

US 92 (SR 600)

From East of I-4 to East of County Line Road

Project Development & Environment (PD&E) Study Reevaluation

Final Comments and Coordination Report

Work Program Item Segment No. 435749-1

Federal Aid Project No: MAF-212-1(34)

Hillsborough County

Prepared for:

Florida Department of Transportation

District Seven



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EXECUTIVE SUMMARY

The Florida Department of Transportation (FDOT) conducted a Project Development and Environment (PD&E) study reevaluation to evaluate alternative improvements for US 92 (SR 600) from east of I-4 (milepoint 6.583) to east of County Line Road (milepoint 24.593) in Hillsborough County (**Figure 1-1**), a distance of approximately 18.1 miles. Study objectives included: reevaluate proposed typical sections, while minimizing impacts to the environment; consider agency and public comments; and ensure project compliance with all applicable federal and state laws. Improvement alternatives were identified which will improve safety and satisfy future transportation demand.

A comprehensive public involvement program was carried out for this study consistent with the Public Involvement Plan (PIP) prepared for this study.

Subsequent coordination with agencies occurred through the submittal and review of various project reports. Agencies which commented on the proposed project included:

- U.S. Fish and Wildlife Service (USFWS)
- Florida Fish and Wildlife Conservation Commission (FWC)
- Florida Department of State, Division of Historical Resources (SHPO)

In addition to agency coordination via report reviews, coordination meetings and/or presentations were given to the following local agencies and other groups to inform them about the project and to solicit comments:

- Hillsborough County Public Works engineering staff
- Hillsborough County Metropolitan Planning Organization (MPO)
- City of Plant City Engineering Staff
- Plant City Strawberry Festival Board
- Plant City Economic Development Corporation
- Seffner Chamber of Commerce

A mailing list was developed which included 3731 property owners located adjacent to or near the proposed project's limits, in addition to other interested individuals. This mailing list was used in distributing three newsletters about the project, included in **Appendix B**:

- An initial Kick-Off newsletter
- A public hearing invitational newsletter, and
- A final newsletter distributed after study approval

A project website (<http://active.fdotd7studies.com/sr600/garden-lane-to-county-line>) was also developed and maintained to make information about the proposed project readily available to the public and to offer a means for citizens to provide comments online to the study team at any time during the reevaluation.

A public hearing was held for this project in two sessions at separate locations. On December 1, 2016, the first session was held from 5:30 p.m. to 7:30 p.m. at the HCC Trinkle Center in Plant City.

The second session was held on December 6th, 2016, from 6:00 p.m. to 8:00 p.m. at the Sheraton Tampa East Hotel. The hearing was held to inform citizens and interested parties about the project details and schedule, and allow them the opportunity to provide comments concerning the proposed improvements. The hearing consisted of an open house for the first hour and a formal presentation and public comment period beginning after and followed by an open house until the end of the session.

A total of 239 people signed in at the public hearing between the two sessions. The public hearing transcript is included in **Appendix C**. Seventy Seven (77) comment forms were received and 12 verbal statements were made during the formal public comment period for a total of 89 comment forms. Of the 89 comment forms, 33 involved requests to be added to the project contact list and 10 pertained to access management issues. Most comments expressed support for the project. **Table 8-1** summarizes public comments received. **Appendix D** contains copies of the written comments and responses. Copies of all public hearing displays and presentation materials are included in the *Public Hearing Scrapbook* that was prepared for this PD&E study re-evaluation.

On April 20, 2018, the Florida Department of Transportation (FDOT) approved the US 92 (SR 600) PD&E Study Design Change Reevaluation.

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SECTION 1 INTRODUCTION

1.1 PD&E STUDY REEVALUATION PURPOSE

The Florida Department of Transportation (FDOT) conducted a Project Development & Environment (PD&E) study reevaluation to consider widening a portion of US 92 (SR 600) in Hillsborough County. The project study limits are from east of I-4 to east of County Line Road, a distance of approximately 18 miles.

US 92 is an important east-west roadway that spans central Florida. In Hillsborough County, US 92 connects to several regionally significant corridors including I-4, County Line Road, and CR 579. US 92 is also a hurricane evacuation route and a designated truck route. On April 20, 2018, the Florida Department of Transportation (FDOT) approved the US 92 (SR 600) PD&E Study Design Change Reevaluation.

1.2 PROJECT DESCRIPTION

The project corridor (**Figure 1-1**) is expected to be improved from an existing, two-lane undivided facility to a four-lane divided facility within the entire study limits, with the exception of the section from Mobley Street through the downtown Plant City area to east of Park Road where it is currently four-lane divided. The proposed improvements will include various intersection improvements, in addition to pedestrian and bicycle accommodations. Improvement alternatives were developed to improve safety, consider cost and capacity needs, and meet future transportation demand. A feasibility analysis of providing grade separation/interchanges at Park Road and County Line Road was also conducted.



Figure 1-1 Study Area Map

1.3 EXISTING FACILITY AND PLANNED IMPROVEMENTS

The existing typical sections throughout the study area vary. The existing typical section for US 92 from Garden Lane to Thonotosassa Road is generally a two-lane undivided rural roadway with 12-foot-wide travel lanes and paved outside shoulders. The paved shoulders are typically five feet wide. Stormwater is collected in swales along the outside of the roadway. Sidewalks or boardwalks have been added along one or both sides of the roadway. No designated bicycle facilities are provided. The existing roadway typical section for US 92 from Garden Lane to Thonotosassa Road is shown in **Figure 1-2**.

From Thonotosassa Road to Mobley Street, the roadway is in transition and consists of two 12-foot-wide eastbound lanes and two 12-foot-wide westbound lanes, of which the outside westbound lane transitions to a right turn at Thonotosassa Road. The existing roadway typical section for US 92 from Thonotosassa Road Mobley Street is shown in **Figure 1-3**.

From North Gordon Street to Park Road, the US 92 existing typical section is a four-lane divided urban roadway with an 18-foot-wide raised grassed median and concrete curb and gutter on both the inside and outside of the roadway. There is a five-foot sidewalk along the north side of the road. The existing roadway typical section for US 92 from North Gordon Street to Park Road is shown in **Figure 1-4**.

From east of Park Road to east of County Line Road, US 92 is a rural facility with two 12-foot-wide lanes and grass shoulders and drainage ditches on both sides. The existing roadway typical section for US 92 east of Park Road to County Line Road is shown in **Figure 1-5**.

Recommended improvements include widening the existing highway to four lanes (except through downtown Plant City from Mobley Street to Maryland Avenue) as well as adding paved shoulders, and improving sidewalk connectivity. Additional right of way will be required throughout the corridor for the recommended improvements. Recommended typical sections are shown in **Figures 1-6, 1-7, 1-8, 1-9** and **1-10**. As with the original PD&E study, Downtown Plant City, from Mobley Street to Maryland Avenue, will be considered a “No-Build” segment, with no improvements proposed. There are only two segments currently scheduled for funding, in FDOT’s current adopted 5-year work program (Fiscal Years 16/17 through 20/21).

Figure 1-2 Existing Typical Section 2-Lane Rural Garden Lane to Thonotosassa Road

Figure 1-3 Existing Typical Section 4-Lane Rural Thonotosassa Road to Mobley Street

Figure 1-4 Existing Typical Section 4-Lane Urban US 92 from North Gordon Street to Park Road

**Figure 1-5 Existing Typical Section 2-Lane Rural US 92 East of Park Road to East of
County Line Road**

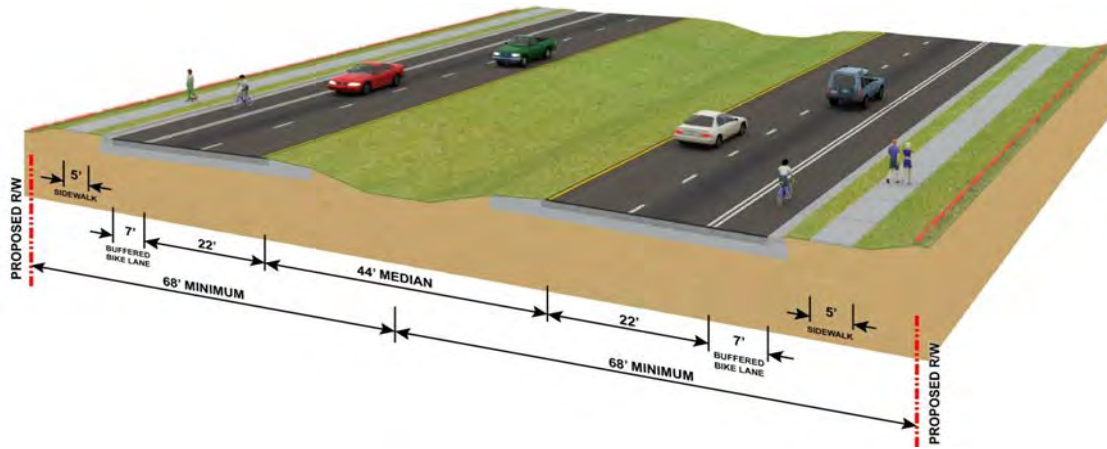


Figure 1-6 Typical Section 1

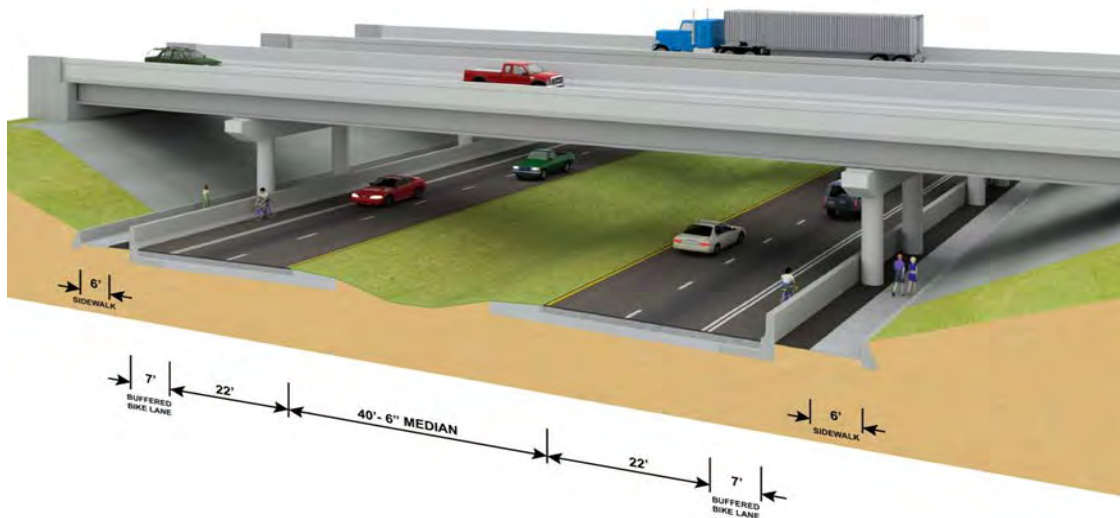


Figure 1-7 Typical Section 2

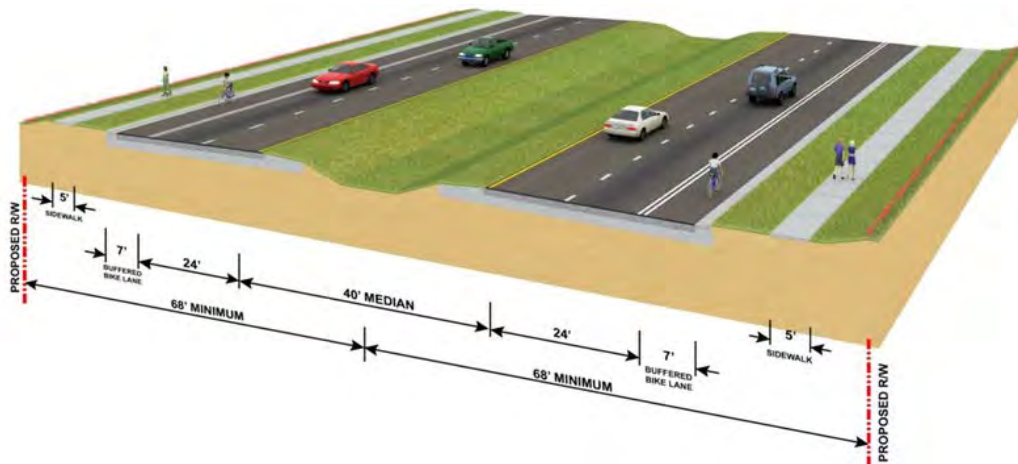


Figure 1-8 Typical Section 3

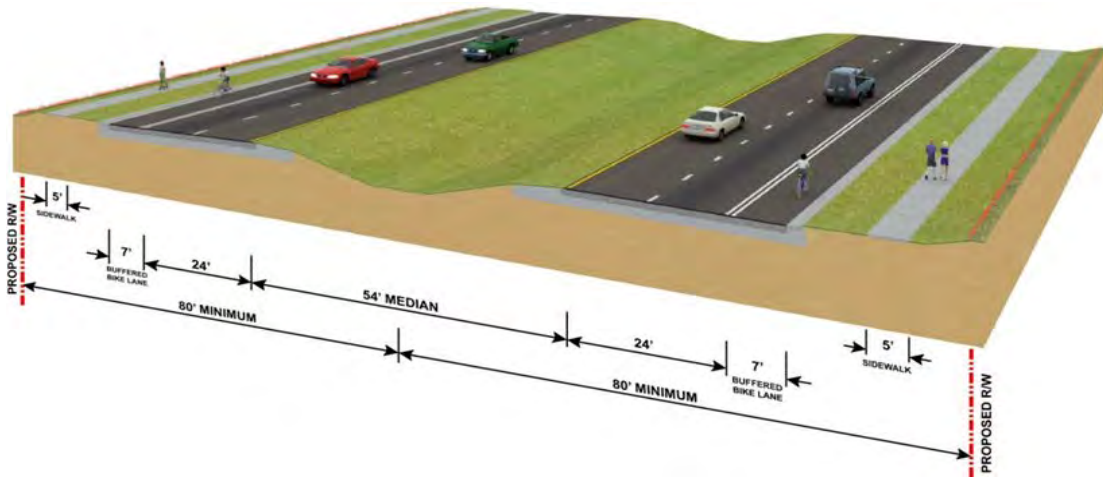


Figure 1-9 Typical Section 4

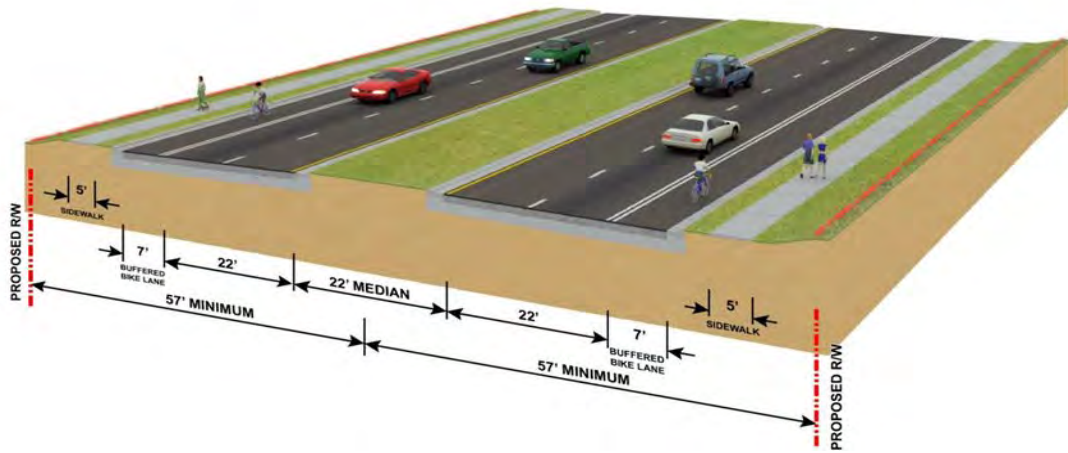


Figure 1-10 Typical Section 5

1.4 PROJECT PURPOSE AND NEED

US 92 within the study area plays a significant role in connecting southern Hillsborough County to the Tampa Bay region. The purpose of the proposed project is to accommodate future traffic demands on US 92 due to growth within the project limits and surrounding areas. This corridor is projected to operate at level of service (LOS) F in the design year (2040) if no increase in capacity is provided. Additional factors which support the need for the project include:

Regional Connectivity - US 92 is a major east-west regional arterial that parallels I-4 and SR 574 (E Dr. Martin Luther King Jr Blvd) and connects eastern Hillsborough County to the Tampa Bay region. It provides connectivity between the communities of Lakeland, Plant City, Valrico, and Seffner.

Safety - With the additional capacity provided in the corridor by the proposed widening to US 92, roadway congestion will be reduced, which will decrease potential conflicts with other vehicles and potentially increase safety. In addition to the proposed widening of US 92, the addition of turn lanes at intersections is expected to improve safety along the corridor. An analysis of traffic crash data for years 2009 thru 2013 revealed that the overall average crash rate within the study limits was higher than the statewide average crash rate for similar type facilities.

Plan Consistency - This project is consistent with local planning. The segments of US 92 from US 301 to County Road 579 and from Park Road to County Line Road have been identified as cost feasible projects in the *Imagine 2040: Hillsborough Long Range Transportation Plan (LRTP)*. These segments have also been included in the State Transportation Improvement Program and the Hillsborough County Metropolitan Planning Organization's Transportation Improvement Program for design.

Emergency Evacuation - US 92 is listed as an evacuation route by the Hillsborough County Emergency Management and shown on the Florida Division of Emergency Management's evacuation route network. US 92 provides access to I-4 via interchanges with north-south connections on CR 579 (Mango Rd), McIntosh Rd and Branch Forbes Rd, Thonotosassa Rd, Park Rd, and County Line Road all in close proximity to the study limits.

Current and Future Transportation Demand - The *Hillsborough County Metropolitan Planning Organization's (MPO) 2040 LRTP Socioeconomic Projections* estimate an employment increase of 56% and a population increase of 48% for Hillsborough County between 2010 and 2040. Based on the growth projected to occur within the corridor, US 92 is projected by the *Tampa Bay Regional Planning Model (TBRPM Version 8.0) – Cost Feasible Network* to have future traffic volumes ranging from approximately 13,800 vehicles to 40,950 vehicles per day (VPD) within the project limits by year 2040, which would yield a Level of Service (LOS) F for the corridor with the current roadway configuration except for the four lane section from Mobley Street through the downtown Plant City area to east of Park Road which will remain acceptable LOS. These volumes would exceed roadway capacity at the adopted standards of LOS for US 92 within the project limits per FDOT; therefore, widening of US 92 needs to be evaluated in order to meet future transportation demand.

1.5 REPORT PURPOSE

This *Final Comments & Coordination Report* is one of several documents prepared as part of this PD&E study reevaluation. This report documents the Public Involvement Plan (PIP), agency coordination efforts, public involvement activities, and comments received during the study reevaluation.

SECTION 2 PUBLIC INVOLVEMENT PLAN

In accordance with Part 1, Chapter 11 of the FDOT PD&E Manual, a comprehensive *Public Involvement Plan (PIP)*, was originally approved in June 2015, with an update prepared in January 2017, which documented the public involvement program. The purpose of this plan was to develop, implement, and document the methods that were to be used to inform and solicit responses from all interested parties including local residents, public officials, agencies and business owners. The PIP helped to identified stakeholders and affected communities and included the following:

- Project background;
- Project goals;
- Outreach activities; and,
- Evaluation of public involvement for the project.

The public involvement program included various techniques to notify the public of the proposed transportation improvements such as legal display newspaper advertisements, news releases to local media and invitational newsletters. The program included three newsletters; the kick-off newsletter, the public hearing newsletter, and a final newsletter published after final project documents are approved by the District. See **Section 5** for more information regarding the project newsletters.

The PIP served as a guidance document for planned public involvement activities. These activities included coordination meetings with local officials, a public hearing, presentations to agencies and business groups, unscheduled meetings, and coordination with adjacent projects.

SECTION 3 COORDINATION EFFORTS

The FDOT coordinated with numerous local, state, and federal agencies throughout the study process. This section summarizes the results of these coordination efforts.

3.1 AGENCY COORDINATION

Throughout the course of the study, coordination was conducted with various federal, state and regional agencies whose agreement is required for this project. The following is a list of the federal, state and regional agencies the FDOT coordinated with:

- U.S. Fish and Wildlife Service (USFWS)
- Florida Fish and Wildlife Conservation Commission (FWC)
- Florida Department of State, Division of Historical Resources (SHPO)

3.1.1 U.S. Fish and Wildlife Service

A copy of the Draft *WEBAR* (now known as a Natural Resources Evaluation (NRE)) was sent to U.S. Fish and Wildlife Service (USFWS) for review and concurrence on December 8, 2016. USFWS found that the proposed improvements were not likely to adversely affect resources protected by the Endangered Species Act of 1973, as amended (16 U.S.C. 1531 et seq.) provided that the standard protection measures for the eastern indigo snake are incorporated into the project plan.

3.1.2 Florida Fish and Wildlife Conservation Commission

A copy of the Draft *WEBAR* was sent to Florida Fish and Wildlife Conservation Commission (FWC) for review and concurrence on July 29, 2015. On December 22, 2016, FWC responded in concurrence with the project biologist's findings and recommendations.

3.1.4 Florida Department of State, Division of Historical Resources (SHPO)

A Cultural Resource Assessment Survey (CRAS) Report was submitted to the Florida Department of State, Division of Historical Resources, and the State's Historic Preservation Officer (SHPO) on September 7, 2016. Since three resources were identified which are considered eligible for listing in the *National Register of Historic Places (NRHP)*, further coordination with the division will be required during future project development phases. SHRO has been given the Draft CSR for their review and concurrence. A copy of the letter from the SHPO and their CSR concurrence are included in **Appendix A**.

3.2 LOCAL GOVERNMENT COORDINATION

Notes or "minutes" from the following meetings are included in **Appendix A**, including copies of slide presentations.

3.2.1 Hillsborough County Metropolitan Planning Organization (MPO)

The project was presented to the MPO's Citizen's Advisory Committee (CAC) on March 21, 2016 to serve as an update on the PD&E study reevaluation. Members were shown a PowerPoint presentation. General project support was conveyed, though no formal motions were discussed.

3.2.2 Hillsborough County

The project was discussed with county staff on July 6, 2016, to review the recommended improvements.

3.2.3 City of Plant City

The project was discussed with City of Plant City engineering staff on July 6, 2016, to review the proposed improvements and the project's impact to Plant City.

3.3 OTHER LOCAL COORDINATION

Throughout the course of the study, coordination was conducted with various local or community groups which would have an interest in this project. The following is a list of local nongovernmental organizations or community groups with which the FDOT coordinated. Notes or "minutes" from these meetings are included in **Appendix A**.

3.3.1 Plant City Strawberry Festival

The project was discussed with Plant City Strawberry Festival staff on August 17, 2016, to review the proposed improvements and the project's impacts to festival traffic patterns.

3.3.2 Plant City Economic Development Corporation

The project was discussed with Plant City Economic Development Corporation staff on October 17, 2016, to review the proposed improvements and the project's impacts to Plant City.

3.3.3 Seffner Chamber of Commerce

The project was discussed with Seffner Chamber of Commerce members at their meeting on April 14, 2016, to review the proposed improvements.

SECTION 4 MAILING LIST

A mailing list was developed for this project. The mailing list was updated throughout the duration of the PD&E study and contained:

- Those property owners whose property lies, in whole or in part, within 300 feet on either side of the centerline of the project alternative as required by *Florida Statutes Section 339.155*. The mailing list was based on information obtained from the property appraiser's database in Hillsborough County. A GIS map showing these parcels is included in **Figures 4-1 to Figure 4-4**.
- Elected and appointed public officials.
- Individuals or groups who requested to be placed on the study's mailing list.
- Public and private groups, organizations, agencies, and businesses and individuals that have an interest in the project.

The property owner mailing list included 3,685 owners. The officials, agency, and interested parties mailing list contained approximately 93 people.

The mailing list was used to disseminate project information and announce the public hearing. Newsletters (**Section 5**) were mailed to all those on the mailing list.

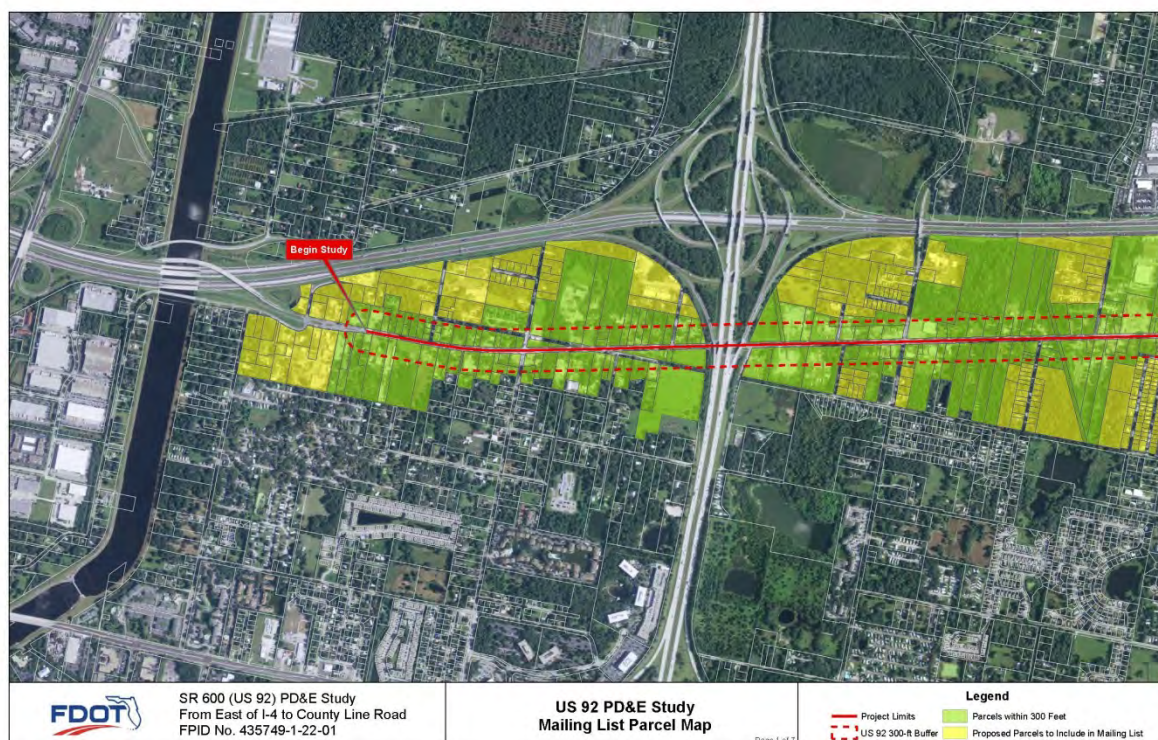


Figure 4-1 GIS Parcel Map of Mailing List

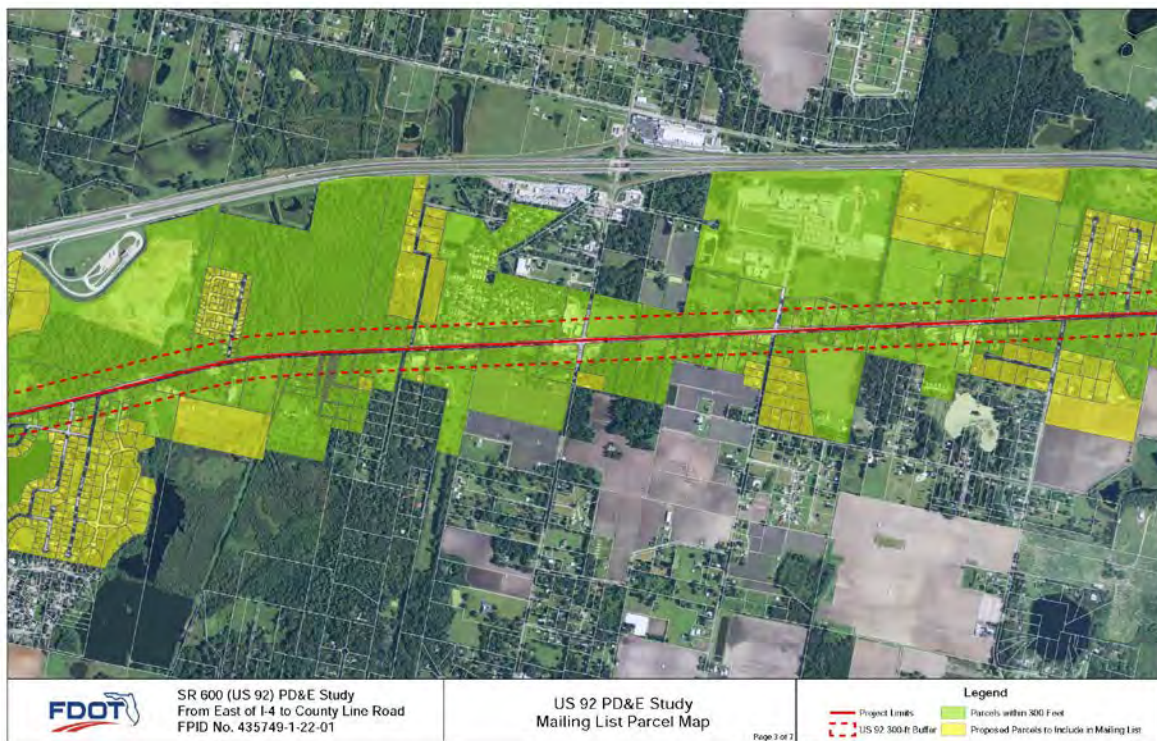
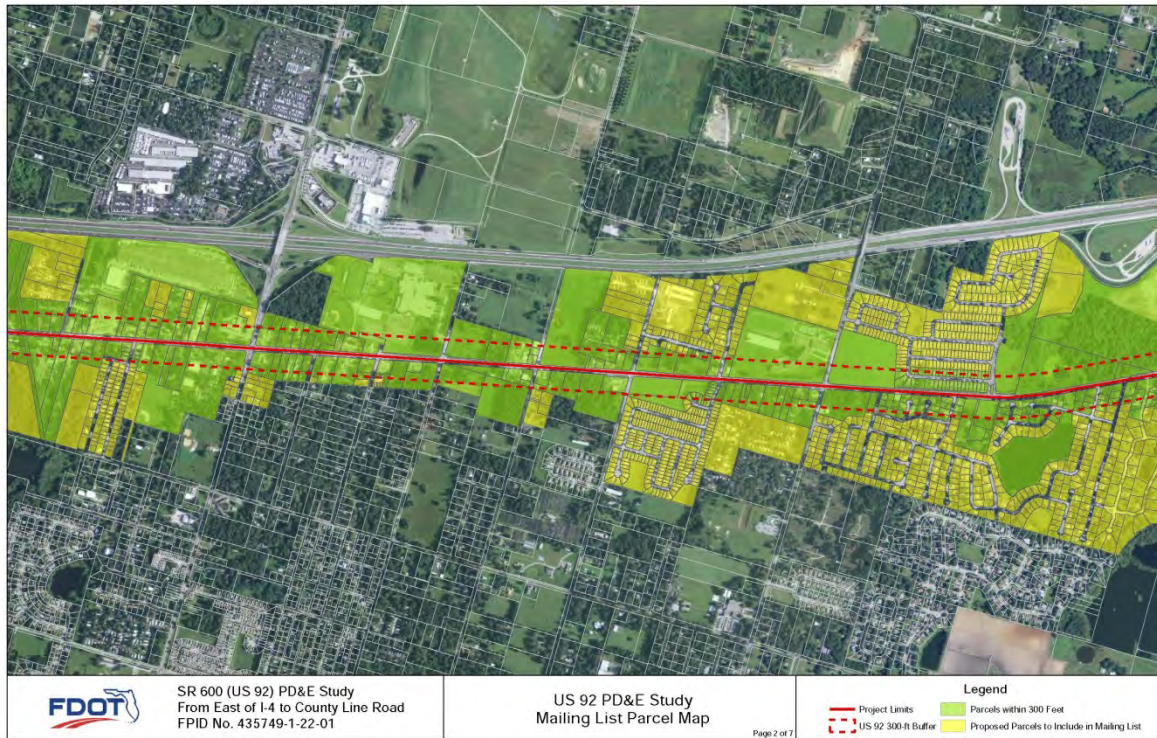


