfigure proposed lots, re-align roadways, and make other changes necessary to avoid or minimize wetland impacts.

An agency with values of environmental stewardship, integrity, honesty, and a culture of fairness and cooperation.

[www.epchc.org](http://www.epchc.org)

E-Mail: [epcinfo@epchc.org](mailto:epcinfo@epchc.org)

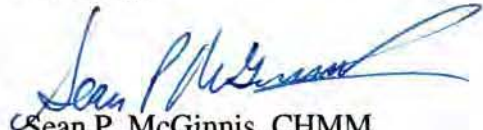
AN AFFIRMATIVE ACTION – EQUAL OPPORTUNITY EMPLOYER

Page 2

Courtney Campbell Causeway Multi-Use Trail PD&E Study  
January 28, 2011

If you have any further questions, please contact me at 813-627-2600, extension 1299 or at [mcginnis@epchc.org](mailto:mcginnis@epchc.org).

Sincerely,

A handwritten signature in blue ink, appearing to read "Sean P. McGinnis". The signature is fluid and cursive, with a large initial "S" and a long, sweeping underline.

Sean P. McGinnis, CHMM  
Environmental Specialist III  
Air Division, Enforcement and Analysis



→ Kirk → Robin



January 28, 2011

Mr. Ming Gao, P. E.  
Department Head, Intermodal Systems Development  
Florida Department of Transportation, District 7  
11201 N. McKinley Drive / MS 7-500  
Tampa, FL 33612-6456

Mayor Joe Affronti, Sr.  
City of Temple Terrace  
MPO Chairman

Commissioner Mark Sharpe  
Hillsborough County  
MPO Vice Chairman

Dear Mr. Gao:

Re: ETDM #13102 Advance Notification  
SR 60 (Courtney Campbell Causeway) Multi-Use Trail PD&E Study

Commissioner Kevin Beckner  
Hillsborough County

Councilman Joseph Caetano  
City of Tampa

Joe Lopano  
Hillsborough Co. Aviation Authority

Commissioner Rick A. Lott  
City of Plant City

Commissioner Lesley "Les" Miller, Jr.  
Hillsborough County

Commissioner Sandra Murman  
Hillsborough County

Councilman Tom Scott  
City of Tampa

Councilman Curtis Stokes  
HART

Joseph Waggoner  
Expressway Authority

Richard Wainio  
Tampa Port Authority

Derek L. Doughty (Ex-Officio)  
The Planning Commission

Donald J. Skelton, P.E. (Ex-Officio)  
FDOT, District Seven

Ramond A. Chiaramonte, AICP  
Executive Director

Pursuant to the Advance Notification sent to the Hillsborough Metropolitan Planning Organization (MPO) we have reviewed the policies of the City of Tampa Comprehensive Plan for consistency with Chapter 163 of the Florida Statutes.

Chapter 6 – Sustainable Environment of the Tampa Comprehensive Plan lists the Ben T. Davis Beach on the Courtney Campbell Causeway as one of three significant man-made public beaches in the City.

- Policy 38.22.2: Maintain or improve the existing, natural condition of the three public beaches in the City.
- Policy 38.22.3: Coordinate with the State to implement state-of-the-art beach and dune stabilization technique where appropriate
- 

For coastal management, the Tampa Comprehensive Plan encourages the improvement or maintenance of the natural condition of the Ben T. Davis Beach on the Courtney Campbell Causeway. And the Conservation Element / Air Quality Section of the Tampa Comprehensive Plan encourage transportation system improvements that reduce air pollution concentrations.

Thank you for the opportunity to provide comments for consistency purposes as this project is implemented.

Sincerely,  
  
Ramond A. Chiaramonte, AICP  
Executive Director

Hillsborough County  
Metropolitan Planning Organization  
P.O. Box 1111  
601 E. Kennedy, 18<sup>th</sup> Floor  
Tampa, Florida 33601-1110  
813/272-5940  
FAX NO: 813/301-7172  
<http://www.hillsboroughmopo.org>  
e-mail: [transportation@plancom.org](mailto:transportation@plancom.org)

RAC:wb

I:\tran\_mpo\corresp\2011 corresp\advance notification\ccc trail mpo response.doc

RECEIVED  
PLANNING UNIT  
2011 JAN 31 PM 1:39



**UNITED STATES DEPARTMENT OF COMMERCE**  
**National Oceanic and Atmospheric Administration**  
NATIONAL MARINE FISHERIES SERVICE  
Southeast Regional Office  
263 13<sup>th</sup> Avenue South  
St. Petersburg, Florida 33701-5505  
(727) 824-5317; FAX 824-5300

March 9, 2011 F/SER46:DR

Joseph Severson  
Environmental Specialist  
Florida Department of Transportation District Seven  
11201 McKinley Drive MS 7-500  
Tampa, Florida 33612-6456

Dear Mr. Severson:

This letter is intended to provide technical assistance in response to your letter dated February 25, 2011. NOAA's National Marine Fisheries Service (NMFS) has reviewed the accompanying Draft Wetland Evaluation Biological Assessment Report regarding the construction of a multi-use trail on the SR 60 Courtney Campbell Causeway in Hillsborough County and Pinellas County, Florida (ETDM No. 13102; Work Program Item Segment No. 422640 2; FAP No. 9045-090-C). The trail would include two structures crossing the waters of Old Tampa Bay. NMFS has assessed the information provided by your agency in reference to potential impacts to essential fish habitat and swimming sea turtles.

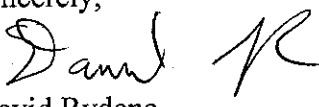
The project lies within an area of the Tampa Bay system that may be inhabited by swimming sea turtles and/or smalltooth sawfish. The draft report does not include an assessment of potential impacts to smalltooth sawfish. Smalltooth sawfish are listed as an endangered species under the Endangered Species Act (ESA) and fall under NMFS' purview. NMFS recommends that a section on smalltooth sawfish be added to your report. In addition, we suggest that NMFS' Sea Turtle and Smalltooth Sawfish Construction Conditions be implemented as part of the project's commitments. When the project's final design has been determined, NMFS recommends that a section 7 consultation be conducted for swimming sea turtles and smalltooth sawfish.

It appears that the recommended trail alternative will have minimal impacts to mangroves. As the report states, the final determination of potential impacts to seagrasses from the two independent bridge structures will need to be determined during the prime seagrass growing season (May-September). Appropriate compensatory mitigation strategies can be discussed when impacts to NMFS trust resources are known with greater certainty.

If you have questions regarding NMFS' views on this project, please contact me at our St. Petersburg, Florida office. You can reach me at the letterhead address or by calling (727) 824-5379.



Sincerely,



David Rydene

Fish Biologist  
Habitat Conservation Division



# United States Department of the Interior

## U. S. FISH AND WILDLIFE SERVICE

7915 BAYMEADOWS WAY, SUITE 200  
JACKSONVILLE, FLORIDA 32256-7517

IN REPLY REFER TO:

FWS Log No. 41910-2011-I-0198

March 24, 2011

Mr. Joseph Severson  
Environmental Specialist  
Florida Department of Transportation  
11201 N. McKinley Drive,  
Tampa, FL 33612-6456

Dear Mr. Severson:

Our office has reviewed the information provided by the Florida Department of Transportation (FDOT) for consultation on the SR 60 Multi-Use Trail and your request for our concurrence on the FDOT effects determination for the federally listed Florida manatee (*Trichechus manatus latirostris*), piping plover (*Charadrius melodus*), wood stork (*Mycteria americana*), green sea turtle (*Chelonia mydas*), loggerhead sea turtle (*Caretta caretta*), leatherback sea turtle (*Dermochelys coriacea*), Kemp's Ridley sea turtle (*Lepidochelys kempii*) and gulf sturgeon (*Acipenser oxyrinchus*). The western terminus begins at latitude 27 59'39.13N and longitude -82 42'13.56W in Pinellas County; the eastern terminus is located at latitude 27 58'19.62N and longitude -82 34'57.53W in Hillsborough County, Florida.

We provide the following comments and recommendations in accordance with section 7 of the Endangered Species Act of 1973 (ESA), as amended (16 U.S.C. 1531 *et seq.*), the Marine Mammal Protection Act of 1972 (MMPA), as amended (16 U.S.C. 1361 *et seq.*), and the Migratory Bird Treaty Act of 1918 (16 U.S.C. 703-712 *et seq.*).

The risks to the Florida manatee have been evaluated. The Florida manatee is protected under the ESA and the MMPA. Both of these laws make it illegal to "take" (i.e. harm, harass, injure or kill) manatees. We are concerned about the presence of manatees and sea grasses in the project area. Any impacts to seagrass beds results in a 'May Affect' determination for the Florida manatee. Seagrass surveys need to be conducted during the growing season (June 1- Sept 30). Direct impacts to the seagrass beds during construction and indirect impacts to seagrasses as a result of shading need to be quantified. Further damage may occur from the operation of boats and barges during the construction phase. Turbidity and siltation during the construction phase may also impact seagrass beds. The Service recommends the National Marine Fisheries Service and U.S. Army Corps of Engineers guidance document entitled, 'Dock Construction Guidelines in Florida for Docks or Other Minor Structures Constructed in or over Submerged Aquatic Vegetation (SAV), Marsh or Mangrove Habitat' (August 2001) when designing structures over SAV. We have attached these guidelines for you. In addition to following the Standard In-Water



Construction Conditions for Manatees (2009) there will be a need for special conditions for this project which may include the following: no nighttime work, dedicated manatee observers, fenders between work barges to prevent crushing, seasonal timing restrictions and the proper siltation or exclusion barriers that will not entrap manatees in the work site.

The Service and the National Oceanic and Atmospheric Administration's National Marine Fisheries Service (NMFS) share Federal jurisdiction for sea turtles under the ESA. The Service has responsibility for sea turtles on nesting beaches. NMFS has jurisdiction for sea turtles in the marine environment. The Service concurs with your determination of effect for all species of sea turtles due to the lack of nesting beaches along the causeway.

The Service also shares jurisdiction for Gulf Sturgeon under the ESA. The Service has responsibility for sturgeon in estuarine areas if FDOT is the action agency. Therefore, the Service recommends that FDOT incorporate the Construction Special Provisions for the protection of the Gulf Sturgeon. We have attached these guidelines for your convenience.

If there are wetland impacts, the Service cannot concur with your determination of 'No Effect' for wood storks because there are several colony sites within 15 miles of the proposed trail. The Service recommends utilizing the wood stork effect determination key that is found on our website.

The Courtney Campbell Causeway is an important area for shorebirds and other migratory birds and frequently supports very large numbers of loafing or foraging flocks. The placement of a pedestrian and bicycle trail too close to the areas where shorebirds are known to gather may result in the flushing and disturbance of shorebirds. If dogs are allowed on the trail, the birds may react (flush) even if the dog is on a leash because the animal is seen as a predator. Please provide the distances between the proposed trail and the known shorebird roosting, feeding and loafing areas.

Although there is no piping plover critical habitat designated inside Tampa Bay, we have attached the results of the 2011 Winter Shorebird and Piping Plover Survey for the C. Campbell Causeway. Red knots (*Caladris spp.*) a candidate species for listing, utilize the causeway for foraging and resting during their long migratory flights. This area also supports one of the largest gathering sites in the region for American oystercatchers with 50+ individuals commonly seen here. Repeated disturbances from humans and dogs can be one of the main threats to our shorebird populations. Shorebirds can be displaced from foraging and resting areas, they can also abandon important areas if the disturbance continues and as a result they may have lower body weights upon arrival on their breeding grounds (Pfister et al. 1992, Burger, et al. 2007). Until we have the information requested above we cannot make a determination on the effects of this project on migratory birds.

If you have any questions regarding this response, please contact Jane Monaghan at (904) 731-3119.

Sincerely,



for

David L. Hankla  
Field Supervisor

Cc: Scott Sanders, FFWCC  
Michael Esquivel, USCG  
David Rydene, NOAA  
Terry Gilbert, URS Corp.

Attachments (3): 2011 Winter Shorebird Survey, Dock Construction over SAV, Special Provisions for Sturgeon Protection.

References Cited:

Pfister, C.; Harrington, B. and Lavin, M., 1992. The impact of human disturbance on shorebirds at a migration staging area. *Biological Conservation* 60:115-126

Burger, D; Carlucci, S.A.; Jeitner, C.W.; and Niles, L., 2007. Habitat choice, disturbance, and management of foraging shorebirds and gulls at a migratory stopover. *Journal of Coastal Research*, 23(5), 1159-1166.

# 2011 Winter Shorebird Survey- Data Sheet

Site name and description: *COURTNEY CAMP BELL CAUSEWAY FROM WATER TREATMENT PLANT (CLEARWATER/PINGULAS TO ROCKY PT. DRIVE TAMPA /HILLSBOROUGH*

Observer (lead): *ROBERT LANE*

Email: *ohiomagpie@hotmail.com*  
 Phone: HOME: 330-537-4341  
 CELL: 216-276-7782

Start time: *6:45 AM FEB 4 2011*

Weather conditions: *MORNING: OVERCAST, FOGGY AFTERNOON: CLEAR, SUNNY*

Other observers: *DENISE LANE JANE MANN STEPHEN MANN*

**RECEIVED**

FEB 18 2011

**GPS Locations.**

Please use a GPS unit or internet tools such as GET LAT LON (<http://www.getlatlon.com/>) to determine latitude and longitude for the following.

U.S. FISH AND WILDLIFE SERVICE  
 JACKSONVILLE, FLORIDA

- 1) Length of your survey route: start point (A) and point furthest from the start (B).
- 2) Individuals or groups of Snowy Plovers, Piping Plover, Wilson's Plover, and Red Knot.
- 3) Color-banded individuals.
- 4) Large aggregations or flocks of birds (i.e. 50+); note species composition and number of birds.

