

Project Development & Environment Study



Final Project Development Summary Report

SR 60 (Courtney Campbell Causeway)

Multi-Use Trail PD&E Study

from Bayshore Boulevard to West of the Ben T. Davis Beach Entrance

Pinellas and Hillsborough Counties, Florida

Florida Department of Transportation
District 7



Work Program Item Segment Number: 422640 2

FAP Number: 9045-090-C

September 2011



Project Development & Environment Study

State Road (SR) 60 (Courtney Campbell Causeway)
Multi-Use Trail
From Bayshore Boulevard
to West of the Ben T. Davis Beach Entrance

Final Project Development Summary Report

WPI Segment No.: 422640 2
FAP Number: 9045-090 C
ETDM Project No.: 13102
Pinellas & Hillsborough Counties, Florida

Prepared for the



Florida Department of Transportation
District Seven

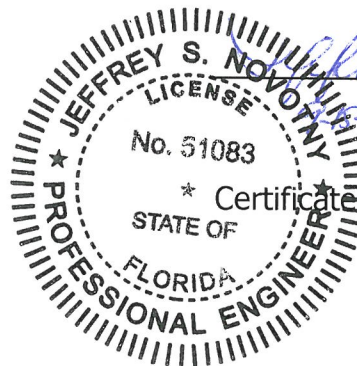
Prepared by:

American Consulting Engineers of Florida, LLC
2818 Cypress Ridge Boulevard, Suite 200 • Wesley Chapel, FL • 33544



Robin Rhinesmith
FDOT Project Manager

September 2011



Jeffrey S. Novotny
Jeffrey S. Novotny, PE
Florida PE # 51083
* Certificate of Authorization No.: 9302

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Florida Department of Transportation

RICK SCOTT
GOVERNOR

11201 McKinley Drive
Tampa, FL 33612

ANANTH PRASAD, P.E.
SECRETARY

September 16, 2011

Mr. Martin Knopp, Division Administrator
Attn.: Ms. Nahir DeTizio, District Transportation Engineer (District 7)
Federal Highway Administration
545 John Knox Road, Suite 200
Tallahassee, Florida 32303

Subject: Type 2 Categorical Exclusion with Project Location & Design Concepts
SR 60 (Courtney Campbell Causeway) Multi-Use Trail
Project Development and Environment (PD&E) Study
From Bayshore Boulevard to west of the Ben T. Davis Beach entrance
Financial Project No.: 422640-2-22-01 / Federal Aid Project No.: 9045-090-C
ETDM Project No.: 13102
Pinellas and Hillsborough Counties

Dear Mr. Knopp:

Enclosed are copies of the Project Development Summary Report, which includes the Summary of Environmental Impacts Checklist for Type 2 Categorical Exclusions, and a transcript of the public hearing held for this project. Upon your review and acceptance of these documents, we request your concurrence that this project is properly classified as a categorical exclusion as described in 23 CFR 771.115 and 771.117, and that the general project location and design concepts described in these documents are acceptable as allowable in 23 CFR 771.113. Please acknowledge your concurrence with these findings by signing and dating this request in the space provided below, and then returning a signed copy for the project files.

Please contact Robin Rhinesmith, Project Manager, at 813-975-6496 or robin.rhinesmith@dot.state.fl.us if we can be of any further assistance.

Sincerely,

Ming Gao, P.E.
Manager,
Intermodal Systems Development

Concurrence by FHWA:

for:

FHWA Division Administrator

10 / 6 / 2011
Date

**Summary of Environmental Impacts Checklist
For Type 2 Categorical Exclusions**

Topical Categories	S	NS	N	NI	Basis for Decision *
A. NATURAL ENVIRONMENT					
1. Air Quality	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<u>See Section 5.1.1</u>
2. Coastal and Marine	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>See Section 5.1.2</u>
3. Contaminated Sites	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>See Section 5.1.3</u>
4. Farmlands	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<u>See Section 5.1.4</u>
5. Floodplains	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>See Section 5.1.5</u>
6. Infrastructure	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<u>See Section 5.1.6</u>
7. Navigation	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>See Section 5.1.7</u>
8. Special Designations	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>See Section 5.1.8</u>
9. Water Quality/Quantity	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>See Section 5.1.9</u>
10. Wetlands	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>See Section 5.1.10</u>
11. Wildlife and Habitat	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>See Section 5.1.11</u>
B. CULTURAL IMPACTS					
1. Historic / Archaeological	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>See Section 5.2.1</u>
2. Recreation Areas	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<u>See Section 5.2.2</u>
3. Section 4(f) Potential	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<u>See Section 5.2.3</u>
C. COMMUNITY IMPACTS					
1. Aesthetics	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<u>See Section 5.3.1</u>
2. Economic	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<u>See Section 5.3.2</u>
3. Land Use	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<u>See Section 5.3.3</u>
4. Mobility	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<u>See Section 5.3.4</u>
5. Relocation	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<u>See Section 5.3.5</u>
6. Social	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<u>See Section 5.3.6</u>
D. OTHER IMPACTS					
1. Noise	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<u>See Section 5.4.1</u>
2. Construction	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<u>See Section 5.4.2</u>

* S = Significant; NS = Not Significant; N = None; NI = No Involvement.

Basis of decision will be a reference to the summary following this checklist that is included in this *Project Development Summary Report (PDSR)*.

Prepared By: Date: 6-27-11

Reviewed By: _____ Date: _____

Signature: Date: 6/28/11

District Environmental Administrator

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Planning Requirements for Environmental Document Approvals with Segmented Implementation

Document Information:
 Date: 10/5/2011 Document Type: CE II Document Status: Final
 Project Name: SR 60 (Courtney Campbell Causeway) Multi-Use Trail WPI Segment #: 422640-2
 Project Limits: Bayshore Blvd. to west of the Ben T. Davis Beach Entrance ETDM #: 13102
 Are the limits consistent with the plans? Y/N (Limits presented for approval should be consistent with LRTP, TIP/STIP. If no, explain)
 Identify MPO(s) (if applicable): Pinellas & Hillsborough Original PD&E FAP# 9045-090-C

Segment Information: Segment 1
 Segment Limits: Bayshore Blvd. to east of Structure No. 1 WPI Segment #: 424561-3

Currently Adopted CFP-LRTP	COMMENTS				
Y	Pinellas County 2035 LRTP Amended September 14, 2011				
PHASE	Currently Approved TIP	Currently Approved STIP	TIP/STIP \$	TIP/STIP FY	COMMENTS
PE (Final Design)	Y	Y	\$ 349,000	2012	
R/W	n/a	n/a	n/a	n/a	Right of way not required
Construction	Y	Y	\$ 4,331,746	2016	

Segment Information: Segment 2
 Segment Limits: East of Structure No. 1 to Pinellas/Hillsborough County Line WPI Segment #: 424561-4

Currently Adopted CFP-LRTP	COMMENTS				
Y	Pinellas County 2035 LRTP Amended September 14, 2011				
PHASE	Currently Approved TIP	Currently Approved STIP	TIP/STIP \$	TIP/STIP FY	COMMENTS
PE (Final Design)	Y	Y	\$ 190,000	2012	
R/W	n/a	n/a	n/a	n/a	Right of way not required
Construction	Y	Y	\$ 722,912	2014	

Segment Information: Segment 3
 Segment Limits: Pinellas/Hillsborough Co. Line to W of Ben T Davis Beach Ent. WPI Segment #: 424561-5

Currently Adopted CFP-LRTP	COMMENTS				
Y	Hillsborough County 2035 LRTP Amended September 6, 2011 (project ORT 350, Appendix B, Table B-1, Page 4)				
PHASE	Currently Approved TIP	Currently Approved STIP	TIP/STIP \$	TIP/STIP FY	COMMENTS
PE (Final Design)	Y	Y	\$ 124,000	2012	Programmed as design-build project
R/W	n/a	n/a	n/a	n/a	Right of way not required
Construction	Y	Y	\$ 13,924,789	2012	Programmed as design-build project

FDOT Preparer's Name: Robin Rhinesmith
 Preparer's Signature: 

Date: 10/6/11 Phone #: 813-975-6496

Email: Robin.Rhinesmith@dot.state.fl.us

*Attach: LRTP, TIP, STIP pages

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Table 61. Trailways Projects with Funds Committed Prior to 2016

TIP#	Project	Length (miles)	Funding Amount (in YOY dollars)					Funding Source
			2011/2012	2012/2013	2013/2014	2014/2015	2015/2016	
920522	Northeast Extension Keystone Road Segment (includes cost of roadway widening)	2.27	14,741,630	11,900,000	4,650,000			PP, TIF
922499	Progress Energy B (US 19 to SR 590)	2.4		1,998,900	1,075,500			PP, TIF
1288	Downtown Connection Trail Overpass at 34th St.	0.15	500,000					TIF
6010	Druid Trail (Pinellas Trail to Glen Oaks Park)	1.5	1,250,000					LF
4137011	Bayway Trail North (34th St. to Toll Plaza)	1.14		290,000			1,097,000	SE
4157382	Oldsmar Trail - 1 of 5 (Forest Rd. @ Pine Ave. N. to Forest Lakes Blvd. @ Racetrack Rd.)	13.31		579,300				SE
4157383	Oldsmar Trail - 2 of 5 (Tampa Rd. to R.E. Olds Park)			347,200				SE
4157384	Oldsmar Trail - 3 of 5 (R.E. Olds Park to Forest Lakes Blvd.)				478,300			SE
4157385	Oldsmar Trail - 4 of 5 (Northside RE Olds Park to Harbor Palms Nature Park)			838,200				SE
4157386	Oldsmar Trail - 5 of 5 (Sheffield Park to Curlew Rd.)				282,900			SE
4245613	SR 60 (Courtney Campbell Causeway Trail) (Bayshore Blvd. to E. of Tampa Bay Bridge)	1.76	349,000				4,331,746	SE/DIH
4245614	SR 60 (Courtney Campbell Causeway Trail) (E of Bridge #138 to Pinellas/Hills Co Line)	1.739	190,000		722,912			SE

ARRA = American Recovery and Reinvestment Act; PP = Penny for Pinellas; HPP = SAFETEA-LU Earmark; SE = Enhancement Program; TIF = Transportation Impact Funds; LF = Local Funds, DIH = District In-House Design Funding

PINELLAS

Five Year Tentative Work Program FY July 1, 2011 Through June 30, 2016 District Seven Florida Department of Transportation

Pinellas TIP

April 8, 2011

Facility	Item No.	Phase	Fund	2011/12	2012/13	2013/14	2014/15	2015/16
SR 595 (SEMINOLE BL) FROM N OF 1ST ST DESCRIPTION: RAILROAD CROSSING PROJECT LENGTH: .127MI NGCN:626654P	FPN: 4274971 TO S OF EAST BAY DR	CST	DIH	42,280	0	0	0	0
		CST	DS	273,125	0	0	0	0
Total:				315,405	0	0	0	0
SR 60 (CCAMPBLL CWY) FROM BAYSHORE BLVD DESCRIPTION: BIKE PATH/TRAIL PROJECT LENGTH: 1.760MI INSTALL A MULTI-USE PATH	FPN: 4245613 TO E OF TAMPA BAY BRIDGE	CST	DIH	0	0	0	0	380,504
		CST	SE	0	0	0	0	3,951,242
		PE	SE	349,000	0	0	0	0
Total:				349,000	0	0	0	4,331,746
SR 60 (CCAMPBLL CWY) FROM E OF BRIDGE #138 DESCRIPTION: BIKE PATH/TRAIL PROJECT LENGTH: 1.739MI SHARED USE TRAIL	FPN: 4245614 TO PINELLAS/HILLS CO/L	CST	SE	0	0	722,912	0	0
		PE	SE	190,000	0	0	0	0
Total:				190,000	0	722,912	0	0

PDE-Project Development Environment; PE-Preliminary Engineering; ROW-Right of Way; CST-Construction; RRU-Rail Road/Utilities; INC-Incentive; ENV-Environmental Mitigation; CAP-Capital Grant; OPS-Operations Grant; LAR-Local Advance Reimbursement; DSB-Design Build; ADM-Administration



HILLSBOROUGH

Five Year Tentative Work Program
 FY July 1, 2011 Through June 30, 2016
 District Seven Florida Department of Transportation

Hillsborough TIP

April 8, 2011

Facility	Item No.	Phase	Fund	2011/12	2012/13	2013/14	2014/15	2015/16
→ SR 60(CCAMPBELL CWY) FROM PINELLAS/HILLS CO/L DESCRIPTION: BIKE PATH/TRAIL PROJECT LENGTH: 4.332MI SHARED USE TRAIL	FPN: 4245615 TO BEN T DAVIS BEACH ENT	DSB PE	SE SE	13,924,789 124,000	0 0	0 0	0 0	0 0
L RTP Reference: ORT350				Total:	14,048,789	0	0	0
SR 616 FROM E OF OBRIEN ST DESCRIPTION: RESURFACING PROJECT LENGTH: 1.731MI 6 LANES	FPN: 4230881 TO DALE MABRY HWY	CST CST PE	DIH DS DS	0 0 1,023,000	0 0 0	664,826 6,983,043 0	0 0 0	0 0 0
L RTP Reference: SWG20				Total:	1,023,000	0	7,647,869	0
SR 616 SPRUCE/BOYSCOUT/COLUMBUS DESCRIPTION: SIDEWALK	FPN: 4242101 N OBRIEN ST TO DALE MABRY	CST PE	SU SU	0 0	0 53,403	0 0	342,260 0	0 0
L RTP Reference: SWG20				Total:	0	53,403	0	342,260

PDE-Project Development Environment; PE-Preliminary Engineering; ROW-Right of Way; CST-Construction; RRU-Rail Road/Utilities; INC-Incentive; ENV-Environmental Mitigation; CAP-Capital Grant; OPS-Operations Grant; LAR-Local Advance Reimbursement; DSB-Design Build; ADM-Administration



STIP page
Pinellas County
segments

FLORIDA DEPARTMENT OF TRANSPORTATION
STATE TRANSPORTATION IMPROVEMENT PROGRAM
FISCAL YEAR 2012

HIGHWAYS

07/12/2011 14.03.29
07/01/2011 17.24.14
GEOGRAPHIC DISTRICT 07
ADOPTED PLAN

ITEM NO	DESCRIPTION	TYPE OF WORK	EXIST/IMPROVE/ADD (LANES)	FISCALYR	FUND	PRELIMINARY ENGINEERING	RIGHT-OF-WAY	RAILROADS & UTILITIES	CONSTRUCTION	GRANTS & MISC.
4230833	SR 688/WALSINGHAM RD /ULMERTON RD FROM SR 699 TO 119TH ST N	SIGNING/PAVEMENT MARKINGS		0	DIH	0]	0]	0]	918]	0]
4242621	I-275 SKYWAY ROOF REPLACEMENT AT NORTH AND SOUTH REST AREAS	REST AREA		4	0	0]	0]	0]	908]	0]
4243981	TANGERINE AVE FROM E OF 53RD ST S TO W OF 51ST ST S	SIDEWALK		0	EB	0]	0]	0]	34,677]	0]
4245326	CITY OF ST PETE PED CROSSG ENHANCEMENT 64 CROSSWALKS	TRAFFIC CONTROL DEVICES/SYSTEM		0	SE	32,000]	0]	0]	0]	0]
4245613	SR 60 (CCAMPBLL CWY) FROM BAYSHORE BLVD TO E OF TAMPA BAY BRIDGE	BIKE PATH/TRAIL		2	SE	349,000]	0]	0]	0]	0]
4245614	SR 60 (CCAMPBLL CWY) FROM E OF BRIDGE #138 TO PINELLAS/HILLS CO/L	BIKE PATH/TRAIL		4	SE	190,000]	0]	0]	0]	0]
4245631	SR 580/584 W OF INTERSECT SR 580/584 TO E OF ST PETE DR E	RESURFACING		8	DIH	0]	0]	0]	178,271]	0]



STIP page
Pinellas County
segment

FLORIDA DEPARTMENT OF TRANSPORTATION
STATE TRANSPORTATION IMPROVEMENT PROGRAM
FISCAL YEAR 2014

HIGHWAYS

07/12/2011 14.03.29
07/01/2011 17.24.14
GEOGRAPHIC DISTRICT 07
ADOPTED PLAN

ITEM NO	DESCRIPTION	PROJ LGTH	FEDERAL AID NUMBER	TYPE OF WORK	EXIST/IMPROVE/ADD (LANES)	FISCALYR FUND	PRELIMINARY ENGINEERING	RIGHT-OF-WAY	RAILROADS & UTILITIES	CONSTRUCTION	GRANTS & MISC.
4230861	BRYAN DAIRY/118TH AV										
	FROM 28TH ST TO ALT US 19										
PINELLAS				ITS COMMUNICATION SYSTEM							
15000260	7.380 MI	6		LFP						1,535,850]	0]
				TRIP						1,535,850]	0]
	** ITEM TOTALS **									3,071,700]	0]
4234741	I-275 (SR 93)										
	SUNSHINE SKYWAY BRIDGE #150189										
PINELLAS				BRIDGE-REPAIR/REHABILITATION							
15170001	4.143 MI	4					300,000]				0]
	** ITEM TOTALS **										0]
4240111	SR 693/66TH/PASADENA										
	FROM US 19 (SR55) TO SR 699 (GULF BLVD)										
PINELLAS				ITS COMMUNICATION SYSTEM							
15061000	11.606 MI	6		LFP						2,933,250]	0]
				TRIP						2,933,250]	0]
	** ITEM TOTALS **									5,866,500]	0]
4240121	SR 694 (CR 694)										
	FROM SR 687 (4TH ST) TO SR 699 (GULF BLVD)										
PINELLAS				ITS COMMUNICATION SYSTEM							
15240000	11.221 MI	6		LFP						2,155,950]	0]
				TRIP						2,155,950]	0]
	** ITEM TOTALS **									4,311,900]	0]
4245326	CITY OF ST PETE										
	PED CROSSG ENHANCEMENT 64 CROSSWALKS										
PINELLAS				TRAFFIC CONTROL DEVICES/SYSTEM							
	.000	0									
	** ITEM TOTALS **									827,900]	0]
4245614	SR 60 (CCAMPBLL CWY)										
	FROM E OF BRIDGE #138 TO PINELLAS/HILLS CO/L										
PINELLAS				BIKE PATH/TRAIL							
15040000	1.739 MI	4								722,912]	0]
	** ITEM TOTALS **										0]



STIP Amendment - Hillsborough Segment

The preparation of this report has been financed in part through grant[s] from the Federal Highway Administration and Federal Transit Administration, U.S. Department of Transportation, under the State Planning and Research Program, Section 505 [or Metropolitan Planning Program, Section 104(f)] of Title 23, U.S. Code.

The contents of this report do not necessarily reflect the official views or policy of the U.S. Department of Transportation.

Transportation Improvement Program Amendment
 FY2010/11 - 2014 /15
 ** This STIP is in an MPO Area **

STIP Amendment Number:11-16
 TIP Page Number: 161-27

On Tuesday, May 03, 2011, the Hillsborough MPO Metropolitan Planning Organization amended the Transportation Improvement Program that was developed and adopted in compliance with Title 23 and Title 49 in a continuing, cooperative and comprehensive transportation planning process as a condition to the receipt of federal assistance.

The amendment does not adversely impact the air quality conformity or financial constraints of the STIP.

The STIP Amendment is consistent with the Adopted Long Range Transportation Plan. (Page Number:Policy 6.2B)

This document was electronically signed
 5/4/2011
 Joe Zambito
 Hillsborough MPO

This document was electronically signed
 5/9/2011
 Janille Smith-Colin
 FI DOT

Metropolitan Planning Organization Chairman or Designee

FDOT District Representative or Designee District 07

This document was electronically signed
 5/11/2011
 Richard Luten
 FDOT Federal Aid

This document was electronically signed
 5/27/2011
 Carl Mikyska
 FHWA

Federal Aid Management Manager or Designee

Federal Highway Authorization

STIP amendment criteria:

A - The change adds new individual projects to the current STIP

An air conformity determination must be made by the MPO on amended projects within the non-attainment or maintenance areas

E - The MPO is not in an air quality non-attainment or maintenance area.

Project Name 424561-5 SR60 (CC Campbell Cwy) from Pinellas/Hills Co/L to Benty Davis Bch

Status	ITEM	Ver	Description	Fund	Phase	FY 2011	FY 2012	FY 2013	FY 2014	FY 2015
Original STIP						0.00	0.00	0.00	0.00	0.00
Proposed Project	424561 5	G1	SR 60(CCAMPBELL CWY) FROM PINELLAS/HILLS CO/L TO BEN T DAVIS BEACH ENT			0.00	13,924,789.00	0.00	0.00	0.00
				SE	DSB	0.00	13,924,789.00	0.00	0.00	0.00
				SE	PE	0.00	124,000.00	0.00	0.00	0.00
Funding Source After Change						0.00	0.00	0.00	0.00	0.00
Funding Source Balance Before Change										
Funding Source Balance After Change										
Net Change to Funding Source										
Proposed Project Before Change										
Proposed Project After Change							14,048,789.00			
Net Change to Project							14,048,789.00			
Net Change to Funding Source										
Net Change to Proposed Project							14,048,789.00			
Net Change to STIP							14,048,789.00			

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FHWA Reviewer: _____

Date: _____

Planning Requirements Summary (FHWA Planners complete): Planner: Date:	YES	NO	Comments
1. Is project fully reflected in current cost-feasible LRTP?			
2. Is project in current TIP?			
3. Is project in current STIP and consistent with the TIP?			
4. Is the project described in the TIP and STIP consistent with the cost-feasible LRTP with regards to project description, limits, implementation and funding? If NO, describe outcome of conversation with District to produce consistency.			
5. Are the cost-feasible LRTP, TIP, and STIP consistent with the project implementation as demonstrated in the project schedule? If NO, describe outcome of conversation with District to produce consistency.			
6. Is the environmental document consistent with the project implementation as demonstrated in the project schedule? If NO, describe outcome of conversation with District to produce consistency.			

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PROFESSIONAL ENGINEER CERTIFICATE

I hereby certify that I am a registered professional engineer in the State of Florida practicing with American Consulting Engineers of Florida, LLC, a Florida Limited Liability Company, authorized to operate as an engineering business, Certificate of Authorization No. 9302, by the State of Florida Department of Professional Regulation, and that I have prepared or approved the evaluation, findings, opinions, conclusions, or technical advice hereby reported for:

Financial Project ID: 422640-2-22-01
FAP Project Number: 9045-090-C
ETDM Project No.: 13102
Project: SR 60 (Courtney Campbell Causeway) Multi-Use Trail
Project Development & Environment (PD&E) Study
from Bayshore Boulevard
to west of the Ben T. Davis Beach entrance
Counties: Pinellas and Hillsborough
FDOT Project Manager: Robin Rhinesmith

I acknowledge that the procedures and references used to develop the results contained in this report are standard to the professional practice of transportation engineering as applied through professional judgment and experience.

SIGNATURE: _____

NAME:

Jeffrey S. Novotny

FIRM:

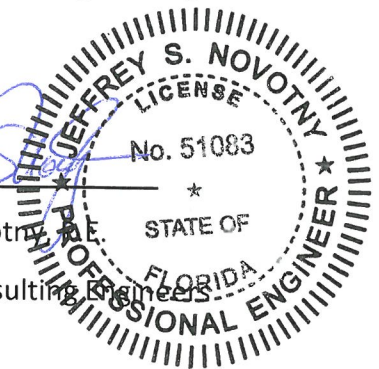
American Consulting Engineers
of Florida, LLC

P.E. No.:

51083

DATE: _____

9-15-2011



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EXECUTIVE SUMMARY

The Florida Department of Transportation (FDOT), District Seven, conducted a Project Development and Environment (PD&E) study to evaluate a proposed multi-use trail along approximately 7.4 miles of State Road (SR) 60 (Courtney Campbell Causeway “Causeway”) (“SR 60”). The limits of the study are from Bayshore Boulevard in Pinellas County to west of the Ben T. Davis Beach entrance in Hillsborough County, Florida (**Figure 1-1** in **Section 1**).

SR 60 is an existing 4-lane divided highway that includes two bridge crossings over Old Tampa Bay to allow watercraft access to and from the north/south sides of SR 60 as well as maintain tidal flow. In 2005, the Causeway was designated as an official scenic highway by the state of Florida. The proposed improvements consist of constructing a multi-use trail along the SR 60 causeway south of the existing roadway. Constructing the proposed multi-use trail is consistent with the goals and objectives of the Corridor Management Plan established by the Courtney Campbell Scenic Highway Committee.

The proposed multi-use trail will serve as a link in a regional network of trail systems serving the Tampa Bay region. This project is consistent with the Comprehensive Plans of the following jurisdictions: Hillsborough County; Pinellas County; City of Tampa; and the City of Clearwater. The trail has also been identified in the City of Tampa *Greenways & Trails Master Plan* (2001), the City of Clearwater *Bikeways and Trails Plan* (1996) and *Shifting Gears: Clearwater’s Bicycle and Pedestrian Master Plan* (2007). Design and construction phases of this project are currently funded for in the FDOT’s *Tentative Work Program* for fiscal years (FY) 2012-2016.

This project is included in the following local plans (copies of the referenced pages are included at the front of this report):

- Pinellas County Metropolitan Planning Organization (MPO) 2035 Long Range Transportation Plan (LRTP), Table 61 – Segments 1 & 2
- Hillsborough County MPO 2035 LRTP, Appendix B-1, page 4, Project ID #ORT350 – Segment 3
- Pinellas County MPO Transportation Improvement Program (TIP) FY 2011/12–2015/16, page 16 – Segments 1 & 2
- Hillsborough County MPO TIP FY 2011/12–2015/16, page 161-27 – Segment 3
- FDOT Statewide Transportation Improvement Program (STIP) FY 2010/11 to 2014/15, amendment number 11-16 – Segment 3. STIP for segments 1 & 2 are pending FHWA approval of STIP (anticipated October 1, 2011).

As part of the Department’s Efficient Transportation Decision Making (ETDM) process, a Programming Screen Summary Report was published on March 29, 2011 (re-published on June 16, 2011) under ETDM #13102. The Federal Highway Administration has determined that the project qualifies as a Type 2 Categorical Exclusion.

The objective of this *Project Development Summary Report (PDSR)* is to present the key findings of the PD&E study and describe the selected Recommended Alternative along with its effects on the socioeconomic, cultural and natural environment. This *PDSR* is divided into seven sections. A brief summary of each section is provided below.

Section 1 - Introduction - explains the study purpose and need, the PD&E study process, and the Efficient Transportation Decision Making (ETDM) process; it also briefly summarizes the scope of the proposed improvements.

Section 2 - Commitments and Recommendations - lists the recommendations and commitments developed throughout the PD&E study. The commitments section lists environmental commitments that will be adhered to during the final Design/Construction phases. The FDOT is committed to the following measures to address potential impacts to the natural and physical environment for this project:

- To assure the protection of wildlife during construction, the FDOT will implement a wildlife watch plan, which includes the FFWCC "*Standard Manatee Conditions for In-Water Work*". The FDOT will require the construction contractor to abide by these guidelines during construction. The Wetland Evaluation and Biological Assessment Report (WEBAR), Appendix E provides an example of the most current "*Standard Manatee Conditions for In-Water Work*" (2011),
- Per coordination with USFWS, special conditions for manatees will need to be addressed during construction and include the following: no nighttime in-water work, dedicated manatee observers, fenders between work barges to prevent crushing, and proper siltation or exclusions barriers that will not entrap manatees in the work site.
- The FDOT will adhere to the NMFS's *Sea Turtle and Smalltooth Sawfish Construction Conditions* (contained in WEBAR Appendix E) during construction of the project. FDOT will initiate Section 7 consultation with NMFS on sea turtles and smalltooth sawfish during final design,
- The FDOT will commit to watching for Gulf Sturgeon during construction of the proposed bridges. FDOT will incorporate the Construction Special Conditions for the protection of the Gulf Sturgeon. These can be found in the WEBAR, Appendix E,
- The FDOT will conduct bald eagle nest surveys prior to construction of the proposed trail project. The FDOT will adhere to the *USFWS Bald Eagle Monitoring Guidelines* if bald eagle nest are identified within the project area, and
- The FDOT will coordinate with the appropriate regulatory and permitting agencies during the design phase of the project. Permits will be obtained prior to commencement of construction and the contractor will adhere to all conditions set forth in the permits.

Section 3 - Alternatives Considered - describes the No-Build and Build Alternatives considered. Alternative trail typical sections are shown in **Figure 3-1**. In addition, trail bridge alternatives evaluated in the 2008 Feasibility Study are shown in **Figure 3-2**. Trail alternatives were considered on both the north and south sides of the Causeway.

Section 4 - Preferred Alternative - describes the Preferred (Build) Alternative relative to engineering requirements for geometric design, utilities, and other engineering and environmental factors. **Figures 4-1A** and **4-1B** provide the roadway/trail typical sections and **Figure 4-2** provides the typical sections for the bridges. These typical sections are also shown on **Figure ES-1**. Preliminary Concept Plans are provided in **Appendix D**. No design exceptions and variations are anticipated. The FDOT Work Program schedule and estimated costs are also included. The current (preliminary) cost estimate includes:

Table ES-1 Preferred Alternative Project Costs

Cost Category	Total Project Costs ¹ (\$'s)
Construction Costs	
Roadway and Bridges ²	\$ 17,600,000
Stormwater Facilities	\$ 0.0
Total Construction Costs	\$ 17,600,000
Engineering Design ³	\$ 2,100,000
Construction Engineering & Inspection ³	\$ 2,100,000
Wetlands Mitigation	TBD
Preliminary Estimate of Total Costs	\$ 21,800,000

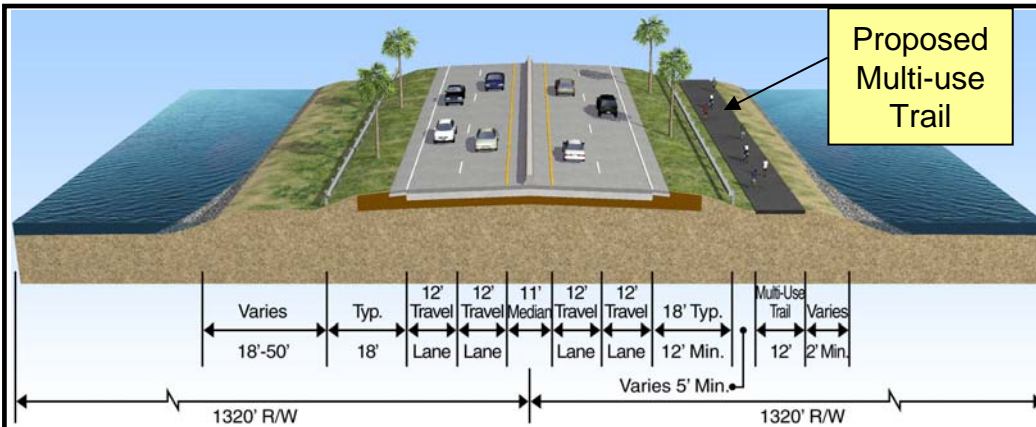
Notes: ¹ Present day costs rounded to nearest \$100,000

² Based on January 2011 Work Program Estimates

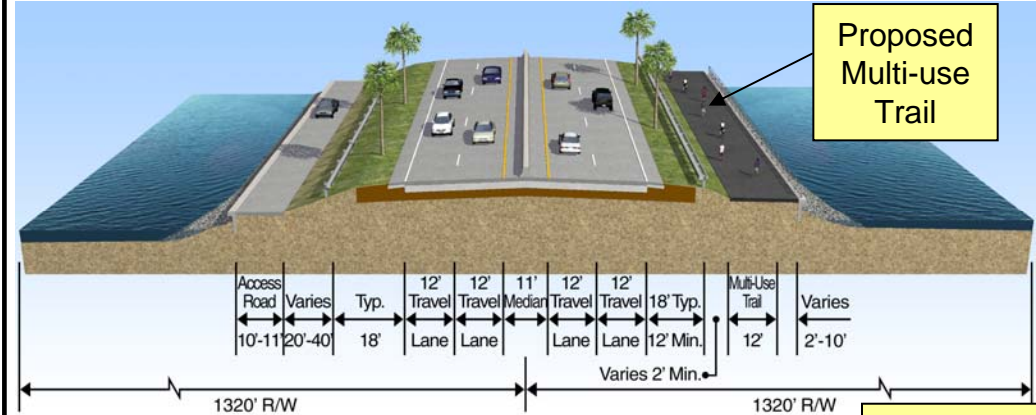
³ Estimated at 12% of construction costs

Section 5 - Summary and Environmental Effects - summarizes the results of the environmental screening conducted during the ETDM process (ETDM Project No. 13102) and the expected environmental effects related to construction of the proposed project.

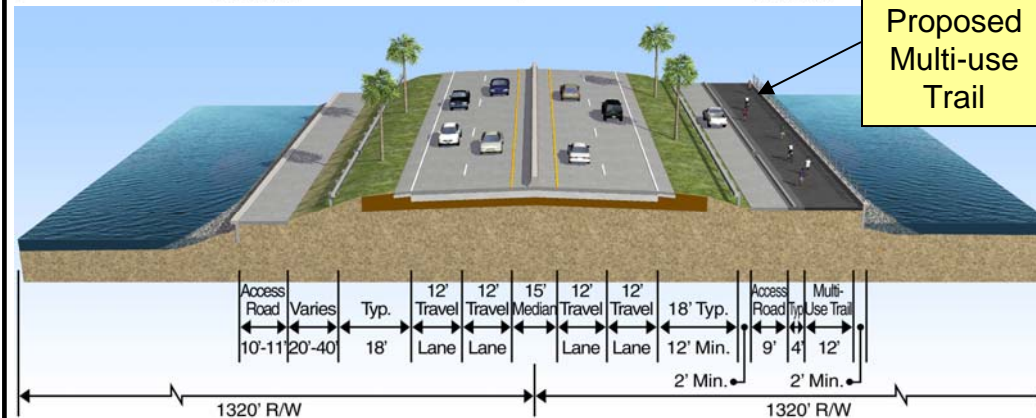
- In accordance with Part 2, Chapter 12 of the *PD&E Manual*, a *Cultural Resource Assessment Survey (CRAS)* was completed. No resources were found which are on or considered eligible for inclusion on the *National Register of Historic Places*. The CRAS was forwarded by FDOT to FHWA on February 2, 2011 and FHWA forwarded the CRAS to the Florida Department of State, Division of Historical Resources (SHPO) on March 1, 2011. SHPO provided a request for additional information on March 15, 2011 and determined in a letter dated April 27, that the proposed project will have no adverse effect on any resources on or eligible for inclusion in the NRHP. All letters are included in **Appendix B**.



Typical Section No. 1
Station 21+00 to Station 69+00



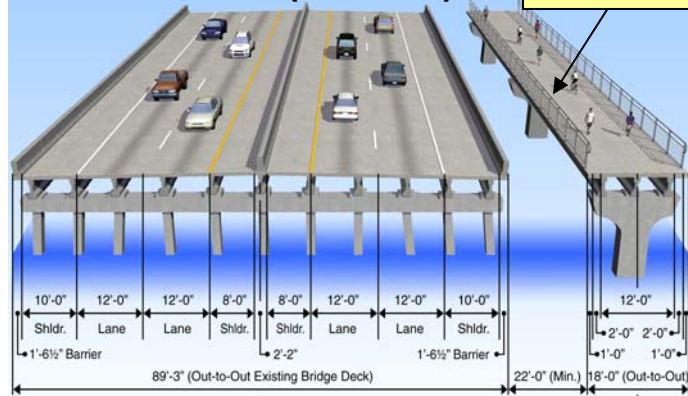
Typical Section No. 2
Station 69+00 to Station 106+00
Station 111+00 to Station 256+50
Station 394+00 to Station 412+00



Typical Section No. 3
Station 256+50 to Station 265+00
Station 300+00 to Station 394+00

Bridge Typical Sections

Structure No. 1 (#150138)



Structure No. 2 (#100301)



- In accordance with Part 2, Chapters 18, 27 and 11 of the *PD&E Manual*, a *Wetland Evaluation and Biological Assessment Report (WEBAR)* was prepared for the proposed project. It addressed any impacts to wetlands, threatened and endangered species and it included an Essential Fish Habitat assessment. The WEBAR was provided to the National Marine Fisheries Service (NMFS) and the United States Fish and Wildlife Service (USFWS) on February 25, 2011.

Coordination with NMFS was conducted to address the potential impacts to EFH within the project corridor. In a letter dated March 9, 2011, NMFS provided suggested construction conditions be implemented regarding Sea Turtle and Smalltooth Sawfish protection. Additionally, NMFS indicated that mitigation would be coordinated after a seagrass survey is performed during the prime seagrass growing season (late May-September). In response to this comment, FDOT conducted seagrass surveys in June 2011. The results of the updated seagrass survey were provided to NMFS and they provided an email concurring with the results of the WEBAR on June 20, 2011.

Coordination with the USFWS was conducted pertaining to threatened and endangered species. A response letter was received on March 24, 2011 providing concurrence with degree of effect for sea turtles. USFWS indicated seagrass surveys would need to be conducted during the growing season (late May – September) due to potential impacts to the West Indian manatee. USFWS recommended incorporation of the Construction Special Provisions for the protection of the Gulf Sturgeon. The response letter also stated that concurrence for the “No Effect” determination for the wood stork could not be provided if wetland impacts occur. USFWS stated that the Courtney Campbell Causeway is an important area for shorebirds and other migratory birds. The placement of the trail too close to the area where the birds are known to gather may result in flushing and disturbance of shorebirds. USFWS requested that distances between the proposed trail and the known shorebird roosting, feeding, and loafing area be provided. Continued coordination was conducted with USFWS throughout the length of the study to address the agency’s comments. FDOT conducted seagrass surveys during the growing season (June 2011). FDOT provided the results of the updated seagrass survey and made an adjustment of the design of the eastern approach to Structure 2 to avoid the seagrass areas determined in the updated survey. USFWS provided a concurrence letter on June 21, 2011

The WEBAR was provided to the Florida Fish and Wildlife Conservation Commission (FFWCC). FFWCC provided a letter dated April 18, 2011 stating concurrence with the findings of the WEBAR although questioning the “No Effect” finding for the wood stork. In their April 18, 2011 letter, FFWCC indicated “We also support the project

commitments to provide mitigation for any wetland impacts, to conduct seagrass surveys during the growing season and provide mitigation for all direct and indirect impacts to seagrass beds, to conduct a pre-construction survey for bald eagle nests within 660 feet of the project, and to develop a wildlife watch plan which includes the FWC (FFWCC)'s Standard Manatee Conditions for In-Water Work.

Copies of correspondence with NMFS, USFWS and FFWCC are included in **Appendix B**.

- Permit conditions and type of permits required, including the review of maps and data in order to determine permit related information for this project. (See Section 6 below).
- In accordance with Part 2, Chapter 22 of the *PD&E Manual*, a *Contamination Screening Evaluation Report (CSER)* was prepared as part of this study. Two potential low risk sites are located adjacent to the project area, but no impacts are anticipated as a result of this project.
- The following resource evaluations were considered “Not Applicable” for this study:
 - Air Quality
 - Farmlands
 - Relocation
 - Noise

Section 6 - Summary of Permits and Mitigation - lists the anticipated permits that will be required for the project. Permits are expected to be required from the following agencies:

- United States Army Corps of Engineers (USACE)
- United States Coast Guard (USCG)
- Southwest Florida Water Management District (SWFWMD)
- Tampa Port Authority (TPA)

An Environmental Resource Permit (ERP) pre-application meeting was held with FDOT staff preparing the design-build criteria package for Segment 3 and SWFWMD on June 23, 2011. The meeting minutes from that meeting are not yet available.

Section 7 - Summary of Public Involvement - summarizes the agency and public involvement activities undertaken to date. These will include the ETDM screening process, Advance Notification, agency meetings, and newsletters.

A public hearing was held at two separate locations on separate days (March 24, 2011 and March 29, 2011) to encourage participation from both Pinellas and Hillsborough County residents and the general public. A total of 79 members of the public attended the two Public Hearing sessions. A total of eight written comment forms were received and six oral comments were made at Session 1 and a seven written comment forms were received and six oral comments made at Session 2. Five official letters of support were received from public groups. Two hundred and fifty-three (253) comments were received via email after both hearing sessions. Most of the comments provided at the hearing sessions expressed support for the project. Some of the comments expressed concern about elimination of fishing on the South side of the Causeway, limiting access to the water and concern for wildlife. Other comments expressed concern about the cost of the project and a perceived wasteful spending to build the project. Of the overall number of comments received, approximately half of them expressed support for the project. Of those expressing concern, a large majority of those provided an opinion that tax dollars should not be spent on this project considering the state of the economy. A *Comments and Coordination Report* was prepared for this study which provides details of the public coordination.

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LIST OF APPENDICES

Appendix A – List of Supporting Documents

Appendix B – Agency Coordination

Appendix C – Public Hearing Transcript

Appendix D – Conceptual Design Plans

LIST OF ACRONYMS

AADT	Annual Average Daily Traffic	LRFD	Load and Factor Resistance Design
AASHTO	American Association of State Highway and Transportation Officials	L RTP	Long Range Transportation Plan
ADA	Americans with Disabilities Act	MP	Mile Post
AN	Advance Notification	MPO	Metropolitan Planning Organization
APE	Area of Potential Effect	MUTCD	Manual of Uniform Traffic Control Devices
AP/OFW	Aquatic Preserve/Outstanding Florida Water	NEPA	National Environmental Policy Act
AWWTP	Advance Waste Water Treatment Plant	NMFS	National Marine Fisheries Service
BMP	Best Management Practices	NRCS	Natural Resources Conservation Service
CE	Categorical Exclusion	NRHP	National Register of Historic Places
CEI	Construction Engineering and Inspection	OFW	Outstanding Florida Water
CMP	Corridor Management Plan	PD&E	Project Development and Environment
CRAS	Cultural Resource Assessment Survey	PDSR	Project Development Summary Report
CSER	Contamination Screening Evaluation Report	PPM	Plans Preparation Manual
EFH	Essential Fish Habitat	PSTA	Pinellas Suncoast Transit Authority
ERP	Environmental Resource Permit	ROW	Right-of-Way
EST	Environmental Screening Tool	SR	State Road
ETAT	Environmental Technical Advisory Team	SHPO	State Historic Preservation Officer
ETDM	Efficient Transportation Decision Making	STA	Station
FAP	Federal Aid Project	STIP	State Transportation Improvement Plan
FDCA	Florida Department of Community Affairs	SWIM	Surface Water Improvement Management
FDEP	Florida Department of Environmental Protection	SWFWMD	Southwest Florida Water Management District
FDOT	Florida Department of Transportation	TIP	Transportation Improvement Plan
FFWCC	Florida Fish and Wildlife Conservation Commission	TPA	Tampa Port Authority
FHWA	Federal Highway Administration	USACE	U.S. Army Corps of Engineers
FS	Florida Statutes	USCG	U.S. Coast Guard
FY	Fiscal Year	USEPA	U.S. Environmental Protection Agency
GIS	Geographical Information Systems	USFWS	U.S. Fish and Wildlife Service
IS	Independent Structure	UST	Underground Storage Tank
LHM	Location Hydraulics Memorandum	WEBAR	Wetland Evaluation and Biological Assessment Report
LOS	Level of Service	WPI	Work Program Identification
LRE	Long Range Estimating		

Section 1 - INTRODUCTION

1.1 Study Purpose and PD&E Process

The objective of the Project Development and Environment (PD&E) study process is to provide the documentation necessary to reach a decision on the type, conceptual design, and location of the proposed improvements identified as being needed. Factors considered include transportation needs, socioeconomic and environmental impacts, and engineering requirements. In general terms, the process involves the following steps:

- (1) the establishment of project need
- (2) the gathering and analysis of detailed information regarding the natural and cultural features of the study area
- (3) the development of a number of alternatives for meeting the project need
- (4) the selection of a Recommended Alternative, and
- (5) documenting the entire process in a series of reports

During the process, communication with the affected public is accomplished directly, through public meetings, and indirectly, through interaction with elected officials and agency representatives.

The Florida Department of Transportation's (FDOT's) Efficient Transportation Decision Making (ETDM) Process provides agencies and the public access to project planning information, as well as potentially affected environmental resources through use of the internet via the Environmental Screening Tool (EST). The EST allows interaction among transportation planners, regulatory agencies and affected communities to provide input on projects. The agency representatives involved in the interaction are referred to as the Environmental Technical Advisory Team, or ETAT members. The team provides a review of the projects on a variety of areas such as environmental and community impacts. Key features of the ETDM Process include:

- early agency and community involvement;
- early identification of avoidance and mitigation strategies;
- access to comprehensive data in standardized formats;
- reviews and studies focused on key issues;
- permit issuance linked to National Environmental Policy Act (NEPA) reviews; and
- maximized use of technology for coordination, project scoping and communication.

The ETDM process provides the opportunity for early agency interaction and coordination during project development, which can improve the quality of decisions and reduce cost and time delays during the PD&E study.

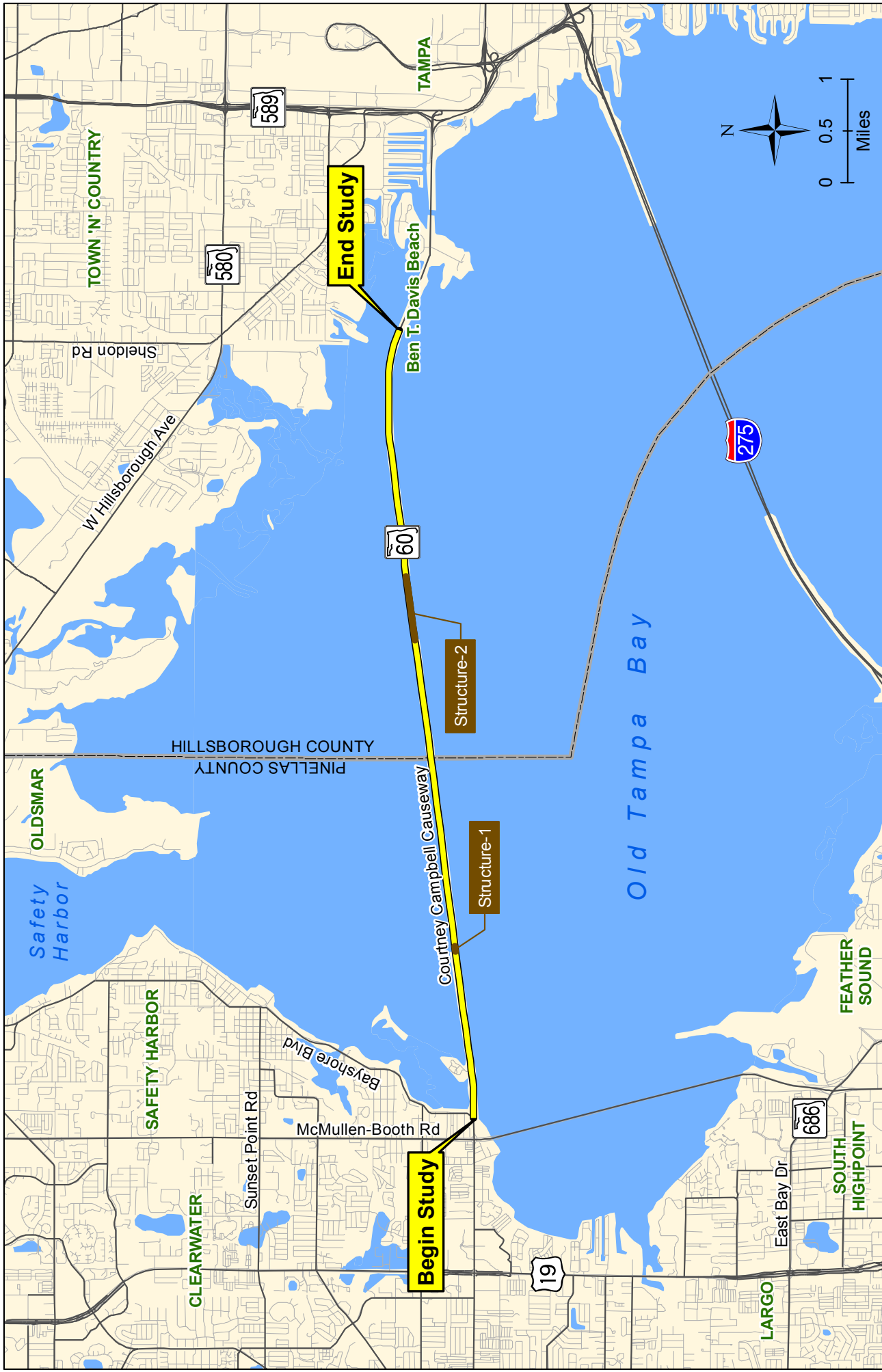
1.2 Project Description and Related Projects

This PD&E study was conducted to evaluate a proposed Multi-Use Trail along State Road (SR) 60 (“SR 60”) (Courtney Campbell Causeway (“Causeway”)) from Bayshore Boulevard in Pinellas County to west of the Ben T. Davis Beach entrance in Hillsborough County, Florida. A project location map is shown in **Figure 1-1**. Prior to this PD&E study, FDOT District Seven conducted a feasibility study which was completed in December 2008. The results of that study were documented in a report entitled *Project Concept Summary Report – Final Report, Feasibility Study, SR 60 (Courtney Campbell Causeway) Multi-Use Trail Feasibility Study from McMullen Booth Road to Veterans Expressway*. This will be referred to as the Feasibility Study Report throughout this report. The limits for the feasibility study were longer than the limits of this PD&E study. There are several other ongoing projects, some of which overlap with the PD&E study. All of these related projects are graphically summarized in **Figure 1-2**. The design and construction for the improvements evaluated under this PD&E study are currently funded in the FDOT’s *Tentative Work Program* for Fiscal Years (FY) 2012-2016. This project is included in the following local plans (copies of the referenced pages are included at the front of this report):

- Pinellas County Metropolitan Planning Organization (MPO) 2035 Long Range Transportation Plan (LRTP), Table 61 – Segments 1 & 2
- Hillsborough County MPO 2035 LRTP, Appendix B-1, page 4, Project ID #ORT350 – Segment 3
- Pinellas County MPO Transportation Improvement Program (TIP) FY 2011/12–2015/16, page 16 – Segments 1 & 2
- Hillsborough County MPO TIP FY 2011/12–2015/16, page 161-27 – Segment 3
- FDOT Statewide Transportation Improvement Program (STIP) FY 2010/11 to 2014/15, amendment number 11-16 – Segment 3. STIP for segments 1 & 2 are pending FHWA approval of STIP (anticipated October 1, 2011).