Figure 4-2 GIS Parcel Map of Mailing List

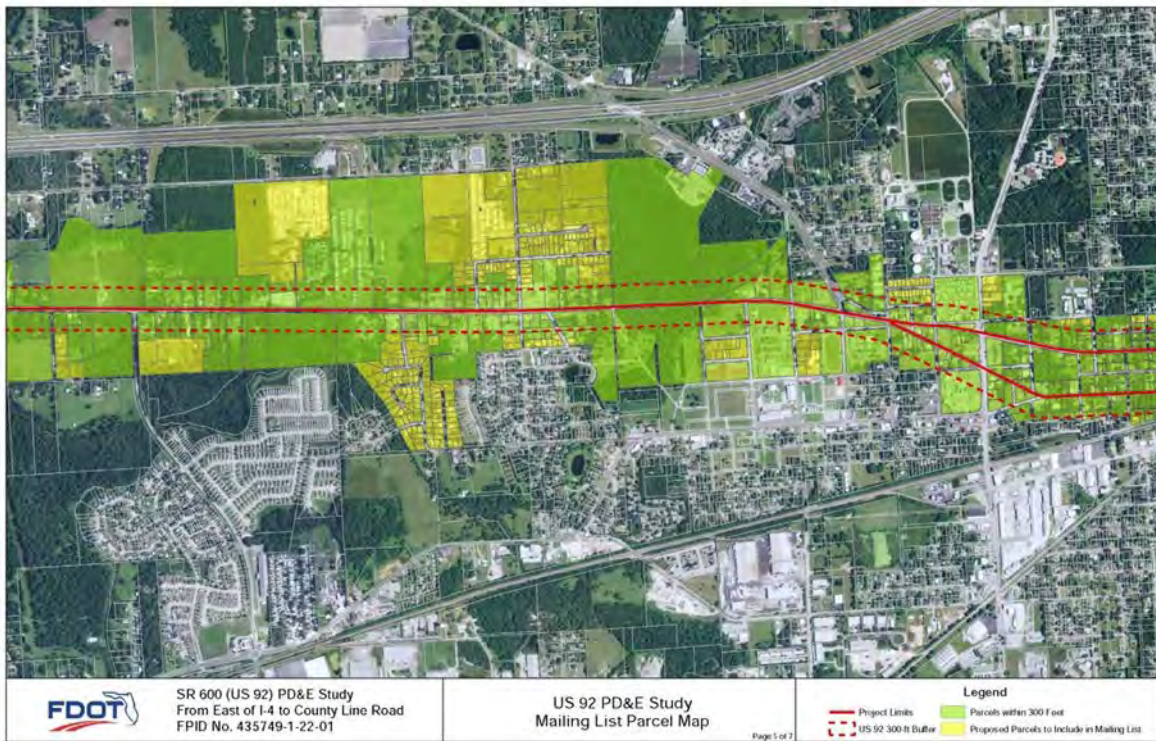
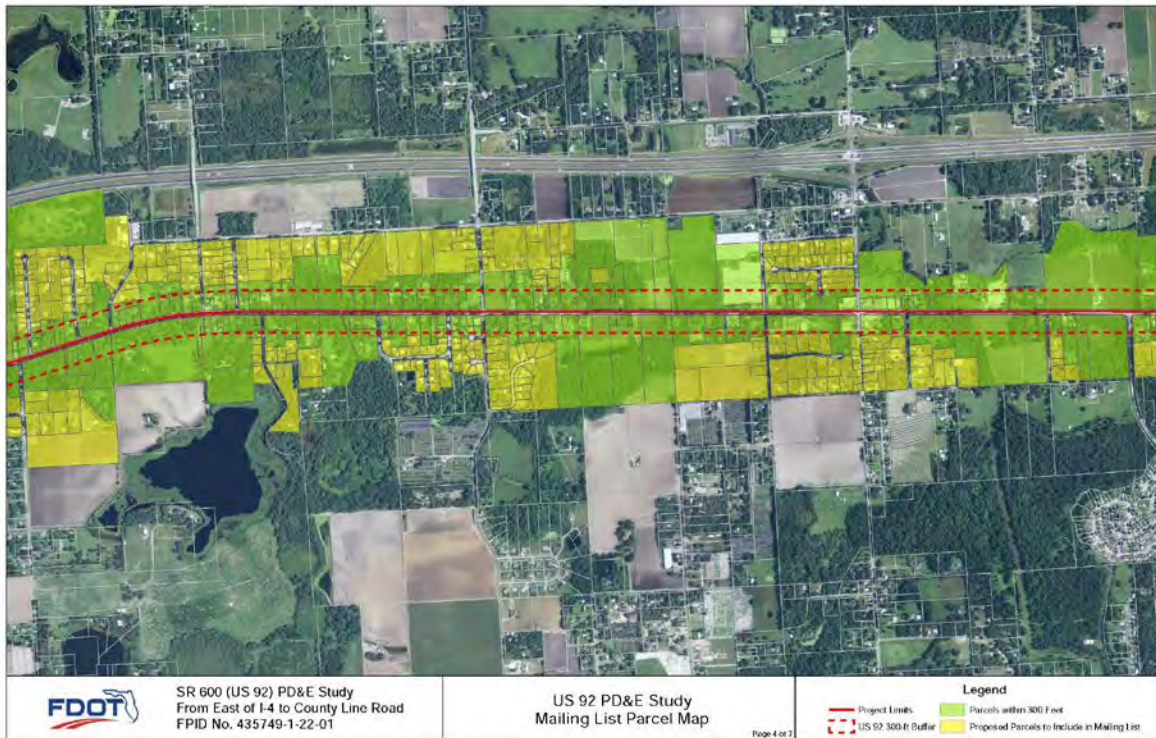


Figure 4-3 GIS Parcel Map of Mailing List

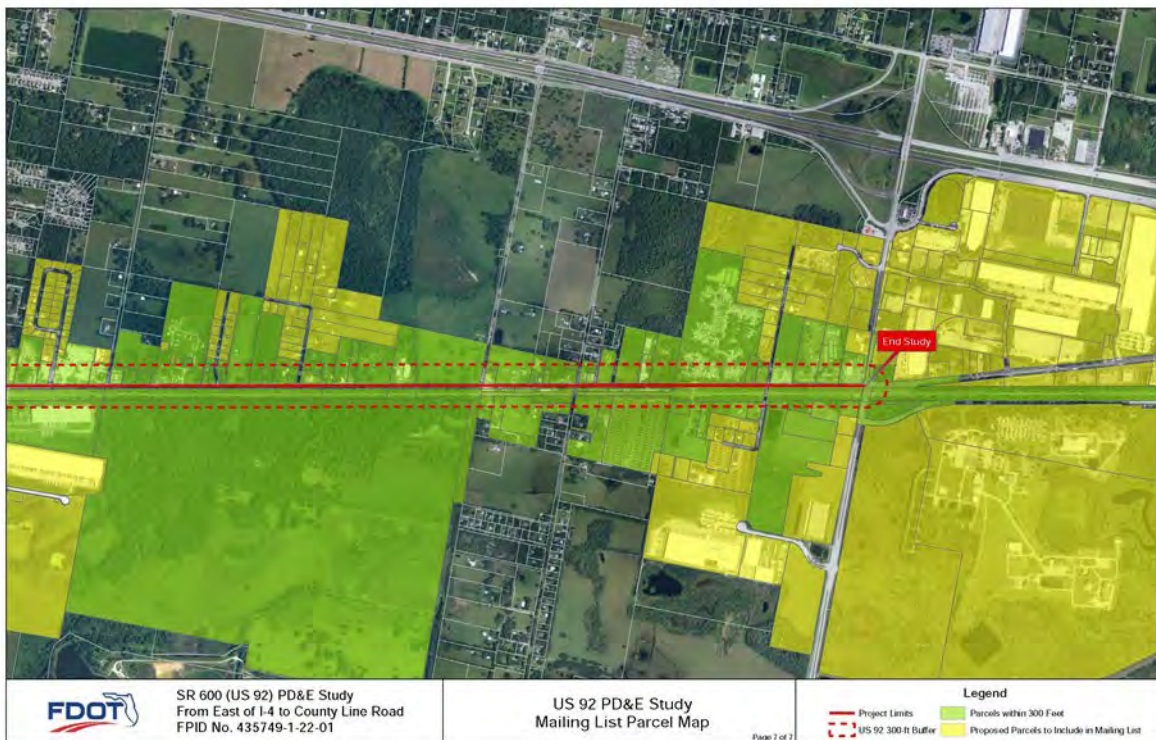
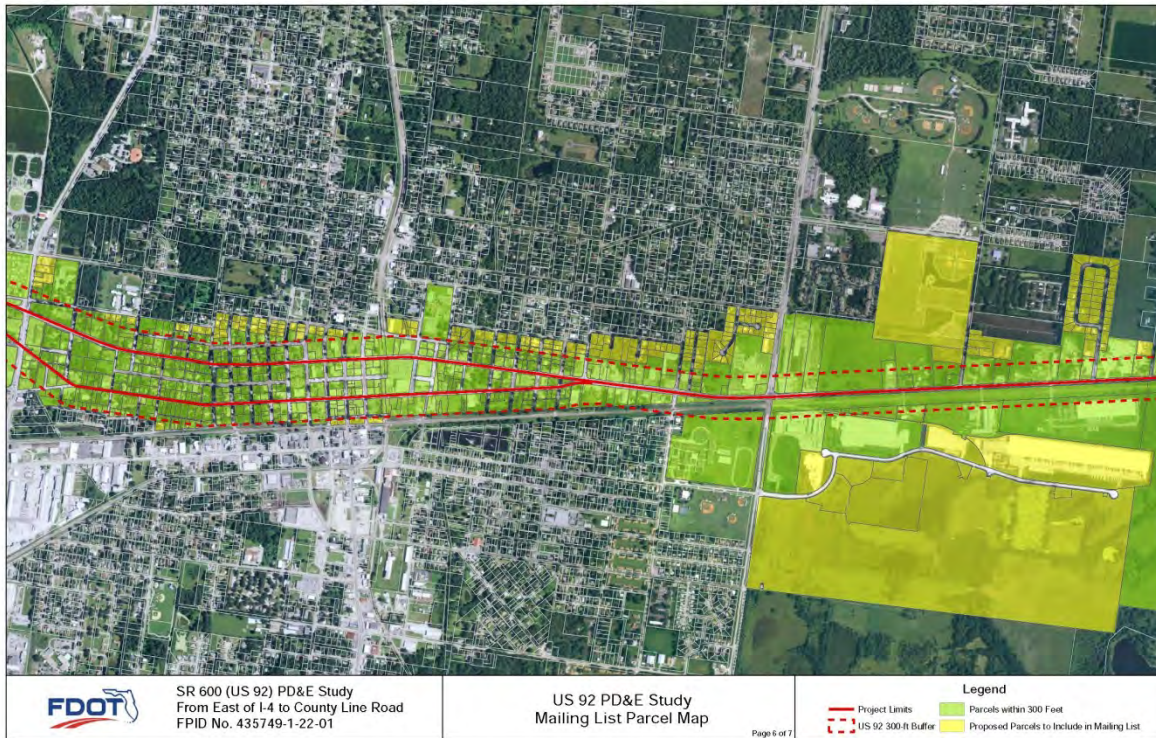


Figure 4-4 GIS Parcel Map of Mailing List

SECTION 5 NEWSLETTERS

Newsletters were mailed to those on the project mailing list as noted in **Section 4**. Newsletters were used to announce the project kick-off, the public hearing, and approval of the reevaluation by the District. Copies of the newsletters are provided in **Appendix C**.

A study kick-off newsletter was distributed in July 2015. The newsletter described the PD&E study reevaluation process, discussed the project purpose, and provided a project schedule with the next steps in the study. The newsletter also included contact information and instructions for those needing special assistance or language support.

A public hearing newsletter was distributed in November 2016 to publicize the public hearing and to encourage participation and comments. The newsletter presented the recommended build alternative and corresponding typical sections. Contact information and instructions for those needing special assistance or language support were also provided.

A newsletter will be published and distributed to the public to announce approval of the project documents and to update the public on changes made to the proposed design concepts subsequent to the public hearing.

SECTION 6 WEBSITE

Public participation is an integral part of the transportation process, which helps to ensure that decisions are made in consideration of public needs and preferences. In an effort to engage and inform the public throughout the study process, a project website was developed (**Figure 6-1**).

The project website (at <http://active.fdot7studies.com/sr600/garden-lane-to-county-line>) was used as an educational tool for the general public; explaining what a PD&E study reevaluation evaluates and why, listing contact information for comments and questions, and providing links to other sites and projects. It was established in April 2015 and updated several times during the study reevaluation.

In addition, the website was used as an information sharing tool. Site visitors could read about project details, review past and current newsletters, follow the project schedule, and peruse available project documents, information sheets, and FAQs. The site was also one of several methods used to notify the public about the public hearing.

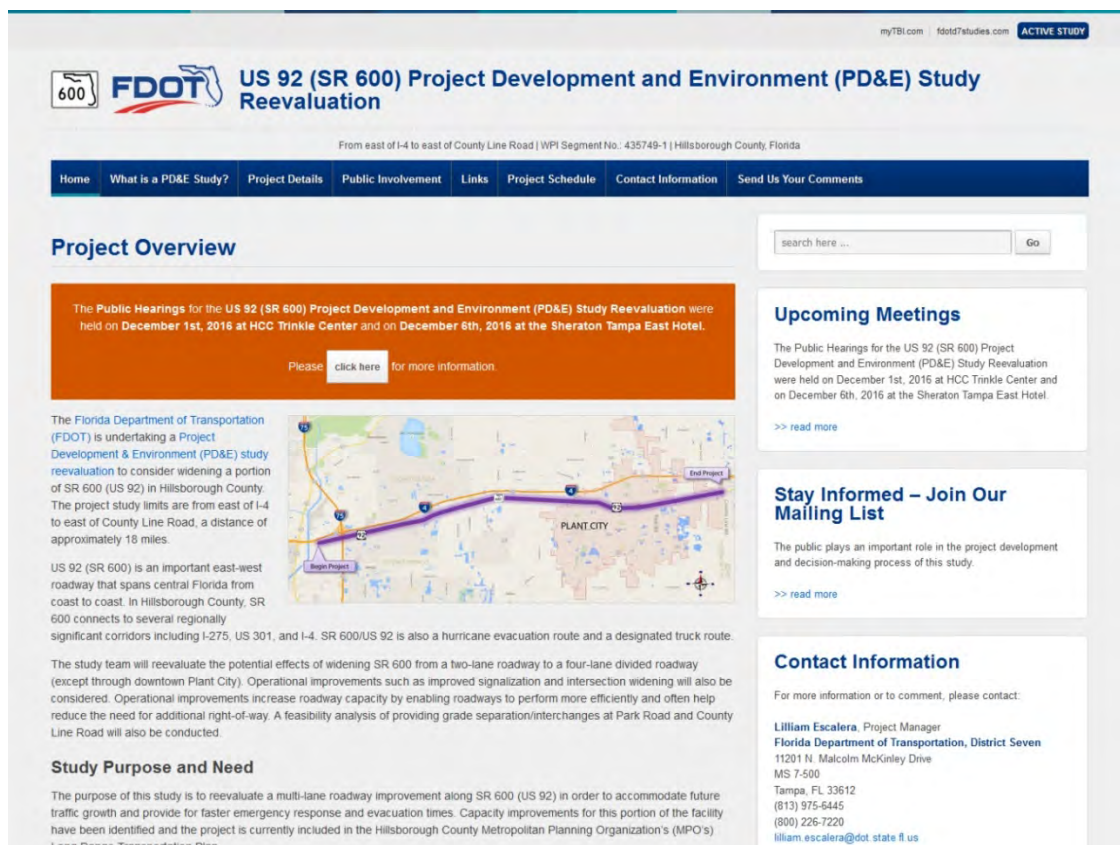


Figure 6-1 US 92 PD&E Reevaluation Study Website Screenshot

SECTION 7 PUBLIC HEARING

A public hearing consisting of an informal open house integrated with a formal portion was held for this project in two sessions, on December 1, 2016 and December 6, 2016. The first session was held at the HCC Trinkle Center from 5:30 p.m. to 7:30 p.m. and the second session at Sheraton Tampa East Hotel from 6:00 p.m. to 8:00 p.m.

The hearing was held to inform citizens and interested parties about the project details and schedule, and allow them the opportunity to provide comments concerning the proposed improvements. The hearing consisted of an open house for the first hour and a formal portion immediately following. After the formal portion, the open house resumed until the end of the session.

The reevaluation’s supporting documents were available for public review from November 8, 2016 through December 19, 2016 on the project website as well as during normal operating hours at the locations shown in **Table 7-1**.

Table 7-1 Locations the Study Documents were Available for Public Review

Location	FDOT District 7	Seffner-Mango Branch Library	Bruton Memorial Library
Address	11201 N. McKinley Dr. Tampa, FL 33612	410 N. Kingsway Road Seffner, Florida 33584	302 W. McLendon Street N. Plant City, Florida 33563
Hours	Mon-Fri 8 a.m-5 p.m.	Mon - 10 a.m.-8 p.m. Tues - 12 p.m.-8 p.m. Wed-Sat - 10 a.m.-6 p.m.	Mon-Thur - 10 a.m.-9 p.m. Fri - 10 a.m.-6 p.m. Sat - 10 a.m.-5 p.m. Sun – 1 p.m.-5 p.m.

A project newsletter was used to announce the public hearing (**Section 5**) and was sent via electronic mail to public officials and agencies, and via direct mail to property owners and interested parties. A legal display notice advertising the public hearing sessions was published in the Tampa Bay Times and La Gaceta on the following dates:

- Tampa Bay Times – November 11, 2016 and November 21, 2016
- La Gaceta – November 11, 2016 and November 18, 2016

A notice was also published in the Florida Administrative Register on November 28, 2016. Copies of these advertisements are shown in the *Public Hearing Scrapbook*.

FDOT staff and its consultant were available at the hearing to discuss the project and answer questions. A continuously-running PowerPoint presentation describing the project and the

recommended build alternative was shown during the open house portion of the hearing. Display boards were also available for review and consisted of:

- Existing and Future Traffic Volumes
- Existing and Proposed Roadway Typical Sections
- Aerial of the Project Limits
- Evaluation Matrix
- Project Schedule and Funding
- Welcome and List of Citations

The materials shown at the public hearing were also posted to the project website following the hearing.

The formal portion of the first public hearing session began at 6:30 p.m., with the second session formal portion beginning at 7:00 p.m., and was moderated by Kirk Bogen, PE, District Seven Environmental Management Engineer. The proceedings were recorded by the court reporter who was present throughout the evening. Mr. Bogen welcomed the audience and discussed the purpose of the hearing. The next portion of the hearing was devoted to verbal comments.

Attendees were given the opportunity to provide comments in one of five ways:

- Make a verbal statement during the formal portion of the hearing;
- Make a verbal statement to the court reporter during the informal portion of the hearing;
- Complete a written comment form and place it in the drop box at the hearing;
- Make a comment on the project website; or,
- Mail comments to the Department by December 19, 2016.

A total of 239 people signed in at the public hearing. Forty One (41) written comments were received at the hearing sessions and between sessions combined along with 11 verbal statements made during the formal public comment period.

The public hearing transcript is included in **Appendix C**. Copies of the public hearing materials, including the legal display advertisement, the sign-in sheets, display graphics, PowerPoint slides, and attendance rosters are included in the *Public Hearing Scrapbook* prepared for this PD&E study reevaluation.

SECTION 8 SUMMARY OF PUBLIC HEARING COMMENTS

The public hearing comment period was advertised to end on December 19, 2016. A total of 89 comment forms were received. Thirty six (38) written comment forms and 12 verbal comments were received from the public hearing. Twenty (20) comment forms were received before the public hearing and 19 comment forms were received after. Of the 89 total comment forms, 33 involved requests to be added to the project contact list, of the 56 project comments, 10 pertained to access management questions. **Table 8-1** summarizes those public comments received that pertain to this project.

Appendix D contains copies of the written comments and responses. Because some individuals submitted several comments in different formats, the total number of comments received does not equal the total number of individuals in favor of or against the project.

Table 8-1 Summary of US 92 Public Hearing Comments

No.	Summary of Comments	Support Build Alternative?	Name	Date
Pre-Hearing Comments				
1	Add to mailing list	N/A	Donna Cline	11/23/2015
2	Are the improvements for U.S. Hwy 92 still going to happen? Will you be using the 1994 plans or have new plans been drawn up? Please keep me posted on all new developments.		Gregory Jewell	2/28/2016
3	Would like to discuss speaking at Seffner Chamber	Yes	Lori Libhart	3/1/2016
4	Request a Q & A 1. How this project will partner with TBX, Express Bus Service and Rail? 2. Is there any consideration for autonomous vehicles. 3. How will this project reduce sprawl and encourage high density living areas	N/A	Mauricio Ross	4/24/2016
5	My husband and I own Parkesdale Market located at 3702 W Baker St. We would like to see plans for the proposed widening of US 92 as it will impact our family business.	N/A	Xiomara Meeks	5/6/2016
6	Add to mailing list	N/A	Phil Waldron	6/14/2016
7	Add to mailing list	N/A	Nicole Cribbs	6/15/2016
8	Add to mailing list	N/A	David Holloway	6/16/2016
9	Add to mailing list	N/A	Kaley Wallace	6/27/2016
10	Add to mailing list	N/A	Tim O'Brien	8/22/2016
11	Hi Lilliam, My name is Ali Shasti and I am the owner of the property located at the above referenced location in Hillsborough County. Could you please keep me abreast of any proposed activities happening in front and around my property? E-notification, Etc. Thank you much, Ali	N/A	Ali Shasti	8/28/2016

No.	Summary of Comments	Support Build Alternative?	Name	Date
12	I own Jarrett-Scott Ford at 2000 E. Baker (SR92). I would like to attend a workshop or have a meeting to learn more about what this project means to my business.	Yes	Jim Scott	10/25/2016
13	Add to mailing list	N/A	Pat Comstock	11/18/2016
14	Add to mailing list	N/A	Valerie Jackson	11/21/2016
15	Add to mailing list	N/A	Jane Charpentier	11/22/2016
16	Add to mailing list	N/A	Louis Bergeron	11/25/2016
17	Would like to know more about our house 4511 Reola Rd. Is the plan to tear it down and when	N/A	Angelo Caltabiano	11/28/2016
18	Add to mailing list	N/A	Jason Fernandes	11/30/2016
19	Add to mailing list	N/A	Link Property II	11/30/2016
20	Add to mailing list	N/A	Bill Yavit	12/1/2016
Hearing Comments 1st Session (Held on December 1, 2016)				
21	* How will water run-off from proposed Hwy be addressed? It is already a problem for those of us whose homes are at a lower elevation than the existing HWY. * What is the timeline for beginning at our address? * Will we be offered a buy-out? At 13512 (Next door to Catabiano - Reola Rd) * From Lynn Oaks Circle to east of Bethlehem Rd pamphlet states build alignment will be centered that would put sidewalk in our front door if the typical section 3 80; minimum is met then 50 mph (minimum) will be flying just feet from our bedroom windows - Dangerous - we would request you buy us out.	N/A	William Brown	12/1/2016

No.	Summary of Comments	Support Build Alternative?	Name	Date
22	The area in front of my house was change with the construction of schools behind me. The electric and septic tank are within the small area marked as increasing. Can this be discussed? 813-781-2519	N/A	Sue Powers	12/1/2016
23	Would love to speak to an engineer about the drainage problem we have now since the last road improvement project.	N/A	Mark Riebow	12/1/2016
24	Segment 1 9611 E US Hwy 92, Access Management, Large Trucks - Semis, Vehicles towing boats being denied access to west bound lanes.		Jason Ryan	12/1/2016
25	We would like to receive information to our address. Had the appraiser not contacted us my husband and I would not have known of this. Please send any communications to our residence. 3970 Medici Lane Wesley Chapel, FL 33543	N/A	Passard & Karine Dean	12/1/2016
26	We are landlords that will be affected. We did not receive any communication. Please send communications to the address below.	N/A	Passard Dean	12/1/2016
27	Add to mailing list	N/A	N/A	12/1/2016
28	Add to mailing list	N/A	Ana Chico-Cruz	12/1/2016
29	Add to mailing list	N/A	Leonardo Arenas	12/1/2016
30	I am concerned about my parking lot at 711 N. Park Road in Plant City. I have a small parking lot in front of my building and a small parking lot in the rear of my building. If you take even a small piece of my front parking space, I will be in trouble parking wise. There are 4 offices in my building and one of those is a hair & nail salon, one is am an income tax business, and another is an immigration services office. I have one office that isn't occupied currently. So you can see that parking is already maxed out.	N/A	Karlene Whidden	12/1/2016