Point	Latitude	Longitude	Species	Information
<b>A</b>	<i>27.958717</i>	<i>-82.708000</i>	<i>not applicable</i>	<i>Start point of your survey route.</i>
<b>B</b>	<i>27.961938</i>	<i>-82.568971</i>	<i>not applicable</i>	<i>Furthest point of your route from the start point (A).</i>
1	<i>27.961028</i>	<i>-82.696403</i>	<i>WILSON'S PLOVER</i>	<i>2 CLEARWATER, CCC BEACH IN LARGE FLOCK OF SHORE BIRDS</i>
2	"	"	<i>LEAST SANDPIPER</i>	<i>470</i>
3	"	"	<i>DUNLIN</i>	<i>118</i>
4	"	"	<i>SHORT-BILLED DOWITCHER</i>	<i>75</i>
5	"	"	<i>RUDDY TURNSTONE</i>	<i>65</i>
6	"	"	<i>RED KNOT</i>	<i>KM3, GROUP OF 6</i>
7	"	"	<i>SANDERLING</i>	<i>100</i>
8	"	"	<i>RING-BILLED GULL</i>	<i>80</i>
9	"	"	<i>LAUGHING GULL</i>	<i>70</i>
10	"	"	<i>AMERICAN OYSTERCATCHER</i>	<i>35</i>
11	"	"	<i>BLACK SKIMMER</i>	<i>36</i>
12	"	"	<i>WESTERN SANDPIPER</i>	<i>32</i>
13	"	"	<i>LESSER SCAUP, BLACK-BELLIED PLOVER, BROWN PELICAN, ALSO: SNOWY EGRET, HERRING GULL, ROYAL TERN, FORSTER'S TERN</i>	
14	"	"	<i>MIN. BALD EAGLE, BONAPARTE'S GULL, TRICOLOR HERON, LITTLE BLUE HERON, CASPIAN TERN</i>	
15	<i>27.969679</i>	<i>-82.576619</i>	<i>BLACK SKIMMER</i>	<i>102 (BENT. DAVIS BEACH)</i>
16	"	"	<i>LESSER SCAUP</i>	<i>220</i>
17	"	"	<i>RING-BILLED GULL</i>	<i>62</i>
18	"	"	<i>LAUGHING GULL</i>	<i>105</i>
19	<i>27.961982</i>	<i>-82.569628</i>	<i>HORNED GREBE</i>	<i>60 (RUSTY PELICAN - ROCKY PT.)</i>
20	<i>27.973441</i>	<i>-82.594267</i>	<i>DUNLIN</i>	<i>840</i>
21	"	"	<i>LEAST SANDPIPER</i>	<i>200</i>
22	"	"	<i>WILLET</i>	<i>106</i>
23	"	"	<i>SHORT-BILLED DOWITCHER</i>	<i>80</i>
24	"	"	<i>SEMI-PALMATED PLOVER</i>	<i>60</i>
25	"	"	<i>BLACK-BELLIED PLOVER</i>	<i>45</i>

# 2011 Winter Shorebird Survey- Data Sheet

Site Information & GPS Locations- Page 2

Site name and description: COURTNEY CAMPBELL CAUSEWAY, CLEARWATER TO TAMPA FL.		Observer (lead): ROBERT LANE	Email:
Start time: 6:45 AM		Other observers: DENISE LANE JANE MANN STEPHEN MANN	Phone:
Weather conditions: MORNING, LIGHT FOG, OVERCAST AFTERNOON, SUNNY			
End time: 4:30 PM			

## GPS Locations.

Please use a GPS unit or internet tools such as GET LAT LON. (<http://www.getlatlon.com/>) to determine latitude and longitude for the following:

- 1) Length of your survey route: start point (A) and point furthest from the start (B).
- 2) Individuals or groups of Snowy Plovers, Piping Plover, Wilson's Plover, and Red Knot.
- 3) Color-banded individuals.
- 4) Large aggregations or flocks of birds (i.e. 50+); note species composition and number of birds.

Point	Latitude	Longitude	Species	Information
	27.958717	-82.708000	not applicable	Start point of your survey route.
	27.961938	-82.568971	not applicable	Furthest point of your route from the start point (A).
1				
2				
3				
4				
5				
26	27.972227	-82.611150	LESSER SCAUP	100
27	"	"	RED-WINGED BLACKBIRD	50
28	27.970869	-82.619801	BROWN PELICAN	80
29	"	"	RUDDY TURNSTONE	44
30	"	"	LESSER SCAUP	46
31	27.965154	-82.666149	LESSER SCAUP	90
32			BROWN PELICAN	42
33			DOUBLE-CRESTED CORMORANT	67
34			HORNED GREBE	260
15				
16				
17				
18				
19				
20				
21				
22				
23				
24				
25				



# 2011 Winter Shorebird Survey- Data Sheet

Site Name:

Observer (lead):

COURTNEY CAMPBELL CAUSEWAY

ROBERT LANE

Species	T	Species	T	Species	T
<b>DUCKS</b>		Caspian Tern	5	<b>RAPTORS</b>	
Redhead	0	Royal Tern	11	Osprey	12
Ring-necked Duck	0	Common Tern	0	Bald Eagle - Adult	1
Greater Scaup	0	Forster's Tern	16	Bald Eagle - Immature	1
Lesser Scaup	464	Sandwich Tern	0	Northern Harrier	0
Scaup sp.	0	Black Skimmer	138	Sharp-shinned Hawk	0
Surf Scoter	0			Cooper's Hawk	1
White-winged Scoter	0	<b>SHOREBIRDS</b>		Red-shouldered Hawk	0
Black Scoter	0	Black-bellied Plover	58	Red-tailed Hawk	0
Scoter sp.	0	Wilson's Plover	2	American Kestrel	0
Long-tailed Duck	0	Semipalmated Plover	130	Merlin	0
Bufflehead	0	Piping Plover	0	Peregrine Falcon	0
Common Goldeneye	0	Snowy Plover	0		
Hooded Merganser	0	Killdeer	0	<b>WADING BIRDS</b>	
Red-breasted Merg.	7	American Oystercatcher	64	Great Blue Heron	4
Ruddy Duck	0	American Avocet	0	Great Egret	3
		Greater Yellowlegs	0	Snowy Egret	52
<b>SEABIRDS</b>		Lesser Yellowlegs	0	Little Blue Heron	38
Red-throated Loon	0	Yellowlegs sp.	0	Tricolored Heron	4
Common Loon	38	Willet	183	Reddish Egret	11
Horned Grebe	348	Spotted Sandpiper	6	White Ibis	31
Northern Gannet	0	Whimbrel	0	Wood Stork	0
Brown Booby	0	Long-billed Curlew	0		
Brown Pelican	153	Marbled Godwit	1	<b>OTHER SPECIES</b>	
White Pelican	0	Ruddy Turnstone	115	BELTED KINGFISHER	3
Double-crested Corm.	91	Red Knot	6	RED-WINGED BLACKBIRD	50
Pomarine Jaeger	0	Sanderling	112	FISH CROW	14
Parasitic Jaeger	0	Western Sandpiper	32	ROCK DOVE	42
Laughing Gull	244	Least Sandpiper	670	EUROPEAN STARLING	46
Bonaparte's Gull	26	Purple Sandpiper	0	TURKEY VULTURE	10
Ring-billed Gull	209	Dunlin	961	ANHINGA	8
Herring Gull	4	Short-billed Dowitcher	169	YELLOW-CROWNED NIGHT HERON	6
Lesser Bk-backed Gull	0	Long-billed Dowitcher	0	BROWN-HEADED COWBIRD	8
Great Bk-backed Gull	0	Dowitcher sp.	0	MOURNING DOVE	6
Franklin's Gull	0	Wilson's Snipe	0	RED-BELLIED WOODPECKER	1
Glaucous Gull	0	Purple Sandpiper	0	MOURNING DOVE	6
Vega Gull	0	Wilson's Snipe	0	RED-BELLIED WOODPECKER	1
				MOURNING DOVE	6
				RED-BELLIED WOODPECKER	1
				MULLARD DUCK	3
				MOTTLED DUCK	1
				EUR-COL. DOVE	3
				MOURNING DOVE	6
				RED-BELLIED WOODPECKER	1
				MULLARD DUCK	3
				MOTTLED DUCK	1

TOTAL 4421

GRAND TOTAL 4644



## CONSTRUCTION SPECIAL PROVISIONS STURGEON PROTECTION GUIDELINES

The shortnose sturgeon (*Acipenser brevirostrum*) and the gulf sturgeon (*A. oxyrinchus desotoi*) are listed under the Endangered Species Act as endangered and threatened, respectively. These species are under the jurisdiction of the National Marine Fisheries Service (NMFS) and the U.S. Fish and Wildlife Service (USFWS). In Florida, the lower St Johns River is habitat for shortnose sturgeon. Major portions of the Suwannee and Withlacoochee Rivers are designated as critical habitat for the gulf sturgeon.

The following special provisions will be incorporated into any construction contract where involvement with sturgeon may occur:

The FDOT will coordinate with the NMFS and USFWS early in the project development stage of new bridge projects. All efforts should be made to avoid known spawning habitats, nursery areas, feeding areas and thermal refuges.

1. Advise construction personnel of the potential presence of these species, of their endangered status and federal protection, and of the need to avoid any actions that would jeopardize these species.
2. The Florida Department of Transportation (FDOT) shall advise all FDOT project personnel and Contractor personnel on the project that there are civil and criminal penalties for harming, harassing or killing sturgeon, which are protected under the Endangered Species Act of 1973. The FDOT and the Contractor will be held responsible for any sturgeon harmed, harassed, or killed as a result of the project activity.
3. The FDOT shall provide information to all FDOT and Contract personnel for identification of sturgeon.
4. Appropriate work shift personnel will be instructed in the appearance, habits, biology, migratory patterns, and preservation of sturgeon. At least one of these trained personnel will be on site during construction activities to maintain a constant surveillance for these species, assure the cessation of activities (such as dredging, excess turbidity, and construction barge activity), which may endanger these species, and assure that uninhibited passage for the animals is provided.
5. Post signs on site warning of the presence of sturgeon, of their endangered status, and precautions needed.
6. Turbidity from construction activity will be adequately controlled to prevent degradation of the quality and transparency of the water. When sturgeon are present, turbidity curtains of appropriate dimension will be used to restrict the

animals access to the work area. Pollution booms or turbidity curtains should use tangle resistant or hemp rope when anchoring, or employ surface anchors to prevent entangling sturgeon. Continuous surveillance will be maintained in order to free animals which may become trapped in silt or turbidity barriers.

7. No dredging of the river bottom will be conducted for barge access.
8. Drilled shaft pile construction will be used whenever prudent and feasible as determined by FDOT.
9. Care shall be taken in lowering equipment or material below the water surface and into the stream bed. These precautions will be taken to ensure no harm occurs to any sturgeon which may have entered the construction area undetected.
10. Construction debris shall not be discarded into the water.
11. If the use of explosives is necessary, no blasting will occur during sturgeon spawning season or in known spawning, staging, feeding, or vital nursery areas.

The following protection measures will be employed for blasting:

- A. For each explosive charge, detonation will **not** occur if a sturgeon is known to be within a circular area ("the danger zone") encompassing the detonation site defined by the following radius:

$$r = 560(\sqrt[3]{W})$$

Where: r = radius of danger zone in feet

W = weight of explosive charge in pounds (tetra or TNT)

- B. In the event that a sturgeon is killed during blasting, the NMFS and/or the USFWS will be notified immediately.
12. Any dead sturgeon will be secured on site for carcass analysis by notified agency representative.
13. Following completion of the project, a report summarizing any involvement with sturgeon will be prepared for NMFS and/or USFWS.



**Dock Construction Guidelines in Florida for Docks or Other Minor Structures  
Constructed in or over Submerged Aquatic Vegetation (SAV), Marsh or Mangrove Habitat  
U.S. Army Corps of Engineers/National Marine Fisheries Service  
August 2001**

**Submerged Aquatic Vegetation:**

1. Avoidance. The pier shall be aligned so as to minimize the size of the footprint over SAV beds.
2. The height of pier shall be a minimum of 5 feet above MHW/OHW as measured from the top surface of the decking.
3. The width of the pier is limited to a maximum of 4 feet. A turnaround area is allowed for piers greater than 200 feet in length. The turnaround is limited to a section of the pier no more than 10 feet in length and no more than 6 feet in width. The turnaround shall be located at the midpoint of the pier.
4. Over-SAV bed portions of the pier shall be oriented in a north-south orientation to the maximum extent that is practicable.
  5. a. If possible, terminal platforms shall be placed in deep water, waterward of SAV beds or in an area devoid of SAV beds.
    - b. If a terminal platform is placed over SAV areas and constructed of grated decking, the total size of the platform shall be limited to 160 square feet. The grated deck material shall conform to the specifications stipulated below. The configuration of the platform shall be a maximum of 8 feet by 20 feet. A minimum of 5 feet by 20 feet shall conform to the 5-foot height requirement; a 3 feet by 20 feet section may be placed 3 feet above MHW to facilitate boat access. The long axis of the platform should be aligned in a north-south direction to the maximum extent that is practicable.
    - c. If the terminal platform is placed over SAV areas and constructed of planks, the total size of the platform shall be limited to 120 square feet. The configuration of the platform shall be a maximum of 6 feet by 20 feet of which a minimum 4-foot wide by 20-foot long section shall conform to the 5-foot height requirement. A section may be placed 3 feet above MHW to facilitate boat access. The 3 feet above MHW section shall be cantilevered. The long axis of the platform should be aligned in a north-south direction to the maximum extent that is practicable. If the 3 feet above MHW section is constructed with grating material, it may be 3 feet wide.
6. One uncovered boat lift area is allowed. A narrow catwalk (2 feet wide if planks are used, 3 feet wide if grating is used ) may be added to facilitate boat maintenance along the outboard side of the boat lift and a 4-foot wide walkway may be added along the stern end of the boat lift, provided all such walkways are elevated 5 feet above MHW. The catwalk shall be cantilevered from the outboard mooring pilings (spaced no closer than 10 feet apart).
7. Pilings shall be installed in a manner which will not result in the formation of sedimentary deposits("donuts" or "halos") around the newly installed pilings. Pile driving is the preferred method of installation, but jetting with a low pressure pump may be used.
8. The spacing of pilings through SAV beds shall be a minimum of 10 feet on center.
9. The gaps between deckboards shall be a minimum of ½ inch.

**Marsh:**

1. The structure shall be aligned so as to have the smallest over-marsh footprint as practicable.
2. The over-marsh portion of the dock shall be elevated to at least 4 feet above the marsh floor.
3. The width of the dock is limited to a maximum of 4 feet. Any exceptions to the width must be accompanied by an equal increase in height requirement.

**Mangroves.**

1. The width of the dock is limited to a maximum of 4 feet.
2. Mangrove clearing is restricted to the width of the pier.
3. The location and alignment of the pier should be through the narrowest area of the mangrove fringe.

**Grid Specifications and Suppliers**

The following information does not constitute a U.S. Army Corps of Engineers endorsement or advertisement for any particular provider and is provided only as an example for those interested in obtaining these materials for dock construction. A type of fiberglass grate panel is manufactured by SeaSafe (Lafayette, LA; phone: 1-800-326-8842) and FiberGrate (1-800-527-4043). Plastic grate panels are also available from Southern Pine Lumber Company (Stuart, FL; phone: 772-692-2300). Panels are available in a variety of sizes and thicknesses. For safety, the grate should contain an anti-slip texture which is integrally molded into the top surface. The manufacturer or local distributor should be consulted to ensure that the load-bearing capacity of the selected product is sufficient to support the intended purpose. Contact the manufacturer(s) for product specifications and a list of regional distributors.