1.3 Existing Conditions

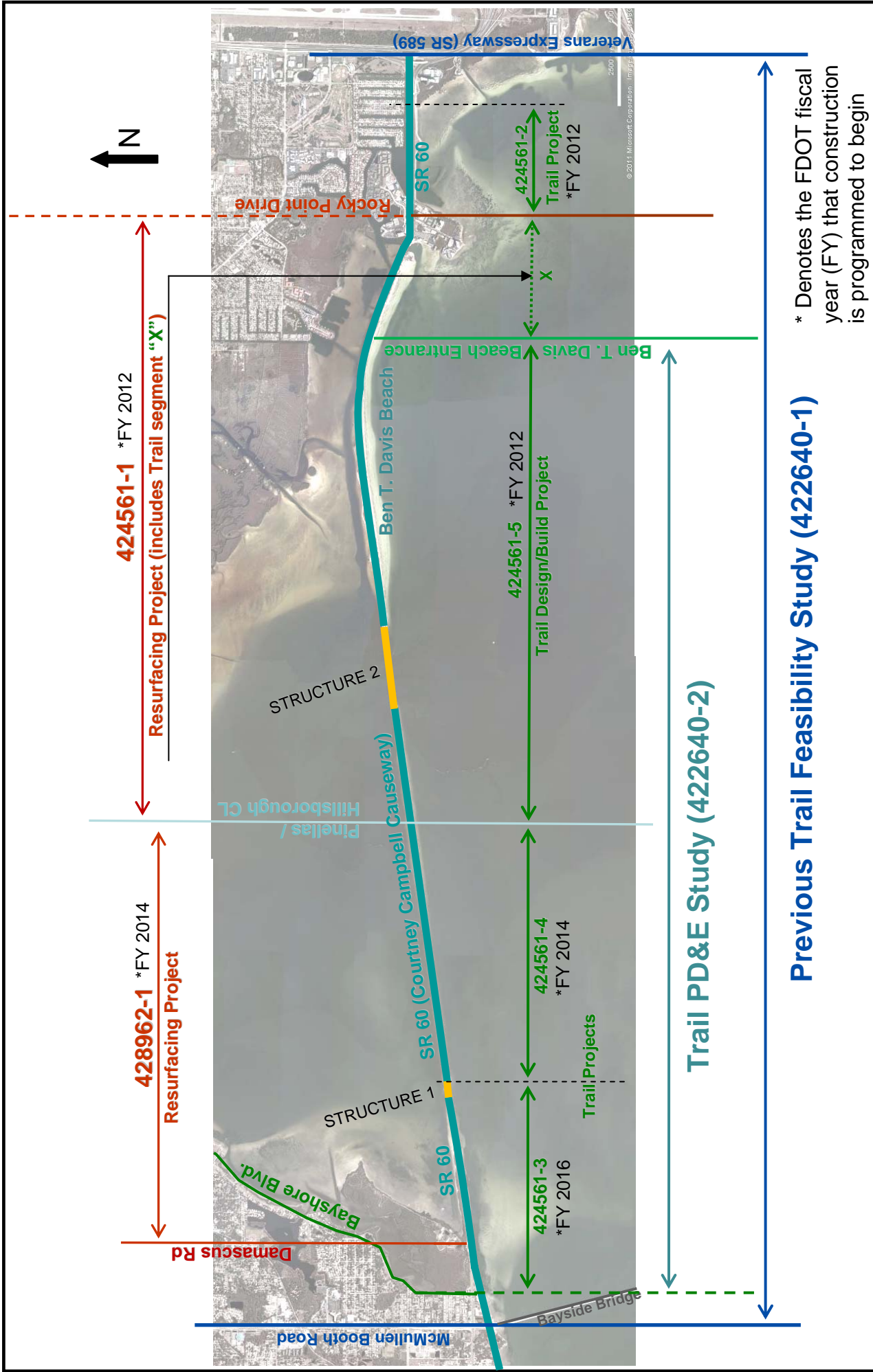
The existing causeway, within the study limits, is approximately 7.4 miles long and is primarily a 4-lane divided rural highway (parkway) that includes two bridges over Old Tampa Bay to allow watercraft connectivity on either side of SR 60 and maintain tidal flow. On May 23, 2005, the Courtney Campbell Causeway was designated as an official scenic highway by the state of Florida. The Causeway presently includes intermittent service roads on both sides of SR 60 which are used to provide access to maintain an existing seawall and to a boat launch along the north side of the Causeway. The existing right of way for



SR 60 Multi-Use Trail PD&E Study
Bayshore Blvd to West of Ben T. Davis Beach Entrance
 WPI Segment No. 422640-2
 Pinellas & Hillsborough Counties

Project Location Map

Figure 1-1

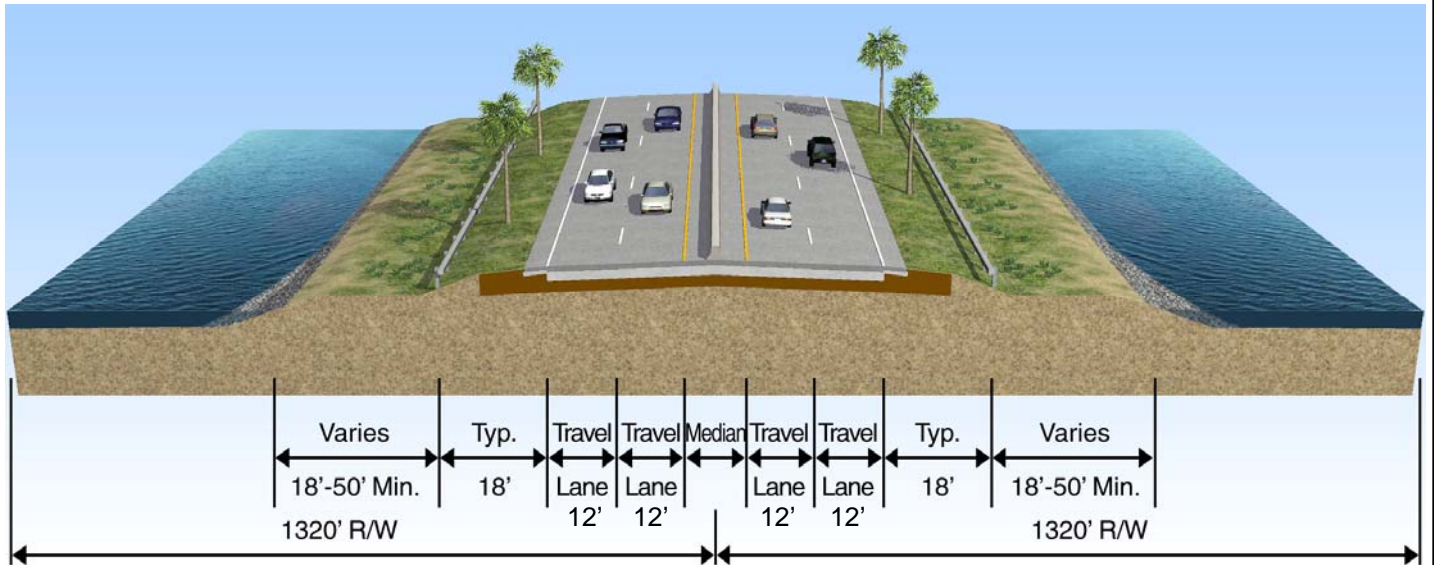


SR 60 Multi-Use Trail PD&E Study
 Bayshore Blvd to West of Ben T. Davis Beach Entrance
 WPI Segment No.: 422640 2
 Pinellas & Hillsborough Counties

Other Related Projects Along SR 60 / Courtney Campbell Causeway

Figure 1-2

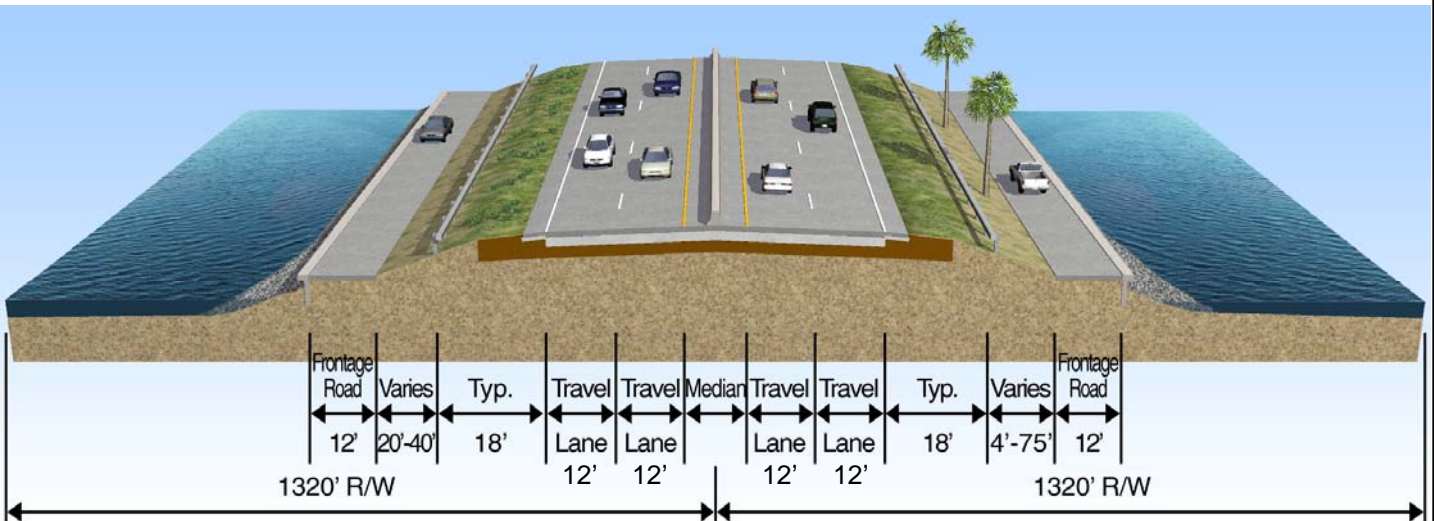
Existing Typical Section No. 1



(Applies from Begin Project to Structure No. 1*)

*Note: Frontage Road and seawall shown in Existing Typical Section 2 below begins at sta 85+00 in eastbound direction, but is no longer accessible to general public

Existing Typical Section No. 2



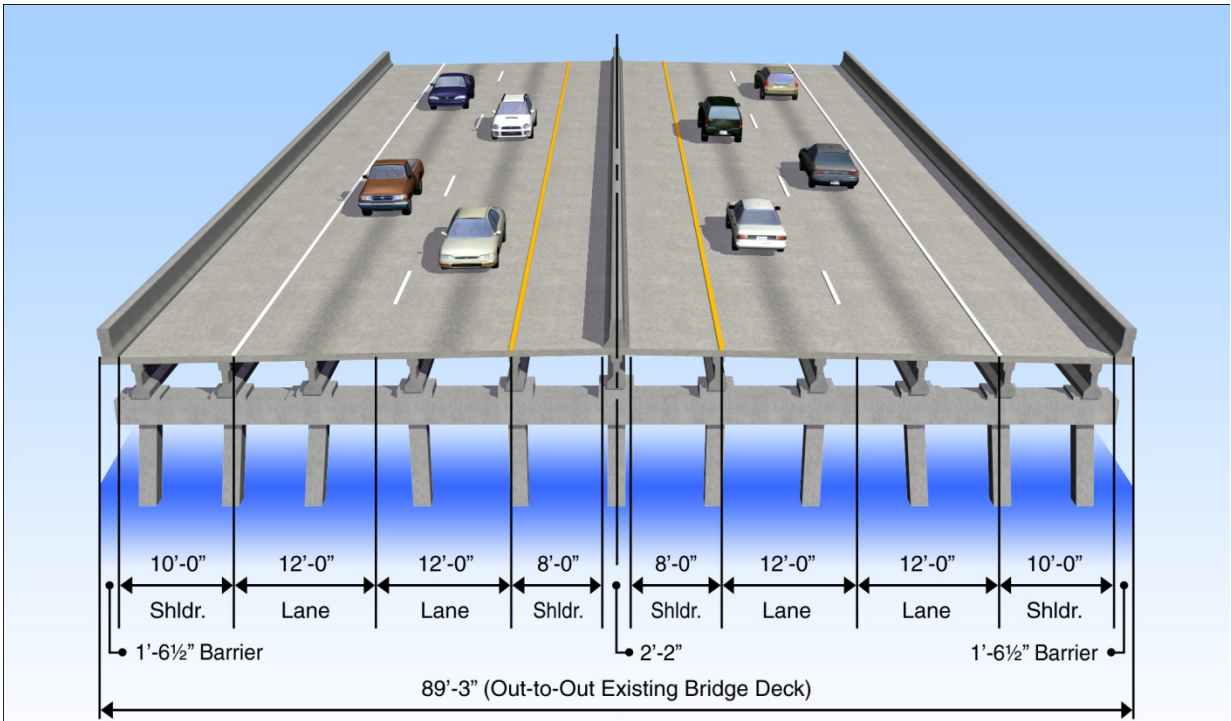
(Applies from Structure No. 1* to End Project)



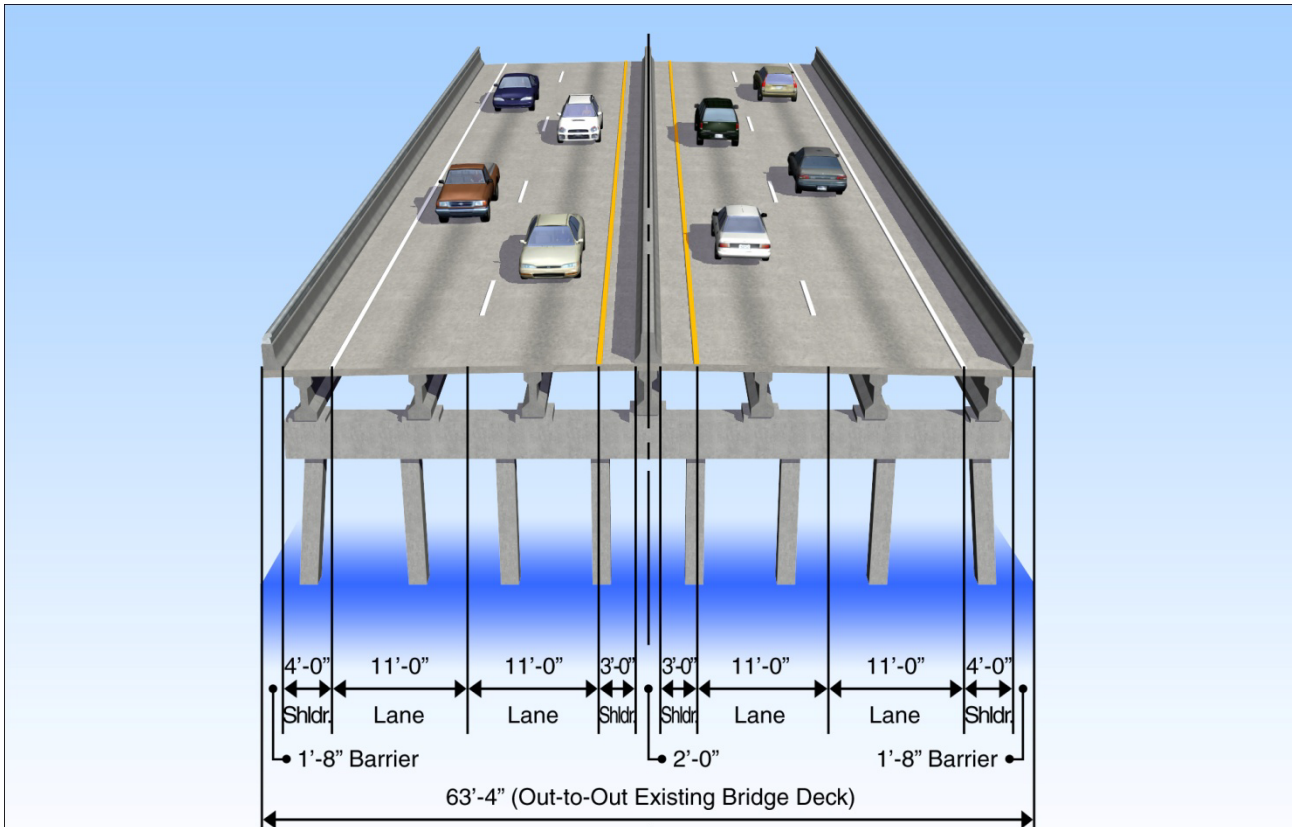
SR 60 Multi-Use Trail PD&E Study
 Bayshore Blvd to West of
 Ben T. Davis Beach Entrance
 WPI Segment No.: 422640 2
 Pinellas & Hillsborough Counties

Existing Roadway
 Typical Sections
 (facing east)

Figure 1-3



Structure No. 1 (#150138)



Structure No. 2 (#100301)



SR 60 Multi-Use Trail PD&E Study
*Bayshore Blvd to West of
 Ben T. Davis Beach Entrance*
 WPI Segment No.: 422640 2
 Pinellas & Hillsborough Counties

**Existing Bridge
 Typical Sections
 (facing east)**

Figure 1-4

transportation purposes is 2,640 feet (0.5 mile) in width along SR 60 including submerged lands. Existing SR 60 roadway typical sections along the causeway are shown in **Figure 1-3**. The two existing bridge typical sections are shown in **Figure 1-4**.

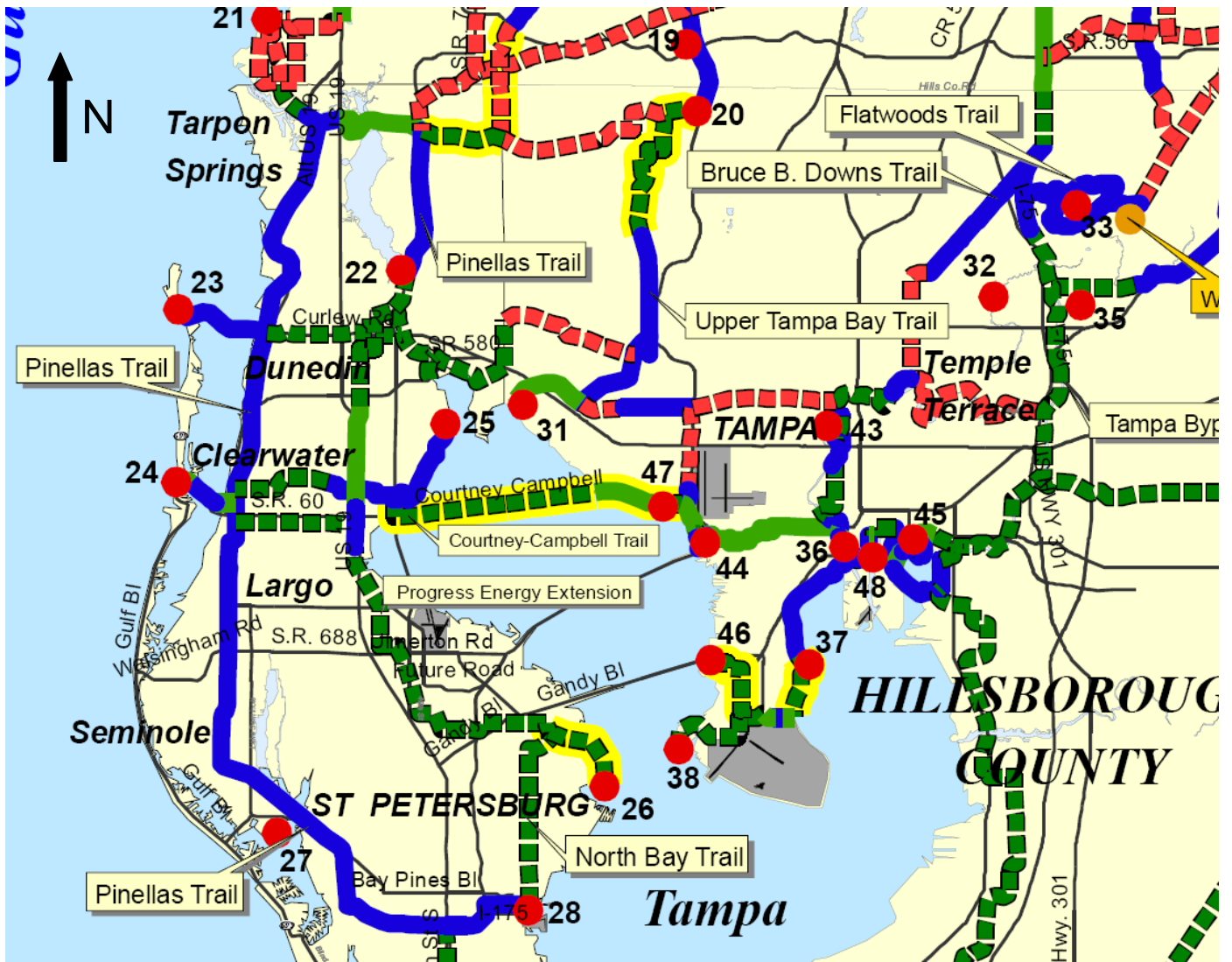
An access road for maintaining and stabilizing an existing seawall along most of the causeway typically consists of a 10 to 11-ft wide one-lane, one-way roadway without defined shoulders. The access roads were constructed between 1978 and 1980 as part of the revetment construction project intended as a permanent erosion control system. From station 85+00 to Structure 1, the seawall and existing access road is on the eastbound side of SR 60, but no longer accessible to the public. Existing vehicle parking is allowed along the corridor at the beach area on the Clearwater end of the study area, at the boat ramp on the east end of Structure 2 and near Ben T. Davis Beach. Undesignated parking areas also exist along the access roads where vehicles are currently allowed.

1.4 Project Purpose and Need

The proposed multi-use trail along SR 60/Courtney Campbell Causeway from Bayshore Boulevard to west of the Ben T. Davis Beach entrance would accommodate recreational users who can experience the scenic qualities of the Causeway, further enhancing tourism and economic development. The proposed multi-use trail has been identified in the Comprehensive Plans of the following jurisdictions: Hillsborough County, Pinellas County, the City of Tampa, and the City of Clearwater. The trail was also identified in the City of Tampa *Greenways & Trails Master Plan* (2001), the City of Clearwater *Bikeways and Trails Plan* (1996) and *Shifting Gears: Clearwater's Bicycle and Pedestrian Master Plan* (2007).

Design and construction phases of this project are currently funded in the FDOT's *Tentative Work Program* for fiscal years (FY) 2012-2016. The proposed trail will serve as a link in a regional network of trail systems serving the Tampa Bay region (**Figure 1-5**). As a needed east-west link, the trail will provide regional connectivity with the trail networks for the jurisdictions noted above across Old Tampa Bay. In providing the east-west link, regional connectivity could be further enhanced offering alternative modes of transportation in the region. The west end of the proposed trail would connect to Clearwater's proposed Bayshore Boulevard Trail, which in turn would connect to numerous other trails in Pinellas County. The east end of the proposed trail would eventually connect to the existing U-Path Trail (**Figure 1-6**) and eventually to additional trails in Hillsborough County.

Beyond the multi-use trail's transportation benefits, the trail could serve the recreational needs for residents in the area and provide linkage to a series of recreational facilities along the Causeway, including boating, fishing and picnicking. It would also recreate a regional recreational opportunity to cross Tampa Bay to link Pinellas and Hillsborough Counties since the existing Friendship Trail Bridge adjacent to Gandy Boulevard was permanently closed due to structural deterioration.



Legend

- Regional Attractors & Trailheads
- Hiking/Bicycle Trail (Alignment Not Shown)

Regional Multi-Use Trails

- Existing Trail
- Planned - Funded Trail
- - - Planned - Unfunded/Partially Funded Trail
- - - Conceptual Trail
- Regional Priorities
- Regional Roadway Network

Note - the Regional Attractor and Trailhead number refers to the table in the Regional Multi-Use Trail Element

West Central Florida MPO
Chairs Coordinating Committee



www.regionaltransportation.org

DATA SOURCES: Regional road base layer was created by compiling each county's major road network. Contact respective MPO for specific information.

ACCURACY: It is intended that the accuracy of the base map comply with US national map accuracy standards. However, such accuracy is not guaranteed. The map is for illustrative purposes only.

Prepared Date: April 14, 2010
Prepared By: Hernando County MPO



SR 60 Multi-Use Trail PD&E Study
Bayshore Blvd to West of Ben T. Davis Beach Entrance
 WPI Segment No.: 422640 2
 Pinellas & Hillsborough Counties

Excerpt From 2010 Regional Multi-Use Trails Map

Figure 1-5



U-Path Trail - Completed July 1, 2010

Connects Cypress Point Park, Courtney Campbell Causeway and Skyway Park

Source: City of Tampa, Parks & Recreation Department, September 2010



SR 60 Multi-Use Trail PD&E Study

*Bayshore Blvd to West of
Ben T. Davis Beach Entrance*

WPI Segment No.: 422640 2
Pinellas & Hillsborough Counties

U-Path Trail

Figure 1-6

A traffic analysis of the study area was performed during the Feasibility Study in 2008 (see **Section 3.2.2** for more information regarding this study). SR 60, along the Courtney Campbell Causeway, is a four-lane, divided highway. Average Annual Daily Traffic (AADT) on the Causeway was approximately 50,500 vehicles per day in 2009, according to the FDOT's *Florida Traffic Information* DVD-ROM. The traffic analysis assumes that no changes will be made to the roadway and that traffic volumes as projected through 2016 are relatively flat for SR 60 across the Causeway. AADT volumes are projected to rise between 1-2 percent on either end and remain constant over the bridges. SR 60 performs at a motor vehicle Level of Service (LOS) "D" for an urbanized, four-lane, divided state highway with uninterrupted flow, based on current traffic volumes. Because the roadway includes paved shoulders along most of its length, there is room for bicyclists in the existing cross-section. Given the traffic characteristics and the roadway geometry, this leads to an existing bicycle LOS "D" based on the FDOT-adopted *Bicycle Level of Service Model*. Pedestrians are not currently accommodated along the roadway, and the *Pedestrian Level of Service Model* indicates a pedestrian LOS "F".

A *Programming Screen Summary Report* was published on March 29, 2011 as part of the FDOT's ETDM process. This project was designated as ETDM Project #13102. The Federal Highway Administration has determined that the project qualifies as a Type 2 Categorical Exclusion.

Section 2 - COMMITMENTS & RECOMMENDATIONS

2.1 Commitments

The FDOT is committed to the following measures to address potential impacts to the natural and physical environment for this project:

- To assure the protection of wildlife during construction, the FDOT will implement a wildlife watch plan, which includes the FFWCC “*Standard Manatee Conditions for In-Water Work*”. The FDOT will require the construction contractor to abide by these guidelines during construction. The Wetland Evaluation and Biological Assessment Report (WEBAR), Appendix E provides an example of the most current “*Standard Manatee Conditions for In-Water Work*” (2011),
- Per coordination with USFWS, special conditions for manatees will need to be addressed during construction and include the following: no nighttime in-water work, dedicated manatee observers, fenders between work barges to prevent crushing, and proper siltation or exclusions barriers that will not entrap manatees in the work site.
- The FDOT will adhere to the NMFS’s *Sea Turtle and Smalltooth Sawfish Construction Conditions* (contained in WEBAR Appendix E) during construction of the project. FDOT will initiate Section 7 consultation with NMFS on sea turtles and smalltooth sawfish during final design,
- The FDOT will commit to watching for Gulf Sturgeon during construction of the proposed bridges. FDOT will incorporate the Construction Special Conditions for the protection of the Gulf Sturgeon. These can be found in the WEBAR, Appendix E,
- The FDOT will conduct bald eagle nest surveys prior to construction of the proposed trail project. The FDOT will adhere to the *USFWS Bald Eagle Monitoring Guidelines* if bald eagle nest are identified within the project area, and
- The FDOT will coordinate with the appropriate regulatory and permitting agencies during the design phase of the project. Permits will be obtained prior to commencement of construction and the contractor will adhere to all conditions set forth in the permits.

2.2 Recommendations

It is recommended that the proposed improvements as described in **Section 4** of this document be approved for advancement to design and construction as funding becomes available.

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Section 3 - ALTERNATIVES CONSIDERED

3.1 The No-Build Alternative

The No-Build Alternative assumes that, with the exception of those improvements that are already planned and funded, the existing conditions would remain for SR 60 within the project limits and only routine maintenance activities would occur. Advantages and disadvantages associated with the No-Build Alternative are outlined below:

Advantages of the No-Build Alternative:

- no new costs for design or construction;
- no adverse effects to natural resources; and
- no disruption to the public during construction.

Disadvantages of the No-Build Alternative:

- would not meet the goals of the Comprehensive Plans of Hillsborough and Pinellas Counties and the Cities of Tampa and Clearwater for constructing the SR 60 Multi-Use Trail across Old Tampa Bay;
- would not provide alternative modes of non-motorized transportation on SR 60 for a roadway that is currently at capacity; and
- would not complete the only link across Old Tampa Bay in the regional trail network for the Tampa Bay Region

These advantages and disadvantages, along with other established criteria, were used in the evaluation process of the No-Build Alternative and its comparison with the Build alternatives.

3.2 Build Alternatives Evaluated

3.2.1 Design Criteria

Recommended design criteria for multi-use trails are summarized in **Table 3-1**.

Table 3-1 Multi-Use Trail Design Criteria

DESIGN ELEMENT	DESIGN CRITERIA	REFERENCE
General		
Design Speed	20 MPH 30 MPH (with > 4% downgrade)	PPM - Section 8.6.7, p. 8-16
Typical Section		
Pavement Width	12 FT (Min.)	PPM - Section 8.6.2, p. 8-14
Shoulder Width	2 FT (unpaved) (1:6 max. slope) (both sides)	PPM - Section 8.6.5, p. 8-15
Cross Slopes	2% (Max.)	PPM - Section 8.6.3, p. 8-14
Separation from Adjacent Roadway	5 FT (Min.) (Rural) 4 FT (Min.) (Urban)	PPM - Section 8.6.10, p. 8-17
Horizontal Alignment		
Horizontal Clearance	4 FT (To Lateral Obstructions, including barriers)	PPM - Section 8.6.5, p. 8-15
Minimum Radius	95 FT (2% - 20 MPH) 110 FT (-2% - 20 MPH) 250 FT (2% - 30 MPH) 300 FT (-2% - 30 MPH)	PPM - Table 8.6.8.1, p. 8-16
Superelevation Transition	75 FT (Min.)	PPM - Section 8.6.8.1, p. 8-16
Vertical Alignment		
Vertical Clearance	8 FT (Min.), 10 FT (Desirable for underpasses and tunnels)	PPM - Section 8.6.6, p. 8-16
Stopping Sight Distance	127 FT (Min.) (20 MPH) 230 FT (Min.) (30 MPH)	PPM - Table 8.6.8.2, p. 8-17
Grades	5% (Max.) > 5% (See "Ramp Grades" below)	PPM - Section 8.6.4, p. 8-15
Ramp Grades	8.33% (Max.) (30 inch max. rise with level landing 60 inches long)	PPM - Section 8.6.4, p. 8-15
Minimum Length of Curve	(S>L) $L=2S-(900/A)$ (S<L) $L=AS^2/900$	PPM - Section 8.6.9, p. 8-17
Drop Off Criteria		
Case I - Vertical drop greater than 10 inches, closer than 2 feet from edge of sidewalk or path.	A railing, fence or other barrier to be placed within these limits in compliance with PPM - Section 8.8	PPM - Figure 8.2, p. 8-26
Case II - A slope steeper than 1:2 and closer than 2 feet from edge of sidewalk or path with a drop off greater than 30 inches	A railing, fence or other barrier to be placed within these limits in compliance with PPM - Section 8.8	PPM - Figure 8.2, p. 8-26
Lighting	Not proposed, as both Pinellas County and Hillsborough County "close" their trails at night	PPM - Section 8.6.12, p. 8-18
Signing and Pavement Marking	Per MUTCD	PPM - Section 8.6.13, p. 8-18

FDOT PPM - Plans Preparation Manual, Volume 1, January 2009 (Revised January 1, 2010) - Chapter 8
MUTCD - Manual on Uniform Traffic Control Devices, 2009, Federal Highway Administration.

3.2.2 General Description of Alternatives

Prior to this PD&E study, a Feasibility Study Report was completed by the FDOT in December 2008 (*Multi-Use Trail Feasibility Study from McMullen Booth to Veterans Expressway – Work Program Identification (WPI) Segment Number: 422640 1 and Federal Aid Project (FAP) No. 9045-090-C*). The Feasibility Study included the evaluation of four separate alternatives and one interim staging option. The trail alternatives were located on the north or south side of the Causeway and include either the Structural Option ‘W2’ (widening with piles in the water) or Structural Option ‘IS’ (Independent Structure). The alternatives studied in the Feasibility Study included:

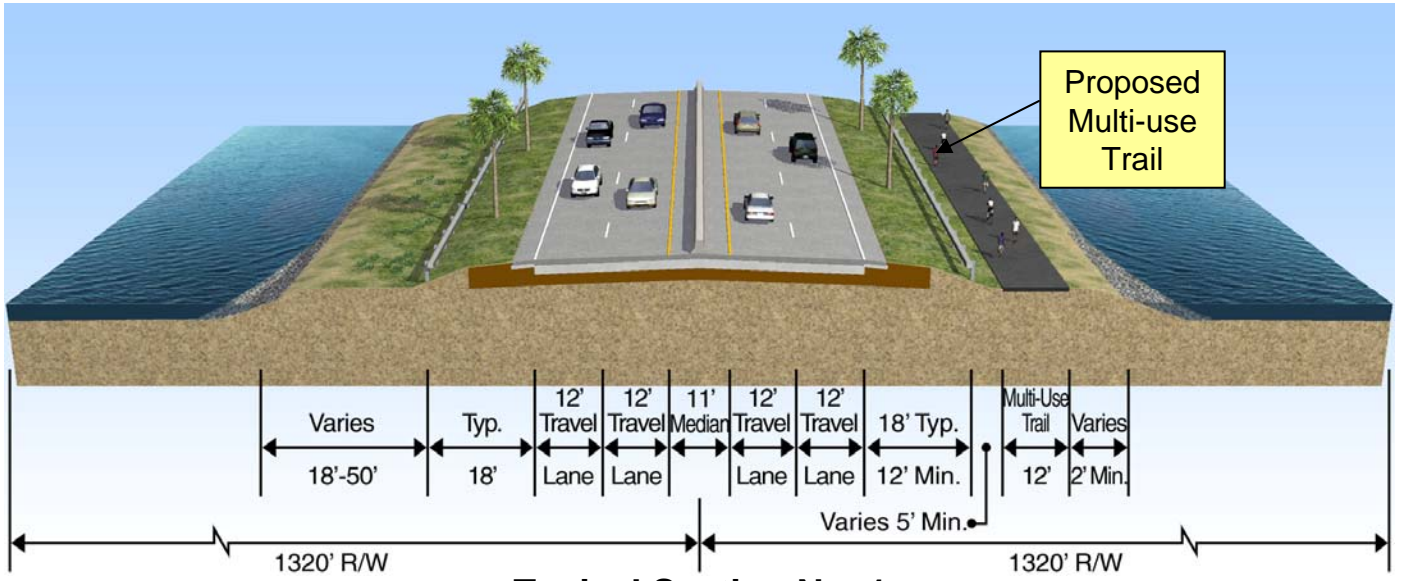
- Alternative N1 - This alternative included the trail on the *north* side of the Causeway and the Structures Widening Option ‘W2’ for Structures 1 and 2, and the reconfiguration of Structure 3. *Note that Structure 3 is outside the limits of this current PD&E study.*
- Alternative N2 - This alternative included the trail on the *north* side of the Causeway and the Independent Structural Option ‘IS’ for Structures 1, 2, and 3.
- Alternative S2 - This alternative included the trail on the *south* side of the Causeway and the Independent Structural Option ‘IS’ for Structures 1, 2, and 3.
- Staging Option S3 - This was an interim staging option which would provide a shared-use facility on the existing Causeway prior to the construction of any new water crossings.

3.2.3 Typical Sections and Trail Concepts

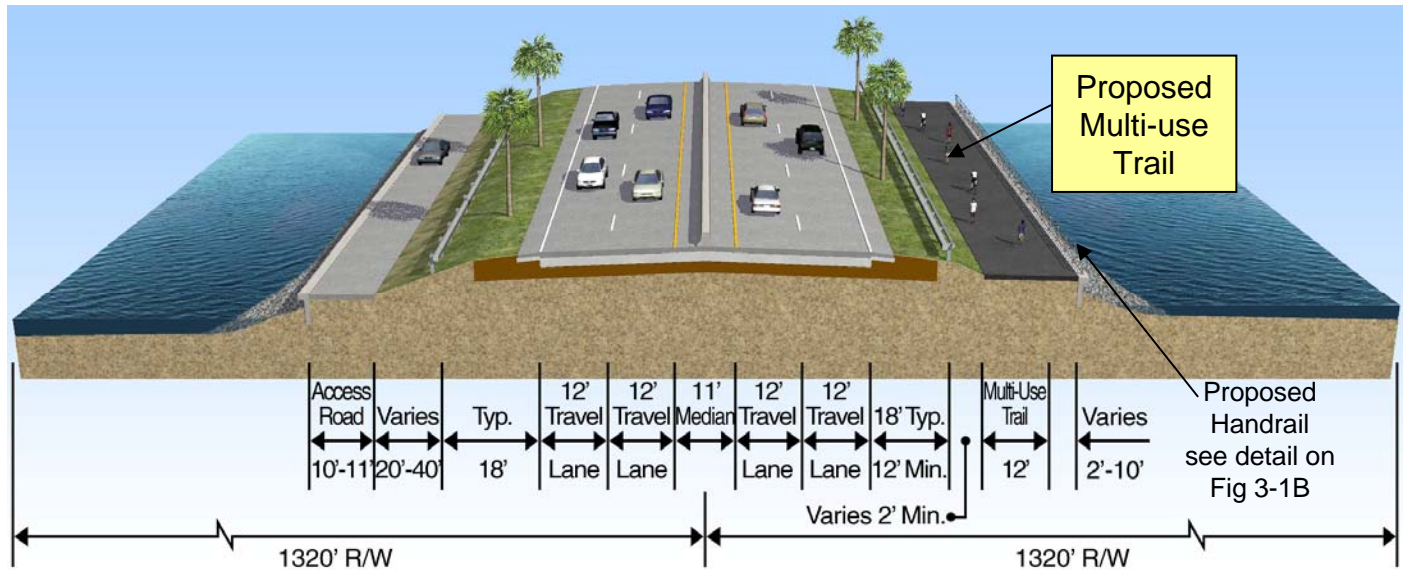
Proposed trail typical sections are shown in **Figures 3-1A** and **3-1B**. These are generally consistent with the typical sections shown in the Feasibility Study. The proposed 12-foot wide multi-use trail is shown on the south side only, although the alternatives studied previously under the Feasibility Study, considered a trail on the north side as well. At all locations, due to the close proximity of the proposed trail to the existing seawall and vertical drop-off, a hand rail is proposed on the seaward side of the trail.

Typical Section #1 - west portion of study area (approx. Sta 21+00 to 69+00)

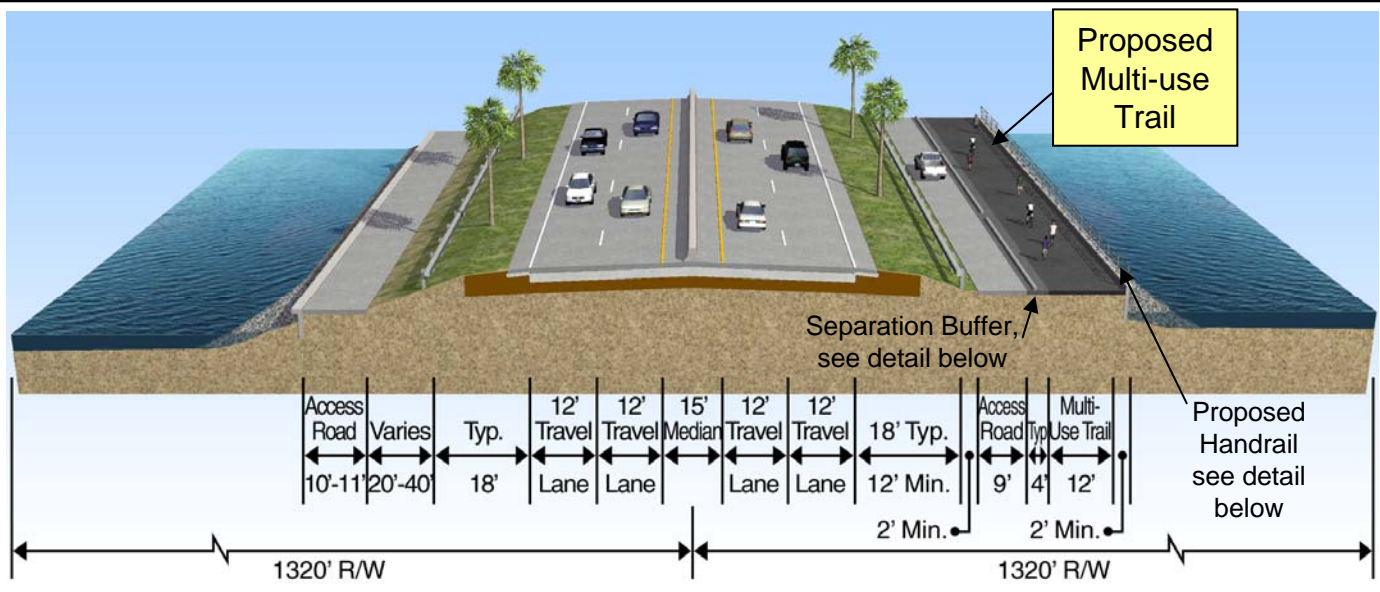
This typical section proposes the trail along the south side of SR 60 between the existing guardrail and beach area. The existing guardrail may need to be relocated from the existing 18 foot offset to a minimum offset of 12 feet from the eastbound edge of the travel lane to the face of the guardrail to accommodate the proposed trail typical section. Where the offset between the back of the steel guardrail posts and the trail is less than or equal to 4 feet, a pipe rail will be attached to the back of the steel guardrail posts. A minimum 4 foot separation from the back of the guardrail posts to the inner edge of the trail is preferred.



Typical Section No. 1
 Station 21+00 to Station 69+00



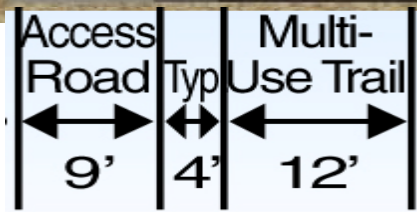
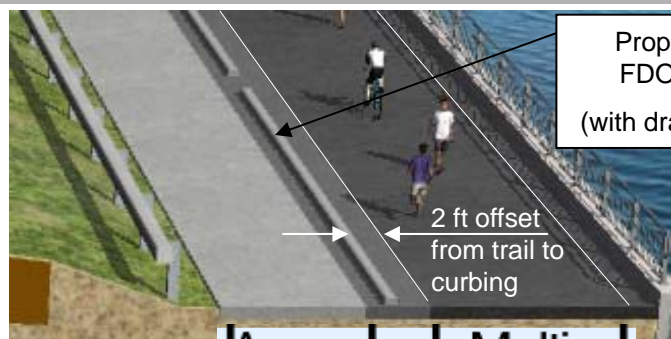
Typical Section No. 2
 Station 69+00 to Station 106+00 / Station 111+00 to Station 256+50
 Station 394+00 to Station 412+00



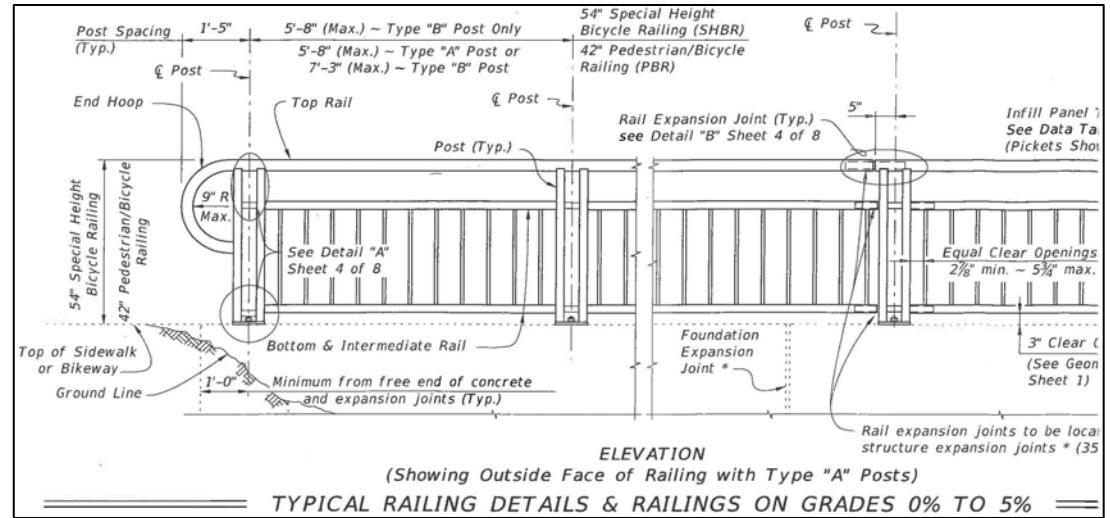
Typical Section No. 3

Station 256+50 to Station 265+00 / Station 300+00 to Station 394+00

Access Road /Trail Separation Buffer Detail



Proposed Handrail Detail
per FDOT Interim Standard Index 862 – Type 1 Picket Railing (sheet 2 of 8)



SR 60 Multi-Use Trail PD&E Study
 Bayshore Blvd to West of Ben T. Davis Beach Entrance
 WPI Segment No.: 422640 2
 Pinellas & Hillsborough Counties

Proposed Roadway/Trail Typical Sections (facing east)

Figure 3-1B

A 2 foot minimum graded separation from the outside edge of the trail to the beach is preferred. This typical section extends from Bayshore Boulevard to approximately 4,800 feet to the east. Between approx. Sta. 45+00 to 56+00, timber ties will be placed parallel to and approx. 7 feet from the beach-side edge of the trail to provide separation from the existing informal parking area to keep vehicles from protruding into the trail. See Concept Plans, **Appendix D** for detailed location information.

Typical Section #2 - from approx. Sta 69+00 to 106+00, 111+00 to 256+50 and 394+00 to 412+00

This typical section proposes the trail along the south side of SR 60 between the existing guardrail and sea wall. The existing access road will no longer exist for this section and the proposed trail, instead will be situated in place of the access road. The pavement will be resurfaced, slightly widened and striped for the trail. The existing guardrail may be relocated from the existing 18 foot offset to a minimum offset of 12 feet from the edge of the travel lane to the face of the guardrail to accommodate the proposed typical section. Where the offset between the back of the steel guardrail posts and the trail is less than or equal to 4 feet a pipe rail will be attached to the back of the steel guardrail posts. A minimum 2 foot separation from the back of the guardrail posts to the inner edge of the trail is required. A 5 foot separation from the outside edge of the trail to the outer edge of the sea wall is preferred with a 2 foot minimum separation. A handrail is proposed to be mounted to the top of the seawall and is shown in more detail on **Figure 3-1B**. This typical is used at three locations for an approximate length of 20,050 feet (see Concept Plans, **Appendix D** for detailed location information).

Typical Section #3 - from approx. Sta 256+50 to 265+00, 300+00 to 394+00

This typical section proposes a 9 foot frontage road, 4 foot buffer separation (with curbing) and the trail along the south side of SR 60 between the existing guardrail and sea wall. The buffer separation is detailed on **Figure 3-1B**. The existing guardrail may be relocated from the existing 18 foot offset to a minimum offset of 12 feet from the edge of the travel lane to the face of the guardrail to accommodate the proposed typical section. A minimum 2 foot separation from the back of the guardrail posts to the inner edge of the frontage road is required. Similar to Typical Section #2, a 5 foot separation from the outside edge of the trail to the outer edge of the sea wall is preferred with a 2 foot minimum along with a handrail mounted atop the seawall. Openings in the handrail are proposed at approximately station 302+50, 346+00 and 374+50 to maintain access to concrete stairs between the seawall and water's edge. This typical section is used at two locations for an approximate length of 10,250 feet (see Concept Plans, **Appendix D** for detailed location information).

According to the Feasibility Study Report, the majority of existing access road pavement that could be incorporated into the proposed trail is located on both sides of the Causeway

directly adjacent to the existing revetment system and seawall. This existing surface of the access road was installed between 1978 and 1980 as a part of a revetment project and was not intended to be utilized as a driving surface but instead as part of the permanent erosion control system. The original pavement section of 6-inch soil cement base with a modified surface treatment was resurfaced in 1998. Based on a visual inspection this pavement seems to be performing well; however, additional resurfacing would be needed in order to remove longitudinal undulations and any non-ADA compliant cross slopes. Since the existing pavement is performing well under current vehicular loads, trail maintenance vehicles would not pose any problems with the current structure with the added structural enhancement from the resurfacing.

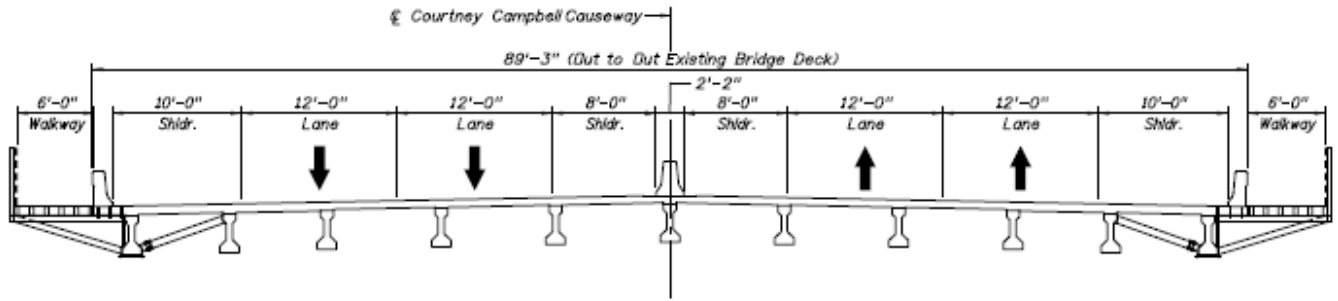
The existing service access road is proposed to be eliminated from the south side of the causeway at several locations in order to construct the trail and avoid relocating the existing seawall. Maintenance vehicles can utilize the trail or unpaved areas adjacent to the proposed trail to access the causeway areas required to maintain the seawall. The preliminary concept plans in **Appendix D**, depict these locations. The access road pavement will no longer exist from stations 111+00 to 256+00 and 395+00 to 412+00. Accordingly, entry points for the south access road along SR 60 will be closed at approximately station 137+00, 225+00 and 412+00. A new SR 60 entrance is proposed at approximately station 256+00. An existing access point at approximately station 362+00 was considered for elimination and optionally presented at the public hearing for closing. No public comments were received. The FDOT decided not to close this access point to the service road. No changes will be made to the existing access road or entry points to it on the north side of the causeway.

3.2.4 Bridge Alternatives

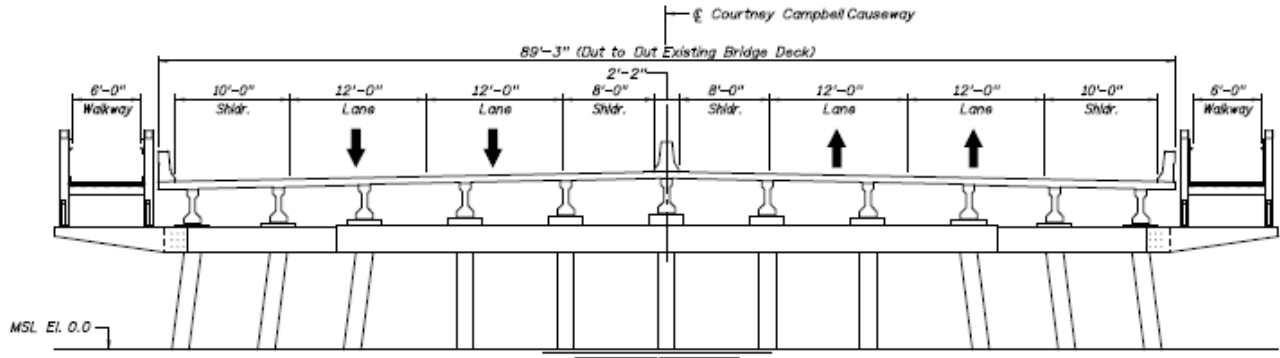
The proposed multi-use trail will require bridge crossings at two locations (within the PD&E study limits) for a continuous pathway. SR 60 Structures 1 and 2 in their current configuration do not have sufficient deck width to accommodate the required trail width. These structures would need to be widened or a parallel structure built to provide a multi-use trail.

Two separate alternatives for widening the existing bridges were studied in the Feasibility Study. The first method involved several options for attaching cantilevered structural components to the existing bridges which would not require the driving of additional support piles. Structural analyses of these alternatives showed that these methods were not structurally feasible. The second method of widening involved driving additional support piles alongside the existing bridges. This method is more costly but is structurally viable. The third bridge alternative consisted of constructing independent bridge structures for the trail parallel to the existing highway bridges on the Causeway. These 3 methods are illustrated in **Figure 3-2A** for Structure No. 1 and **Figure 3-2B** for Structure No. 2.

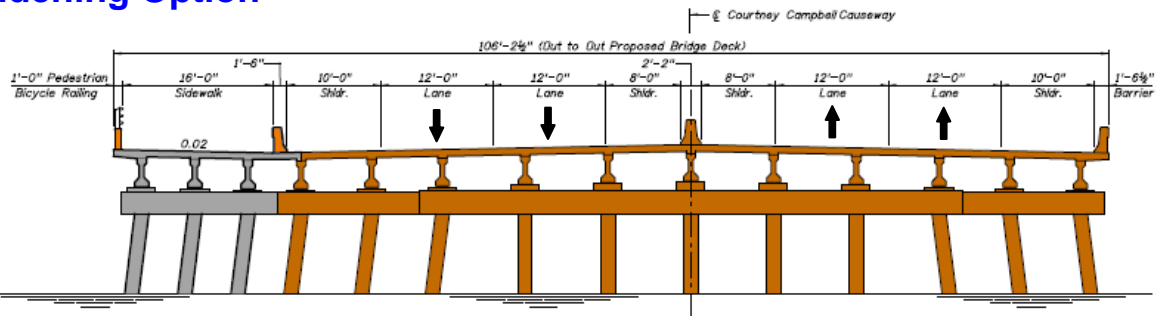
Cantilevered Walkways Option



Cantilevered Prefab Truss Option



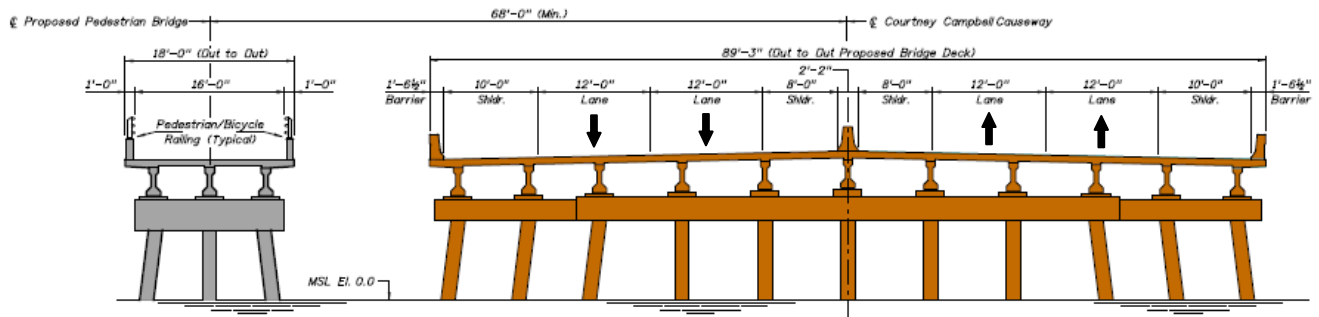
Bridge Widening Option



OPTION W2

NORTH ALIGNMENT ALTERNATIVE SHOWN
SOUTH ALIGNMENT ALTERNATIVE IS A MIRROR IMAGE

Independent Structure Option



OPTION IS

NORTH ALIGNMENT ALTERNATIVE SHOWN
SOUTH ALIGNMENT ALTERNATIVE IS A MIRROR IMAGE

Structure 1



SR 60 Multi-Use Trail PD&E Study

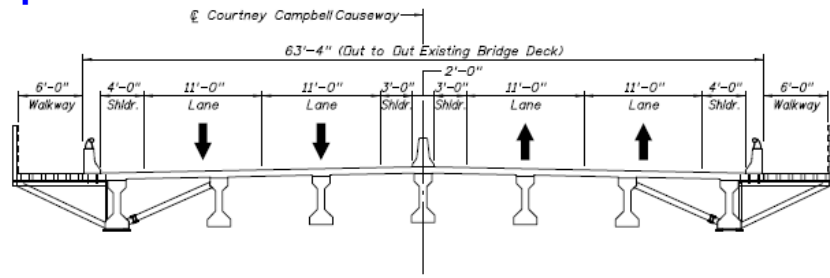
Bayshore Blvd to west of the
Ben T. Davis Beach entrance

WPI Segment No.: 422640 2
Pinellas & Hillsborough Counties

Bridge Alternatives Considered
in the Feasibility Study

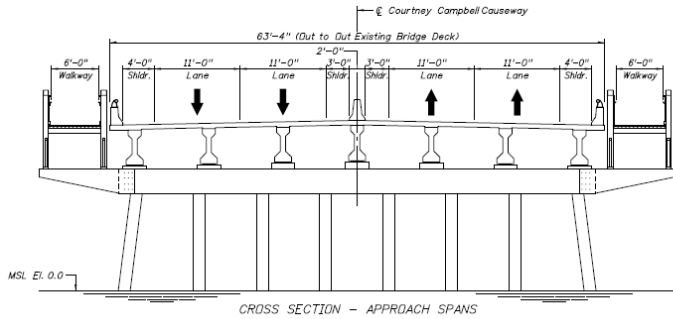
Figure 3-2A

Cantilevered Walkways Option

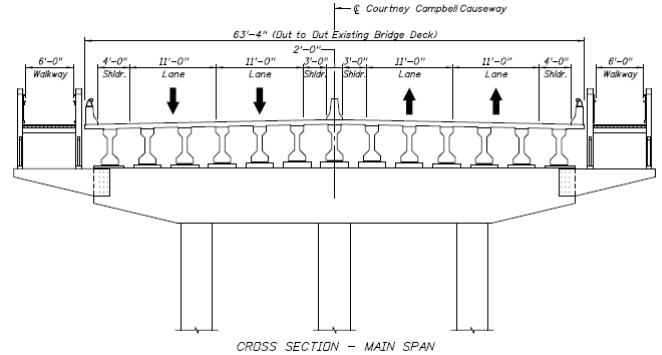


CROSS SECTION - STRUCTURE 2

Cantilevered Prefab Truss Option

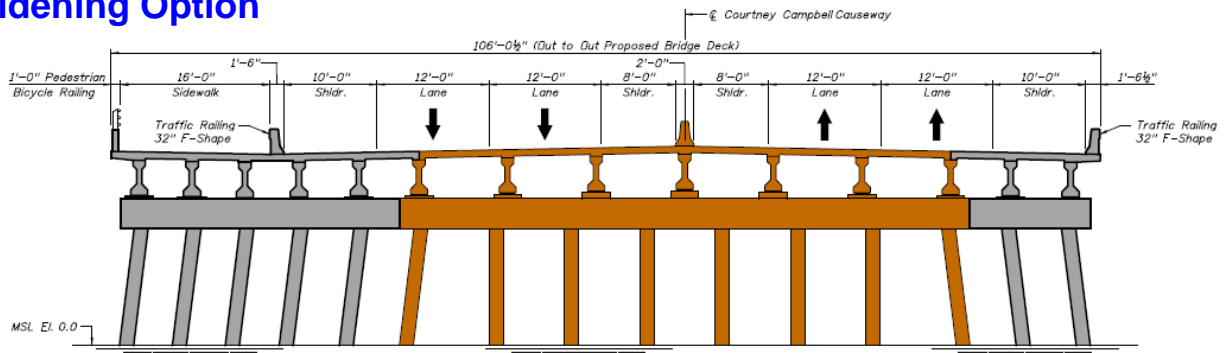


CROSS SECTION - APPROACH SPANS



CROSS SECTION - MAIN SPAN

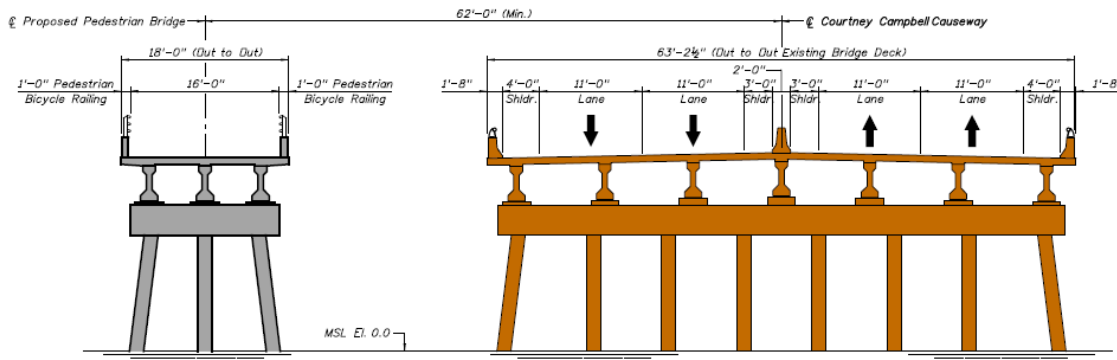
Bridge Widening Option



OPTION W2

NORTH ALIGNMENT ALTERNATIVE SHOWN
SOUTH ALIGNMENT ALTERNATIVE IS A MIRROR IMAGE

Independent Structure Option



OPTION IS

NORTH ALIGNMENT ALTERNATIVE SHOWN
SOUTH ALIGNMENT ALTERNATIVE IS A MIRROR IMAGE

Structure 2



SR 60 Multi-Use Trail PD&E Study
Bayshore Blvd to west of the Ben T. Davis Beach entrance
WPI Segment No.: 422640 2
Pinellas & Hillsborough Counties

Bridge Alternatives Considered
in the Feasibility Study

Figure 3-2B

Structure No. 1 - The existing SR 60 bridge (Bridge No. 150138) is a prestressed concrete girder facility that was originally built in 1974 and widened in 1992. This bridge is located from Mile Post (MP) 7.543 to MP 7.633 in Pinellas County. The superstructure consists of an 89'-3" wide reinforced concrete deck cast over 11 - 43'-0" spans. The deck slab is cast continuously in two separate units. The prestressed concrete girders are AASHTO Type II. The substructure consists of pile bents utilizing 18" square prestressed concrete piles. Joints depend on a compression type seal. The current structure has a vertical clearance of 10.70' above the mean high water elevation and a horizontal clearance of 40'. According to a structural inventory and appraisal performed in March 2010, the existing Causeway Bridge has a structural sufficiency rating of 85.0 percent and was classified as "not deficient, above minimum criteria." The structure has no Load Rating restrictions.