No.	Summary of Comments	Support Build Alternative?	Name	Date
31	I am a mobile home park manager. I have called DOT to try and do something about HWY 92 between Turkey Creek Rd and Whitehurst because of accidents and there is no crossover from mobile home park (n side of 92) to sidewalks. I believe Hwy 92 from Turkey Creed to Alexander are dangerous and a hazard.	Yes	Kay Vance	12/1/2016
32	I am writing you in regards to the US 92 project. As you know, our business will be severely impacted by this project. I have visited Bruton Memorial Library and I looked through the project documents. I have a major concern, and that is the ability for east bound traffic on US 92 to turn north on our property. And on the flip side, the ability for vehicles leaving our business to turn on to east bound US 92. Currently we have two separate openings in the boulevard for our business and for the parcel of land that we recently purchased directly west of us. We have a tremendous number of customers and vendors daily that currently have the ability to turn north into my business when heading east bound. We also have a tremendous number of customers and vendors (semi-truck car carriers) who use this boulevard opening to head east on US 92 and ultimately north on Park Road to travel to I-4. Looking at the new design, it appears that we would lose both boulevard openings (with the new parcel directly west of us that we purchased). We cannot go from two of these boulevard openings to zero. It would have a major negative impact on our business. Please take this comment into consideration when redesigning US 92, and please keep me informed. -Jim Scott (Jarrett Scott Ford)	Yes	Jim Scott	12/1/2016

No.	Summary of Comments	Support Build Alternative?	Name	Date
33	Suggest FDOT consider improving the intersection of US 92 and Mobley Street. The present condition is not the best, because vehicles headed west turn south onto Mobley and vehicles traveling east turn North and cause congestion. At the least, turn lanes should be built for both directions.	N/A	Joseph Herrmann	12/1/2016
34	We have lived on this road, Park Rd. and County Line Road for over 25 years. We have seen this road become very dangerous to drive on. Many pedestrians and bicycles use the side of the road in great danger. We have had numerous accidents occur in front of our home. We truly fear getting rear ended attempting to turn left into our property. In our minds this project from Park Rd to County Line Rd. needs major improvement as soon as possible. We will keep praying no one on a cell phone rear ends us as we wait to turn into our property. Thank you for your consideration to our problem. Another issue is that any blip on I-4 throws enormous amounts of traffic onto 92 - this happens regularly.	Yes	Ronald Boles	12/1/2016
35	My name is Miguel Quirino. I am the Pastor of God's Strong Tower Church in Dover, Inc. 5335 W US Hwy 92 Plant City 33566. My concern is the impact it will have on the property of the church. What will the impact be to the parking and the existing construction project on the temple.	N/A	Miguel Quirino	12/1/2016
36	Consider the accommodation of a multi-use trail through all of the corridor. This can be accommodated within the proposed cross section - a possible merging of the buffered bike-lane and sidewalk may be an option to accommodate a multi-use trail. Such a trail would be consistent with the Hillsborough MPO's Hillsborough County Greenways & Trails Plan Update.	N/A	Richard Ranck	12/1/2016

No.	Summary of Comments	Support Build Alternative?	Name	Date
Comments Between Sessions				
37	<p>I am writing you in regards to the US 92 project. As you know, our business will be severely impacted by this project. I have visited Bruton Memorial Library and I looked through the project documents. I have a major concern, and that is the ability for east bound traffic on US 92 to turn north on our property. And on the flip side, the ability for vehicles leaving our business to turn on to east bound US 92. Currently we have two separate openings in the boulevard for our business and for the parcel of land that we recently purchased directly west of us. We have a tremendous number of customers and vendors daily that currently have the ability to turn north into my business when heading east bound. We also have a tremendous number of customers and vendors (semi-truck car carriers) who use this boulevard opening to head east on US 92 and ultimately north on Park Road to travel to I-4. Looking at the new design, it appears that we would lose both boulevard openings (with the new parcel directly west of us that we purchased). We cannot go from two of these boulevard openings to zero. It would have a major negative impact on our business. Please take this comment into consideration when redesigning US 92, and please keep me informed. -Jim Scott (Jarrett Scott Ford)</p>	Yes	Jim Scott	12/5/2016
38	Yes we agree with this project we like selection #1. 92 gets backed up when I-4 is backed up	Yes	Joe and Debbie Wos	12/5/2016
39	We have to do something about US 92. Back in September I got rear ended while stopped for a truck. We need more lanes before someone gets killed	Yes	Hollis Lynn	12/5/2016

No.	Summary of Comments	Support Build Alternative?	Name	Date
40	US 92 is dangerous, I like to ride my bike and US 92 is the shortest route I can take. I do not feel safe on the road. Get us some good bike lanes. Any of your proposals will work for me.	Yes	Michael Wos	12/5/2016
Hearing Comments 2nd Session (Held on December 6, 2016)				
41	Change contact address	N/A	Timothy & Pamela Conway	12/6/2016
42	Hwy 92 between Falkenburg and Williams is so dangerous, I've been hit by a car while riding my bike 2 times in the last 6 months, we need a bike lane.	N/A	John Simmon	12/6/2016
43	When you are taking land belonging to an entire community, is there a point of contact to address the community and their concerns other than this meeting?	N/A	Charlene Adewenmi	12/6/2016
44	Fix 92 traffic is terrible	Yes	N/A	12/6/2016
45	We want a left turn out of Anna Drive	Yes	Clay Schafer	12/6/2016
46	We need a left turn onto Hwy 92 off of Anna Drive. Boat and trailers, Semi-Trucks and RV's are exiting Anna Drive due to the nature of the existing businesses. U-Turns 1/2 a mile away will negatively effect our business.	N/A	Jon Reinke	12/6/2016
47	To whom it may concern, Being of sound mind get us some help with this traffic, took me half hour to get to this meeting from 92 and Williams. Why didn't you hold the meeting on 92?	N/A	N/A	12/6/2016
48	Add to mailing list	N/A	Gerald & Carli Harrison	12/6/2016

No.	Summary of Comments	Support Build Alternative?	Name	Date
49	My name is Miguel Quirino. I am the Pastor of the Iglesia de Dios Torre Fuente, which is located at 5335 W. US Hwy 92 in Plant City. I just want to relate the concern of the impact that this project will have, not only to myself, but to nearly 100 families that attend our church. We are also in the process of rebuilding for our future sanctuary. This project will definitely effect the construction of our future sanctuary.	N/A	Miguel Quirino	12/6/2016
50	Please have the noise wall come all the way past all the homes on US 92. Makes better sense.	Yes	Patricia Williams	12/6/2016
51	I don't want to have to turn west to go east. Do to the fact that there will be a grass divide in front of my property.	N/A	Greg Moore	12/6/2016
52	A roadway of this magnitude will negatively impact the local businesses in the area. It is designated as a high speed roadway, in a suburban business and residential area. A roadway similar to the parallel road at MLK would be more convince to the area and people living and shopping in the community. I vigorously object to this plan as currently proposed.	No	Salvatore Socci	12/6/2016
53	Add to mailing list	N/A	Marion Smith	12/6/2016
54	Add to mailing list	N/A	Cornelio Vazquez-Hernandez	12/6/2016
55	I do not feel that this project will be an improvement to the area. There are no traffic issues in front of my neighborhood (Hammocks at Kingsway). This construction project is going to force my family out of our home as my backyard backs up to Hwy 92.	No	Ashley Johnson	12/6/2016
56	Add to mailing list	N/A	Leonard Arenas	12/6/2016
57	I will not be able to make a left turn from Passadena Dr to US 92. Not acceptable.	No	Doris Cooper	12/6/2016

No.	Summary of Comments	Support Build Alternative?	Name	Date
58	Add to mailing list	N/A	Christine Burbage Trust	12/6/2016
Post-Hearing Comments				
59	Add to mailing list	N/A	Patricia Crabb	12/7/2016
60	<p>I am opposed to the widening of Highway 92 as you have outlined it. You are turning a country into a city, and it is wrong and deceitful to the people who have bought out this way to get away from all of that city mess that you call Tampa etc. You have decided to take a whole bunch of people's homes, or part of their property, and mess up their lives forever to plan to have just more people in this area than we need. We are already overcrowded and it is a mess to get around even Plant City, not to mention trying to go further than that. Just widening roads does not relieve the problem, it just makes it worse. You are already widening 574, and it looks like you are getting ready to make a mess of highway 60. It just brings more and more people out this way. We can't even enjoy our lives because there is always something you are tearing up or messing with. Build it and they will come has been your go to word for ages. It has ruined our lives long ago, and you are fixing to ruin more people's lives. No, No, No, to your plans of widening highway 92.</p>	No	Corlene Findley	12/8/2016

No.	Summary of Comments	Support Build Alternative?	Name	Date
61	<p>Our driveway was fine for 30+ years until it was resurfaced several years ago. Now every time we get a hard rain we have erosion issues - it washes gullies down our portion of the driveway even though they paved the apron up to our property line. Can this be fixed now and will it be fixed correctly when the new road will be completed. How high will the road be compared to the height that it is now. Will the driveway be angled/sloped so that we can get our travel trailer in and out without major problems. How is this going to affect the traffic Independence Academy at Hwy 92 & McIntosh Roads. Where will the retention ponds be located?</p>	N/A	George Forte	12/8/2016
62	<p>Thank you for the informative meeting, Dec 1. I declined to speak publicly but I was grateful to Mr. Ed McKinney who patiently answered my questions and spoke with compassion as I somewhat emotionally unloaded on him about the future of US 92. I have lived in Florida for 24 years in the same location, 4712 Frtizke Rd. We chose a rural location even though my husband's job was in east Tampa. Knowing that living in a rural area meant "inconvenience" at times and a slower pace of life (including getting behind trucks going 30 mph) we chose this location and we have been very happy. I am concerned that the rural lifestyle we have enjoyed with our children and with our grandchildren is being threatened. I understand about I-4 overflow traffic and other issues. However, building and widening our roads only encourages more traffic, more building, etc. I am grateful for the roads we have, but do we have to make quiet Dover into a place of noise and speed? I would be at a great loss.</p>	No	Deborah Lewis	12/8/2016

No.	Summary of Comments	Support Build Alternative?	Name	Date
63	Its about time! US 92 gets backed up all the time. This is good for my business as cars are crawling past my place and my inventory gets better exposure. But not good for traffic flow. The wooden sidewalks are a joke	Yes	A Car Lot	12/13/2016
64	Fix I-4 before you do anything on 92. I never use 92 to many trucks. Highway 60 has its own exit lane I-4 doesn't. Traffic always backed up.	N/A	Mary Darner	12/13/2016
65	I vote yes for #3 as long as I have a bicycle lane. I am afraid to ride my bicycle on 92. I never use the wooden sidewalks very uneven I go into the street and that scares me. We need more street lights Thank you.	Yes	Tanya Brad	12/13/2016
66	Add to mailing list	N/A	Samantha Silber	12/14/2016
67	Highway 92 is very dangerous. Please get us some sidewalks.	Yes	Richard Baranas	12/19/2016
68	Just spent hour on I-4 -US 92 this morning 12-15-16	Yes	N/A	12/19/2016
69	Widen I-4 so trucks stop using 92. Trucks are getting terrible.	N/A	R. Sosa	12/19/2016
70	I think the speed limit should be lowered it is way too fast. Trucks should not be allowed on US 92. They have Martin Luther or I-4 they can use! Get these people with their bicycles off the road have them use side streets.	N/A	Carlos Gorrdes	12/19/2016
71	I am concerned about access onto US 92. From our private road each property owner on Brackwood has a driveway to US 92 at this time we share some of the road with only one small access to get onto US 92. The boardwalk also blocks a lot of vision when you are in a car not so bad. When in a truck I have avoided many head ons on our road Brackwood Road. It is to narrow also when I take my horse trailer out onto US 92 I have to pick a certain time of day to enter 92 can't make a good swing. When the state put the boardwalk in they never considered all the separate access onto 92!	N/A	Nora Menge	12/19/2016

No.	Summary of Comments	Support Build Alternative?	Name	Date
72	<p>If this project is 20 or more years away- you still won't need it. We have I-4 to the North and 574 (MLK) to the South (which is already being widened). 2 line in segment #5 and cannot imagine us ever needing 4 lanes plus bike lanes and a 54' median. We already have drainage problems what will this project do. Why not spend the money on I-4. That's what Interstates are for - to move cars. (add a toll lane) Widening US 92 will only make the backup problems on McIntosh worse. Fix problems like that. Don't ruin one of the last rural areas left. Again focus on I-4 and 574.</p>	No	Anette Friese	12/19/2016

List of Appendices

Appendix A – Agency Letters and Meeting Minutes

Appendix B – Newsletters

Appendix B – Public Hearing Transcript

Appendix C – Public Hearing Comments and Responses

Appendix A

Agency Letters and Meeting Minutes



FWS Log No. 2017-I-0144
 The proposed action is not likely to adversely affect resources protected by the Endangered Species Act of 1973, as amended (16 U.S.C. 1531 et seq.) provided the standard protection measures for the eastern indigo snake are incorporated into the project plan. This finding fulfills the requirements of the Act.



Jay B. Herrington
 Jay B. Herrington
 Field Supervisor
 Date 1-10-17

Florida Department of Transportation

RICK SCOTT
 GOVERNOR

11201 N. McKinley Drive
 Tampa, Florida 33612

ANANTH PRASAD, P.E.
 SECRETARY

December 8, 2016

Ms. Zakia Williams
 U.S. Fish and Wildlife Service
 U.S. Department of the Interior
 7915 Baymeadows Way, Suite 200
 Jacksonville, FL 32256-7517

RE: Endangered Species Act Section 7 Coordination
 SR 600 (US 92) From East of I-4 to East of County Line Road
 Hillsborough County, Florida
 WPI Segment No: 435749-1

Dear Ms. Williams:

The Florida Department of Transportation, District Seven, is conducting a Project Development and Environment Study Re-evaluation for the proposed widening of State Road 600 (US 92) from east of Interstate 4 to east of County Line Road in Hillsborough County, Florida. The total project length is approximately 18.1 miles. The environmental document that is being reevaluated is a Type 2 Categorical Exclusion (Approved by the FHWA on March 24, 1994) (Figure 1).

This Draft Wetland Evaluation and Biological Assessment Report (WEBAR) was prepared as part of this PD&E study. This report summarizes potential impacts to wetlands, federal- and state-listed species and their habitats. Identification of measures to avoid, minimize, and mitigate for any potential impacts are also discussed.

Proposed Project

The study generally recommended four and six-lane build alternatives from east of I-4 to Mobley Street and from Park Road to County Line Road. However, the no-build

Alex Hull

To: Jason Houck
Subject: RE: Document Review Confirmation for US 92 Draft WEBAR COMPLETE REPORT

Alex B. Hull, PE

INWOOD CONSULTING ENGINEERS

3000 Dovera Dr., Suite 200, Oviedo, FL 32765

Office: 407-971-8850

Mobile: 321-303-6253

Direct: 407-542-0309

From: Selly, Nicole [<mailto:Nicole.Selly@dot.state.fl.us>]
Sent: Wednesday, December 28, 2016 10:58 AM
To: Lilliam Escalera <lilliam.escalera@dot.state.fl.us>; Jason Houck <jhouck@inwoodinc.com>
Subject: FW: Document Review Confirmation for US 92 Draft WEBAR COMPLETE REPORT

A review was received for the following:

Event: 435749-1 US 92 from East of I-4 to East of County Line Road WEBAR Review
Document: US 92 Draft WEBAR COMPLETE REPORT
Submitted By: Jennifer Goff
Global: Yes
Comments:

The Florida Fish and Wildlife Conservation Commission (FWC) staff has reviewed the Draft Wetland Evaluation and Biological Assessment Report (WEBAR) for the above-referenced project, prepared as part of the Project Development and Environment (PD&E) Reevaluation Study. We provide the following comments and recommendations for your consideration in accordance with Chapter 379, Florida Statutes and Rule 68A-27, Florida Administrative Code (F.A.C.).

The project involves an evaluation of widening US 92 (SR 600) from a two-lane roadway to a four-lane divided roadway between just east of I-4 to east of County Line Road in Hillsborough County. The total project length is approximately 18.1 miles, but the No Build Alternative has been selected for a segment approximately 2 miles long between Mobley Street and Park Road in Plant City. This WEBAR also includes an analysis of 21 Stormwater Management Facility and 14 Floodplain Compensation alternative sites. The project vicinity is a mix of residential and commercial development, agriculture, upland forests, herbaceous and forested wetlands, and man-made ponds and lakes.

The WEBAR evaluated potential project impacts to 22 wildlife species classified under the Endangered Species Act as Federally Endangered (FE) or Threatened (FT), or by the State of Florida as Threatened (ST) or Species of Special Concern (SSC). Listed species were evaluated based on range and potential appropriate habitat or

because the project is within a U.S. Fish and Wildlife Service Consultation Area. Included were: Eastern indigo snake (FT), sand skink (FT), American alligator (FT due to similarity in appearance to the American crocodile), crested caracara (FT), wood stork (FE), Florida scrub jay (FT), gopher frog (SSC), gopher tortoise (ST), Suwannee cooter (SSC), Florida pine snake (SSC), short-tailed snake (ST), Florida burrowing owl (SSC), Southeastern American kestrel (ST), Florida sandhill crane (ST), roseate spoonbill (SSC), limpkin (SSC), snowy egret (SSC), little blue heron (SSC), tricolored heron (SSC), white ibis (SSC), Sherman's fox squirrel (SSC), and Florida mouse (SSC).

Also evaluated were the bald eagle, which was delisted by state and federal agencies, but this species remains protected under state rule in Section 68A-16.002, F.A.C. and by the federal Bald and Golden Eagle Protection Act (16 U.S.C. 668-668d), and the Florida black bear, which is protected by the FWC pursuant to the Florida Black Bear Conservation Rule 68A-4.009 F.A.C.

Project biologists made a finding of "no effect" for the sand skink, crested caracara, Florida scrub jay, Suwannee cooter, Florida pine snake, short-tailed snake, and Florida black bear due to a lack of suitable habitat for these species within the project area. The biologists determined that the project "may affect, but is unlikely to adversely affect" all the other species. We agree with these determinations.

We support the project commitments for protected species, which include the following.

1. The standard FDOT Construction Precautions for the Eastern Indigo Snake will be followed during construction.
2. Due to the presence of gopher tortoise habitat within the project area, a gopher tortoise survey in appropriate habitat will be performed within construction limits within 72 hours to 90 days prior to construction. The survey will follow the latest survey criteria from the FWC's Gopher Tortoise Permitting Guidelines and the FDOT will secure any required relocation permit from the FWC.
3. Impacts to potential wood stork suitable foraging habitat will be evaluated during the design phase, and mitigation for unavoidable impacts will be provided as appropriate. This, along with other required wetland mitigation, is anticipated to provide mitigation for the loss of foraging habitat for other listed wading bird species.

The wildlife surveys did not record individual or nest sitings of Florida burrowing owls, Southeastern American kestrels, Florida sandhill cranes, Sherman's fox squirrels, or bald eagles, largely due to either very limited or suboptimal habitat for these species within the project area. Should a nest of any of these species be discovered

near the project limits prior to or during construction, please coordinate with the FWC staff identified below to discuss avoidance, minimization, and permitting options.

The WEBAR identified 11.33 acres of wetlands that will be impacted by the project, including 9.84 acres of forested wetlands, 1.43 acres of herbaceous wetlands, and 0.06 acres of surface waters. Mitigation would be provided via one or more of several mitigation banks or using the FDOT Mitigation Program with the Southwest Florida Water Management District. We agree with the findings of this evaluation.

We appreciate the opportunity to provide input on highway design and the conservation of fish and wildlife resources. Please contact Brian Barnett at (772) 579-9746 or email

brian.barnett@MyFWC.com

to initiate the process for further overall coordination on this project.



Florida Department of Transportation

RICK SCOTT
GOVERNOR

11201 N. McKinley Drive
Tampa, Florida 33612-6456

JIM BOXOLD
SECRETARY

September 7, 2016

Dr. Timothy A. Parsons, Ph.D., Director
State Historic Preservation Officer
Florida Division of Historical Resources
500 South Bronough Street
Tallahassee, Florida 32399-0250

Attention: Transportation Compliance Review Program

RE: SR 600 (US 92) Project Development and Environment (PD&E) Study
Re-Evaluation from East of I-4 to East of County Line Road
Work Program Item Segment No.: 435749-1
FAP No.: TBD
Hillsborough County, Florida

Dear Dr. Parsons:

The Florida Department of Transportation (FDOT), District Seven is preparing a Project Development and Environment (PD&E) Study Re-Evaluation for the proposed widening of SR 600 (US 92) from east of Interstate 4 (I-4) to east of County Line Road in Hillsborough County, Florida. The total project length is approximately 18.1 miles. The original PD&E Study was completed in 1994. The Study recommended a build alternative from east of I-4 to Mobley Street and from Park Road to County Line Road. The no-build alternative was selected for the segment between Mobley Street and Park Road with the exception of improving one section of Baker Street where it was recommended for conversion to an urban section between Mobley Street and Whitehall Street. Due to a change in design standards and existing conditions, the project's PD&E study is being re-evaluated. The no-build alternative between Mobley Street and Park Road remains as the recommended alternative. Sidewalk and drainage improvements were made to the section of Baker Street between Mobley Street and Whitehall Street which meet the intent of the original PD&E study recommendation for this segment of the project.

A Cultural Resource Assessment Survey (CRAS) Update was prepared for this project and submitted to your office on March 17, 2016. In July 2016, the CRAS Update was revised based on comments provided by Alyssa McManus (letter dated May 10, 2016). At this time, edits were made to the original report and select pages were resubmitted. The CRAS Update was again revised in August 2016. At this time, the eligibility for previously recorded Frame Vernacular style residence located at 104 Thrasher Road (8HI04739) was updated and revised pages resubmitted. Enclosed are

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FDOT
DISTRICT SEVEN
HILLSBOROUGH COUNTY
STATE HISTORIC PRESERVATION

one set of revised pages to the CRAS Update (February 2016, Revised August 2016) that were prepared for the above referenced project.

The CRAS Update included background research and a field survey. The purpose was to locate and identify any archaeological sites and historic resources located within the project area of potential effect (APE) and to assess their significance in terms of eligibility for listing in the National Register of Historic Places (NRHP). This is an update to the CRAS that was prepared for the original PD&E Study. The SR 600 (US 92) PD&E Re-evaluation will replicate the original PD&E approved alignment with modifications as needed. As a result, the project is planned to be mostly developed within the existing right-of-way. A CRAS of the original PD&E corridor was performed in 1991-1992 (ACI 1993), and approved by the SHPO in October 1993. Therefore, systematic archaeological field survey will focus only on areas of new right-of-way. For historic resources, the APE is defined as the properties adjacent to the existing and proposed right-of-way. As contained within these adjacent properties, only the historic resources located within 200 feet of the existing and proposed right-of-way were recorded and evaluated. No improvements to the approximate two-mile long one-way pair system through Plant City between Mobley Street and Park Road (Segment 10) are anticipated. However, the historic resources located along Reynolds Street (US 92 Eastbound) between Mobley Street and Park Road were included in the CRAS. Stormwater management facilities and floodplain compensation sites have been preliminarily sized for the project's ongoing PD&E Study, and an addendum to the CRAS will be prepared to document the historical and archaeological evaluations for these locations later in the PD&E Study.

Background research indicated that 14 previously recorded archaeological sites are located, at least in part, within the project archaeological APE. All were evaluated as ineligible for listing in the NRHP by the SHPO. Limited field survey did not confirm the presence of any of these sites within the project APE. Field survey focused on areas of newly proposed right-of-way yielded negative results. Given these results, and the findings of previous investigations, there are no archaeological sites within the project APE that are listed, eligible, or considered potentially eligible for listing in the NRHP.

Historical/architectural field survey resulted in the identification of 510 historic resources within the project APE, including 102 extant previously recorded and 408 newly identified encompassing 476 buildings, 26 historic districts and building complex resource groups, four bridges, two objects, and two linear resources. In addition, 20 previously recorded historic resources were confirmed demolished.

As a result of field survey, 71 NRHP-listed, eligible and potentially eligible historic resources were located within the SR 600 (US 92) project APE. Of these, 44 were previously listed or determined eligible by the SHPO, and 27 were newly evaluated as potentially eligible. These include two historic districts, three building complex resource groups, one object, and 65 buildings.

Dr. Timothy Parsons, SHPO
SR 600 (US 92) Project Development and Environment (PD&E) Study
Re-Evaluation from East of I-4 to East of County Line Road
Work Segment No.: 435749-1
September 7, 2016
Page 3 of 4

No NRHP-listed, eligible, or potentially eligible historic resources were identified within evaluation Segments 2 through 8. Twelve historic resources, including one potentially eligible building complex resource group (8HI04634) with 11 contributing resources, are contained within Segment 1; nine historic resources, including one potentially eligible building complex resource group (8HI13404) with eight contributing resources, are contained within Segment 9; and one individually significant historic resource (8HI05328) is located within Segment 11. Forty-nine of the total significant historic resources are contained within Segment 10, the no-build alternative. These include one NRHP listed individual property (8HI00174) plus three historic districts and building complex resource groups (8HI05386, 8HI05923, and 8HI13405) that collectively contain 45 contributing resources within the project APE. Although there are significant historic resources identified within Segment 10, the proposed project should have no effect on the resources since this segment will only have a no-build alternative.

A previously recorded Frame Vernacular style residence located at 104 Thrasher Road (8HI04739), was determined ineligible by the SHPO in 1993 and was re-evaluated as part of this study (August 2016). After revisiting the site and collecting additional background information this resource remains ineligible for listing in the NRHP. This residence is located within build Segment 11.

This information is being provided in accordance with the provisions of the National Historic Preservation Act of 1966 (as amended), which are implemented by the procedures contained in 36 CFR, Part 800, as well as the provisions contained in the revised Chapter 267, Florida Statutes.

Provided you approve the recommendations and findings in the enclosed cultural resource document, please sign below for concurrence. If you have any questions, please contact me at (813) 975-6496 or robin.rhinesmith@dot.state.fl.us or Rebecca Spain Schwarz at (813) 281- 8308 or rebecca.spain-schwarz@atkinsglobal.com.

Sincerely,



Robin M. Rhinesmith
Environmental Administrator

RR/RSS
Enclosure

cc:	Cathy Kendall (FHWA)	Roy Jackson (FDOT SEMO)
	Lilliam Escalera (FDOT)	Alex Hull (Inwood)
	Rebecca Spain Schwarz (Atkins/GEC)	Marion Almy (ACI)

Dr. Timothy Parsons, SHPO
SR 600 (US 92) Project Development and Environment (PD&E) Study
Re-Evaluation from East of I-4 to East of County Line Road
Work Segment No.: 435749-1
September 7, 2016
Page 4 of 4

The Florida State Historic Preservation Officer finds the attached Cultural Resource Assessment Survey Update complete and sufficient and concurs with the recommendations and findings provided in this cover letter for SHPO/DHR Project File Number 2016-1350. Or, the SHPO finds the attached report contains ___ insufficient information.

SHPO Comments:

We acknowledge that a separate effects document will be submitted for SHPO review for segments 1, 9 and 11. We concur with finding of no effect for segs 2-8 and a finding of no adverse affect for segment 10.

For Isi J. Alden Igo, Deputy SHPO
Timothy A. Parsons, Ph.D.
Director, Florida Division of Historical Resources
& State Historic Preservation Officer

9/16/2016
Date



Meeting Minutes

3000 Dovera Drive, Suite 200, Oviedo, FL 32765 | P: 407-971-8850 | F: 407-971-8955 | www.inwoodinc.com

DATE: July 6, 2016

TO: Lilliam Escalera, PE

FROM: Alex B. Hull, PE

RE: 435749-1 US 92 FROM I-4 TO COUNTY LINE Road PD&E Study Reevaluation
Presentation to Hillsborough County Staff Meeting Minutes

ATTENDEES: See attached sign-in sheet

A meeting was held with Hillsborough County Engineering staff on July 1, 2016 at the County Center, 22 Floor Meeting Room, 601 E. Kennedy Boulevard, Tampa, FL. The purpose of the meeting was to discuss the US 92 from I-4 to County Line Road PD&E Study Reevaluation. A PowerPoint presentation regarding the project was given and is attached to these meeting minutes.

The meeting began at 2:00 pm.

There was general discussion about the project. The main points of discussion follow:

1. The PD&E traffic analysis showed the need to improve the side streets (county streets) in order to improve the LOS of the intersection. County staff noted there is no plan anticipated in the near future to improve any of the side streets. However, the county staff requested the cost estimates of the proposed side street improvements to consider coordinating their own improvements with the department's during the upcoming design phases.
2. The north approach at Park Road is a State facility (SR 553) and its improvements will be included in the concept plans. County noted there are future developments planned at this location and requested a copy of the cost estimates at this location to consider if design of the south side of Park Rd (CR 574) can be included as part of the design project (WPIS No. 438998).
3. The County recommended that the Department consider providing a roundabout at Falkenburg Road and all other intersections along the corridor as an intersection alternative.
4. The County recommended contacting the Hillsborough County School Transportation Working Group as part of the Department's small group meetings efforts. The department indicated that it will send an email to the School District to ask if they will like a presentation of the PD&E Study Reevaluation efforts.

indicated that it will send an email to the School District to ask if they will like a presentation of the PD&E Study Reevaluation efforts.

5. The County noted that there is an existing operational problem at the US 92 and Kingsway Rd. intersection during school days. The department noted that there is no design project presently scheduled for this location.

The meeting was concluded at 3:15 pm.