April 18, 2011

**Florida Fish  
and Wildlife  
Conservation  
Commission**

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Division of Habitat and  
Species Conservation  
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Mr. Joseph Severson  
Environmental Specialist  
Florida Department of Transportation (FDOT) District Seven  
11201 N. McKinley Drive  
Tampa, FL 33612-6456

Re: State Road (SR) 60 Multi-Use Trail, Hillsborough and Pinellas Counties, Draft  
Wetland Evaluation Biological Assessment Report, Project Development and  
Environment (PD&E) Study

Dear Mr. Severson:

The Division of Habitat and Species Conservation, Habitat Conservation Scientific Services Section, of the Florida Fish and Wildlife Conservation Commission (FWC) has coordinated an agency review of the Draft Wetland Evaluation Biological Assessment Report (WEBAR) for the above-referenced project, and offers the following comments. The WEBAR was prepared as part of the PD&E Study for the proposed project.

The project involves the construction of a paved, multi-use trail adjacent to SR 60 across the Courtenay Campbell Causeway in Old Tampa Bay. The trail would be constructed on the south side of SR 60, and would include two independent bridge structures parallel to the existing bridges for the highway.

The FWC evaluated this project as Efficient Transportation Decision Making (ETDM) project #13102 in January of this year. At that time, we ranked the project's potential direct and indirect impacts to fish and wildlife resources as substantial, due to the uncertainty as to where the trail would be proposed for construction (north or south of SR 60), and the potential for the trail to impact both mangrove and herbaceous wetlands, as well as seagrass beds at the bridge crossings.

The WEBAR evaluated potential project impacts to 19 wildlife species classified by the federal Endangered Species Act as Endangered or Threatened, or by the State of Florida as Threatened or Species of Special Concern, and also the bald eagle, which is protected by the federal Bald and Golden Eagle Protection Act. Project biologists made a finding of "no effect" for 3 of these species: the Gulf sturgeon, wood stork, and bald eagle. The WEBAR determined that the project "may affect, but is unlikely to adversely affect" all other evaluated species, including the loggerhead sea turtle, Kemp's ridley sea turtle, green sea turtle, leatherback sea turtle, piping plover, snowy plover, American oystercatcher, black skimmer, brown pelican, least tern, little blue heron, tricolored heron, reddish egret, snowy egret, roseate spoonbill, white ibis, and Florida manatee. It is unclear why the wood stork was given a "no effect" determination while the other wading birds were included in the "may affect, but is unlikely to adversely affect" list of species. If there is the potential for wading bird feeding areas like wetlands or shallow seagrass beds to be impacted by this project, then the effects on all the wading bird species should be similarly classified. Otherwise, we concur with the evaluations in the WEBAR.

We also support the project commitments to provide mitigation for any wetland impacts, to conduct seagrass surveys during the growing season and provide mitigation for all direct and indirect impacts to seagrass beds, to conduct a pre-construction survey for bald eagle nests within 660 feet of the project, and to develop a wildlife watch plan which includes the FWC's *Standard Manatee Conditions for In-Water Work*. Further coordination with our agency will be necessary in order to determine site-specific measures for this project. For technical assistance and coordination on manatees and sea turtles, respectively, please contact Ms. Mary Duncan and Dr. Robbin Trindell of our Imperiled Species Management Section in Tallahassee at (850) 922-4330.

Thank you for the opportunity to review the WEBAR for the SR 60 Multi-Use Trail project in Hillsborough and Pinellas counties. Please contact Mr. Brian Barnett at (850) 528-6316 or email [brian\\_barnett@urscorp.com](mailto:brian_barnett@urscorp.com) to initiate the process for further overall coordination on this project.

Sincerely,



Scott Sanders  
Habitat & Species Conservation Section Leader

ss/bb

ENV 1-13-2

Courtney Campbell Causeway\_3343\_041811

cc: Brian Barnett, URS Corporation, Vero Beach  
Mary Duncan, FWC, Tallahassee  
Robbin Trindell, FWC, Tallahassee





FLORIDA DEPARTMENT OF STATE  
Kurt S. Browning  
DIVISION OF HISTORICAL RESOURCES

Ms. Linda Anderson  
US Department of Transportation  
Federal Highway Administration/ Florida Division Office  
545 John Knox Road, Suite 200  
Tallahassee, Florida 32303

April 27, 2011

RE: DHR Project File No.: 2011-00919B/Additional Information Received by DHR: April 26, 2011  
Financial Project ID No: 422640-2/FAP No.: 9045-090-C  
Project: *Cultural Resource Assessment Survey: SR 60 (Courtney Campbell Causeway)  
Multi-Use Trail PD&E Study from Bayshore Boulevard to West of Ben T. Davis  
Beach Entrance, Pinellas and Hillsborough Counties*

Dear Ms. Anderson:

This office received and reviewed the above referenced project in March 2011. As a result of the review, this office requested additional information on the presence of submerged cultural resources within the proposed project area. This office subsequently participated in a series of meetings with staff from the Florida Department of Transportation (FDOT) Central Environmental Management Office (CEMO) and FDOT District 7 discussing this project. After reviewing additional information provided by the FDOT District 7, this office is recommending that the proposed project will have no effect on properties listed or eligible for listing on the National Register of Historic Places.

This office would like to commend both the FDOT CEMO staff and FDOT District 7 staff on the consultation efforts undertaken for this project. If you have any questions, please contact Ginny Jones, Architectural Historian, Transportation Compliance Review Program, via email [gljones@dos.state.fl.us](mailto:gljones@dos.state.fl.us), or at 850.245.6333.

Sincerely,

Laura A. Kammerer  
Deputy State Historic Preservation Officer  
For Review and Compliance

PC: Roberto Gonzalez, FDOT District 7, Tampa  
Rebecca Spain Schwarz, Atkins, Tampa  
Roy Jackson, FDOT CEMO, Tallahassee/#5500  
Nahir DeTizio, FHWA, Tallahassee  
Robin Rhinesmith, FDOT District 7, Tampa

500 S. Bronough Street • Tallahassee, FL 32399-0250 • <http://www.flheritage.com>

Director's Office  
(850) 245-6444 • FAX: 245-6452

Archaeological Research  
(850) 245-6333 • FAX: 245-6437

Historic Preservation  
(850) 245-6400 • FAX: 245-6433



RECEIVED  
DISTRICT SECRETARY  
DISTRICT SEVEN  
2011 MAY -6 11:37

May 3, 2011

Mr. Donald Skelton, PE  
District Secretary  
Florida Department of Transportation  
11201 N. McKinley Drive  
Tampa, FL 33612

RECEIVED  
PLANNING UNIT  
2011 MAY 19 PM 2:11

Mayor Joe Affronti, Sr.  
City of Temple Terrace  
MPO Chairman

Commissioner Mark Sharpe  
Hillsborough County  
MPO Vice Chairman

RE: Courtney Campbell Multi-use Trail

Dear Secretary Skelton:

Commissioner Kevin Beckner  
Hillsborough County

Councilman Joseph Caetano  
City of Tampa

Joe Lopano  
Hillsborough Co. Aviation Authority

Commissioner Rick A. Lott  
City of Plant City

Commissioner Lesley "Les" Miller, Jr.  
Hillsborough County

Commissioner Sandra Murman  
Hillsborough County

Councilman Tom Scott  
City of Tampa

Councilman Curtis Stokes  
HART

Joseph Waggoner  
Expressway Authority

Richard Wainio  
Tampa Port Authority

Derek L. Doughty (Ex-Officio)  
The Planning Commission

Donald J. Skelton, P.E. (Ex-Officio)  
FDOT, District Seven

Ramond A. Chiaramonte, AICP  
Executive Director

The Hillsborough County Metropolitan Planning Organization (MPO) supports the continued funding of the Courtney Campbell Multi-use Trail. The project is consistent with the high priorities of the 2035 Long Range Transportation Plan (LRTP) that was adopted by the MPO Board on December 9, 2009.

The proposed Courtney Campbell Causeway multi-use trail will connect the Bayshore Trail extension (Bayshore Boulevard at SR 60) in Pinellas County to Hillsborough County's sidewalk/multi-use path around the Tampa Airport Interchange in Hillsborough County. The proposed facility is intended for bicycle, pedestrian, and other recreational users thereby providing alternate modes of transportation currently unavailable for local and regional non-motorized trips.

The Courtney Campbell Causeway Multi-use Trail will accommodate recreational users that can experience the scenic qualities of the Causeway that could further enhance tourism and economic development.

We strongly support this project and are providing our full endorsement of the Courtney Campbell Multi-use Trail.

Sincerely

Joseph Affronti, Sr.  
MPO Chairman

Hillsborough County  
Metropolitan Planning Organization  
P.O. Box 1110  
601 E. Kennedy, 18<sup>th</sup> Floor  
Tampa, Florida 33601-1110  
813/272-5940  
FAX NO: 813/301-7172  
<http://www.hillsboroughmpo.org>  
e-mail: [transportation@plancom.org](mailto:transportation@plancom.org)

Xc: Ming Gao, FDOT



March 28, 2011

CITY OF TAMPA GREENWAYS  
AND TRAILS CITIZENS  
ADVISORY COMMITTEE  
Sally Thompson, Chair  
Randy Stribling, Vice-Chair

Ming Gao  
Intermodal Systems Development Manager  
FDOT-District 7  
11201 N McKinley Drive, MS 7-500  
Tampa, FL 33612-6456

Dear Mr. Gao,

The City of Tampa Greenways and Trails Citizen Advisory Committee would like to express our support of the proposed Courtney Campbell Causeway trail and pedestrian bridge. This trail is included in the City of Tampa Greenways and Trails Master Plan that was adopted by City Council in 2001. It is also an important connection in a regional trail that would link Hillsborough and Pinellas Counties.

We commend the Florida Department of Transportation for supporting and pursuing funding for this multi-modal transportation effort.

Sincerely,

Sally Thompson, Chair  
City of Tampa Greenways and Trails Citizen Advisory Committee

FOR MORE INFORMATION  
Karla Price, RLA, ASLA  
Tampa Parks and Recreation  
Department  
3402 W. Columbus Drive  
Tampa, FL 33607  
(813) 274-5134 (phone)  
(813) 274-7429 (fax)  
[www.tampagov.net/dept\\_parks/trails](http://www.tampagov.net/dept_parks/trails)

Xc: Karen Palus, Director, City of Tampa Parks and Recreation  
Lori Snively, Bicycle/Pedestrian/Public Involvement Coordinator, FDOT

RECEIVED  
PLANNING UNIT  
2011 MAR 30 PM 2:44



*Chair*  
Vice Mayor William D. Dodson

*Vice-Chair*  
Commissioner Larry Bustle

*Secretary/Treasurer*  
Mayor Robert Minning

*Executive Director*  
Manny Pumariega

April 4, 2011

Mr. Ming Gao  
Florida Department of Transportation  
District 7, SIS Coordinator Alternate  
11201 N. Malcolm McKinley Dr., MS 7-500  
Tampa, Florida 33612-6403

Dear Mr. Gao:

It is my pleasure to write a letter in support of the FDOT, District 7, State Road 60 Multi-Use Trail project.

The trail will provide bicycle and pedestrian access along an eight-mile stretch from Bayshore Blvd. in Pinellas County to west of the Ben T. Davis Beach entrance in Hillsborough County.

On May 23, 2005 the Courtney Campbell Highway was designated as a Florida Scenic Highway. One of the requirements of this designation was the development of a Corridor Management Plan. An important element of that Plan was the goal of completing a multi-use trail across the full length of the causeway. The Courtney Campbell Scenic Highway Corridor Advisory Committee (CCSH CAC) finds the proposed S.R. 60 Multi-Use Trail project consistent with the Goals and Policies found in its Corridor Management Plan.

In addition the CCSH CAC would like to encourage the utilization of public art reflecting the intrinsic resources found along the trail to enhance visual interest for the trail users.

In conclusion, the Courtney Campbell Scenic Highway Corridor Advisory Committee fully supports the SR 60 Multi-Use Trail Project and the ability for the citizens of the Tampa Bay region to be able to travel safely from Pinellas County to Hillsborough County without having to use an automobile.

Sincerely,

Manny L. Pumariega  
Executive Director



# APPENDIX C

Newsletters





# SR 60 (Courtney Campbell Causeway) Multi-Use Trail Project Development & Environment (PD&E) Study



Florida Department  
of Transportation  
District Seven

From Bayshore Boulevard to west  
of the Ben T. Davis Beach entrance

Pinellas County  
Hillsborough County  
WPI Seg. No: 422640 2  
FAP No: 9045-090-C  
March 2011

## Public Hearing Scheduled

Dear Property Owner or Interested Citizen:

You are invited to attend and participate in a public hearing to be held by the Florida Department of Transportation (FDOT) regarding the proposed multi-use trail along SR 60 (Courtney Campbell Causeway) from Bayshore Boulevard in Pinellas County to west of the Ben T. Davis Beach entrance in Hillsborough County. The hearing is being held to give the public an opportunity to provide comments concerning the conceptual design of the proposed multi-use trail along SR 60 within the project limits. This hearing will be split into two separate sessions that will be held at two separate locations.

### Public Hearing Session 1:

Date: Thursday, March 24, 2011  
Place: The Clearwater Christian College (see map) ★  
3400 Gulf-To-Bay Boulevard, Building D  
Clearwater, FL 33759  
Time: Open House 5:00 p.m. until 7:00 p.m.  
Formal Hearing at 6:00 p.m.

### Public Hearing Session 2:

Date: Tuesday, March 29, 2011 ★  
Place: The Westin Tampa Bay (see map)  
7627 West Courtney Campbell Causeway  
Tampa, FL 33607  
Time: Open House 5:00 p.m. until 7:00 p.m.  
Formal Hearing at 6:00 p.m.

Department representatives will be available at each session of the hearing beginning at 5:00 p.m. to answer questions and discuss the project informally. Exhibits and other project related materials will be displayed showing the proposed improvements.

A court reporter will be available to receive comments in a one-on-one setting. At 6:00 p.m., Department representatives will begin the formal portion of the hearing, which will provide an opportunity to make formal public comments. Following the formal portion of the hearing, the informal open house will resume and continue until 7:00 p.m. You may also mail your comments to the address preprinted on the back of the comment form. All comments must be postmarked by Friday, April 8, 2011 to become part of the official public hearing record.

This newsletter serves as notice to property owners (pursuant to Florida Statutes 339.155) that all or a portion of their property is within 300 feet of the centerline of the proposed project. However, this does not mean that all properties would be directly affected.

Maps, drawings, and other pertinent information depicting the project's recommended alignment and proposed improvements will be available for public review from Friday, March 1, 2011 to Friday, April 8, 2011 at:

St. Petersburg College  
Clearwater Campus Library  
2465 Drew Street  
Clearwater, Florida 33765  
M-Th: 7:30 a.m. to 9:00 p.m.  
F: 7:30 a.m. to 4:00 p.m.  
Sa: 10:00 a.m. to 5:00 p.m.