Structure No. 2 - The existing SR 60 bridge (Bridge No. 100301) is a prestressed concrete girder facility that was originally built in 1974. This bridge is located from MP 1.758 to MP 2.374 in Hillsborough County. The superstructure consists of a 63'-4" wide reinforced concrete deck cast over 45 spans. There are 12 approach spans on either side of the bridge which are 61'-6" in length and consist of AASHTO Type III girders. The inner spans are made up of ten 83'- 6" spans on either side of a 110'-0" navigational span. The inner superstructures consist of Type IV girders. The approach spans are supported on pile bents utilizing 18" (end bents) and 24" (interior bents) square prestressed concrete piles. The 83'-6" inner spans are supported on two column bents grounded on pile footings. The navigational span is supported by three column bents with a 47' x 22' concrete crash walls between the columns. Joints depend on a compression type seal. The navigational span has a vertical clearance of 43.50' above the mean high water elevation and a horizontal clearance of 75.'

The substructure is protected by a timber fender system. According to a structural inventory and appraisal performed in November 2009, the existing Causeway bridge has a structural sufficiency rating of 70.0 percent and was classified as "not deficient, above minimum tolerable." The structure has no Load Rating restrictions.

Navigational Issues - The SR 60 bridges cross over the northern regions of Old Tampa Bay. The navigable channel consists of:

Structure 1 – The current SR 60 structure has a vertical clearance of 10.70' above the mean high water and a horizontal clearance of 40'. Deepest high water depth – 6' (Based on as-built construction drawings)

Structure 2 – The current SR 60 structure has a vertical clearance of 43.50' above the mean high water elevation and a horizontal clearance of 75'. Vessels are guided thru the channel by a timber fender system at the main span location. Deepest high water depth – 19' (Based on as-built construction drawings). There have been no significant navigational impacts to the structures on the Causeway since it was constructed.

For the separate trail bridges option, the bridges will be designed to meet the requirements in AASHTO's Load and Resistance Factor Design (LRFD) Guide Specifications for the Design of Pedestrian Bridges (2009) which specifies a live load equal to the greater of either a 90 per square foot pedestrian load or the H10 design vehicular truck load that would perform routine maintenance and inspection. The independent structures option ("IS") noted in the Feasibility Study Report is recommended due to significant cost savings and ease of construction compared to the bridge widening option. The proposed bridges will be built to maintain the existing vertical and horizontal clearances of the existing SR 60 bridges. Also, the proposed span arrangement and substructure elements for the proposed trail bridges are intended to be consistent with the SR 60 roadway bridges and "line-up" to facilitate navigation and tidal flow. The existing fender system under Structure 2 will be extended under the new adjacent trail bridge. The recommended trail bridge typical sections are shown in **Figure 3-3**.

Based on the designation of SR 60 as a scenic highway, the FDOT intends to consider Level Two aesthetic treatments as defined in the FDOT's PPM (section 26.9.4) for the bridge structures. This treatment level will be further evaluated during the design phase. The PPM defines Level Two aesthetics as:

"The emphasis is on full integration of efficiency, economy and elegance in all bridge components and the structure as a whole. Consideration should be given to structural systems that are inherently more pleasing, such as hammerhead or "T" shaped piers, oval or polygonal shaped columns, integral caps, piers in lieu of bents, smooth transitions at superstructure depth change locations, box-type superstructures, etc."

3.3 Alternatives Evaluation Matrix

A matrix comparing the alternatives studied as part of the earlier Feasibility Study is included in **Table 3-2**. FDOT determined Alternative S2 should move forward from the Feasibility Study based on connectivity, costs, environmental impacts, safety considerations, and constructability.

Feasibility Study Alternative S2 was further refined with this PD&E Study. A matrix comparing the No-build alternative with the Build alternative is included in **Table 3-3** including updated cost estimates and potential environmental impacts. The preliminary cost estimates include costs for engineering/design, construction, and construction engineering and inspection costs (CEI). The construction cost of the Build Alternative was calculated using the FDOT's Long Range Estimating system using current construction unit prices as of January 2011. No land acquisition will be required due to this project, so no costs for land acquisition are included. The engineering (preliminary and final design) cost was calculated as a percentage (12.0 percent) of the construction cost. The CEI costs were also estimated at 12.0 percent of the construction cost.

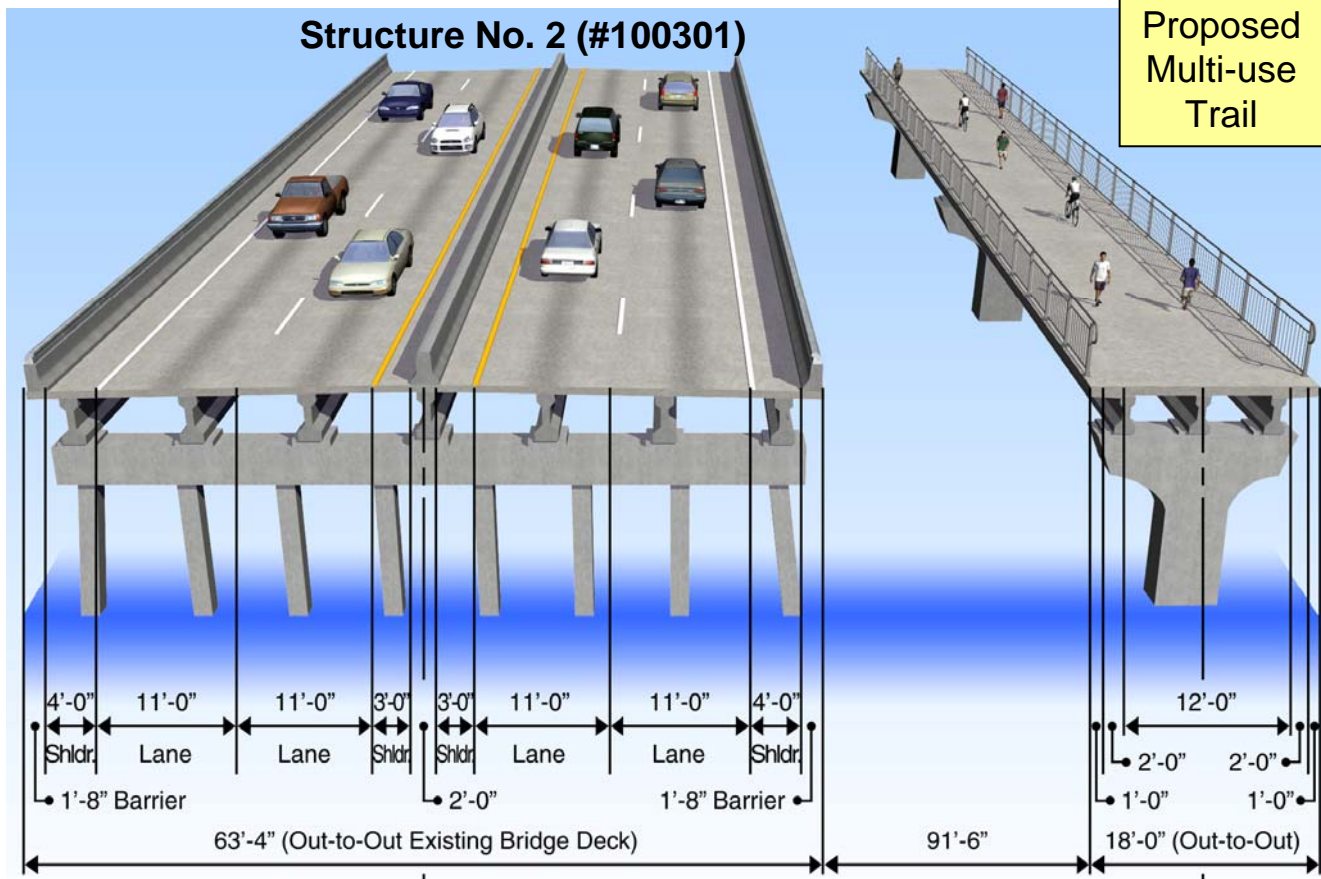
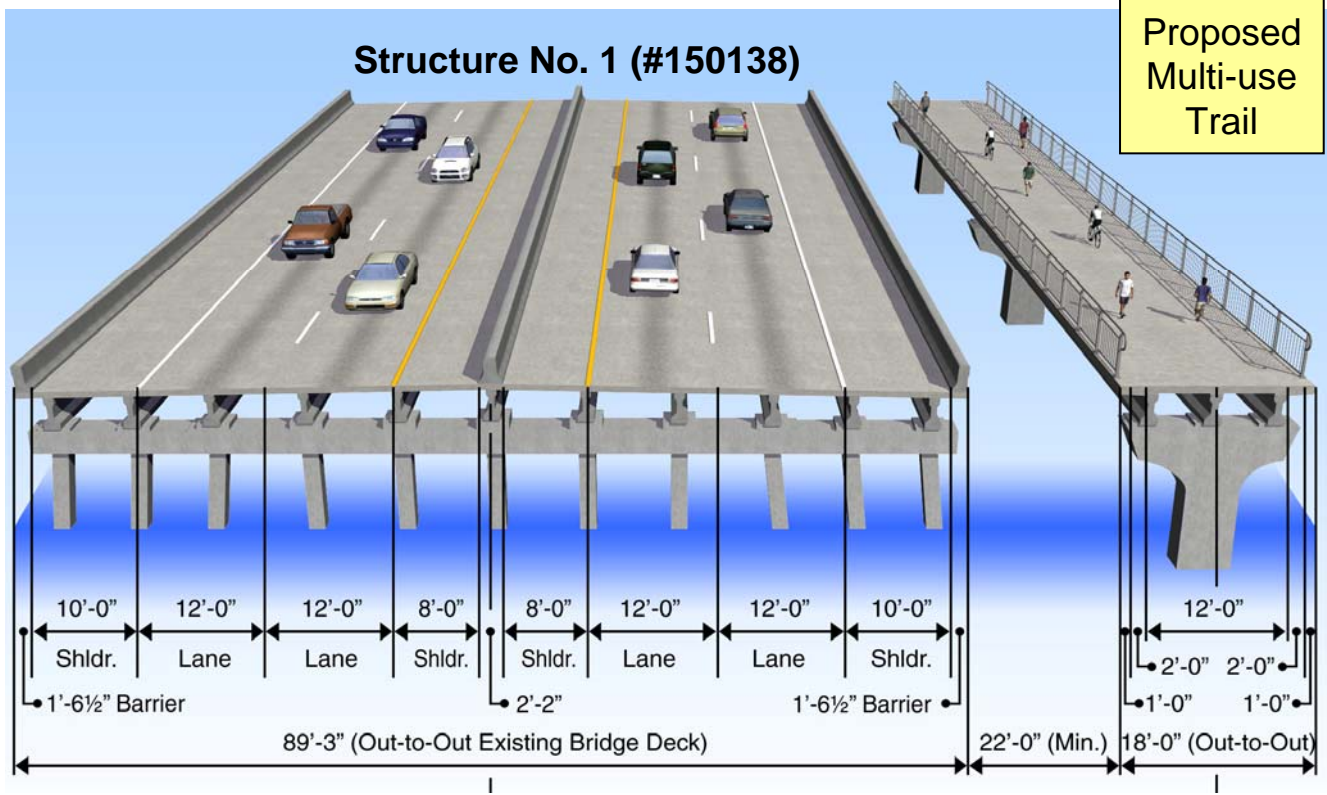


Table 3-2 Alternatives Evaluation Matrix from Feasibility Study

EVALUATION FACTORS	FEASIBILITY STUDY ALTERNATIVES				
	N1	N2	S1	S2	No Build
RELOCATIONS & RIGHT OF WAY ACQUISITION					
Number of residents or businesses expected to be relocated	none	none	none	none	none
Right-of-way to be acquired in acres, including ponds	none	none	none	none	none
COMMUNITY EFFECTS (within right-of-way)					
Churches, schools, public services, etc.	none	none	none	none	none
EFFECTS ON CULTURAL/HISTORICAL RESOURCES AND PUBLIC PARKS					
Number of historic sites within or adjacent to right-of-way	none	none	none	none	none
NATURAL ENVIRONMENTAL EFFECTS					
Total wetland area encroachment in acres (not quantified)	minor	minor	minor	minor	none
Potential impacts to mangroves*	minor	minor	minor	minor	none
Potential impacts to Bird Nesting Protected Area*	minor	minor	none	none	none
FLOODPLAIN AND FLOODWAY ENCROACHMENT					
Area of base floodplain encroachment in acres	none	none	none	none	none
ESTIMATED PROJECT COSTS (Year 2007 present value in million \$)¹					
Trail construction cost	\$22.0	\$14.7	\$23.9	\$16.6	none
Structures construction cost	\$26.6	\$10.0	\$26.6	\$10.0	none
SUBTOTAL CONSTRUCTION COST	\$46.6	\$24.7	\$50.5	\$26.6	none
Engineering cost, 10% of construction cost	\$4.9	\$2.5	\$5.1	\$2.7	none
Construction engineering & inspection, 15% of Construction	\$7.3	\$3.7	\$7.6	\$4.0	none
Right-of-way acquisition cost	none	none	none	none	none
TOTAL COST	\$60.8	\$30.9	\$63.2	\$33.3	none

The information above was taken from Table 8-1 in the 2008 Feasibility Study Report.

¹The cost estimates above include an approximately 2-mile longer study area as well as an additional Bay crossing at Structure #3, which is east of the current study limits.

ALTERNATIVE N1

This alternative includes the trail on the *north* side of the Causeway and the Structures Widening Option "W2" for Structures 1 and 2, and the reconstruction of Structure 3.

ALTERNATIVE N2

This alternative includes the trail on the *north* side of the Causeway and the Independent Structural Option "IS" for Structures 1, 2 and 3.

ALTERNATIVE S1

This alternative includes the trail on the *south* side of the Causeway and the Structures Widening Option "W2" for Structures 1 and 2, and the reconstruction of Structure 3.

ALTERNATIVE S2 – Preferred from Feasibility Study

This alternative includes the trail on the *south* side of the Causeway and the Independent Structural Option "IS" for Structures 1, 2 and 3.

Table 3-3 No-Build and Build Alternatives Evaluation Matrix

Evaluation Criteria ¹	Alternatives	
	No-Build	Build
Potential Environmental Effects		
Wetlands (acres)	0	0 ²
Seagrass (acres)	0	0
Threatened & endangered species	0	Minimal
Potential Petroleum or hazardous material sites	0	1
Estimated Costs ³ (Present day costs as of January 2011)		
Right-of-way Acquisition	\$ 0	\$ 0
Construction Costs		
Roadway & Bridges	\$ 0	\$ 17,600,000
Stormwater Facilities	\$ 0	\$ 0
Total Construction Costs	\$ 0	\$ 17,600,000
Engineering Design ⁴	\$ 0	\$ 2,100,000
Construction Engineering & Inspection ⁴	\$ 0	\$ 2,100,000
Wetland Mitigation	\$ 0	To be determined
Preliminary Estimate of Total Costs	\$ 0	\$ 21,800,000

Notes: ¹ Criteria shown where a numerical difference exists between no-build and build.

² Mangrove trimming may be needed for construction of the trail. No fill or dredging in wetlands.

³ Costs rounded to the nearest \$100,000

⁴ Estimated at 12% of construction costs

3.4 Selection of the Recommended Alternative

All options discussed previously were evaluated with regards to costs, operational factors and environmental impacts. Based on these evaluations, the Study Team determined the Build Alternative should be considered the Recommended Alternative. This determination was made considering the proposed multi-use trail meets the need for several reasons.

- The proposed multi-use trail provides a missing regional trail connection between Pinellas and Hillsborough Counties and links proposed facilities on either sides of Tampa Bay,
- The proposed multi-use trail provides enhanced access and pedestrian/bicyclist opportunities for users of the causeway and Ben T. Davis beach area connection
- The proposed trail can be constructed with minimal environmental impacts,
- Constructing the proposed trail is consistent with local government plans.

This build alternative was presented at the public hearing. Features of the Build Alternative are described further in the **Section 4**.

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Section 4 - PREFERRED ALTERNATIVE

4.1 Typical Sections

The proposed multi-use trail is proposed to be constructed on the south side of SR 60. Typical sections for the recommended build alternative are shown in **Figures 4-1A** and **4-1B** for the trail along the causeway and **Figure 4-2** for the new trail bridge structures 1 and 2. A handrail is proposed to run along the bay-side of the trail mounted on an existing seawall. At locations where the proposed trail is situated adjacent to a maintenance access road, a proposed asphaltic curb (per FDOT Standard Index 300) will be located between the trail and access road to protect trail users from vehicles. The handrail and curbing are detailed on **Figure 4-1B**.

4.2 Horizontal Alignment

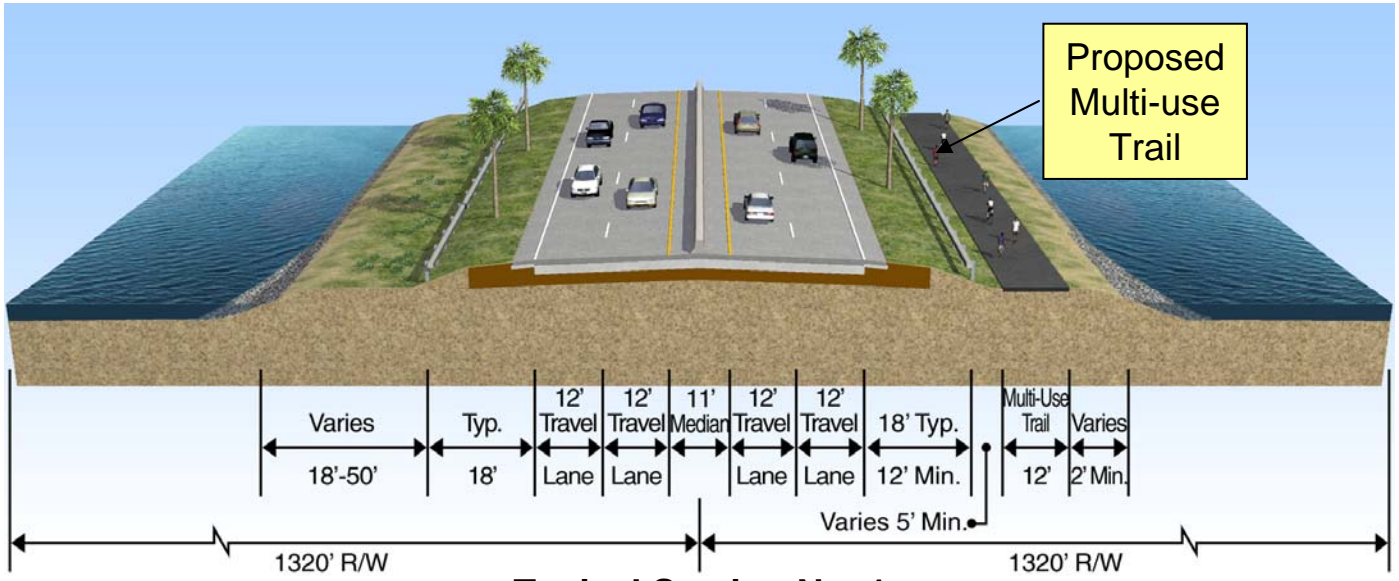
The horizontal alignment for the proposed trail is mostly tangent and gently curvilinear in most areas. The currently proposed minimum radius on the conceptual design plans is 110 ft which occurs near the following stations: 22+00, 28+00, 43+00, 45+00, 69+00, 74+00, 129+00, 192+00, 206+00, and 208+00. The proposed trail design speed is 20 miles per hour which allows for a minimum radius of 95 feet or a 60 degree curve. The horizontal alignment of the trail at the eastern end of Structure No. 2 was shifted closer to SR 60 to reduce the project footprint and potentially avoid impacts to existing seagrasses as surveyed. See Section 5.1.2 for additional information pertaining to coastal/marine resources. Preliminary Concept Plans are shown in **Appendix D**.

4.3 Vertical Alignment

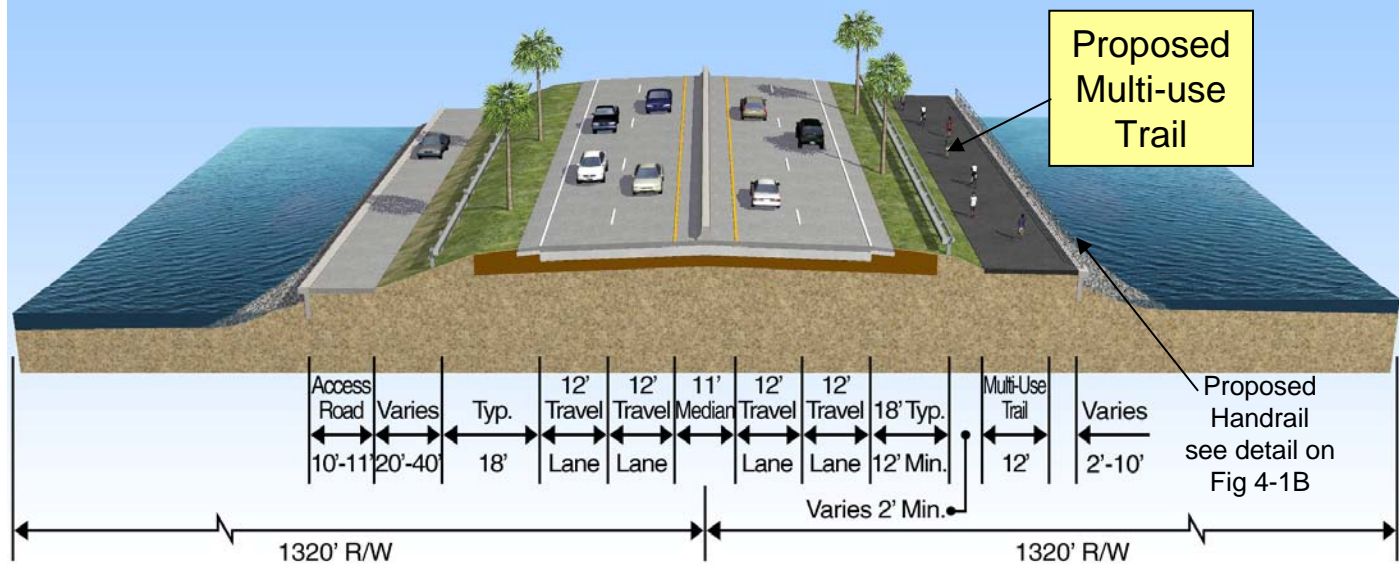
The proposed vertical alignment generally follows the existing Causeway ground line. The maximum proposed grade is approximately 3 percent at the approaches to the eastern proposed trail bridge (structure no. 2). The proposed trail will generally follow the existing roadway and structures vertical alignment. ADA requirements will need to be met for the proposed trail. A grade as high as 8.33 percent could be used; however, that would require a 5-ft landing for every 30 ft of rise.

4.4 Drainage

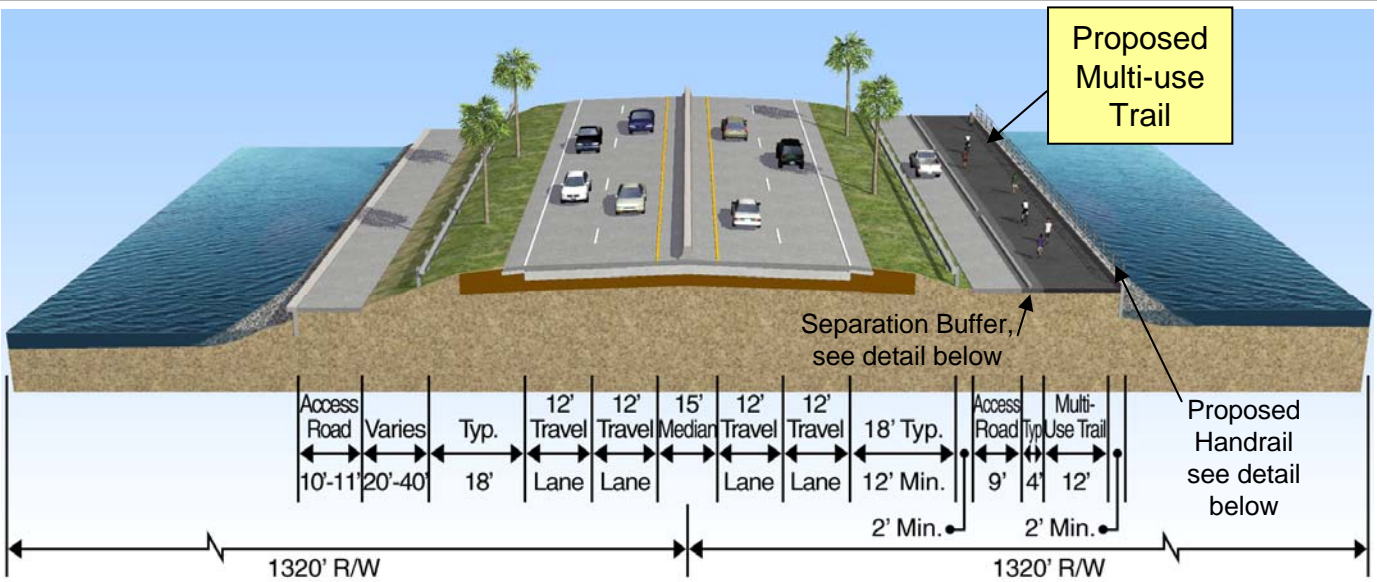
No changes are proposed to existing drainage patterns. Currently runoff from the roadway flows directly over flat vegetated slopes to the riprap revetment or to natural sandy shores along the Causeway. Slots are proposed in the curbing between the access road and trail to maintain drainage flows and avoid the need for drainage structures. Pending coordination with the SWFWMD, it is anticipated that no attenuation or treatment of stormwater runoff



Typical Section No. 1
 Station 21+00 to Station 69+00



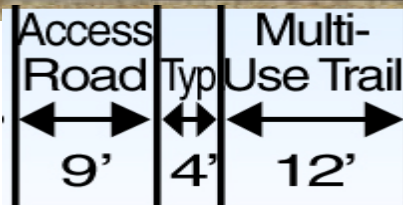
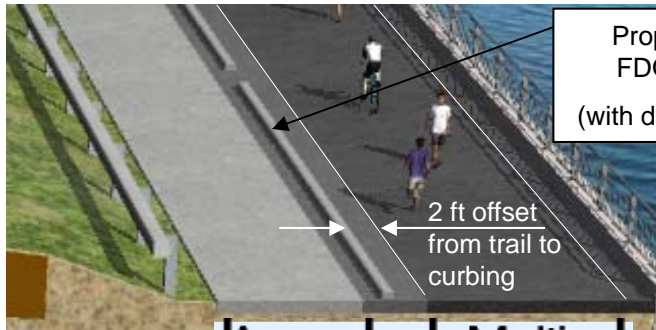
Typical Section No. 2
 Station 69+00 to Station 106+00 / Station 111+00 to Station 256+50
 Station 394+00 to Station 412+00



Typical Section No. 3

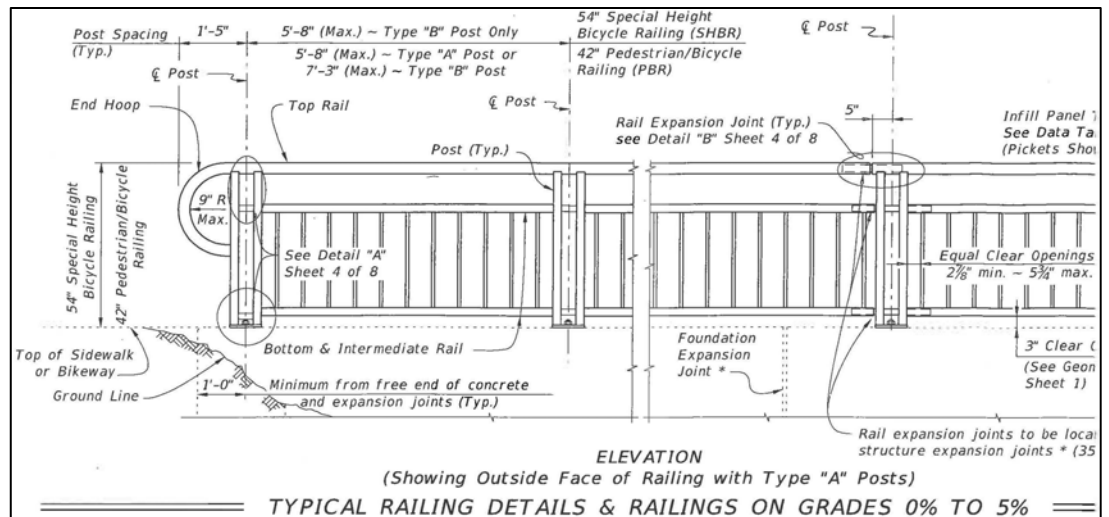
Station 256+50 to Station 265+00 / Station 300+00 to Station 394+00

Access Road / Trail Separation Buffer Detail



Proposed Handrail Detail

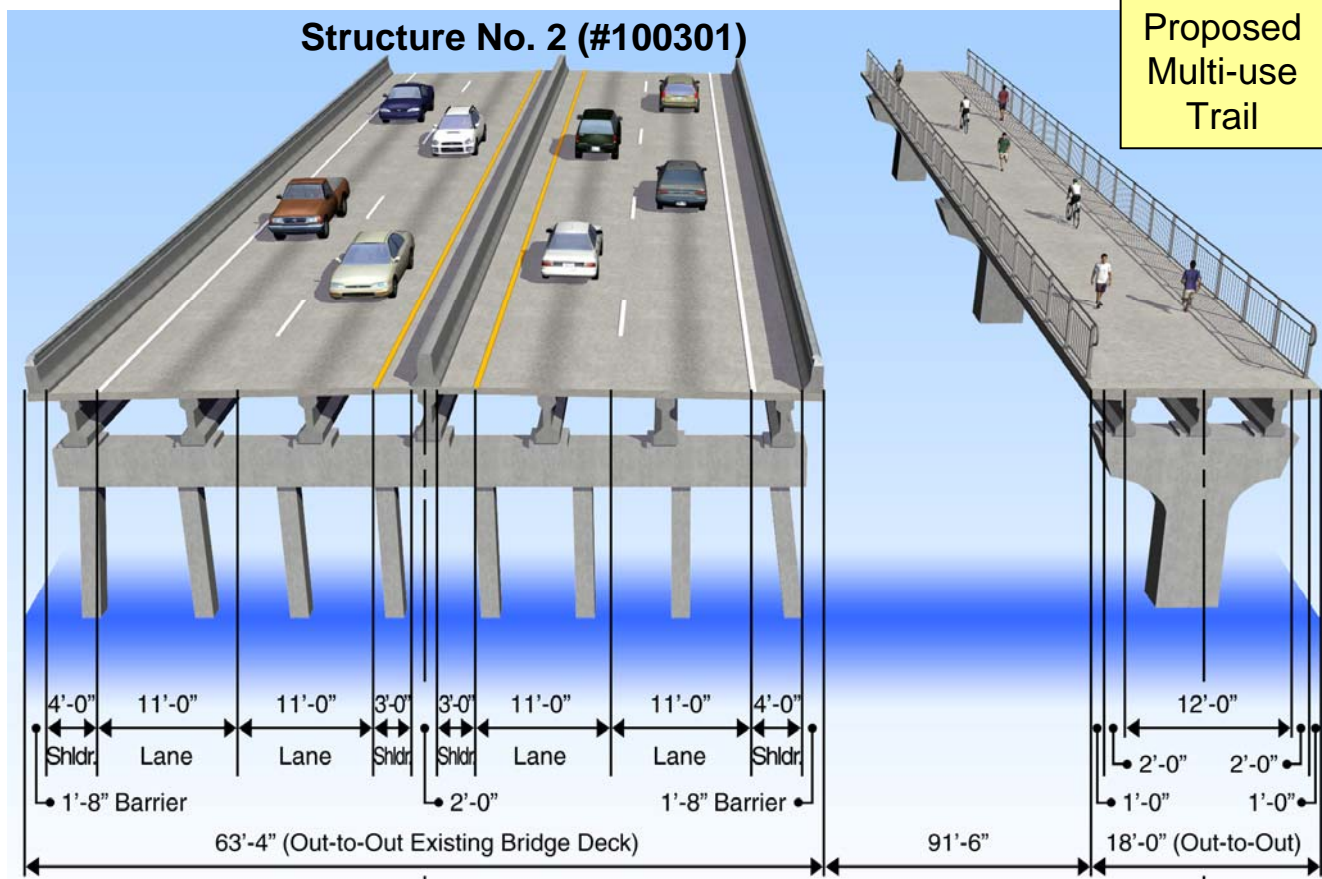
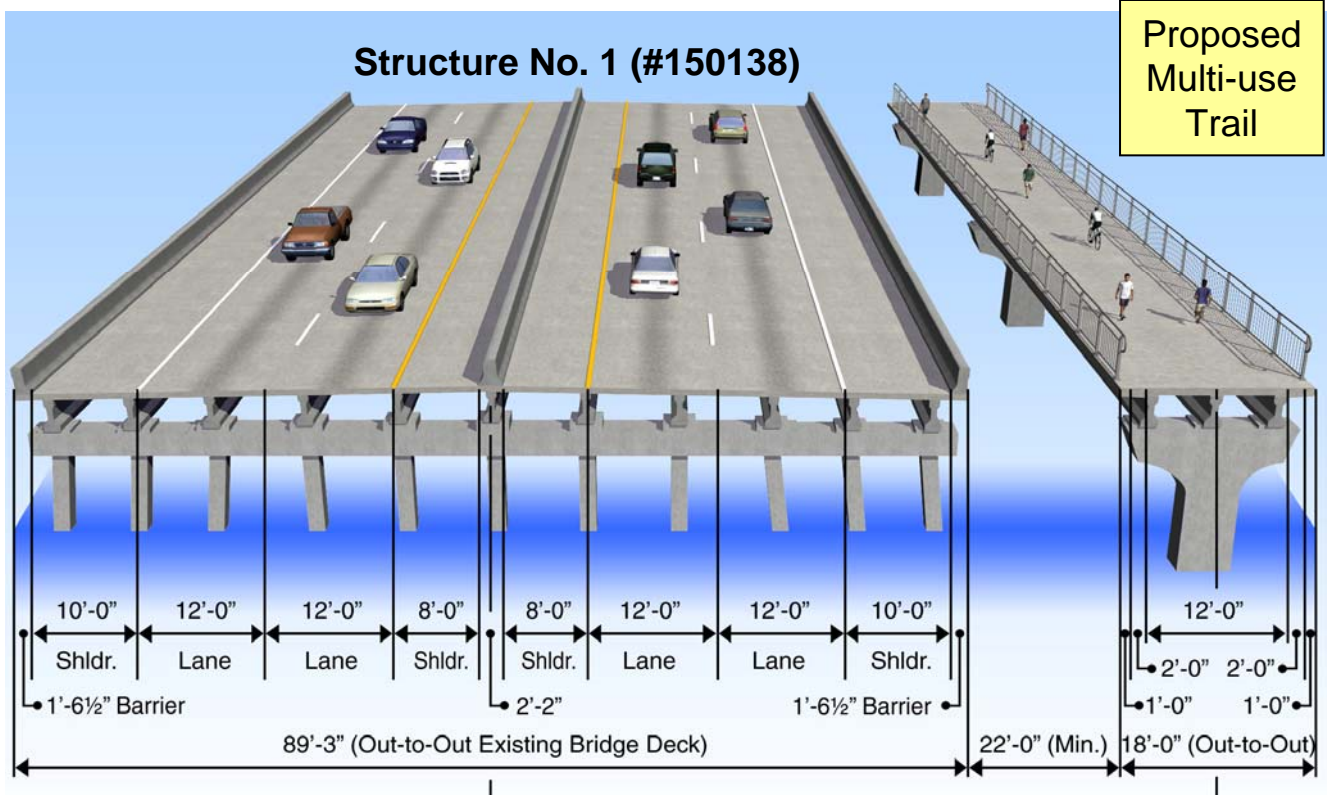
per FDOT Interim Standard Index 862 – Type 1 Picket Railing (sheet 2 of 8)



SR 60 Multi-Use Trail PD&E Study
 Bayshore Blvd to West of Ben T. Davis Beach Entrance
 WPI Segment No.: 422640 2
 Pinellas & Hillsborough Counties

Preferred Roadway/Trail
 Typical Sections
 (facing east)

Figure 4-1B



from the proposed trail is required due to the non-motorized trail users expected. On June 23, 2011, an Environmental Resource Permit (ERP) pre-application meeting was held at SWFWMD with FDOT staff preparing the design-build criteria package for Segment 3. Minutes from the meeting have not been released.

4.5 Structures

The recommended trail bridge typical sections are shown in **Figure 4-2**. The clear widths on the bridge decks will be 16 feet in width which accommodates at 12 foot trail and 2 foot clear distance on each side which aligns with the clear shoulder area along the trail approaches. Span lengths will be determined during the design phase, considering the existing highway bridge span lengths and navigational needs. It is intended the substructure elements of the new trail bridges will be consistent and align with the substructure elements of the adjacent SR 60 bridges with respect to navigational openings. Horizontal and vertical clearances of the existing adjacent SR 60 highway bridges are assumed to be carried to the new trail bridges. Level Two aesthetic treatments as defined in the FDOT's PPM (section 26.9.4) will be considered during the design phase for the bridge structures.

4.6 Design Traffic Volumes

Design traffic volumes are not applicable for this multi-use trail PD&E study.

4.7 Intersection Requirements

The proposed trail will intersect several minor connections to service roads and parking areas. Special design treatments may be required including enhanced crosswalk designs, possible use of speed tables, and other signs/markings to increase motorist's awareness of potential conflicts with trail users. Bollards may be constructed on the trail at interface points with the access road or intersections to discourage public vehicles from using the trail. These will be determined during the design phase.

4.8 Access Management Designation

Access management requirements are not applicable for this multi-use trail project.

4.9 Pedestrian & Bicycle Facilities

The proposed facilities for pedestrians and bicyclists are described throughout Sections 3 and 4 of this report.

4.10 Right-of-Way Requirements & Relocations

No right of way acquisition or relocations of residents or businesses are proposed as part of the recommended trail construction.

4.11 Utilities & Lighting

Based on a Sunshine One Call (January 18, 2011) utility owners along the Causeway include the following companies or agencies:

- Bright House Networks
- City of Clearwater Water, Sewer & Drainage
- City of Tampa Transportation, Water & Wastewater
- City of Clearwater Gas Systems
- Fiber Light, LLC
- FPL Fibernet
- MCI
- Pinellas County Highway/Engineering
- Telecommunications Management, Pinellas County
- Progress Energy
- Tampa Electric Company
- Verizon
- Hillsborough County Traffic Service Unit

Coordination with these utility owners will occur during the design stage to identify and resolve any potential utility conflicts.

4.12 Aesthetics and Landscaping

Through the collective efforts of many citizens and officials of the City's of Clearwater and Tampa, Westshore Alliance, the Clearwater Regional Chamber of Commerce, the Commissions of Pinellas and Hillsborough Counties, the FDOT Secretary officially designated SR 60 from McMullen Booth Road in Clearwater to the intersection with Veterans Expressway in Tampa as a Florida Scenic Highway on May 23, 2005. A part of this designation effort included the formation of the Courtney Campbell Causeway Scenic Highway Corridor Advisory Committee. This group established a *Corridor Management Plan (CMP)* that included a series of Goals, Objectives, and Implementation Strategies for the Corridor. One of the primary goals of this plan was to establish bicycle/pedestrian connectivity between Clearwater and Tampa. Thus, the proposed multi-use trail is consistent with the goals of this scenic highway *CMP*.

This nine-mile scenic highway corridor is unique since it already contains the intrinsic resources necessary for a viable Scenic Highway. Two beaches, a boat launch facility, and a series of intermittent service roads generally running parallel to the highway provide

recreational opportunities throughout the corridor. The service roads provide access to fishing, bicycling, jogging, picnicking and similar activities.

With respect to landscaping, agreements with the Cities of Tampa and Clearwater ensure the landscaping is maintained by these two local agencies, according to the Scenic Highway Final Designation Application and *CMP*.

Based on the scenic highway designation, a Level Two aesthetics program is proposed for the bridges as per the FDOT's PPM (section 26.9.4). Additionally, the proposed handrail to be mounted atop the existing seawall may be made of an aluminum material to enhance the aesthetics. Both of these elements will be defined during the design phase.

The FDOT coordinated with the Scenic Highway Committee. As a result, FDOT prepared a graphically enhanced photo image shown in **Figure 4-3** with a perspective of the proposed trail from the perspective of a motorist on SR 60.

4.13 Traffic Control Plan

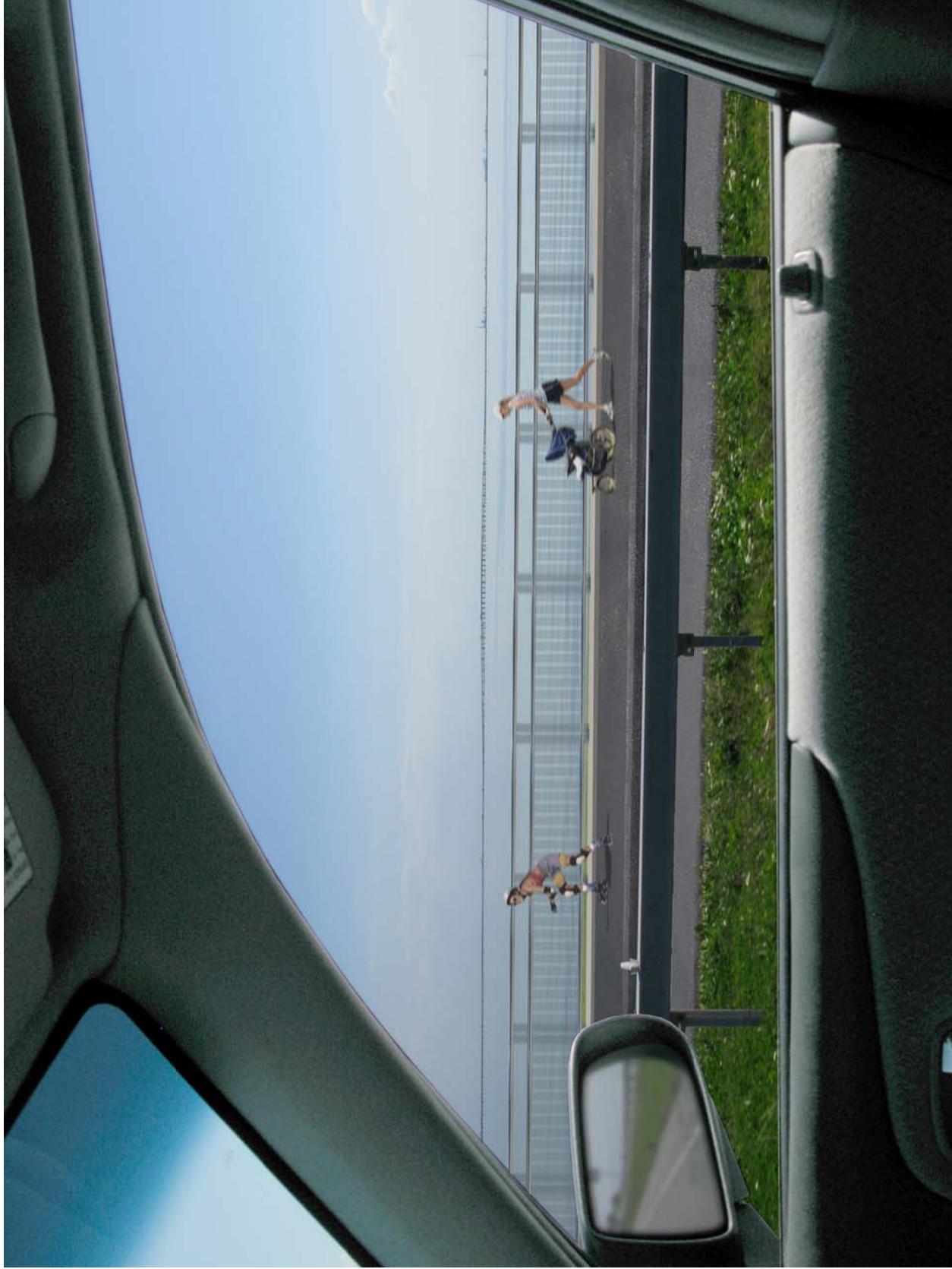
Traffic will be maintained at all times along SR 60 during construction of the project. There will be no lane reductions necessary to construct the proposed trail. Construction vehicles and equipment will access the project area via SR 60. There may be construction activities in close proximity to the SR 60 roadway where the entry points to the service road are adjusted.

4.14 Value Engineering

Not Applicable

4.15 Production Schedule

The current project production schedule is shown in **Table 4-1**. The future design and construction phases for this project are currently funded in the FDOT's Tentative 5-year work program covering fiscal years 2012-2016. As of the date of this draft report, this tentative work program has not yet been adopted. This tentative work program indicates the design and construction phases of this project are anticipated to be split into 3 segments. The eastern segment (Hillsborough County) is being programmed for a design-build project which would combine the design and construction phases. The two segments in Pinellas County are programmed for separate design and construction phases. As the construction of these three segments is anticipated to not be performed simultaneously, the future design phases will address any construction sequencing between the segments related to construction details of the proposed trail and the existing maintenance access road.



SR 60 Multi-Use Trail PD&E Study
Bayshore Blvd to West of Ben T. Davis Beach Entrance
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**Enhanced Photo-image of Trail
from SR 60**

Figure 4-3

Table 4-1 Work Program Schedule

Activity in FDOT Tentative 5-Year Work Program (FY 2012-2016)

PROJECT PHASE	(FISCAL YEAR) ¹		
PD&E Study	2011 (this PD&E Study)		
Design	2012	2012	2012 ²
ROW Acquisition	Not Applicable	Not Applicable	Not Applicable
Construction	2016	2014	2012 ²
County:	Pinellas		Hillsborough
WPI Segment No:	424561-3	424561-4	424561-5
Limits:	Bayshore to east of Structure 1	East of Structure 1 to Pin/Hills County Line	Pin/Hills County Line to west of Ben T. Davis Beach entrance

¹ FY denotes Fiscal Year which spans from July 1 to June 30 (i.e. FY 2011 = 7/1/2010 to 6/30/2011)

² WPI Segment 424561-5 planned as a design-build project

4.16 Project Cost Estimates

Current cost estimates for the preferred build alternative are shown in **Table 4-2**, based on 2011 dollars. Cost estimates are based on the Departments Long Range (Cost) Estimate (LRE) system, as of January 2011. No land acquisition will be required due to this project, so no costs for land acquisition are included.

Table 4-2 Preferred Alternative Project Costs

Cost Category	Total Project Costs ¹ (\$'s)
Construction Costs	
Roadway and Bridges ²	\$ 17,600,000
Stormwater Facilities	\$ 0.0
Total Construction Costs	\$ 17,600,000
Engineering Design ³	\$ 2,100,000
Construction Engineering & Inspection ³	\$ 2,100,000
Wetlands Mitigation	TBD
Preliminary Estimate of Total Costs	\$ 21,800,000

Notes: ¹ Present day costs rounded to nearest \$100,000

² Based on January 2011 Work Program Estimates

³ Estimated at 12% of construction costs

4.17 Design Exceptions & Variations

No design exceptions or variations are anticipated on this project. During the design phase, the need to obtain variations or exceptions will be evaluated using FDOT's *Plans Preparation Manual* (Volume 1, Section 8.6.2)

Section 5 - SUMMARY OF ENVIRONMENTAL EFFECTS

This section documents the environmental impacts of the proposed project. The project was evaluated through the FDOT's ETDM process under ETDM Project No. 13102. **Table 5-1** provides the evaluation Degree of Effect and corresponding definitions used to evaluate the environmental impacts. As part of the screening process, the FDOT initiated the Advance Notification (AN) process with the Florida Clearinghouse using the EST.

Table 5-1 Degree of Effect - Programming Screen

Color Code	Degree of Effect	Definition	
		ETAT	Public Involvement
Purple	Not Applicable/ No Involvement	There is no presence of the issue in relationship to the project, or the issue is irrelevant in relationship to the proposed transportation action.	
Light Blue	None (after 12/5/2005)	The issue is present, but the project will have no impact on the issue; project has no adverse effect on ETAT resources; permit issuance or consultation involves routine interaction with the agency. The <i>None</i> degree of effect level is new as of 12/5/2005.	No community opposition to the planned project. No adverse effect on the community
Blue	Enhanced	Project has positive effect on the ETAT resource or can reverse a previous adverse effect leading to environmental improvement.	Affected community supports the proposed project. Project has positive effect.
Green	Minimal (after 12/5/2005)	Project has little adverse effect on ETAT resources. Permit issuance or consultation involves routine interaction with the agency. Log-cost options are available to address concerns.	Minimum community opposition to the planned project. Minimum adverse effect on the community.
Yellow	Moderate	Agency resources are affected by the proposed project, but avoidance and minimization options are available and can be addressed during development with a moderate amount of agency involvement and moderate cost impact.	Project has adverse effect on elements of the affected community. Public involvement is needed to seek alternatives more acceptable to the community. Moderate community interaction will be required during Project Development.
Orange	Substantial	The project has substantial adverse effects, but ETAT understands the project need and will be able to seek avoidance and minimization or mitigation options during Project Development. Substantial interaction will be required during Project Development and permitting.	Project has substantial adverse effects on the community and faces substantial community opposition. Intensive community interaction with focused Public Involvement will be required during Project Development to address community concerns.
Red	Dispute Resolution (Programming)	Project does not conform to agency statutory requirements and will not be permitted. Dispute resolution is required before the project proceeds to programming.	Community strongly opposes the project. Project is not in conformity with local comprehensive plan and has severe negative impact on the affected community.
	No ETAT Consensus	ETAT members from different agencies assigned a different degree of effect to this project, and the ETDM coordinator has not assigned a summary degree of effect.	
	No ETAT Reviews	No ETAT members have reviewed the corresponding issue for this project, and the ETDM coordinator has not assigned a summary degree of effect.	

The Degree of Effect shown below for each environmental category is based on the ETDM Programming Screen. Supplemental information is provided based upon the environmental analysis performed as part of this study to substantiate the impact designation noted on the Summary of Environmental Impacts Checklist at the front of this report.

5.1 Natural Environment

5.1.1 Air Quality

The ETDM Programming Screen Summary Report documented the following Degrees of Effect assigned by agencies along with a Summary Degree of Effect assigned by FDOT:

Agency	Degree of Effect
USEPA	Minimal
Summary Degree of Effect	
Final Summary Degree of Effect	Minimal

The USEPA noted that they do not anticipate any negative air quality impacts related specifically to the project.

There will be no capacity or widening improvements associated with this project. This is a recreational trail project with a purpose to provide a separate facility for non-motorized transportation within the transportation right of way. As a Transportation Enhancement project, this proposed trail is expected to have a positive benefit on local air quality by providing a safe option for non-motorized travel. Therefore on the Summary of Environmental Impacts Checklist, this category has been designated as NO INVOLVEMENT.