SR 600 (US 92)

**PROJECT DEVELOPMENT & ENVIRONMENT
(PD&E) STUDY RE-EVALUATION**

FROM I-4 TO COUNTY LINE ROAD

WPI Segment No.: 435749-1 | Hillsborough County, Florida

PROJECT LOCATION



US 92 PD&E STUDY
RE-EVALUATION

- Project begins at Garden Lane (East of I-4)
- Project ends at County Line Road
- Re-evaluation of original 1994 PD&E Study

18 miles



PROJECT DESCRIPTION



US 92 PD&E STUDY
RE-EVALUATION



US 92 is:

- Major east-west roadway through Hillsborough County
- Important in Tampa Bay area regional transportation network
- Project will increase capacity and improve safety
- Four-lane improvement from Garden Lane to Mobley Street and from Park Road to County Line Road
- No-build through Plant City

- Re-evaluation of the original PD&E Study approved by the Federal Highway Administration on March 24, 1994
- Focuses on changed conditions
- Re-evaluation of proposed improvements includes:
 - Costs
 - Engineering aspects
 - Environmental aspects
 - Public and agency input

- Capacity
 - Future estimated traffic volumes will cause US 92 to operate below desired standards
- Transportation Planning
 - US 92 from US 301 to CR 579 (Mango Road) and from Park Road to County Line Road
 - Included in the Imagine 2040: Hillsborough County MPO Long Range Transportation Plan Cost Feasible Plan
- Safety
 - Crash data shows that the average five-year crash rate is higher than the statewide average
- Socioeconomic Demand
 - Projected regional growth will increase traffic

RECOMMENDED BUILD ALTERNATIVE



US 92 PD&E STUDY
RE-EVALUATION

- Five typical sections are proposed for the project:
 - Garden Lane to west of I-4 overpass and east of I-4 overpass to east of Crow Wing Drive (purple area)
 - West of I-4 overpass to east of I-4 overpass (yellow area)
 - East of Crow Wing Drive to Edwards Street (orange area)
 - Edwards Street to Mobley Street (blue area)
 - West of Park Road to County Line Road (teal area)
- No-build
 - Mobley Street to west of Park Road (green area)

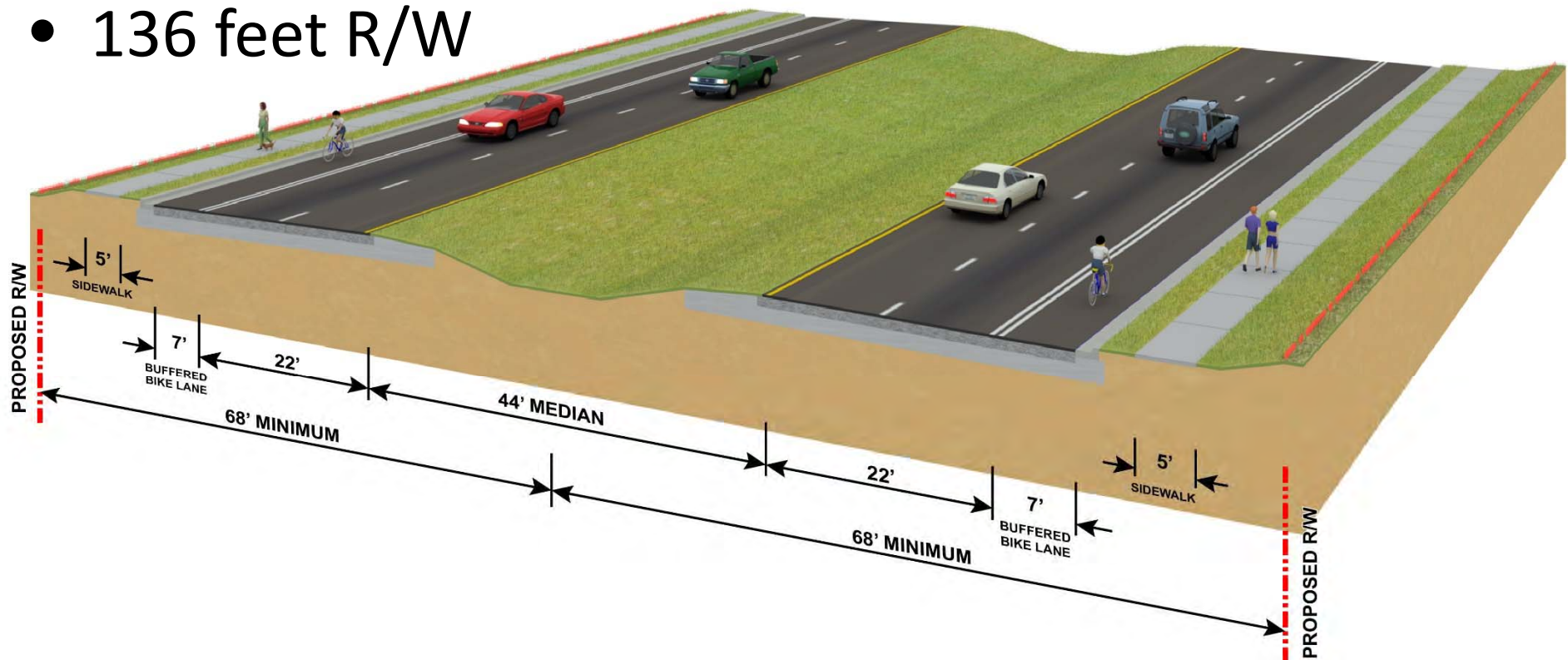


RECOMMENDED BUILD ALTERNATIVE



US 92 PD&E STUDY
RE-EVALUATION

- Garden Lane to west of I-75 overpass
- East of I-75 overpass to east of Crow Wing Drive
- 45 mph design speed
- 136 feet R/W

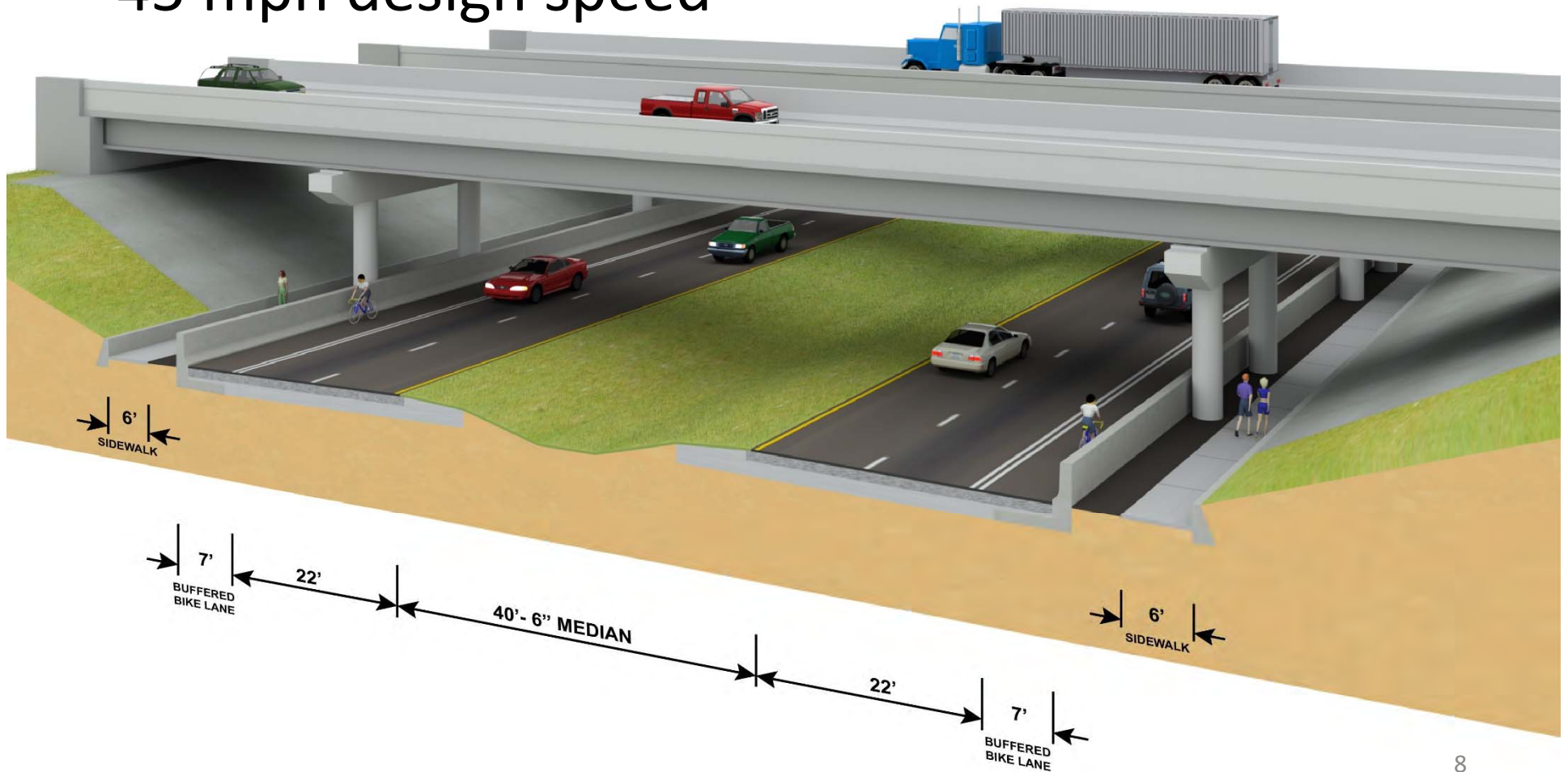


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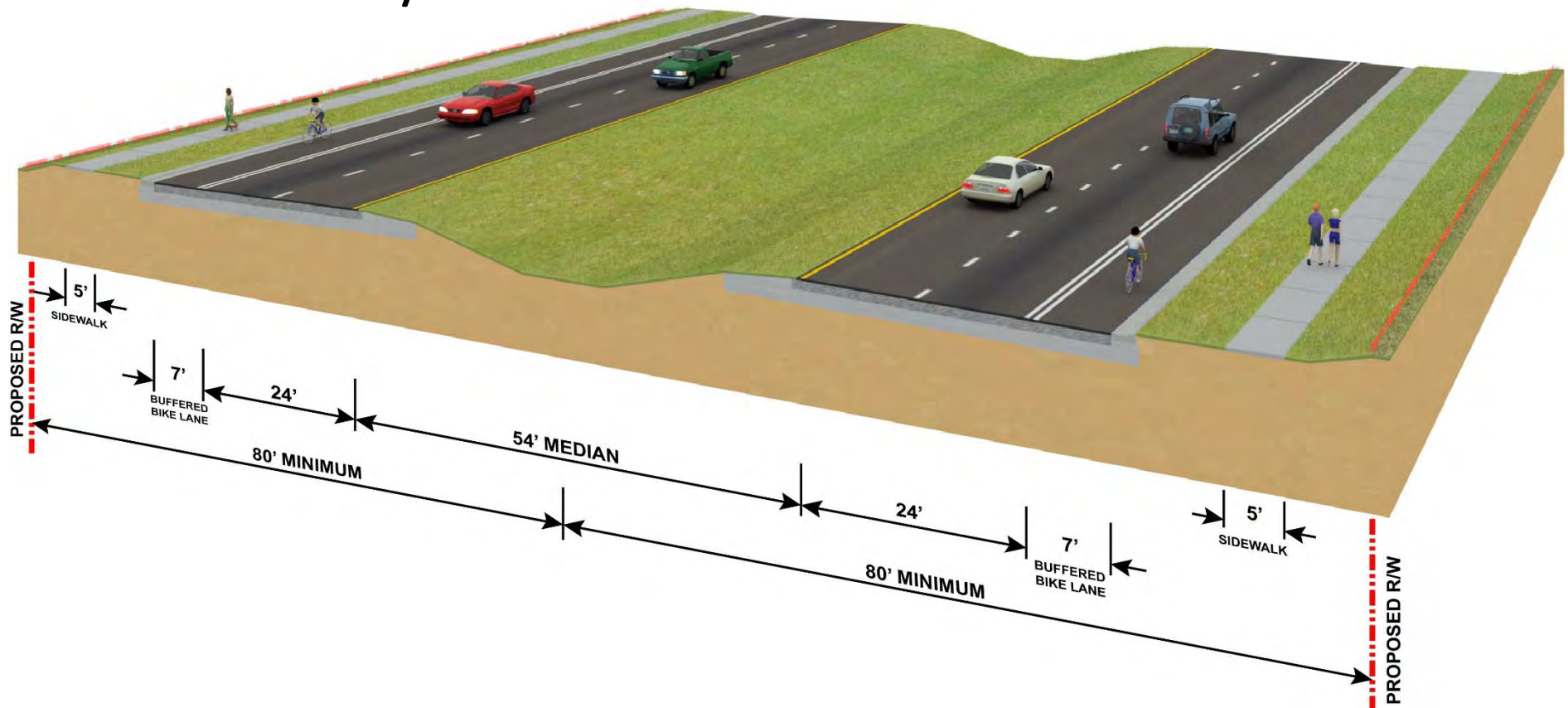


RECOMMENDED BUILD ALTERNATIVE



US 92 PD&E STUDY
RE-EVALUATION

- East of Crow Wing Drive to Edwards Street
- 50 mph design speed
- 160 feet R/W

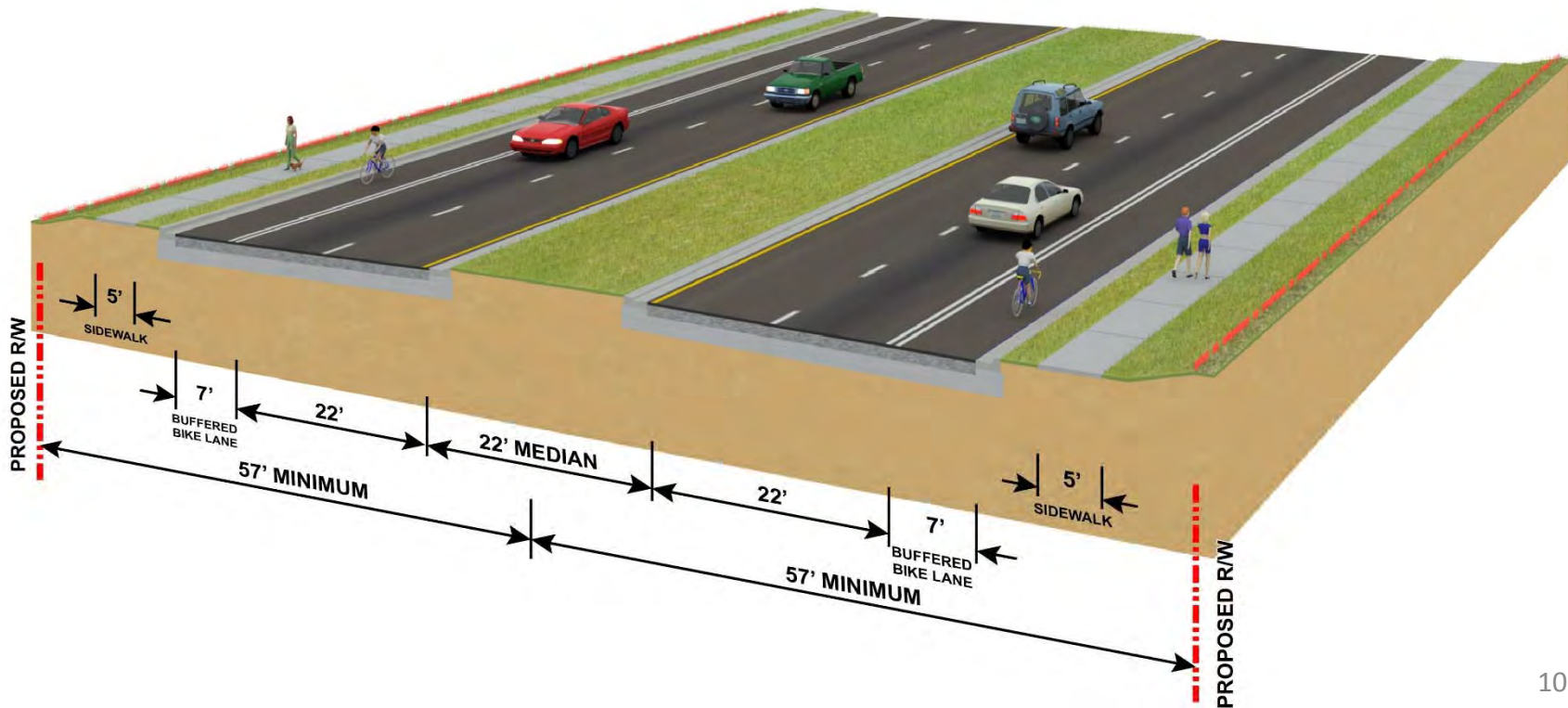


RECOMMENDED BUILD ALTERNATIVE



US 92 PD&E STUDY
RE-EVALUATION

- Edwards Street to Mobley Street
- 45 mph design speed
- 114 feet R/W

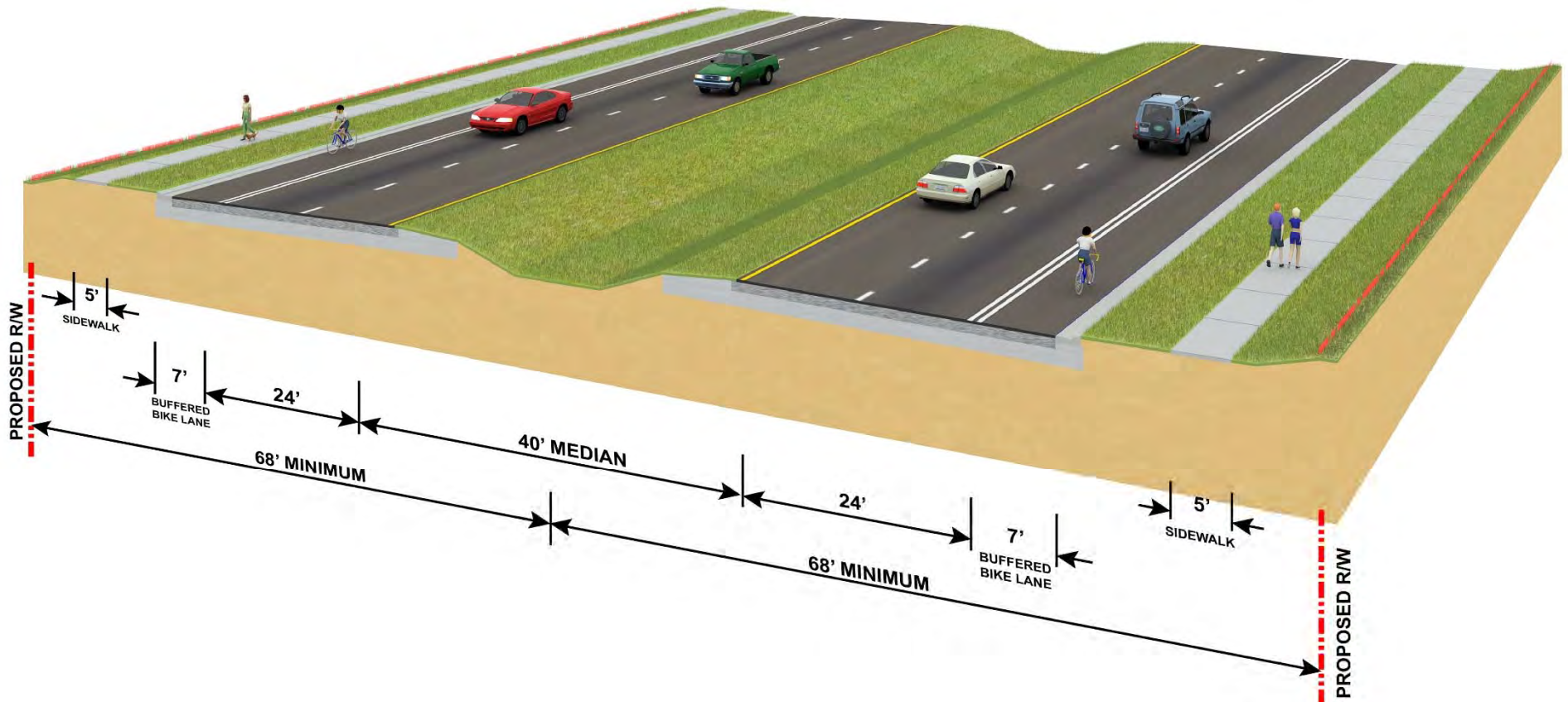


RECOMMENDED BUILD ALTERNATIVE



US 92 PD&E STUDY
RE-EVALUATION

- West of Park Road to County Line Road
- 50 mph design speed
- 136 feet R/W



- Combination of directional and full median openings proposed
 - Directional median openings allow some turns
 - Full median openings allow turns in all directions
- Driveway and crossroad locations considered
- Access Management Class 5 spacing criteria

Class	Medians	Median Openings		Signal	Connection	
		Full	Directional		More than 45 mph posted speed	45 mph and less posted speed
5	Restrictive	2,640' At <u>greater than 45 mph</u> posted speed	660'		440'	245'
		1,320' At <u>45 mph or less</u> posted speed				

- Advantages
 - No acquisition of right-of-way
 - No design, right-of-way or construction costs
 - No inconvenience to the public during construction
 - No construction impacts to natural, physical, and social environment
- Disadvantages
 - Increased traffic congestion
 - Emergency vehicle access is degraded
 - Does not improve pedestrian or bicyclist safety
 - Increased user costs due to congestion
 - Not consistent with the identified needs of the corridor by the Hillsborough Metropolitan Planning Organization

- Advantages
 - Reduced traffic congestion and intersection delay
 - Reduced potential for crashes
 - Reduced vehicle emissions
 - Improved emergency vehicle response
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- Disadvantages
 - Additional right-of-way needed
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Tentative Design Schedule



US 92 PD&E STUDY
RE-EVALUATION

- Two segments tentative scheduled for design
 - US 92 from US 301 (east of I-4) to Mango Road
 - US 92 from west of Park Road to County Line Road
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- Re-evaluation began January 2015
- Small group meetings are currently scheduled with local community organizations
- Public hearing – October 18, 2016
- PD&E study re-evaluation completion - end of 2016

- <http://active.fdotd7studies.com/sr600/i4-to-county-line/>
- **FDOT Project Manager**
Lilliam E. Escalera
EMO Project Manager
FDOT District VII
Planning & Environmental Management Office (PLEMO)
11201 N. McKinley Dr., 7-800
Tampa, FL 33612
P: (813)975-6445
F: (813) 975-6451

DATE: July 6, 2016, 2016

TO: Lilliam Escalera, PE

FROM: Alex B. Hull, PE

RE: 435749-1 US 92 PD&E Study Plant City Staff Presentation and Discussion Meeting Minutes

ATTENDEES: See attached sign-in sheet

A meeting was held at Plant City City Hall on June 30, 2016. The purpose of the meeting was to discuss the US 92 PD&E Study Re-evaluation with the City staff. A PowerPoint presentation regarding the project was given and is attached to these meeting minutes.

Issues that the City staff brought up for discussion are as follows:

1. There is planned development being considered along Charlie Taylor Road north of US 92. It is anticipated that the development will generate a considerable amount of traffic. Therefore, the City would like to give preference to Charlie Taylor Road for a full median opening.
2. The Hillsborough MPO is planning on conducting a bike plan study for the area. Mark Hudson with Plant City is the City's point of contact for the study.
3. Parksdale Farm Market representatives have expressed concern to the City Council about the US 92 widening project and the impacts to their site.
4. The City staff will organize a joint meeting of the Greater Plant City Chamber of commerce, Strawberry Festival, and the Economic Development Council (EDC) for the project team to make a presentation informing these groups about the project.



US 92 PD&E Study - Plant City Staff Presentation

From I-4 to County Line Road

Financial ID No.: 435749-1-22-01

June 30, 2016

Name	Organization	Address	E-mail
Michael Schenk	Plant City	302 W. Reynolds Street, PC	mschenk@plantcitygov.com
Terry R. Ritter	PLANT CITY	"	TRITTER@PLANTCITYGOV.COM
Tatjana Austin	City of PC/Engineering	308 W. Reynolds St, PC, FL	tausting@plantcitygov.com
Lucy Gassaway	CPC / Utilities	1802 Spooner, PC, FL	LGassaway@plantcitygov.com
Dave Buyens	"	"	dbyens@plantcitygov.com
MICHAEL KAPP	PLANT CITY	302 W. REYNOLDS ST. PC	MKAPP@PLANTCITYGOV.COM
Alex Hull	Inwood	3000 Dovera Dr. Suite 200 Oviedo, FL 32765	ahull@inwoodinc.com
Mark Hudson	City of Plant City	"	mhudson@plantcitygov.com
William Escalera	PD&E PM	701 McKinley Dr., Tampa, FL	William.escalera@dot.state.fl.us



SR 600 (US 92)

**PROJECT DEVELOPMENT & ENVIRONMENT
(PD&E) STUDY RE-EVALUATION**

FROM I-4 TO COUNTY LINE ROAD

WPI Segment No.: 435749-1 | Hillsborough County, Florida

PROJECT LOCATION



US 92 PD&E STUDY
RE-EVALUATION

- Project begins at Garden Lane (East of I-4)
- Project ends at County Line Road
- Re-evaluation of original 1994 PD&E Study

18 miles



PROJECT DESCRIPTION



US 92 PD&E STUDY
RE-EVALUATION



US 92 is:

- Major east-west roadway through Hillsborough County
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 - Crash data shows that the average five-year crash rate is higher than the statewide average
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RECOMMENDED BUILD ALTERNATIVE



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- Five typical sections are proposed for the project:
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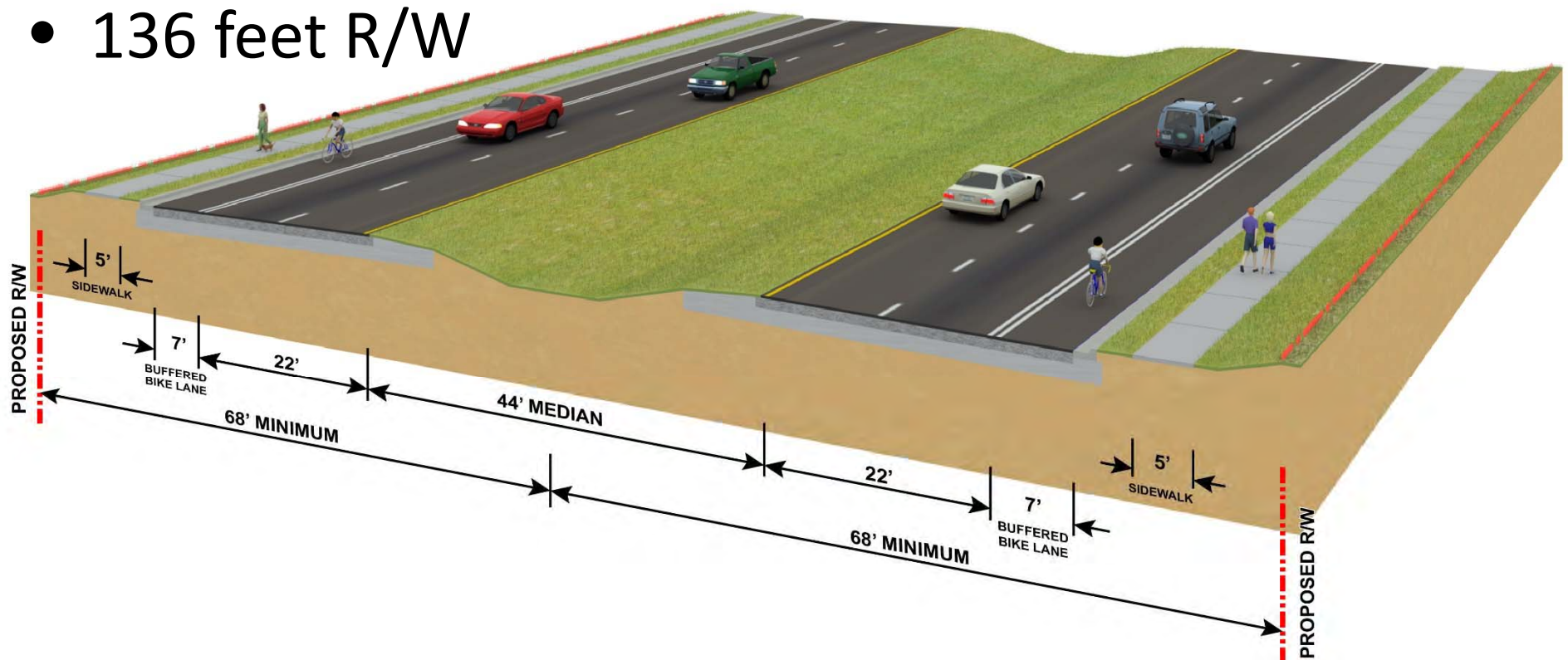