The Dale Mabry Learning Resource  
4001 W. Tampa Bay Boulevard  
Tampa, Florida 33614  
M-Th: 7:30 a.m. to 8:00 p.m.  
F-Sa: 8:00 a.m. to 2:00 p.m.

This material will also be available for review at the FDOT, District Seven Intermodal Systems Development Office, 11201 N. McKinley Drive, Tampa, FL 33612 Monday through Friday from 8:00 a.m. to 5:00 p.m.

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Robin Rhinesmith Project Manager, at (813) 975-6496 or (800) 226-7220 at least 7 working days in advance of the hearing.

Sincerely,

Ming Gao, P.E.  
Intermodal Systems Development Manager  
Florida Department of Transportation, District Seven



## What is a Project Development and Environment (PD&E) Study?

A PD&E study is a comprehensive study that evaluates social, cultural, economic and environmental effects associated with the proposed transportation improvements. The PD&E study allows the Department to reach a decision on the type, location and conceptual design of the necessary improvement along SR 60 (Courtney Campbell Causeway) to accommodate future users in a safe and efficient manner. It represents a combined effort by transportation and environmental professionals who analyze information and document the best alternative for a community's transportation needs. The PD&E study efforts are accomplished by working in cooperation with other state/federal agencies and local governments. This coordination allows the Department to better determine the effects a transportation project will have on the natural and human environment. A Project Development Summary Report (PDSR) and other study documents are being prepared which contain the results of analyses of potential effects to the social, cultural, natural and physical environment.

## Project Description & Need

SR 60 (Courtney Campbell Causeway) is an existing four-lane divided highway. The purpose of this PD&E study is to evaluate a proposed multi-use trail along SR 60 (Courtney Campbell Causeway) from Bayshore Boulevard in Pinellas County to west of the Ben T. Davis Beach entrance in Hillsborough County. This project will provide regional linkage for non-motorized travel between Pinellas and Hillsborough Counties and, with connection to other facilities, travel into Pasco and Hernando Counties. The proposed trail will connect to other existing and planned facilities to the east and west of the causeway. On the Pinellas (west) side, the project will connect to Pinellas County's extensive trail system (proposed Bayshore Trail extension). On the Hillsborough (east) side, the trail will connect to the West Tampa Greenway which will eventually connect via on-street facilities to the Upper Tampa Bay Trail and then from there to the Suncoast Parkway Trail into Pasco and Hernando Counties.



## Florida Scenic Highway

The Courtney Campbell Causeway was designated a Florida Scenic Highway on May 23, 2005. It is part of the network of 26 Florida State Designated Scenic Highways. The Courtney Campbell Causeway Scenic Highway Corridor Advisory Committee established a Corridor Management Plan which includes a series of Vision, Goals, Objectives and Implementation Strategies for the Corridor. These goals include resource protection, enhancing vehicle and pedestrian safety, improving pedestrian and bicycle transportation and enhancing the aesthetics of the corridor. The proposed multi-use trail along SR 60 is consistent with the goals of the Corridor Management Plan.

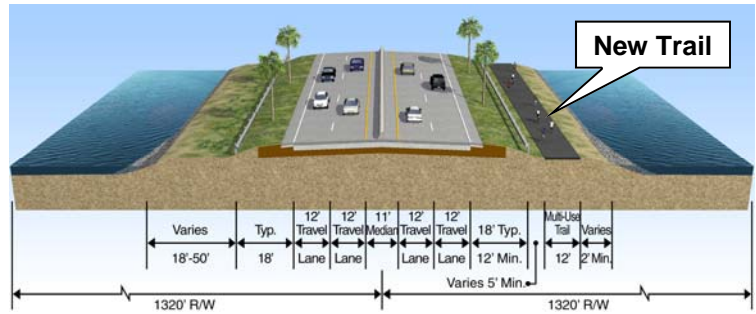
## No-Build Alternative

The No-Build Alternative assumes that the existing conditions would remain for SR 60 within the project limits and only routine maintenance activities would occur, meaning that no new trail would be built. The No-Build Alternative is considered to be a viable alternative and will remain so for the duration of the PD&E study process.

## Recommended Build Alternative

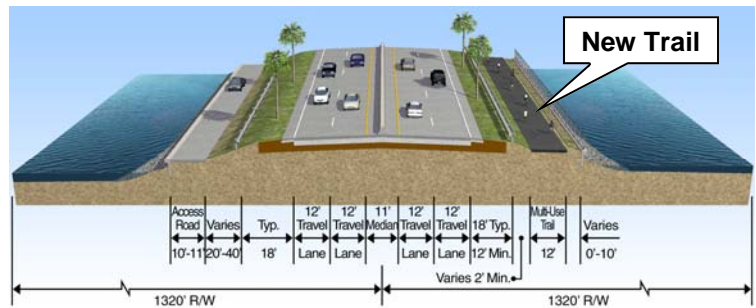
The recommended build alternative includes adding a proposed 12-foot wide multi-use trail to the south side of SR 60. Three different typical sections along different segments of the causeway are proposed within the study limits. The different typical sections are shown below.

**Typical Section 1:** This typical section applies to the west portion of the study area with a proposed trail along the south side of SR 60 between the existing guard rail and seawall.



**Proposed Roadway/Trail Typical Section 1**

**Typical Section 2:** This typical section proposes a trail along the south side of SR 60 between the existing guard rail and sea wall. It will be constructed in the place of an existing maintenance access road.



**Proposed Roadway/Trail Typical Section 2**

**Typical Section 3:** This typical section proposes a trail along the south side of SR 60 where the existing maintenance access road is situated. The access road will be relocated between SR 60 and the trail with a 5-foot separation from the trail and cable guardrail.

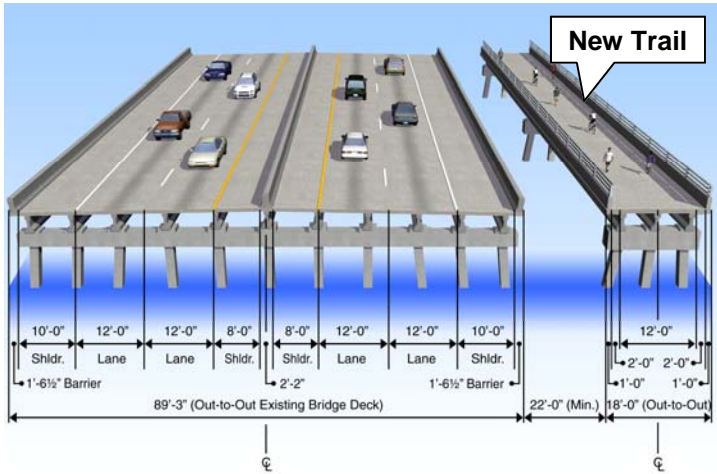


**Proposed Roadway/Trail Typical Section 3**

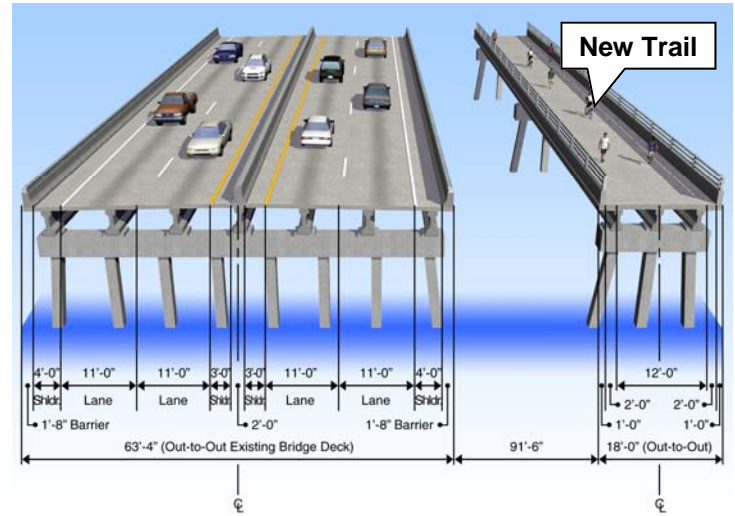


The proposed multi-use trail will require bridge crossings adjacent to SR 60 at two locations within the PD&E study limits for a continuous pathway. The existing SR 60 highway bridges do not have sufficient deck width to accommodate the new trail. Parallel structures are proposed for the multi-use trail.

The separate bridges would be designed to accommodate the heaviest required vehicle to perform routine maintenance and inspection. Independent structures for the trail are recommended due to significant cost savings and ease of construction compared to widening the existing SR 60 highway bridges. The recommended trail bridge typical sections are shown below.



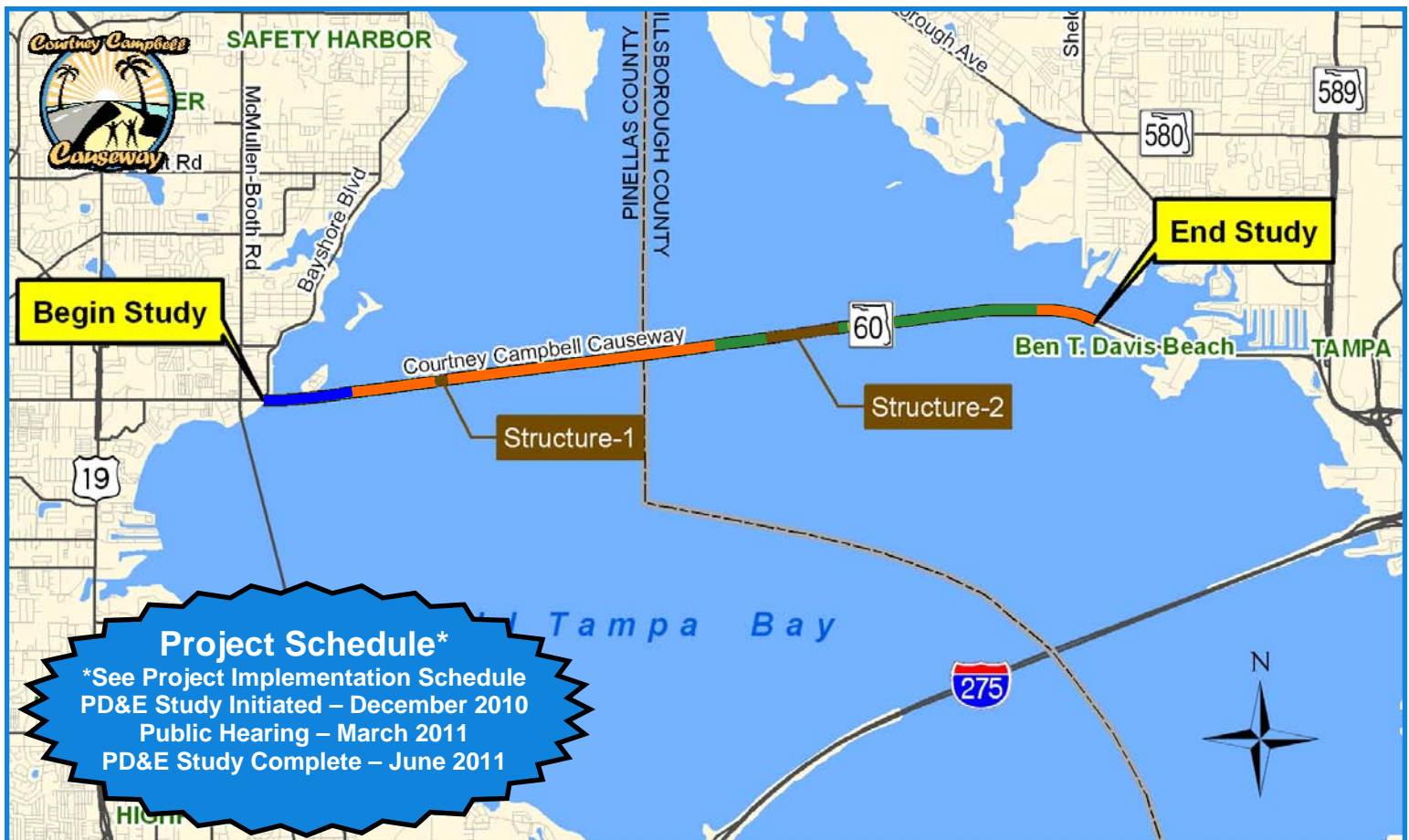
**Proposed Bridge Typical Section Structure 1**



**Proposed Bridge Typical Section Structure 2**

**Environmental Issues Considered**

Within the project limits, portions of Old Tampa Bay are located in the Pinellas County Aquatic Preserve which is designated as an Outstanding Florida Water (OFW). There are seagrass beds located adjacent to the project, but any impacts are minimal. The entire project, with the exception of the trail bridges, will be located on the existing fill used to construct the Causeway. Best management practices will be implemented during construction to prevent impacts to Old Tampa Bay and associated wildlife and marine species.



**Project Schedule\***  
 \*See Project Implementation Schedule  
 PD&E Study Initiated – December 2010  
 Public Hearing – March 2011  
 PD&E Study Complete – June 2011





Florida Department of Transportation, District Seven  
 11201 N. McKinley Drive MS 7-500  
 Tampa, Florida 33612-6456

### Your Comments are Important to Us

We encourage your participation in this SR 60 (Courtney Campbell Causeway) Multi-Use Trail PD&E study. If you wish to discuss any issues related to this project, schedule a small group meeting, or add your name to the mailing list, please contact Robin Rhinesmith, Project Manager, by calling 813-975-6496 or 800-226-7220 or by e-mail to: robin.rhinesmith@dot.state.fl.us.

Written comments may be sent to:

Ming Gao, P.E.  
 Intermodal Systems Development Manager  
 Florida Department of Transportation, District Seven  
 11201 N. McKinley Drive, MS 7-500  
 Tampa, Florida 33612-6456

### Non-Discrimination Laws

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Robin Rhinesmith Project Manager, at (813) 975-6496 or (800) 226-7220 at least 7 working days in advance of the hearing.

### Para Preguntas en español

Valoramos la opinión del público sobre este proyecto. Si usted tiene preguntas o comentarios o si simplemente desea más información en español, favor ponerse en

### Project Implementation Schedule

Activity in FDOT Tentative 5-Year Work Program (FY 2012-2016)			
PROJECT PHASE	(FISCAL YEAR) <sup>1</sup>		
PD&E Study	2011 (this PD&E Study)		
Design	2012	2012	2012 <sup>2</sup>
ROW Acquisition	Not Applicable	Not Applicable	Not Applicable
Construction	2016	2014	2012 <sup>2</sup>
County:	Pinellas		Hillsborough
WPI Seg. No:	424561-3	424561-4	424561-5
Limits:	Bayshore to east of Structure #1	East of Structure #1 to Pin/Hills County Line	Pin/Hills County Line to west of Ben T. Davis Beach entrance

<sup>1</sup> FY denotes Fiscal Year which spans from July 1 to June 30 (i.e. FY 2011 = 7/1/2010 to 6/30/2011)  
<sup>2</sup> WPI Segment 424561-5 planned as a design-build project

contacto con el ingeniero a cargo de este proyecto, el señor Manny Santos, al teléfono (813) 975-6173 o correo electrónico manuel.santos@dot.state.fl.us.