5.1.2 Coastal & Marine

The ETDM Programming Screen Summary Report documented the following Degrees of Effect assigned by agencies along with a Summary Degree of Effect assigned by FDOT:

Agency	Degree of Effect
SWFWMD	Moderate
NMFS	Moderate
Summary Degree of Effect	
Final Summary Degree of Effect	Moderate

The NMFS staff conducted a site inspection of the project area on December 20, 2010, to assess potential concerns to living marine resources within Old Tampa Bay and Safety Harbor and concluded that the project could directly impact NMFS trust resources. Some isolated mangroves occur along the causeway's southern shoreline. Seagrass beds occur adjacent to the shoreline at various points along the south side of the causeway. Certain estuarine habitats within the project area are designated as essential fish habitat (EFH) as identified in the 2005 generic amendment of the Fishery Management Plans for the Gulf of Mexico. Mangroves have been identified as EFH for postlarval/juvenile, subadult, and adult red drum and gray snapper, schoolmaster, cubera snapper, yellowtail snapper, dog snapper, and juvenile goliath grouper by the Gulf of Mexico Fishery Management Council under provisions of the Magnuson-Stevens Act. Seagrasses have been identified as EFH for juvenile and subadult penaeid shrimp, juvenile and adult stone crab, postlarval, juvenile, and subadult and adult red drum, juvenile and adult schoolmaster and mutton snapper, and

juvenile gag, goliath grouper, red grouper, black grouper, yellowfin grouper, Nassau grouper, lane snapper, dog snapper, yellowtail snapper, and cubera snapper.

The NMFS requested that an EFH Assessment be prepared for this project. Upon review of the initial EFH Assessment, which is located within the Wetland Evaluation and Biological Assessment Report (WEBAR), the NMFS determined that seagrass surveys shall be conducted prior to construction to determine potential impacts to EFH. NMFS requested that seagrass surveys be conducted during the prime seagrass growing season between late May and October.

The SWFWMD noted that the project occupies watersheds that are included in the Tampa Bay Estuary Watershed designated estuary of national significance. The SWFWMD also noted that while it is intended that the project be constructed within the cross section of existing Causeway fill, it may be necessary to add fill to accommodate the proposed facilities. In that case, elimination/disruption of the mangroves and estuarine vegetation now established along much of the project length on the causeway may occur.

The project will be constructed on fill material that was used to construct the existing Causeway and two new bridges will be constructed to span Old Tampa Bay. There are sensitive marine and estuarine resources located near the project corridor. Since the project will be located on the south side of the Causeway and will be mostly located on the existing fill, there should be minimal impacts to these resources. Avoidance and minimization efforts will be implemented during design. The FDOT will commit to using proper best management practices (BMPs) during construction to avoid or minimize any direct or secondary impacts to coastal and marine resources.

The FDOT prepared a WEBAR during the PD&E study. This report assessed wetlands, potential species and their habitats, and potential essential fish habitat (EFH) within the project area. The project is located along SR 60 (Courtney Campbell Causeway) within Old Tampa Bay. The waters within Pinellas County are part of the Pinellas County Aquatic Preserve and are classified as Outstanding Florida Waters (OFW). There are minimal seagrasses located adjacent to the project. According to recent SWFWMD GIS data and field reviews conducted in January 2011, there are portions of the seagrass beds that are continuous and others that are discontinuous (or patchy). The majority of the project will be located on the existing spoil of the Causeway, landward of the existing seawall and riprap. An area of new construction over the Bay would be the proposed free-standing structures located at the existing bridge locations. Seagrasses are located near the proposed bridge structures. This report and the FHWA's initial findings have been coordinated with the USFWS, NMFS, and FFWCC. In response to comments from the USFWS and NMFS, the FDOT conducted additional seagrass surveys during the growing season (June 2011). As a result, the FDOT adjusted the trail alignment at the eastern end of Structure No. 2 to avoid impacts to existing seagrasses identified in the June 2011 survey. The proposed structures will be located over open water and will have minimal to no

impacts to marine species, and are anticipated to have no impacts to seagrasses. Best management practices will be implemented during construction to prevent impacts to Old Tampa Bay and associated wildlife and marine species. Permitting will be conducted with the appropriate regulatory agencies during design and prior to construction. Therefore, on the Summary of Environmental Impacts Checklist, this category has been designated as NOT SIGNIFICANT.

5.1.3 Contaminated Sites

The ETDM Programming Screen Summary Report documented the following Degrees of Effect assigned by agencies along with a Summary Degree of Effect assigned by FDOT:

Agency	Degree of Effect
SWFWMD	Minimal
USEPA	None
FDEP	None
Summary Degree of Effect	
Final Summary Degree of Effect	Minimal

The City of Clearwater Advanced Waste Water Treatment Plant (AWWTP) and a Sunoco gas station are located outside the western terminus of the project, and both facilities include petroleum storage facilities on-site. Discharges have been reported at each site. The City of Tampa Rocky Point Pump Station was located to the east of Structure 2. This facility contained an underground storage tank (UST), but has been closed since 1994, and the tank was removed. No impacts to the existing facilities are anticipated from the proposed construction.

A *Contamination Screening Evaluation Report* (CSER) was prepared as part of this PD&E study. This report was prepared in accordance with the Federal Highway Administration’s (FHWA) Technical Advisory 26640.8a, dated October 30, 1987, and the FDOT’s *PD&E Manual, Part 2, Chapter 22* (revised January 17, 2008). Three potential contamination sites were identified within the vicinity of the project corridor. Risk rankings were assigned to each potential contamination site after reviewing data obtained from FirstSearch Technology Corporation, regulatory site lists, land uses and an on-site field review conducted in December 2010. Of the three sites evaluated as part of this report, two (2) were assigned a risk ranking of “No” and one (1) was assigned a risk ranking of “Low”. No sites were assigned a risk ranking of “Medium” or “High” for potential contamination impacts associated with the construction of the recommended alternative. No additional assessment of these sites is recommended during the design phase of this project unless changes are made to the design that could potentially impact these facilities. Therefore on the Summary of Environmental Impacts Checklist, this category has been designated as NOT SIGNIFICANT.

5.1.4 Farmlands

The ETDM Programming Screen Summary Report documented the following Degrees of Effect assigned by agencies along with a Summary Degree of Effect assigned by FDOT:

Agency	Degree of Effect
NRCS	None
Summary Degree of Effect	
Final Summary Degree of Effect	None

A review of the Geographical Information Systems (GIS) analysis data and NRCS comments indicates that there are no Prime Farmlands, Farmlands of Unique Importance, or Farmlands of Local Importance are within the 5,280-foot buffer distance. This project will not result in any impacts to farmlands. Therefore on the Summary of Environmental Impacts Checklist, this category has been designated as NO INVOLVEMENT.

5.1.5 Floodplains

The ETDM Programming Screen Summary Report documented the following Degrees of Effect assigned by agencies along with a Summary Degree of Effect assigned by FDOT:

Agency	Degree of Effect
USEPA	Minimal
SWFWMD	Minimal
Summary Degree of Effect	
Final Summary Degree of Effect	Minimal

A review of the Geographical Information Systems (GIS) analysis data indicates that the project is located within Coastal Flood Zone VE, which is tidally influenced and is a Special Flood Hazard Area.

Following the ETDM Programming Screen, the FDOT prepared a *Location Hydraulic Memorandum (LHM)* to address base floodplain encroachments and evaluated the impacts of the proposed improvements on each floodplain in accordance with Chapter 24 of the FDOT's *PD&E Manual*.

Minimal to no fill will be required for the trail, with the exception of the pilings for the construction of the bridges. Fill will be needed for the construction of the bridge approaches. Since the flood zones are influenced tidally and by storm surge, no floodplain compensation will be required for this project. The FDOT will adhere to SWFWMD criteria and permitting requirements during design and construction. Therefore on the Summary of Environmental Impacts Checklist, this category has been designated as NOT SIGNIFICANT.

5.1.6 Infrastructure

The ETDM Programming Screen Summary Report documented the following Degrees of Effect assigned by agencies along with a Summary Degree of Effect assigned by FDOT:

Agency	Degree of Effect
SWFWMD	None
Summary Degree of Effect	
Final Summary Degree of Effect	None

A review of the GIS analysis data indicates that no existing infrastructure was identified within the project limits.

Several existing utilities have been identified as being located within the project limits along the causeway. **Section 4.11** details information related to *Utilities and Lighting*. However, no utility or lighting impacts are expected as a result of the project. Therefore on the Summary of Environmental Impacts Checklist, this category has been designated as NONE.

5.1.7 Navigation

The ETDM Programming Screen Summary Report documented the following Degrees of Effect assigned by agencies along with a Summary Degree of Effect assigned by FDOT:

Agency	Degree of Effect
USACE	N/A No Involvement
USCG	Moderate
Summary Degree of Effect	
Final Summary Degree of Effect	Moderate

The project is located within waters that are considered to be navigable, tidal, Section 10 waters of the United States. The USACE noted that it does not have regulatory authority over this project. The USCG noted that a Coast Guard Bridge Permit will be acquired during design and permitting of the project. The proposed trail bridges are intended to at least match the existing horizontal and vertical clearances of the adjacent SR 60 highway bridges. The proposed bridges will at a minimum meet the existing horizontal and vertical clearances of the adjacent existing SR 60 bridges. Additional coordination will be conducted with the U.S. Coast Guard during the project’s design or design/build phase.

Consideration was given such that the build option does not impede the passage of vessels currently traveling beneath Structures 1 and 2. Any build option chosen will provide at least the existing horizontal and vertical clearances. Therefore on the Summary of Environmental Impacts Checklist, this category has been designated as NOT SIGNIFICANT.

5.1.8 Special Designations

The ETDM Programming Screen Summary Report documented the following Degrees of Effect assigned by agencies along with a Summary Degree of Effect assigned by FDOT:

Agency	Degree of Effect
SWFWMD	Moderate
USEPA	Moderate
Summary Degree of Effect	
Final Summary Degree of Effect	Moderate

A review of the GIS analysis data indicates that Public Land known as Cooper’s Point is located within the 500-foot buffer distance. The western portion of the project is located within the Pinellas County Aquatic Preserve which is an Outstanding Florida Water (OFW); however, the project will be constructed within SR 60’s right of way (ROW) that is designated for transportation purposes. No fill material will be placed within the Pinellas County Aquatic Preserve with the exception of the construction of Structure 1. This project is in the public’s interest since it provides recreational opportunities for non-motorized users to enjoy this FDOT designated Scenic Highway. Also, please see Special Flood Hazard Areas and Mangroves information in the Floodplain and Coastal and Marine DOEs, respectively.

The SWFWMD stated that Tampa Bay is one of the Priority Waterbodies in the SWFWMD’s Surface Water Improvement and Management (SWIM) program. The SWFWMD also noted that estuarine habitats within the project area, including mangroves and seagrass beds, are designated as essential fish habitat for numerous juvenile, sub-adult and adult fish species. The project is located within Class II waters designated for shellfish propagation or harvesting. Designated areas for bird nesting are located on the north side of the Causeway. The project will be located on the south side of the Causeway on existing fill, with the exception of the proposed bridges. The project is a proposed trail for non-motorized users and involves no roadway capacity improvements so there will be no additional pollutants that have the potential to runoff into the Bay. Proper best management practices will be implemented during construction to prevent sedimentation, erosion, or turbidity in the Bay. Therefore on the Summary of Environmental Impacts Checklist, this category has been designated as NOT SIGNIFICANT.

5.1.9 Water Quality/Quantity

The ETDM Programming Screen Summary Report documented the following Degrees of Effect assigned by agencies along with a Summary Degree of Effect assigned by FDOT:

Agency	Degree of Effect
USEPA	Moderate
SWFWMD	Moderate
FDEP	Minimal
Summary Degree of Effect	
Final Summary Degree of Effect	Moderate

A review of the GIS analysis data

indicates that the project is located within portions of the Pinellas County Aquatic Preserve which is an OFW. The current list of 303(d) Verified List of Impaired Waters states that surrounding waters are listed for nutrients, fecal coliforms/bacteria, and mercury in fish. The project consists of a non-motorized trail that should not contribute to degradation of the surrounding waters. Trail users, such as bicyclists and pedestrians, would not generate the release of any oils, greases or other pollutants that could enter the Bay from this type of activity. The construction of the proposed project is not anticipated to contribute to increases in pollutant loads within the Bay.

The SWFWMD noted that the project occupies Old Tampa Bay and Courtney Campbell Beach coastal watersheds and the entire project is located in Class II waters designated for Shellfish Propagation or Harvesting; commercial crabbing occurs in Old Tampa Bay.

On June 23, 2011, an ERP pre-application meeting was held at SWFWMD with FDOT staff preparing the design-build criteria package for Segment 3. Minutes from the meeting have not been released. The FDOT will implement proper best management practice (BMPs) during construction to ensure there are no violations to water quality standards. Therefore on the Summary of Environmental Impacts Checklist, this category has been designated as NOT SIGNIFICANT.

5.1.10 Wetlands

The ETDM Programming Screen Summary Report documented the following Degrees of Effect assigned by agencies along with a Summary Degree of Effect assigned by FDOT:

Agency	Degree of Effect
USEPA	Moderate
SWFWMD	Moderate
USACE	N/A No involvement
USFWS	Substantial
NMFS	Moderate
FDEP	Moderate
Summary Degree of Effect	
Final Summary Degree of Effect	Substantial

The USEPA noted that mitigation to provide enhanced or increased function of mangroves should be evaluated within the project area and the PD&E

Study should identify wetland areas to be potentially impacted by the project.

The USFWS noted that with proper design and the right materials, the trail could have minimal impacts to wetlands, wildlife, and the natural environment. The FDEP noted that an ERP permit will be required from the SWFWMD for this project.

The entire project, with the exception of the two proposed trail bridges, will be constructed on the existing fill section that was used to construct the Causeway. The proposed recommended build alternative is located on the south side of the Causeway. Isolated mangroves (mainly white mangroves) are located on the south side of the Causeway waterward of the existing seawall in the riprap. During the initial findings, the proposed

bridges had the potential to impact seagrass within limited areas on the eastern end of each bridge. Mangroves and seagrasses provide habitat for numerous fish and wildlife for feeding, breeding, and nesting.

The FDOT prepared a Wetlands Evaluation and Biological Assessment Report (WEBAR) as part of the PD&E study. The WEBAR assessed existing wetlands and seagrass within the project limits. Permitting will be conducted with the appropriate regulatory agencies during design and prior to construction. The FDOT will take measures to minimize and/or avoid impacts to wetlands. There are mangroves and seagrasses located within and/or adjacent to the project corridor. The mangroves within the corridor are sparse and are located mainly within the existing riprap areas. The seagrasses are located adjacent to much of the project corridor. According to recent SWFWMD Geographical Information Systems (GIS) data and field reviews conducted in January 2011, there are portions of the seagrass beds that are continuous and others that are discontinuous (or patchy). The majority of the project will be located on the existing spoil of the Causeway, landward of the existing seawall and riprap. Areas for new construction over the Bay would be the proposed free-standing structures located adjacent to the existing bridge locations. Minimal seagrasses are located near the proposed bridge structures. As requested by USFWS and NMFS, additional seagrass surveys were conducted during the growing season (June 2011). It is anticipated the proposed structures will be located to avoid seagrass impacts. Best management practices will be implemented during construction to prevent impacts to Old Tampa Bay and associated wildlife and marine species. Any impacts to mangroves and/or seagrasses will require mitigation. Mitigation for mangrove impacts will be provided using the Senate Bill (373.4137, Florida Statute (F.S.)). A detailed mitigation plan would need to be developed during the design phase for impacts to seagrasses if applicable; however, no impacts to seagrasses are anticipated by the construction of the proposed project. Therefore, on the Summary of Environmental Impacts Checklist, this category has been designated as NOT SIGNIFICANT.

5.1.11 Wildlife & Habitat

The ETDM Programming Screen Summary Report documented the following Degrees of Effect assigned by agencies along with a Summary Degree of Effect assigned by FDOT:

Agency	Degree of Effect
SWFWMD	Minimal
USFWS	Substantial
FFWCC	Substantial
Summary Degree of Effect	
Final Summary Degree of Effect	Substantial

The USFWS recommended that the trail be constructed of permeable material along the causeway rather than asphalt. The USFWS recommends against using asphalt in natural areas and areas where erosion will be a constant problem. The USFWS noted that with proper design and the right materials, the trail could have minimal impacts to wetlands, wildlife, and the natural environment. The USFWS also noted that the western half of the

project is within the Pinellas County Aquatic Preserve and if so, interpretive signage could be added to inform the public about this preserve and the role that preservation serves in our environment. The USFWS also recommended removal of the Friendship Trail Bridge on Gandy Boulevard and any habitat restoration that might be needed as a result of the old bridge and removal of the bridge could be considered a possible mitigation option, if feasible.

The FFWCC recommended land acquisition and restoration of appropriate tracts adjacent to existing public lands near the project area or tracts placed under conservation easement or located adjacent to large areas of jurisdictional wetlands that currently serve as regional core foraging habitat areas.

The recommended build alternative is along the south side of the Causeway where there are minimal to no wetlands with isolated mangroves that are likely to be located within the project's limits of construction. The entire trail, with the exception of the proposed bridges, will be constructed on the existing fill section of the causeway. The FDOT will use proper best management practices (BMPs) during construction and will implement a wildlife watch plan, which includes the FFWCC "*Standard Manatee Conditions for In-Water Work*". The FDOT will require the construction contractor to abide by these guidelines during construction. No USFWS Critical Habitat is documented within the project area. Portions of the Causeway where the proposed trail will be located are currently utilized by motor vehicles, pedestrians and other recreational users. The existing beach areas are susceptible to high pedestrian and vehicular traffic throughout much of the year. There will be no land use changes as a result of the construction of the proposed trail. The project will be constructed within current FDOT transportation right of way (ROW).

The FDOT prepared a Wetland Evaluation and Biological Assessment Report (WEBAR) during the PD&E study. This report assessed potential species, existing habitat, and potential essential fish habitat (EFH) within the project area. This report and the FDOT's findings have been coordinated with the USFWS, NMFS, and FFWCC. Response letters for the WEBAR were received from USFWS, NMFS, and FFWCC on March 24, 2011, March 9, 2011, and April 18, 2011, respectively. These letters are attached in **Appendix B**. USFWS concurred with the determination of effect for sea turtles. Concurrence was not provided for determinations of effect for the West Indian manatee, wood stork, and migratory and shore birds based on further information needed to make determinations. NMFS requested that a smalltooth sawfish section be added to the WEBAR and the NMFS's Sea Turtle and Smalltooth Sawfish Construction Conditions be implemented as part of the commitments. FFWCC concurred with the finding stated in the WEBAR. Continued coordination was conducted with USFWS and NMFS throughout the PD&E study. After review of additional information provided to these agencies and results of seagrass surveys that were conducted in June 2011, final concurrence was received via email from NMFS on June 17, 2011, and via letter from USFWS on June 21, 2011. This correspondence is also attached in **Appendix B**.

There is minimal wildlife habitat located along the project corridor. The majority of the project limits consists of an existing access/frontage road with some areas of open green space. There are sparse mangroves located within the riprap along the edges of the Causeway. These mangroves provide minimal to no habitat for wildlife. Seagrasses are located along the project corridor, although no impacts to seagrasses are anticipated from the proposed project. No bald eagle nests were identified within the project corridor during field reviews; however, the project area and any areas within 660 feet of the limits of construction will be surveyed during permitting and design. USFWS Monitoring Guidelines shall be followed if any nests are observed within the project corridor during design. Therefore on the Summary of Environmental Impacts Checklist, this category has been designated as NOT SIGNIFICANT.

5.2 Cultural Impacts

5.2.1 Historical/Archaeological

The ETDM Programming Screen Summary Report documented the following Degrees of Effect assigned by agencies along with a Summary Degree of Effect assigned by FDOT:

Agency	Degree of Effect
SWFWMD	N/A No involvement
FHWA	Minimal
Seminole Tribe of FL	Moderate
FDOS	Moderate
Miccosukee Tribe	Minimal
Summary Degree of Effect	
Final Summary Degree of Effect	Moderate

The FHWA, Seminole Tribe of Florida, SHPO, and Miccosukee Tribe of Indians of Florida recommended that a Cultural Resource Assessment Survey (CRAS) for archaeology and historic structures be prepared for this project. The SHPO also recommended that the CRAS include appropriate underwater survey to identify, document, and evaluate any submerged cultural resources. The Seminole Tribe of Florida Tribal Historic Preservation Officer (STOF-THPO) noted that they would like to review a CRAS before commenting on direct effects to archaeological sites in the project area. The Miccosukee Tribe of Indians of Florida commented that there are no recorded archaeological sites, including burial mounds, reported near this project; a CRAS will need to be done to ascertain if there are any archaeological sites within the project boundaries. If no impacts are found, then no further consultation is necessary.

A CRAS was prepared as part of this PD&E study. This report documents the results of background research and historical/architectural field survey; the scope of work for this project did not include archaeological field survey. With minor exception, the project corridor is comprised of dredged fill, and considered to have a low potential for in situ archaeological sites. The project Area of Potential Effect (APE) for the historical/architectural survey was defined as the existing SR 60 right of way (ROW) and adjacent properties. This CRAS was performed in December 2010.

Background research indicated that one previously recorded archaeological site, the Ben Davis Municipal Beach Site (8HI456), is located within the project APE, at its eastern terminus. This site is comprised of redeposited dredged fill, and is not considered eligible for listing in the National Register of Historic Places (NRHP). Historical/architectural field survey resulted in the identification of one historic resource, a ca. (circa) 1957 Masonry Vernacular style building (8PI11966). It is not considered eligible for listing in the NRHP due to the commonality of type, lack of significant historical associations, and alterations. Thus, no archaeological sites or historic resources which are currently listed, determined eligible, or considered potentially eligible for listing in the NRHP are located within the SR 60 Multi-Use Trail PD&E study project APE.

The Florida Department of State, Division of Historical Resources (SHPO) determined in a letter dated April 27, 2011 (**Appendix B**), that the proposed project will have no adverse effect on any resources on or eligible for inclusion in the NRHP. No further work is recommended. Therefore on the Summary of Environmental Impacts Checklist, this category has been designated as NOT SIGNIFICANT.

5.2.2 Recreation Areas

The ETDM Programming Screen Summary Report documented the following Degrees of Effect assigned by agencies along with a Summary Degree of Effect assigned by FDOT:

Agency	Degree of Effect
USEPA	None
SWFWMD	None
FDEP	Enhanced
Summary Degree of Effect	
Final Summary Degree of Effect	Enhanced

The project will be constructed within current FDOT transportation right-of-way (ROW). Ben T. Davis Beach is located east of the project study area. There will be no impacts to this facility with the construction of the proposed project. The proposed trail will not hinder current uses at Ben T. Davis beach. The proposed trail will improve access to these areas for people using non-motorized means of transportation. The proposed trail will provide improved recreational opportunities along the Causeway, including fishing, biking, hiking, and observation of wildlife within the area. This project is also a component in connecting already existing trails in Pinellas County to trails in Hillsborough County and throughout the Tampa Bay region. No impacts to any recreation resources at the beaches, to boat traffic under the bridge structures, or on the north side of the causeway including the existing boat ramp would occur due to construction of the trail. During trail construction, the public may experience limited access to areas of active construction. The project is divided into three construction segments. It is currently planned that the segments will be under construction at different times, so access to other segments are not anticipated to be limited. Therefore on the Summary of Environmental Impacts Checklist, this category has been designated as NONE.

5.2.3 Section 4(f) Potential

The ETDM Programming Screen Summary Report documented the following Degrees of Effect assigned by agencies along with a Summary Degree of Effect assigned by FDOT:

Agency	Degree of Effect
FHWA	None
Summary Degree of Effect	
Final Summary Degree of Effect	None

Based on the Environmental Screening Tool's (EST) GIS screening results, the only potential Section 4(f) resources within the project study limits are the Pinellas County Aquatic Preserve/ Outstanding Florida Waters (AP/OFW) and Ben T. Davis Beach. This recreational trail project would not permanently require nor incorporate any ROW or permanent easement from the AP/OFW or Ben T. Davis Beach resources. The project would be entirely constructed and maintained within the existing transportation ROW that the State of Florida owns and manages for transportation purposes. The project would not cause any proximity impacts that would permanently impair or diminish these resources' attributes which qualify them for protection under the provisions of Section 4(f). With respect to the AP/OFW resource, all construction activities are planned to occur with the existing transportation ROW which is generally ¼ mile in width on either side of the SR 60 causeway. No project construction activities are planned to occur within the Ben T. Davis Beach resource either.

Recreational opportunities within these resources will not be temporarily or permanently affected by either the construction of the project or operation of the facility for its intended purpose. There are no water based recreational trails that are officially designated, marked or signed as such either within, along or perpendicular (intersecting) to the project's study limits. Access to navigational activities within the OFW will be maintained during the project's construction as it is expected that this provision would be a condition of the USCG permits that would be required to construct the westernmost SR 60 relief structure which is within the OFW. The construction of the SR 60 main span over Old Tampa Bay will not occur within the OFW since this structure is located in Hillsborough County. It is likely that this recreational project would enhance the use of the resource by improving access to it.

There is an unofficially designated Courtney Campbell Trail that is actually a service road system that is used to maintain the SR 60 transportation ROW. There are only incidental or secondary uses of this service road system for recreational activities.

The ETDM metadata and its use in generating what resources are "found" within the EST GIS buffers indicate that there are statewide (typically land based) Ecological Greenways Critical Linkages and Greenways Ecological Priority Linkages that could be associated with the proposed project. These FDEP designations contain all of the largest areas of ecological and natural resource significance and the landscape linkages necessary to link these areas together in one functional statewide network. This data was created as part of the Florida

Statewide Greenways Planning Process. The Florida Ecological Greenways Network identifies the opportunities to protect large, intact landscapes important for conserving Florida's biodiversity and ecosystem services.

There are no FDEP designated Ecological Greenways Critical Linkages and Greenways Ecological Priority Linkages that are officially designated, marked or signed as such either within, along or perpendicular (intersecting) to the project’s study limits.

The ETDM metadata and its use in generating what resources are “found” within the EST GIS buffers indicate that there are Paddling Trails Priorities that could be associated with the proposed project. This dataset contains prioritized paddling trail opportunities from the Office of Greenways and Trails Prioritization Project. The areas shown in this layer are intended to identify opportunity corridors of statewide and regional significance. These corridors are 4 kilometers (approx. 2.5 miles) wide to reflect the variability of actual trail location after planning and design is completed. This GIS layer was created by the Office of Greenways of Trails and the University of Florida GeoPlan Center, to support the Florida Statewide Greenways & Trails System.

There are no FDEP designated Paddling Trails Priority locations that are officially designated, marked or signed as such either within, along or perpendicular (intersecting) to the project’s study limits. Since the project location is situated within the open waters of Upper Tampa Bay, it would be expected that no officially designated recreational paddling opportunity would be identified for this area’s open waters due to the susceptibility of the waters becoming rough due to weather or tidal changes.

Since the construction and maintenance of the proposed project will occur within the existing highway right of way, this project would not involve any Section 4(f) uses. This proposed project is considered exempt from the requirements of Section 4(f) since the existing/proposed trail is located entirely within the existing transportation facility’s right of way. Therefore on the Summary of Environmental Impacts Checklist, this category has been designated as NONE.

5.3 Community Impacts

5.3.1 Aesthetics

The ETDM Programming Screen Summary Report documented the following Degrees of Effect assigned by agencies along with a Summary Degree of Effect assigned by FDOT:

Agency	Degree of Effect
No ETAT Reviews	
Summary Degree of Effect	
Final Summary Degree of Effect	Minimal

The Courtney Campbell Causeway (SR 60) was designated as a Scenic Highway in 2005 by the FDOT. The trail will be designed and constructed to consider safety of trail users while

minimizing any impedance to views along the corridor. The construction of the trail is consistent with the Courtney Campbell Causeway Scenic Highway Corridor Management Plan as stated in Section 2, Goal 2(b)(i). The objective of this goal is to improve bicycle and pedestrian safety by working with FDOT, MPOs, and local governments to develop a continuous bicycle/pedestrian trail parallel to the main roadway to avoid auto traffic conflicts. Additional information is provided in **Section 4.12** related to Aesthetics and Landscaping. Therefore on the Summary of Environmental Impacts Checklist, this category has been designated as NONE.

5.3.2 Economic

The ETDM Programming Screen Summary Report documented the following Degrees of Effect assigned by agencies along with a Summary Degree of Effect assigned by FDOT:

Agency	Degree of Effect
No ETAT Reviews	
Summary Degree of Effect	
Final Summary Degree of Effect	Enhanced

The proposed multi-use trail will increase ecotourism in the Tampa Bay region. The trail will serve as an east-west connection between Pinellas and Hillsborough counties, providing a link in a regional network of trail systems serving the Tampa Bay area. This trail will enhance regional connectivity by providing alternative modes of transportation. The trail provides increased recreational opportunities along the SR 60 corridor allowing more users to enjoy the scenic qualities of the Causeway, further enhancing tourism and economic.

This project has been developed in accordance with the Civil Rights Act of 1964, as amended by the Civil Rights Act of 1968, along with Title VI of the Civil Rights Act, Executive Order 12898 (Environmental Justice), which ensures that minority and/or low-income households are neither disproportionately adversely impacted by major transportation projects, nor denied reasonable access to them by excessive costs or physical barriers (Environmental Protection Agency [EPA], 1994). Therefore on the Summary of Environmental Impacts Checklist, this category has been designated as NONE.

5.3.3 Land Use

The ETDM Programming Screen Summary Report documented the following Degrees of Effect assigned by agencies along with a Summary Degree of Effect assigned by FDOT:

Agency	Degree of Effect
FDCA	Enhanced
Summary Degree of Effect	
Final Summary Degree of Effect	Enhanced

The trail will be located within right of way (ROW) designated for transportation purposes. The trail, with the exception of the two proposed bridges over Old Tampa Bay, will be

constructed on existing fill material used to construct the Causeway. No changes to land use should occur as a result of the construction of the proposed trail.

The trail is consistent with the Comprehensive Plans for Hillsborough County, Pinellas County, City of Tampa, and City of Clearwater. . The FDCA noted the goals, objectives, and policies of these plans that are furthered by the proposed project. The trail has also been identified in the City of Tampa Greenways & Trails Master Plan (2001), the City of Clearwater Bikeways and Trails Plan (1996) and Shifting Gears: Clearwater’s Bicycle and Pedestrian Master Plan (2007). The construction of the trail is consistent with the Courtney Campbell Causeway Scenic Highway Corridor Management Plan (CMP) as stated in Section 2, Goal 2(b)(i). The objective of this goal is to improve bicycle and pedestrian safety by working with FDOT, MPOs, and local governments to develop a continuous bicycle/pedestrian trail parallel to the main roadway to avoid auto traffic conflicts. The trail provides alternative, non-motorized, means of transportation in the region. Therefore on the Summary of Environmental Impacts Checklist, this category has been designated as NONE.

5.3.4 Mobility

The ETDM Programming Screen Summary Report documented the following Degrees of Effect assigned by agencies along with a Summary Degree of Effect assigned by FDOT:

Agency	Degree of Effect
FDCA	Enhanced
Hillsborough MPO	Enhanced
Summary Degree of Effect	
Final Summary Degree of Effect	Enhanced

As a needed east-west link, the construction of the trail would provide regional connection between Pinellas and Hillsborough counties and other areas within the Tampa Bay region.

The proposed trail will provide regional linkage for non-motorized travel between Pinellas and Hillsborough Counties and, with connection to other facilities, travel into Pasco and Hernando Counties. The project will connect to other existing and planned facilities to the east and west of the Causeway. On the Pinellas (west) side, the project will connect to Pinellas County's extensive trail system (proposed Bayshore Trail extension). On the Hillsborough (east) side, the trail will connect to the West Tampa Greenway (4.6 miles of this 16.6 miles Greenway is completed to date) which will eventually connect via on-street facilities to the Upper Tampa Bay Trail and then from there to the Suncoast Parkway Trail into Pasco and Hernando Counties.

There are express and local bus routes that operate along SR 60 (Courtney Campbell Causeway) and that intersect SR 60 near the proposed project area. The Hillsborough Area Regional Transit (HART) 200X route is a commuter express route that operates between downtown Tampa and the Eddie Moore Park and Ride Lot in Clearwater. This route only runs during weekday commuter rush hours. Furthermore, HART Route 30 runs near the east

end of the proposed trail, and the Pinellas Suncoast Transit Authority (PSTA) Route 60 runs near the west end of the proposed trail. The combination of the existing transit routes and the proposed trail offers additional connections between Pinellas and Hillsborough Counties. The transit routes also provide additional opportunities for use of the proposed trail.

The Hillsborough County MPO noted that the Courtney Campbell Causeway Trail is the number 2 priority of the Chairs Coordinating Committee for all of West Central Florida. The DCA noted the goals, objectives, and policies of these plans that are furthered by the proposed project. Therefore on the Summary of Environmental Impacts Checklist, this category has been designated as NONE.

5.3.5 Relocation

The ETDM Programming Screen Summary Report documented the following Degrees of Effect assigned by agencies along with a Summary Degree of Effect assigned by FDOT:

Agency	Degree of Effect
No ETAT Reviews	
Summary Degree of Effect	
Final Summary Degree of Effect	None

The project is situated on state right of way for transportation purposes. The proposed project, as presently conceived, will not displace any residences or businesses within the community. Should this change over the course of the project, the Florida Department of Transportation will carry out a Right of Way and relocation program in accordance with Florida Statute 339.09 and the Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970 (Public Law 91-646 as amended by Public Law 100-17). The brochures that describe in detail the department’s relocation assistance program and Right of Way acquisition program are “Your Relocation: Residential,” “Your Relocation: Business, Farms and Nonprofit Organizations,” “Your Relocation: Signs” and “The Real Estate Acquisition Process.” All of these brochures are distributed at all public hearings and made available upon request to any interested persons. Therefore on the Summary of Environmental Impacts Checklist, this category has been designated as NO INVOLVEMENT.

5.3.6 Social/Community Services

The ETDM Programming Screen Summary Report documented the following Degrees of Effect assigned by agencies along with a Summary Degree of Effect assigned by FDOT:

Agency	Degree of Effect
FDCA	Enhanced
USEPA	Minimal
Summary Degree of Effect	
Final Summary Degree of Effect	Minimal

The FDEP noted that the FDOT conducted a Feasibility Study for this project in 2008. During a public workshop held in May 2008, 23 public comments were received and 21 of these comments indicated support for the project. The FDOT coordinated with local agencies, groups, and the Courtney Campbell Causeway Scenic Highway during the Feasibility Process to seek input. Additional public coordination has continued throughout the PD&E study. The project will provide alternative modes of transportation between Pinellas and Hillsborough counties and throughout the Tampa Bay region.

The USEPA noted support for alternative modes of transportation and recommended that any negative direct or indirect impacts be avoided or minimized to the best extent practicable. The FDCA noted the goals, objectives, and policies of these plans that are furthered by the proposed project.

This project has been developed in accordance with the Civil Rights Act of 1964, as amended by the Civil Rights Act of 1968, along with Title VI of the Civil Rights Act, Executive Order 12898 (Environmental Justice), which ensures that minority and/or low-income households are neither disproportionately adversely impacted by major transportation projects, nor denied reasonable access to them by excessive costs or physical barriers (Environmental Protection Agency [EPA], 1994). Residential and commercial services are located at each end of the Causeway. The Ben T. Davis Beach is located east of the project along the Causeway and is a signature attraction for the Tampa Parks and Recreation Department. The Ben T. Davis Beach is a popular destination for recreational users and offers many areas for fishing and picnicking. The proposed trail will accommodate a greater diversity of users including recreational cyclists, families with children, and other non-motorized users. Therefore on the Summary of Environmental Impacts Checklist, this category has been designated as NONE.

5.4 Other Impacts

5.4.1 Noise

Noise was not a screening element for this project in the ETDM process.

This project involves no capacity improvements or lane shifts along SR 60 so no noise impacts will occur from the proposed project. Therefore on the Summary of Environmental Impacts Checklist, this category has been designated as NO INVOLVEMENT.

5.4.2 Construction

Construction was not a screening element for this project in the ETDM process.

The construction of the trail will have minimal to no impacts on the community. Other than construction vehicles using SR 60 to access the proposed project areas, the traveling public will not be impacted. No lane closures along SR 60 are expected except during limited time

when equipment needs safe entry to the construction zones or when the existing access road entrance points are under construction. There may be some time when the existing maintenance access road may be closed for construction activities, but this time will be minimal and have very little impact on access to the Causeway. The project is located away from any residential or commercial areas. Since the new bridges will be independent structures and the trail will be located to the outside of the existing guardrail, there will be no disruption to SR 60 or vehicles that use this facility on a regular basis. A USCG permit is anticipated for the bridge structures which will address construction activities related to Tampa Bay boaters. Navigational access under the proposed bridges is anticipated to remain open at all times. Per coordination with USFWS, no night-time bridge work will be authorized over water during construction

With the construction of the trail, no impacts would occur to any recreation resources at the beaches, to boat traffic under the bridge structures, or on the north side of the causeway including the existing boat ramp. During trail construction, the public may experience limited access to areas of active construction.

The project is planned to be divided into three construction segments. As identified on **Table 4-1**, the construction of these three segments is anticipated to not be performed simultaneously. Segment 3 is planned for FY 2012, Segment 2 for FY 2014 and Segment 1 for FY 2016. The future design phases will address any construction sequencing between the segments related to construction details of the proposed trail and the existing maintenance access road. Therefore on the Summary of Environmental Impacts Checklist, this category has been designated as NOT SIGNIFICANT.

5.4.3 Secondary and Cumulative

The ETDM Programming Screen Summary Report documented the following Degrees of Effect assigned by agencies along with a Summary Degree of Effect assigned by FDOT:

Agency	Degree of Effect
SWFWMD	Moderate
Summary Degree of Effect	
Final Summary Degree of Effect	Moderate

This project is consistent with the Comprehensive plans for Hillsborough County, Pinellas County, City of Tampa, and City of Clearwater. Minimal environmental impacts are anticipated since construction of the trail will be conducted on existing fill used to construct the Causeway, with the exception of the two proposed bridges. Many of the areas on the existing Causeway are currently paved for the existing access road. The FDOT commits to using proper best management practices to avoid potential secondary impacts during construction. The proposed trail should not contribute to increased pollutant loading in Old Tampa Bay since this facility will be used for non-motorized transportation.

Section 6 - SUMMARY OF PERMITS & MITIGATION

6.1 Permits

Potential permits may be required from the following agencies:

- United States Army Corps of Engineers (USACE)
- United States Coast Guard (USCG)
- Southwest Florida Water Management District (SWFWMD)
- Tampa Port Authority (TPA)

6.2 Avoidance/Minimization/Mitigation

Mitigation will be provided for any impacts to wetlands and seagrasses for any areas where impacts are unavoidable. Most of the project will be constructed within the limits of the existing seawall along the Causeway. Potential impacts were assessed at the proposed bridge locations. Variations to the typical section were evaluated along the corridor to avoid extending the seawall and causing potential impacts to wetlands or seagrasses.

Section 7 - SUMMARY OF PUBLIC INVOLVEMENT

7.1 Public Involvement Program

A Public Involvement Plan was developed for the project in accordance with FDOT's *PD&E Manual, Part 1, Chapter 8*, and Florida Statutes Sections 120.525 and 399.155. The program identified federal, state, regional and local agencies that have involvement with the project due to jurisdictional review or expressed interest. The program also included coordination with those on the ETDM's ETAT, the formal review committee. A separate *Comments and Coordination Report* was prepared which detailed the public involvement efforts for this project.

The following sections summarize the public involvement activities that have taken place throughout the study.

7.2 Efficient Transportation Decision Making Screening

A *Programming Screen Summary Report* was published on March 29, 2011 as part of the FDOT's ETDM process. This project was designated as ETDM Project #13102. The Federal Highway Administration has determined that the project qualifies as a Type 2 Categorical Exclusion.

The following information was included for review under the screening process:

- Project Description
- Purpose and Need Statement
- Alternative Description
- Class of Action Determination
- Segment Details
- Project Effects
- General Project Commitments
- Required Permits
- Required Technical Studies
- Dispute Resolution Activity Log
- Agency-Assigned Degrees of Effect and FDOT Feedback

7.3 Advance Notification

An Advance Notification (AN) Package was forwarded to the Florida State Clearinghouse at the FDEP, as well as local and Federal agencies on December 17, 2010 in accordance with Executive Order 95-359. The AN Package consisted of:

- AN Transmittal Letter
- Mailing List
- Fact Sheet
- ETDM *Programming Screen Summary Report* (via website)

- Project Location Map

The following agencies provided review comments [pending receipt of agency documentation]:

- Hillsborough County Metropolitan Planning Organization
- Environmental Protection Commission of Hillsborough County

7.4 Newsletters

A project newsletter was prepared and distributed to inform the public of the public hearing (two locations) for the study. It notified the public of the locations where study documents could be reviewed during the public availability period, and included a summary of the Recommended Alternative. It also stressed the opportunity for public input and provided information on points of contact within the FDOT regarding citizen comments and concerns. A second newsletter will be published after the FHWA has issued Location and Design Concept Acceptance for the project. A copy of the initial newsletter is provided in the *Comments and Coordination Report* prepared for this study.

7.5 Public Information Meetings

Two public information meetings were held during the Feasibility Study to solicit public comment. Since the project traverses both Pinellas and Hillsborough Counties, one meeting was held in Tampa on May 19, 2008 and one meeting was held in Clearwater on May 22, 2008. A total of 23 written comments were received at the meetings. Sixteen (16) people expressed general support for the project.

7.6 Public Hearing

A blended (began with an informal open house format integrated with a time-specified formal hearing) public hearing for this project was held from 5:00 p.m. to 7:00 p.m. in two sessions at two locations. The first session was held in Pinellas County at the Clearwater Christian College, 3400 Gulf-To-Bay Boulevard, Building D - Dambach Hall 101, Clearwater, Florida, 33759 on Thursday March 24, 2011. The second session was held in Hillsborough County at the Westin Tampa Bay, 7627 West Courtney Campbell Causeway, BluVu Room, Tampa, Florida 33607 on Tuesday March 29, 2011. The hearing was held to inform citizens about the project details and schedule, and afford them the opportunity to express their views concerning the proposed improvements. The hearing during both sessions consisted of an open house from 5:00 p.m. to 6:00 p.m. and a formal presentation and public comment period beginning at 6:00 p.m. After the public comment period, the open house resumed until 7:00 p.m. The study's supporting documents were available for public review from March 1 through April 8, 2011.

Newsletters announced the public hearing. The newsletters sent via electronic mail to public officials and via direct mail to property owners within 1,000 feet of the project, current tenants, agencies, and interested parties. A Legal display advertising the hearing was published in the Tampa Tribune on March 9 and March 16, 2011 and in the TBT* (Tampa Bay Times) on March 7 and 17, 2011. An advertisement was also placed in the Florida Administrative Weekly on February 25, 2011.

FDOT staff and its consultants were available at the hearing to discuss the project and answer questions. A continuously-running video (PowerPoint presentation) that described the project and the recommended build alternative was shown during the open house portion of the hearing. Display boards available for review consisted of:

- Aerial photographs depicting the concept plans of the recommended alternative
- Courtney Campbell Causeway Proposed Improvements System Map
- Work Program Schedule
- Existing and recommended typical sections
- Evaluation matrix

The formal portion of the hearing sessions each began at 6:00 p.m. Kirk Bogen, Project Development Engineer for the FDOT, District Seven, presided at both sessions of the hearing. The proceedings were recorded by the court reporter that was on hand throughout the evening. Mr. Bogen welcomed the audience, discussed the purpose of the hearing. The next portion of the Hearing was devoted to oral comments.

Attendees were given the opportunity to provide comments in one of four ways:

- Make an oral statement during the formal portion of the hearing
- Make an oral statement to the court reporter during the informal portion of the hearing
- Complete the written comment form and place it in the drop box at the hearing
- Complete written comments and mail the Comment Form to the FDOT - District Seven

The number of attendees at the Hearing sessions totaled 79. A total of 8 written comment forms were received and six oral comments were made during the formal public comment period at Session 1 and a total of 9 written comment forms were received and six oral comments made at Session 2 of the hearing:

About half of the comments expressed support for the project. Some of the comments were concerned about elimination of fishing on the South side of the Causeway, limited access to the water and some concern for wildlife. Other comments expressed concern about the cost of the project and wasting money to build the project.

Copies of the public hearing materials, including the legal display advertisement, the sign-in sheets, the speaker cards, and the public hearing transcript are included in the *Comments and Coordination Report*. Copies of the display graphics, the PowerPoint slides, and

attendance rosters are included in the *Public Hearing Scrapbook* that was prepared for this project and is located in the project files.

In addition to the *Comments and Coordination Report*, a *Public Hearing Summary and Comments* document was prepared which contains all comments received specifically during and after the public hearing. This document is included in the project file.

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APPENDIX A

List of Supporting Documents



Engineering Items

- Project Concept Summary Report, Final Report, December 2008, from the SR 60 Feasibility Study Multi-Use Trail from McMullen Booth Road to Veterans Expressway.

Environmental & Public Involvement Items

- ETDM Programming Screen Summary Report, published June 16, 2011
- Advance Notification Package, submitted December 10, 2010
- Public Involvement Plan, prepared February 2011
- Cultural Resource Assessment Survey, prepared February 2011
- Location Hydraulics Memorandum, prepared May 2011
- Contamination Screening Evaluation Report, prepared June 2011
- Wetlands Evaluation Biological Assessment Report, prepared June 2011
- Comments and Coordination Report, prepared June 2011
- Public Hearing Transcript, prepared April 2011
- Public Hearing Scrapbook, prepared April 2011
- Public Hearing Summary and Comments, prepared May 2011

APPENDIX B

Agency Coordination



Appendix B – Agency Coordination Table of Contents

Agency (# of pages) -----	<u>Correspondence Date</u>
Hillsborough County Environmental Protection Commission (2) -----	January 28, 2011
Hillsborough MPO (1) -----	May 3, 2011
Hillsborough MPO (1) -----	January 28, 2011
US Dept of Commerce – NOAA – National Marine Fisheries Service (2) -----	June 20, 2011
US Dept of Commerce – NOAA – NMFS (1)-----	March 9, 2011
FDOT to US Dept of Commerce – NOAA – NMFS (2) -----	February 25, 2011
US Dept of Interior – US Fish & Wildlife Service (USFWS) (2) -----	June 21, 2011
FDOT to US Dept of Interior – USFWS (4 w/attachments) -----	June 15, 2011
US Dept of Interior – USFWS (11 w/attachments) -----	March 24, 2011
FDOT to US Dept of Interior – USFWS (2) -----	February 25, 2011
Florida Fish & Wildlife Conservation Commission (2) -----	April 18, 2011
Florida Department of State – Division of Historic Resources (1) -----	April 27, 2011
Florida Department of State – Division of Historic Resources (2) -----	March 15, 2011
FDOT to FHWA – Cultural Resources Assessment Survey Review (2) -----	February 2, 2011
FHWA concurrence form to Fl. Dept. of State–Div of Hist. Resources (1)----	March 1, 2011
City of Tampa Greenways & Trails Citizens Advisory Committee (1)-----	March 28, 2011
Tampa Bay Regional Planning Council (1)-----	April 4, 2011

COMMISSION
Kevin Beckner – Chairman
Lesley "Les" Miller – Vice Chairman
Victor Crist
Ken Hagan
Al Higginbotham
Sandy Murman
Mark Sharpe



Executive Director
Richard D. Garrity, Ph.D.

Roger P. Stewart Center
3629 Queen Palm Dr. Tampa, FL 33619
Ph: (813) 627-2600

Fax Numbers (813):
Admin 627-2620 Waste 627-2640
Legal 627-2602 Wetlands 627-2630
Water 627-2670 Air 627-2660
Lab 635-8061

VIA EMAIL

January 28, 2011

Mr. Ming Gao, P.E.

ming.gao@dot.state.fl.us

Department Head, Intermodal Systems Development

Florida Department of Transportation, District 7

11201 N. McKinley Drive / MS-7-500

Tampa, FL 33612-6456

Subject: EPC Comments – Courtney Campbell Causeway Multi-Use Trail PD&E Study

Staff from the Environmental Protection Commission of Hillsborough County (EPC) has conducted a review of the subject Courtney Campbell Causeway Multi-Use Trail PD&E Study and does not object to the study as proposed subject to the following comments/conditions:

1. Wetlands exist within the project boundaries as indicated through review of aerial photography and Soil Conservation Service soils maps.
2. Review of this PD&E by EPC staff does not constitute a guarantee that the Environmental Protection Commission approvals/permits necessary for the development as proposed will be issued, does not itself serve to justify any impact to wetlands, and does not grant any implied or vested right to environmental approvals.
3. Development within wetlands of Hillsborough County which destroys, reduces, or impairs the wetland or which contributes to the present or potential future destruction, reduction, or impairment of the environmental benefits provided by the wetland, or a portion thereof, constitutes pollution as defined by Chapter 84-446, Laws of Florida, as amended. Impacts to wetlands are prohibited except unless specifically authorized in writing by the EPC Executive Director or his authorized agent. Pursuant to EPC Wetland Rule Section 1-11.07(1), Rules of the EPC, "written authorization may be given to conduct proposed development affecting wetlands only if reasonable use of the land cannot be accomplished without affecting the wetland."
4. EPC staff requires that all efforts be taken to avoid or reduce wetland impacts prior to submittal of any site development plans. The size, location, and configuration of the wetlands may result in requirements to reduce or reconfigure proposed lots, re-align roadways, and make other changes necessary to avoid or minimize wetland impacts.

An agency with values of environmental stewardship, integrity, honesty, and a culture of fairness and cooperation.

www.epchc.org

E-Mail: epcinfo@epchc.org

AN AFFIRMATIVE ACTION – EQUAL OPPORTUNITY EMPLOYER

Page 2

Courtney Campbell Causeway Multi-Use Trail PD&E Study
January 28, 2011

If you have any further questions, please contact me at 813-627-2600, extension 1299 or at mcginnis@epchc.org.

Sincerely,



Sean P. McGinnis, CHMM
Environmental Specialist III
Air Division, Enforcement and Analysis



RECEIVED
DISTRICT SECRETARY
DISTRICT SEVEN
2011 MAY -6 11:37

May 3, 2011

Mr. Donald Skelton, PE
District Secretary
Florida Department of Transportation
11201 N. McKinley Drive
Tampa, FL 33612

Mayor Joe Affronti, Sr.
City of Temple Terrace
MPO Chairman

Commissioner Mark Sharpe
Hillsborough County
MPO Vice Chairman

RECEIVED
PLANNING UNIT
2011 MAY 19 PM 2:11

RE: Courtney Campbell Multi-use Trail

Dear Secretary Skelton:

Commissioner Kevin Beckner
Hillsborough County

Councilman Joseph Caetano
City of Tampa

Joe Lopano
Hillsborough Co. Aviation Authority

Commissioner Rick A. Lott
City of Plant City

Commissioner Lesley "Les" Miller, Jr.
Hillsborough County

Commissioner Sandra Murman
Hillsborough County

Councilman Tom Scott
City of Tampa

Councilman Curtis Stokes
HART

Joseph Waggoner
Expressway Authority

Richard Wainio
Tampa Port Authority

Derek L. Doughty (Ex-Officio)
The Planning Commission

Donald J. Skelton, P.E. (Ex-Officio)
FDOT, District Seven

Ramond A. Chiaramonte, AICP
Executive Director

The Hillsborough County Metropolitan Planning Organization (MPO) supports the continued funding of the Courtney Campbell Multi-use Trail. The project is consistent with the high priorities of the 2035 Long Range Transportation Plan (LRTP) that was adopted by the MPO Board on December 9, 2009.

The proposed Courtney Campbell Causeway multi-use trail will connect the Bayshore Trail extension (Bayshore Boulevard at SR 60) in Pinellas County to Hillsborough County's sidewalk/multi-use path around the Tampa Airport Interchange in Hillsborough County. The proposed facility is intended for bicycle, pedestrian, and other recreational users thereby providing alternate modes of transportation currently unavailable for local and regional non-motorized trips.

The Courtney Campbell Causeway Multi-use Trail will accommodate recreational users that can experience the scenic qualities of the Causeway that could further enhance tourism and economic development.

We strongly support this project and are providing our full endorsement of the Courtney Campbell Multi-use Trail.

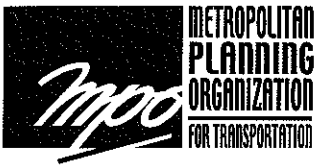
Sincerely

Joseph Affronti, Sr.
MPO Chairman

Hillsborough County
Metropolitan Planning Organization
P.O. Box 1110
601 E. Kennedy, 18th Floor
Tampa, Florida 33601-1110
813/272-5940
FAX NO: 813/301-7172
<http://www.hillsboroughmpo.org>
e-mail: transportation@plancom.org

Xc: Ming Gao, FDOT

→ Kirk → Robin



January 28, 2011

Mr. Ming Gao, P. E.
Department Head, Intermodal Systems Development
Florida Department of Transportation, District 7
11201 N. McKinley Drive / MS 7-500
Tampa, FL 33612-6456

Mayor Joe Affronti, Sr.
City of Temple Terrace
MPO Chairman

Commissioner Mark Sharpe
Hillsborough County
MPO Vice Chairman

Dear Mr. Gao:

Re: ETDM #13102 Advance Notification
SR 60 (Courtney Campbell Causeway) Multi-Use Trail PD&E Study

Commissioner Kevin Beckner
Hillsborough County

Councilman Joseph Caetano
City of Tampa

Joe Lopano
Hillsborough Co. Aviation Authority

Commissioner Rick A. Lott
City of Plant City

Commissioner Lesley "Les" Miller, Jr.
Hillsborough County

Commissioner Sandra Murman
Hillsborough County

Councilman Tom Scott
City of Tampa

Councilman Curtis Stokes
HART

Joseph Waggoner
Expressway Authority

Richard Wainio
Tampa Port Authority

Derek L. Doughty (Ex-Officio)
The Planning Commission

Donald J. Skelton, P.E. (Ex-Officio)
FDOT, District Seven

Ramond A. Chiaramonte, AICP
Executive Director

Pursuant to the Advance Notification sent to the Hillsborough Metropolitan Planning Organization (MPO) we have reviewed the policies of the City of Tampa Comprehensive Plan for consistency with Chapter 163 of the Florida Statutes.

Chapter 6 – Sustainable Environment of the Tampa Comprehensive Plan lists the Ben T. Davis Beach on the Courtney Campbell Causeway as one of three significant man-made public beaches in the City.

- Policy 38.22.2: Maintain or improve the existing, natural condition of the three public beaches in the City.
- Policy 38.22.3: Coordinate with the State to implement state-of-the-art beach and dune stabilization technique where appropriate
-

For coastal management, the Tampa Comprehensive Plan encourages the improvement or maintenance of the natural condition of the Ben T. Davis Beach on the Courtney Campbell Causeway. And the Conservation Element / Air Quality Section of the Tampa Comprehensive Plan encourage transportation system improvements that reduce air pollution concentrations.

Thank you for the opportunity to provide comments for consistency purposes as this project is implemented.