RECOMMENDED BUILD ALTERNATIVE



US 92 PD&E STUDY
RE-EVALUATION

- Garden Lane to west of I-75 overpass
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- 45 mph design speed
- 136 feet R/W

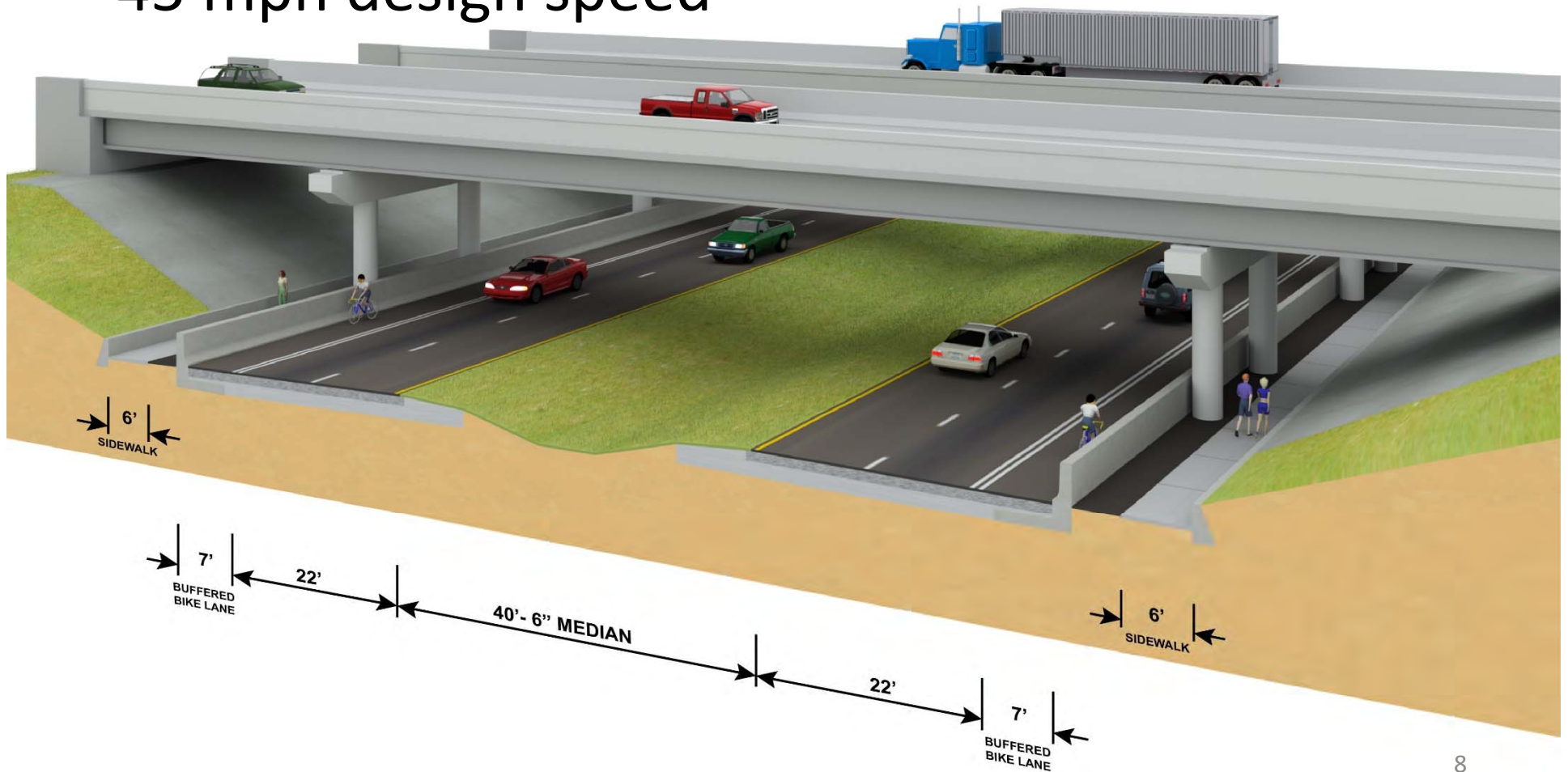


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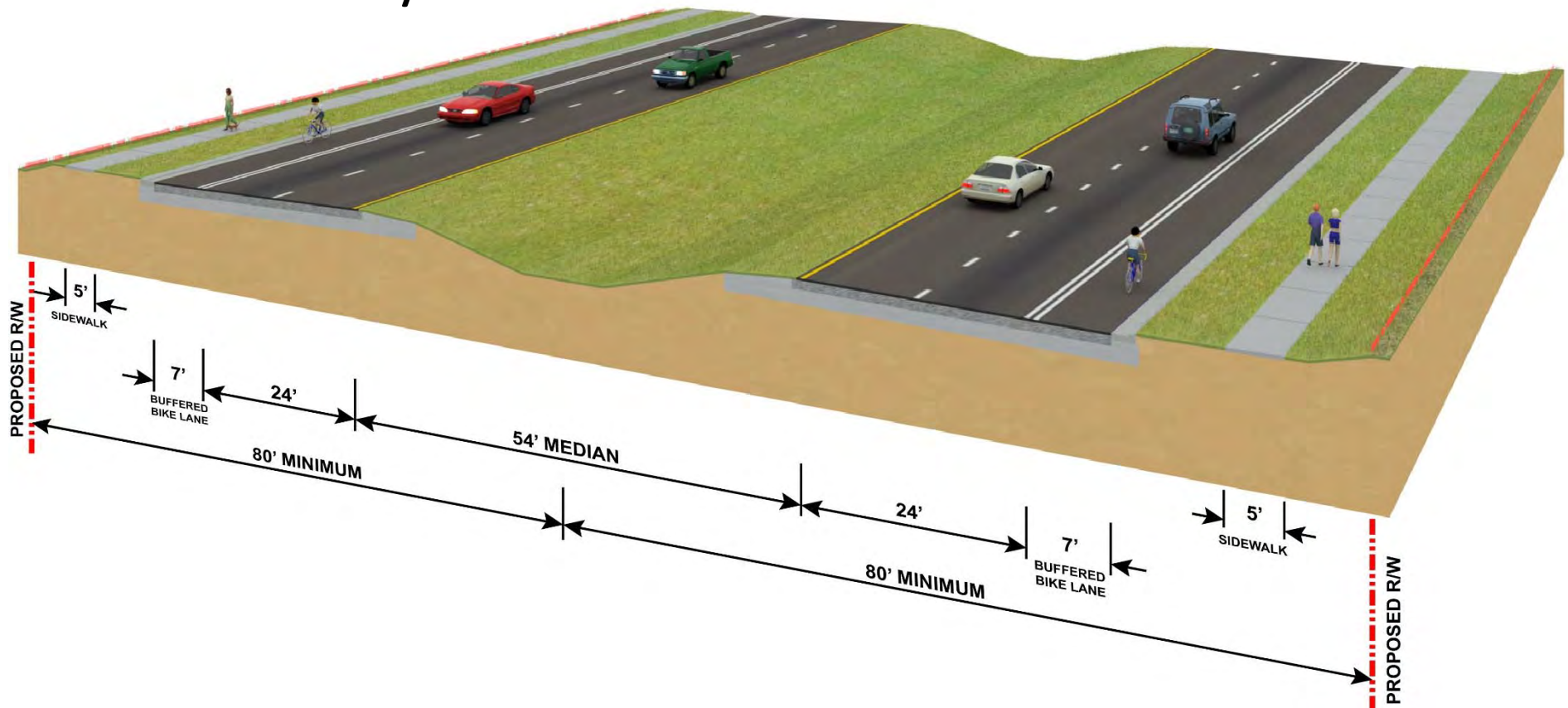


RECOMMENDED BUILD ALTERNATIVE



US 92 PD&E STUDY
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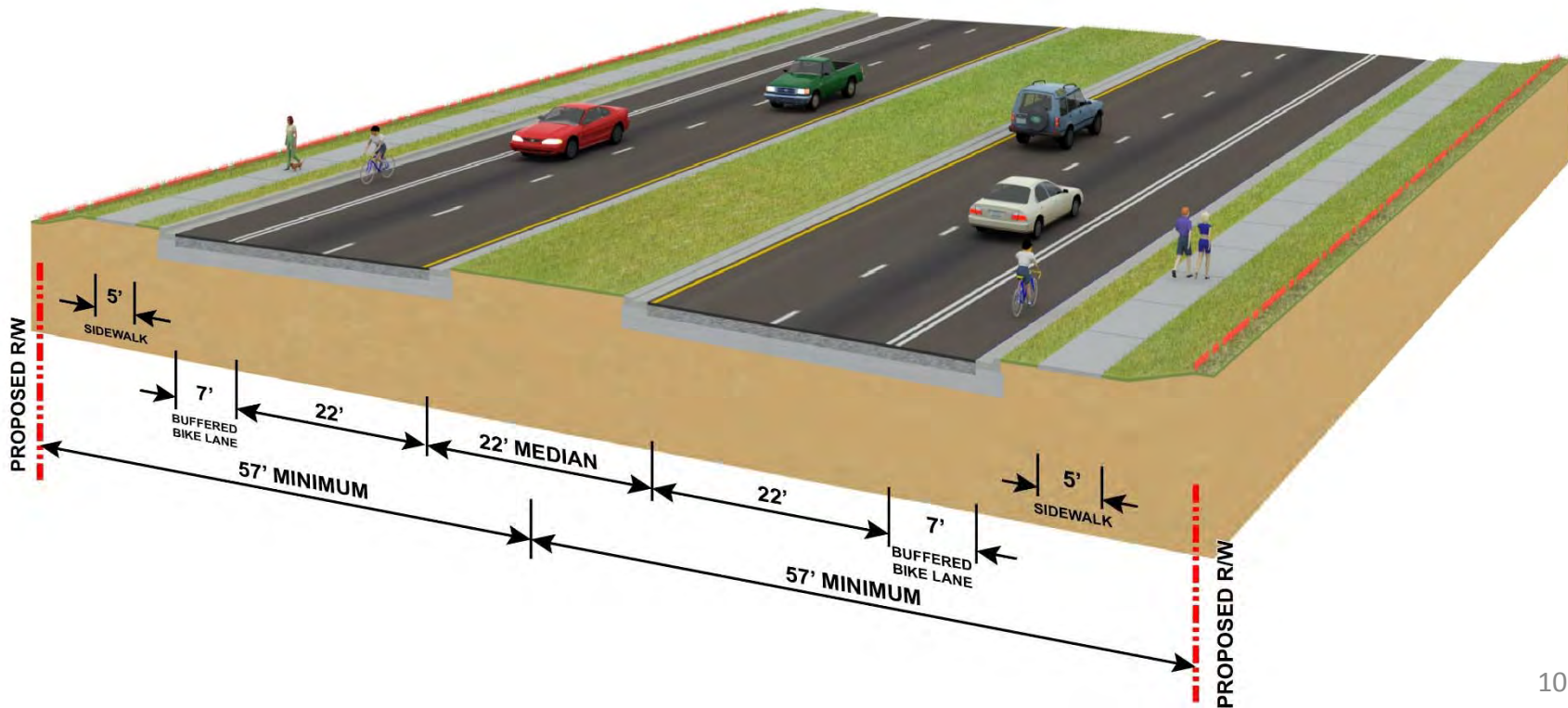


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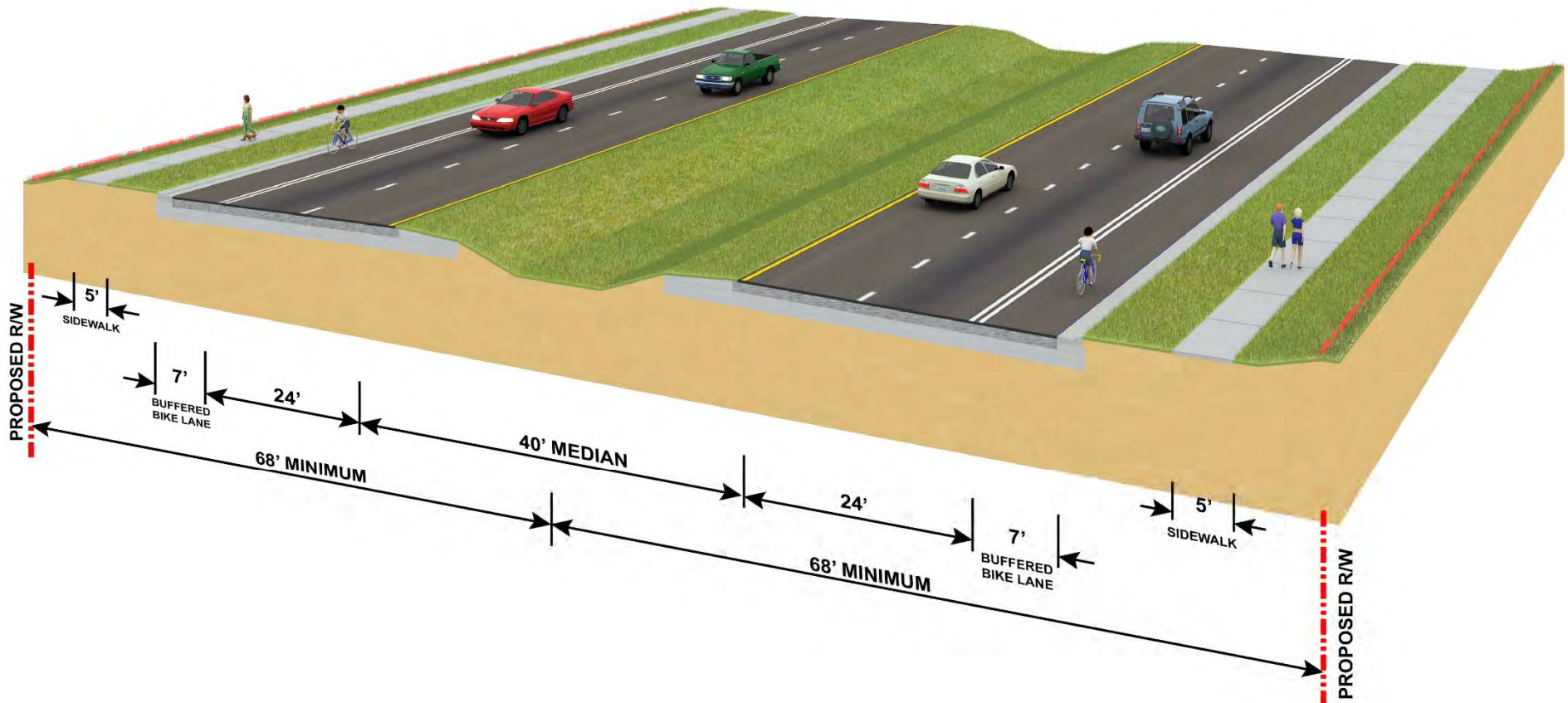


RECOMMENDED BUILD ALTERNATIVE



US 92 PD&E STUDY
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Tentative Design Schedule



US 92 PD&E STUDY
RE-EVALUATION

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- PD&E study re-evaluation completion - end of 2016

- <http://active.fdotd7studies.com/sr600/i4-to-county-line/>
- **FDOT Project Manager**
 - Lilliam E. Escalera
 - EMO Project Manager
 - FDOT District VII
 - Planning & Environmental Management Office (PLEMO)
 - 11201 N. McKinley Dr., 7-800
 - Tampa, FL 33612
 - P: (813)975-6445
 - F: (813) 975-6451

DATE: August 17, 2016, 2016

TO: Lilliam Escalera, PE

FROM: Alex B. Hull, PE

RE: 435749-1 US 92 PD&E Study Plant City Strawberry Festival Committee
Representatives Presentation and Discussion Meeting Minutes

ATTENDEES: Lee Bakst, Phil Waldron, Lilliam Escalera, Alex Hull

A meeting was held at the Plant City Strawberry Festival offices in Plant City on August 3, 2016. The purpose of the meeting was to discuss the US 92 PD&E Study Re-evaluation with the Strawberry Festival Committee representatives. A PowerPoint presentation regarding the project was given and is attached to these meeting minutes.

Issues that the City staff brought up for discussion are as follows:

1. The Strawberry Festival representatives expressed concern about the impact that construction of the project would have on Festival traffic patterns. Ms. Escalera explained that the construction documents can contain provisions addressing this issue.
2. They explained that the parking lot on the west side of the Festival grounds received significant eastbound right turns into the lot. Westbound traffic is primarily directed to lots on the eastside of the Festival, but eastbound traffic also reaches the west lot. They requested that FDOT consider adding a eastbound right turn into the west parking lot and adding a median opening that would serve the west lot westbound ingress and egress traffic. Ms. Escalera stated that she would discuss this request with the access management representative from FDOT.
3. Mr. Waldron requested a rollout print of the concept map (Note: The map was mailed to Mr. Waldron on the following day.)



SR 600 (US 92)

**PROJECT DEVELOPMENT & ENVIRONMENT
(PD&E) STUDY RE-EVALUATION**

FROM I-4 TO COUNTY LINE ROAD

WPI Segment No.: 435749-1 | Hillsborough County, Florida

PROJECT LOCATION



US 92 PD&E STUDY
RE-EVALUATION

- Project begins at Garden Lane (East of I-4)
- Project ends at County Line Road
- Re-evaluation of original 1994 PD&E Study

18 miles



PROJECT DESCRIPTION



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 - Crash data shows that the average five-year crash rate is higher than the statewide average
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RECOMMENDED BUILD ALTERNATIVE



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- No-build
 - Mobley Street to west of Park Road (green area)
- Preferred alignment shown on concept plans

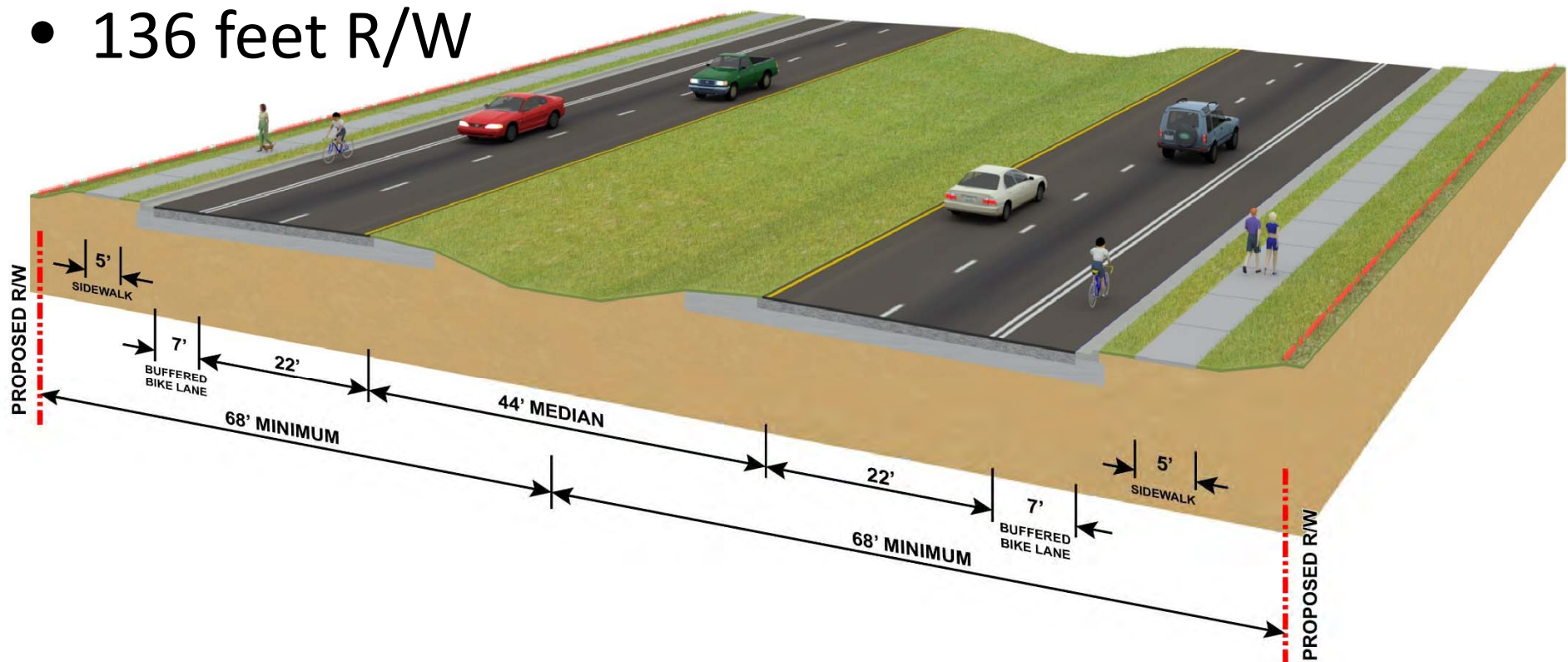


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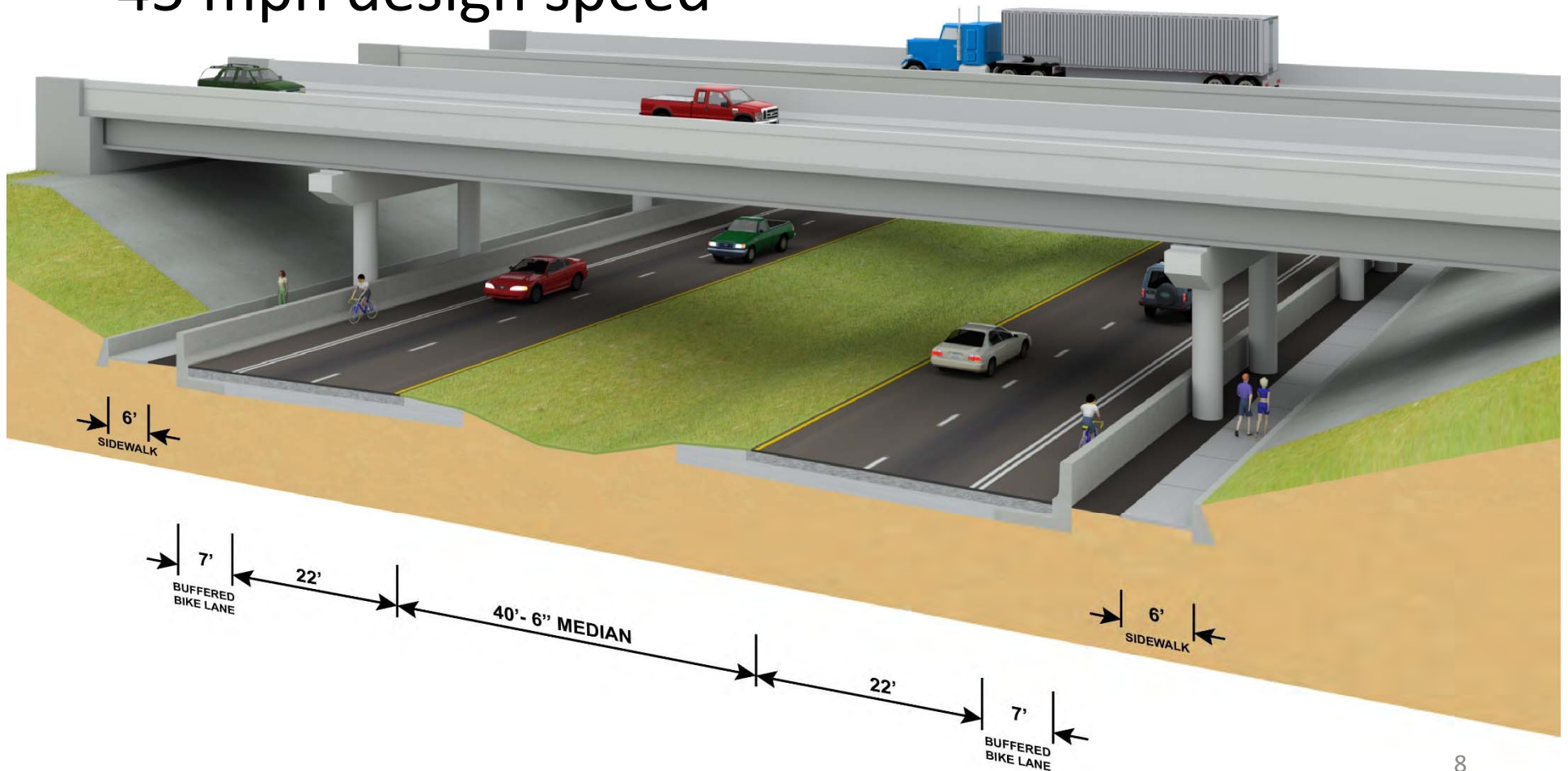


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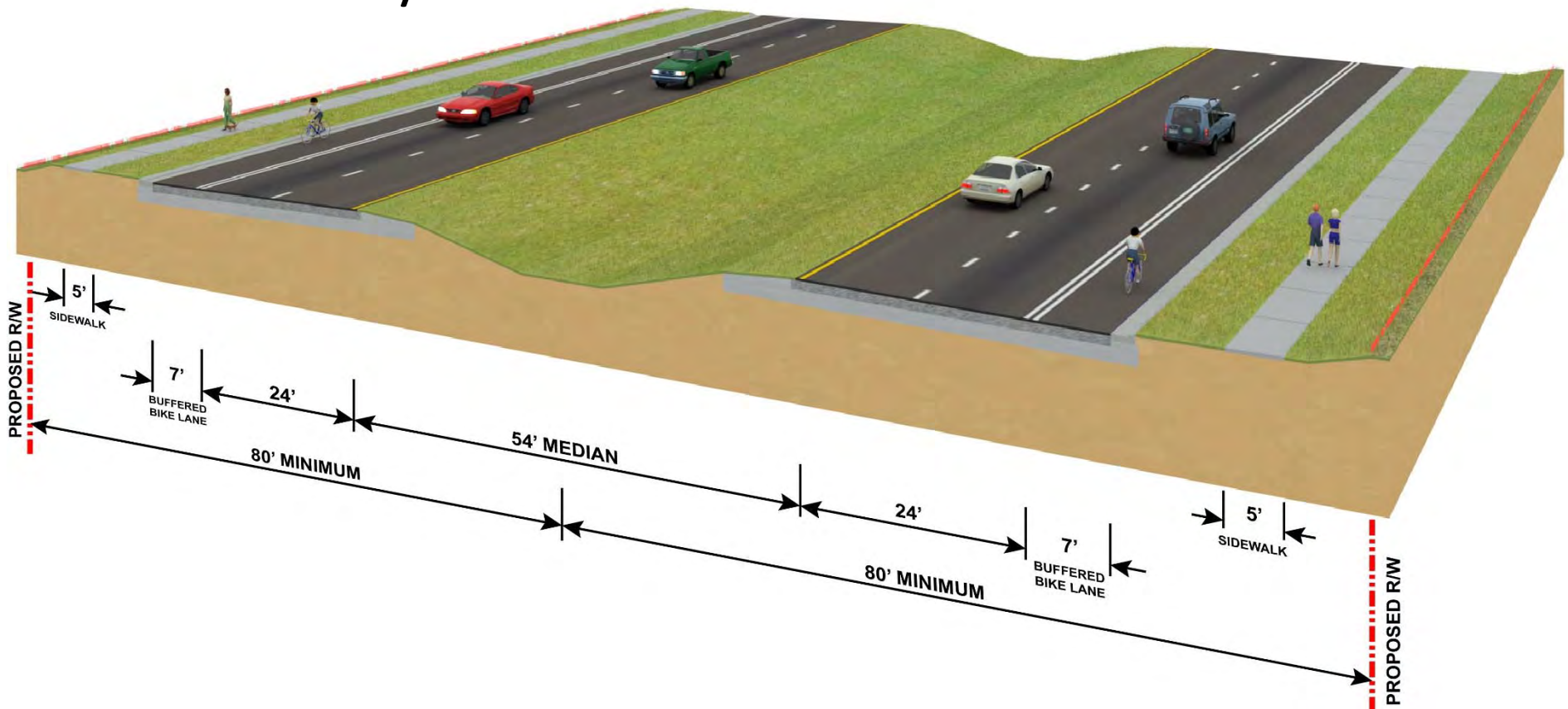