# APPENDIX D

Public Hearing Materials and Transcript





**SR 60 (Courtney Campbell Causeway) Public Hearing**  
**Multi-Use Trail Project Development & Environment (PD&E) study**  
**from Bayshore Boulevard to west of the Ben T. Davis Beach entrance**  
 FDOT District 7, WPI Segment No: 422640-2, FAP No: 9045-090-C  
 Pinellas and Hillsborough Counties

The Florida Department of Transportation (FDOT) invites you to attend and participate in a Public Hearing regarding the SR 60 (Courtney Campbell Causeway) improvements from Bayshore Boulevard to west of the Ben T. Davis Beach entrance in Pinellas and Hillsborough Counties, a distance of approximately 7.4 miles. The Public Hearing is being conducted to allow interested persons to comment on the conceptual design, and social, economic and environmental effects of the proposed improvements, and in accordance with Federal Executive Orders 11990 and 11988. This Public Hearing will be split into two separate sessions that will be held at two separate locations.

**Session 1**

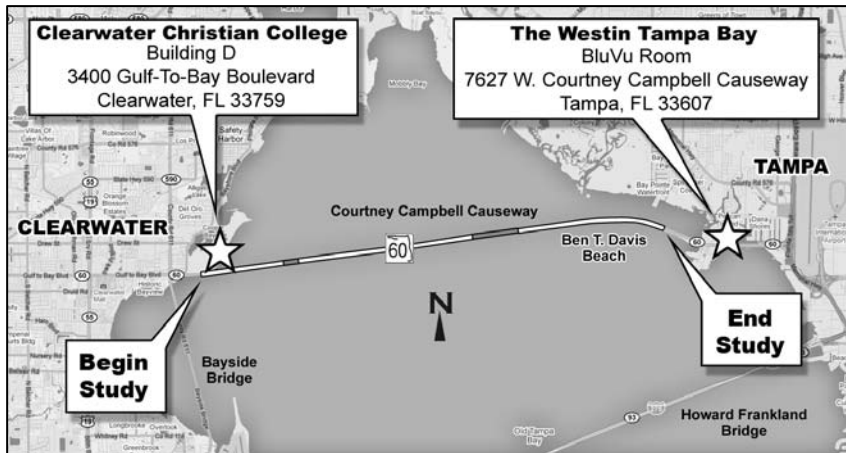
**Date:** Thursday, March 24, 2011  
**Time:** 5:00 p.m. to 7:00 p.m. Open House  
 6:00 p.m. Formal Presentation  
**Where:** Clearwater Christian College  
 Building D  
 3400 Gulf-to-Bay Boulevard  
 Clearwater, FL 33759

Enter from Damascus Road. Free Parking.

**Session 2**

**Date:** Tuesday, March 29, 2011  
**Time:** 5:00 p.m. to 7:00 p.m. Open House  
 6:00 p.m. Formal Presentation  
**Where:** The Westin Tampa Bay  
 BluVu Room  
 7627 West Courtney Campbell Causeway  
 Tampa, FL 33607

Turn into the Westin Tampa Bay Entrance from Gulf-to-Bay Boulevard/SR 60 (Courtney Campbell Causeway). Free Parking.



FDOT representatives will be present to answer questions prior to and following the formal portion at each session. Interested parties may submit written comments at the Hearing or mail them at a later date to: Ming Gao, P.E. Intermodal Systems Development Manager, FDOT-District 7, 11201 North McKinley Drive, MS 7-500, Tampa FL, 33612-6456. Written comments will be accepted throughout the PD&E study, but to be included in the Public Hearing record, comments should be postmarked no later than April 8, 2011.

Project documents will be available for public inspection during regular operating hours between March 1, 2011 and April 8, 2011 at these locations:

FDOT District Seven  
 Intermodal Systems  
 Development Office  
 11201 N. McKinley Drive  
 Tampa, FL 33612  
 (813) 975-6000  
 Hours of Operation: Monday-  
 Friday 8:00 a.m. - 5:00 p.m.  
 Saturday & Sunday Closed

Clearwater Campus Library  
 St. Petersburg College  
 2465 Drew Street  
 Clearwater, FL 33765  
 (727) 791-2617  
 Hours of Operation: Monday-  
 Thursday 7:30 a.m. - 9:00 p.m.  
 Friday 7:30 a.m. - 4:00 p.m.  
 Saturday 10:00 a.m. - 5:00 .pm.  
 Sunday Closed

Dale Mabry Learning Resource  
 4001 West Tampa Bay  
 Boulevard  
 Tampa, FL 33614  
 (813) 253-7381  
 Hours of Operation: Monday-  
 Thursday 7:30 a.m. - 8:00 p.m.  
 Friday & Saturday 8:00 a.m. -  
 2:00 p.m.  
 Sunday & Holidays Closed

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Robin Rhinesmith, Project Manager, at (813) 975-6496 or (800) 226-7220 at least 7 working days in advance of the Hearing session.

## NOTICE OF MEETING / WORKSHOP / HEARING

The FLORIDA DEPARTMENT OF TRANSPORTATION, DISTRICT SEVEN

announces:  Public Meeting  
 Workshop  
 Hearing

Date & Time: Thursday, March 24 & Tuesday, March 29, 2011 5:00 p.m. to 7:00 p.m.

Place:

**Session 1 – March 24, 2011 (Pinellas County)**  
Clearwater Christian College  
3400 Gulf-to-Bay Boulevard, Building D  
Clearwater, FL 33759  
Open house from 5:00-7:00 p.m. with the Formal Hearing at 6:00 p.m.

**Session 2 – March 29, 2011 (Hillsborough County)**  
The Westin Tampa Bay  
7627 West Courtney Campbell Causeway, BluVu Room  
Tampa, FL 33607  
Open house from 5:00-7:00 p.m. with the Formal Hearing at 6:00 p.m.

General Subject Matter to be considered:

This Public Hearing is being held to allow interested persons an opportunity to express their views concerning the location, conceptual design, and social, economic, and environmental effects of the proposed multi-use trail along SR 60 (Courtney Campbell Causeway). WPI Segment No: 422640-2, FAP No: 9045-090-C.

A copy of the agenda may be obtained by contacting:

Ming Gao, P.E., Intermodal Systems Development Manager,  
11201 North McKinley Drive, MS 7-500, Tampa, FL 33612-6456

Pursuant to the provisions of the Americans with Disabilities Act, any person requiring special accommodations to participate in this meeting / hearing / workshop is asked to advise the agency at least:

7 days before the meeting / workshop / hearing by contacting:

Robin Rhinesmith, Project Manager, 11201 North McKinley Drive,  
MS 7-500, Tampa, FL 33612-6456, 1(800)226-7220 or (813)975-6496

If you are hearing or speech impaired, please contact the agency using the Florida Relay Service, 1(800)955-8771 (TDD) or 1(800)955-8770 (voice).

For more information, you may contact:

Robin Rhinesmith, Project Manager at information listed above.





# SR 60 (Courtney Campbell Causeway) Multi-Use Trail

From Bayshore Boulevard to West of the Ben T. Davis Beach Entrance



## Public Hearing Sign-In

WPI Number: 422640-2  
FAP Number: 9045-090-C

Clearwater Christian College  
The Westin Tampa Bay

Thursday, March 24, 2011  
Tuesday, March 29, 2011

Please Select One

NAME (Please Print)	Citizen	Elected Official	Federal / State	Regional / Local	Private Sector	Other	E-mail Address (Please Print)
1 Glen A. Gullickson	✓						svlaurel@rockisland.com
2 PETER ROGAS					✓		PETE.ROGAS@RSANDH.COM
3 RICHARD KOLHOPP	✓						rjk@dosp.org
4 Barbara Landis	✓						
5 Stan Mickelsen	✓				✓		stantheGardenman@gmail.com
6 BILL THOMAS	✓						BTThomas3777@yahoo.com
7 RICHARD VALENTINE	✓						rvalent@tampabay.rr.com
8 KIMM WARDON	✓				✓		
9 CHIP & JOHNN HAYNES	✓						chip.haynes@yahoo.com
10 Doug Burg					✓		Doug.BurgeWantedgroup.com

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Robin Rhinesmith Project Manager, at (813) 975-6496 or (800) 226-7220 at least 7 working days in advance of the hearing.



# SR 60 (Courtney Campbell Causeway) Multi-Use Trail

From Bayshore Boulevard to West of the Ben T. Davis Beach Entrance



## Public Hearing Sign-In

WPI Number: 422640-2  
FAP Number: 9045-090-C

Clearwater Christian College  
The Westin Tampa Bay

Thursday, March 24, 2011  
Tuesday, March 29, 2011

Please Select One

NAME (Please Print)	Please Select One						E-mail Address (Please Print)
	Citizen	Elected Official	Federal / State	Regional / Local	Private Sector	Other	
1 ALFREDO CEY					✓		ACEY@SPRINKLECONSULTING.COM
2 BILL JOHNSON		✓					BILL.JOHNSON@MYCLEARWATER.COM
3 JIM LEONARD						✓	Medica
4 Felicia Leonard				✓			Felicia.Leonard@myclearwater.com
5 Chris Gupstur					✓		Chris.Gupstur@wamtmgroup.com
6 Susan & George Miller	✓			✓			smiller@pinellascounty.org
7 TRACY HOOD	✓						HOODT@PBWORLD.COM
8 JUAN OSVIZARES	✓						OSVIZARES@PBWORLD.COM
9 Jim Bernard					✓		Jim.bernard@cardno.com
10 Tammy Vrana	✓						tvrana@vciplanning.com

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Robin Rhinesmith Project Manager, at (813) 975-6496 or (800) 226-7220 at least 7 working days in advance of the hearing.





# SR 60 (Courtney Campbell Causeway) Multi-Use Trail

From Bayshore Boulevard to West of the Ben T. Davis Beach Entrance



## Public Hearing Sign-In

WPI Number: 422640-2  
FAP Number: 9045-090-C

Clearwater Christian College  
The Westin Tampa Bay

Thursday, March 24, 2011  
Tuesday, March 29, 2011

NAME (Please Print)	Please Select One						E-mail Address (Please Print)
	Citizen	Elected Official	Federal / State	Regional / Local	Private Sector	Other	
1 Courtney Bieh	X	BAE					cbieh145@hotmail.com
2 May McArthur	X						
3 Randall Overton			X				randall.d.Overton@USCG.mil
4 Cheryl McDonald	X				X		cmcdonald@drmp.com cheryl
5 Ron Perbody	X						rperbody@gmail.com
6 Gary Rose	✓						rose4ut@tampabay.rr.com
7 Barbara Borgan	✓						bborgan@acp-fl.com
8 Joe Daniel	✓						jdaniel18@tampabay.rr.com
9 Cherie Smith	✓						alexsmom01@gmail.com
10 Tom MONTGOMERY	✓						t98tri1@msn.com

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Robin Rhinesmith Project Manager, at (813) 975-6496 or (800) 226-7220 at least 7 working days in advance of the hearing.



# SR 60 (Courtney Campbell Causeway) Multi-Use Trail

From Bayshore Boulevard to West of the Ben T. Davis Beach Entrance



## Public Hearing Sign-In

WPI Number: 422640-2  
 FAP Number: 9045-090-C

Clearwater Christian College  
 The Westin Tampa Bay

Thursday, March 24, 2011  
 Tuesday, March 29, 2011

Please Select One

Citizen    Elected Official    Federal / State    Regional / Local    Private Sector    Other

NAME (Please Print)	Citizen	Elected Official	Federal / State	Regional / Local	Private Sector	Other	E-mail Address (Please Print)
1 Marcia Biggs	✓						marciabiggs@yahoo.com
2 Charlotte Thomas			✓				charlotte.thomas@dot.state.fl.us
3 Cory Martens	✓						cmartens@tampabay.rr.com
4 Mike Palozzi	✓						michael.palozzi@shawgrp.com
5 CARL MARTIN	✓						CARLMARTIN@CLEARWATER.EDU
6							
7							
8							
9							
10							

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Robin Rhinesmith Project Manager, at (813) 975-6496 or (800) 226-7220 at least 7 working days in advance of the hearing.





# SR 60 (Courtney Campbell Causeway) Multi-Use Trail

From Bayshore Boulevard to West of the Ben T. Davis Beach Entrance



## Public Hearing Sign-In

WPI Number: 422640-2  
 FAP Number: 9045-090-C

Thursday, March 24, 2011  
 Tuesday, March 29, 2011

Clearwater Christian College  
 The Westin Tampa Bay

NAME (Please Print)	Please Select One						E-mail Address (Please Print)
	Citizen	Elected Official	Federal / State	Regional / Local	Private Sector	Other	
1 EDDIE WALL	✓					✓	ewall5@verizon.net
2 PETER ROGAS						✓	PETE.ROGAS@RSANDU.COM
3 NICK CARROLL	✓						NICK.CARROLL@GMAIL.COM
4 Susan Makara	✓						sue.makara@yahoo.com
5 WENDY FERRO	✓						WENDY.FERRO@HOPKINS.COM
6 Tom Quinn	✓						TQUINN@HOPKINS.COM
7 Chris Bridges	✓						bridgesc@hillsboroughcounty.org
8 Jessica Wright	✓						wakegrvr1@hotmail.com
9 BILL JOHNSON		✓					BILL.JOHNSON@USA.NET
10 BOB CAMPBELL				✓			campbellv@hillsboroughcounty.org

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# SR 60 (Courtney Campbell Causeway) Multi-Use Trail

From Bayshore Boulevard to West of the Ben T. Davis Beach Entrance



WPI Number: 422640-2  
FAP Number: 9045-090-C

## Public Hearing Sign-In

Clearwater Christian College  
The Westin Tampa Bay

Thursday, March 24, 2011  
Tuesday, March 29, 2011

Please Select One

NAME (Please Print)	Please Select One						E-mail Address (Please Print)
	Citizen	Elected Official	Federal / State	Regional / Local	Private Sector	Other	
1 Jamie McClead	X						jamie.mcclead@gmail.com
2 Kelly Myer	X						Kelly_myer@hotmail.com
3 Michael Morris	X						mmorris3@tampabay-fl.com
4 Scott Mahan	X						Scott.Mahan@gmail.com
5 Chris Brown	X						chris.d.brown.1016@gmail.com
6 Tom Fulton	X						Tom.Fulton@Cardno.com
7 Linda Sault-sena	X						lsaulsena@aol.com
8 Lynn Stambnager	X						
9 Scott Walters				X			SCOTT.WALTERS@TAMPAGOV.NET
10 Katherine Dave Carrester	✓						katherine.christian@chicesto.com