Sincerely,

Ramond A. Chiaramonte, AICP
Executive Director

Hillsborough County
Metropolitan Planning Organization
P.O. Box 1111
601 E. Kennedy, 18th Floor
Tampa, Florida 33601-1110
813/272-5940
FAX NO: 813/301-7172
<http://www.hillsboroughmpo.org>
e-mail: transportation@plancom.org

RAC:wb

I:\tran_mpo\corresp\2011 corresp\advance notification\ccc trail mpo response.doc

RECEIVED
PLANNING UNIT
2011 JAN 31 PM 1:39

Salicco, Christopher

From: Severson, Joseph [Joseph.Severson@dot.state.fl.us]
Sent: Monday, June 20, 2011 9:12 AM
To: Rhinesmith, Robin
Cc: Gonzalez, Roberto; Bogen, Kirk; Salicco, Christopher; Novotny, Jeffrey S.
Subject: FW: NMFS response to SR 60 multi-use Trail (ETDM 13102) June seagrass survey information

Attachments: David_Rydene.vcf



David_Rydene.vcf
(440 B)

FYI

-----Original Message-----

From: David Rydene [mailto:David.Rydene@noaa.gov]
Sent: Friday, June 17, 2011 1:17 PM
To: Severson, Joseph; Rhinesmith, Robin
Cc: Jane Monaghan
Subject: NMFS response to SR 60 multi-use Trail (ETDM 13102) June seagrass survey information

NOAA's National Marine Fisheries Service (NMFS), Habitat Conservation Division (HCD), has reviewed the June 2011 seagrass survey information provided by the Florida Department of Transportation District 7 (FDOT). Based on this information and a site inspection conducted by NMFS staff on June 17, 2011, NMFS concurs with FDOT's opinion that there will be no direct seagrass impacts from the project. Assuming that Best Management Practices are implemented during construction, NMFS anticipates that any adverse effects that might occur on marine and anadromous fishery resources will be minimal and, therefore, does not object to the project.

--

David Rydene, Ph.D.
Fishery Biologist
National Marine Fisheries Service
Habitat Conservation Division
263 13th Avenue South
St. Petersburg, FL 33701
Office (727) 824-5379
Cell (727) 512-6782
Fax (727) 824-5300



UNITED STATES DEPARTMENT OF COMMERCE
National Oceanic and Atmospheric Administration
NATIONAL MARINE FISHERIES SERVICE
Southeast Regional Office
263 13th Avenue South
St. Petersburg, Florida 33701-5505
(727) 824-5317; FAX 824-5300

March 9, 2011 F/SER46:DR

Joseph Severson
Environmental Specialist
Florida Department of Transportation District Seven
11201 McKinley Drive MS 7-500
Tampa, Florida 33612-6456

Dear Mr. Severson:

This letter is intended to provide technical assistance in response to your letter dated February 25, 2011. NOAA's National Marine Fisheries Service (NMFS) has reviewed the accompanying Draft Wetland Evaluation Biological Assessment Report regarding the construction of a multi-use trail on the SR 60 Courtney Campbell Causeway in Hillsborough County and Pinellas County, Florida (ETDM No. 13102; Work Program Item Segment No. 422640 2; FAP No. 9045-090-C). The trail would include two structures crossing the waters of Old Tampa Bay. NMFS has assessed the information provided by your agency in reference to potential impacts to essential fish habitat and swimming sea turtles.

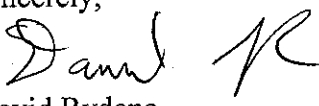
The project lies within an area of the Tampa Bay system that may be inhabited by swimming sea turtles and/or smalltooth sawfish. The draft report does not include an assessment of potential impacts to smalltooth sawfish. Smalltooth sawfish are listed as an endangered species under the Endangered Species Act (ESA) and fall under NMFS' purview. NMFS recommends that a section on smalltooth sawfish be added to your report. In addition, we suggest that NMFS' Sea Turtle and Smalltooth Sawfish Construction Conditions be implemented as part of the project's commitments. When the project's final design has been determined, NMFS recommends that a section 7 consultation be conducted for swimming sea turtles and smalltooth sawfish.

It appears that the recommended trail alternative will have minimal impacts to mangroves. As the report states, the final determination of potential impacts to seagrasses from the two independent bridge structures will need to be determined during the prime seagrass growing season (May-September). Appropriate compensatory mitigation strategies can be discussed when impacts to NMFS trust resources are known with greater certainty.

If you have questions regarding NMFS' views on this project, please contact me at our St. Petersburg, Florida office. You can reach me at the letterhead address or by calling (727) 824-5379.



Sincerely,

A handwritten signature in black ink, appearing to read "David Rydene". The signature is written in a cursive style with a large, stylized "R" at the end.

David Rydene

Fish Biologist
Habitat Conservation Division



Florida Department of Transportation

RICK SCOTT
GOVERNOR

11201 McKinley Drive MS 7-500
Tampa, FL 33612

OFFICE OF THE
SECRETARY

February 25, 2011

Dr. David Rydene, Ph.D.
National Marine Fisheries Service
263 13th Avenue South
St. Petersburg, FL 33701

RE: WPI Segment No: 422640-2
State Road (SR) 60 (Courtney Campbell Causeway) Multi-Use Trail
From Bayshore Boulevard to West of the Ben T. Davis Beach Entrance
Pinellas and Hillsborough Counties

Dear Dr. Rydene,

The Florida Department of Transportation (Department) is conducting a Project Development and Environment (PD&E) Study to construct a multi-use trail within the limits noted above. The study corridor is the existing Courtney Campbell Causeway (Causeway). The Causeway consists of fill material that was used to construct SR 60. The entire project will be located on the existing fill material with the exception of two proposed bridges over Old Tampa Bay, adjacent to the existing SR 60 bridges. The proposed bridges, at a minimum, will meet the existing horizontal and vertical clearances of the existing SR 60 bridges.

As a part of conducting this study, the Department is initialing informal consultation with the National Marine Fisheries Service (NMFS). In order to fulfill the requirements of the various federal and state environmental and regulatory processes the Department is soliciting comments from federal, state, and local agencies. A Draft Wetland Evaluation and Biological Assessment Report (WEBAR) has been prepared for the study. This report is attached for your review.

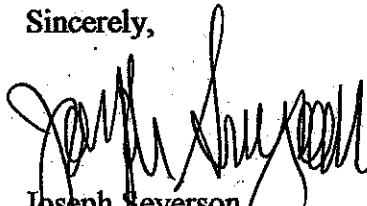
This project has been evaluated for impacts on federally protected threatened and endangered species. Based on the results of the study conducted, the Department has concluded that the West Indian manatee, wood stork, gulf sturgeon, piping plover, and loggerhead, leatherback, Kemp's Ridley, and green sea turtles may utilize habitat within the project area. It is anticipated

that this project will have minimal temporary impacts to habitat during construction of the proposed bridges, but will not have any permanent adverse effects to these species or their habitat. As mentioned above, this project will be constructed on the existing fill limits of the Causeway, with the exception of the proposed bridges.

No suitable foraging habitat for the wood stork and no critical habitat for the gulf sturgeon, West Indian manatee, and piping plover will be impacted by the construction of the proposed multi-use trail. There is no suitable nesting habitat for sea turtles located within the project area; therefore no impacts to sea turtle nesting are anticipated for this project. The Department will develop a wildlife watch plan, which includes the FFWCC "Standard Manatee Conditions for In-Water Work", during the design and permitting phase and will adhere to the guidelines set in this plan during construction. The Department will coordinate this plan with the USFWS. Therefore, the Department, on behalf of the Federal Highway Administration (FHWA), has determined that the proposed actions will have a "No Effect" determination for the wood stork, and a "May Affect, Not Likely to Adversely Affect" determination for the West Indian manatee, gulf sturgeon, piping plover, and sea turtles.

If your office concurs with this determination, please respond to the Department in writing by March 10, 2011. If your agency would like a site review or any additional information, please feel free to call me at (813) 975-6455.

Sincerely,



Joseph Severson
Environmental Specialist

cc: Robin Rhinesmith
Roberto Gonzalez

Enclosed: Draft Wetland Evaluation Biological Assessment Report



United States Department of the Interior

U. S. FISH AND WILDLIFE SERVICE

7915 BAYMEADOWS WAY, SUITE 200
JACKSONVILLE, FLORIDA 32256-7517

IN REPLY REFER TO:

FWS Log No. 41910-2011-I-0198

June 21, 2011

Mr. Joseph Severson
Environmental Specialist
Florida Department of Transportation
11201 N. McKinley Drive,
Tampa, FL 33612-6456

Dear Mr. Severson:

Our office has reviewed the additional information provided by the Florida Department of Transportation (FDOT) for consultation on the SR 60 Multi-Use Trail and your request for our concurrence on the FDOT effects determination for the federally listed Florida manatee (*Trichechus manatus latirostris*), piping plover (*Charadrius melodus*), wood stork (*Mycteria americana*), green sea turtle (*Chelonia mydas*), loggerhead sea turtle (*Caretta caretta*), leatherback sea turtle (*Dermochelys coriacea*), Kemp's Ridley sea turtle (*Lepidochelys kempii*) and gulf sturgeon (*Acipenser oxyrinchus*). The western terminus begins at latitude 27 59'39.13N and longitude -82 42'13.56W in Pinellas County; the eastern terminus is located at latitude 27 58'19.62N and longitude -82 34'57.53W in Hillsborough County, Florida.

We provide the following comments and recommendations in accordance with section 7 of the Endangered Species Act of 1973 (ESA), as amended (16 U.S.C. 1531 *et seq.*), the Marine Mammal Protection Act of 1972 (MMPA), as amended (16 U.S.C. 1361 *et seq.*), and the Migratory Bird Treaty Act of 1918 (16 U.S.C. 703-712 *et seq.*).

The Florida manatee is protected under the ESA and the MMPA. Both of these laws make it illegal to "take" (i.e. harm, harass, injure or kill) manatees. Because seagrass impacts were possible the U.S. Fish and Wildlife Service (Service) and the National Marine Fisheries Service (NMFS) requested a seagrass survey to be conducted during the growing season. The results of the survey were submitted to our office on June 15, 2011. Dr. David Rydene with NMFS reviewed the results of the survey and concurred with your findings via email on June 17, 2011. No seagrass beds will be impacted by this project. In addition to following the Standard In-Water Construction Conditions for Manatees (2011) there will be a need for special conditions for this project which will include the following: no nighttime work, dedicated manatee observers, fenders between work barges to prevent crushing, and the proper siltation or exclusion barriers that will not entrap manatees in the work site. Information on manatee observer experience and requirements can be found on MyFWC.com.

The Service and NMFS share Federal jurisdiction for sea turtles under the ESA. The Service has responsibility for sea turtles on nesting beaches. NMFS has jurisdiction for sea turtles in the marine environment. The Service concurs with your determination of effect for all species of sea turtles due to the lack of nesting beaches along the causeway.

The Service also shares jurisdiction for Gulf sturgeon under the ESA. The Service has responsibility for sturgeon in estuarine areas if FDOT is the action agency. Therefore, the Service recommends that FDOT incorporate the Construction Special Provisions for the protection of the Gulf Sturgeon.

The Service concurs with your determination of 'May Affect but Not Likely to Adversely Affect' for wood storks because there will be no wetland impacts to suitable foraging habitat with the preferred alternative.

The Courtney Campbell Causeway frequently supports very large numbers of loafing or foraging flocks of migratory shorebirds. The placement of a pedestrian and bicycle trail too close to the areas where shorebirds are known to gather may result in flushing and disturbance. If dogs are allowed on the trail, the birds may react (flush) even if the dog is on a leash because the animal is seen as a predator. This area also supports one of the largest gathering sites in the region for American oystercatchers with 50+ individuals commonly seen here. Repeated disturbances from humans and dogs can be one of the main threats to our shorebird populations. Shorebirds can be displaced from foraging and resting areas, they can also abandon important areas if the disturbance continues and as a result they may have lower body weights upon arrival on their breeding grounds (Pfister et al. 1992, Burger, et al. 2007). Information submitted to our office by FDOT indicates that shorebirds do not use areas within the footprint of the project. The proposed trail will be located on the existing service road in some areas and it is always located to the North of the beach parking areas. Vehicles are allowed to drive and park on the beach along the causeway. We do not anticipate that the trail will add to the existing impacts to shorebirds in this area. However, we recommend that the land managers address the ongoing disturbances to loafing and nesting shorebirds on the beach as a result of the vehicle traffic. If additional information becomes available or if an increase in shorebird disturbance is documented as a result of this trail, consultation with our office should be reinitiated.

If you have any questions regarding this response, please contact Jane Monaghan at (904) 731-3119.

Sincerely,



for David L. Hankla
Field Supervisor

Cc: Scott Sanders, FFWCC
David Rydene, NMFS
Terry Gilbert, URS Corp.

References Cited:

Pfister, C.; Harrington, B. and Lavin, M., 1992. The impact of human disturbance on shorebirds at a migration staging area. *Biological Conservation* 60:115-126

Burger, D; Carlucci, S.A.; Jeitner, C.W.; and Niles, L., 2007. Habitat choice, disturbance, and management of foraging shorebirds and gulls at a migratory stopover. *Journal of Coastal Research*, 23(5), 1159-1166.



Florida Department of Transportation

RICK SCOTT
GOVERNOR

11201 North McKinley Drive
Tampa, FL 33612-6456

ANANTH PRASAD, P.E.
SECRETARY

June 15, 2011

Jane Monaghan
USFWS-Ecological Services
7915 Baymeadows Way, Suite 200
Jacksonville, Florida 32256-7517

Re: WPI Segment No: 422640-2
USFWS RAI for State Road 60 (Courtney Campbell Causeway) Multi-Use Trail
From Bayshore Boulevard to West of Ben T. Davis Beach Entrance
Pinellas and Hillsborough Counties

Dear Ms. Monaghan:

In response to USFWS' request for additional information (RAI), dated March 24, 2011. Please find enclosed two figures displaying the results of the submerged aquatic vegetation surveys in the vicinity of the SR 60 Multi-Use Trail Project. The surveys were performed on June 8 & 9, 2011.

1. The first attachment is a concept plan sheet that demonstrates an opportunity to avoid sea grasses on the east side of proposed bridge which would be adjacent to bridge #109801 (bridge structure 2). As a result, no anticipated sea grass impacts would be associated with the construction of the proposed bridge adjacent to the existing structure 2. No sea grasses were observed on the west end of proposed bridge structure 2.
2. The second attachment is an aerial-based figure that shows the location of the sea grass bed we discovered next to the proposed bridge which would be adjacent to bridge #150138 (bridge structure 1). The figure also delineates the sea grass survey areas that were investigated on both sides of the bridge (red outlines). Sea grasses were observed in the southwest quadrant of the existing bridge, but well outside the potential limits of construction. The submerged bed is visible under the label "EXIST Sea Wall" on the attached aerial PDF. Since it was outside the anticipated limits of construction, GPS points were not recorded. This bed was visually inspected to confirm its presence on the aerial photos. A small area of grass was observed in the southeast quadrant, but the proposed bridge adjacent to structure 1 connects to the spoil area at the western

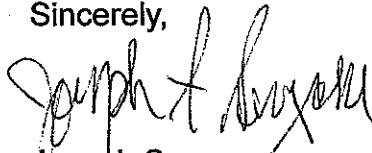
Jane Monaghan
Page 2
June 15, 2011

most point of the spoil area and angles landward away from the water. This sea grass bed is not expected to be impacted.

Based on the above additional information and information provided in the Department's May 20, 2011 response to the USFWS RAI, The Department, on behalf of the FHWA, continues to expect that the project "may affect, not likely to adversely affect" the West Indian Manatee and requests that the USFWS concur with the Department's original determination.

If you have any questions or if I can be of any further assistance please do not hesitate to contact me at Joseph.Severson@dot.state.fl.us or at 813-975-6455.

Sincerely,



Joseph Severson
Environmental Scientist

Enclosure(s) 2

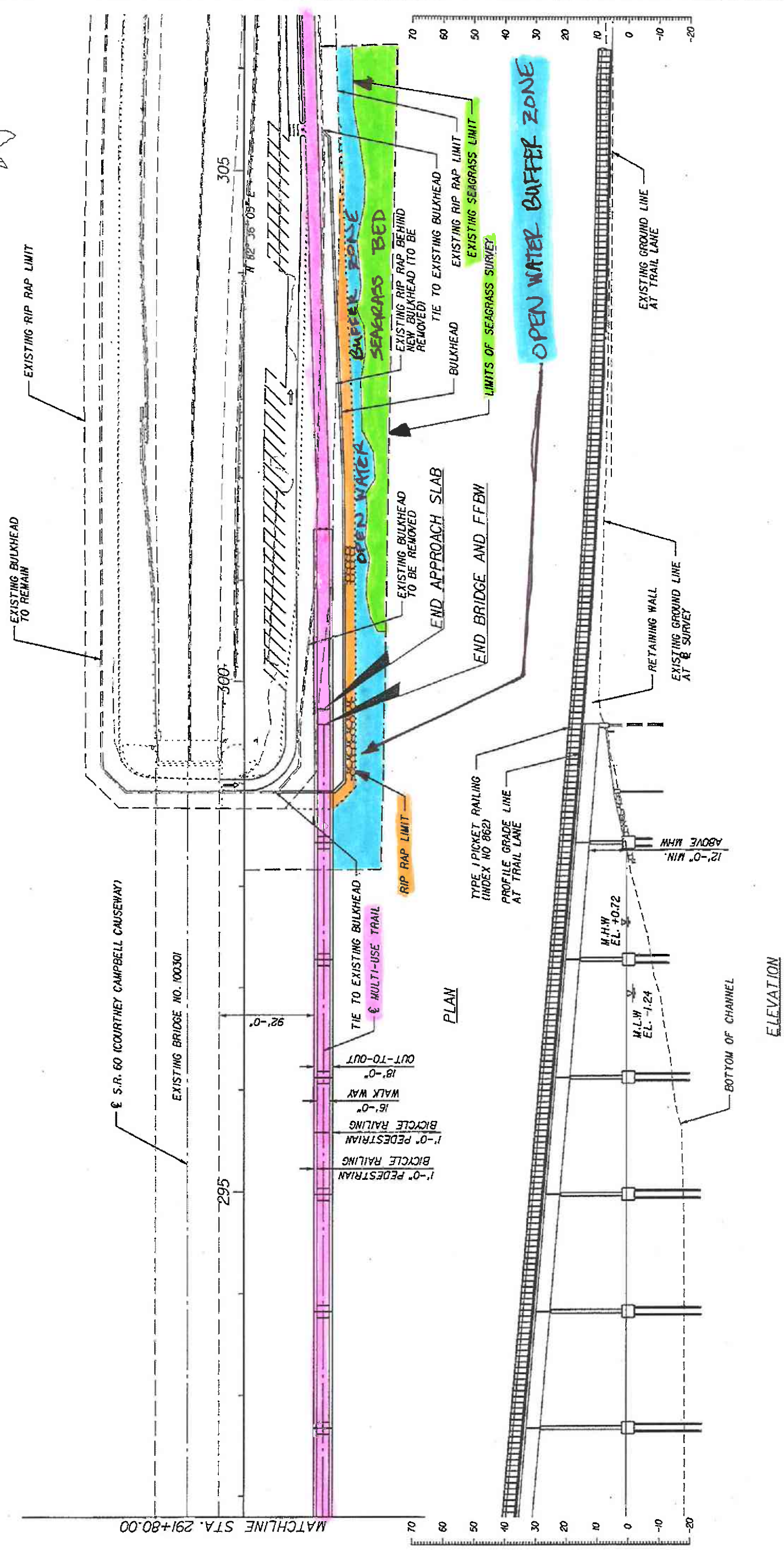
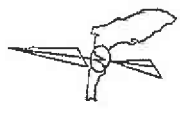
cc: File

Nahir DeTizio, FHWA

Robin Rhinesmith, FDOT

Roberto Gonzalez, FDOT

Christopher Salicco, ACE



NOTES:

1. ELEVATIONS ARE BASED ON NAVD 1988 DATUM.

BRIDGE NO. 109801

DATE	BY	DESCRIPTION	REVISIONS	DATE	BY	DESCRIPTION
			BI			

FLORIDA DEPARTMENT OF TRANSPORTATION		SHEET TITLE	
ROAD NO.	COUNTY	FINANCIAL PROJECT ID	PLAN AND ELEVATION (4 OF 4)
SR 60	HILLSBOROUGH	42-4561-5-52-01	
HDR ENGINEERING, INC. 5426 BAY CENTER DRIVE, SUITE 400 TAMPA, FL 3609-3444 CERTIFICATE OF AUTHORIZATION 4213		PROJECT NAME	
		SR 60 MULTI-USE TRAIL	
		SHEET NO.	
		BI-4	

OLD TAMPA BAY

OLD TAMPA BAY



RELOCATE EXIST GUARDRAIL TO 12' OFFSET FROM EDGE OF TRAVEL LANE TO FACE OF GUARDRAIL

COURTNEY CAMPBELL CAUSEWAY



110

115

105

1320'

1320'

PROPOSED MULTI-USE TRAIL

STRUCTURE 1

12 PARKING SPACES

RELOCATED ACCESS ROAD

PROPOSED MULTI-USE TRAIL

Exist Sea Wall

LEGEND

-  PROPOSED MULTI-USE TRAIL
-  RELOCATED ACCESS ROAD / RELOCATED PARKING AREA
-  PROPOSED MULTI-USE TRAIL BRIDGE
-  EXISTING RIGHT OF WAY



DATE OF AERIAL: NOVEMBER 2008

American
 Consulting Engineers of Florida, LLC
 2818 Cypress Ridge Blvd, Suite 200
 Wesley Chapel, Florida 33544
 Phone: (813) 445-1616 Fax: (813) 445-2601
 Certificate Authority: N. 9302
 Jeffrey S. Novakny, P.E. No. 51083

STATE OF FLORIDA	
DEPARTMENT OF TRANSPORTATION	
ROAD NO.	COUNTY
SR 60	PINELLAS & HILLSBOROUGH
FINANCIAL PROJECT ID	
422640-2-22-01	

SR 60 (COURTNEY CAMPBELL CAUSEWAY) PD&E STUDY
CONCEPT PLANS
 FROM BAYSHORE BLVD TO WEST OF BEN T DAVIS BEACH ENTRANCE

**PRELIMINARY
CONCEPT PLANS**

SHEET NO. 7



United States Department of the Interior

U. S. FISH AND WILDLIFE SERVICE

7915 BAYMEADOWS WAY, SUITE 200
JACKSONVILLE, FLORIDA 32256-7517

IN REPLY REFER TO:

FWS Log No. 41910-2011-I-0198

March 24, 2011

Mr. Joseph Severson
Environmental Specialist
Florida Department of Transportation
11201 N. McKinley Drive,
Tampa, FL 33612-6456

Dear Mr. Severson:

Our office has reviewed the information provided by the Florida Department of Transportation (FDOT) for consultation on the SR 60 Multi-Use Trail and your request for our concurrence on the FDOT effects determination for the federally listed Florida manatee (*Trichechus manatus latirostris*), piping plover (*Charadrius melodus*), wood stork (*Mycteria americana*), green sea turtle (*Chelonia mydas*), loggerhead sea turtle (*Caretta caretta*), leatherback sea turtle (*Dermochelys coriacea*), Kemp's Ridley sea turtle (*Lepidochelys kempii*) and gulf sturgeon (*Acipenser oxyrinchus*). The western terminus begins at latitude 27 59'39.13N and longitude -82 42'13.56W in Pinellas County; the eastern terminus is located at latitude 27 58'19.62N and longitude -82 34'57.53W in Hillsborough County, Florida.

We provide the following comments and recommendations in accordance with section 7 of the Endangered Species Act of 1973 (ESA), as amended (16 U.S.C. 1531 *et seq.*), the Marine Mammal Protection Act of 1972 (MMPA), as amended (16 U.S.C. 1361 *et seq.*), and the Migratory Bird Treaty Act of 1918 (16 U.S.C. 703-712 *et seq.*).

The risks to the Florida manatee have been evaluated. The Florida manatee is protected under the ESA and the MMPA. Both of these laws make it illegal to "take" (i.e. harm, harass, injure or kill) manatees. We are concerned about the presence of manatees and sea grasses in the project area. Any impacts to seagrass beds results in a 'May Affect' determination for the Florida manatee. Seagrass surveys need to be conducted during the growing season (June 1- Sept 30). Direct impacts to the seagrass beds during construction and indirect impacts to seagrasses as a result of shading need to be quantified. Further damage may occur from the operation of boats and barges during the construction phase. Turbidity and siltation during the construction phase may also impact seagrass beds. The Service recommends the National Marine Fisheries Service and U.S. Army Corps of Engineers guidance document entitled, 'Dock Construction Guidelines in Florida for Docks or Other Minor Structures Constructed in or over Submerged Aquatic Vegetation (SAV), Marsh or Mangrove Habitat' (August 2001) when designing structures over SAV. We have attached these guidelines for you. In addition to following the Standard In-Water

Construction Conditions for Manatees (2009) there will be a need for special conditions for this project which may include the following: no nighttime work, dedicated manatee observers, fenders between work barges to prevent crushing, seasonal timing restrictions and the proper siltation or exclusion barriers that will not entrap manatees in the work site.

The Service and the National Oceanic and Atmospheric Administration's National Marine Fisheries Service (NMFS) share Federal jurisdiction for sea turtles under the ESA. The Service has responsibility for sea turtles on nesting beaches. NMFS has jurisdiction for sea turtles in the marine environment. The Service concurs with your determination of effect for all species of sea turtles due to the lack of nesting beaches along the causeway.

The Service also shares jurisdiction for Gulf Sturgeon under the ESA. The Service has responsibility for sturgeon in estuarine areas if FDOT is the action agency. Therefore, the Service recommends that FDOT incorporate the Construction Special Provisions for the protection of the Gulf Sturgeon. We have attached these guidelines for your convenience.

If there are wetland impacts, the Service cannot concur with your determination of 'No Effect' for wood storks because there are several colony sites within 15 miles of the proposed trail. The Service recommends utilizing the wood stork effect determination key that is found on our website.

The Courtney Campbell Causeway is an important area for shorebirds and other migratory birds and frequently supports very large numbers of loafing or foraging flocks. The placement of a pedestrian and bicycle trail too close to the areas where shorebirds are known to gather may result in the flushing and disturbance of shorebirds. If dogs are allowed on the trail, the birds may react (flush) even if the dog is on a leash because the animal is seen as a predator. Please provide the distances between the proposed trail and the known shorebird roosting, feeding and loafing areas.

Although there is no piping plover critical habitat designated inside Tampa Bay, we have attached the results of the 2011 Winter Shorebird and Piping Plover Survey for the C. Campbell Causeway. Red knots (*Caladris spp.*) a candidate species for listing, utilize the causeway for foraging and resting during their long migratory flights. This area also supports one of the largest gathering sites in the region for American oystercatchers with 50+ individuals commonly seen here. Repeated disturbances from humans and dogs can be one of the main threats to our shorebird populations. Shorebirds can be displaced from foraging and resting areas, they can also abandon important areas if the disturbance continues and as a result they may have lower body weights upon arrival on their breeding grounds (Pfister et al. 1992, Burger, et al. 2007). Until we have the information requested above we cannot make a determination on the effects of this project on migratory birds.

If you have any questions regarding this response, please contact Jane Monaghan at (904) 731-3119.

Sincerely,



for

David L. Hankla
Field Supervisor

Cc: Scott Sanders, FFWCC
Michael Esquivel, USCG
David Rydene, NOAA
Terry Gilbert, URS Corp.

Attachments (3): 2011 Winter Shorebird Survey, Dock Construction over SAV, Special Provisions for Sturgeon Protection.

References Cited:

Pfister, C.; Harrington, B. and Lavin, M., 1992. The impact of human disturbance on shorebirds at a migration staging area. *Biological Conservation* 60:115-126

Burger, D; Carlucci, S.A.; Jeitner, C.W.; and Niles, L., 2007. Habitat choice, disturbance, and management of foraging shorebirds and gulls at a migratory stopover. *Journal of Coastal Research*, 23(5), 1159-1166.

2011 Winter Shorebird Survey- Data Sheet

Site name and description: *COURTNEY CAMP BELL CAUSEWAY FROM WATER TREATMENT PLANT (CLEARWATER/PINGULAS TO ROCKY PT. DRIVE TAMPA /HILLSBOROUGH*

Observer (lead): *ROBERT LANE*

Email: *ohiomagpie@hotmail.com*
 Phone: HOME: 330-537-4341
 CELL: 216-276-7782

Start time: *6:45 AM FEB 4 2011*

Weather conditions: *MORNING: OVERCAST, FOGGY AFTERNOON: CLEAR, SUNNY*

Other observers: *DENISE LANE JANE MANN STEPHEN MANN*

RECEIVED

FEB 18 2011

GPS Locations.

Please use a GPS unit or internet tools such as GET LAT LON (<http://www.getlatlon.com/>) to determine latitude and longitude for the following.

U.S. FISH AND WILDLIFE SERVICE
 JACKSONVILLE, FLORIDA

- 1) Length of your survey route: start point (A) and point furthest from the start (B).
- 2) Individuals or groups of Snowy Plovers, Piping Plover, Wilson's Plover, and Red Knot.
- 3) Color-banded individuals.
- 4) Large aggregations or flocks of birds (i.e. 50+); note species composition and number of birds.

Point	Latitude	Longitude	Species	Information
A	<i>27.958717</i>	<i>-82.708000</i>	<i>not applicable</i>	<i>Start point of your survey route.</i>
B	<i>27.961938</i>	<i>-82.568971</i>	<i>not applicable</i>	<i>Furthest point of your route from the start point (A).</i>
<i>1</i>	<i>27.961028</i>	<i>-82.696403</i>	<i>WILSON'S PLOVER</i>	<i>2 CLEARWATER, CCC BEACH IN LARGE FLOCK OF SHORE BIRDS</i>
<i>2</i>	<i>"</i>	<i>"</i>	<i>LEAST SANDPIPER</i>	<i>470</i>
<i>3</i>	<i>"</i>	<i>"</i>	<i>DUNLIN</i>	<i>118</i>
<i>4</i>	<i>"</i>	<i>"</i>	<i>SHORT-BILLED DOWITCHER</i>	<i>75</i>
<i>5</i>	<i>"</i>	<i>"</i>	<i>RUDDY TURNSTONE</i>	<i>65</i>
<i>6</i>	<i>"</i>	<i>"</i>	<i>RED KNOT</i>	<i>KM3, GROUP OF 6</i>
<i>7</i>	<i>"</i>	<i>"</i>	<i>SANDERLING</i>	<i>100</i>
<i>8</i>	<i>"</i>	<i>"</i>	<i>RING-BILLED GULL</i>	<i>80</i>
<i>9</i>	<i>"</i>	<i>"</i>	<i>LAUGHING GULL</i>	<i>70</i>
<i>10</i>	<i>"</i>	<i>"</i>	<i>AMERICAN OYSTERCATCHER</i>	<i>35</i>
<i>11</i>	<i>"</i>	<i>"</i>	<i>BLACK SKIMMER</i>	<i>36</i>
<i>12</i>	<i>"</i>	<i>"</i>	<i>WESTERN SANDPIPER</i>	<i>32</i>
<i>13</i>	<i>"</i>	<i>"</i>	<i>LESSER SCAUP, BLACK-BELLIED PLOVER, BROWN PELICAN, ALSO: SNOWY EGRET, HERRING GULL, ROYAL TERN, FORSTER'S TERN</i>	
<i>14</i>	<i>"</i>	<i>"</i>	<i>MM. BALD EAGLE, BONAPARTE'S GULL, TRICOLOR HERON, LITTLE BLUE HERON, CASPIAN TERN</i>	
<i>15</i>	<i>27.969679</i>	<i>-82.576619</i>	<i>BLACK SKIMMER</i>	<i>102 (BENT. DAVIS BEACH)</i>
<i>16</i>	<i>"</i>	<i>"</i>	<i>LESSER SCAUP</i>	<i>220</i>
<i>17</i>	<i>"</i>	<i>"</i>	<i>RING-BILLED GULL</i>	<i>62</i>
<i>18</i>	<i>"</i>	<i>"</i>	<i>LAUGHING GULL</i>	<i>105</i>
<i>19</i>	<i>27.961982</i>	<i>-82.569628</i>	<i>HORNED GREBE</i>	<i>60 (RUSTY PELICAN - ROCKY PT.)</i>
<i>20</i>	<i>27.973441</i>	<i>-82.594267</i>	<i>DUNLIN</i>	<i>840</i>
<i>21</i>	<i>"</i>	<i>"</i>	<i>LEAST SANDPIPER</i>	<i>200</i>
<i>22</i>	<i>"</i>	<i>"</i>	<i>WILLET</i>	<i>106</i>
<i>23</i>	<i>"</i>	<i>"</i>	<i>SHORT-BILLED DOWITCHER</i>	<i>80</i>
<i>24</i>	<i>"</i>	<i>"</i>	<i>SEMI-PALMATED PLOVER</i>	<i>60</i>
<i>25</i>	<i>"</i>	<i>"</i>	<i>BLACK-BELLIED PLOVER</i>	<i>45</i>

2011 Winter Shorebird Survey- Data Sheet

Site Information & GPS Locations- Page 2

Site name and description: COURTNEY CAMPBELL CAUSEWAY, CLEARWATER TO TAMPA FL.		Observer (lead): ROBERT LANE	Email:
Start time: 6:45 AM		Other observers: DENISE LANE JANE MANN STEPHEN MANN	Phone:
End time: 4:30 PM	Weather conditions: MORNING, LIGHT FOG, OVERCAST AFTERNOON, SUNNY		

GPS Locations.

Please use a GPS unit or internet tools such as GET LAT LON. (<http://www.getlatlon.com/>) to determine latitude and longitude for the following:

- 1) Length of your survey route: start point (A) and point furthest from the start (B).
- 2) Individuals or groups of Snowy Plovers, Piping Plover, Wilson's Plover, and Red Knot.
- 3) Color-banded individuals.
- 4) Large aggregations or flocks of birds (i.e. 50+); note species composition and number of birds.

Point	Latitude	Longitude	Species	Information
	27.958717	-82.708000	not applicable	Start point of your survey route.
	27.961938	-82.568971	not applicable	Furthest point of your route from the start point (A).
1				
2				
3				
4				
5				
26	27.972227	-82.611150	LESSER SCAUP	100
27	"	"	RED-WINGED BLACKBIRD	50
28	27.970869	-82.619801	BROWN PELICAN	80
29	"	"	RUDDY TURNSTONE	44
30	"	"	LESSER SCAUP	46
31	27.965154	-82.666149	LESSER SCAUP	90
32			BROWN PELICAN	42
33			DOUBLE-CRESTED CORMORANT	67
34			HORNED GREBE	260
15				
16				
17				
18				
19				
20				
21				
22				
23				
24				
25				

2011 Winter Shorebird Survey- Data Sheet

Site Name:

Observer (lead):

COURTNEY CAMPBELL CAUSEWAY

ROBERT LANE

Species	T	Species	T	Species	T
DUCKS		Caspian Tern	5	RAPTORS	
Redhead	0	Royal Tern	11	Osprey	12
Ring-necked Duck	0	Common Tern	0	Bald Eagle - Adult	1
Greater Scaup	0	Forster's Tern	16	Bald Eagle - Immature	1
Lesser Scaup	464	Sandwich Tern	0	Northern Harrier	0
Scaup sp.	0	Black Skimmer	138	Sharp-shinned Hawk	0
Surf Scoter	0			Cooper's Hawk	1
White-winged Scoter	0	SHOREBIRDS		Red-shouldered Hawk	0
Black Scoter	0	Black-bellied Plover	58	Red-tailed Hawk	0
Scoter sp.	0	Wilson's Plover	2	American Kestrel	0
Long-tailed Duck	0	Semipalmated Plover	130	Merlin	0
Bufflehead	0	Piping Plover	0	Peregrine Falcon	0
Common Goldeneye	0	Snowy Plover	0		
Hooded Merganser	0	Killdeer	0	WADING BIRDS	
Red-breasted Merg.	7	American Oystercatcher	64	Great Blue Heron	4
Ruddy Duck	0	American Avocet	0	Great Egret	3
		Greater Yellowlegs	0	Snowy Egret	52
SEABIRDS		Lesser Yellowlegs	0	Little Blue Heron	38
Red-throated Loon	0	Yellowlegs sp.	0	Tricolored Heron	4
Common Loon	38	Willet	183	Reddish Egret	11
Horned Grebe	348	Spotted Sandpiper	6	White Ibis	31
Northern Gannet	0	Whimbrel	0	Wood Stork	0
Brown Booby	0	Long-billed Curlew	0		
Brown Pelican	153	Marbled Godwit	1	OTHER SPECIES	
White Pelican	0	Ruddy Turnstone	115	BELTED KINGFISHER	3
Double-crested Corm.	91	Red Knot	6	RED-WINGED BLACKBIRD	50
Pomarine Jaeger	0	Sanderling	112	FISH CROW	14
Parasitic Jaeger	0	Western Sandpiper	32	ROCK DOVE	42
Laughing Gull	244	Least Sandpiper	670	EUROPEAN STARLING	46
Bonaparte's Gull	26	Purple Sandpiper	0	TURKEY VULTURE	10
Ring-billed Gull	209	Dunlin	961	ANHINGA	8
Herring Gull	4	Short-billed Dowitcher	169	YELLOW-CROWNED NIGHT HERON	6
Lesser Bk-backed Gull	0	Long-billed Dowitcher	0	BROWN-HEADED COWBIRD	8
Great Bk-backed Gull	0	Dowitcher sp.	0	MOURNING DOVE	6
Franklin's Gull	0	Wilson's Snipe	0	RED-BELLIED WOODPECKER	1
Glaucous Gull	0	Purple Sandpiper	0	MOURNING DOVE	6
Vega Gull	0	Wilson's Snipe	0	RED-BELLIED WOODPECKER	1
				MOURNING DOVE	6
				RED-BELLIED WOODPECKER	1
				MULLARD DUCK	3
				MOTTLED DUCK	1
				EUR-COL. DOVE	3
				MOURNING DOVE	6
				RED-BELLIED WOODPECKER	1
				MULLARD DUCK	3
				MOTTLED DUCK	1

TOTAL 4421

GRAND TOTAL 4644

CONSTRUCTION SPECIAL PROVISIONS STURGEON PROTECTION GUIDELINES

The shortnose sturgeon (*Acipenser brevirostrum*) and the gulf sturgeon (*A. oxyrinchus desotoi*) are listed under the Endangered Species Act as endangered and threatened, respectively. These species are under the jurisdiction of the National Marine Fisheries Service (NMFS) and the U.S. Fish and Wildlife Service (USFWS). In Florida, the lower St Johns River is habitat for shortnose sturgeon. Major portions of the Suwannee and Withlacoochee Rivers are designated as critical habitat for the gulf sturgeon.

The following special provisions will be incorporated into any construction contract where involvement with sturgeon may occur:

The FDOT will coordinate with the NMFS and USFWS early in the project development stage of new bridge projects. All efforts should be made to avoid known spawning habitats, nursery areas, feeding areas and thermal refuges.

1. Advise construction personnel of the potential presence of these species, of their endangered status and federal protection, and of the need to avoid any actions that would jeopardize these species.
2. The Florida Department of Transportation (FDOT) shall advise all FDOT project personnel and Contractor personnel on the project that there are civil and criminal penalties for harming, harassing or killing sturgeon, which are protected under the Endangered Species Act of 1973. The FDOT and the Contractor will be held responsible for any sturgeon harmed, harassed, or killed as a result of the project activity.
3. The FDOT shall provide information to all FDOT and Contract personnel for identification of sturgeon.
4. Appropriate work shift personnel will be instructed in the appearance, habits, biology, migratory patterns, and preservation of sturgeon. At least one of these trained personnel will be on site during construction activities to maintain a constant surveillance for these species, assure the cessation of activities (such as dredging, excess turbidity, and construction barge activity), which may endanger these species, and assure that uninhibited passage for the animals is provided.
5. Post signs on site warning of the presence of sturgeon, of their endangered status, and precautions needed.
6. Turbidity from construction activity will be adequately controlled to prevent degradation of the quality and transparency of the water. When sturgeon are present, turbidity curtains of appropriate dimension will be used to restrict the

animals access to the work area. Pollution booms or turbidity curtains should use tangle resistant or hemp rope when anchoring, or employ surface anchors to prevent entangling sturgeon. Continuous surveillance will be maintained in order to free animals which may become trapped in silt or turbidity barriers.

7. No dredging of the river bottom will be conducted for barge access.
8. Drilled shaft pile construction will be used whenever prudent and feasible as determined by FDOT.
9. Care shall be taken in lowering equipment or material below the water surface and into the stream bed. These precautions will be taken to ensure no harm occurs to any sturgeon which may have entered the construction area undetected.
10. Construction debris shall not be discarded into the water.
11. If the use of explosives is necessary, no blasting will occur during sturgeon spawning season or in known spawning, staging, feeding, or vital nursery areas.

The following protection measures will be employed for blasting:

- A. For each explosive charge, detonation will **not** occur if a sturgeon is known to be within a circular area ("the danger zone") encompassing the detonation site defined by the following radius:

$$r = 560(\sqrt[3]{W})$$

Where: r = radius of danger zone in feet

W = weight of explosive charge in pounds (tetra or TNT)

- B. In the event that a sturgeon is killed during blasting, the NMFS and/or the USFWS will be notified immediately.
12. Any dead sturgeon will be secured on site for carcass analysis by notified agency representative.
13. Following completion of the project, a report summarizing any involvement with sturgeon will be prepared for NMFS and/or USFWS.

**Dock Construction Guidelines in Florida for Docks or Other Minor Structures
Constructed in or over Submerged Aquatic Vegetation (SAV), Marsh or Mangrove Habitat
U.S. Army Corps of Engineers/National Marine Fisheries Service
August 2001**

Submerged Aquatic Vegetation:

1. Avoidance. The pier shall be aligned so as to minimize the size of the footprint over SAV beds.
2. The height of pier shall be a minimum of 5 feet above MHW/OHW as measured from the top surface of the decking.
3. The width of the pier is limited to a maximum of 4 feet. A turnaround area is allowed for piers greater than 200 feet in length. The turnaround is limited to a section of the pier no more than 10 feet in length and no more than 6 feet in width. The turnaround shall be located at the midpoint of the pier.
4. Over-SAV bed portions of the pier shall be oriented in a north-south orientation to the maximum extent that is practicable.
 5. a. If possible, terminal platforms shall be placed in deep water, waterward of SAV beds or in an area devoid of SAV beds.
 - b. If a terminal platform is placed over SAV areas and constructed of grated decking, the total size of the platform shall be limited to 160 square feet. The grated deck material shall conform to the specifications stipulated below. The configuration of the platform shall be a maximum of 8 feet by 20 feet. A minimum of 5 feet by 20 feet shall conform to the 5-foot height requirement; a 3 feet by 20 feet section may be placed 3 feet above MHW to facilitate boat access. The long axis of the platform should be aligned in a north-south direction to the maximum extent that is practicable.
 - c. If the terminal platform is placed over SAV areas and constructed of planks, the total size of the platform shall be limited to 120 square feet. The configuration of the platform shall be a maximum of 6 feet by 20 feet of which a minimum 4-foot wide by 20-foot long section shall conform to the 5-foot height requirement. A section may be placed 3 feet above MHW to facilitate boat access. The 3 feet above MHW section shall be cantilevered. The long axis of the platform should be aligned in a north-south direction to the maximum extent that is practicable. If the 3 feet above MHW section is constructed with grating material, it may be 3 feet wide.
6. One uncovered boat lift area is allowed. A narrow catwalk (2 feet wide if planks are used, 3 feet wide if grating is used) may be added to facilitate boat maintenance along the outboard side of the boat lift and a 4-foot wide walkway may be added along the stern end of the boat lift, provided all such walkways are elevated 5 feet above MHW. The catwalk shall be cantilevered from the outboard mooring pilings (spaced no closer than 10 feet apart).
7. Pilings shall be installed in a manner which will not result in the formation of sedimentary deposits("donuts" or "halos") around the newly installed pilings. Pile driving is the preferred method of installation, but jetting with a low pressure pump may be used.
8. The spacing of pilings through SAV beds shall be a minimum of 10 feet on center.
9. The gaps between deckboards shall be a minimum of ½ inch.

Marsh:

1. The structure shall be aligned so as to have the smallest over-marsh footprint as practicable.
2. The over-marsh portion of the dock shall be elevated to at least 4 feet above the marsh floor.
3. The width of the dock is limited to a maximum of 4 feet. Any exceptions to the width must be accompanied by an equal increase in height requirement.

Mangroves.

1. The width of the dock is limited to a maximum of 4 feet.
2. Mangrove clearing is restricted to the width of the pier.
3. The location and alignment of the pier should be through the narrowest area of the mangrove fringe.

Grid Specifications and Suppliers

The following information does not constitute a U.S. Army Corps of Engineers endorsement or advertisement for any particular provider and is provided only as an example for those interested in obtaining these materials for dock construction. A type of fiberglass grate panel is manufactured by SeaSafe (Lafayette, LA; phone: 1-800-326-8842) and FiberGrate (1-800-527-4043). Plastic grate panels are also available from Southern Pine Lumber Company (Stuart, FL; phone: 772-692-2300). Panels are available in a variety of sizes and thicknesses. For safety, the grate should contain an anti-slip texture which is integrally molded into the top surface. The manufacturer or local distributor should be consulted to ensure that the load-bearing capacity of the selected product is sufficient to support the intended purpose. Contact the manufacturer(s) for product specifications and a list of regional distributors.



Florida Department of Transportation

**RICK SCOTT
GOVERNOR**

11201 McKinley Drive MS 7-500
Tampa, FL 33612

**OFFICE OF THE
SECRETARY**

February 25, 2011

**Ms. Jane Monaghan
U.S. Fish and Wildlife Service
7915 Baymeadows Way, Suite 200
Jacksonville, FL 32256-7517**

**RE: WPI Segment No: 422640-2
State Road (SR) 60 (Courtney Campbell Causeway) Multi-Use Trail
From Bayshore Boulevard to West of the Ben T. Davis Beach Entrance
Pinellas and Hillsborough Counties**

Dear Ms. Monaghan,

The Florida Department of Transportation (Department) is conducting a Project Development and Environment (PD&E) Study to construct a multi-use trail within the limits noted above. The study corridor is the existing Courtney Campbell Causeway (Causeway). The Causeway consists of fill material that was used to construct SR 60. The entire project will be located on the existing fill material with the exception of two proposed bridges over Old Tampa Bay, adjacent to the existing SR 60 bridges. The proposed bridges, at a minimum, will meet the existing horizontal and vertical clearances of the existing SR 60 bridges.

As a part of conducting this study, the Department is initialing informal consultation with the U.S. Fish and Wildlife Service (USFWS). In order to fulfill the requirements of the various federal and state environmental and regulatory processes the Department is soliciting comments from federal, state, and local agencies. A Draft Wetland Evaluation and Biological Assessment Report (WEBAR) has been prepared for the study. This report is attached for your review.

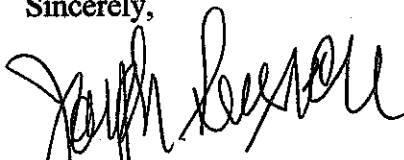
This project has been evaluated for impacts on federally protected threatened and endangered species. Based on the results of the study conducted, the Department has concluded that the West Indian manatee, wood stork, gulf sturgeon, piping plover, and loggerhead, leatherback, Kemp's Ridley, and green sea turtles may utilize habitat within the project area. It is anticipated that this project will have minimal temporary impacts to habitat during construction of the

proposed bridges, but will not have any permanent adverse effects to these species or their habitat. As mentioned above, this project will be constructed on the existing fill limits of the Causeway, with the exception of the proposed bridges.

No suitable foraging habitat for the wood stork and no critical habitat for the gulf sturgeon, West Indian manatee, and piping plover will be impacted by the construction of the proposed multi-use trail. There is no suitable nesting habitat for sea turtles located within the project area; therefore no impacts to sea turtle nesting are anticipated for this project. The Department will develop a wildlife watch plan, which includes the FFWCC "Standard Manatee Conditions for In-Water Work", during the design and permitting phase and will adhere to the guidelines set in this plan during construction. The Department will coordinate this plan with the USFWS. Therefore, the Department, on behalf of the Federal Highway Administration (FHWA), has determined that the proposed actions will have a "No Effect" determination for the wood stork, and a "May Affect, Not Likely to Adversely Affect" determination for the West Indian manatee, gulf sturgeon, piping plover, and sea turtles.

If your office concurs with this determination, please respond to the Department in writing by March 10, 2011. If your agency would like a site review or any additional information, please feel free to call me at (813) 975-6455.

Sincerely,



Joseph Severson
Environmental Specialist

cc: Robin Rhinesmith
Roberto Gonzalez

Enclosed: Draft Wetland Evaluation Biological Assessment Report



April 18, 2011

**Florida Fish
and Wildlife
Conservation
Commission**

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Assistant Executive Director

Karen Ventimiglia
Deputy Chief of Staff

Division of Habitat and
Species Conservation
Timothy A. Breault
Director
(850)488-3831
(850)921-7793 FAX

*Managing fish and wildlife
resources for their long-term
well-being and the benefit
of people*

620 South Meridian Street
Tallahassee, Florida
32399-1600
Voice: (850) 488-4676

Hearing/speech impaired:
(800) 955-8771 (T)
(800) 955-8770 (V)

MyFWC.com

Mr. Joseph Severson
Environmental Specialist
Florida Department of Transportation (FDOT) District Seven
11201 N. McKinley Drive
Tampa, FL 33612-6456

Re: State Road (SR) 60 Multi-Use Trail, Hillsborough and Pinellas Counties, Draft
Wetland Evaluation Biological Assessment Report, Project Development and
Environment (PD&E) Study

Dear Mr. Severson:

The Division of Habitat and Species Conservation, Habitat Conservation Scientific Services Section, of the Florida Fish and Wildlife Conservation Commission (FWC) has coordinated an agency review of the Draft Wetland Evaluation Biological Assessment Report (WEBAR) for the above-referenced project, and offers the following comments. The WEBAR was prepared as part of the PD&E Study for the proposed project.

The project involves the construction of a paved, multi-use trail adjacent to SR 60 across the Courtenay Campbell Causeway in Old Tampa Bay. The trail would be constructed on the south side of SR 60, and would include two independent bridge structures parallel to the existing bridges for the highway.

The FWC evaluated this project as Efficient Transportation Decision Making (ETDM) project #13102 in January of this year. At that time, we ranked the project's potential direct and indirect impacts to fish and wildlife resources as substantial, due to the uncertainty as to where the trail would be proposed for construction (north or south of SR 60), and the potential for the trail to impact both mangrove and herbaceous wetlands, as well as seagrass beds at the bridge crossings.

The WEBAR evaluated potential project impacts to 19 wildlife species classified by the federal Endangered Species Act as Endangered or Threatened, or by the State of Florida as Threatened or Species of Special Concern, and also the bald eagle, which is protected by the federal Bald and Golden Eagle Protection Act. Project biologists made a finding of "no effect" for 3 of these species: the Gulf sturgeon, wood stork, and bald eagle. The WEBAR determined that the project "may affect, but is unlikely to adversely affect" all other evaluated species, including the loggerhead sea turtle, Kemp's ridley sea turtle, green sea turtle, leatherback sea turtle, piping plover, snowy plover, American oystercatcher, black skimmer, brown pelican, least tern, little blue heron, tricolored heron, reddish egret, snowy egret, roseate spoonbill, white ibis, and Florida manatee. It is unclear why the wood stork was given a "no effect" determination while the other wading birds were included in the "may affect, but is unlikely to adversely affect" list of species. If there is the potential for wading bird feeding areas like wetlands or shallow seagrass beds to be impacted by this project, then the effects on all the wading bird species should be similarly classified. Otherwise, we concur with the evaluations in the WEBAR.

We also support the project commitments to provide mitigation for any wetland impacts, to conduct seagrass surveys during the growing season and provide mitigation for all direct and indirect impacts to seagrass beds, to conduct a pre-construction survey for bald eagle nests within 660 feet of the project, and to develop a wildlife watch plan which includes the FWC's *Standard Manatee Conditions for In-Water Work*. Further coordination with our agency will be necessary in order to determine site-specific measures for this project. For technical assistance and coordination on manatees and sea turtles, respectively, please contact Ms. Mary Duncan and Dr. Robbin Trindell of our Imperiled Species Management Section in Tallahassee at (850) 922-4330.

Thank you for the opportunity to review the WEBAR for the SR 60 Multi-Use Trail project in Hillsborough and Pinellas counties. Please contact Mr. Brian Barnett at (850) 528-6316 or email brian_barnett@urscorp.com to initiate the process for further overall coordination on this project.

Sincerely,



Scott Sanders
Habitat & Species Conservation Section Leader

ss/bb

ENV 1-13-2

Courtney Campbell Causeway_3343_041811

cc: Brian Barnett, URS Corporation, Vero Beach
Mary Duncan, FWC, Tallahassee
Robbin Trindell, FWC, Tallahassee



FLORIDA DEPARTMENT OF STATE
Kurt S. Browning
DIVISION OF HISTORICAL RESOURCES

Ms. Linda Anderson
US Department of Transportation
Federal Highway Administration/ Florida Division Office
545 John Knox Road, Suite 200
Tallahassee, Florida 32303

April 27, 2011

RE: DHR Project File No.: 2011-00919B/Additional Information Received by DHR: April 26, 2011
Financial Project ID No: 422640-2/FAP No.: 9045-090-C
Project: *Cultural Resource Assessment Survey: SR 60 (Courtney Campbell Causeway)
Multi-Use Trail PD&E Study from Bayshore Boulevard to West of Ben T. Davis
Beach Entrance, Pinellas and Hillsborough Counties*

Dear Ms. Anderson:

This office received and reviewed the above referenced project in March 2011. As a result of the review, this office requested additional information on the presence of submerged cultural resources within the proposed project area. This office subsequently participated in a series of meetings with staff from the Florida Department of Transportation (FDOT) Central Environmental Management Office (CEMO) and FDOT District 7 discussing this project. After reviewing additional information provided by the FDOT District 7, this office is recommending that the proposed project will have no effect on properties listed or eligible for listing on the National Register of Historic Places.

This office would like to commend both the FDOT CEMO staff and FDOT District 7 staff on the consultation efforts undertaken for this project. If you have any questions, please contact Ginny Jones, Architectural Historian, Transportation Compliance Review Program, via email gljones@dos.state.fl.us, or at 850.245.6333.