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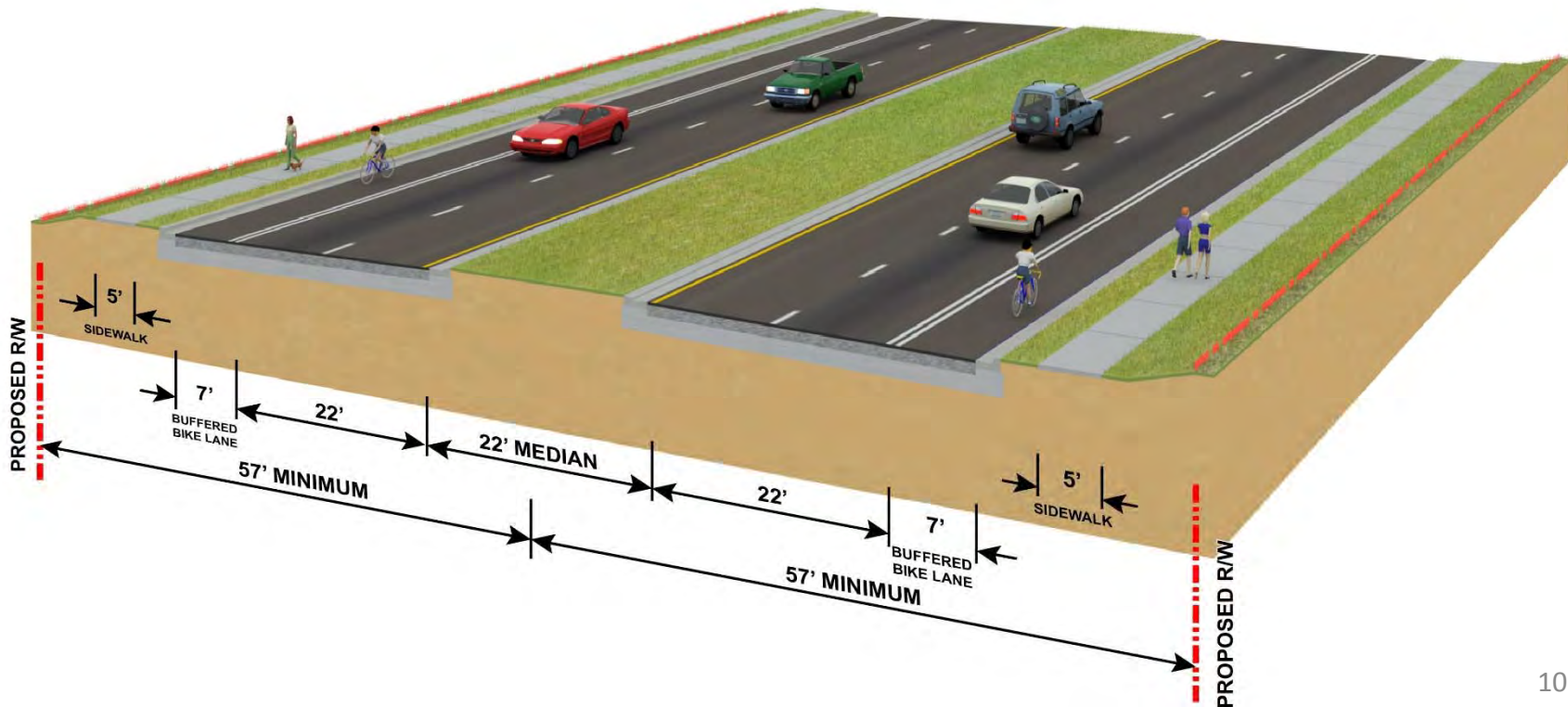


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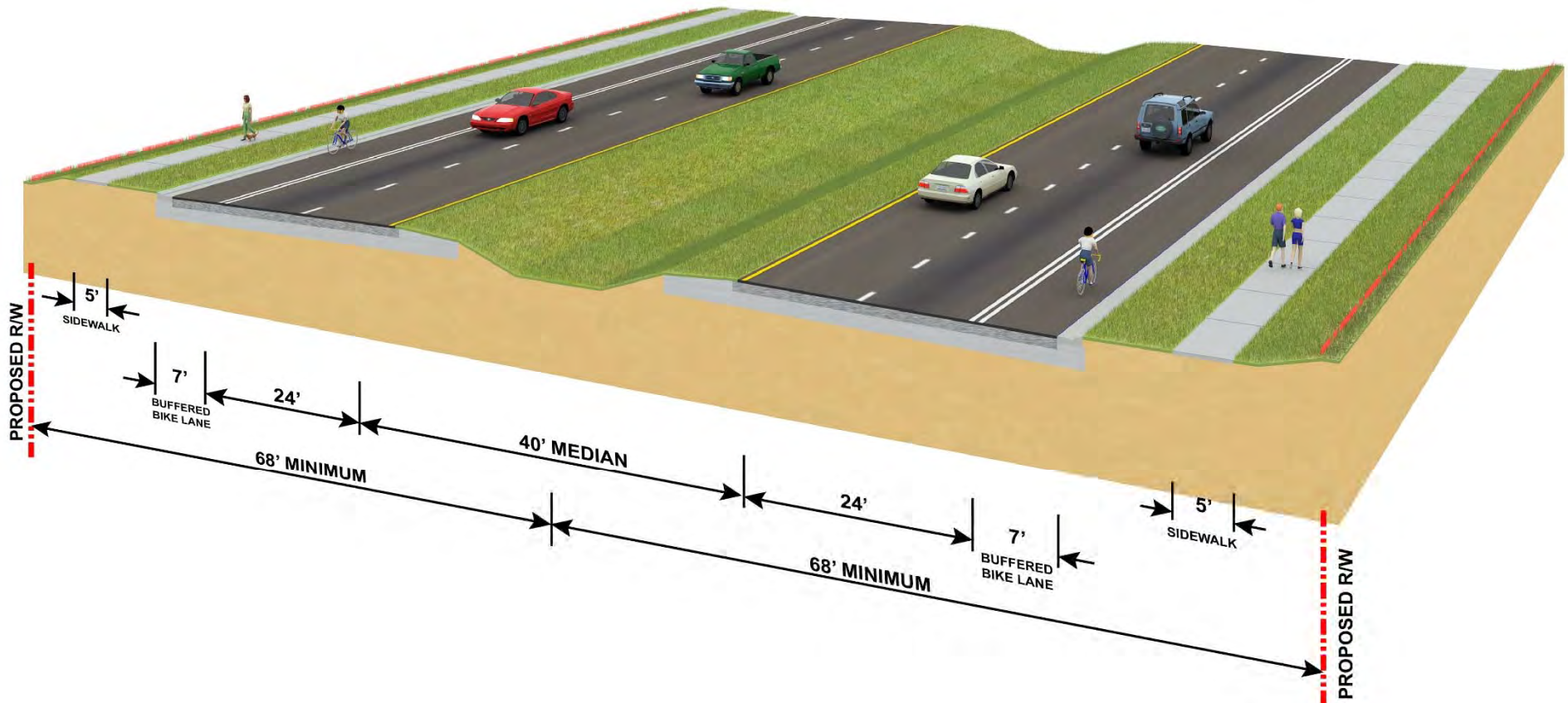


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Design Schedule



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RE-EVALUATION

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- **FDOT Project Manager**
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Appendix B

Newsletters



SR 600 (US 92) Project Development & Environment Re-evaluation

From I-4 to County Line Road
Hillsborough County | WPI Segment No.: 435749-1

July 2015

JOIN THE CONVERSATION!

We want your comments and suggestions.
Now is the time to get involved!

One of the most important aspects of a PD&E study is public involvement. Your input is important to the success of the project and opportunities to provide input will be available throughout the study.

This newsletter is being circulated to those who have contacted us regarding the project and to all property owners adjacent to the study area. If you are not currently on our mailing list and would like to be included, please contact:

Stephanie Pierce
FDOT Project Manager
stephanie.pierce@dot.state.fl.us
813-975-6445
800-226-7220

— Or —

for Media Inquiries

Kris Carson
Public Information Office
kristen.carson@dot.state.fl.us
813-975-6202
800-226-7220

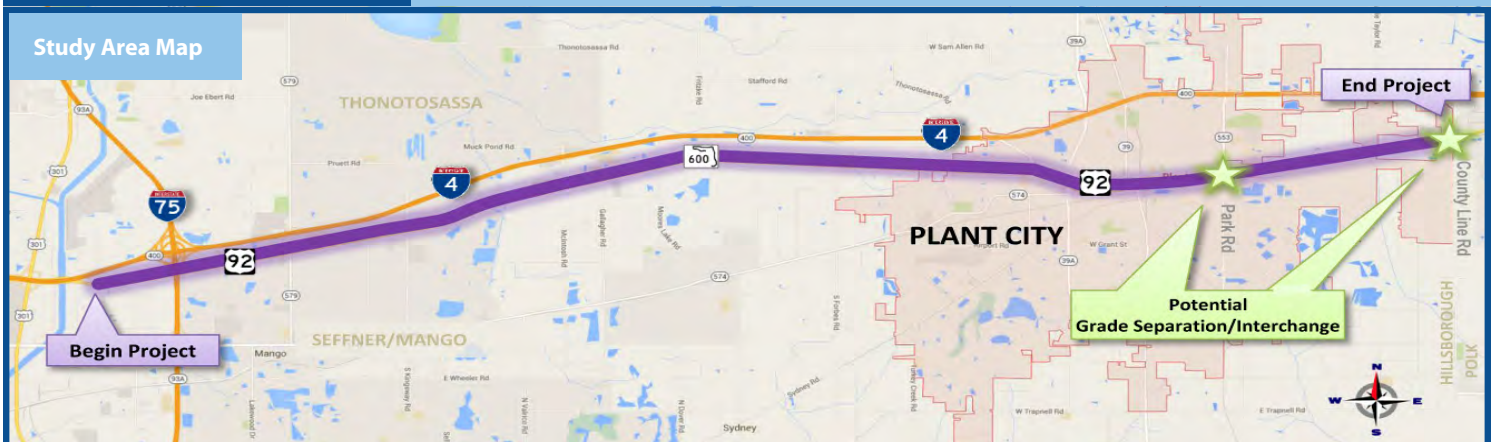
RE-EVALUATION BEGINS!

The Florida Department of Transportation (FDOT), District Seven has begun a Project Development & Environment (PD&E) Re-evaluation for **SR 600 (US 92) from east of I-4 to east of County Line Road** in Hillsborough County, a distance of approximately 18 miles (see study area map below).

The re-evaluation, which began in January 2015, will consider a full range of alternatives for improving mobility and safety along US 92, including adding traffic lanes, adding turn lanes in key locations and improving bicycle and pedestrian access. A feasibility analysis of providing grade separation/interchanges at Park Road and County Line Road will also be conducted.

What is a Re-evaluation?

The purpose of this re-evaluation is to update the original PD&E environmental document completed in 1993 and approved by the Federal Highway Administration on March 24, 1994. A re-evaluation is the process used to document compliance with new federal laws and to identify any changes that may have occurred on a project since the approval of the original environmental document. A re-evaluation is used to update the earlier study, document changes in the design concept, reassess environmental impacts, incorporate commitments, and to maintain eligibility for future federal funding before the project advances to the next phase of project development. The re-evaluation will also determine if new alternative options exist and compare them to the previously approved improvements.



SR 600 (US 92) Project Development & Environment Re-evaluation

FDOT Adopted Five Year Work Program FY 2015-2020

SR 600 (US 92) from Kingsway Rd. to McIntosh Rd. is funded for design in FY 2016 and SR 600 (US 92) from McIntosh Rd. to SR 566 is funded for design in FY 2016. No other portions of SR 600 (US 92) within the project limits, including this segment, are funded for design, right-of-way acquisition or construction at this time.

Para Preguntas En Español

Si usted tiene preguntas o comentarios o si simplemente desea más información sobre este en Español, favor de ponerse en contacto con la señora Elba Lopez al telefono 813-975-6403 o correo electronico elba.lopez@dot.state.fl.us.


Project Website

If you would like to learn more about this study or submit a comment, please visit the project website at:

<http://active.fdotd7studies.com/sr600/i4-to-county-line/>

Project Schedule

The SR 600 (US 92) re-evaluation began in January 2015. Data collection is currently underway. A public hearing is scheduled for Spring 2016. Additional public meetings may be held throughout the duration of the study as engineering and environmental analyses are completed. The re-evaluation is anticipated to be completed by Summer 2016.

TASKS	2015						2016			
	Jan	Mar	May	July	Sept	Nov	Jan	Mar	May	July
Re-evaluation Begins										
Data Collection										
Engineering & Environmental Analysis										
Public Hearing										
Study Complete										

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation services (free of charge) should contact Lee Royal by calling 813-975-6427 or by email to: lee.royal@dot.state.fl.us.



Florida Department of Transportation
District Seven
 11201 N. McKinley Drive MS 7-500
 Tampa, FL 33612-6456

FDOT Adopted Five Year Work Program FY 2017-2021

US 92 (SR 600) from US 301 to CR 579 is funded for design in FY 2017 and US 92 (SR 600) from Park Road to County Line Road is funded for design in FY 2017. The right-of-way and construction phases for these segments are not currently funded in FDOT's Adopted Five-Year Work Program, but they are included in the Hillsborough MPO Imagine 2040 Long Range Transportation (LRTP) Plan Cost Feasible Plan. There is no current or future funding programmed for the segment between Mango Road and Mobley Street.

Funding Summary: Garden Lane to Mango Road		
Phase	Time Frame (Fiscal Year)	Est. Cost
Preliminary Engineering (Final Design) ⁽¹⁾	2017	\$1,250,000
Right-of-Way ⁽²⁾	2026-2030	\$21,570,000
Construction ⁽²⁾	2026-2030	\$43,140,000
Total		\$65,960,000
Funding Summary: Mango Road to Mobley Street		
Phase	Time Frame (Fiscal Year)	Est. Cost
Preliminary Engineering (Final Design) ⁽³⁾	Not Currently Funded	\$9,150,000
Right-of-Way ⁽³⁾	Not Currently Funded	\$100,431,300
Construction ⁽³⁾	Not Currently Funded	\$91,500,000
Total		\$201,081,300
Funding Summary: Park Road to County Line Road Segment		
Phase	Time Frame (Fiscal Year)	Est. Cost
Preliminary Engineering (Final Design) ⁽¹⁾	2017	\$1,800,000
Right-of-Way ⁽²⁾	2021-2025	\$19,380,000
Construction ⁽²⁾	2021-2025	\$38,780,000
Total		\$59,960,000

(1) Hillsborough County MPO TIP for FY 2016/17 to 2020/21

(2) Hillsborough County MPO Imagine 2040 LRTP, Cost Feasible Plan

(3) FDOT Long Range Estimates (LRE) program

Comuniquese con Nosotros

Nos importa mucho la opinión del público sobre el proyecto. Si tiene preguntas o comentarios, o simplemente desea más información, por favor comuníquese con nosotros. Nuestra representante en español es: **Lilliam Escalera**, (813) 975-6445, Departamento de Transporte de la Florida – Distrito 7.

Right-of-Way Acquisition (ROW) Procedure

We understand that when a transportation project proposes the acquisition of private property, you may have questions and concerns. To better educate and inform you about the right-of-way acquisition process and your rights, the department has created real estate acquisition and relocation brochures. These brochures and other education materials will be available at the public hearing. Copies of the brochures may also be found on our website: <http://www.dot.state.fl.us/rightofway/Documents.shtm>. We are interested in hearing your concerns and answering your questions. We also encourage you to speak with the departments' Project Manager or a Right-of-Way Representative at your convenience.

Non-Discrimination Laws

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation services (free of charge) should contact Christopher Speese, Public Involvement Coordinator, at (813) 975-6405, or by email to: christopher.speese@dot.state.fl.us at least seven (7) days before the public hearing.



US 92 (SR 600)

From East of I-4 to East of County Line Road
 Project Development and Environment (PD&E) Study Re-evaluation

WPI Segment No: 435749-1 | Hillsborough County, Florida | November 2016

The Public Hearing is being held at the following locations:

Public Hearing Session 1 (East)

Date: December 1, 2016
Place: HCC Trinkle Center
 1206 N. Park Road
 Plant City, FL 33563
Time: 5:30 pm-7:30 pm | Open House
 6:30 pm | Formal Presentation

Public Hearing Session 2 (West)

Date: December 6, 2016
Place: Sheraton Tampa East Hotel
 10221 Princess Palm Avenue
 Tampa, FL 33610
Time: 6:00 pm-8:00 pm | Open House
 7:00 pm | Formal Presentation

WE WANT YOUR INPUT!

A successful project depends on the public's participation in the project's development.

To provide comments, ask questions, and make suggestions about the project contact:

Lilliam Escalera
 Project Manager

813-975-6445 · 800-226-7220

— Or —

Kris Carson
 Public Information

813-975-6202 · 800-226-7220

Send written comments to:
Kirk Bogen, P.E.
 Environmental Management Engineer

Florida Department of Transportation
 District Seven
 11201 N. McKinley Drive
 MS 7-500
 Tampa, FL 33612-6456

Email comments to:
lilliam.escalera@dot.state.fl.us

You may submit written comments or other exhibits, in place of or in addition to oral comments, at the hearing or by mailing your comments to the address preprinted on the back of the attached comment form or enter them on the project website at:

<http://active.fdotd7studies.com/sr600/i4-to-county-line/>

All comments or other exhibits must be postmarked no later than **Monday, December 19, 2016** to become part of the official public hearing record.

Dear Property Owner or Interested Citizen:

You are invited to attend and participate in the Florida Department of Transportation (FDOT), District Seven public hearing for a Project Development and Environment (PD&E) Study Re-evaluation for proposed improvements to US 92 (SR 600) in Hillsborough County, Florida. This public hearing is being held to allow interested persons an opportunity to provide comments concerning the location, conceptual design, and social, economic, and environmental effects of widening US 92 (SR 600) from east of I-4 to east of County Line Road, a distance of approximately 18.1 miles.

This letter serves as notice to property owners (pursuant to F.S.339.155) that all or a portion of their property is within 300 feet of the edge of right-of-way of the proposed project. However, this does not mean that all properties will be directly affected.



The public hearing will be held in two sessions (two different days/two different locations). Please note the time differences as well. The same information will be shown at both sessions, so feel free to attend the session that is most convenient to you. Department representatives will be available at the public hearing beginning at 5:30 pm on December 1st (6:00 pm on December 6th) to answer questions and discuss the project informally. Draft project documents and other project related materials will be displayed, and a PowerPoint presentation will run continuously during the open house. FDOT representatives will begin the formal portion of the hearing at 6:30 pm on December 1st (7:00 pm on December 6th), which will provide an opportunity for attendees to make formal oral public comments. Following the formal portion of the hearing, the informal open house will resume and continue until 7:30 pm on December 1st (8:00 pm on December 6th). A court reporter will be available to receive comments in a one-on-one setting. Persons wishing to submit written statements or other exhibits, in place of or in addition to oral statements, may do so at the hearing or by mailing them to Lilliam Escalera, Project Manager, FDOT, District Seven, 11201 N. McKinley Drive MS 7-500, Tampa, FL 33612-6456, or electronically to the project website at <http://active.fdotd7studies.com/sr600/i4-to-county-line/>. All exhibits or statements must be postmarked or emailed no later than **Monday, December 19, 2016** to become part of the official public hearing record.

If you have questions about the project or the scheduled hearing, please contact:

Lilliam Escalera, Project Manager
 (813) 975-6445
lilliam.escalera@dot.state.fl.us

Sincerely,

Kirk Bogen, P.E.

Environmental Management Engineer

Draft project documents will be available for public review at the following locations from **Tuesday, November 8, 2016 to Monday, December 19, 2016**

Seffner-Mango Branch Library
 410 N. Kingsway Road
 Seffner, FL 33584
 Mon 10 a.m. - 8 p.m.
 Tues 12 p.m. - 8 p.m.
 Wed-Sat 10 a.m. - 6 p.m.
 Sun Closed

Bruton Memorial Library
 302 W. McLendon Street
 Plant City, FL 33563
 Mon-Thur 10 a.m. - 9 p.m.
 Fri 10 a.m. - 6 p.m.
 Sat 10 a.m. - 5 p.m.
 Sun 1 p.m. - 5 p.m.

FDOT District Seven
 11201 N. McKinley Drive
 Tampa, FL 33612
 Mon-Fri 8 a.m. - 5 p.m.

What is a PD&E Study Re-evaluation?

A **re-evaluation** is the process used to document compliance with federal laws and to identify any changes that may have occurred since the approval of the original environmental document. The original PD&E study for this portion of US 92 (SR 600) was approved on March 24, 1994. However, upon completion, the study was put on hold and not advanced to the next phase of development.

This re-evaluation is needed to update the 1994 study, documenting changes in the design standards, reassessing socio-economic and environmental impacts, and comparing any new alternative options with the previously approved roadway improvement.

Project Location & Existing Conditions

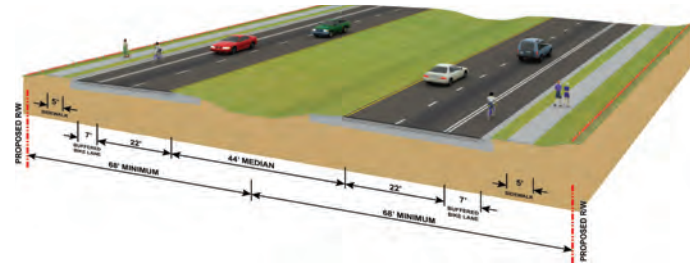
The FDOT is conducting this PD&E re-evaluation to improve mobility and safety along US 92 from east of I-4 to east of County Line Road, a distance of approximately 18.1 miles. This section of US 92 (SR 600) is currently a two-lane, undivided roadway with two 12-foot travel lanes and 4-foot outside paved shoulders. Sidewalks in the area are intermittent. Turn lanes have been provided at certain locations.

Project Purpose and Need

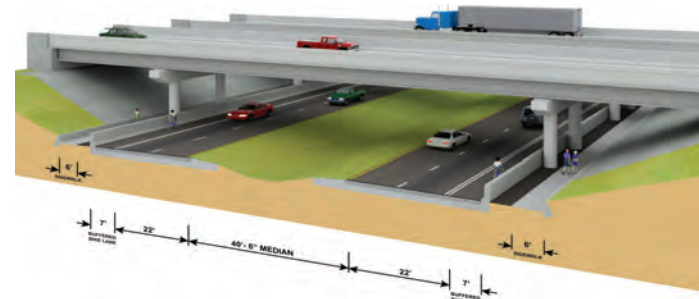
US 92 is a major east-west arterial of regional significance that parallels Interstate I-4 and SR 574 (E Dr. Martin Luther King Jr. Blvd) in Hillsborough County. Within the study area, US 92 plays a significant role in connecting eastern Hillsborough County to the Tampa Bay region. The purpose of the proposed project is to accommodate future traffic growth and provide for faster emergency response and evacuation times. This corridor is projected to operate at level of service (LOS) F in the design year (2040) if no increase in capacity is provided. Other factors which support the need for the project include: regional connectivity, safety, consistency with transportation plans, emergency evacuation, and modal interrelationships.

Recommended Build Alternative

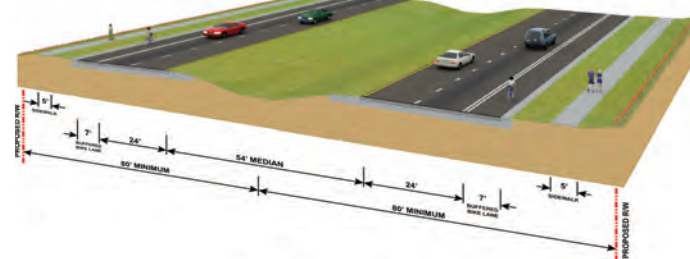
Recommended improvements include widening the existing roadway to four lanes (except for Mobley Street to Maryland Avenue), adding paved shoulders and improving sidewalks. Bicycle facilities and transit accommodations will be considered as part of the project. Proposed roadway typical sections include rural, suburban, and urban typical sections. The project was divided into 11 evaluation segments based on changes in land use and the proposed typical section in comparison with the land use and typical sections from the original PD&E Study. Five different typical section alternatives were recommended for the project in addition to the No-Build Alternative which was recommended for the project segment from Mobley Street to Maryland Avenue. The five typical section alternatives are listed below and the proposed action for each segment is described in the table on the next page.



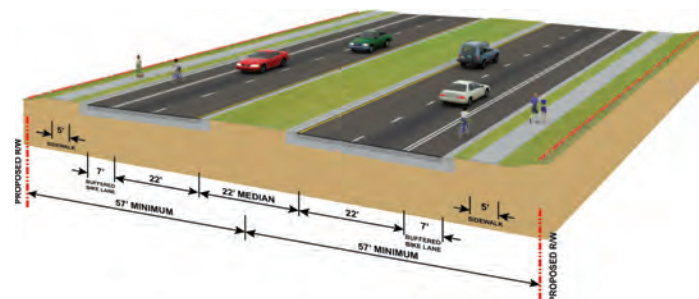
Typical Section 1 - is a suburban roadway with two 11-foot travel lanes and a seven-foot buffered bike lane in each direction. This typical section requires a minimum of 136 feet of right-of-way and complies with the FDOT minimum 45 mph design speed.



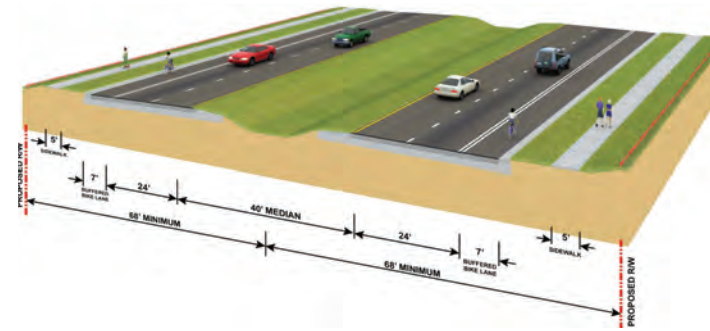
Typical Section 2 - is a suburban roadway with two 11-foot travel lanes and a seven-foot buffered bike lane in each direction. This typical section complies with the FDOT minimum 45 mph design speed but would require a design variation if the roadway is expanded to six lanes in the future.



Typical Section 3 - is a high speed suburban roadway with two 12-foot travel lanes and a seven-foot buffered bike lane in each direction. This typical section requires a minimum of 160 feet of right-of-way and complies with the FDOT minimum 50 mph design speed.



Typical Section 4 - is an urban roadway with two 11-foot travel lanes and a seven-foot buffered bike lane in each direction. This typical section requires a minimum of 114 feet of right-of-way and complies with the FDOT minimum 45 mph design speed.



Typical Section 5 - is a high speed suburban roadway with two 12-foot travel lanes, a five-foot sidewalk, and a seven-foot buffered bike lane in each direction. The travel lanes are separated by a 40-foot median with eight-foot inside shoulders with a 50 mph design speed.

Project Impacts

The Recommended Build Alternative will meet future traffic needs and have minimal impact on the environment. Three resources within the Build Segments have been identified as being eligible for listing in the National Register of Historic Places. They are the Polk County Obelisk, Camp Knox Tourist Court Resource Group, and the Tomlin Middle School Resource Group. None of these are anticipated to be impacted. The hearing process will provide the hearing process participants an opportunity to comment on effects to these resources. Details of all project impacts separated by segment will be displayed at the public hearing.

Evaluation Segment	Portion of Segment	Build Typical Section	Build Alignment (Shifted North, Shifted South, or Centered)
1	Garden Lane to West of I-75 & East of I-75 to West of Mango Road	Typical Section 1	Garden Ln. to Falkenburg Rd.: North Falkenburg Rd. to Mango Road: South
	Just West of I-75 to Just East of I-75	Typical Section 2	
2	West of Mango Road to East of Mango Road	Typical Section 1	Mango Road Intersection: North
3	East of Mango Road to North Parsons Avenue	Typical Section 1	South
4	North Parsons Avenue to East of Crow Wing Drive	Typical Section 1	North
5	East of Crow Wing Drive to Castlewood Road	Typical Section 3	North
6	Castlewood Road to West of Gallagher Road	Typical Section 3	South
7	West of Gallagher Road to Lynn Oaks Circle	Typical Section 3	South
8	Lynn Oaks Circle to East of Bethlehem Road	Typical Section 3	Centered
9	East of Bethlehem Road to Edwards Street	Typical Section 3	E. of Bethlehem Rd. to Turkey Creek Rd.: North Turkey Creek Rd. to Woodrow Wilson St.: North
	Edwards Street to Mobley Road	Typical Section 4	Woodrow Wilson St. to Mobley St.: Centered
10	Mobley Street to Maryland Avenue	No-Build	No-Build
11	West of Park Road to County Line Road	Typical Section 5	North

No-Build Alternative

The Recommended Build Alternative applies to the entire project length except for a segment from Mobley Street to Maryland Avenue. The No-Build Alternative applies to this segment. This No-Build Alternative was established in the original PD&E Study completed in 1994. The original PD&E Study recommended that the section of Baker Street, between Mobley Street and Whitehall Street, be converted from a rural roadway to an urban roadway in order to provide sidewalks and drainage enhancements.

These improvements were completed for this section and met the intent of the original PD&E Study recommendation. This re-evaluation concludes that this segment, Mobley Street to Maryland Avenue, will remain a No-Build Alternative. The No-Build Alternative assumes that no further improvements will be made to this segment of US 92 through the year 2040 except for routine maintenance and safety enhancements as required.