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# SR 60 (Courtney Campbell Causeway) Multi-Use Trail

From Bayshore Boulevard to West of the Ben T. Davis Beach Entrance



## Public Hearing Sign-In

WPI Number: 422640-2  
FAP Number: 9045-090-C

Clearwater Christian College  
The Westin Tampa Bay

Thursday, March 24, 2011  
Tuesday, March 29, 2011

NAME (Please Print)	Please Select One						E-mail Address (Please Print)
	Citizen	Elected Official	Federal / State	Regional / Local	Private Sector	Other	
1 Ignatius James DeMio	Yes				✓		Suit321@yahoo.com
2 Mark Krawczyk	Yes						
3 Alan Snel	Yes						
4 Chris Wexler	Yes				✓		
5 NATHANIEL G. WINTHROP	Yes				✓		twinthrop@mac.com
6 JULIAN GUTIERREZ	Yes				✓		JGUTIERREZ@KCAENG.COM
7 John Harrison	YES						JOHNPMX@VERIZON.NET
8 ADRIAN PINEDO	YES					✓	<del>ADRIAN</del> ADRIAN@JONIES@YAHOO.COM
9 JANICE EASTBURN	YES			YES			JANICE.EASTBURN@TAMPAGOV.NET
10 TRACY HOOD	YES						HOOD@PBWORLD.COM

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# SR 60 (Courtney Campbell Causeway) Multi-Use Trail

From Bayshore Boulevard to West of the Ben T. Davis Beach Entrance



## Public Hearing Sign-In

WPI Number: 422640-2  
FAP Number: 9045-090-C

Clearwater Christian College  
The Westin Tampa Bay

Thursday, March 24, 2011  
Tuesday, March 29, 2011

NAME (Please Print)	Please Select One						E-mail Address (Please Print)
	Citizen	Elected Official	Federal / State	Regional / Local	Private Sector	Other	
1 Sidney Crawford	X					X	blumax100@gmail.com Clearwater Audubon Society
2 JOE HITTERMAN	X						JPHITTERMAN@PBSST.COM
3 Scott Garth						X	Sgarth@drmp.com
4 Tina Diack	X						teddiack@spectaenergy.com
5 SCOTT COUSTON			X				SCOTT.COUSTON@POL.STATE.FL.US
6 KEVIN M. TIMONS	X						KTIMONS@AMAZONH324.COM
7 MIKE FERRAGAMO	✓						MFerragamo@aol.com
8 Marty Morlan					✓		MARTY.MORLAN@shawgrp.com
9 JD CAVICROSS	✓						CAVICROSS@FBWORLD.COM
10 JOHN DEWEE	✓						DEWEE@FBWORLD.COM

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# SR 60 (Courtney Campbell Causeway) Multi-Use Trail

From Bayshore Boulevard to West of the Ben T. Davis Beach Entrance



## Public Hearing Sign-In

WPI Number: 422640-2  
FAP Number: 9045-090-C

Clearwater Christian College  
The Westin Tampa Bay

Thursday, March 24, 2011  
Tuesday, March 29, 2011

NAME (Please Print)	Please Select One						E-mail Address (Please Print)
	Citizen	Elected Official	Federal / State	Regional / Local	Private Sector	Other	
1 Drane P Loweke	✓						dlwakey@gmail.com
2 Ryan Bose	✓						ryan.bose@verizon.net
3 Jai Ramlikon	✓						jramkisson@rkk.com
4 Ron Gregory	✓						ron-gregory@URS Corp.com
5 Barbara Bishop	✓						beachsk8tr@yahoo.com
6							
7							
8							
9							
10							

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# SR 60 (Courtney Campbell Causeway) Multi-Use Trail

From Bayshore Boulevard to West of the Ben T. Davis Beach Entrance



WPI Number: 422640-2  
FAP Number: 9045-090-C

## Public Hearing Sign-In

Clearwater Christian College  
The Westin Tampa Bay

Thursday, March 24, 2011  
Tuesday, March 29, 2011

Please Select One

NAME (Please Print)	Citizen	Elected Official	Federal / State	Regional / Local	Private Sector	Other	E-mail Address (Please Print)
1 <i>Bridget Haardt</i>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<i>bshaardt@yahoo.com</i>
2	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
3	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
4	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
5	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
6	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
7	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
8	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
9	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
10	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	

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## Florida Department of Transportation – District Seven Public Hearing Sign-In

Name (Please Print)	E-mail Address (Please Print)
Jeff Novotny	American
SHANNON NILES	AMERICAN
Lori Snively	FDOT
Lee Royal	FDOT
Kirk Bogen	FDOT
Robin Rhinesmith	FDOT
Maria Carmen Hernandez	FDOT
Kris Carson	FDOT
Daniel Intrigo	American
Chris Saliceo	American





## Florida Department of Transportation – District Seven Public Hearing Sign-In

Name (Please Print)	E-mail Address (Please Print)
GORDANA JOVANOVIĆ	gordana.jovanovic@dot.state.fl.us
Peter Hsu	ping.hsu@dot.state.fl.us
MING GAO	ming.gao@dot.state.fl.us
Janille Smith - Colin	janille.smith-colin@dot.state.fl.us
Joe Severson	Joseph.severson@dot.state.fl.us





Number

1

**FLORIDA DEPARTMENT OF TRANSPORTATION  
PUBLIC HEARING  
REQUEST FOR OPPORTUNITY TO SPEAK**

SR 60 (Courtney Campbell Causeway) Multi-Use Trail PD&E study  
Thursday, March 24, 2011 & Tuesday, March 29, 2011

**Please print clearly**

Name: Glen Gullickson

Address: 11800 Park Blvd #205

Seminole FL 33772  
City State Zip Code

Organization (if applicable): \_\_\_\_\_

**Note:** In order to allow all persons the opportunity to speak, please limit your comments to 3 minutes. Should you need additional time, a court reporter will be available to complete your statement immediately following the Hearing.

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Number

2

**FLORIDA DEPARTMENT OF TRANSPORTATION  
PUBLIC HEARING  
REQUEST FOR OPPORTUNITY TO SPEAK**

SR 60 (Courtney Campbell Causeway) Multi-Use Trail PD&E study  
Thursday, March 24, 2011 & Tuesday, March 29, 2011

**Please print clearly**

Name: Stan Mickelson

Address: 128 Kendale Dr

Safety Harbor FL 34695  
City State Zip Code

Organization (if applicable): \_\_\_\_\_

**Note:** In order to allow all persons the opportunity to speak, please limit your comments to 3 minutes. Should you need additional time, a court reporter will be available to complete your statement immediately following the Hearing.

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Number

3

**FLORIDA DEPARTMENT OF TRANSPORTATION  
PUBLIC HEARING  
REQUEST FOR OPPORTUNITY TO SPEAK**

SR 60 (Courtney Campbell Causeway) Multi-Use Trail PD&E study  
Thursday, March 24, 2011 & Tuesday, March 29, 2011

**Please print clearly**

Name: Marcia Biggs

Address: 350 Bailey St.

Safety Harbor 34095

City State Zip Code

Organization (if applicable): Sierra Club

**Note:** In order to allow all persons the opportunity to speak, please limit your comments to 3 minutes. Should you need additional time, a court reporter will be available to complete your statement immediately following the Hearing.

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Number

4

**FLORIDA DEPARTMENT OF TRANSPORTATION  
PUBLIC HEARING  
REQUEST FOR OPPORTUNITY TO SPEAK**

SR 60 (Courtney Campbell Causeway) Multi-Use Trail PD&E study  
Thursday, March 24, 2011 & Tuesday, March 29, 2011

**Please print clearly**

Name: BILL THOMAS

Address: PO BOX 4656

CLEARWATER FL 33758

City State Zip Code

Organization (if applicable): \_\_\_\_\_

**Note:** In order to allow all persons the opportunity to speak, please limit your comments to 3 minutes. Should you need additional time, a court reporter will be available to complete your statement immediately following the Hearing.

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Number

5

**FLORIDA DEPARTMENT OF TRANSPORTATION  
PUBLIC HEARING  
REQUEST FOR OPPORTUNITY TO SPEAK**

SR 60 (Courtney Campbell Causeway) Multi-Use Trail PD&E study  
Thursday, March 24, 2011 & Tuesday, March 29, 2011

**Please print clearly**

Name: KATHY MADDON

Address: 10561 127 PL

LARGO FL 33753  
City State Zip Code

Organization (if applicable): \_\_\_\_\_

**Note:** In order to allow all persons the opportunity to speak, please limit your comments to 3 minutes. Should you need additional time, a court reporter will be available to complete your statement immediately following the Hearing.

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Number 6

**FLORIDA DEPARTMENT OF TRANSPORTATION  
PUBLIC HEARING  
REQUEST FOR OPPORTUNITY TO SPEAK**

SR 60 (Courtney Campbell Causeway) Multi-Use Trail PD&E study  
Thursday, March 24, 2011 & Tuesday, March 29, 2011

**Please print clearly**

Name: Courtney Biehl

Address: 2717 Seville Blvd ~~#12107~~

Clearwater FL 33764  
City State Zip Code

Organization (if applicable): Metro Planning Organization  
Pinellas County Bicycle Advisory Committee

**Note:** In order to allow all persons the opportunity to speak, please limit your comments to 3 minutes. Should you need additional time, a court reporter will be available to complete your statement immediately following the Hearing.

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Number 1

**FLORIDA DEPARTMENT OF TRANSPORTATION  
PUBLIC HEARING  
REQUEST FOR OPPORTUNITY TO SPEAK**

SR 60 (Courtney Campbell Causeway) Multi-Use Trail PD&E study  
Thursday, March 24, 2011 & Tuesday, March 29, 2011

**Please print clearly**

Name: Chris Weber

Address: Weynton Allace

City \_\_\_\_\_ State \_\_\_\_\_ Zip Code \_\_\_\_\_

Organization (if applicable): \_\_\_\_\_

**Note:** In order to allow all persons the opportunity to speak, please limit your comments to 3 minutes. Should you need additional time, a court reporter will be available to complete your statement immediately following the Hearing.

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Number

2

**FLORIDA DEPARTMENT OF TRANSPORTATION  
PUBLIC HEARING  
REQUEST FOR OPPORTUNITY TO SPEAK**

SR 60 (Courtney Campbell Causeway) Multi-Use Trail PD&E study  
Thursday, March 24, 2011 & Tuesday, March 29, 2011

**Please print clearly**

Name: JOHN HARRISON

Address: 16040 SHINNECOCK DR

ODESSA FL 33556

City State Zip Code

Organization (if applicable): \_\_\_\_\_

**Note:** In order to allow all persons the opportunity to speak, please limit your comments to 3 minutes. Should you need additional time, a court reporter will be available to complete your statement immediately following the Hearing.

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Number

3

**FLORIDA DEPARTMENT OF TRANSPORTATION  
PUBLIC HEARING  
REQUEST FOR OPPORTUNITY TO SPEAK**

SR 60 (Courtney Campbell Causeway) Multi-Use Trail PD&E study  
Thursday, March 24, 2011 & Tuesday, March 29, 2011

**Please print clearly**

Name: KEVIN TIMONS

Address: 4140 N. 68<sup>TH</sup> AVE

PINEHURST PARK, FL 33781

City State Zip Code

Organization (if applicable): League of American Bicyclists

**Note:** In order to allow all persons the opportunity to speak, please limit your comments to 3 minutes. Should you need additional time, a court reporter will be available to complete your statement immediately following the Hearing.

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Number 4

**FLORIDA DEPARTMENT OF TRANSPORTATION  
PUBLIC HEARING  
REQUEST FOR OPPORTUNITY TO SPEAK**

SR 60 (Courtney Campbell Causeway) Multi-Use Trail PD&E study  
Thursday, March 24, 2011 & Tuesday, March 29, 2011

**Please print clearly**

Name: Linda Saul-sena

Address: 157 Biscayne Ave.

Tampa FL 33606

City State Zip Code  
Organization (if applicable): Livable Roadways (MPO com.)

**Note:** In order to allow all persons the opportunity to speak, please limit your comments to 3 minutes. Should you need additional time, a court reporter will be available to complete your statement immediately following the Hearing.

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Number

5

**FLORIDA DEPARTMENT OF TRANSPORTATION  
PUBLIC HEARING**

**REQUEST FOR OPPORTUNITY TO SPEAK**

SR 60 (Courtney Campbell Causeway) Multi-Use Trail PD&E study  
Thursday, March 24, 2011 & Tuesday, March 29, 2011

**Please print clearly**

Name: Alan Snel

Address: 1203 E Powhatan Ave

City: Tampa State: FL Zip Code: 33604

Organization (if applicable): SWFBUD

**Note:** In order to allow all persons the opportunity to speak, please limit your comments to 3 minutes. Should you need additional time, a court reporter will be available to complete your statement immediately following the Hearing.

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Number

6

**FLORIDA DEPARTMENT OF TRANSPORTATION  
PUBLIC HEARING  
REQUEST FOR OPPORTUNITY TO SPEAK**

SR 60 (Courtney Campbell Causeway) Multi-Use Trail PD&E study  
Thursday, March 24, 2011 & Tuesday, March 29, 2011

**Please print clearly**

Name: Ron Gregory

Address: 732 Snug Island

Clearwater, FL 33767

City State Zip Code

Organization (if applicable): \_\_\_\_\_

**Note:** In order to allow all persons the opportunity to speak, please limit your comments to 3 minutes. Should you need additional time, a court reporter will be available to complete your statement immediately following the Hearing.

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SR 60 (COURTNEY CAMPBELL CAUSEWAY)  
PUBLIC HEARING  
MULTI-USE TRAIL PD&E STUDY

SESSION 1

DATE: Thursday, March 24, 2011  
TIME: 5 p.m. to 7 p.m.  
PLACE: Clearwater Christian College  
3400 Gulf-to-Bay Boulevard  
Building D  
Clearwater, Florida 33759

REPORTED BY: Rachelle I. Castellana, CSR  
Certified Shorthand Reporter  
Notary Public  
State of Florida

SESSION 2

DATE: Tuesday, March 29, 2011  
TIME: 5 p.m. to 7p.m.  
PLACE: The Westin Tampa Bay  
7627 West Courtney Campbell  
Causeway, BluVu Room  
Tampa, Florida 33607

REPORTED BY: Rachelle I. Castellana, CSR  
Certified Shorthand Reporter  
Notary Public  
State of Florida

I N D E X

**SESSION 1**

Public Comments . . . . . 3

Formal Presentation . . . . . 4

A T T A C H M E N T S

Copy of letter dated 3/24/11 from Marcia Biggs,  
Tampa Bay Sierra Club

**SESSION 2**

Formal Presentation . . . . . 16

Certificate of Reporter . . . . . 30

A T T A C H M E N T S

Copy of Letter dated 3/28/11 from Sally Thomsson, City of  
Tampa Greenways and Trails Citizen Advisory Committee

Copy of letter from Chris Weber, Westshore Alliance

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1 THOMAS CUBA  
2 3760 1ST AVENUE NORTH  
3 ST. PETERSBURG, FLORIDA 33713  
4  
5

6 MR. CUBA: My name is Tom Cuba. I live at 3760  
7 1st Avenue North, St. Petersburg, Florida 33713. This  
8 looks like a very nice project. My only comment is  
9 \$14 trillion in debt and we can probably wait a few  
10 years until we get out of that. Okay.