Sincerely,

Laura A. Kammerer
Deputy State Historic Preservation Officer
For Review and Compliance

PC: Roberto Gonzalez, FDOT District 7, Tampa
Rebecca Spain Schwarz, Atkins, Tampa
Roy Jackson, FDOT CEMO, Tallahassee/#5500
Nahir DeTizio, FHWA, Tallahassee
Robin Rhinesmith, FDOT District 7, Tampa

500 S. Bronough Street • Tallahassee, FL 32399-0250 • <http://www.flheritage.com>

Director's Office
(850) 245-6444 • FAX: 245-6452

Archaeological Research
(850) 245-6333 • FAX: 245-6437

Historic Preservation
(850) 245-6400 • FAX: 245-6433



FLORIDA DEPARTMENT OF STATE
Kurt S. Browning
DIVISION OF HISTORICAL RESOURCES

Ms. Linda Anderson
US Department of Transportation
Federal Highway Administration
Florida Division Office
545 John Knox Road, Suite 200
Tallahassee, Florida 32303

March 15, 2011

RE: DHR Project File No.: 2011-00919/Received by DHR: March 4, 2011
Financial Project ID No: 422640-2/FAP No.: 9045-090-C
Project: *Cultural Resource Assessment Survey: SR 60 (Courtney Campbell Causeway)
Multi-Use Trail PD&E Study from Bayshore Boulevard to West of Ben T. Davis
Beach Entrance*
County: Pinellas and Hillsborough

Dear Ms. Anderson:

This office received and reviewed the above referenced project in accordance with Section 106 of the National Historic Preservation Act of 1966 as amended, 36 CFR Part 800: Protection of Historic Properties, and Chapter 267, *Florida Statutes*. It is the responsibility of the State Historic Preservation Officer to advise and assist, as appropriate, Federal and State agencies in carrying out their historic preservation responsibilities; to cooperate with agencies to ensure that historic properties are taken into consideration at all levels of planning and development; and to consult with the appropriate agencies in accordance with the National Historic Preservation Act of 1966 as amended, on undertakings that may affect historic properties and the content and sufficiency of any plans developed to protect, manage, or to reduce or mitigate harm to such properties.

This proposed project involves the construction of a multi-use trail parallel to the existing SR 60/Courtney Campbell Causeway. The project will require the construction of two new bridges placed to the south, and parallel, of two existing vehicular bridges that span Tampa Bay. Archaeological Consultants, Inc. completed an architectural survey of the project area in December 2010. The survey resulted in the identification and recordation of one historic structure (8PII1966). Background research revealed the presence of one previously recorded archaeological site (8HI456). The Florida Department of Transportation recommended that both of the resources are not eligible for listing on the National Register of Historic Places (NRHP). This office concurs with the recommendations regarding the historic structure (8PII1966) and the archaeological site (8HI456).

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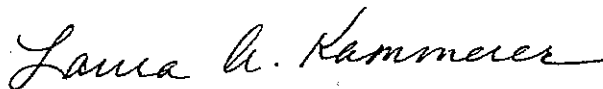
- Director's Office (850) 245-6444 • FAX: 245-6452
- Archaeological Research (850) 245-6333 • FAX: 245-6437
- Historic Preservation (850) 245-6400 • FAX: 245-6433

Ms. Linda Anderson
DHR Project File Number: 2011-00919
March 15, 2011
Page 2

However, based on the likelihood of prehistoric and historic underwater resources and the nature of the proposed project we are recommending that an underwater survey be conducted to locate and assess any underwater resources. The underwater survey should include the use of remote sensing and diver verification of potentially significant anomalies. The results of the analysis will determine if significant cultural resources would be disturbed by the construction of the two pedestrian bridges. In addition, if significant remains are located, the data described in the report and the consultant's conclusions will assist this office in determining measures that must be taken to avoid, minimize, or mitigate adverse impacts to archaeological sites and historical properties listed, or eligible for listing in the NRHP, or otherwise significant.

This office looks forward to receiving the results of the underwater survey. If you have any questions, please contact Ginny Jones, Architectural Historian, Transportation Compliance Review Program, via email gljones@dos.state.fl.us, or at 850.245.6333.

Sincerely,



Laura A. Kammerer
Deputy State Historic Preservation Officer
For Review and Compliance

PC: Roberto Gonzalez, FDOT District 7, Tampa
Rebecca Spain Schwarz, PBS&J, Tampa
Roy Jackson, FDOT CEMO, Tallahassee/#5500
Nahir DeTizio, FHWA, Tallahassee



Florida Department of Transportation

11201 N. McKinley Drive Tampa, FL 33612-6456 Phone (813) 975-6000 1-800-226-7220

RICK SCOTT
GOVERNOR

STEPHANIE C. KOPELOUSOS
SECRETARY

February 2, 2011

Ms. Linda Anderson
Federal Highway Administration
Florida Division
545 John Knox Road, Suite 200
Tallahassee, Florida 32303

RE: WPI Segment No: 422640 2; FAP No. 9045-090-C
SR 60 (Courtney Campbell Causeway) Multi-Use Trail PD&E Study
From Bayshore Boulevard to West of Ben T. Davis Beach Entrance
Pinellas and Hillsborough Counties

Dear Ms. Anderson:

Enclosed you will find two copies of the Cultural Resource Assessment Survey (February 2011), one original Florida Master Site File (FMSF) form (8PI11966), and one Survey Log Sheet for the above referenced Project Development and Environment (PD&E) Study project. The Florida Department of Transportation (FDOT) is conducting this PD&E Study to evaluate a proposed multi-use trail along approximately 8 miles of SR 60 (Courtney Campbell Causeway) from Bayshore Boulevard in Pinellas County to west of Ben T. Davis Beach entrance in Hillsborough County.

The Cultural Resource Assessment Survey (CRAS) documents the results of background research (archaeological and historical) and historical/architectural field survey. The scope of work did not include archaeological field survey. With minor exception, the project corridor is comprised of dredged fill, and considered to have a low potential for in situ archaeological sites. The project Area of Potential Effect (APE) for the historical/architectural field survey was defined as the existing SR 60 right of way and adjacent properties.

Background research indicated that one previously recorded archaeological site, the Ben Davis Municipal Beach Site (8HI456), is located within the project APE, at its eastern terminus. This site is comprised of redeposited dredged fill, and is not considered eligible for listing in the National Register of Historic Places (NRHP).

Ms. Linda Anderson
WPI Segment No: 422640 2; FAP No. 9045-090-C
SR 60 (Courtney Campbell Causeway) Multi-Use Trail PD&E Study
February 2, 2011
Page 2

Background research identified no previously recorded historic structures within the project APE. As a result of the historical/architectural field survey, one historic resource, a circa 1957 masonry vernacular style building (8PI11966) was identified but is not considered eligible for listing in the NRHP.

This proposed project will have no impact on any cultural resources, including archaeological sites and historic structures that are listed, determined eligible, or considered potentially eligible for listing in the NRHP.

Provided your office concurs with the findings, please transmit one copy of the CRAS, the FMSF form and the Survey Log Sheet to the State Historic Preservation Officer (SHPO) for review and concurrence. If you have any questions, please do not hesitate to call me at (813) 975-6923 or via email at roberto.gonzalez@dot.state.fl.us.

Sincerely,



Roberto G. Gonzalez
Environmental/Hazardous Materials
Administrator

RGG/rss

Enclosures

cc: Nahir DeTizio (FHWA); Roy Jackson (FDOT CEMO);
Robin Rhinesmith (FDOT D7); Rebecca Spain Schwarz (PBS&J);
Jeff Novatney (ACE); Joan Deming (ACI)

Ms. Linda Anderson
WPI Segment No: 422640 2; FAP No. 9045-090-C
SR 60 (Courtney Campbell Causeway) Multi-Use Trail PD&E Study
February 2, 2011
Page 3

The FHWA finds the attached Cultural Resources Assessment Report complete and sufficient and approves / does not approve the above recommendations and findings.

The FHWA requests the SHPO's opinion on the sufficiency of the attached report and the SHPO's opinion on the recommendations and findings contained in this cover letter and in the comment block below.

FHWA Comments:

PLEASE ADDRESS COMMENTS/OPINION TO LINDA ANDERSON, FHWA,
P: 850-553-2226, e: linda.anderson@dot.gov.
PLEASE CC: ROBERTO GONZALEZ, FDOT D7; NAHIR DETIZIO, FHWA;
AND BOB JACKSON, FDOT C&M

S/



Martin C. Knopp
Division Administrator
Florida Division
Federal Highway Administration

3/1/11
Date

The Florida State Historic Preservation Officer finds the attached Cultural Resources Assessment Report complete and sufficient and concurs with the recommendations and findings provided in this cover letter for SHPO/DHR Project File Number _____.

Scott M. Stroh, III
State Historic Preservation Officer
Florida Division of Historical Resources

Date



March 28, 2011

CITY OF TAMPA GREENWAYS
AND TRAILS CITIZENS
ADVISORY COMMITTEE
Sally Thompson, Chair
Randy Stribling, Vice-Chair

Ming Gao
Intermodal Systems Development Manager
FDOT-District 7
11201 N McKinley Drive, MS 7-500
Tampa, FL 33612-6456

Dear Mr. Gao,

The City of Tampa Greenways and Trails Citizen Advisory Committee would like to express our support of the proposed Courtney Campbell Causeway trail and pedestrian bridge. This trail is included in the City of Tampa Greenways and Trails Master Plan that was adopted by City Council in 2001. It is also an important connection in a regional trail that would link Hillsborough and Pinellas Counties.

We commend the Florida Department of Transportation for supporting and pursuing funding for this multi-modal transportation effort.

Sincerely,

Sally Thompson, Chair
City of Tampa Greenways and Trails Citizen Advisory Committee

FOR MORE INFORMATION
Karla Price, RLA, ASLA
Tampa Parks and Recreation
Department
3402 W. Columbus Drive
Tampa, FL 33607
(813) 274-5134 (phone)
(813) 274-7429 (fax)
www.tampagov.net/dept_parks/trails

Xc: Karen Palus, Director, City of Tampa Parks and Recreation
Lori Snively, Bicycle/Pedestrian/Public Involvement Coordinator, FDOT

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Chair
Vice Mayor William D. Dodson

Vice-Chair
Commissioner Larry Bustle

Secretary/Treasurer
Mayor Robert Manning

Executive Director
Manny Pumariega

April 4, 2011

Mr. Ming Gao
Florida Department of Transportation
District 7, SIS Coordinator Alternate
11201 N. Malcolm McKinley Dr., MS 7-500
Tampa, Florida 33612-6403

Dear Mr. Gao:

It is my pleasure to write a letter in support of the FDOT, District 7, State Road 60 Multi-Use Trail project.

The trail will provide bicycle and pedestrian access along an eight-mile stretch from Bayshore Blvd. in Pinellas County to west of the Ben T. Davis Beach entrance in Hillsborough County.

On May 23, 2005 the Courtney Campbell Highway was designated as a Florida Scenic Highway. One of the requirements of this designation was the development of a Corridor Management Plan. An important element of that Plan was the goal of completing a multi-use trail across the full length of the causeway. The Courtney Campbell Scenic Highway Corridor Advisory Committee (CCSH CAC) finds the proposed S.R. 60 Multi-Use Trail project consistent with the Goals and Policies found in its Corridor Management Plan.

In addition the CCSH CAC would like to encourage the utilization of public art reflecting the intrinsic resources found along the trail to enhance visual interest for the trail users.

In conclusion, the Courtney Campbell Scenic Highway Corridor Advisory Committee fully supports the SR 60 Multi-Use Trail Project and the ability for the citizens of the Tampa Bay region to be able to travel safely from Pinellas County to Hillsborough County without having to use an automobile.

Sincerely,

Manny L. Pumariega
Executive Director

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APPENDIX C

Public Hearing Transcript



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1
2 SR 60 (COURTNEY CAMPBELL CAUSEWAY)
3 PUBLIC HEARING
4 MULTI-USE TRAIL PD&E STUDY

5 SESSION 1

6 DATE: Thursday, March 24, 2011
7 TIME: 5 p.m. to 7 p.m.
8 PLACE: Clearwater Christian College
9 3400 Gulf-to-Bay Boulevard
10 Building D
11 Clearwater, Florida 33759

12 REPORTED BY: Rachelle I. Castellana, CSR
13 Certified Shorthand Reporter
14 Notary Public
15 State of Florida

16 SESSION 2

17 DATE: Tuesday, March 29, 2011
18 TIME: 5 p.m. to 7p.m.
19 PLACE: The Westin Tampa Bay
20 7627 West Courtney Campbell
21 Causeway, BluVu Room
22 Tampa, Florida 33607

23 REPORTED BY: Rachelle I. Castellana, CSR
24 Certified Shorthand Reporter
25 Notary Public
State of Florida

I N D E X

SESSION 1

Public Comments 3

Formal Presentation 4

A T T A C H M E N T S

Copy of letter dated 3/24/11 from Marcia Biggs,
Tampa Bay Sierra Club

SESSION 2

Formal Presentation 16

Certificate of Reporter 30

A T T A C H M E N T S

Copy of Letter dated 3/28/11 from Sally Thomsson, City of
Tampa Greenways and Trails Citizen Advisory Committee

Copy of letter from Chris Weber, Westshore Alliance

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1 THOMAS CUBA
2 3760 1ST AVENUE NORTH
3 ST. PETERSBURG, FLORIDA 33713
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5

6 MR. CUBA: My name is Tom Cuba. I live at 3760
7 1st Avenue North, St. Petersburg, Florida 33713. This
8 looks like a very nice project. My only comment is
9 \$14 trillion in debt and we can probably wait a few
10 years until we get out of that. Okay.

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SESSION 1

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2 MR. BOGEN: Good evening. Today is Thursday,
3 March 24th, 2011, and it is approximately 6 p.m. We
4 are assembled at the Clearwater Christian College in
5 Clearwater, Florida.

6 My name is Kirk Bogen, and I am the Project
7 Development Engineer for District Seven of the Florida
8 Department of Transportation. Welcome to the public
9 hearing for the State Road 60, Courtney Campbell
10 Causeway Multi-use Trail Project Development and
11 Environment Study or PD&E Study.

12 This public hearing is being held relative to
13 Work Program Item Segment Number: 422640-2 and Federal
14 Aid Project Number 9045-090 C. The proposed
15 improvements include adding a multi-use trail along
16 the Courtney Campbell Causeway from Bayshore Boulevard
17 in Pinellas County to west of the Ben T. Davis Beach
18 entrance in Hillsborough County.

19 This public hearing is being held to give all
20 interested persons the right to understand the project
21 and comment on their concerns to the department.
22 Public participation at this hearing is encouraged and
23 solicited without regard to race, color, creed,
24 religion, sex, age, national origin, disability, or
25 family status.

1 This public hearing is being held in accordance
2 with the applicable federal and state laws. Those
3 citations are listed on the board to my right. This
4 public hearing was advertised consistent with federal
5 and state requirements and is being conducted
6 consistent with the Americans with Disabilities Act of
7 1990. This information is also provided in the
8 project brochure.

9 This public hearing is being conducted in two
10 sessions. Both sessions will be combined into a
11 single public hearing record for this project.

12 The first session is tonight, the 24th day of
13 March 2011, at the Clearwater Christian College, 3400
14 Gulf-to-Bay Boulevard in Clearwater, Florida; and the
15 hearing will continue at the second session on
16 Tuesday, March 29th, 2011, at 5 p.m. at The Westin
17 Tampa Bay, 7627 West Courtney Campbell Causeway in
18 Tampa, Florida.

19 This is your opportunity to receive information
20 on the project and officially comment on the
21 recommended "Build" Alternative and the other
22 documents available here tonight. The recommended
23 "Build" Alternative is based on comprehensive
24 environmental and engineering analyses completed to
25 date, as well as on public comments that have been

1 received.

2 This project meets the maximum air quality
3 standards established by the U.S. Environmental
4 Protection Agency (EPA). When you arrived this
5 evening, you should have received an information
6 packet containing an informational newsletter and a
7 comment form.

8 If you weren't able to sign in or did not receive
9 an information packet, please stop by our sign-in
10 table before leaving this evening. You should also
11 have had the opportunity to view the audio-visual
12 presentation that is continuously running throughout
13 this public hearing.

14 On projects such as this, one of the unavoidable
15 consequences is the necessary acquisition of
16 properties and the relocation of families and
17 businesses. However, on this project, we anticipate
18 no property acquisitions and no relocations.

19 Before I continue, I would like to recognize any
20 elected officials or their representatives who are
21 here tonight. If you would, please stand and
22 introduce yourself for the record.

23 MR. JONSON: Bill Jonson. Council member for the
24 City of Clearwater.

25 MR. BOGEN: Thank you, Councilman. Anyone

1 desiring to make a statement or present written views
2 and/or exhibits regarding the location, conceptual
3 design, social, economic, or environmental effects of
4 the improvements will now have an opportunity to do
5 so.

6 You may also make a statement at the public
7 hearing's second session, as noted earlier, on
8 Tuesday, March 29th, 2011.

9 If you have completed a speaker's card, please
10 give them to a Department staff, and it looks like
11 this. If you have not received a speaker's card and
12 wish to speak, please raise your hand so that we can
13 get you a card to complete.

14 Written statements and exhibits may be presented
15 in lieu of or in addition to oral statements. All
16 written statements received at either session of the
17 public hearing and at the Florida Department of
18 Transportation District Seven office located at 11201
19 North McKinley Drive, Tampa, Florida 33612, postmarked
20 no later than April 8, 2011, will become a part of the
21 public record for this hearing.

22 All written comments should be addressed to Ming
23 Gao. The address is noted on the back of the comment
24 form and in the advertisement for this public hearing.

25 We now call upon those who have turned in speaker

1 cards. When you come forward, please state your name
2 and address, for the record.

3 If you represent an organization, municipality or
4 other public body, please provide that information as
5 well. We ask that you limit your comments to 3
6 minutes. If you have additional comments, you may
7 continue with the court reporter after the formal
8 session.

9 Please come to the microphone so that the court
10 reporter can hear you and get a complete record of
11 your comment. The first speaker is Glen Gullickson.

12 MR. GULLICKSON: My name is Glen Gullickson, and
13 I live in Seminole, Florida. I'm here to speak on
14 behalf of the proposed multi-use trail along SR 60
15 Courtney Campbell Causeway from Bayshore Boulevard in
16 Pinellas County to west of Ben. T Davis Beach entrance
17 in Hillsborough County, a distance of 8 miles.

18 Completion of a cross-bay connector for
19 pedestrians and bicycles is important -- is an
20 important step in linking both Pinellas and
21 Hillsborough County trail systems.

22 I strongly encourage the Florida Department
23 of Transportation to approve this proposal. Thank
24 you.

25 MR. BOGEN: Thank you for your comment. The next

1 speaker is Stan Mickelsen. Please state your name and
2 address.

3 MR. MICKELSEN: Stan Mickelsen, and I live at 128
4 Kendale Drive in Safety Harbor. I wanted to speak in
5 support of a trail like this. It's definitely going
6 to make a big difference to cyclists. Cyclists do it
7 recreationally and we also do it just for sheer
8 necessity.

9 It's not unheard of to ride across the Bay on
10 a bicycle. Along Highway 60, you kind of need to have
11 more trails. It's a very heavily traveled road, and I
12 would like encourage everyone to consider that it
13 would facilitate cycling a lot if all roads were
14 simply built with an extra 4 feet of pavement along
15 the side.

16 It doesn't have to be a designated bike trail, it
17 just makes it possible. Now it might not make it
18 possible for all cyclists to use it, but it would
19 benefit all cyclists if most cyclists could use it.

20 It just creates an atmosphere that's friendly to
21 the bicycle. That's what is so needed here.

22 MR. BOGEN: Thank you, sir. Our next speaker is
23 Marsha Biggs.

24 MS. BIGGS: Hi, everybody. My name is Marsha
25 Biggs, and I am the Chair of the Tampa Bay Sierra

1 Club. I'm also a resident of Safety Harbor and a
2 recreational bicycler. I have a comment letter here I
3 am going to submit from the Sierra Club and I'm just
4 going to read it to you real quick now.

5 "This letter serves as an official statement of
6 support by the Tampa Bay Sierra Club for the building
7 and funding of a multi-use trail as a regional
8 connector for non-motorized travel adjacent to the
9 Courtney Campbell Causeway.

10 "The establishment of such a trailway would serve
11 to encourage noncarbon producing modes of
12 transportation such as bicycles, walking, inline
13 skating and running, which would ultimately serve to
14 benefit our local residents.

15 "The Tampa Bay Sierra Club supports the use of
16 alternative forms of transit which would help reduce
17 congestion and carbon emissions and support healthier
18 lifestyles for those living on both sides of the Bay.

19 "Our group, with nearly 1700 members, embraces
20 the natural beauty of this area and many regularly
21 walk and ride our bicycles.

22 "Not only would this trail connect two counties,
23 it would also connect us as neighbors and residents of
24 one regional community. By connecting each end to
25 another trail system, which is the Upper Tampa Bay

1 Trail in Hillsborough and the Ream Wilson Trail in
2 Pinellas, this Causeway stretch would offer an
3 incredibly scenic ride which would easily be the
4 subject of positive regional and national publicity
5 and coverage.

6 "As a recreational bicycler, I personally can
7 assure you I would be one of thousands who would use
8 this trail. Build it and we will come."

9 MR. BOGEN: Thank you. Our next speaker is Bill
10 Thomas.

11 MR. THOMAS: Good evening. My name is Bill
12 Thomas. I'm from Clearwater. I want to speak in
13 opposition of the trail. With our current deficit
14 that's going on in our nation to accept \$21 million
15 worth of federal funds, I believe is -- it 's almost
16 criminal.

17 We need to return that money to the federal
18 government and tell them that for trails and stuff
19 like that that we want to do should be the
20 responsibility of the American people or the citizens
21 of Florida.

22 Two other things. Once the trail is completed,
23 one thing I would like to know about is who is going
24 to be responsible for any liability if someone gets
25 hurt out there? Is it going to be the State of

1 Florida or is it going to be Pinellas County or
2 Hillsborough County, depending; and maintenance of the
3 trail. Who is going to be responsible for that? Is
4 it going to fall on the county level? Thank you.

5 MR. BOGEN: Thank you for your comment. You can
6 talk with us afterwards. We will try to answer some
7 of your questions.

8 MR. THOMAS: Okay. Thank you.

9 MR. BOGEN: Our next speaker is Cathy Maddon or
10 Haddon?

11 MS. HADDON: Haddon. Hi. My name is Cathy
12 Haddon, and my address is 10561 127th Place, Largo,
13 Florida 33773. And I'm just here to voice my
14 opposition to this trail.

15 And as the prior speaker said, in the economy
16 that we face today, we have to stop taking federal
17 money sometime. If the bikers in this room want to
18 provide a trail, then let them pay for it.

19 I would like to see the ridership study that was
20 done and I also would like to know what percentage of
21 the population in Pinellas County are, in fact,
22 cyclists. Thank you.

23 MR. BOGEN: Okay. Our next speaker is Courtney
24 Biehl.

25 MS. BIEHL: My name is Courtney Biehl. My

1 address is 2717 Seville Boulevard, Clearwater,
2 Florida. I am here as a citizen of this area and a
3 user of the Courtney Campbell Causeway, first and
4 foremost.

5 Secondly, representing the Bicycle Advisory
6 Committee of the Pinellas County Metropolitan Planning
7 Organization Bicycle Advisory Committee. Are my 3
8 minutes up yet? We would like to extend our interest
9 in this project. Our support of this project and
10 willingness to partner with you in developing
11 resources and safety through our marketing and public
12 relations and safety committees. Thank you.

13 MR. BOGEN: Thank you. That was my last card.
14 Does anyone else wish to speak?

15 The verbatim transcript of both sessions of the
16 hearing's oral proceedings will be available for
17 inspection at the District Seven Office for public
18 review upon request within 3 weeks. Thank you for
19 attending this public hearing and for providing your
20 input into this project. It is now approximately
21 6:15.

22 I hereby officially close the formal portion of
23 the public hearing's first session for the State Road
24 60, Courtney Campbell Causeway Multi-use Trail PD&E
25 Study.

SESSION 2

1
2 MR. BOGEN: Good evening. Today is Tuesday,
3 March 29th, 2011, and it is approximately 6 p.m. We
4 are assembled at The Westin Tampa Bay in Tampa,
5 Florida.

6 My name is Kirk Bogen, and I am the Project
7 Development Engineer for District Seven of the Florida
8 Department of Transportation. Welcome to the public
9 hearing for the State Road 60, Courtney Campbell
10 Causeway Multi-use Trail Project -- Project
11 Development and Environment Study or PD&E Study.

12 This public hearing is being held relative to
13 Work Program Item -- Item Segment Number: 422640-2 and
14 Federal Aid Project Number 9045-090 C.

15 The proposed improvements involve adding a multi-
16 use trail along the Courtney Campbell Causeway from
17 Bayshore Boulevard in Pinellas County to west of the
18 Ben T. Davis Beach entrance in Hillsborough County.

19 This public hearing is being held to give all
20 interested persons the right to understand the project
21 and comment on their concerns to the Department.
22 Public participation at this hearing is encouraged and
23 solicited without regard to race, color, creed,
24 religion, sex, age, national origin, disability, or
25 family status.

1 This public hearing is being held in accordance
2 with applicable federal and state laws. Those
3 citations are listed on the board next to the sign-in
4 table. This public hearing was advertised consistent
5 with federal and state requirements and is being
6 conducted consistent with the Americans with
7 Disabilities Act of 1990. This information is also
8 provided in the project brochure.

9 This public hearing is being conducted in two
10 sessions. Both sessions will be combined into a
11 single public hearing record for this project.

12 The first session was held on Thursday March
13 24th, 2011, at the Clearwater Christian College, 3400
14 Gulf-to-Bay Boulevard in Clearwater, Florida; and the
15 second session is tonight, the 29th day of March,
16 2011, at The Westin Tampa Bay, 7627 West Courtney
17 Campbell Causeway in Tampa, Florida.

18 This is your opportunity to receive information
19 on the project and officially comment on the
20 recommended "Build" Alternative and other documents
21 available here tonight. The recommended "Build"
22 Alternative is based on comprehensive environmental
23 and engineering analyses completed to date, as well as
24 on public comments that have been received.

25 This project meets the maximum air quality

1 standards established by the U.S. Environmental
2 Protection Agency or EPA.

3 When you arrived this evening, you should have
4 received an information packet -- like this --
5 containing an informational newsletter and a comment
6 form. If you weren't able to sign in or did not
7 receive an information packet, please stop by our
8 sign-in table before leaving this evening.

9 You should also have had the opportunity to view
10 the audio-visual presentation that is continuously
11 running throughout this public hearing.

12 On projects such as this, one of the unavoidable
13 consequences is the necessary acquisition of property
14 and the relocation of family and businesses. On this
15 project, however, we anticipate no property
16 acquisitions and no relocations.

17 Before I continue, I would like to recognize any
18 elected officials or their representatives who are
19 here tonight. If you would, please stand and
20 introduce yourself.

21 MR. JONSON: Bill Jonson. Council member for the
22 City of Clearwater.

23 MR. BOGEN: Thank you, Councilman. Anyone
24 desiring to make a statement or present written views
25 and/or exhibits regarding the location, conceptual

1 design, social, economic, or environmental effects of
2 the improvements will now have an opportunity to do
3 so.

4 If you have completed a speaker's card, please
5 give them to a Department staff member. If you have
6 not received a speaker card and wish to speak, please
7 raise your hand so that we can get you a card to
8 complete. The speaker card looks like this.

9 Written statements and exhibits may be presented
10 in lieu of or in addition to oral statements. All
11 written statements received at either session of this
12 public hearing and at the Florida Department of
13 Transportation District Seven office located at 11201
14 North McKinley Drive, Tampa, Florida 33612, postmarked
15 no later than April 8, 2011, will become a part of the
16 public record for this hearing.

17 All written comments should be addressed to Ming
18 Gao. The address is noted on the back of the comment
19 form and in the advertisement for the public hearing.

20 We now call upon those who have turned in speaker
21 cards. When you come forward, please state your name
22 and address, for the record. If you represent an
23 organization, municipality, or other public body,
24 please provide that information as well.

25 We ask that you limit your comments to 3 minutes.

1 If you have additional comments, you provide them --
2 you may continue with the court reporter after the
3 formal session. Please come to the microphone so that
4 the court reporter can hear you and get a complete
5 record of your comment. The first speaker is Chris
6 Weber.

7 MR. WEBER: Thank you. Chris Weber. I'm
8 representing Westshore Alliance. I have a letter to
9 introduce for the record. I just want to say, the
10 alliance fully supports this project. We appreciate
11 the Department's efforts in working this.

12 We know many people in this room. A lot of
13 people have worked on this for a long time. We are
14 glad to be a part of it. We are willing to help in
15 any way we can and look forward to the project. Thank
16 you.

17 MR. BOGEN: Thank you. Do you want to submit
18 that to one of the staff so that we can give it to the
19 court reporter. The next speaker is John Harrison.

20 MR. HARRISON: Good evening. I'm John Harrison.
21 16040 Shinnecock Drive in Odessa. I fully support
22 this project and I'm glad to see it proceeding. A
23 little story in support.

24 I was at the Bike Fest that Hillsborough County
25 put on this past year and was talking to some of the

1 people that were attending, and there was one
2 individual who had biked to the festival, which is
3 clear across Hillsborough County at Wilderness Park,
4 and he biked from Clearwater Beach.

5 So that was quite a ride. He did not take
6 Courtney Campbell. He went up north because he didn't
7 think this was safe. So obviously, there is a need
8 for this type of trail and two counties being involved
9 here. I think it is a terrific thing to be hooking up
10 the different trails that are currently available.

11 Also, I know some people in the environmental or
12 in the Ironman Triathalon, and one of those
13 individuals was looking for connections to trails to
14 bike over and get some extra miles in. It's very
15 difficult. We are proceeding slowly to interconnect
16 all the trails such as the Suncoast, now this, the
17 Tampa Bay Trail, that type of thing.

18 I think it's going to become a tourist
19 attraction and many people will come as a result. It
20 is going to improve the scenic access to the bay, and
21 also, you know, be the generator of connections to
22 downtown Tampa -- obviously -- Clearwater, and alot of
23 other regional trails. Thank you.

24 MR. BOGEN: Thank you. Our next speaker is Kevin
25 Timons.

1 MR. TIMONS: My name is Kevin Timons. I reside
2 at 4140 North 68th Avenue, Pinellas Park. I have got
3 a long-time bicycle rider in the Bay area. Most of it
4 was in Tampa. I'm living over there now in Pinellas.

5 But I have a story going back to the late '70s
6 when a group of my friends got together and we did the
7 trail or did the road -- I should say -- not the
8 trail.

9 We started at like 7 o'clock in the morning
10 because of the traffic. We didn't want to be involved
11 with anything coming by us at 60 miles an hour. So at
12 7:00 in the morning it's a little more peaceful.

13 But I have done along Courtney Campbell several
14 times. The little frontage road is a little easier
15 than trying to ride the bike shoulder, if you want to
16 call it a shoulder, traffic going by at 60 miles an
17 hour is not very friendly.

18 But the trail would be a great enhancement since
19 Friendship Trail is no longer in operation anymore.
20 There is a lot of call for people wanting to go across
21 the water, great scenic views, and I personally loved
22 Friendship Trail.

23 But it's an opportunity here with the new trail
24 opening up and being very friendly and no automobile
25 traffic would be great. Thank you.

1 MR. BOGEN: Thank you. Our next speaker is Linda
2 Saul-Sena.

3 MS. SAUL-SENA: Good afternoon. I live at 157
4 Biscayne in Tampa. And for many years -- for 19 years
5 -- actually for 23 years, I served as a member of The
6 Metropolitan Planning Organization and we had many
7 traditional initiatives.

8 But this particular initiative was an outgrowth
9 of a corroboration between Commissioner Bill Jonson,
10 representing Pinellas County; and myself, representing
11 Hillsborough County.

12 It seemed to us that this would be a tremendous
13 attribute for both areas to have a connecting trail.
14 When the Friendship Trail -- when it became evident
15 that that could no longer continue, this was a
16 tremendous opportunity and we are so pleased to see
17 that it is going to happen.

18 We have made steps to get Courtney Campbell named
19 an official scenic corridor according to the Florida
20 rules. But to be able to have a safe way for
21 pedestrians and bicyclists to go from one county to
22 the other, is a huge win for both of our communities.

23 It is wonderful in terms of the connectivity, the
24 beauty of the corridor; the ability not only for
25 tourists, but for locals to use it for recreational

1 riding and even potentially for commuting.

2 There are so many reasons why this is a stellar
3 project and I want to thank the Florida Department of
4 Transportation for recognizing the value of this
5 particular project, and I encourage its support and I
6 can't wait to ride it. Thank you.

7 MR. BOGEN: Thank you. Our next speaker is Allen
8 Snel.

9 MR. SNEL: Good evening. Alan Snel. 1203 East
10 Powhatan Avenue in Tampa. I'm the director of SWFBUD,
11 Southwest Florida Bicycle United Dealers. We are an
12 alliance of 12 bicycle stores in the Tampa Bay area.

13 We represent thousands of bicyclists among our
14 customers. It is my great pleasure to come here and
15 express and voice my support for this terrific
16 project.

17 It's a long time coming. And it is really
18 unfolding against a backdrop of a time when bicycle
19 safety has emerged as a central issue. So that needs
20 to be considered as well. Right now you can
21 technically bicycle across the Causeway but because of
22 the high speed and limited access, it forces
23 bicyclists to cross a span that is -- offers perhaps 3
24 or 4 feet of current space to ride a bicycle which,
25 quite frankly, imperils the safety of bicyclists right

1 now.

2 The drivers who pass current bicyclists over the
3 high span, I'm guessing probably by -- a 3-foot buffer
4 right now. There are many reasons why this is a
5 terrific project.

6 We here in Tampa Bay define our identity through
7 this incredible body of water to the right and Tampa
8 Bay, this body of water unites our whole region.
9 Providing access and safe trail access for
10 pedestrians, bicyclists, all people in non-vehicular
11 or motorized vehicles, provides -- will provide
12 literally access to thousands and thousands of more
13 users than what you have right now.

14 The Friendship Trail from what I understand had
15 600,000 users per year and I would suspect that we
16 will see a very similar number of users of all
17 backgrounds. It's very important of all backgrounds
18 in terms of all kinds of users.

19 And also what's really interesting of all kinds
20 of geographic backgrounds as well. Also there is a
21 tourism aspect -- an economic development aspect of
22 this.

23 You will have tourists interested in coming to
24 this particular stretch of trailway because of the
25 tremendous access to the bay and of the scenic view.

1 So there is a tourism factor.

2 I'm told that when tourists come to Florida, one
3 of their first requests is "where can I safely ride a
4 bike in Florida?" So this will provide safe access to
5 one of the great bodies of water in our state.

6 In addition, we will have an important link in a
7 regional paved trail system that is badly needed here
8 in Tampa Bay. Believe it or not, people do ride bikes
9 to commute.

10 Even though I think people might think there is a
11 strong recreational aspect to this project, it will
12 also provide a commuter link because of the U-trail on
13 the Tampa side providing access to Tampa and Cypress,
14 and on the other side we will have clear access to
15 points west on the Clearwater side.

16 So, in summaries, SWFBUD believes this is a
17 terrific project that enhances the quality of life,
18 that can provide safe bike commuting, and is adding to
19 the quality of life in the Tampa Bay region. I
20 appreciate your time.

21 MR. BOGEN: Thank you. The next speaker is Ron
22 Gregory.

23 MR. GREGORY: My name is Ron Gregory, and I
24 reside at 732 Snug Island in Clearwater, Florida
25 33767. I would like to comment that I really

1 appreciate the work that the Department is doing,
2 finally connecting the trail and pedestrian access,
3 thank you from Pinellas County for all the work that's
4 being done on the Tampa side.

5 I work at the building across the road here and
6 we have a lot of people who during their lunch hour
7 want to go jogging -- and because we have a gym and
8 everything -- one of the issues they have always had
9 is safety issues, both crossing the road and being
10 parallel to it and they are kind of restricted.

11 I also want to reiterate what the other gentleman
12 said. From personal experience, when you build one of
13 these facilities you sometimes wonder is it going to
14 be used. Where I live they recently completed a
15 bridge structure over to Clearwater Beach from
16 Memorial Causeway, and also in conjunction with an
17 earlier reconstruction the Department did with the
18 city on the Memorial Causeway Bridge over the
19 Intercoastal.

20 And there has been a tremendous increase in the
21 amount of joggers, bicyclers, all hours of the day and
22 night, I mean literally. So I'm pretty confident that
23 once this link gets established and particularly with
24 the issue about Freedom Trail being down and
25 Friendship Trail down or not being there anymore, that

1 it will be a very vital part of the multi-mobile
2 aspect of the area.

3 So just from a personal standpoint, I think this
4 is a very good idea and I compliment the Department on
5 moving forward with it.

6 As I understand, some portions of it are funded
7 fairly quickly, so that's good. And I know that Linda
8 and I and Bill Jonson serve on the Scenic Highway
9 Committee, and I'm sure that they are in support.
10 The Scenic Highway Committee is dedicated to enhancing
11 the use of Causeway and everything about it and I
12 complement you on that.

13 I believe the Westshore Alliance also probably
14 made a statement in support of it. And we just concur
15 with all of those comments. Thank you.

16 MR. BOGEN: Thank you. Does anyone else wish to
17 speak?

18 Seeing none, the verbatim transcript of both
19 sessions of the hearing's oral proceedings will be
20 available for inspection at the District Seven Office
21 for public review upon request within 3 weeks.

22 Thank you for attending this public hearing and
23 for providing your input into this project. It is now
24 approximately 6:20 and I hereby close -- officially
25 close the formal portion of the public hearing's

1 second and last session for the State Road 60,
2 Courtney Campbell Causeway Multi-use Trail PD&E Study.

3 You may continue to view the materials on display
4 and speak with our project staff. On behalf of the
5 Florida Department of Transportation, thank you for
6 attending. Good night and please drive home safely.

7 (END OF SESSION 2)

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REPORTER'S CERTIFICATE

STATE OF FLORIDA :

COUNTY OF HILLSBOROUGH :

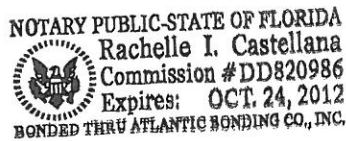
I, Rachelle I. Castellana, certify that I was authorized to and did stenographically report the foregoing FDOT Public Hearings, Session 1 taken on March 24, 2011, and Session 2 taken on March 29, 2011, and that the transcript is a true and complete record of my stenographic notes.

I further certify that I am not a relative, employee, attorney, or counsel of any of the parties, nor am I a relative or employee of any of the parties' attorney or counsel connected with the action, nor am I financially interested in the outcome of the foregoing action.

Dated this 2nd day of April, 2011, IN THE CITY OF TAMPA, COUNTY OF HILLSBOROUGH, STATE OF FLORIDA.

Rachelle I. Castellana

Rachelle I. Castellana, Certified Reporter



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<p>18:13 23:4 28:5 travel 10:8 traveled 9:11 tremendous 22:12,16 24:25 26:20 Triathlon 20:12 trillion 3:9 true 29:9 try 12:6 trying 21:15 Tuesday 1:15 5:16 7:8 14:2 15:2 turned 7:25 18:20 two 5:9 10:22 11:22 16:9 20:8 type 20:8,17</p> <hr/> <p style="text-align: center;">U</p> <p>ultimately 10:13 unavoidable 6:14 17:12 understand 4:20 15:20 24:14 27:6 unfolding 23:18 unheard 9:9 United 23:11 unites 24:8 until 3:10 Upper 10:25 use 9:18,19 10:15 11:7 15:16 22:25 27:11 used 26:14 user 13:3 users 24:13,15,16,18 U-trail 25:12 U.S 6:3 17:1</p> <hr/> <p style="text-align: center;">V</p> <p>value 23:4 vehicles 24:11 verbatim 13:15 27:18 very 3:8 9:11 20:14 21:17,24 24:16,17 27:1,4 view 6:11 14:7 17:9 24:25 28:3 views 7:1 17:24 21:21 vital 27:1 voice 12:13 23:15</p> <hr/> <p style="text-align: center;">W</p> <p>wait 3:9 23:6 walk 10:21 walking 10:12 want 11:12,19 12:17 19:9,17 21:10,15 23:3 26:7,11 wanted 9:4 wanting 21:20</p>	<p>water 21:21 24:7,8 25:5 way 19:15 22:20 Weber 2:21 19:6,7,7 weeks 13:18 27:21 Welcome 4:8 15:8 well 5:25 8:5 16:23 18:24 23:20 24:20 went 20:6 were 9:13 20:1 weren't 6:8 17:6 west 1:17 4:17 5:17 8:16 15:17 16:16 25:15 Westin 1:17 5:16 15:4 16:16 Westshore 2:21 19:8 27:13 whole 24:8 Wilderness 20:3 willing 19:14 willingness 13:10 Wilson 11:1 win 22:22 wish 7:12 13:14 18:6 27:16 wonder 26:13 wonderful 22:23 work 4:13 15:13 26:1,3 26:5 worked 19:13 working 19:11 worth 11:15 written 7:1,14,16,22 17:24 18:9,11,17</p> <hr/> <p style="text-align: center;">X</p> <p>X 2:1</p> <hr/> <p style="text-align: center;">Y</p> <p>year 19:25 24:15 years 3:10 22:4,4,5</p> <hr/> <p style="text-align: center;">S</p> <p>\$14 3:9 \$21 11:14</p> <hr/> <p style="text-align: center;">1</p> <p>1 1:5 2:3 4:1 29:8 1st 3:2,7 10561 12:12 11201 7:18 18:13 12 23:12 1203 23:9 127th 12:12 128 9:3 157 22:3 16 2:14 16040 19:21</p>	<p>1700 10:19 19 22:4 1990 5:7 16:7</p> <hr/> <p style="text-align: center;">2</p> <p>2 1:14 2:13 15:1 28:7 29:8 2nd 29:14 2011 1:6,15 4:3 5:13,16 7:8,20 14:2 15:3 16:13,16 18:15 29:8,8 29:14 23 22:5 24 1:6 29:8 24th 4:3 5:12 16:13 2717 13:1 29 1:15 29:8 29th 5:16 7:8 14:2 15:3 16:15</p> <hr/> <p style="text-align: center;">3</p> <p>3 2:4 8:5 13:7,18 18:25 23:23 27:21 3-foot 24:3 3/24/11 2:8 3/28/11 2:18 30 2:15 33607 1:18 33612 7:19 18:14 33713 3:3,7 33759 1:9 33767 25:25 33773 12:13 3400 1:8 5:13 16:13 3760 3:2,6</p> <hr/> <p style="text-align: center;">4</p> <p>4 2:5 9:14 23:24 4140 21:2 422640-2 4:13 15:13</p> <hr/> <p style="text-align: center;">5</p> <p>5 1:7,16 5:16 14:3</p> <hr/> <p style="text-align: center;">6</p> <p>6 4:3 14:6 15:3 6:15 13:21 6:20 27:24 60 1:1 4:9 8:14 9:10 13:24 15:9 21:11,16 28:1 600,000 24:15 68th 21:2</p> <hr/> <p style="text-align: center;">7</p> <p>7 1:7 21:9 7p.m 1:16 7:00 21:12</p>	<p>70s 21:5 732 25:24 7627 1:17 5:17 16:16</p> <hr/> <p style="text-align: center;">8</p> <p>8 7:20 8:17 18:15</p> <hr/> <p style="text-align: center;">9</p> <p>9045-090 4:14 15:14</p>
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→ Robin



March 24, 2011

TO: Florida Department of Transportation

RE: Public comment on proposed multi-use trail along Courtney Campbell Causeway

FROM: Marcia Biggs
Chair, Tampa Bay Sierra Club

To Whom It May Concern

This letter serves as official statement of support by the Tampa Bay Sierra Club for the building and funding of a multi-use trail as a regional connector for non-motorized travel adjacent to the Courtney Campbell Causeway. The establishment of such a trailway would serve to encourage non-carbon producing modes of transportation such as bicycles, walking, inline skating and running, which would ultimately serve to benefit our local residents.

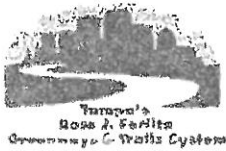
The Tampa Bay Sierra Club supports the use of alternative forms of transit which would help reduce congestion and carbon emissions, and support healthier lifestyles for those living on both sides of the bay. Our group, with nearly 1,700 members, embraces the natural beauty of this area and many regularly enjoy walking and bicycling. Not only would this trail connect two counties, it would also connect us as neighbors and residents of one regional community.

By connecting each end to another trail system (Upper Tampa Bay Trail and Ream Wilson Trail), this Causeway stretch would offer an incredibly scenic ride which would easily be the subject of positive regional and national publicity and coverage. As a recreational bicyclist, I personally can assure you I would be one of thousands who would put this trail to use!

Build it and we will come!

Marcia Biggs

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March 28, 2011

CITY OF TAMPA GREENWAYS
AND TRAILS CITIZENS
ADVISORY COMMITTEE
Sally Thompson, Chair
Kandy Stribling, Vice-Chair

Ming Gao
Intermodal Systems Development Manager
FDOT-District 7
11201 N McKinley Drive, MS 7-500
Tampa, FL 33612-6456

Dear Mr. Gao,

The City of Tampa Greenways and Trails Citizen Advisory Committee would like to express our support of the proposed Courtney Campbell Causeway trail and pedestrian bridge. This trail is included in the City of Tampa Greenways and Trails Master Plan that was adopted by City Council in 2001. It is also an important connection in a regional trail that would link Hillsborough and Pinellas Counties.

We commend the Florida Department of Transportation for supporting and pursuing funding for this multi-modal transportation effort.

Sincerely,

Sally Thompson, Chair
City of Tampa Greenways and Trails Citizen Advisory Committee

FOR MORE INFORMATION
Kerla Price, RLA, ASLA
Tampa Parks and Recreation
Department
3402 W. Columbus Drive
Tampa, FL 33607
(813) 274-5134 (phone)
(813) 274-7420 (fax)
www.tampaparks.net/dept_parks/trails

Xc: Karen Palus, Director, City of Tampa Parks and Recreation
Lori Suively, Bicycle/Pedestrian/Public Involvement Coordinator, FDOT



3109 W. Dr. M.L. King Blvd ♦ Suite 140
Tampa, Florida 33607
Phone: 813-289-5488 ♦ FAX 813-289-6727
www.westshorealliance.org

March 29, 2011

Robin Rhinesmith
Project Manager
Florida Department of Transportation, District 7
11201 N. McKinley Drive, MS 7-500
Tampa, Florida 33612-6456

Dear Ms. Rhinesmith:

The Westshore Alliance, representing approximately 100,000 employees in the Westshore area, fully supports the Departments' current plans to provide a multi-use trail along the length of the Courtney Campbell Causeway. When completed, the multi-use trail will not only provide a significant recreational amenity for residents and visitors to the area, but will provide non-automotive transportation enhancements critical to improving the mobility environment in the Westshore area.

The Department's plans are also consistent with the guiding principals of the Westshore Alliance Pedestrian Plan by helping provide the foundation for a comfortable, accessible pedestrian/bicycle network within the greater Westshore area that interconnects to other Tampa Bay trail systems.

The Alliance sincerely appreciates the Department's continued efforts to expand multimodal mobility options for the Westshore area and look forward to continued collaboration on future projects.