US 92 (SR 600)

From East of I-4 to East of County Line Road
Project Development and Environment (PD&E) Study Re-evaluation
WPI Segment No: 435749-1 | Hillsborough County, Florida



PUBLIC HEARING SESSION 2

Public Hearing Session 2 Directions:

Sheraton Tampa East Hotel | 10221 Princess Palm Avenue, Tampa, FL 33610 | 6:00 pm-8:00 pm

- **From Interstate 75**

From I-75 take Martin Luther King, Jr. Boulevard Exit, proceed west to Falkenburg Road, turn left onto Falkenburg Road, then turn left onto Princess Palm Avenue and proceed to the hotel.

- **From Broadway Ave**

Head east on E Broadway Avenue toward Queen Palm Drive, turn left onto N Falkenburg Road, then turn right at the 2nd cross street onto Princess Palm Avenue and proceed to the hotel.

- **From US 92**

Head west on US 92 toward N Falkenburg Road, turn left onto N Falkenburg Road, turn left onto FL 574 E/ E Dr. Martin Luther King, Jr. Boulevard, turn right onto Queen Palm Drive, then turn left onto Princess Palm Avenue and proceed to the hotel.

- **From Mango Road**

Head south on Mango Road, turn right onto FL 574 W/E Dr. Martin Luther King, Jr. Boulevard, turn left onto Queen Palm Drive, then turn left onto Princess Palm Avenue and proceed to the hotel.



US 92 (SR 600) PD&E STUDY RE-EVALUATION

From East of I-4 to East of County Line Road

Hillsborough County, Florida

WPI Segment Number 435749-1

Public Hearing Comment Form

We encourage your comments regarding this project

Multiple horizontal lines for writing comments.

Name (Print): _____

Address: _____

City, State, Zip: _____

Email: _____

PUBLIC HEARING SESSION ATTENDED:

- Session 1**
Thursday, December 1, 2016
HCC Trinkle Center
- Session 2**
Tuesday, December 6, 2016
Sheraton Tampa East Hotel

If you did not receive notice of the public hearing but would like to be included on the mailing list for this project, please check.

NOTE: Please complete and place in the "Comments" box or mail Kirk Bogen, P.E., at the address on the back of this comment form. All comments postmarked by **Monday, December 19, 2016** will become part of the public hearing record and are available for viewing by the public and the media.

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation services (free of charge) should contact Christopher Speese, Public Involvement Coordinator, at 813-975-6405 or 800-226-7220 at least seven working days in advance of the Public Hearing.

Fold

Place
Stamp
Here

**Kirk Bogen, PE, Environmental Management Engineer
Florida Department of Transportation, District 7
MS 7-500
11201 N. McKinley Drive
Tampa, Florida 33612-6456**

Fold

Appendix C

Public Hearing Transcript

PUBLIC HEARING TRANSCRIPT CERTIFICATION

I hereby certify that on December 1, 2016, beginning at 5:30 p.m., and on December 6, 2016, beginning at 6:00 p.m., I presided over a Public Hearing for the following project:

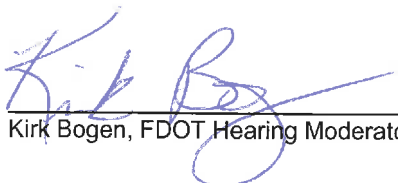
US 92 (SR 600)

**Project Development and Environment (PD&E) Study Reevaluation
from east of I-4 to East of County Line Road**

Hillsborough County, Florida

Financial Project ID: 435749-1

I further certify that the subject public hearing was conducted relative to the economic and social effects of the location and design concept for the subject project and its impact on the environment, that a transcript was made and the document attached herein is a full, true, and complete transcript of what was said at the hearing, and that the Florida Department of Transportation has considered the social, economic, and environmental effects of the proposed improvement and is of the opinion that it is properly located and should be constructed.


Kirk Bogen, FDOT Hearing Moderator

3/14/17
(Date)

PUBLIC HEARING TRANSCRIPT CERTIFICATION

I hereby certify that on December 1, 2016, beginning at 5:30 p.m., and on December 6, 2016, beginning at 6:00 p.m., I presided over a Public Hearing for the following project:

US 92 (SR 600)

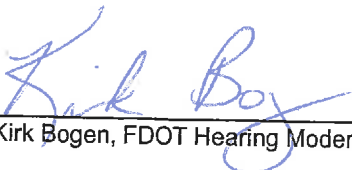
Project Development and Environment (PD&E) Study Reevaluation

from east of I-4 to East of County Line Road

Hillsborough County, Florida

Financial Project ID: 435749-1

I further certify that the subject public hearing was conducted relative to the economic and social effects of the location and design concept for the subject project and its impact on the environment, that a transcript was made and the document attached herein is a full, true, and complete transcript of what was said at the hearing, and that the Florida Department of Transportation has considered the social, economic, and environmental effects of the proposed improvement and is of the opinion that it is properly located and should be constructed.


Kirk Bogen, FDOT Hearing Moderator

3/14/17
(Date)



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PUBLIC HEARING

US 92 (SR 600) PD&E STUDY REEVALUATION

DATE: Thursday, December 1, 2016
TIME: 5:30 p.m. - 7:30 p.m.

PLACE: Trinkle Center
1206 North Park Road
Plant City, Florida

- and -

DATE: Tuesday, December 6, 2016
TIME: 6:00 p.m. - 8:00 p.m.

PLACE: Sheraton Tampa East Hotel
10221 Princess Palm Avenue
Tampa, Florida

REPORTED BY: CATHY J. JOHNSON MESSINA, RMR, FPR
REGISTERED MERIT REPORTER
FLORIDA PROFESSIONAL REPORTER
NOTARY PUBLIC, STATE OF FLORIDA

I N D E X

SESSION #1:

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Computer-Aided Transcription

1 (Session #1)

2 MR. BOGEN: Good evening. Today is Thursday,
3 December 1, 2016, and it is approximately 6:30 p.m.
4 We are assembled at Trinkle Center located at 1206 North
5 Park Road in Plant City, Florida.

6 Welcome to the Public Hearing for the reevaluation
7 of the US 92 (State Road 600) Project Development and
8 Environment Study, or PD&E from east of I-4 to east of
9 County Line Road.

10 My name is Kirk Bogen and I am the Environmental
11 Management Engineer for District Seven of the Florida
12 Department of Transportation.

13 This public hearing is being held relative to Work
14 Program Item Segment Number 435749-1. We are conducting
15 the hearing this evening to provide you an opportunity to
16 discuss the project and to submit comments on the PD&E
17 study reevaluation.

18 This public hearing is being held in accordance
19 with applicable federal and state laws and public
20 participation is encouraged and solicited without regard
21 to race, color, religion, sex, age, national origin,
22 disability or family status.

23 This hearing was advertised consistent with federal
24 and state requirements and is being conducted in
25 accordance with the Americans With Disabilities Act of

1 1990. This information is provided in the project
2 brochure and can be found at the sign-in table as well.

3 This public hearing is being conducted in two
4 sessions at two separate locations. Both sessions will
5 be combined into a single public hearing record for the
6 PD&E study.

7 The first session tonight is the 1st day of
8 December, 2016, from 5:30 p.m. to 7:30 p.m., at Trinkle
9 Center located at 1206 North Park Road in Plant City,
10 Florida.

11 The second session will be held on Tuesday,
12 December 6, 2016, from 6:00 p.m. to 8:00 p.m., at the
13 Sheraton Tampa East Hotel located at 10221 Princess Palm
14 Avenue, Tampa, Florida.

15 This is your opportunity to receive information on
16 the US 92 (State Road 600) PD&E study reevaluation and
17 officially comment on the Department's recommended build
18 alternative and other documents available here tonight.

19 Re evaluations of originally approved PD&E studies
20 are conducted for several different instances. Due to a
21 substantial time lapse since the original PD&E study's
22 approval in 1994, the project team looked at its approved
23 build alternative with respect to changes in policies,
24 laws and regulations, land uses, and design standards.
25 The team updated the original PD&E study's preferred

1 build alternative and estimated the new costs and impacts
2 as a result.

3 The recommended build alternative shown at
4 tonight's public hearing is based on comprehensive
5 environmental and engineering analyses completed to date,
6 as well as on public comments that have been received
7 throughout the duration of the reevaluation study.

8 I will now explain information about the proposed
9 widening of US 92 (State Road 600) from east of I-4 to
10 east of County Line Road.

11 The recommended build alternative consists of
12 widening US 92 (State Road 600) through the project
13 limits in Hillsborough County with the exception of a
14 segment between Mobley Street to Park Road in Plant
15 City.

16 US 92 is a major east-west roadway through
17 Hillsborough County and plays a significant role in
18 connecting eastern Hillsborough County to the Tampa Bay
19 region.

20 The proposed project includes widening the existing
21 roadway from two to four lanes (except through downtown
22 Plant City) adding paved shoulders to the travel lanes,
23 and improving sidewalk connectivity.

24 The project was divided into 11 evaluation segments
25 based on changes in land use and typical sections when

1 compared to the original PD&E study.

2 There are four major typical sections proposed for
3 the recommended build alternative that include urban and
4 suburban roadways. The typical sections consist of two
5 11-foot or 12-foot lanes in each direction, curb and
6 gutter, bike lanes, sidewalks, and grassed medians that
7 vary between 22 feet to 54 feet. The length of the
8 proposed project is approximately 18 miles. This
9 widening is intended to accommodate future traffic
10 growth, provide faster emergency response and evaluation
11 times, and improve safety and mobility for vehicles,
12 bicyclists and pedestrians.

13 The no-build alternative applies to the segment
14 from Mobley Street to Maryland Avenue. The original PD&E
15 study recommended that the section of Baker Street
16 between Mobley Street and Whitehall Street be converted
17 from a rural roadway to an urban roadway in order to
18 provide sidewalks and drainage enhancements. The
19 sidewalk and drainage enhancements were completed
20 previously for this section and met the intent of the
21 original PD&E study recommendation.

22 This PD&E study reevaluation of US 92 concludes
23 that the remaining part of this segment will remain a
24 no-build alternative. The no-build alternative involves
25 foregoing major improvements to the existing roadway and

1 provides only routine maintenance, pavement resurfacing,
2 and safety enhancements as required.

3 During the PD&E study reevaluation, specialists
4 conducted a cultural resources assessment survey in
5 accordance with Section 106 of the National Historic
6 Preservation Act of 1966. Three resources located within
7 the build segments have been identified as eligible for
8 listing on the National Register of Historic Places.
9 They are the Polk County Obelisk, the Camp Knox Tourist
10 Court Resource Group, and the Tomlin Middle School
11 Resource Group. No new archaeological sites were found,
12 and no new evidence of any previous identified
13 archaeological sites was discovered in the reevaluation.
14 A no effect and a no adverse effect conclusion is
15 anticipated as a result of completing the draft case
16 study report.

17 A draft Section 106 case study report was prepared
18 and is available for review and comment at this hearing.
19 Details of all project impacts are on display tonight.

20 The estimated total cost of this project is
21 approximately \$399 million in 2016 dollars. The total
22 cost includes design, right of way, acquisition,
23 mitigation, construction, and construction inspection.

24 Now I am going to give you some information about
25 right-of-way acquisition and how you can make comments on

1 the project.

2 On transportation projects such as this, one of the
3 unavoidable consequences is the necessary acquisition of
4 properties and the relocation of families and businesses.
5 On this project, we anticipate the relocation of 230
6 properties, 120 residential locations, and 110 business
7 relocations.

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9 property is to be acquired, the acquisition will be done
10 in accordance with all applicable eminent domain law.
11 If a project requires people and/or businesses to
12 relocate, the relocation process will be done in
13 accordance with the federal Uniform Relocation Act.

14 Informational brochures describing both the
15 acquisition and the relocation process are available
16 this evening at the right-of-way table.

17 In addition to the printed material, experienced
18 right-of-way specialists who are familiar with the
19 acquisition and relocation process are available this
20 evening to answer your questions.

21 Jackie Fernandez and Zenia Gallo, if you would
22 please stand so that anyone who is an affected property
23 owner, displaced resident, or displaced business will
24 know who to see to discuss these issues.

25 Thank you.

1 Before I continue, I would like to recognize any
2 elected officials or their representatives who are here
3 tonight. I ask that you please stand and introduce
4 yourself for the record.

5 When you arrived this evening, you should have
6 received an informational newsletter and a comment form.
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18 second session scheduled for Tuesday December 6th, 2016,
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21 them to the Department staff member. If you have not
22 received a speaker's card and wish to speak, please raise
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25 portion of the hearing, you can also make a comment after

1 the presentation to the court reporter who is here
2 tonight.

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4 in writing. Comment forms can be placed in one of the
5 comment boxes this evening, or you can complete the form
6 at a later date and mail it to us at the preprinted
7 address on the back of the sheet.

8 You can also email comments to us at the project
9 website found on the front of the handout. Please keep
10 in mind that comments must be postmarked or emailed no
11 later than Monday, December 19, 2016, to be included in
12 the official public hearing record.

13 At this time, we will begin taking public comments.
14 I will call each speaker in the order in which their
15 request was received.

16 Please limit your comments to the US 92 PD&E study
17 reevaluation and keep them to three minutes. This is in
18 order to allow everyone an opportunity to speak. Those
19 who wish to provide additional comments may return to the
20 microphone following the last speaker, or you may present
21 your additional comments related to the PD&E study
22 directly to the court reporter after the formal session
23 has concluded.

24 As I call your name, please step to the microphone
25 and state your name and address clearly into the

1 microphone before making your comment. If you represent
2 an organization, municipality or other public agency,
3 please provide that information as well.

4 The first speaker is Xiomara Meeks.

5 MS. MEEKS: Good evening. My name is Xiomara
6 Meeks, and I'm here for Parksdale Farms. Our address is
7 3702 West Baker Street, Plant City.

8 As you know, Parksdale has been a community
9 landmark for almost 60 years. About ten years ago my
10 husband and I moved to Plant City from Boca Raton to
11 carry on his grandpa's legacy, Roy Parks.

12 The existing right-of-way and road in front of the
13 market extends about 75 feet. In the '90s, the DOT had
14 conducted a PD&E study for widening US 92. This is
15 nothing new to us. We're aware this has been going on.
16 That study showed a much smaller cross section than the
17 one that's shown on the plans in the other room.

18 Initially, we had thought that on the previous
19 cross section we'd only lose our front spots. With the
20 new cross section, it would completely eliminate all of
21 our business space.

22 Another point I want to make is if you think of the
23 intersection of State Road 60 and Polk County Line going
24 towards Polk County, that intersection is still smaller
25 than what you have designed on the plans out there. I

1 believe that that type of road is too big for coming into
2 the city limits of Plant City.

3 So my final comment is I look forward to having
4 further discussions in making the design more feasible
5 for us for Parksdale to stay as an institution and a
6 community member of Plant City.

7 Thank you.

8 MR. BOGEN: Our next speaker is Bob DuBe'.

9 MR. DUBE': I've already made my comment to the
10 reporter.

11 MR. BOGEN: Thank you.

12 Our next speaker is Donald Owen.

13 MR. OWEN: My name is Donald Owen. I'm
14 representing Motel 92, LLC. It's a nine and a half acre
15 piece of property that in 1957 was not zoned. There was
16 no zoning in the area. It was zoned as 150 feet highway
17 commercial from the center line of the existing
18 Highway 92 then.

19 The presentation today indicated several things
20 that are of great concern: Number one, the cross section
21 that was presented in the PowerPoint display showed that
22 the well that supplies all the water for the property,
23 the power supply from TECO Electric. All of the motel's
24 business and RV spaces will be basically destroyed
25 because there will be no power and no water to them.

1 The center line of -- I'm sorry -- the presentation
2 said that there would be -- I thought it said that there
3 would be stop lights at all entrances. I don't know if
4 that was true or not, if I misread that, or misheard that
5 or whatever, but if you don't do that there is no way in
6 the world anybody is going to make a U-turn on
7 Highway 92, whatever direction you're coming from, if
8 you widen it to a four-lane highway.

9 Right now it's two lanes. You can't even get
10 across the street anywhere. From McIntosh Road to Plant
11 City, forget it. There is just too much traffic.

12 So if there is not any consideration there, there
13 needs to be some type of consideration; otherwise, you're
14 going to have a bunch of people killed because of this
15 project. There is just no way to get in and out of your
16 property.

17 And Kirk Bogen, did you say B-O-G-A-N?

18 MR. BOGEN: No, B-O-G-E-N.

19 MR. OWEN: Okay. This project, I guess it was
20 435-749-1, has a noise issue also. Even if the project
21 is not going to be completed until 20 -- what did you
22 say -- 21? Basically, when is the end of the project?

23 MR. BOGEN: Sir, it's not a question and answer --

24 MR. OWEN: Okay. Whenever the project is going to
25 be done, as soon as you guys come through there and wipe

1 out the business and the power supply and the water,
2 we're effectively out of business, period. No if, and,
3 or buts.

4 I have spoken both to Zenia Gallo -- or I have
5 spoken to her. She gave us the information and we had
6 the forms here. We probably are going to make the
7 report.

8 Thank you.

9 MR. BOGEN: Thank you for your time.

10 The next speaker is Rene' Owen.

11 MS. OWEN: I decline.

12 MR. BOGEN: Okay.

13 Our next speaker is Warren Russell.

14 MR. RUSSELL: I decline also.

15 MR. BOGEN: And the next speaker is Mark Riebow.

16 MR. RIEBOW: Riebow. I'm not going to decline.

17 My name is Mark Riebow. I'm with Hollemans Mobile
18 Home Park just past 92. What I'm trying to figure out is
19 you're going to widen the road in sections, so to speak,
20 segments, and you're going to widen starting at Martin's
21 Gardens where you did a widening project five years ago,
22 six years ago, or maybe a little further where you
23 widened it just past 579, which when people come off at
24 work at night it already is backed up tremendously.

25 We've had people killed out in front of our

1 property because of the traffic flow, and now you're
2 going to widen it to four lanes and stop in that area and
3 not carry on, and all of that traffic is going to dump
4 four lanes up from 301, four lanes coming off of I-4, and
5 you're going to create a traffic congestion and nobody
6 seems to be giving this much thought.

7 The second part I'm worried about is the drainage
8 system. When you did it last time -- my front yard
9 floods out when it rains because all the water comes into
10 my yard where they stopped the gutter at my property line
11 for some unusual reason.

12 And I don't understand why we're doing these things
13 and don't think it through. I mean I have to pump my
14 front yard out for hours because all the water comes off
15 and runs in my yard because they stopped the gutters and
16 now you're going to add the traffic. What more? And
17 you're not going to build and do the right thing. I
18 think that section was not being done for a few years.
19 You're jumping to Plant City and leaving that where all
20 of the traffic problems are?

21 Is someone not thinking this through or they just
22 don't care?

23 MR. BOGEN: If you'll speak with me after this
24 formal portion and we can sit down --

25 MR. RIEBOW: Well, we've got phone call after phone

1 call to the FDOT about the draining problem. He had come
2 out and build some stupid speed bump thinking that would
3 stop the water. They won't answer our calls anymore.

4 It should have been done during the initial
5 construction. So I guess -- I'm reading this off -- is
6 anyone thinking this through that we're going to widen
7 the street where all of this traffic comes to and stop
8 where it's going to two lanes again, with two major roads
9 coming through? And we don't care. It's not being
10 thought through very clear where you're stopping and
11 where you're starting.

12 Also, you're destroying businesses. I understand
13 there's going to be some people that will be losing their
14 homes, and hopefully you're going to compensate them,
15 but I don't think it's being thought through some of the
16 segments of what you're doing, and I don't think you've
17 really looked at the traffic flow that comes off of I-75
18 to 92, and the visibility causes wrecks.

19 And so you're going to dump all this traffic down
20 here where there's no visibility or nothing, with the
21 school where they won't even give a light to Wal-Mart who
22 wanted to build something there because they didn't want
23 to stop the traffic, and you're going to clog it up the
24 nose. Something's just not -- I don't know.

25 And the students walk along there. They're just

1 always walking back and forth.

2 Anybody else want to say anything?

3 It's just not -- you're not thinking it through.

4 We want to say that's government, but we're just not
5 thinking it through.

6 MR. BOGEN: That is the last card that I have.

7 Was there anyone else that would like to speak?

8 If you would state your name and address and we'll
9 have you fill out a card afterwards.

10 MR. HERRMAN: Joseph Herrmann, 6011 Highway 92
11 West. I understand the project is stopping at Mobley
12 Street here in Plant City. And that intersection, I'd
13 like to call your attention to there's a school there,
14 people bringing them kids and dropping them off on
15 Mobley, very congested intersection, and you absolutely
16 are not doing anything to improve it, and it just seems
17 like another hundred feet of you all putting in there
18 could put proper turn lanes of sufficient lengths so
19 that moms and dads picking up students could safely turn
20 off 92. I don't know what else you can do. Maybe a
21 traffic light. That's out of the question because
22 they -- a traffic light would be nice, but mainly proper
23 turn lanes, because it's very bad, people going east and
24 west. Turning north or south causes a problem.

25 MR. BOGEN: Thank you. We'll refer that comment to

1 our traffic operations office.

2 MR. QUIRINO: Yes. My name is Miguel Quirino, and
3 I am the pastor of the Iglesia de Dios Torre Fuerte in
4 Dover, Inc., there at 5335 West US Highway 92.

5 I'm just concerned that this will impact our church
6 facilities there where we were at, and I just wanted to
7 know more information, if it will impact our church and
8 our facilities.

9 Thank you.

10 MR. BOGEN: Thank you for your comment.

11 Is there anyone else?

12 MR. BERGERON: Yeah, I'd like to make a comment,
13 actually a couple of them. Louis Bergeron. I own the
14 property at 337 West US 92.

15 Yeah, I noticed out there this was a one of nine
16 step process. Did I miss something? I just found out
17 you guys are in process step number eight.

18 You guys normally send stuff out. I mean I finally
19 got something in the mail the other day about this
20 meeting. Was there anything before this that was sent
21 out?

22 MR. BOGEN: We send out notifications to all
23 property owners at least 21 days before.

24 MR. BERGERON: For these meetings for comment?

25 MR. BOGEN: Yes.

1 MR. BERGERON: Okay. So we only have 21 days. You
2 guys have, what, a year and a half?

3 Okay. Drainage, I notice it's done in sections
4 and I see these nice typical sections here in your
5 brochure. I see these big retention areas and water
6 drainage. Where's that water go when it leaves the new
7 area? It goes into our old, inadequate drainage system
8 out here. Very inadequate.

9 MR. BOGEN: If you would get with me afterwards, I
10 can answer some of those questions.

11 MR. BERGERON: You can't answer them for
12 everybody?

13 MR. BOGEN: This is not a question-and-answer
14 session.

15 MR. BERGERON: Okay. I understand. And I do
16 support better roads and I do support the four-laning,
17 but it needs to be done right and in one nice unison
18 stretch, not all chopped up, or you're just going to
19 destroy everybody's businesses out here.

20 MR. BOGEN: Thank you for your comment.

21 Is there anyone else?

22 Seeing none, the public hearing transcript, written
23 statements and exhibits and reference material will be
24 available for public inspection at the District Seven
25 Office, 11201 North McKinley Drive, Tampa, Florida,

1 within three weeks.

2 Thank you for attending this session and for
3 providing your input into this project.

4 It is approximately 6:57. I hereby officially
5 suspend the formal portion of the public hearing for the
6 US 92 PD&E study reevaluation.

7 This hearing will be continued at the second
8 session on Tuesday, December the 6th, 2016, from 6:00 to
9 8:00 p.m., in Tampa, Florida.

10 Department representatives will be available to
11 answer questions and the materials shown this evening
12 will be on display. You may continue to view the
13 materials on display and speak with our project staff.

14 On behalf of the Florida Department of
15 Transportation, thank you for attending.

16 Remember to be alert today, alive tomorrow. Safety
17 doesn't happen by accident.

18 Good night.
19
20
21
22
23
24
25

C O M M E N T S

1
2 Greg Pilcher
3 RV Clear Coat
4 10716 East US Highway 92
5 Tampa, Florida 33610
6 Phone: 813.626.4487

7 I own the business on the corner of Anna and
8 10716 East Highway 92. It's a RV paint and body shop.
9 We have large 40-foot RVs pulling in and out of there.
10 Right now we only have access through Anna. We have
11 no direct access onto 92.

12 With the proposed plans, I'll only be able to
13 take a left when leaving my property and not a right,
14 and I need to have access to my property. I need to be
15 able to turn left and right.

16 I'm also one of the few businesses, I don't know
17 if there is another business, but doesn't have direct
18 access to 92, and I want to see about possibly getting
19 that done when they widen the road to where you can
20 actually pull off 92 onto my property like the other
21 business is, but then when you pull out of Anna I need
22 to be able to turn right or left.

23 These are huge 40-foot homes towing cars.
24 Sometimes they'll have a 40-foot RV with a car that they
25 pull behind. They're pretty big.

Okay. I just wanted to put that on the record.

1 Teresa Yavit
2 Hammocks of Kingsway
3 437 Down Pine Drive
4 Seffner, Florida 33584
5 Phone: 813.655.3735

6 We are the first house coming in leaving the
7 development and we have a bigger lot and we also have a
8 pool and a spa back there. They want to come in to the
9 middle of the pool. That's sort of my problem.

10 We are the only person that -- to my knowledge, on
11 the pool, on the whole side there. I have terminal
12 cancer. By right, I should be buried already, and I'm
13 concerned about -- my bedroom is right next to the pool
14 area and everything and I'm concerned about my comfort
15 while this is going on.

16 My husband wants to keep my home. I want to go
17 into a nursing home. I don't want him to bother with me
18 with therapy, but my concern is when are they going to do
19 my area and can they work around me with this, you know.
20 Like, I don't know what to do or what my actual complaint
21 is, but that's my concern and my statement.
22
23
24
25

1 Bob Dube'
2 5407 Anna Drive
3 Tampa, Florida 33601
4 Phone: 813.381.7650

4 The entrance or easement, whatever they get, in the
5 drive on the map is undoable. They're going to block the
6 road off.

7 I have a towing company, a repo company, operating
8 24/7 bringing in large semis and stuff over there.
9 They would ruin my business and make it impossible for
10 me to sell it if they block off the ability to go to the
11 east.

12 They want to put a glass thing in front of there
13 and make you go one way a half mile down the road and try
14 to turn around. I cannot do that. It's going to ruin my
15 business and ruin my property values.

16 Yeah, we can't get in with the semis and stuff.
17 That's not a residential anymore. That's part of the
18 I-75 industrial corridor, which means industry, which
19 means big trucks in and out. If they block that off,
20 we're not going to have that.

21 So it's going it ruin everything. I don't know
22 what to say, but I'm against whatever they're doing. I'm
23 against the blocking off Anna Drive and McCloud Avenue.
24 We need to be able to go across the grassy knoll.

25 Okay.

1 (Session #2)

2 MR. BOGEN: Good evening. Today is Tuesday,
3 December 6, 2016, and it is approximately 7:00 p.m.
4 We are assembled at the Shearton Tampa East Hotel located
5 at 10221 Princess Palm Avenue, Tampa, Florida.

6 Welcome to the Public Hearing for the reevaluation
7 of the US 92 (State Road 600) Project Development and
8 Environment Study, or PD&E from east of I-4 to east of
9 County Line Road.

10 My name is Kirk Bogen and I am the Environmental
11 Management Engineer for District Seven of the Florida
12 Department of Transportation.

13 This public hearing is being held relative to Work
14 Program Item Segment Number 435749-1. We are conducting
15 the hearing this evening to provide you an opportunity to
16 discuss the project and to submit comments on the PD&E
17 study reevaluation.

18 This public hearing is being held in accordance
19 with applicable federal and state laws and public
20 participation is encouraged and solicited without regard
21 to race, color, religion, sex, age, national origin,
22 disability or family status.

23 This hearing was advertised consistent with federal
24 and state requirements and is being conducted in
25 accordance with the Americans With Disabilities Act of

1 1990. This information is provided in the project
2 brochure and can be found at the sign-in table as well.

3 This public hearing is being conducted in two
4 sessions at two separate locations. Both sessions will
5 be combined into a single public hearing record for the
6 PD&E study.

7 The first session was held on Thursday, December 1,
8 2016, 5:30 p.m. to 7:30 p.m., at the Trinkle Center
9 located at 1206 North Park Road in Plant City, Florida.

10 The second session is being conducted tonight, on
11 the 6th day of December, 2016, at the Sheraton Tampa
12 East Hotel located at 10221 Princess Palm Avenue, Tampa,
13 Florida.

14 This is your opportunity to receive information on
15 the US 92 (State Road 600) PD&E study reevaluation and
16 officially comment on the Department's recommended build
17 alternative and other documents available here tonight.

18 Reevaluations of originally approved PD&E studies
19 are conducted for several different instances. Due to a
20 substantial time lapse since the original PD&E study's
21 approval in 1994, the project team looked at its approved
22 build alternative with respect to changes in policies,
23 laws and regulations, land uses, and design standards.
24 The team updated the original PD&E study's preferred
25 build alternative and estimated the new costs and impacts

1 as a result.