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**SESSION 1**

1  
2 MR. BOGEN: Good evening. Today is Thursday,  
3 March 24th, 2011, and it is approximately 6 p.m. We  
4 are assembled at the Clearwater Christian College in  
5 Clearwater, Florida.

6 My name is Kirk Bogen, and I am the Project  
7 Development Engineer for District Seven of the Florida  
8 Department of Transportation. Welcome to the public  
9 hearing for the State Road 60, Courtney Campbell  
10 Causeway Multi-use Trail Project Development and  
11 Environment Study or PD&E Study.

12 This public hearing is being held relative to  
13 Work Program Item Segment Number: 422640-2 and Federal  
14 Aid Project Number 9045-090 C. The proposed  
15 improvements include adding a multi-use trail along  
16 the Courtney Campbell Causeway from Bayshore Boulevard  
17 in Pinellas County to west of the Ben T. Davis Beach  
18 entrance in Hillsborough County.

19 This public hearing is being held to give all  
20 interested persons the right to understand the project  
21 and comment on their concerns to the department.  
22 Public participation at this hearing is encouraged and  
23 solicited without regard to race, color, creed,  
24 religion, sex, age, national origin, disability, or  
25 family status.



1           This public hearing is being held in accordance  
2 with the applicable federal and state laws. Those  
3 citations are listed on the board to my right. This  
4 public hearing was advertised consistent with federal  
5 and state requirements and is being conducted  
6 consistent with the Americans with Disabilities Act of  
7 1990. This information is also provided in the  
8 project brochure.

9           This public hearing is being conducted in two  
10 sessions. Both sessions will be combined into a  
11 single public hearing record for this project.

12           The first session is tonight, the 24th day of  
13 March 2011, at the Clearwater Christian College, 3400  
14 Gulf-to-Bay Boulevard in Clearwater, Florida; and the  
15 hearing will continue at the second session on  
16 Tuesday, March 29th, 2011, at 5 p.m. at The Westin  
17 Tampa Bay, 7627 West Courtney Campbell Causeway in  
18 Tampa, Florida.

19           This is your opportunity to receive information  
20 on the project and officially comment on the  
21 recommended "Build" Alternative and the other  
22 documents available here tonight. The recommended  
23 "Build" Alternative is based on comprehensive  
24 environmental and engineering analyses completed to  
25 date, as well as on public comments that have been

1 received.

2 This project meets the maximum air quality  
3 standards established by the U.S. Environmental  
4 Protection Agency (EPA). When you arrived this  
5 evening, you should have received an information  
6 packet containing an informational newsletter and a  
7 comment form.

8 If you weren't able to sign in or did not receive  
9 an information packet, please stop by our sign-in  
10 table before leaving this evening. You should also  
11 have had the opportunity to view the audio-visual  
12 presentation that is continuously running throughout  
13 this public hearing.

14 On projects such as this, one of the unavoidable  
15 consequences is the necessary acquisition of  
16 properties and the relocation of families and  
17 businesses. However, on this project, we anticipate  
18 no property acquisitions and no relocations.

19 Before I continue, I would like to recognize any  
20 elected officials or their representatives who are  
21 here tonight. If you would, please stand and  
22 introduce yourself for the record.

23 MR. JONSON: Bill Jonson. Council member for the  
24 City of Clearwater.

25 MR. BOGEN: Thank you, Councilman. Anyone

1 desiring to make a statement or present written views  
2 and/or exhibits regarding the location, conceptual  
3 design, social, economic, or environmental effects of  
4 the improvements will now have an opportunity to do  
5 so.

6 You may also make a statement at the public  
7 hearing's second session, as noted earlier, on  
8 Tuesday, March 29th, 2011.

9 If you have completed a speaker's card, please  
10 give them to a Department staff, and it looks like  
11 this. If you have not received a speaker's card and  
12 wish to speak, please raise your hand so that we can  
13 get you a card to complete.

14 Written statements and exhibits may be presented  
15 in lieu of or in addition to oral statements. All  
16 written statements received at either session of the  
17 public hearing and at the Florida Department of  
18 Transportation District Seven office located at 11201  
19 North McKinley Drive, Tampa, Florida 33612, postmarked  
20 no later than April 8, 2011, will become a part of the  
21 public record for this hearing.

22 All written comments should be addressed to Ming  
23 Gao. The address is noted on the back of the comment  
24 form and in the advertisement for this public hearing.

25 We now call upon those who have turned in speaker

1 cards. When you come forward, please state your name  
2 and address, for the record.

3 If you represent an organization, municipality or  
4 other public body, please provide that information as  
5 well. We ask that you limit your comments to 3  
6 minutes. If you have additional comments, you may  
7 continue with the court reporter after the formal  
8 session.

9 Please come to the microphone so that the court  
10 reporter can hear you and get a complete record of  
11 your comment. The first speaker is Glen Gullickson.

12 MR. GULLICKSON: My name is Glen Gullickson, and  
13 I live in Seminole, Florida. I'm here to speak on  
14 behalf of the proposed multi-use trail along SR 60  
15 Courtney Campbell Causeway from Bayshore Boulevard in  
16 Pinellas County to west of Ben. T Davis Beach entrance  
17 in Hillsborough County, a distance of 8 miles.

18 Completion of a cross-bay connector for  
19 pedestrians and bicycles is important -- is an  
20 important step in linking both Pinellas and  
21 Hillsborough County trail systems.

22 I strongly encourage the Florida Department  
23 of Transportation to approve this proposal. Thank  
24 you.

25 MR. BOGEN: Thank you for your comment. The next



1 speaker is Stan Mickelsen. Please state your name and  
2 address.

3 MR. MICKELSEN: Stan Mickelsen, and I live at 128  
4 Kendale Drive in Safety Harbor. I wanted to speak in  
5 support of a trail like this. It's definitely going  
6 to make a big difference to cyclists. Cyclists do it  
7 recreationally and we also do it just for sheer  
8 necessity.

9 It's not unheard of to ride across the Bay on  
10 a bicycle. Along Highway 60, you kind of need to have  
11 more trails. It's a very heavily traveled road, and I  
12 would like encourage everyone to consider that it  
13 would facilitate cycling a lot if all roads were  
14 simply built with an extra 4 feet of pavement along  
15 the side.

16 It doesn't have to be a designated bike trail, it  
17 just makes it possible. Now it might not make it  
18 possible for all cyclists to use it, but it would  
19 benefit all cyclists if most cyclists could use it.

20 It just creates an atmosphere that's friendly to  
21 the bicycle. That's what is so needed here.

22 MR. BOGEN: Thank you, sir. Our next speaker is  
23 Marsha Biggs.

24 MS. BIGGS: Hi, everybody. My name is Marsha  
25 Biggs, and I am the Chair of the Tampa Bay Sierra

1 Club. I'm also a resident of Safety Harbor and a  
2 recreational bicycler. I have a comment letter here I  
3 am going to submit from the Sierra Club and I'm just  
4 going to read it to you real quick now.

5 "This letter serves as an official statement of  
6 support by the Tampa Bay Sierra Club for the building  
7 and funding of a multi-use trail as a regional  
8 connector for non-motorized travel adjacent to the  
9 Courtney Campbell Causeway.

10 "The establishment of such a trailway would serve  
11 to encourage noncarbon producing modes of  
12 transportation such as bicycles, walking, inline  
13 skating and running, which would ultimately serve to  
14 benefit our local residents.

15 "The Tampa Bay Sierra Club supports the use of  
16 alternative forms of transit which would help reduce  
17 congestion and carbon emissions and support healthier  
18 lifestyles for those living on both sides of the Bay.

19 "Our group, with nearly 1700 members, embraces  
20 the natural beauty of this area and many regularly  
21 walk and ride our bicycles.

22 "Not only would this trail connect two counties,  
23 it would also connect us as neighbors and residents of  
24 one regional community. By connecting each end to  
25 another trail system, which is the Upper Tampa Bay

1 Trail in Hillsborough and the Ream Wilson Trail in  
2 Pinellas, this Causeway stretch would offer an  
3 incredibly scenic ride which would easily be the  
4 subject of positive regional and national publicity  
5 and coverage.

6 "As a recreational bicycler, I personally can  
7 assure you I would be one of thousands who would use  
8 this trail. Build it and we will come."

9 MR. BOGEN: Thank you. Our next speaker is Bill  
10 Thomas.

11 MR. THOMAS: Good evening. My name is Bill  
12 Thomas. I'm from Clearwater. I want to speak in  
13 opposition of the trail. With our current deficit  
14 that's going on in our nation to accept \$21 million  
15 worth of federal funds, I believe is -- it 's almost  
16 criminal.

17 We need to return that money to the federal  
18 government and tell them that for trails and stuff  
19 like that that we want to do should be the  
20 responsibility of the American people or the citizens  
21 of Florida.

22 Two other things. Once the trail is completed,  
23 one thing I would like to know about is who is going  
24 to be responsible for any liability if someone gets  
25 hurt out there? Is it going to be the State of

1 Florida or is it going to be Pinellas County or  
2 Hillsborough County, depending; and maintenance of the  
3 trail. Who is going to be responsible for that? Is  
4 it going to fall on the county level? Thank you.

5 MR. BOGEN: Thank you for your comment. You can  
6 talk with us afterwards. We will try to answer some  
7 of your questions.

8 MR. THOMAS: Okay. Thank you.

9 MR. BOGEN: Our next speaker is Cathy Maddon or  
10 Haddon?

11 MS. HADDON: Haddon. Hi. My name is Cathy  
12 Haddon, and my address is 10561 127th Place, Largo,  
13 Florida 33773. And I'm just here to voice my  
14 opposition to this trail.

15 And as the prior speaker said, in the economy  
16 that we face today, we have to stop taking federal  
17 money sometime. If the bikers in this room want to  
18 provide a trail, then let them pay for it.

19 I would like to see the ridership study that was  
20 done and I also would like to know what percentage of  
21 the population in Pinellas County are, in fact,  
22 cyclists. Thank you.

23 MR. BOGEN: Okay. Our next speaker is Courtney  
24 Biehl.

25 MS. BIEHL: My name is Courtney Biehl. My



1 address is 2717 Seville Boulevard, Clearwater,  
2 Florida. I am here as a citizen of this area and a  
3 user of the Courtney Campbell Causeway, first and  
4 foremost.

5 Secondly, representing the Bicycle Advisory  
6 Committee of the Pinellas County Metropolitan Planning  
7 Organization Bicycle Advisory Committee. Are my 3  
8 minutes up yet? We would like to extend our interest  
9 in this project. Our support of this project and  
10 willingness to partner with you in developing  
11 resources and safety through our marketing and public  
12 relations and safety committees. Thank you.

13 MR. BOGEN: Thank you. That was my last card.  
14 Does anyone else wish to speak?

15 The verbatim transcript of both sessions of the  
16 hearing's oral proceedings will be available for  
17 inspection at the District Seven Office for public  
18 review upon request within 3 weeks. Thank you for  
19 attending this public hearing and for providing your  
20 input into this project. It is now approximately  
21 6:15.

22 I hereby officially close the formal portion of  
23 the public hearing's first session for the State Road  
24 60, Courtney Campbell Causeway Multi-use Trail PD&E  
25 Study.

1           This formal portion will be continued with the  
2           second session on Tuesday, March 29th, 2011, in Tampa,  
3           Florida. Doors will open at 5 p.m. where the same  
4           materials on display here tonight will also be shown.  
5           The formal portion for that second session will begin  
6           at 6 p.m.

7           You may continue to view the material on display  
8           and speak with our project staff. On behalf of the  
9           Florida Department of Transportation, thank you for  
10          attending. Good night and please drive home safely.

11    (END OF SESSION ONE)

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**SESSION 2**

1  
2 MR. BOGEN: Good evening. Today is Tuesday,  
3 March 29th, 2011, and it is approximately 6 p.m. We  
4 are assembled at The Westin Tampa Bay in Tampa,  
5 Florida.

6 My name is Kirk Bogen, and I am the Project  
7 Development Engineer for District Seven of the Florida  
8 Department of Transportation. Welcome to the public  
9 hearing for the State Road 60, Courtney Campbell  
10 Causeway Multi-use Trail Project -- Project  
11 Development and Environment Study or PD&E Study.

12 This public hearing is being held relative to  
13 Work Program Item -- Item Segment Number: 422640-2 and  
14 Federal Aid Project Number 9045-090 C.

15 The proposed improvements involve adding a multi-  
16 use trail along the Courtney Campbell Causeway from  
17 Bayshore Boulevard in Pinellas County to west of the  
18 Ben T. Davis Beach entrance in Hillsborough County.

19 This public hearing is being held to give all  
20 interested persons the right to understand the project  
21 and comment on their concerns to the Department.  
22 Public participation at this hearing is encouraged and  
23 solicited without regard to race, color, creed,  
24 religion, sex, age, national origin, disability, or  
25 family status.

1           This public hearing is being held in accordance  
2 with applicable federal and state laws. Those  
3 citations are listed on the board next to the sign-in  
4 table. This public hearing was advertised consistent  
5 with federal and state requirements and is being  
6 conducted consistent with the Americans with  
7 Disabilities Act of 1990. This information is also  
8 provided in the project brochure.

9           This public hearing is being conducted in two  
10 sessions. Both sessions will be combined into a  
11 single public hearing record for this project.

12           The first session was held on Thursday March  
13 24th, 2011, at the Clearwater Christian College, 3400  
14 Gulf-to-Bay Boulevard in Clearwater, Florida; and the  
15 second session is tonight, the 29th day of March,  
16 2011, at The Westin Tampa Bay, 7627 West Courtney  
17 Campbell Causeway in Tampa, Florida.

18           This is your opportunity to receive information  
19 on the project and officially comment on the  
20 recommended "Build" Alternative and other documents  
21 available here tonight. The recommended "Build"  
22 Alternative is based on comprehensive environmental  
23 and engineering analyses completed to date, as well as  
24 on public comments that have been received.

25           This project meets the maximum air quality



1 standards established by the U.S. Environmental  
2 Protection Agency or EPA.

3 When you arrived this evening, you should have  
4 received an information packet -- like this --  
5 containing an informational newsletter and a comment  
6 form. If you weren't able to sign in or did not  
7 receive an information packet, please stop by our  
8 sign-in table before leaving this evening.