Sincerely,



Ron Rotella
Executive Director

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APPENDIX D

Conceptual Design Plans



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Conceptual Design Plans

SR 60 (Courtney Campbell Causeway) Multi-Use Trail PD&E Study

From Bayshore Boulevard
to West of Ben T Davis Beach Entrance

WPI Segment No.: 422640-2

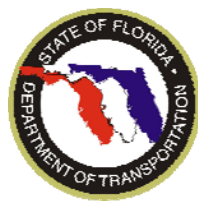
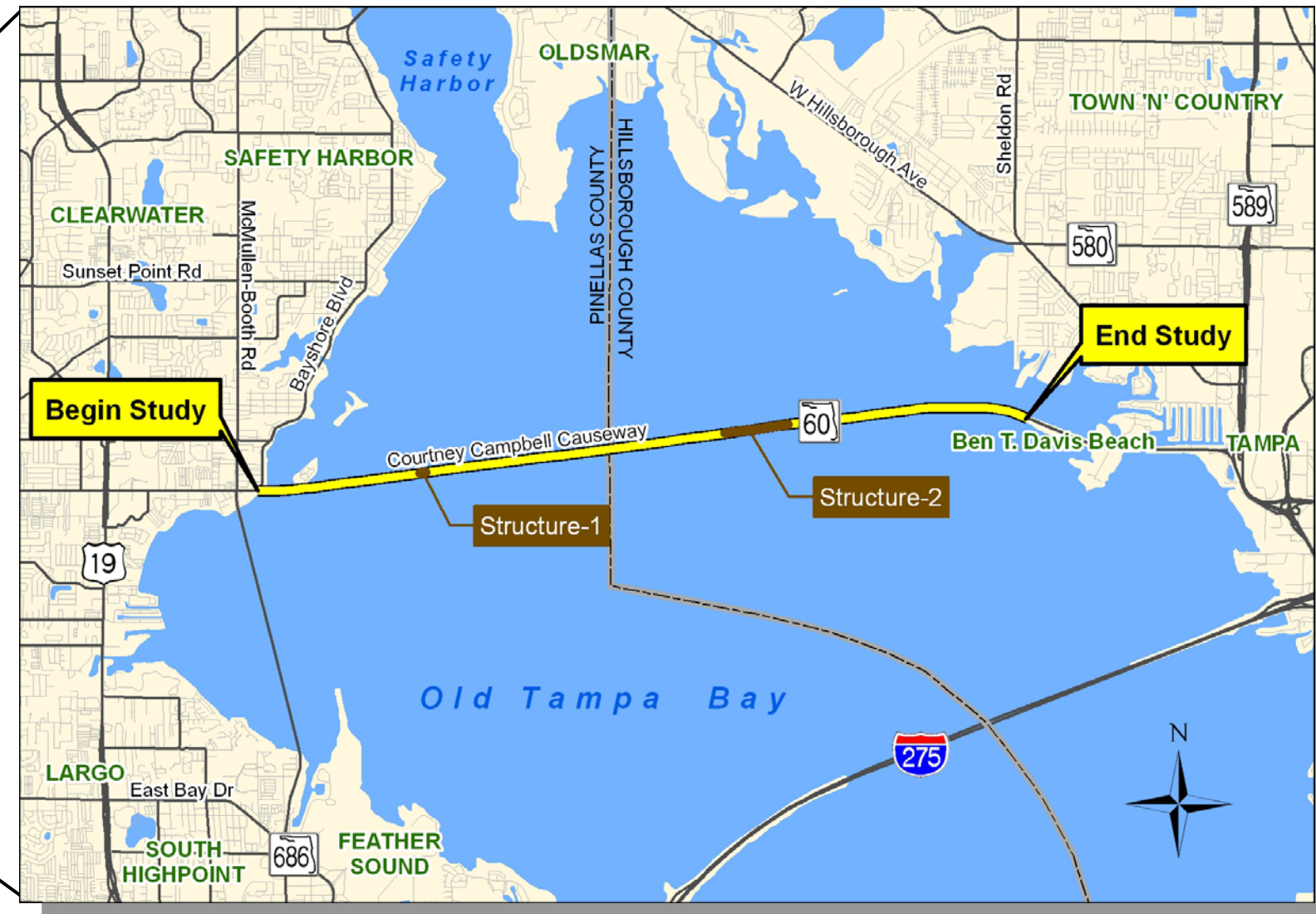
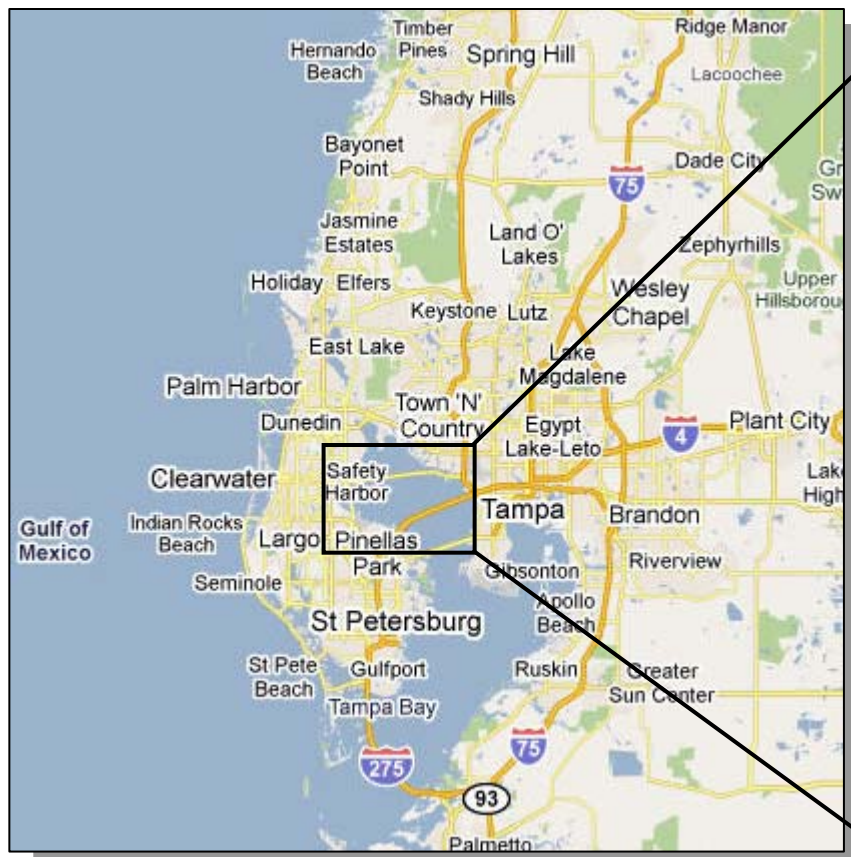
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Pinellas and Hillsborough County



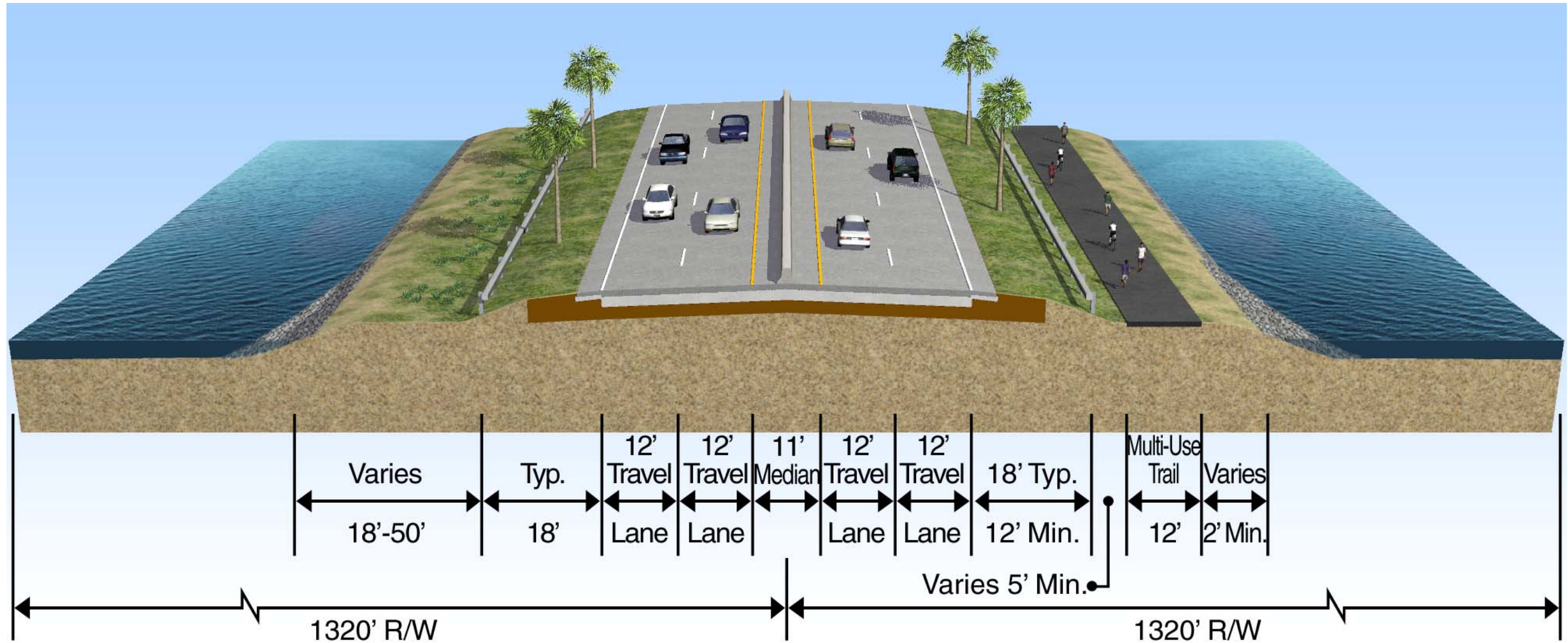
Project Location

INDEX OF CONCEPTUAL PLANS	
SHEET NO.	SHEET DESCRIPTION
I - IV	Conceptual Typical Sections
V	Conceptual Layout Sheet
1 thru 27	Concept Plans

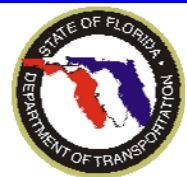


Prepared For:
Florida Department of Transportation
District Seven

FDOT Project Manager: Robin Rhinesmith



SR 60 (Courtney Campbell Causeway)
Typical Section No. 1
Station 21+00 to Station 69+50

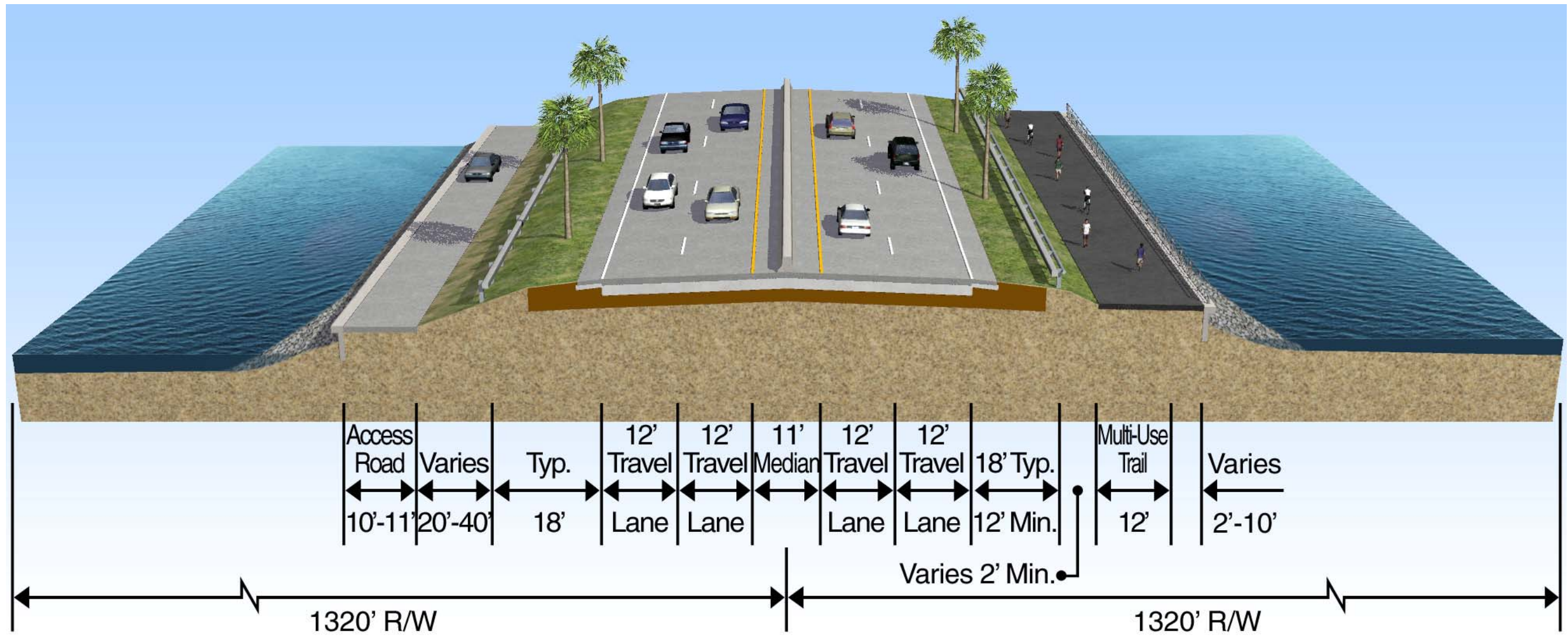


SR 60 (Courtney Campbell Causeway) Multi-Use Trail PD&E Study
From Bayshore Blvd. to West of Ben T. Davis Beach Entrance
 FPID: 422640-2-22-01
 Pinellas and Hillsborough County

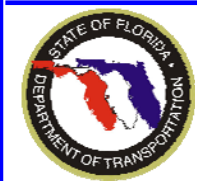
American Consulting Engineers of Florida, LLC.
 2818 Cypress Ridge Blvd, Suite 200
 Wesley Chapel, Florida 33544
 Phone: (813) 435-2600 Fax: (813) 435-2601
 Certificate of Authorization No. 9302
 Jeffrey S. Novotny, P.E. No. 51083

CONCEPTUAL TYPICAL SECTIONS

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I



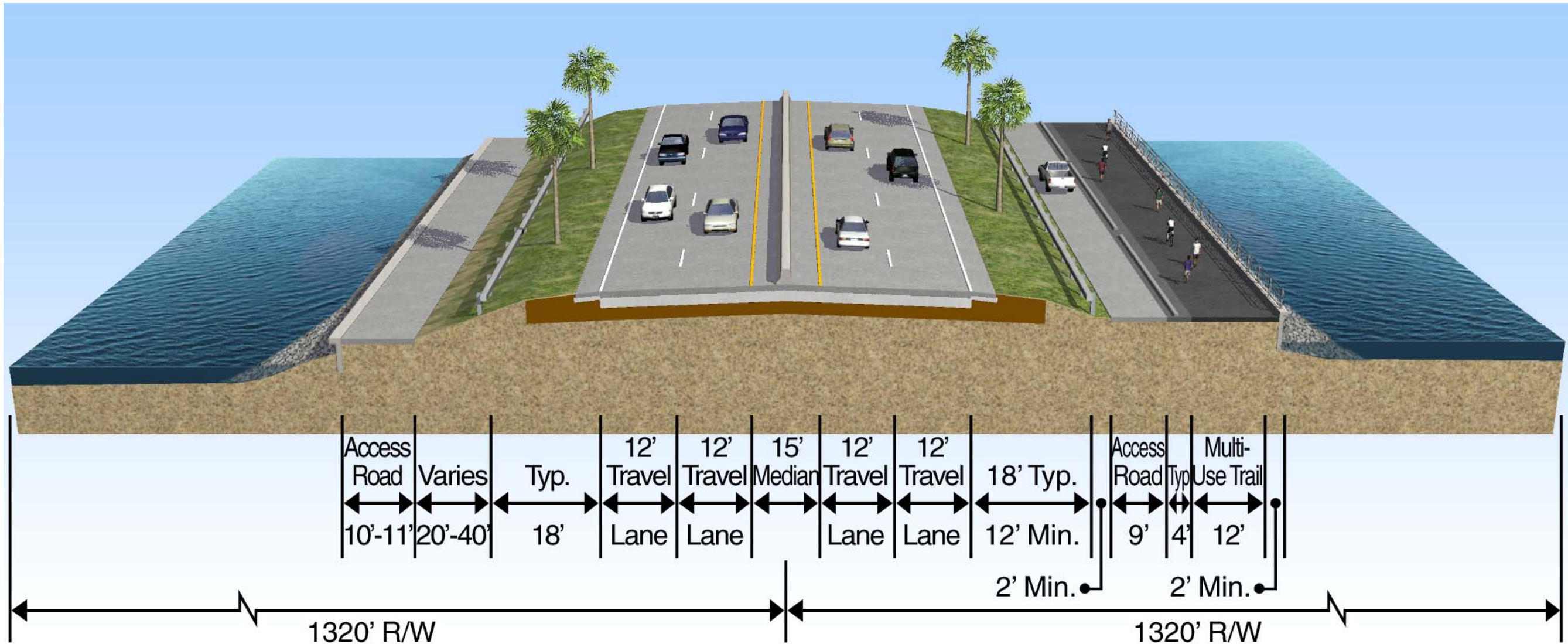
SR 60 (Courtney Campbell Causeway)
Typical Section No. 2
 Station 69+50 to Station 106+00
 Station 111+00 to Station 256+00
 Station 394+00 to Station 412+00



SR 60 (Courtney Campbell Causeway) Multi-Use Trail PD&E Study
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 FPID: 422640-2-22-01
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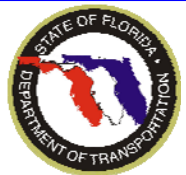


SR 60 (Courtney Campbell Causeway)

Typical Section No. 3

Station 256+00 to Station 265+00

Station 300+00 to Station 394+00

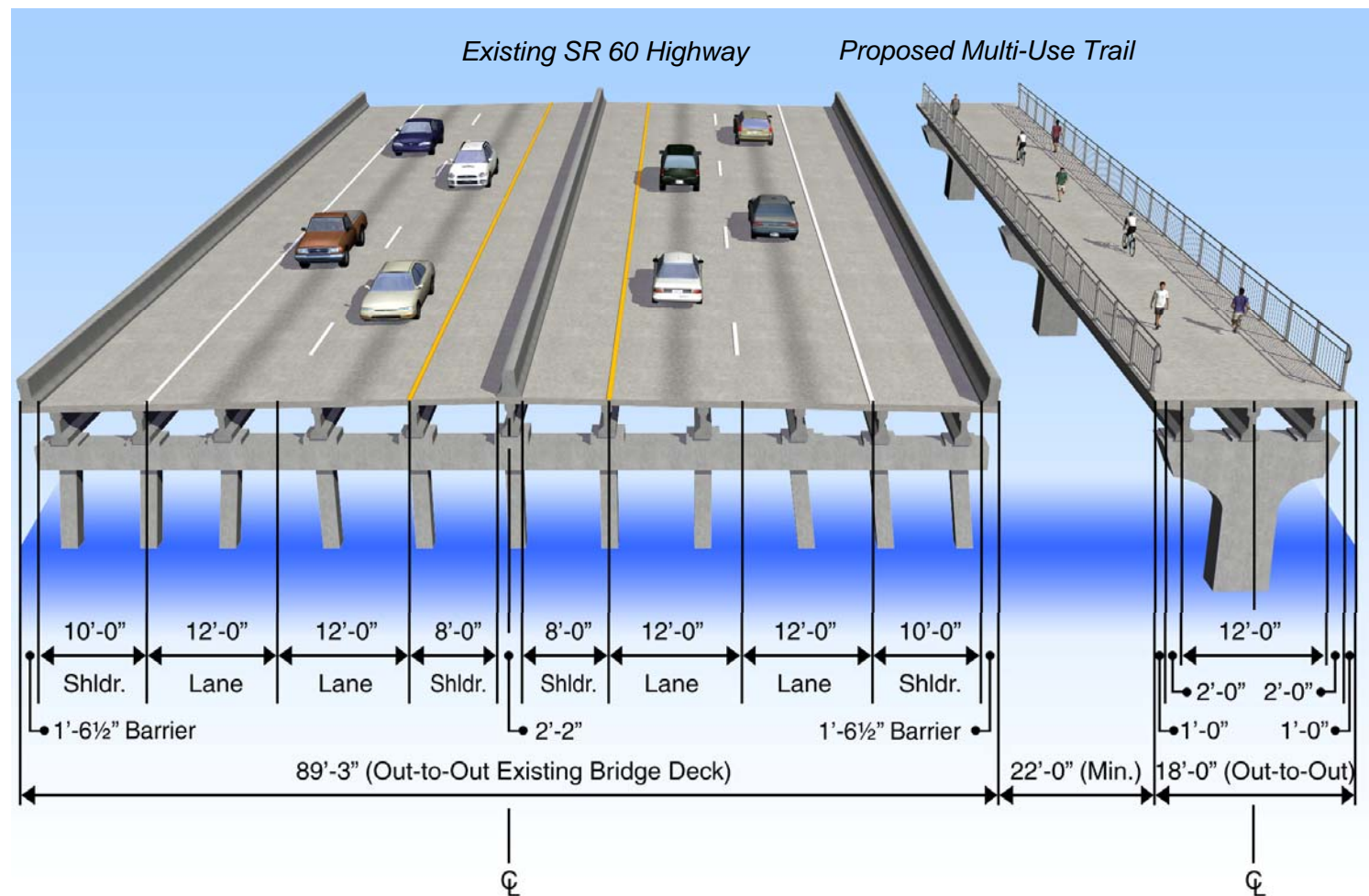


SR 60 (Courtney Campbell Causeway) Multi-Use Trail PD&E Study
From Bayshore Blvd. to West of Ben T. Davis Beach Entrance
 FPID: 422640-2-22-01
 Pinellas and Hillsborough County

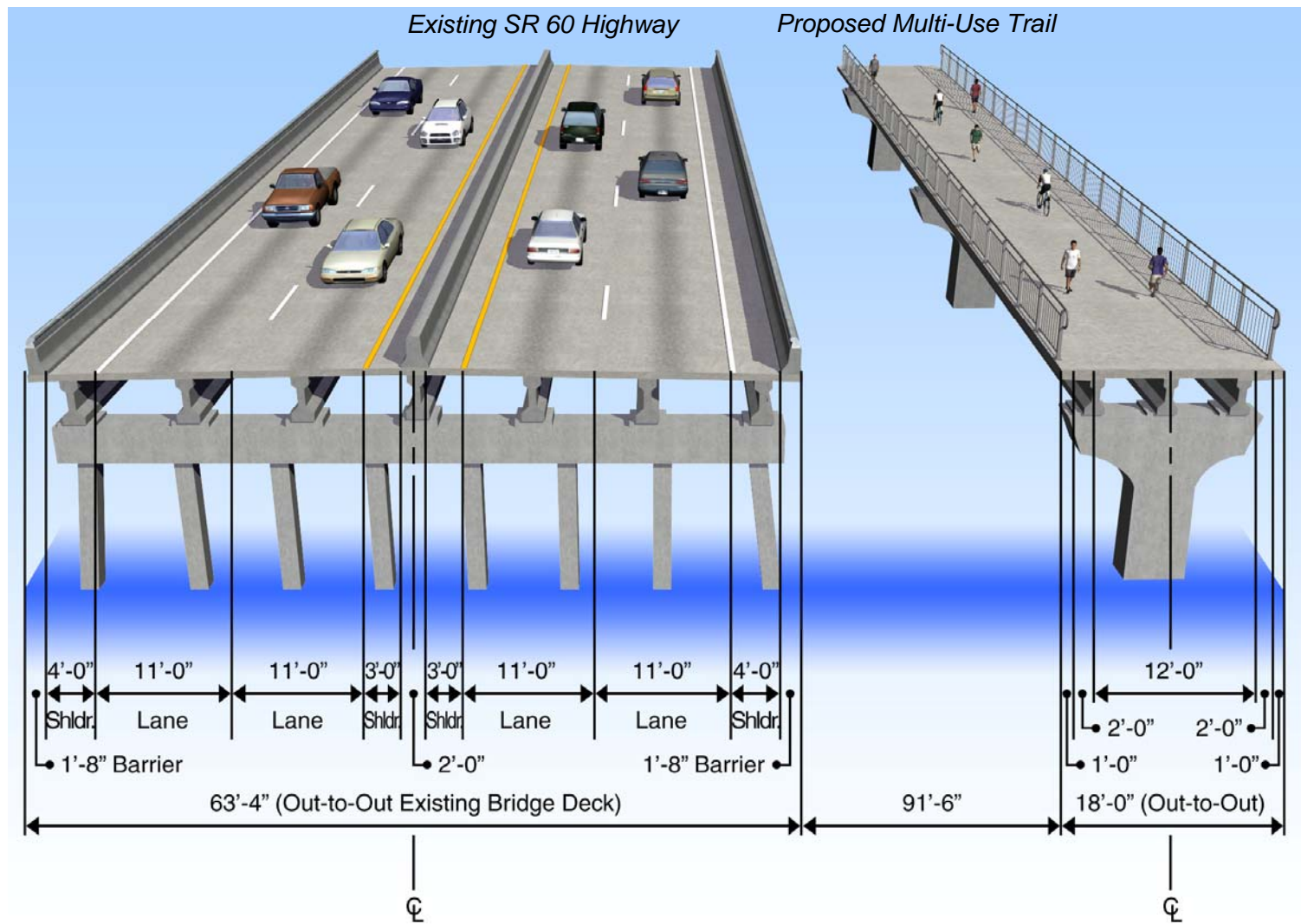
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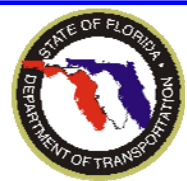
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**SR 60 (Courtney Campbell Causeway)
Bridge Typical Section Structure No. 1
Station 106+00 to Station 111+00**



**SR 60 (Courtney Campbell Causeway)
Bridge Typical Section Structure No. 2
Station 265+00 to Station 300+00**

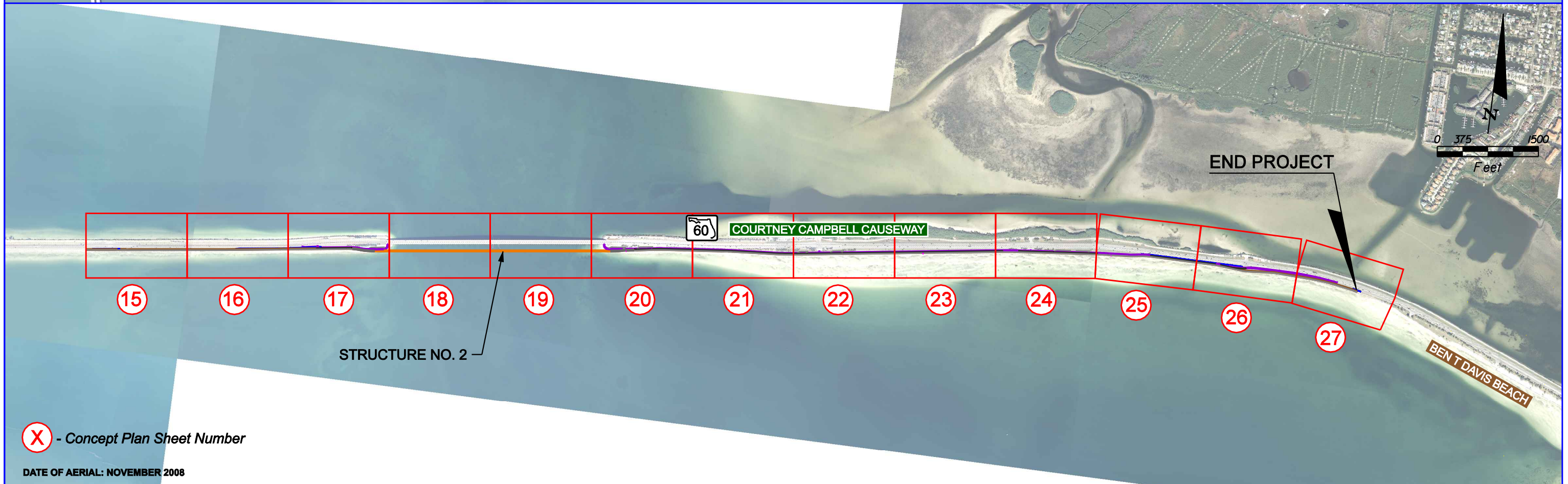
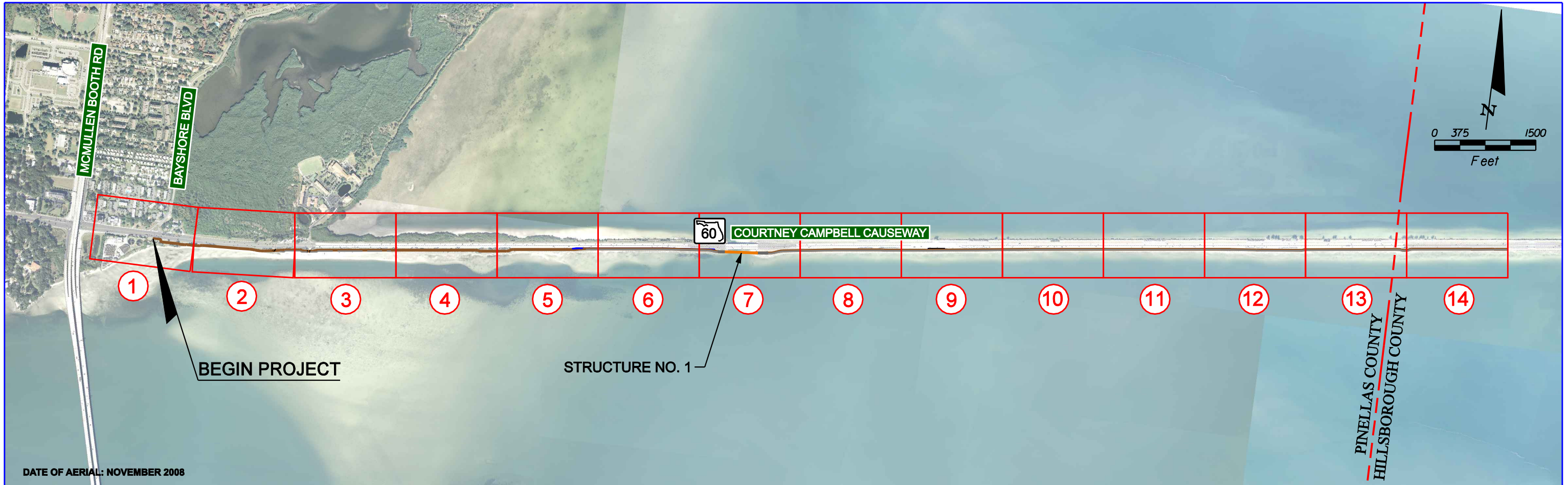


SR 60 (Courtney Campbell Causeway) Multi-Use Trail PD&E Study
From Bayshore Blvd. to West of Ben T. Davis Beach Entrance
 FPID: 422640-2-22-01
 Pinellas and Hillsborough County

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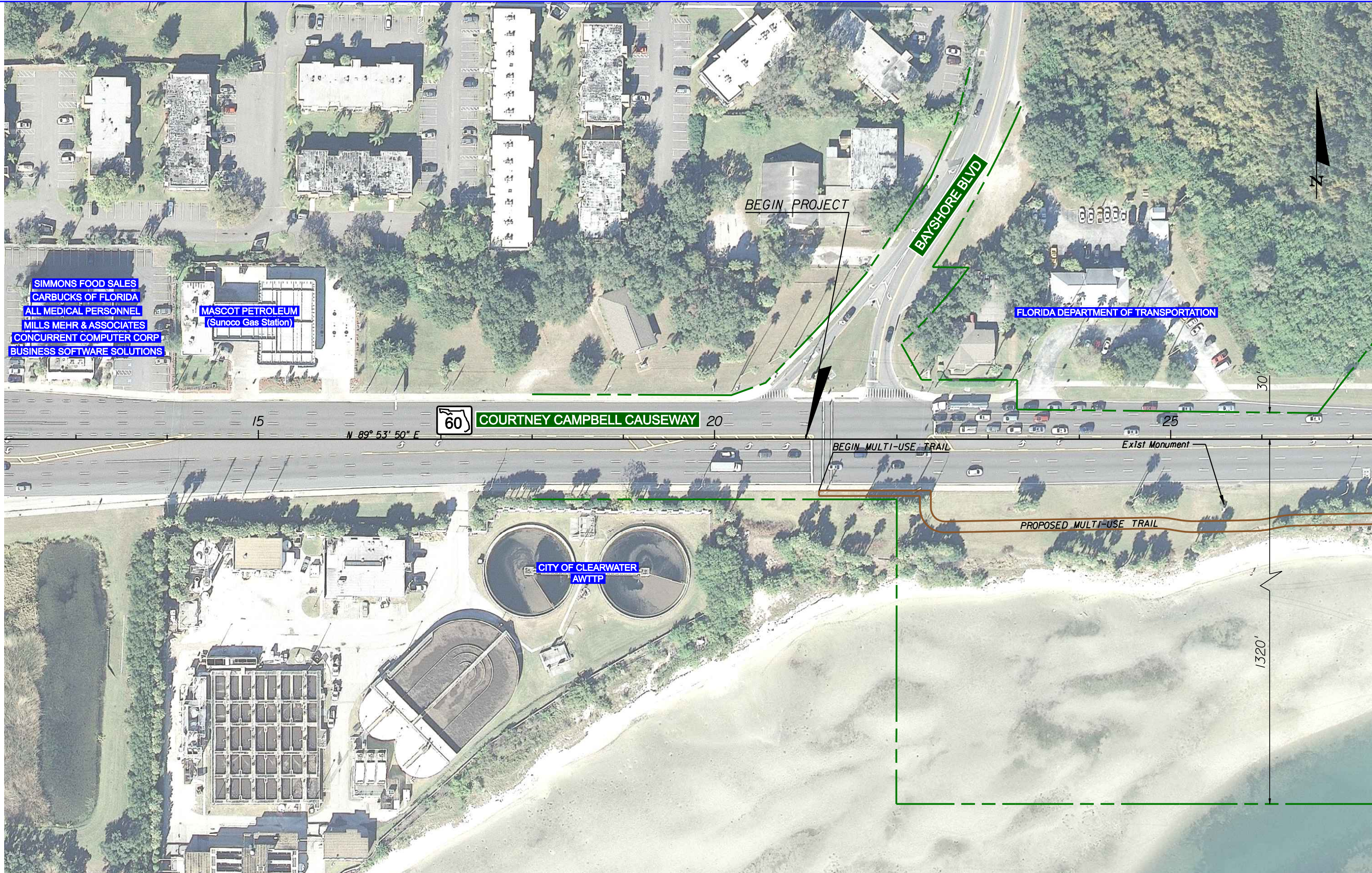
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IV*



X - Concept Plan Sheet Number

DATE OF AERIAL: NOVEMBER 2008

REVISIONS				American Consulting Engineers of Florida, LLC. 2818 Cypress Ridge Blvd, Suite 200 Wesley Chapel, Florida 33544 Phone: (813) 435-2600 Fax: (813) 435-2601 Certificate of Authorization No. 9302 Jeffrey S. Novotny, P.E. No. 51083	STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			SR 60 (COURTNEY CAMPBELL CAUSEWAY) MULTI-USE TRAIL PD&E STUDY CONCEPTUAL LAYOUT SHEET FROM BAYSHORE BLVD TO WEST OF BEN T DAVIS BEACH ENTRANCE	SHEET NO. V
DATE	DESCRIPTION	DATE	ROAD NO.		COUNTY	FINANCIAL PROJECT ID			
			SR 60		PINELLAS & HILLSBOROUGH	422640-2-22-01			



LEGEND

	PROPOSED MULTI-USE TRAIL
	RELOCATED ACCESS ROAD
	PROPOSED MULTI-USE TRAIL BRIDGE
	UNPAVED PARKING AREA
	EXISTING RIGHT OF WAY

PROPOSED IMPROVEMENTS BY OTHERS (424561-1)

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Feet
DATE OF AERIAL: NOVEMBER 2008

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STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
SR 60	PINELLAS & HILLSBOROUGH	422640-2-22-01

SR 60 (COURTNEY CAMPBELL CAUSEWAY) MULTI-USE TRAIL
PD&E STUDY

CONCEPT PLANS

FROM BAYSHORE BLVD TO WEST OF BEN T DAVIS BEACH ENTRANCE

SHEET NO.
1



LEGEND

- PROPOSED MULTI-USE TRAIL
 - RELOCATED ACCESS ROAD
 - PROPOSED MULTI-USE TRAIL BRIDGE
 - UNPAVED PARKING AREA
 - EXISTING RIGHT OF WAY
 - PROPOSED IMPROVEMENTS BY OTHERS (424561-1)
- 0 25 100
 Feet
 DATE OF AERIAL: NOVEMBER 2008

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ROAD NO.	COUNTY	FINANCIAL PROJECT ID
SR 60	PINELLAS & HILLSBOROUGH	422640-2-22-01

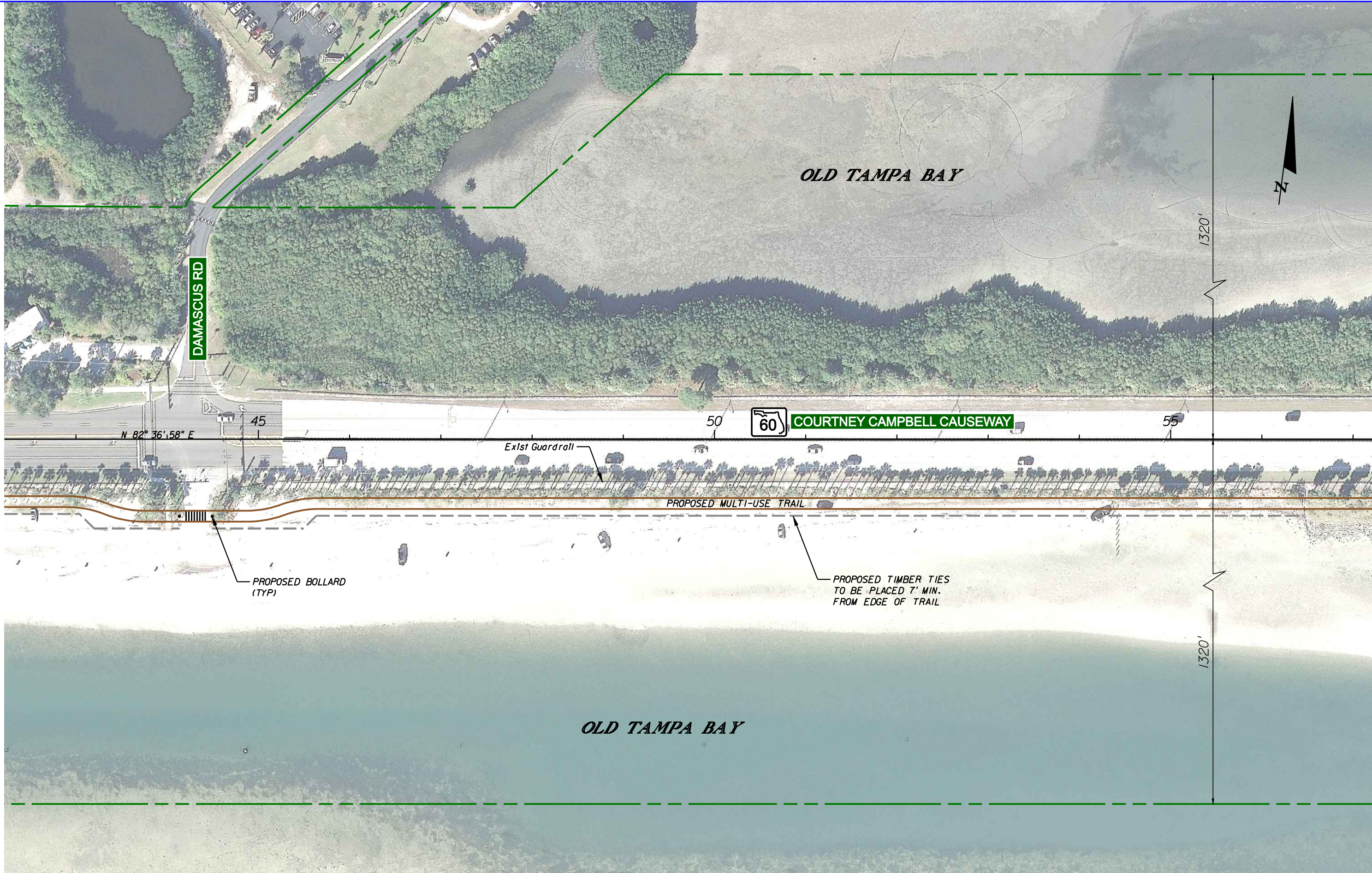
SR 60 (COURTNEY CAMPBELL CAUSEWAY) MULTI-USE TRAIL
 PD&E STUDY

CONCEPT PLANS

FROM BAYSHORE BLVD TO WEST OF BEN T DAVIS BEACH ENTRANCE

SHEET
NO.

2



LEGEND

- PROPOSED MULTI-USE TRAIL
- RELOCATED ACCESS ROAD
- PROPOSED MULTI-USE TRAIL BRIDGE
- UNPAVED PARKING AREA
- EXISTING RIGHT OF WAY
- PROPOSED IMPROVEMENTS BY OTHERS (424561-1)

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**SR 60 (COURTNEY CAMPBELL CAUSEWAY) MULTI-USE TRAIL
PD&E STUDY**

CONCEPT PLANS

FROM BAYSHORE BLVD TO WEST OF BEN T DAVIS BEACH ENTRANCE

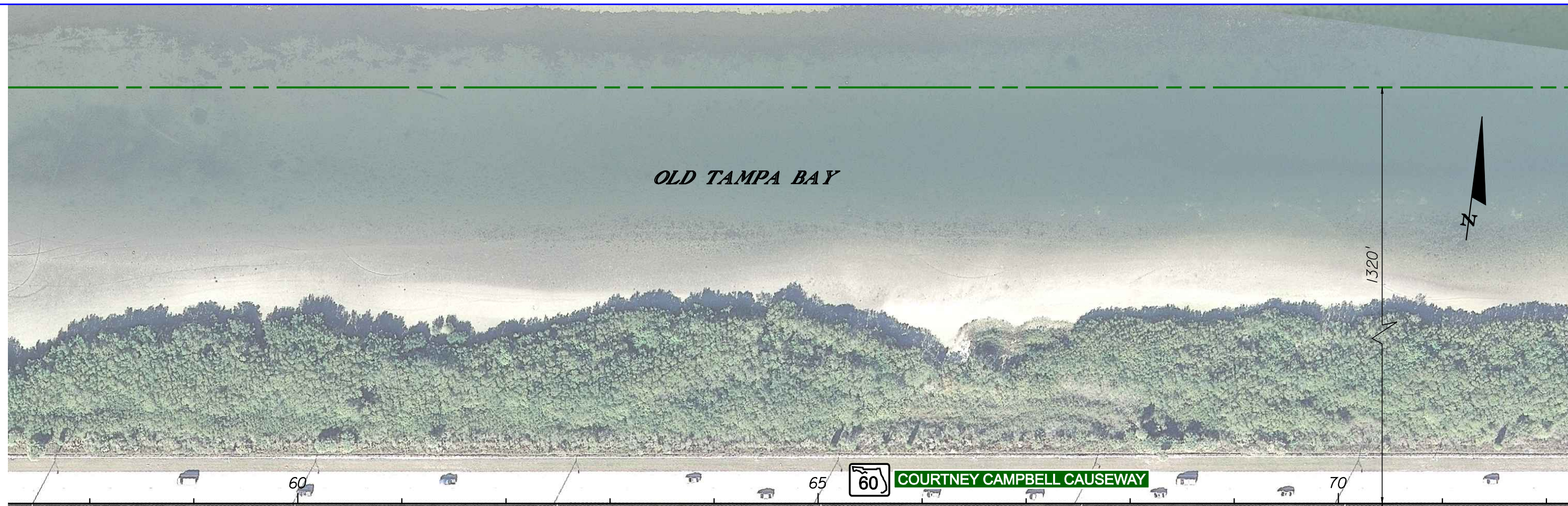
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3

OLD TAMPA BAY



1320'



OLD TAMPA BAY

LEGEND

- PROPOSED MULTI-USE TRAIL
 - RELOCATED ACCESS ROAD
 - PROPOSED MULTI-USE TRAIL BRIDGE
 - UNPAVED PARKING AREA
 - EXISTING RIGHT OF WAY
 - PROPOSED IMPROVEMENTS BY OTHERS (424561-1)
- Feet
 DATE OF AERIAL: NOVEMBER 2008

American Consulting Engineers of Florida, LLC.
 2818 Cypress Ridge Blvd, Suite 200
 Wesley Chapel, Florida 33544
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 Certificate of Authorization No. 9302
 Jeffrey S. Novotny, P.E. No. 51083

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
SR 60	PINELLAS & HILLSBOROUGH	422640-2-22-01

**SR 60 (COURTNEY CAMPBELL CAUSEWAY) MULTI-USE TRAIL
PD&E STUDY**

CONCEPT PLANS

FROM BAYSHORE BLVD TO WEST OF BEN T DAVIS BEACH ENTRANCE

SHEET
NO.

4

OLD TAMPA BAY

OLD TAMPA BAY



COURTNEY CAMPBELL CAUSEWAY

RELOCATE EXIST GUARDRAIL TO 12' OFFSET FROM EDGE OF TRAVEL LANE TO FACE OF GUARDRAIL

Exist Guardrail

PROPOSED MULTI-USE TRAIL

PROPOSED PEDESTRIAN/BICYCLE RAILING (MOUNTED ON TOP OF SEA WALL)

Exist Sea Wall

1320'

1320'



LEGEND

- PROPOSED MULTI-USE TRAIL
 - RELOCATED ACCESS ROAD
 - PROPOSED MULTI-USE TRAIL BRIDGE
 - UNPAVED PARKING AREA
 - EXISTING RIGHT OF WAY
 - PROPOSED IMPROVEMENTS BY OTHERS (424561-1)
- 0 25 100
Feet
DATE OF AERIAL: NOVEMBER 2008

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**SR 60 (COURTNEY CAMPBELL CAUSEWAY) MULTI-USE TRAIL
PD&E STUDY**

CONCEPT PLANS

FROM BAYSHORE BLVD TO WEST OF BEN T DAVIS BEACH ENTRANCE

SHEET NO.
5

OLD TAMPA BAY



RELOCATE EXIST GUARDRAIL TO 12' OFFSET FROM EDGE OF TRAVEL LANE TO FACE OF GUARDRAIL



COURTNEY CAMPBELL CAUSEWAY

90

95

100

PROPOSED MULTI-USE TRAIL

Exist Sea Wall

PROPOSED PEDESTRIAN/BICYCLE RAILING (MOUNTED ON TOP OF SEA WALL)

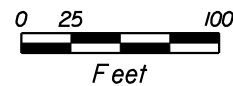
1320'

1320'

OLD TAMPA BAY

LEGEND

- PROPOSED MULTI-USE TRAIL
- PROPOSED IMPROVEMENTS BY OTHERS (424561-1)
- RELOCATED ACCESS ROAD
- PROPOSED MULTI-USE TRAIL BRIDGE
- UNPAVED PARKING AREA
- EXISTING RIGHT OF WAY



DATE OF AERIAL: NOVEMBER 2008

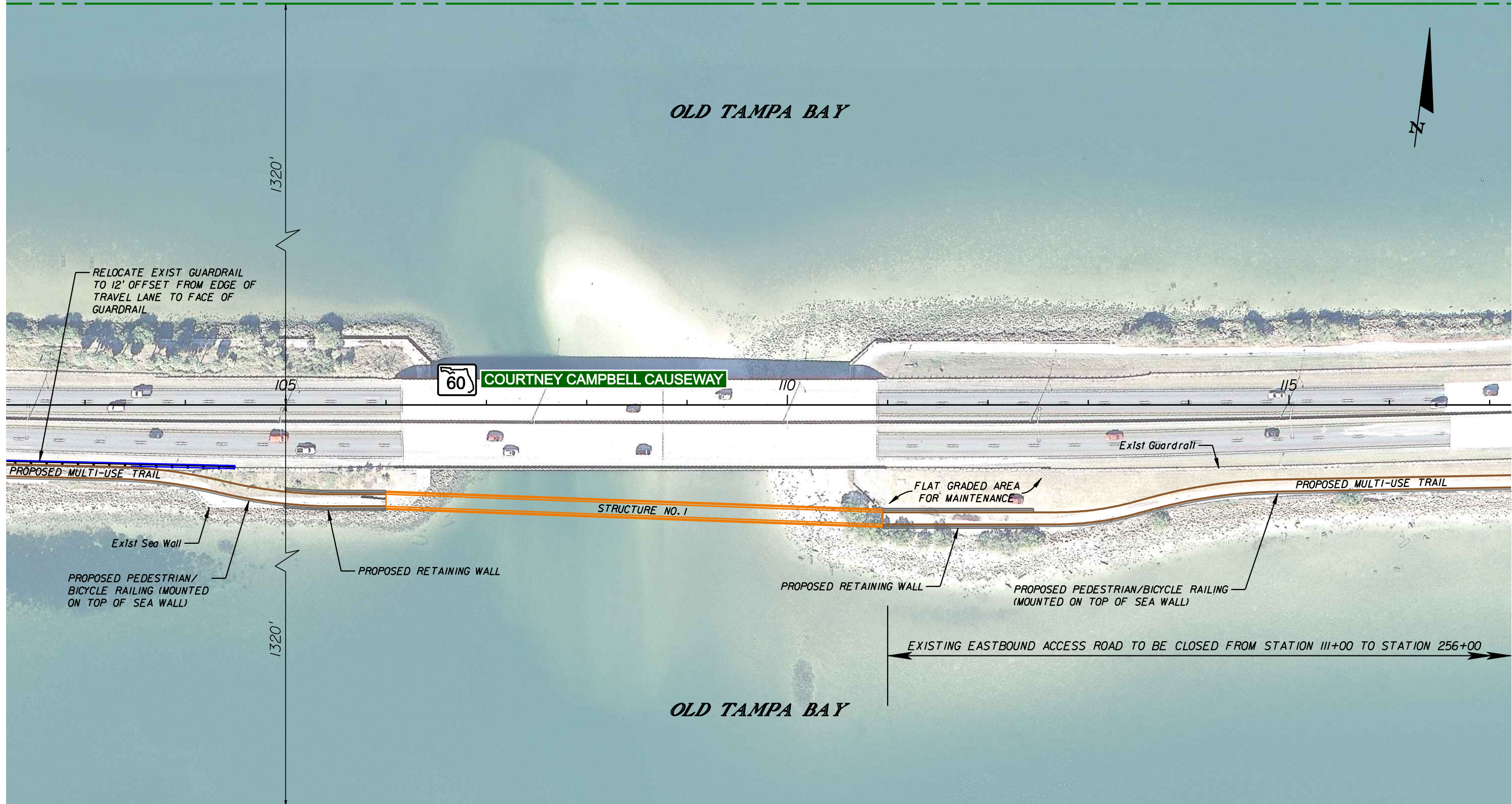
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SR 60 (COURTNEY CAMPBELL CAUSEWAY) MULTI-USE TRAIL
 PD&E STUDY
CONCEPT PLANS
 FROM BAYSHORE BLVD TO WEST OF BEN T DAVIS BEACH ENTRANCE

SHEET NO.
6

OLD TAMPA BAY



RELOCATE EXIST GUARDRAIL TO 12' OFFSET FROM EDGE OF TRAVEL LANE TO FACE OF GUARDRAIL

60 COURTNEY CAMPBELL CAUSEWAY

Exist Guardrail

PROPOSED MULTI-USE TRAIL

FLAT GRADED AREA FOR MAINTENANCE

PROPOSED MULTI-USE TRAIL

STRUCTURE NO. 1

Exist Sea Wall

PROPOSED PEDESTRIAN/BICYCLE RAILING (MOUNTED ON TOP OF SEA WALL)

PROPOSED RETAINING WALL

PROPOSED RETAINING WALL

PROPOSED PEDESTRIAN/BICYCLE RAILING (MOUNTED ON TOP OF SEA WALL)

EXISTING EASTBOUND ACCESS ROAD TO BE CLOSED FROM STATION 111+00 TO STATION 256+00

OLD TAMPA BAY

LEGEND

- PROPOSED MULTI-USE TRAIL
 - RELOCATED ACCESS ROAD
 - PROPOSED MULTI-USE TRAIL BRIDGE
 - UNPAVED PARKING AREA
 - EXISTING RIGHT OF WAY
 - PROPOSED IMPROVEMENTS BY OTHERS (424561-1)
- 0 25 100
Feet
DATE OF AERIAL: NOVEMBER 2008

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SR 60 (COURTNEY CAMPBELL CAUSEWAY) MULTI-USE TRAIL
PD&E STUDY

CONCEPT PLANS

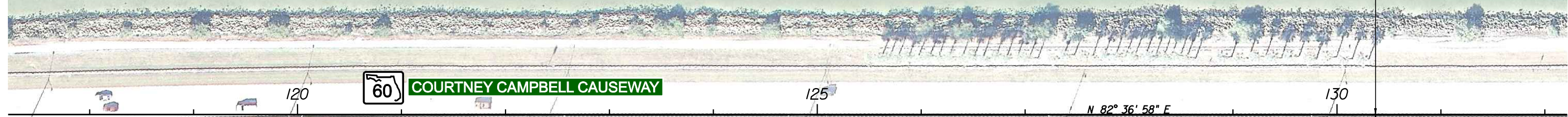
FROM BAYSHORE BLVD TO WEST OF BEN T DAVIS BEACH ENTRANCE

SHEET NO.
7

OLD TAMPA BAY



1320'



60 COURTNEY CAMPBELL CAUSEWAY

N 82° 36' 58" E

Exlst Guardrail

PROPOSED MULTI-USE TRAIL

Exlst Sea Wall

PROPOSED PEDESTRIAN/BICYCLE RAILING
(MOUNTED ON TOP OF SEA WALL)

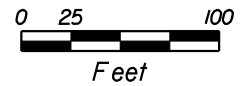
EXISTING EASTBOUND ACCESS ROAD TO BE CLOSED FROM STATION 111+00 TO STATION 256+00

1320'

OLD TAMPA BAY

LEGEND

- PROPOSED MULTI-USE TRAIL
- PROPOSED IMPROVEMENTS BY OTHERS (424561-1)
- RELOCATED ACCESS ROAD
- PROPOSED MULTI-USE TRAIL BRIDGE
- UNPAVED PARKING AREA
- EXISTING RIGHT OF WAY



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PD&E STUDY

CONCEPT PLANS

FROM BAYSHORE BLVD TO WEST OF BEN T DAVIS BEACH ENTRANCE

SHEET NO.
8

OLD TAMPA BAY



PROPOSED GUARDRAIL
REMOVE EXISTING SR 60
ACCESS ROAD ENTRANCE/
EXIT



COURTNEY CAMPBELL CAUSEWAY

135

140

145

Exist Guardrail

PROPOSED MULTI-USE TRAIL

Exist Sea Wall

PROPOSED PEDESTRIAN/BICYCLE RAILING
(MOUNTED ON TOP OF SEA WALL)

EXISTING EASTBOUND ACCESS ROAD TO BE CLOSED FROM STATION III+00 TO STATION 256+00

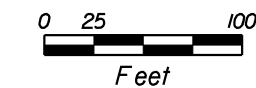
OLD TAMPA BAY

1320'

1320'

LEGEND

- PROPOSED MULTI-USE TRAIL
- PROPOSED IMPROVEMENTS BY OTHERS (424561-1)
- RELOCATED ACCESS ROAD
- PROPOSED MULTI-USE TRAIL BRIDGE
- UNPAVED PARKING AREA
- EXISTING RIGHT OF WAY



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SR 60 (COURTNEY CAMPBELL CAUSEWAY) MULTI-USE TRAIL
PD&E STUDY

CONCEPT PLANS

FROM BAYSHORE BLVD TO WEST OF BEN T DAVIS BEACH ENTRANCE

SHEET NO.
9

OLD TAMPA BAY



1320'



COURTNEY CAMPBELL CAUSEWAY

Exist Guardrail

PROPOSED MULTI-USE TRAIL

Exist Sea Wall

PROPOSED PEDESTRIAN/BICYCLE RAILING
(MOUNTED ON TOP OF SEA WALL)

EXISTING EASTBOUND ACCESS ROAD TO BE CLOSED FROM STATION III+00 TO STATION 256+00

1320'

OLD TAMPA BAY

LEGEND

- PROPOSED MULTI-USE TRAIL
 - RELOCATED ACCESS ROAD
 - PROPOSED MULTI-USE TRAIL BRIDGE
 - UNPAVED PARKING AREA
 - EXISTING RIGHT OF WAY
 - PROPOSED IMPROVEMENTS BY OTHERS (424561-1)
- 0 25 100
Feet
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SR 60 (COURTNEY CAMPBELL CAUSEWAY) MULTI-USE TRAIL
PD&E STUDY

CONCEPT PLANS

FROM BAYSHORE BLVD TO WEST OF BEN T DAVIS BEACH ENTRANCE

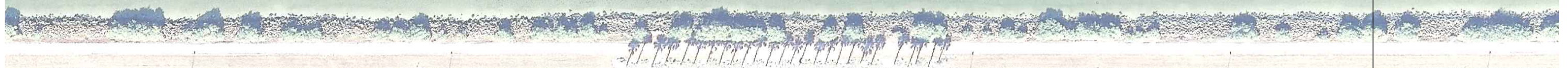
SHEET
NO.

10

OLD TAMPA BAY



1320'



COURTNEY CAMPBELL CAUSEWAY

165

170

175

Exist Guardrail

PROPOSED MULTI-USE TRAIL

Exist Sea Wall

PROPOSED PEDESTRIAN/BICYCLE RAILING
(MOUNTED ON TOP OF SEA WALL)

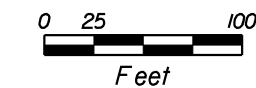
EXISTING EASTBOUND ACCESS ROAD TO BE CLOSED FROM STATION III+00 TO STATION 256+00

1320'

OLD TAMPA BAY

LEGEND

- PROPOSED MULTI-USE TRAIL
- PROPOSED IMPROVEMENTS BY OTHERS (424561-1)
- RELOCATED ACCESS ROAD
- PROPOSED MULTI-USE TRAIL BRIDGE
- UNPAVED PARKING AREA
- EXISTING RIGHT OF WAY



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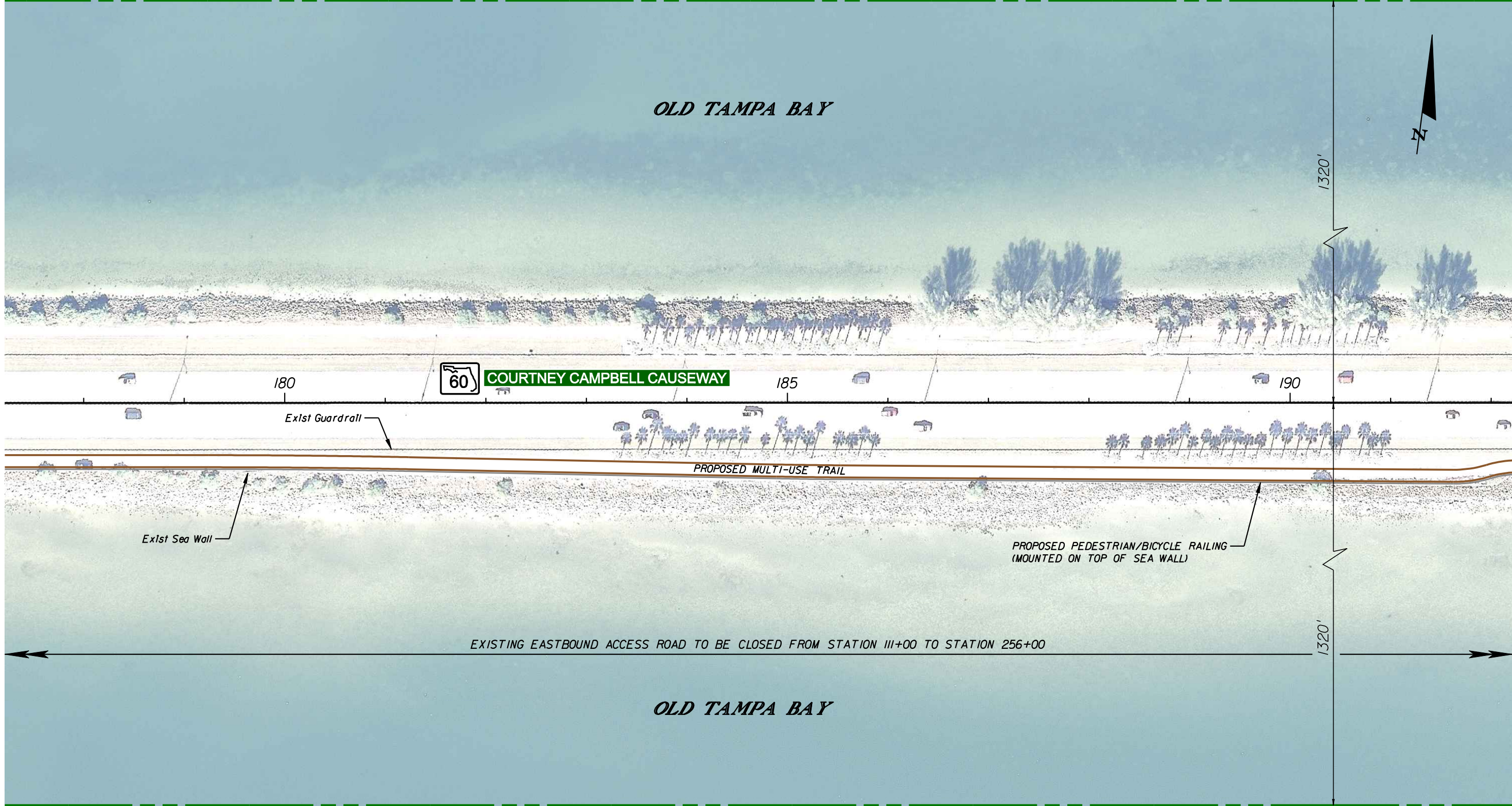
STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
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SR 60	PINELLAS & HILLSBOROUGH	422640-2-22-01

SR 60 (COURTNEY CAMPBELL CAUSEWAY) MULTI-USE TRAIL
PD&E STUDY

CONCEPT PLANS

FROM BAYSHORE BLVD TO WEST OF BEN T DAVIS BEACH ENTRANCE

SHEET NO.
11



LEGEND

- PROPOSED MULTI-USE TRAIL
 - RELOCATED ACCESS ROAD
 - PROPOSED MULTI-USE TRAIL BRIDGE
 - UNPAVED PARKING AREA
 - EXISTING RIGHT OF WAY
 - PROPOSED IMPROVEMENTS BY OTHERS (424561-1)
- 0 25 100
 Feet
 DATE OF AERIAL: NOVEMBER 2008

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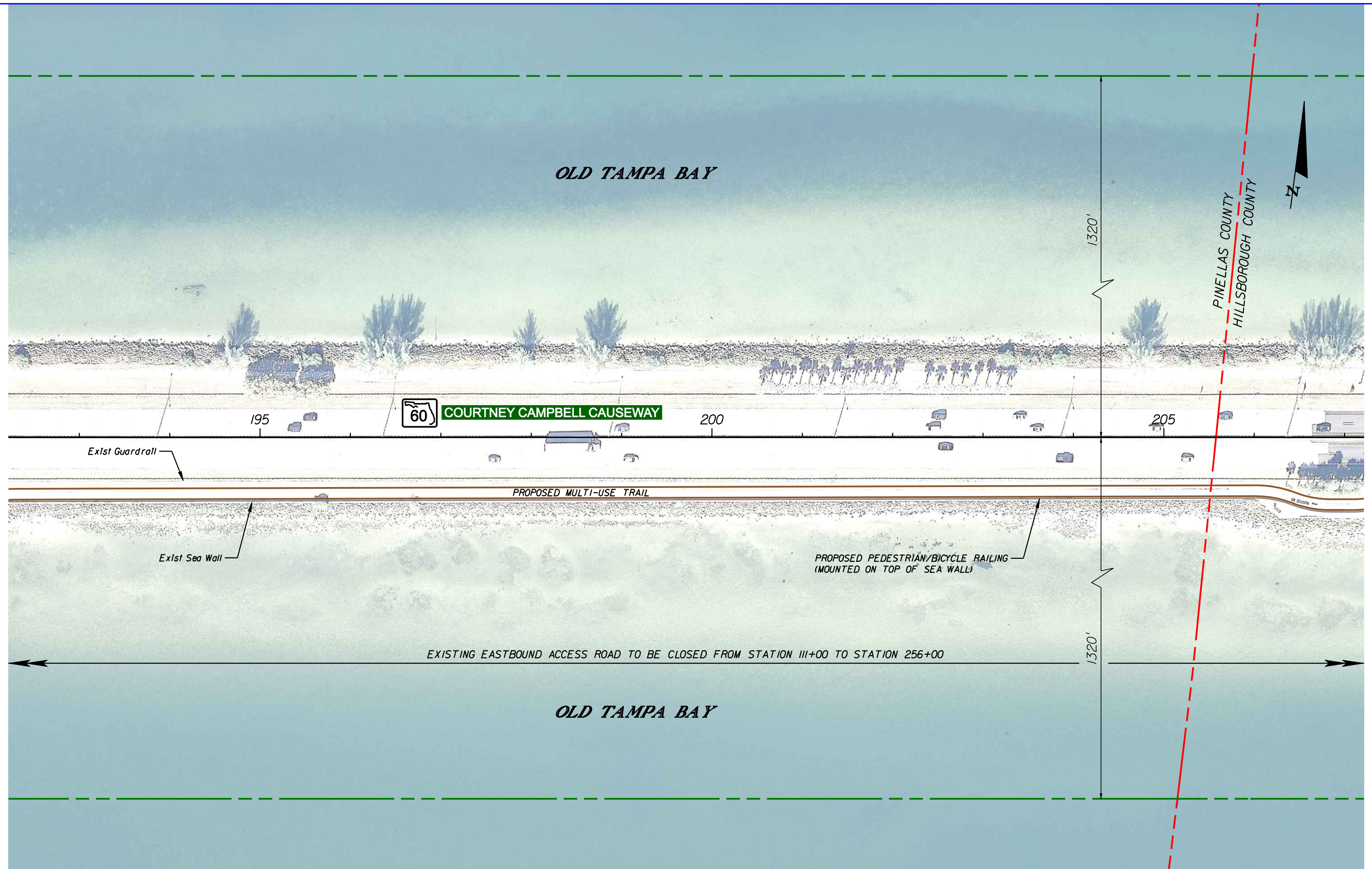
**SR 60 (COURTNEY CAMPBELL CAUSEWAY) MULTI-USE TRAIL
PD&E STUDY**

CONCEPT PLANS

FROM BAYSHORE BLVD TO WEST OF BEN T DAVIS BEACH ENTRANCE

SHEET
NO.