9 You should also have had the opportunity to view  
10 the audio-visual presentation that is continuously  
11 running throughout this public hearing.

12 On projects such as this, one of the unavoidable  
13 consequences is the necessary acquisition of property  
14 and the relocation of family and businesses. On this  
15 project, however, we anticipate no property  
16 acquisitions and no relocations.

17 Before I continue, I would like to recognize any  
18 elected officials or their representatives who are  
19 here tonight. If you would, please stand and  
20 introduce yourself.

21 MR. JONSON: Bill Jonson. Council member for the  
22 City of Clearwater.

23 MR. BOGEN: Thank you, Councilman. Anyone  
24 desiring to make a statement or present written views  
25 and/or exhibits regarding the location, conceptual

1 design, social, economic, or environmental effects of  
2 the improvements will now have an opportunity to do  
3 so.

4 If you have completed a speaker's card, please  
5 give them to a Department staff member. If you have  
6 not received a speaker card and wish to speak, please  
7 raise your hand so that we can get you a card to  
8 complete. The speaker card looks like this.

9 Written statements and exhibits may be presented  
10 in lieu of or in addition to oral statements. All  
11 written statements received at either session of this  
12 public hearing and at the Florida Department of  
13 Transportation District Seven office located at 11201  
14 North McKinley Drive, Tampa, Florida 33612, postmarked  
15 no later than April 8, 2011, will become a part of the  
16 public record for this hearing.

17 All written comments should be addressed to Ming  
18 Gao. The address is noted on the back of the comment  
19 form and in the advertisement for the public hearing.

20 We now call upon those who have turned in speaker  
21 cards. When you come forward, please state your name  
22 and address, for the record. If you represent an  
23 organization, municipality, or other public body,  
24 please provide that information as well.

25 We ask that you limit your comments to 3 minutes.

1 If you have additional comments, you provide them --  
2 you may continue with the court reporter after the  
3 formal session. Please come to the microphone so that  
4 the court reporter can hear you and get a complete  
5 record of your comment. The first speaker is Chris  
6 Weber.

7 MR. WEBER: Thank you. Chris Weber. I'm  
8 representing Westshore Alliance. I have a letter to  
9 introduce for the record. I just want to say, the  
10 alliance fully supports this project. We appreciate  
11 the Department's efforts in working this.

12 We know many people in this room. A lot of  
13 people have worked on this for a long time. We are  
14 glad to be a part of it. We are willing to help in  
15 any way we can and look forward to the project. Thank  
16 you.

17 MR. BOGEN: Thank you. Do you want to submit  
18 that to one of the staff so that we can give it to the  
19 court reporter. The next speaker is John Harrison.

20 MR. HARRISON: Good evening. I'm John Harrison.  
21 16040 Shinnecock Drive in Odessa. I fully support  
22 this project and I'm glad to see it proceeding. A  
23 little story in support.

24 I was at the Bike Fest that Hillsborough County  
25 put on this past year and was talking to some of the

1 people that were attending, and there was one  
2 individual who had biked to the festival, which is  
3 clear across Hillsborough County at Wilderness Park,  
4 and he biked from Clearwater Beach.

5 So that was quite a ride. He did not take  
6 Courtney Campbell. He went up north because he didn't  
7 think this was safe. So obviously, there is a need  
8 for this type of trail and two counties being involved  
9 here. I think it is a terrific thing to be hooking up  
10 the different trails that are currently available.

11 Also, I know some people in the environmental or  
12 in the Ironman Triathalon, and one of those  
13 individuals was looking for connections to trails to  
14 bike over and get some extra miles in. It's very  
15 difficult. We are proceeding slowly to interconnect  
16 all the trails such as the Suncoast, now this, the  
17 Tampa Bay Trail, that type of thing.

18 I think it's going to become a tourist  
19 attraction and many people will come as a result. It  
20 is going to improve the scenic access to the bay, and  
21 also, you know, be the generator of connections to  
22 downtown Tampa -- obviously -- Clearwater, and alot of  
23 other regional trails. Thank you.

24 MR. BOGEN: Thank you. Our next speaker is Kevin  
25 Timons.



1 MR. TIMONS: My name is Kevin Timons. I reside  
2 at 4140 North 68th Avenue, Pinellas Park. I have got  
3 a long-time bicycle rider in the Bay area. Most of it  
4 was in Tampa. I'm living over there now in Pinellas.

5 But I have a story going back to the late '70s  
6 when a group of my friends got together and we did the  
7 trail or did the road -- I should say -- not the  
8 trail.

9 We started at like 7 o'clock in the morning  
10 because of the traffic. We didn't want to be involved  
11 with anything coming by us at 60 miles an hour. So at  
12 7:00 in the morning it's a little more peaceful.

13 But I have done along Courtney Campbell several  
14 times. The little frontage road is a little easier  
15 than trying to ride the bike shoulder, if you want to  
16 call it a shoulder, traffic going by at 60 miles an  
17 hour is not very friendly.

18 But the trail would be a great enhancement since  
19 Friendship Trail is no longer in operation anymore.  
20 There is a lot of call for people wanting to go across  
21 the water, great scenic views, and I personally loved  
22 Friendship Trail.

23 But it's an opportunity here with the new trail  
24 opening up and being very friendly and no automobile  
25 traffic would be great. Thank you.

1 MR. BOGEN: Thank you. Our next speaker is Linda  
2 Saul-Sena.

3 MS. SAUL-SENA: Good afternoon. I live at 157  
4 Biscayne in Tampa. And for many years -- for 19 years  
5 -- actually for 23 years, I served as a member of The  
6 Metropolitan Planning Organization and we had many  
7 traditional initiatives.

8 But this particular initiative was an outgrowth  
9 of a corroboration between Commissioner Bill Jonson,  
10 representing Pinellas County; and myself, representing  
11 Hillsborough County.

12 It seemed to us that this would be a tremendous  
13 attribute for both areas to have a connecting trail.  
14 When the Friendship Trail -- when it became evident  
15 that that could no longer continue, this was a  
16 tremendous opportunity and we are so pleased to see  
17 that it is going to happen.

18 We have made steps to get Courtney Campbell named  
19 an official scenic corridor according to the Florida  
20 rules. But to be able to have a safe way for  
21 pedestrians and bicyclists to go from one county to  
22 the other, is a huge win for both of our communities.

23 It is wonderful in terms of the connectivity, the  
24 beauty of the corridor; the ability not only for  
25 tourists, but for locals to use it for recreational

1 riding and even potentially for commuting.

2 There are so many reasons why this is a stellar  
3 project and I want to thank the Florida Department of  
4 Transportation for recognizing the value of this  
5 particular project, and I encourage its support and I  
6 can't wait to ride it. Thank you.

7 MR. BOGEN: Thank you. Our next speaker is Allen  
8 Snel.

9 MR. SNEL: Good evening. Alan Snel. 1203 East  
10 Powhatan Avenue in Tampa. I'm the director of SWFBUD,  
11 Southwest Florida Bicycle United Dealers. We are an  
12 alliance of 12 bicycle stores in the Tampa Bay area.

13 We represent thousands of bicyclists among our  
14 customers. It is my great pleasure to come here and  
15 express and voice my support for this terrific  
16 project.

17 It's a long time coming. And it is really  
18 unfolding against a backdrop of a time when bicycle  
19 safety has emerged as a central issue. So that needs  
20 to be considered as well. Right now you can  
21 technically bicycle across the Causeway but because of  
22 the high speed and limited access, it forces  
23 bicyclists to cross a span that is -- offers perhaps 3  
24 or 4 feet of current space to ride a bicycle which,  
25 quite frankly, imperils the safety of bicyclists right

1 now.

2 The drivers who pass current bicyclists over the  
3 high span, I'm guessing probably by -- a 3-foot buffer  
4 right now. There are many reasons why this is a  
5 terrific project.

6 We here in Tampa Bay define our identity through  
7 this incredible body of water to the right and Tampa  
8 Bay, this body of water unites our whole region.  
9 Providing access and safe trail access for  
10 pedestrians, bicyclists, all people in non-vehicular  
11 or motorized vehicles, provides -- will provide  
12 literally access to thousands and thousands of more  
13 users than what you have right now.

14 The Friendship Trail from what I understand had  
15 600,000 users per year and I would suspect that we  
16 will see a very similar number of users of all  
17 backgrounds. It's very important of all backgrounds  
18 in terms of all kinds of users.

19 And also what's really interesting of all kinds  
20 of geographic backgrounds as well. Also there is a  
21 tourism aspect -- an economic development aspect of  
22 this.

23 You will have tourists interested in coming to  
24 this particular stretch of trailway because of the  
25 tremendous access to the bay and of the scenic view.



1 So there is a tourism factor.

2 I'm told that when tourists come to Florida, one  
3 of their first requests is "where can I safely ride a  
4 bike in Florida?" So this will provide safe access to  
5 one of the great bodies of water in our state.

6 In addition, we will have an important link in a  
7 regional paved trail system that is badly needed here  
8 in Tampa Bay. Believe it or not, people do ride bikes  
9 to commute.

10 Even though I think people might think there is a  
11 strong recreational aspect to this project, it will  
12 also provide a commuter link because of the U-trail on  
13 the Tampa side providing access to Tampa and Cypress,  
14 and on the other side we will have clear access to  
15 points west on the Clearwater side.

16 So, in summaries, SWFBUD believes this is a  
17 terrific project that enhances the quality of life,  
18 that can provide safe bike commuting, and is adding to  
19 the quality of life in the Tampa Bay region. I  
20 appreciate your time.

21 MR. BOGEN: Thank you. The next speaker is Ron  
22 Gregory.

23 MR. GREGORY: My name is Ron Gregory, and I  
24 reside at 732 Snug Island in Clearwater, Florida  
25 33767. I would like to comment that I really

1 appreciate the work that the Department is doing,  
2 finally connecting the trail and pedestrian access,  
3 thank you from Pinellas County for all the work that's  
4 being done on the Tampa side.

5 I work at the building across the road here and  
6 we have a lot of people who during their lunch hour  
7 want to go jogging -- and because we have a gym and  
8 everything -- one of the issues they have always had  
9 is safety issues, both crossing the road and being  
10 parallel to it and they are kind of restricted.

11 I also want to reiterate what the other gentleman  
12 said. From personal experience, when you build one of  
13 these facilities you sometimes wonder is it going to  
14 be used. Where I live they recently completed a  
15 bridge structure over to Clearwater Beach from  
16 Memorial Causeway, and also in conjunction with an  
17 earlier reconstruction the Department did with the  
18 city on the Memorial Causeway Bridge over the  
19 Intercoastal.

20 And there has been a tremendous increase in the  
21 amount of joggers, bicyclers, all hours of the day and  
22 night, I mean literally. So I'm pretty confident that  
23 once this link gets established and particularly with  
24 the issue about Freedom Trail being down and  
25 Friendship Trail down or not being there anymore, that

1 it will be a very vital part of the multi-mobile  
2 aspect of the area.

3 So just from a personal standpoint, I think this  
4 is a very good idea and I compliment the Department on  
5 moving forward with it.

6 As I understand, some portions of it are funded  
7 fairly quickly, so that's good. And I know that Linda  
8 and I and Bill Jonson serve on the Scenic Highway  
9 Committee, and I'm sure that they are in support.  
10 The Scenic Highway Committee is dedicated to enhancing  
11 the use of Causeway and everything about it and I  
12 complement you on that.

13 I believe the Westshore Alliance also probably  
14 made a statement in support of it. And we just concur  
15 with all of those comments. Thank you.

16 MR. BOGEN: Thank you. Does anyone else wish to  
17 speak?

18 Seeing none, the verbatim transcript of both  
19 sessions of the hearing's oral proceedings will be  
20 available for inspection at the District Seven Office  
21 for public review upon request within 3 weeks.

22 Thank you for attending this public hearing and  
23 for providing your input into this project. It is now  
24 approximately 6:20 and I hereby close -- officially  
25 close the formal portion of the public hearing's

1 second and last session for the State Road 60,  
2 Courtney Campbell Causeway Multi-use Trail PD&E Study.

3 You may continue to view the materials on display  
4 and speak with our project staff. On behalf of the  
5 Florida Department of Transportation, thank you for  
6 attending. Good night and please drive home safely.

7 (END OF SESSION 2)

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REPORTER'S CERTIFICATE

STATE OF FLORIDA :

COUNTY OF HILLSBOROUGH :

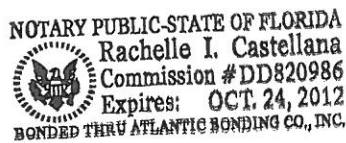
I, Rachelle I. Castellana, certify that I was authorized to and did stenographically report the foregoing FDOT Public Hearings, Session 1 taken on March 24, 2011, and Session 2 taken on March 29, 2011, and that the transcript is a true and complete record of my stenographic notes.

I further certify that I am not a relative, employee, attorney, or counsel of any of the parties, nor am I a relative or employee of any of the parties' attorney or counsel connected with the action, nor am I financially interested in the outcome of the foregoing action.

Dated this 2nd day of April, 2011, IN THE CITY OF TAMPA, COUNTY OF HILLSBOROUGH, STATE OF FLORIDA.

*Rachelle I. Castellana*

Rachelle I. Castellana, Certified Reporter





<p style="text-align: center;"><b>A</b></p> <p><b>ability</b> 22:24  <b>able</b> 6:8 17:6 22:20  <b>about</b> 11:23 26:24  27:11  <b>accept</b> 11:14  <b>access</b> 20:20 23:22 24:9  24:9,12,25 25:4,13,14  26:2  <b>accordance</b> 5:1 16:1  <b>according</b> 22:19  <b>acquisition</b> 6:15 17:13  <b>acquisitions</b> 6:18 17:16  <b>across</b> 9:9 20:3 21:20  23:21 26:5  <b>Act</b> 5:6 16:7  <b>action</b> 29:12,13  <b>actually</b> 22:5  <b>adding</b> 4:15 15:15  25:18  <b>addition</b> 7:15 18:10  25:6  <b>additional</b> 8:6 19:1  <b>address</b> 7:23 8:2 9:2  12:12 13:1 18:18,22  <b>addressed</b> 7:22 18:17  <b>adjacent</b> 10:8  <b>advertised</b> 5:4 16:4  <b>advertisement</b> 7:24  18:19  <b>Advisory</b> 2:19 13:5,7  <b>after</b> 8:7 19:2  <b>afternoon</b> 22:3  <b>afterwards</b> 12:6  <b>against</b> 23:18  <b>age</b> 4:24 15:24  <b>Agency</b> 6:4 17:2  <b>Aid</b> 4:14 15:14  <b>air</b> 6:2 16:25  <b>Alan</b> 23:9  <b>Allen</b> 23:7  <b>alliance</b> 2:21 19:8,10  23:12 27:13  <b>almost</b> 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