12



LEGEND

- PROPOSED MULTI-USE TRAIL
 - RELOCATED ACCESS ROAD
 - PROPOSED MULTI-USE TRAIL BRIDGE
 - UNPAVED PARKING AREA
 - EXISTING RIGHT OF WAY
 - PROPOSED IMPROVEMENTS BY OTHERS (424561-1)
- 0 25 100
 Feet
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**SR 60 (COURTNEY CAMPBELL CAUSEWAY) MULTI-USE TRAIL
PD&E STUDY**

CONCEPT PLANS

FROM BAYSHORE BLVD TO WEST OF BEN T DAVIS BEACH ENTRANCE

SHEET
NO.

13

OLD TAMPA BAY



1320'



COURTNEY CAMPBELL CAUSEWAY

Exst Guardrail

PROPOSED MULTI-USE TRAIL

Exst Sea Wall

PROPOSED PEDESTRIAN/BICYCLE RAILING
(MOUNTED ON TOP OF SEA WALL)

EXISTING EASTBOUND ACCESS ROAD TO BE CLOSED FROM STATION III+00 TO STATION 256+00

1320'

OLD TAMPA BAY

LEGEND

- PROPOSED MULTI-USE TRAIL
 - RELOCATED ACCESS ROAD
 - PROPOSED MULTI-USE TRAIL BRIDGE
 - UNPAVED PARKING AREA
 - EXISTING RIGHT OF WAY
 - PROPOSED IMPROVEMENTS BY OTHERS (424561-1)
- 0 25 100
Feet
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PD&E STUDY

CONCEPT PLANS

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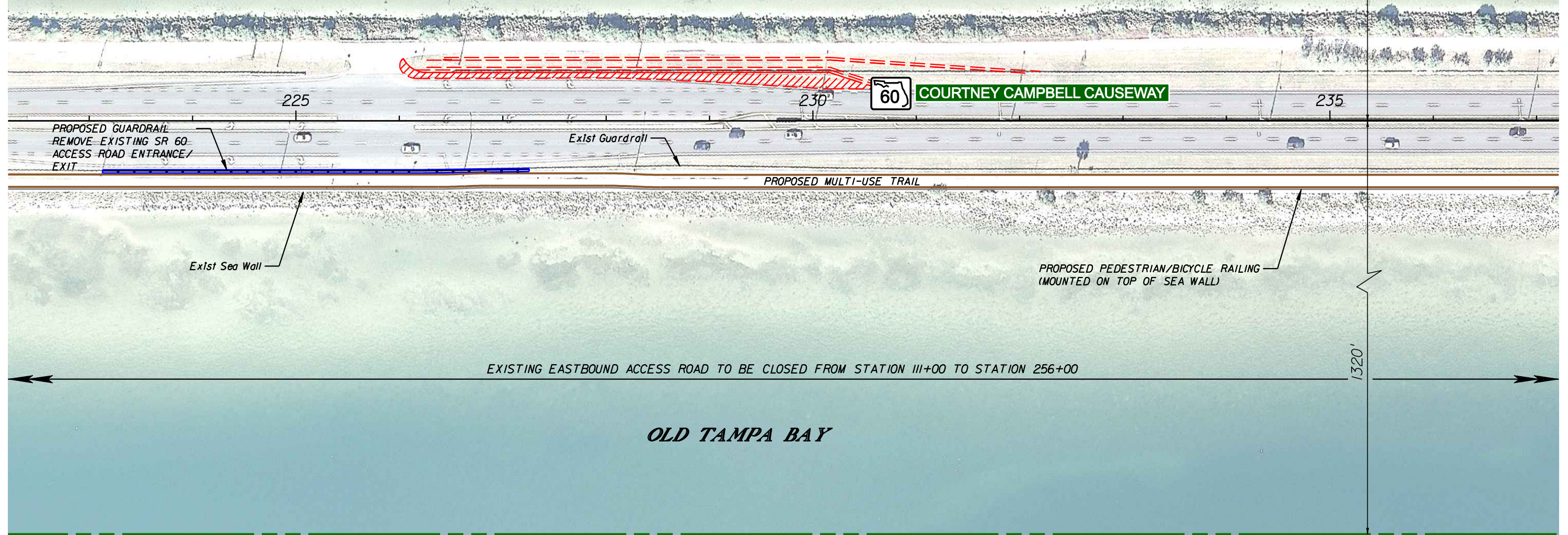
SHEET
NO.

14

OLD TAMPA BAY



1320'



PROPOSED GUARDRAIL
REMOVE EXISTING SR 60
ACCESS ROAD ENTRANCE/
EXIT

Exist Guardrail

PROPOSED MULTI-USE TRAIL

Exist Sea Wall

PROPOSED PEDESTRIAN/BICYCLE RAILING
(MOUNTED ON TOP OF SEA WALL)

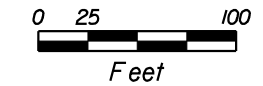
EXISTING EASTBOUND ACCESS ROAD TO BE CLOSED FROM STATION 111+00 TO STATION 256+00

OLD TAMPA BAY

1320'

LEGEND

- PROPOSED MULTI-USE TRAIL
- RELOCATED ACCESS ROAD
- PROPOSED MULTI-USE TRAIL BRIDGE
- UNPAVED PARKING AREA
- EXISTING RIGHT OF WAY
- PROPOSED IMPROVEMENTS BY OTHERS (424561-1)



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PD&E STUDY**

CONCEPT PLANS

FROM BAYSHORE BLVD TO WEST OF BEN T DAVIS BEACH ENTRANCE

SHEET
NO.

15

OLD TAMPA BAY

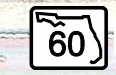


1320'

RELOCATE EXIST GUARDRAIL TO 12' OFFSET FROM EDGE OF TRAVEL LANE TO FACE OF GUARDRAIL

240

245



COURTNEY CAMPBELL CAUSEWAY

250

Exist Guardrail

PROPOSED MULTI-USE TRAIL

Exist Sea Wall

PROPOSED PEDESTRIAN/BICYCLE RAILING (MOUNTED ON TOP OF SEA WALL)

EXISTING EASTBOUND ACCESS ROAD TO BE CLOSED FROM STATION 111+00 TO STATION 256+00

1320'

OLD TAMPA BAY

LEGEND

- PROPOSED MULTI-USE TRAIL
 - RELOCATED ACCESS ROAD
 - PROPOSED MULTI-USE TRAIL BRIDGE
 - UNPAVED PARKING AREA
 - EXISTING RIGHT OF WAY
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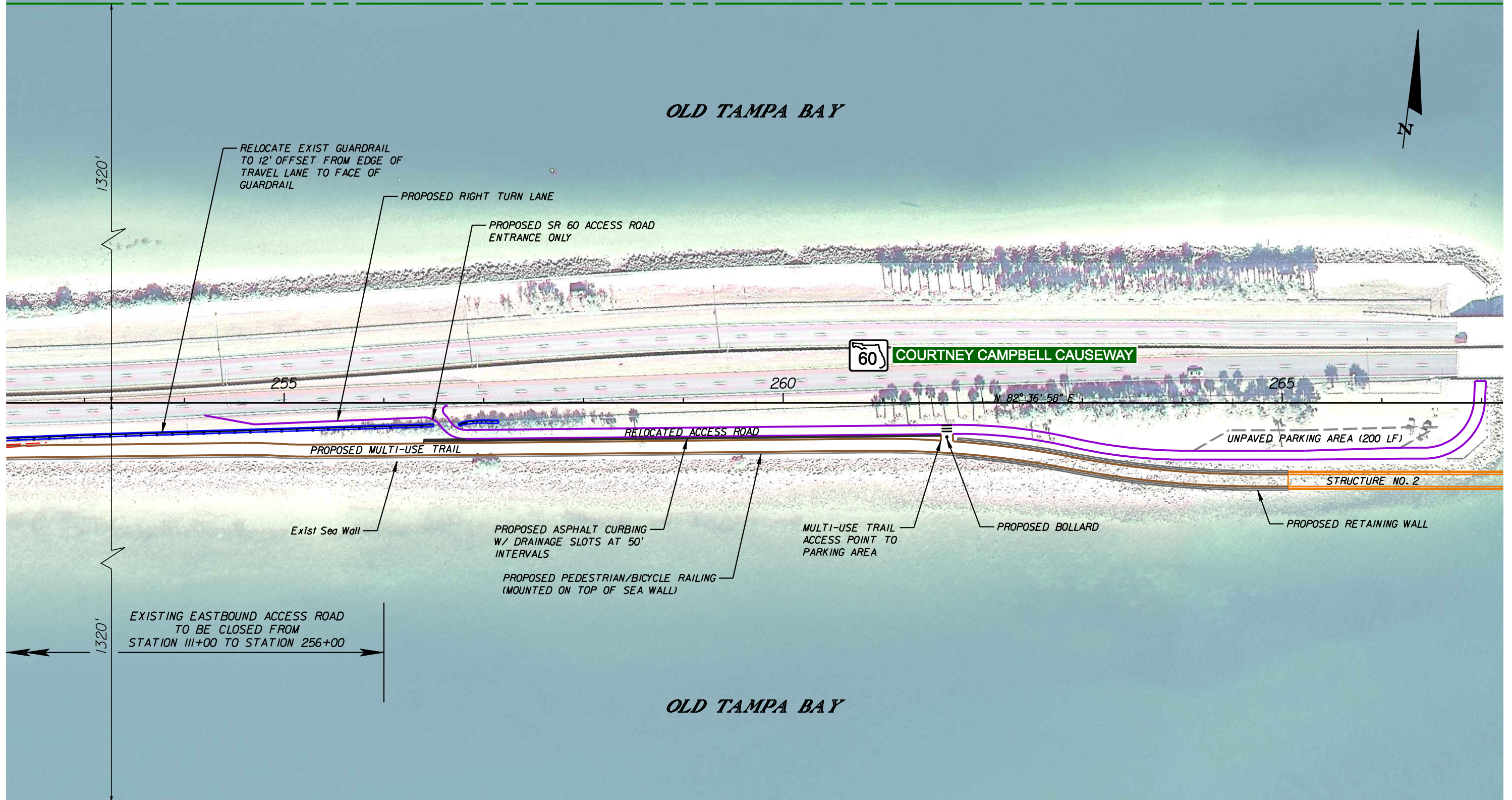
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PD&E STUDY

CONCEPT PLANS

FROM BAYSHORE BLVD TO WEST OF BEN T DAVIS BEACH ENTRANCE

SHEET NO.
16

OLD TAMPA BAY



RELOCATE EXIST GUARDRAIL TO 12' OFFSET FROM EDGE OF TRAVEL LANE TO FACE OF GUARDRAIL

PROPOSED RIGHT TURN LANE

PROPOSED SR 60 ACCESS ROAD ENTRANCE ONLY

60 COURTNEY CAMPBELL CAUSEWAY

255

260

265

N 82° 36' 58" E

PROPOSED MULTI-USE TRAIL

RELOCATED ACCESS ROAD

UNPAVED PARKING AREA (200 LF)

STRUCTURE NO. 2

Exist Sea Wall

PROPOSED ASPHALT CURBING W/ DRAINAGE SLOTS AT 50' INTERVALS

MULTI-USE TRAIL ACCESS POINT TO PARKING AREA

PROPOSED BOLLARD







PROPOSED RETAINING WALL

PROPOSED PEDESTRIAN/BICYCLE RAILING (MOUNTED ON TOP OF SEA WALL)

EXISTING EASTBOUND ACCESS ROAD TO BE CLOSED FROM STATION III+00 TO STATION 256+00

OLD TAMPA BAY

LEGEND

-  PROPOSED MULTI-USE TRAIL
 -  RELOCATED ACCESS ROAD
 -  PROPOSED MULTI-USE TRAIL BRIDGE
 -  UNPAVED PARKING AREA
 -  EXISTING RIGHT OF WAY
 -  PROPOSED IMPROVEMENTS BY OTHERS (424561-1)
- 0 25 100
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SHEET NO.
17

OLD TAMPA BAY



1320'



COURTNEY CAMPBELL CAUSEWAY

270

275

280

STRUCTURE NO. 2

1320'

OLD TAMPA BAY

LEGEND

- PROPOSED MULTI-USE TRAIL
 - PROPOSED IMPROVEMENTS BY OTHERS (424561-1)
 - RELOCATED ACCESS ROAD
 - PROPOSED MULTI-USE TRAIL BRIDGE
 - UNPAVED PARKING AREA
 - EXISTING RIGHT OF WAY
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SHEET
NO.

18

OLD TAMPA BAY



1320'



COURTNEY CAMPBELL CAUSEWAY

285

290

295

STRUCTURE NO. 2

BRIDGE FENDERS TO BE EXTENDED
UNDER PROPOSED TRAIL BRIDGE

1320'

OLD TAMPA BAY

LEGEND

- PROPOSED MULTI-USE TRAIL
 - RELOCATED ACCESS ROAD
 - PROPOSED MULTI-USE TRAIL BRIDGE
 - UNPAVED PARKING AREA
 - EXISTING RIGHT OF WAY
 - PROPOSED IMPROVEMENTS BY OTHERS (424561-1)
- Feet
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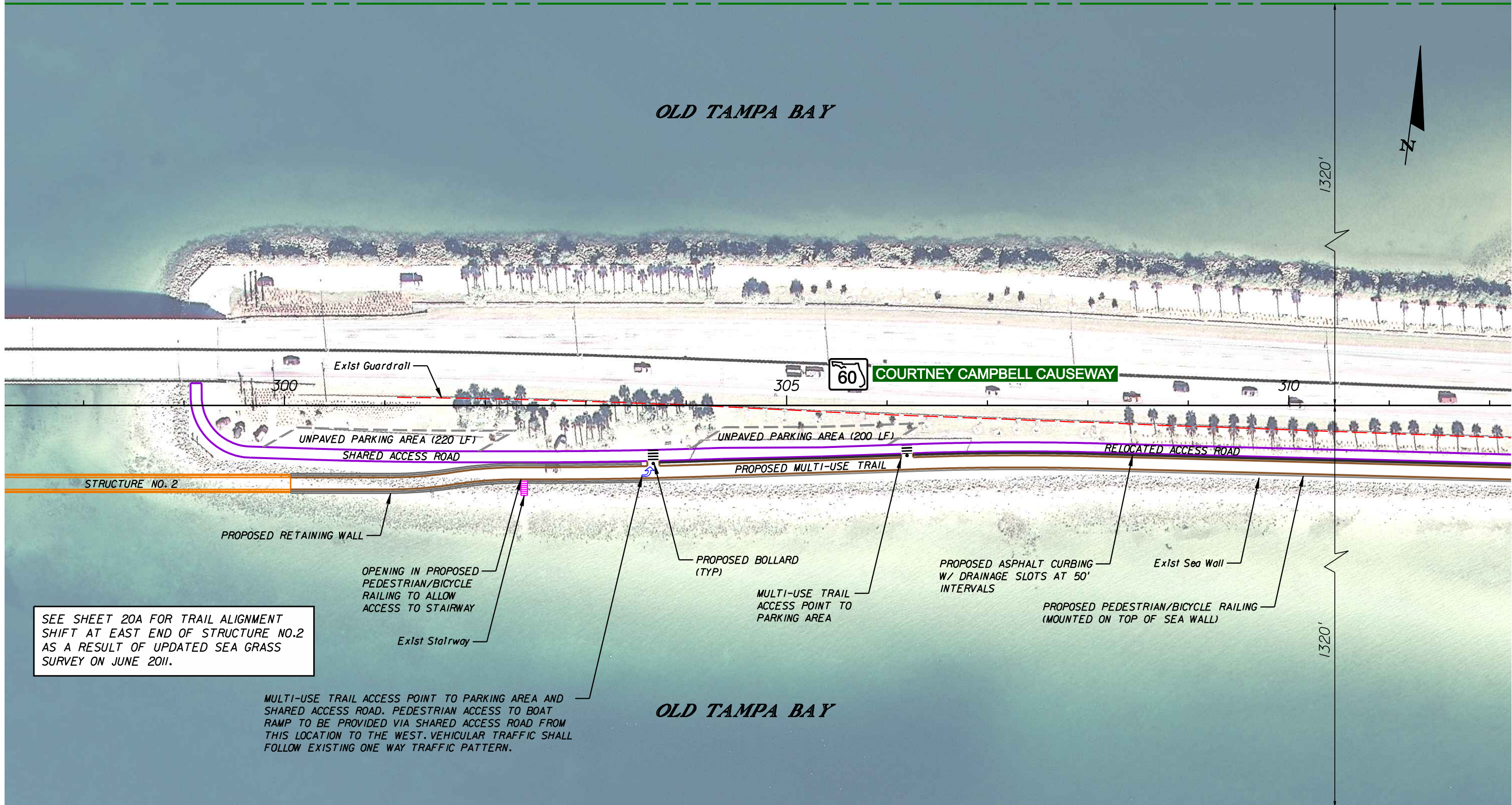
CONCEPT PLANS

FROM BAYSHORE BLVD TO WEST OF BEN T DAVIS BEACH ENTRANCE

SHEET
NO.

19

OLD TAMPA BAY

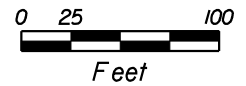


SEE SHEET 20A FOR TRAIL ALIGNMENT SHIFT AT EAST END OF STRUCTURE NO.2 AS A RESULT OF UPDATED SEA GRASS SURVEY ON JUNE 2011.

MULTI-USE TRAIL ACCESS POINT TO PARKING AREA AND SHARED ACCESS ROAD. PEDESTRIAN ACCESS TO BOAT RAMP TO BE PROVIDED VIA SHARED ACCESS ROAD FROM THIS LOCATION TO THE WEST. VEHICULAR TRAFFIC SHALL FOLLOW EXISTING ONE WAY TRAFFIC PATTERN.

LEGEND

- PROPOSED MULTI-USE TRAIL
- RELOCATED ACCESS ROAD
- PROPOSED MULTI-USE TRAIL BRIDGE
- UNPAVED PARKING AREA
- EXISTING RIGHT OF WAY
- PROPOSED IMPROVEMENTS BY OTHERS (424561-1)



DATE OF AERIAL: NOVEMBER 2008

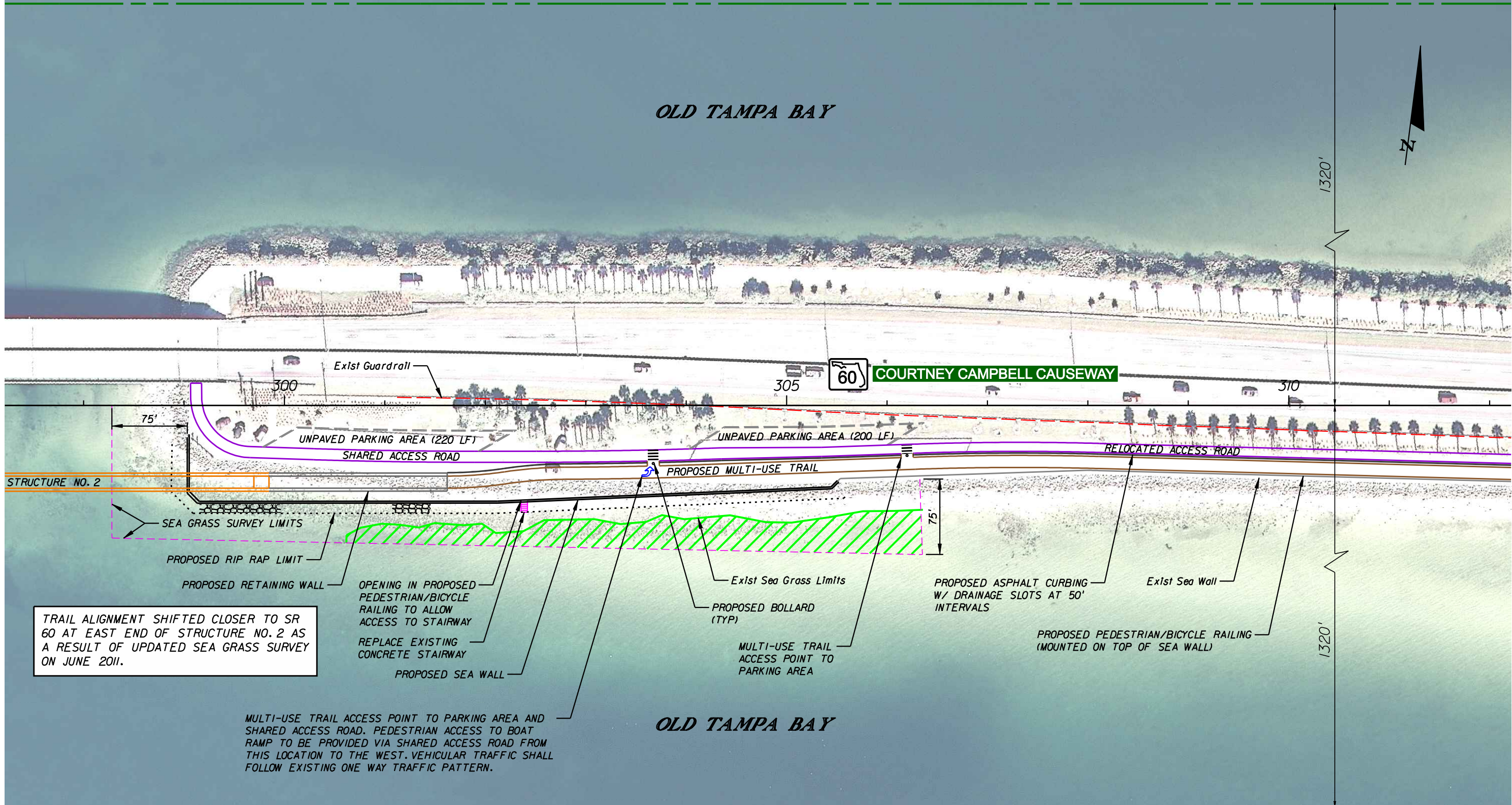
American Consulting Engineers of Florida, LLC.
 2818 Cypress Ridge Blvd, Suite 200
 Wesley Chapel, Florida 33544
 Phone: (813) 435-2600 Fax: (813) 435-2601
 Certificate of Authorization No. 9302
 Jeffrey S. Novotny, P.E. No. 51083

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
SR 60	PINELLAS & HILLSBOROUGH	422640-2-22-01

SR 60 (COURTNEY CAMPBELL CAUSEWAY) MULTI-USE TRAIL
 PD&E STUDY
CONCEPT PLANS
 FROM BAYSHORE BLVD TO WEST OF BEN T DAVIS BEACH ENTRANCE

SHEET NO.
 20

OLD TAMPA BAY



MULTI-USE TRAIL ACCESS POINT TO PARKING AREA AND SHARED ACCESS ROAD. PEDESTRIAN ACCESS TO BOAT RAMP TO BE PROVIDED VIA SHARED ACCESS ROAD FROM THIS LOCATION TO THE WEST. VEHICULAR TRAFFIC SHALL FOLLOW EXISTING ONE WAY TRAFFIC PATTERN.

LEGEND

	PROPOSED MULTI-USE TRAIL		PROPOSED IMPROVEMENTS BY OTHERS (424561-1)
	RELOCATED ACCESS ROAD		SEA GRASS LIMITS (SURVEYED)
	PROPOSED MULTI-USE TRAIL BRIDGE		
	UNPAVED PARKING AREA		
	EXISTING RIGHT OF WAY		

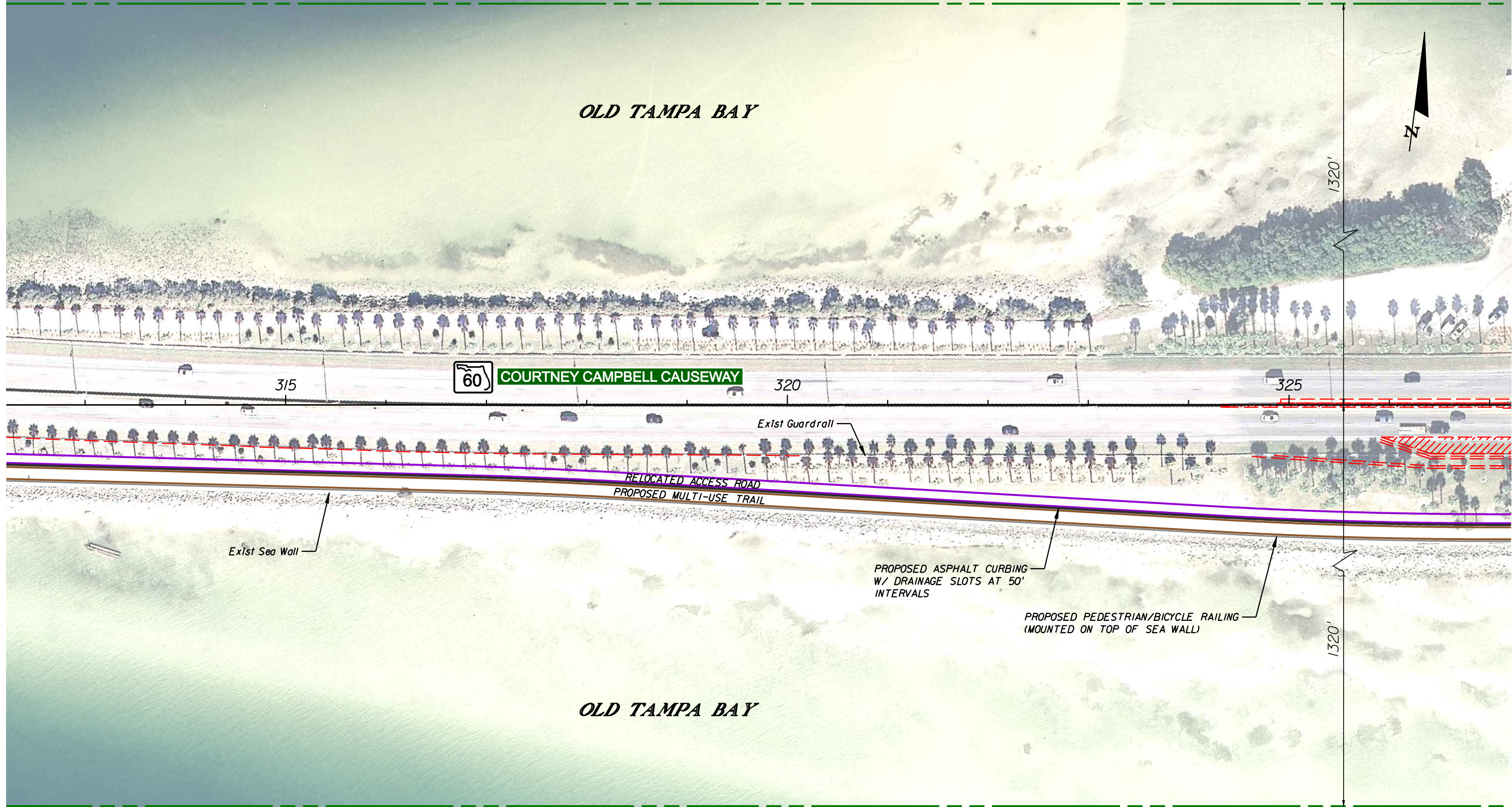
0 25 100
Feet
DATE OF AERIAL: NOVEMBER 2008

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PD&E STUDY
CONCEPT PLANS
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SHEET NO.
20A



OLD TAMPA BAY

OLD TAMPA BAY

LEGEND

- PROPOSED MULTI-USE TRAIL
 - RELOCATED ACCESS ROAD
 - PROPOSED MULTI-USE TRAIL BRIDGE
 - UNPAVED PARKING AREA
 - EXISTING RIGHT OF WAY
 - PROPOSED IMPROVEMENTS BY OTHERS (424561-1)
- 0 25 100
 Feet
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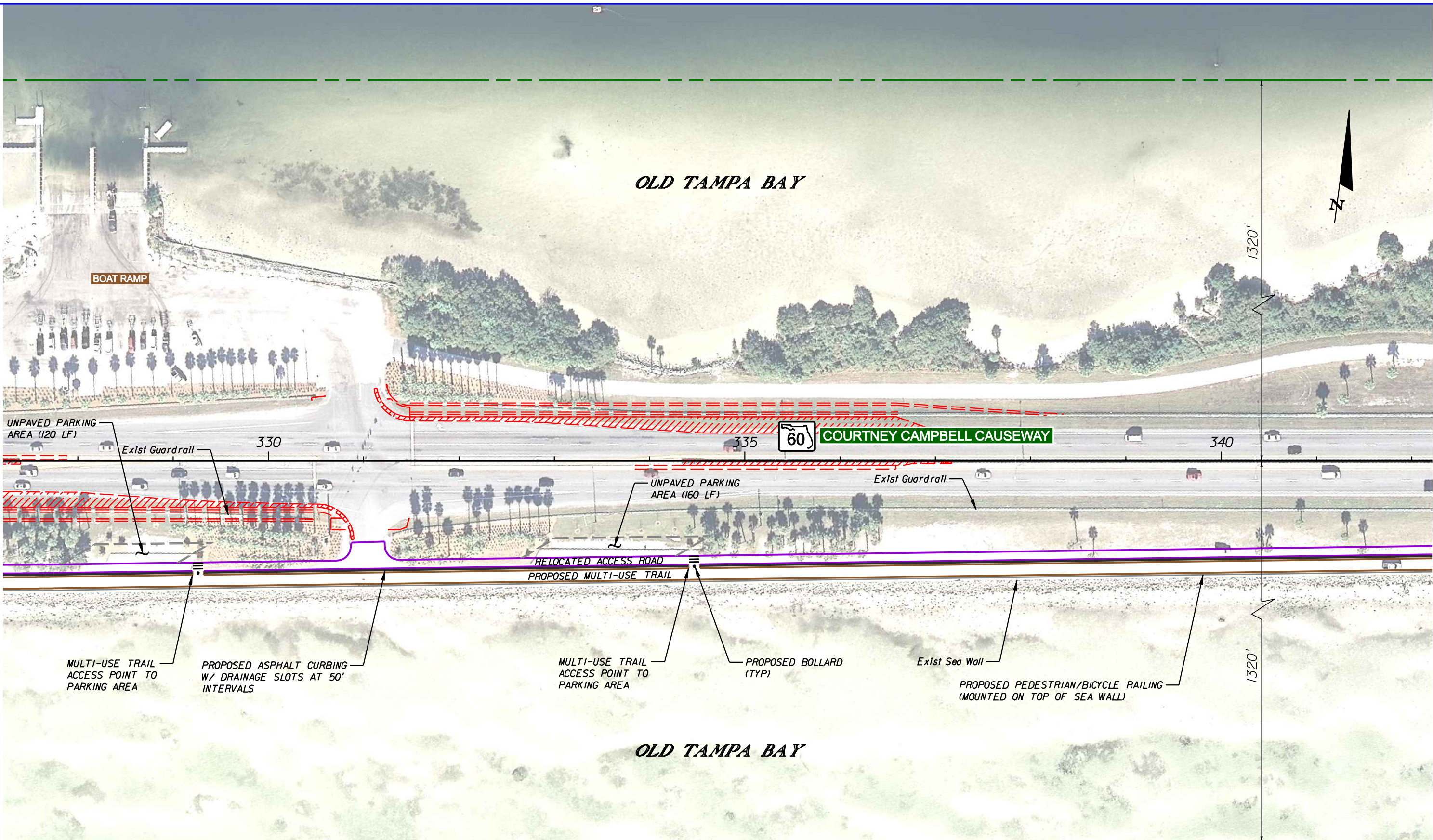
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PD&E STUDY**

CONCEPT PLANS

FROM BAYSHORE BLVD TO WEST OF BEN T DAVIS BEACH ENTRANCE

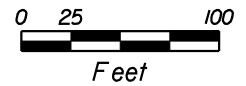
SHEET
NO.

21



LEGEND

- PROPOSED MULTI-USE TRAIL
- RELOCATED ACCESS ROAD
- PROPOSED MULTI-USE TRAIL BRIDGE
- UNPAVED PARKING AREA
- EXISTING RIGHT OF WAY
- PROPOSED IMPROVEMENTS BY OTHERS (424561-1)



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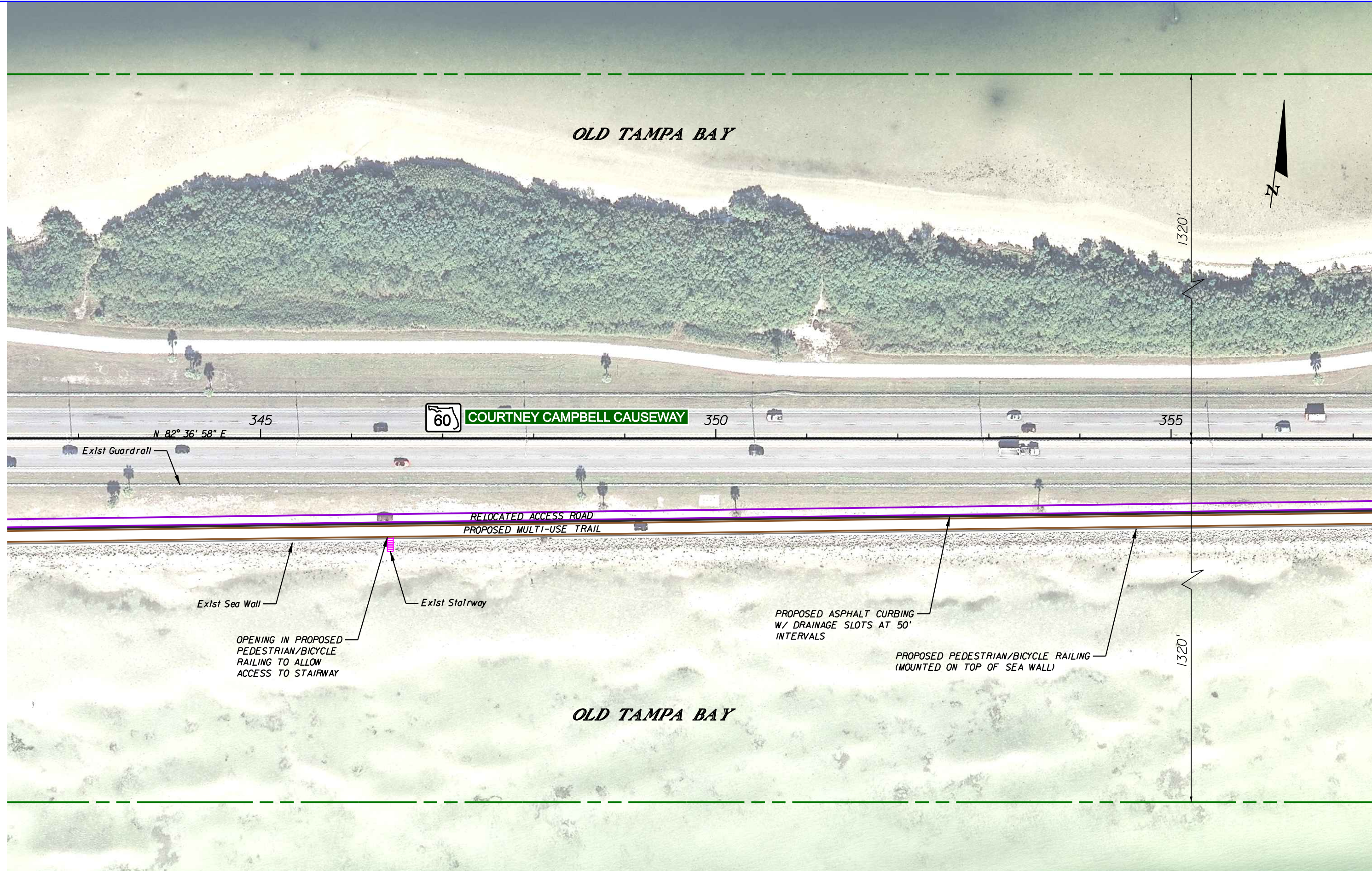
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 PD&E STUDY**

CONCEPT PLANS

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SHEET
NO.

22



LEGEND

- PROPOSED MULTI-USE TRAIL
 - RELOCATED ACCESS ROAD
 - PROPOSED MULTI-USE TRAIL BRIDGE
 - UNPAVED PARKING AREA
 - EXISTING RIGHT OF WAY
 - PROPOSED IMPROVEMENTS BY OTHERS (424561-1)
- 0 25 100
 Feet
 DATE OF AERIAL: NOVEMBER 2008

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SHEET
NO.

23

OLD TAMPA BAY

OLD TAMPA BAY

COURTNEY CAMPBELL CAUSEWAY



Exist Guardrail

Exist Guardrail

PROPOSED ASPHALT CURBING
W/ DRAINAGE SLOTS AT 50'
INTERVALS

PROPOSED PEDESTRIAN/BICYCLE RAILING
(MOUNTED ON TOP OF SEA WALL)

MULTI-USE TRAIL
ACCESS POINT TO
PARKING AREA

PROPOSED BOLLARD
(TYP)

Exist Sea Wall

MULTI-USE TRAIL
ACCESS POINT TO
PARKING AREA

RELOCATED ACCESS ROAD
PROPOSED MULTI-USE TRAIL

UNPAVED PARKING AREA (500 LF)

1320'

1320'

360

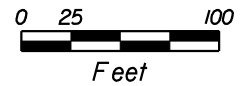
365

60

370

LEGEND

- PROPOSED MULTI-USE TRAIL
- RELOCATED ACCESS ROAD
- PROPOSED MULTI-USE TRAIL BRIDGE
- UNPAVED PARKING AREA
- EXISTING RIGHT OF WAY
- PROPOSED IMPROVEMENTS BY OTHERS (424561-1)



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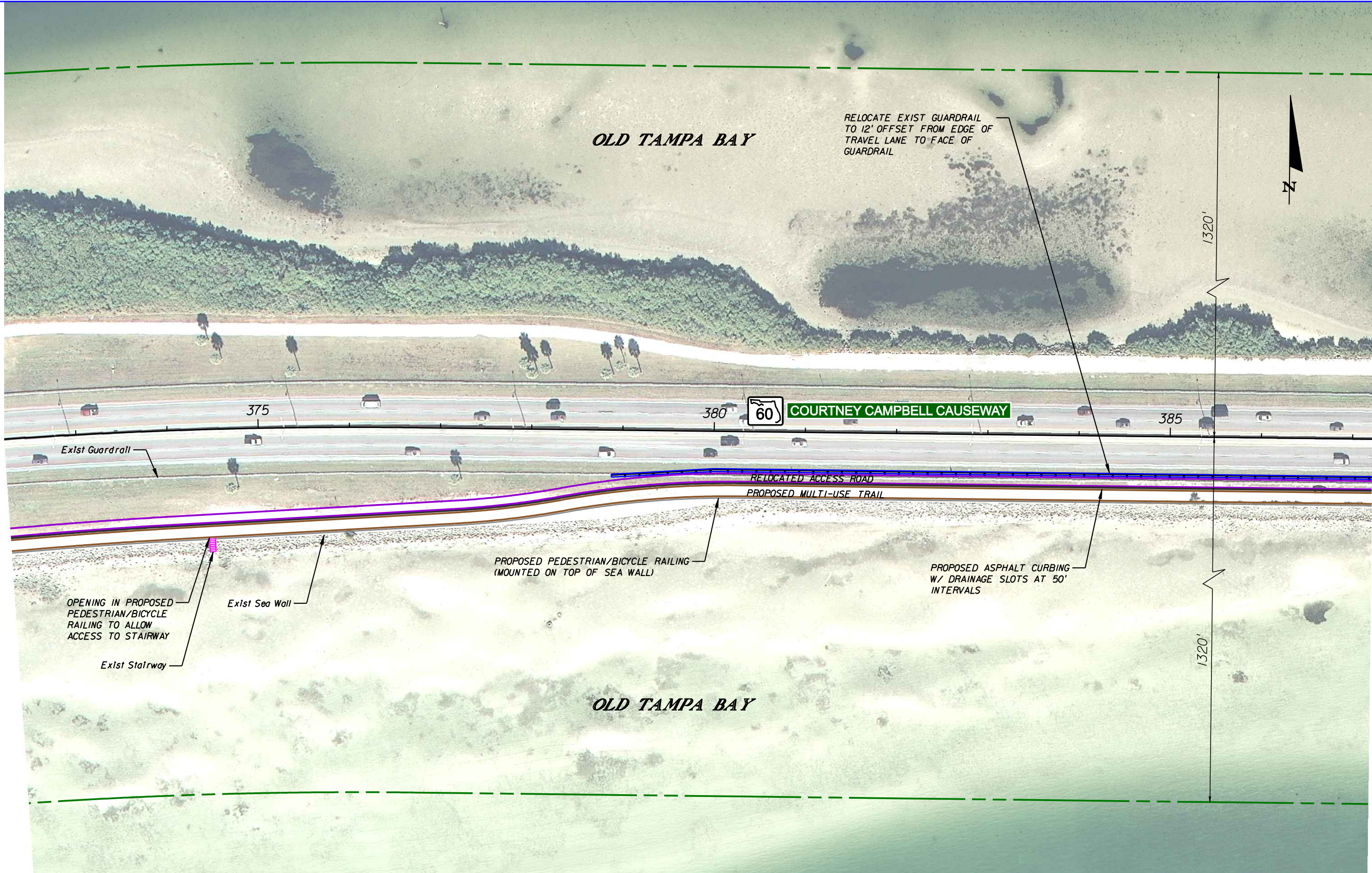
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CONCEPT PLANS

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SHEET NO.
24



LEGEND

	PROPOSED MULTI-USE TRAIL		PROPOSED IMPROVEMENTS BY OTHERS (424561-1)
	RELOCATED ACCESS ROAD		PROPOSED PEDESTRIAN/BICYCLE RAILING
	PROPOSED MULTI-USE TRAIL BRIDGE		EXISTING STAIRWAY
	EXIST GUARDRAIL		EXIST SEA WALL
	EXIST STAIRWAY		PROPOSED ASPHALT CURBING
	EXISTING RIGHT OF WAY		OPENING IN PROPOSED PEDESTRIAN/BICYCLE RAILING



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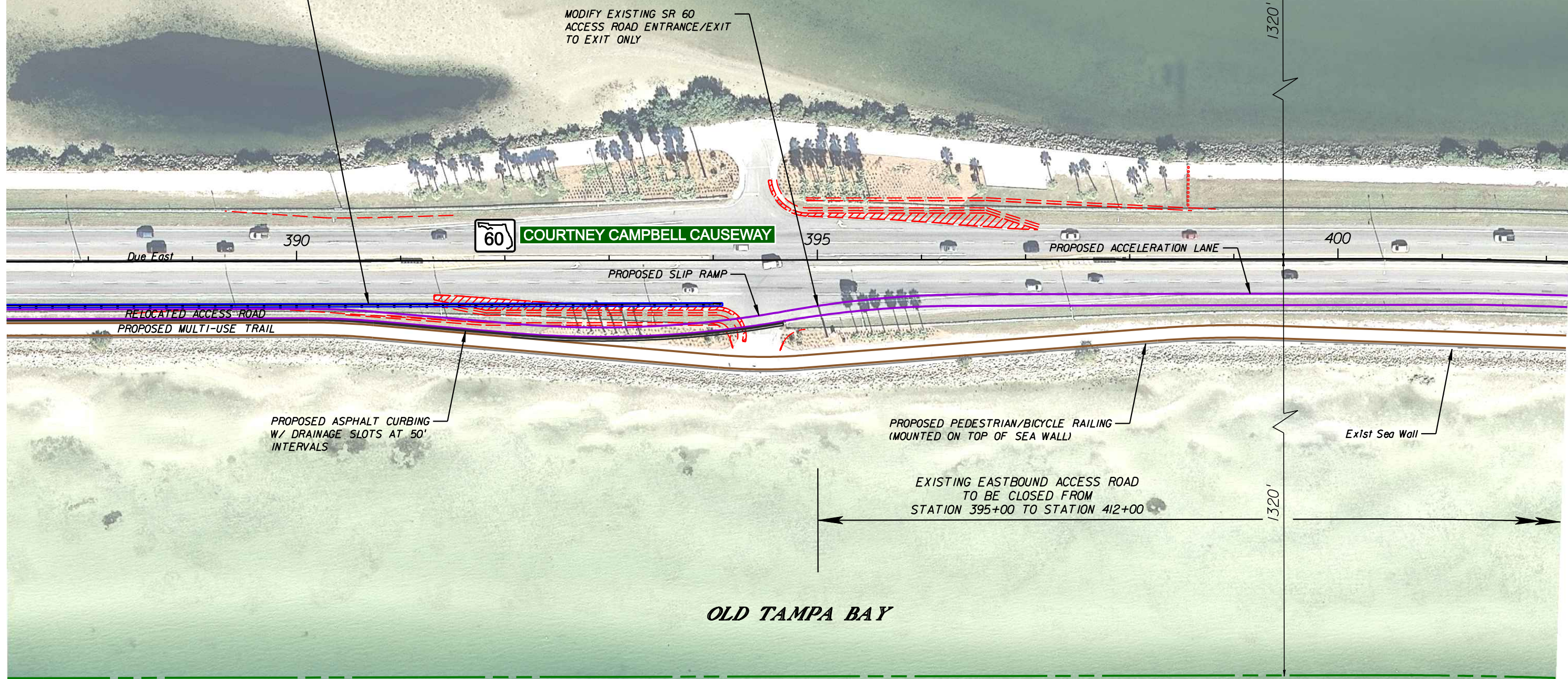
SHEET NO.
 25

OLD TAMPA BAY



RELOCATE EXIST GUARDRAIL TO 12' OFFSET FROM EDGE OF TRAVEL LANE TO FACE OF GUARDRAIL

MODIFY EXISTING SR 60 ACCESS ROAD ENTRANCE/EXIT TO EXIT ONLY



PROPOSED ASPHALT CURBING W/ DRAINAGE SLOTS AT 50' INTERVALS

PROPOSED PEDESTRIAN/BICYCLE RAILING (MOUNTED ON TOP OF SEA WALL)

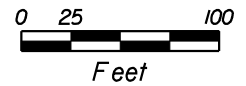
Exist Sea Wall

EXISTING EASTBOUND ACCESS ROAD TO BE CLOSED FROM STATION 395+00 TO STATION 412+00

OLD TAMPA BAY

LEGEND

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- RELOCATED ACCESS ROAD
- PROPOSED MULTI-USE TRAIL BRIDGE
- UNPAVED PARKING AREA
- EXISTING RIGHT OF WAY
- PROPOSED IMPROVEMENTS BY OTHERS (424561-1)



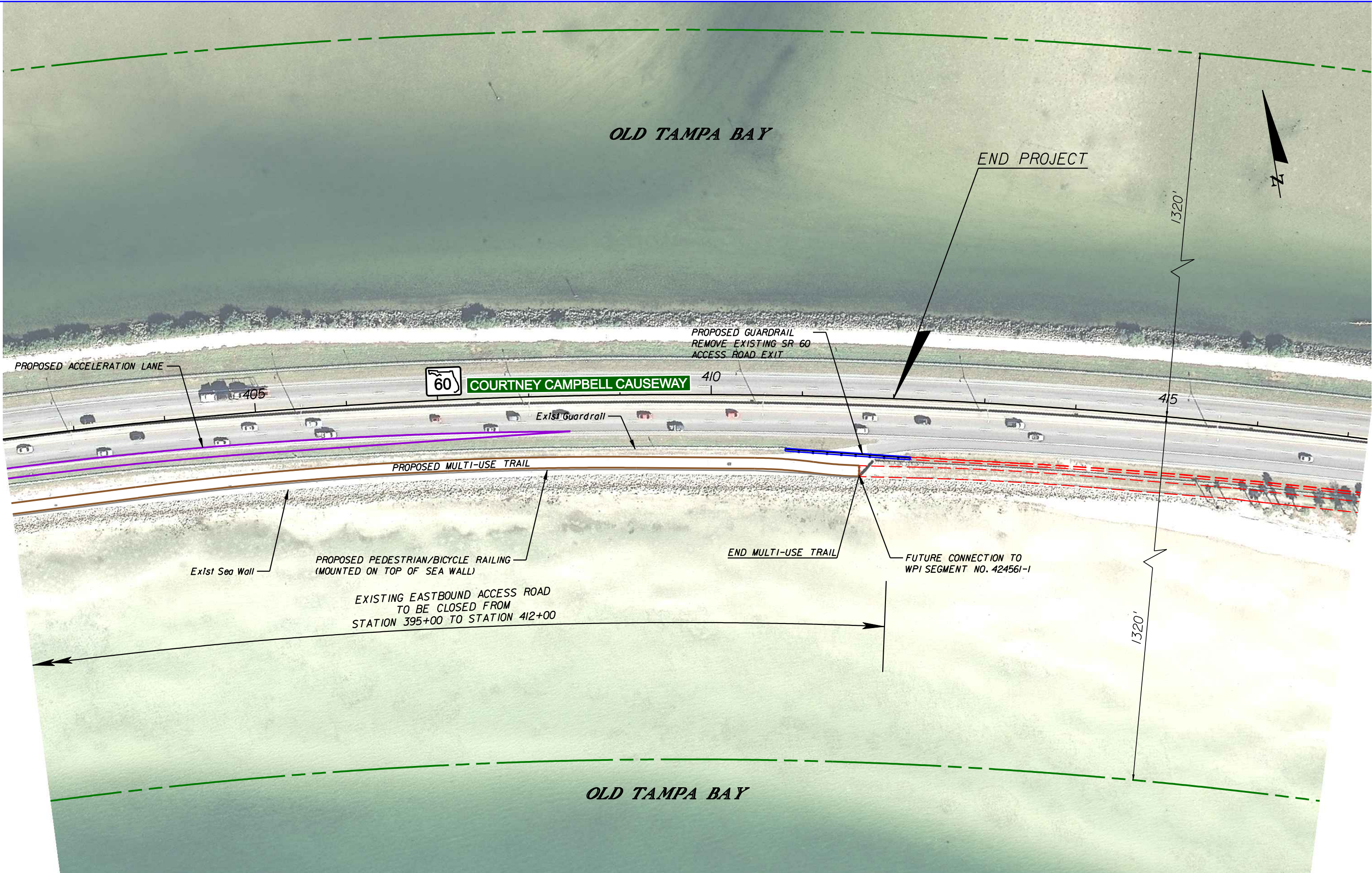
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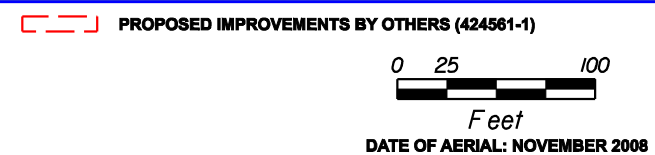
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SHEET NO.
 26



LEGEND

	PROPOSED MULTI-USE TRAIL		PROPOSED IMPROVEMENTS BY OTHERS (424561-1)
	RELOCATED ACCESS ROAD		
	PROPOSED MULTI-USE TRAIL BRIDGE		
	UNPAVED PARKING AREA		
	EXISTING RIGHT OF WAY		



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