2 The recommended build alternative shown at
3 tonight's public hearing is based on comprehensive
4 environmental and engineering analyses completed to date,
5 as well as on public comments that have been received
6 throughout the duration of the reevaluation study.

7 I will now explain information about the proposed
8 widening of US 92 (State Road 600) from east of I-4 to
9 east of County Line Road.

10 The recommended build alternative consists of
11 widening US 92 (State Road 600) through the project
12 limits in Hillsborough County with the exception of a
13 segment between Mobley Street to Park Road in Plant
14 City.

15 US 92 is a major east-west roadway through
16 Hillsborough County and plays a significant role in
17 connecting eastern Hillsborough County to the Tampa Bay
18 region.

19 The proposed project includes widening the existing
20 roadway from two to four lanes (except through downtown
21 Plant City) adding paved shoulders to the travel lanes,
22 and improving sidewalk connectivity.

23 The project was divided into 11 evaluation segments
24 based on changes in land use and typical sections when
25 compared to the original PD&E study.

1 There are four major typical sections proposed for
2 the recommended build alternative that include urban and
3 suburban roadways. The typical sections consist of two
4 11-foot or 12-foot lanes in each direction, curb and
5 gutter, bike lanes, sidewalks, and grassed medians that
6 vary between 22 feet to 54 feet. The length of the
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20 approximately \$399 million in 2016 dollars. The total
21 cost includes design, right of way, acquisition,
22 mitigation, construction, and construction inspection.

23 Now I am going to give you some information about
24 right-of-way acquisition and how you can make comments on
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1 On transportation projects such as this, one of the
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3 properties and the relocation of families and businesses.
4 On this project, we anticipate the relocation of 230
5 properties, 120 residential locations, and 110 business
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7 If a project requires that all or part of your
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16 In addition to the printed material, experienced
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18 acquisition and relocation process are available this
19 evening to answer your questions.

20 Pat Berg and Zenia Gallo, if you would please stand
21 so that anyone who is an affected property owner,
22 displaced resident, or displaced business will know who
23 to see to discuss these issues.

24 Thank you.

25 Before I continue, I would like to recognize any

1 elected officials or their representatives who are here
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10 later than Monday, December 19, 2016, to be included in
11 the official public hearing record.

12 At this time, we will begin taking public comments.
13 I will call each speaker in the order in which their
14 request was received.

15 Please limit your comments to the US 92 PD&E study
16 reevaluation and keep them to three minutes in order to
17 allow everyone an opportunity to speak. Those who wish
18 to provide additional comments may return to the
19 microphone following the last speaker, or you may present
20 your additional comments related to the PD&E study
21 directly to the court reporter after the formal session
22 has concluded.

23 As I call your name, please step to the microphone
24 and state your name and address clearly into the
25 microphone before making your comment. If you represent

1 an organization, municipality or other public agency,
2 please provide that information as well.

3 Is there anyone that wishes to speak?

4 Seeing none, the public hearing transcript,
5 written statements and exhibits and reference material
6 will be available for public inspection at the District
7 Seven Office, 11201 North McKinley Drive, Tampa,
8 Florida, within three weeks.

9 It is approximately 7:11. I hereby officially
10 close the formal portion of the public hearing for the US
11 92 PD&E study reevaluation.

12 You may continue to view the materials on display
13 and speak with our project staff.

14 On behalf of the Florida Department of
15 Transportation, thank you for attending.

16 Remember to be alert today, alive tomorrow.
17 Safety doesn't happen by accident.

18 Good night.
19
20
21
22
23
24
25

C O M M E N T S

1
2
3 Greg Moore
4 9816 US 92 East
5 Tampa, Florida 33610
6 Phone: 813.363.3613

7 My concern is when I pull out of my driveway, if I
8 want to go to the east I don't want to have to cross over
9 a grass median or have to go to the west down.

10 I don't know. I don't know how they're planning on
11 wanting me to go to the left out of my driveway is my
12 concern, because we're talking about having a grass
13 median in front of my house that I can only go to the
14 right, according to the map, and I don't want to have to
15 go to the right if I want to go to the left.

16 I don't know what else to say.
17
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21
22
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25

1 Salvatore Socci
2 206 & 208 West Highway 92
3 Seffner, Florida 33584
4 Phone: 813.684.6934

5 I just feel that the roadway, the way it's
6 proposed, is too large for the community. It's an urban
7 roadway in a suburban community.

8 It's going to adversely affect the local residents,
9 businesses and residential properties as well. It's
10 certainly going to impact the business as far as harming
11 the traffic that has access to the businesses.

12 I cannot get -- travel into the property due to the
13 center median being so wide. A roadway similar to the
14 MLK project that is parallel in Seffner would be more
15 suitable to the local community.

16 That's about all I can think of for right now.
17
18
19
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22
23
24
25

1 Patricia Williams
2 319 Down Pine Drive
3 Seffner Florida 33584
4 Phone: 813.720.5566

5 Based on the display up there, it looks like the
6 noise -- the wall is only going to go to a certain home.
7 My home is the last one on the lot.

8 I recommend if they're going to do it that they
9 bring it all the way past the last home, which is Lot 18
10 off of US 92.

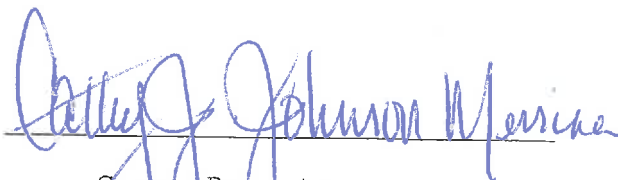
11 That's what I wanted to say.

1 STATE OF FLORIDA
2 COUNTY OF HILLSBOROUGH
3

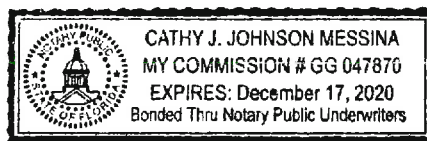
4 I, CATHY J. JOHNSON MESSINA, Registered Merit
5 Reporter, Registered Florida Reporter, and Notary Public
6 in and for the State of Florida at large, hereby certify
7 that the proceedings were recorded in Stenotypy by me and
8 that the foregoing pages constitute a true and correct
9 transcription of my recordings thereof.

10 I FURTHER CERTIFY that I am neither an
11 attorney nor of counsel for the parties to this cause
12 nor a relative or employee of any attorney or party
13 connected with this litigation and that I have no
14 interest in the outcome of this action.

15 WITNESS my hand and seal this 3rd day of
16 January, 2017, at Tampa, Hillsborough County,
17 Florida.

18
19
20 

21 Court Reporter
22 My Commission Expires:



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Appendix D

Public Hearing Comments and Responses

Hall, Justin P.

To: Escalera, Lilliam
Subject: RE: 435749-1: SR 600 (US 92) PD&E Study Re-evaluation Meeting Request

From: Bogen, Kirk [<mailto:Kirk.Bogen@dot.state.fl.us>]
Sent: Monday, May 2, 2016 11:16 AM
To: mrosas1001@mac.com
Cc: Escalera, Lilliam; Adair, Rick; Hull, Alex
Subject: RE: 435749-1: SR 600 (US 92) PD&E Study Re-evaluation Meeting Request

Dear Mr. Rosas:

Thank you for your interest in the US 92 Project Development and Environment (PD&E) Study. We received the questions you submitted through the project website and we offer the following responses.

1. How will this project partner with TBX, Express Bus Service and Rail?

The proposed US 92 multi-laning project would be independent of the TBX, Express Bus Service and Rail projects. However, the Tampa Bay Regional Model on which the need for this project is based takes into account those projects that are in the cost feasible Long Range Transportation Plan (LRTP)

2. Is there any consideration for autonomous vehicles?

Although the use of autonomous vehicles is not precluded, the project does not specifically provide for special features related to autonomous vehicles at this time.

3. How will this project reduce sprawl and encourage high density living areas?

The project's purpose and need is to address future capacity needs due to projected regional population growth. Also, the project is intended to improve safety for motorists and other facility users. The project's implementation is not specifically intended to reduce urban sprawl nor encourage high density urban growth but to address the stated need. The goals you mentioned would be best addressed to Hillsborough County Planning Commission as well as the City of Plant City as they undertake their comprehensive planning activities.

I hope this response has addressed your questions. However, if you require additional information or have any further questions, please do not hesitate to contact me via e-mail at kirk.bogen@dot.state.fl.us or by phone at (813) 975-6448.

Sincerely,

Kirk Bogen, P.E.
Environmental Management Engineer
FDOT District Seven
Planning & Environmental Management Office (PLEMO)
kirk.bogen@dot.state.fl.us
(813) 975-6448 / (800) 226-7220 x6448
FAX: (813) 975-6451

-----Original Message-----

From: Mauricio Rosas [<mailto:mrosas1001@mac.com>]
Sent: Sunday, April 24, 2016 12:55 PM
To: Bogen, Kirk
Subject: 435749-1: SR 600 (US 92) PD&E Study Re-evaluation Meeting Request

To:

Kirk Bogen

Name:
Mauricio Rosas

Email:
mrosas1001@mac.com

Phone:
8137276680

Meeting Details:

Request a Q & A

1. How this project will partner with TBX, Express Bus Service and Rail?
2. Is there any consideration for autonomous vehicles.
3. How will this project reduce sprawl and encourage high density living areas

Sent from (ip address): 96.252.207.60 (96.252.207.60)

Date/Time: April 24, 2016 12:55 pm

Coming from (referer):

<http://active.fdotd7studies.com/sr600/i4-to-county-line/public-involvement/meeting-request/>

Using (user agent): Mozilla/5.0 (iPhone; CPU iPhone OS 9_3_1 like Mac OS X) AppleWebKit/601.1.46 (KHTML, like Gecko) Version/9.0

Mobile/13E238 Safari/601.1



US 92 (SR 600) PD&E STUDY RE-EVALUATION

From East of I-4 to East of County Line Road

Hillsborough County, Florida

WPI Segment Number 45749-1

ROAD PLANNING DIVISION 12/20/16 3:33

Public Hearing Comment Form

We encourage your comments regarding this project

Would like to know more about
 our house 4511 Reola Rd. Is the
 plan to tear it down and when.

Name (Print): Angelo Caltabiano
 Address: 4511 Reola Rd
 City, State, Zip: Dover, FL 33527
 Email: _____

PUBLIC HEARING SESSION ATTENDED:

- Session 1**
 Thursday, December 1, 2016
 HCC Trinkle Center
- Session 2**
 Tuesday, December 6, 2016
 Sheraton Tampa East Hotel

If you did not receive notice of the public hearing but would like to be included on the mailing list for this project, please check.

NOTE: Please complete and place in the "Comments" box or mail Kirk Bogen, P.E., at the address on the back of this comment form. All comments postmarked by **Monday, December 19, 2016** will become part of the public hearing record and are available for viewing by the public and the media.

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Florida Department of Transportation

RICK SCOTT
GOVERNOR

11201 North McKinley Drive
Tampa, FL 33612

JIM BOXOLD
SECRETARY

May 10, 2017

Angelo Caltabiano
4511 Reola Rd.
Dover, FL 33527

Re: WPI Segment No. 435749-1/US 92 PD&E Study Reevaluation – Right of way Acquisition

Dear Angelo Caltabiano,

This letter is in response to your comments about the proposed future widening of US 92, from east of I-4 to east of County Line Road. Specifically, you inquired about the acquisition and demolition of your property.

Following the hearing, the FDOT evaluated your request along with the other comments received during the comment period. Currently this segment of the project corridor is not funded for design, right of way acquisition, or construction based on the FDOT 5-year work program. Of course, once the project receives further funding, impacts to properties along the corridor will be reviewed further during the design phase with the most current survey data to determine the exact location of the roadway improvements.

Thank you for your interest in this reevaluation, and do not hesitate to contact Ms. Lilliam Escalera, Project Manager at lilliam.escalera@dot.state.fl.us or (813) 975-6445 with any future questions or concerns.

Sincerely,

Kirk Bogen, P.E.
Environmental Management Engineer
FDOT District Seven



US 92 (SR 600) PD&E STUDY RE-EVALUATION

From East of I-4 to East of County Line Road

Hillsborough County, Florida

WPI Segment Number 435749-1

Public Hearing Comment Form

We encourage your comments regarding this project

* How will the water run-off from proposed Hwy be addressed? It is already a problem for those of us whose homes are at a lower elevation than the existing Hwy.

* What is the timeline for beginning at our address?

* Will we be offered a buy-out? At 13512 (Next door to Caltabiano - Reola Rd.)

* From Lynn Oaks Circle to East of Bethlehem R. Plan states Build Alignment will be centered that would put sidewalk in our front door if the Typical Section 3 80' minimum is met then 50 mph (minimum) will be flying just feet from our bedroom windows - DANGEROUS - we would request you buy us out

Name (Print): William E Brown

Address: 13512 E US Highway 92

City, State, Zip: Dover, FL 33527

Email:

PUBLIC HEARING SESSION ATTENDED:

Session 1
Thursday, December 1, 2016
HCC Trinkle Center

Session 2
Tuesday, December 6, 2016
Sheraton Tampa East Hotel

If you did not receive notice of the public hearing but would like to be included on the mailing list for this project, please check.

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Florida Department of Transportation

RICK SCOTT
GOVERNOR

11201 North McKinley Drive
Tampa, FL 33612

JIM BOXOLD
SECRETARY

May 10, 2017

William Brown
13512 E US Highway 92
Dover, FL 33527

Re: WPI Segment No. 435749-1/US 92 PD&E Study Reevaluation – Right of way Acquisition

Dear William Brown:

This email is in response to your comments made at the December 1, 2016 public hearing session held for the proposed future widening of US 92, from east of I-4 to east of Count Line Road. Specifically, you inquired about the drainage impact to your property and about the acquisition of your property.

Following the hearing, the FDOT evaluated your request along with the other comments received during the comment period. Currently this segment of the project corridor is not funded for design, right of way, or construction based on the FDOT 5-year work program. Of course, once the proposed project enters the design phase, this plan could be revised to reflect future traffic patterns, crash information, land use conditions as well as the latest design standards. Until the project is funded for right of way acquisition there will not be any properties purchased along the corridor. As far as the drainage is concerned, your home is located in an area where the drainage basins are semi closed with significant relief. During storms you will see water being conveyed downstream through your property. US 92 is a small contributor of runoff compared to the total runoff in the overall basin.

Thank you for your interest in this reevaluation, and do not hesitate to contact Ms. Lilliam Escalera, Project Manager at lilliam.escalera@dot.state.fl.us or (813) 975-6445 with any future questions or concerns.

Sincerely,

Kirk Bogen, P.E.

Environmental Management Engineer
FDOT District Seven



US 92 (SR 600) PD&E STUDY RE-EVALUATION

From East of I-4 to East of County Line Road

Hillsborough County, Florida

WPI Segment Number 435749-1

Public Hearing Comment Form

We encourage your comments regarding this project

My Name is Miguel Quirino. Soy el Pastor de la Iglesia de Dios Torre Fuerte in Dover, Inc. 5335 W. US Hwy 92 Plant City 33566. Mi ~~Preve~~ Preocupacion es el Impacto que hara a la Propiedad de la Iglesia, que sera el Parking el Edificio Existente y el Proyecto de Reconstrucion del Templo en Construcion.

Name (Print): Miguel Quirino
Address: 3706 Moores Lake Rd
City, State, Zip: Dover, FL 33527
Email: quirino41@aol.com

PUBLIC HEARING SESSION ATTENDED:

- Session 1**
Thursday, December 1, 2016
HCC Trinkle Center
- Session 2**
Tuesday, December 6, 2016
Sheraton Tampa East Hotel

If you did not receive notice of the public hearing but would like to be included on the mailing list for this project, please check.

NOTE: Please complete and place in the "Comments" box or mail Kirk Bogen, P.E., at the address on the back of this comment form. All comments postmarked by **Monday, December 19, 2016** will become part of the public hearing record and are available for viewing by the public and the media.

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US 92 (SR 600) PD&E STUDY RE-EVALUATION

From East of I-4 to East of County Line Road

Hillsborough County, Florida

WPI Segment Number 435749-1

Public Hearing Comment Form

We encourage your comments regarding this project

My Name is Miguel Quirino, I am the Pastor of the Iglesia de Dios Torre Fuerte, which is located at 5335 W. US Hwy 92. in Plant City.

I just want to relate the concern of the impact that this project will have, not only to myself but to nearly 100 families that attend our church. We are also in the process of rebuilding for our future Sanctuary. This project will definitely effect the construction of our future Sanctuary.

Name (Print): Miguel Quirino

Address: 3706 Moores Lake Rd

City, State, Zip: Dover, FL 33527

Email: quirino41@aol.com

PUBLIC HEARING SESSION ATTENDED:

- Session 1**
Thursday, December 1, 2016
HCC Trinkle Center
- Session 2**
Tuesday, December 6, 2016
Sheraton Tampa East Hotel

If you did not receive notice of the public hearing but would like to be included on the mailing list for this project, please check.

NOTE: Please complete and place in the "Comments" box or mail Kirk Bogen, P.E., at the address on the back of this comment form. All comments postmarked by **Monday, December 19, 2016** will become part of the public hearing record and are available for viewing by the public and the media.

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Florida Department of Transportation

11201 North McKinley Drive
Tampa, FL 33612

RICK SCOTT
GOVERNOR

JIM BOXOLD
SECRETARY

May 10, 2017

Miguel Quirino - Pastor
Iglesia de Dios Torre Fuente, Inc.
5335 W US Hwy 92
Plant City, FL 33566

Re: WPI Segment No. 435749-1/US 92 PD&E Study Reevaluation – Right of way Acquisition

Dear Miguel Quirino,

This letter is in response to your comments made at the public hearing session held on December 1, 2016 for the proposed future widening of US 92, from east of I-4 to east of County Line Road. Specifically, you inquired about the impact to your property adjacent to US 92.

Following the hearing, the FDOT evaluated your request along with the other comments received during the comment period. Based on the concept displayed at the public hearing, your business is listed as a potential relocation. Currently this segment of the project corridor is not funded for design, right of way acquisition, or construction based on the FDOT 5-year work program. Of course, once the proposed project enters the design phase, this plan could be revised to reflect the most current survey data to determine the exact location of the roadway improvements using the latest design standards. If you have any questions about the right of way acquisition or business relocation process please feel free to contact our right of way acquisition office at (813) 975-6495.

Thank you for your interest in this reevaluation, and do not hesitate to contact Ms. Lilliam Escalera, Project Manager at lilliam.escalera@dot.state.fl.us or (813) 975-6445 with any future questions or concerns.

Sincerely,

Kirk Bogen, P.E.
Environmental Management Engineer
FDOT District Seven



US 92 (SR 600) PD&E STUDY RE-EVALUATION

From East of I-4 to East of County Line Road

Hillsborough County, Florida

WPI Segment Number 435749-1

Public Hearing Comment Form

1. Our drive way was fine for 30+ years until it was resurfaced several years ago. Now every time we get a hard rain we have erosion issues - it washes gullies down our portion of the driveway even though they paved the apron up to our property line. Can this be fixed now and will it be fixed correctly when the new road will be completed.
2. How high will be road be compared to the height that it is now.
3. Will the driveway be angled/sloped so that we can get our travel trailer in and out without major problems.
4. How is this going to affect the traffic Independence Academy at Hwy 92 & McIntosh Roads.
5. Where will the retention ponds be located?



Email: gforte53@gmail.com



Session -
Tuesday, December 6, 2016
Sheraton Tampa East Hotel

- If you did not receive notice of the public hearing but would like to be included on the mailing list for this project, please check.

NOTE: Please complete and place in the "Comments" box or mail Kirk Bogen, P.E., at the address on the back of this comment form. All comments postmarked by **Monday, December 19, 2016** will become part of the public hearing record and are available for viewing by the public and the media.

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George Forte
13526 E US Highway 92
Dover, FL 33527-4002

1. Our drive way was fine for every time we get a hard portion of the driveway. Can this be fixed now and completed.
2. How high will be road be
3. Will the driveway be any without major problems.
4. How is this going to affect Roads.
5. Where will the retention



Email: gforte53@gmail.com

Session: Tuesday, December 6, 2016
Sheraton Tampa East Hotel

If you did not receive notice of the public hearing but would like to be included on the mailing list for this project, please check.

NOTE: Please complete and place in the "Comments" box or mail Kirk Bogen, P.E., at the address on the back of this comment form. All comments postmarked by **Monday, December 19, 2016** will become part of the public hearing record and are available for viewing by the public and the media.

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Hall, Justin P.

To: Escalera, Lilliam
Subject: RE: WPI 435749-1: US 92 FROM I-4 TO COUNTY LINE PD&E Reevaluation- Project Concern

From: Escalera, Lilliam
Sent: Tuesday, May 30, 2017 12:14 PM
To: 'gforte53@gmail.com'
Cc: Bogen, Kirk; Waris, Abdul
Subject: WPI 435749-1: US 92 FROM I-4 TO COUNTY LINE PD&E Reevaluation- Project Concern

Re: WPI Segment No. 435749-1/US 92 (SR 600) PD&E Study Reevaluation – Project Concerns

Dear George Forte:

This email is in response to your comments made at the December 1, 2016 public hearing session held for the proposed future widening of US 92, from east of I-4 to east of Count Line Road. Specifically, you inquired about erosion issues, road height, traffic at Independence Academy, and retention ponds.

Following the hearing, the FDOT evaluated your request along with the other comments received during the comment period. Currently this segment of the project corridor is not funded for design, right of way acquisition, or construction based on the FDOT 5-year work program. Of course, once the proposed project enters the design phase, this plan could be revised to reflect future traffic patterns, crash information, land use conditions as well as the latest design standards. Until the project is funded for right of way acquisition there will not be any properties purchased along the corridor. In addition the vertical alignment and retention pond locations are all issues that will be handled during the design phase when it is funded. As far as the drainage is concerned, your home is located in an area where the drainage basins are semi closed with significant relief. During storms you will see water being conveyed downstream through your property. US 92 is a small contributor of runoff compared to the total runoff in the overall basin.

Thank you for your interest in this reevaluation do not hesitate to contact me at lilliam.escalera@dot.state.fl.us or (813) 975-6445 with any future questions or concerns or you can access the project website for any project update: <http://active.fdotd7studies.com/sr600/garden-lane-to-county-line/>

Sincerely,

Lilliam E. Escalera
EMO Project Manager
FDOT District VII
Planning & Environmental Management Office (PLEMO)



US 92 (SR 600) PD&E STUDY RE-EVALUATION
 From East of I-4 to East of County Line Road
 Hillsborough County, Florida
 WPI Segment Number 435749-1

Public Hearing Comment Form

We encourage your comments regarding this project

When you are taking land
 belonging to an entire community,
 IS there a point of contact to
 address the community and their
 concerns other than this meeting

Name (Print): Cherlene Adewumi

Address: _____

City, State, Zip: _____

Email: Cherlene@premiumsolutionsgrp.com

PUBLIC HEARING SESSION ATTENDED:

- Session 1**
Thursday, December 1, 2016
HCC Trinkle Center
- Session 2**
Tuesday, December 6, 2016
Sheraton Tampa East Hotel

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NOTE: Please complete and place in the "Comments" box or mail Kirk Bogen, P.E., at the address on the back of this comment form. All comments postmarked by **Monday, December 19, 2016** will become part of the public hearing record and are available for viewing by the public and the media.

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Hall, Justin P.

To: Escalera, Lilliam
Subject: RE: WPI Segment No. 435749-1/US 92 (SR 600) PD&E Study Reevaluation – Right of way Acquisition

From: Escalera, Lilliam [<mailto:Lilliam.Escalera@dot.state.fl.us>]
Sent: Monday, May 15, 2017 11:34 AM
To: Cherlene@premiumsolutionsgrp.com
Cc: Hull, Alex; Hall, Justin P.
Subject: WPI Segment No. 435749-1/US 92 (SR 600) PD&E Study Reevaluation – Right of way Acquisition

Dear Cherlene Adewinmi:

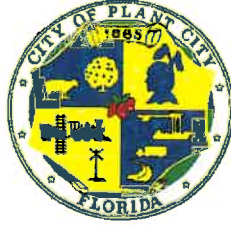
This email is in response to your comments made at the December 6, 2016 public hearing session held for the proposed future widening of US 92, from east of I-4 to east of County Line Road. Specifically, you inquired about the point of contact for right of way acquisition for this project.

Following the hearing, the FDOT evaluated your request along with the other comments received during the comment period. Currently this project is not funded for right of way acquisition based on the FDOT 5-year work program, so there is not a specific point of contact assigned. You can always request public records via email at D7.PublicRecords@dot.state.fl.us or contact the project team through the project website <http://active.fdotd7studies.com/sr600/garden-lane-to-county-line/>.

Thank you for your interest in this reevaluation, and do not hesitate to contact me with any future questions or concerns.

Sincerely,

Lilliam E. Escalera
EMO Project Manager
FDOT District VII
Planning & Environmental Management Office (PLEMO)
11201 N. McKinley Dr., 7-800
Tampa, FL 33612
P: (813)975-6445
F: (813) 975-6451
Lilliam.escalera@dot.state.fl.us



CITY OF PLANT CITY

Engineering Division
P.O. Box C
Plant City, FL 33564
(813) 659-4200

January 23, 2017

Ms. Lilliam E. Escalera
Project Development Engineering Specialist I
Florida Department of Transportation
11201 N. Malcolm McKinley Drive
Tampa, FL 33612

RE: US 92 PD&E Study
Jarrett Scott Ford
Plant City, FL

Dear Ms. Escalera:

I would like to thank you for our telephone discussion regarding Jarrett Scott Ford access on US 92 we had on January 17th. The telephone conversation follows a meeting that was held with the City staff and Jim Scott regarding the preliminary plans for US 92 / SR 600 that were issued for public comment. A summary of our discussion is provided in the following paragraphs. It should be noted that Jarrett Scott Ford employees 67 full time staff and 3 part time staff at the business, has approximately 12 semi-trucks and large trucks entering and exiting the business to drop off cars, trucks and parts and has a substantial volume of customer traffic that enter and exit the business on a daily basis.

Jarrett Scott Ford new and used car and truck business is located in the northwest corner of the intersection of N. Park Road and US 92 (E. Baker Street). I have attached a map which identifies the properties that are currently owned by Jarrett Scott Ford. Currently the business has four (4) driveway connections; two (2) are on N. Park Road and two (2) are on US 92 / SR 600 as shown on the map.

The northern driveway on N. Park Road is closed off to traffic in order for the business to maintain traffic flow within its car and truck display area. The southern driveway has full access to entering and exiting vehicles from the north and south bound traffic lanes on N. Park Road. There are currently no concrete medium barriers prohibiting the flow of traffic in and out of the southern driveway on N. Park Road.

Ms. Lilliam E. Escalera
Project Development Engineering Specialist I
US 92 PD&E Study – Jarrett Scott Ford
January 23, 2017
Page 2 of 2

On US 92 the eastern driveway is an entrance in and a right turn only exit onto westbound US 92 (E. Baker Street). The western driveway has a full medium access that allows vehicles the ability to enter and exit the business from east bound or west bound US 92 / SR 600 (E. Baker Street). The adjacent 3.65 acre parcel also has two driveways. The eastern driveway has an entrance in and a right turn only exit onto westbound US 92 / SR 600 (E. Baker Street) and the western driveway has a full medium access that allows vehicles the ability to enter and exit from east bound or west bound US 92 / SR 600 (E. Baker Street).

In reviewing the PD&E US 92 / SR 600 Plan Sheets, see attached plan sheet, the preliminary design layout of N. Park Road and US 92 / SR 600 shows a number of road improvements which will directly impact the Jarrett Scott Ford business entrances and exits in a negative manner. The plan sheet indicates that the two driveways on N. Park Road would become a right turn only entrance and a south bound exit out of the business. Those vehicles wanting to go north on N. Park Road would have to perform a U-Turn at the N. Park Road and US 92 / SR 600 intersection. This movement would be difficult for semi-truck and large truck traffic to complete within the paved traffic lanes.

A larger negative impact to the business is the closure of all medium openings on US 92 / SR 600 from the intersection of N. Park Road west to the intersection of N. Maryland Avenue. The closures would require vehicles that want to go east on US 92 / SR 600 to perform a U-Turn at the N. Maryland Avenue and US 92 / SR 600 intersection. This movement would be extremely difficult for semi-truck and large truck traffic to complete within the paved traffic lanes.

In closing, when the Department of Transportation starts the design phase of the improvements to US 92 / SR 600 and N. Park Road I would suggest that this letter be provided to the design firm that will be responsible for the improvements and I further strongly suggest that the design firm contact Jarrett Scott Ford to discuss the issues presented in this letter. I think that the issues can be somewhat worked out to provide Jarrett Scott Ford an acceptable means for vehicles to enter and exit the business and provide for the safety and convenience of the vehicles that will be traveling on US 92 / SR 600 and N. Park Road.

Should you have any questions or comments regarding the above, please contact me.

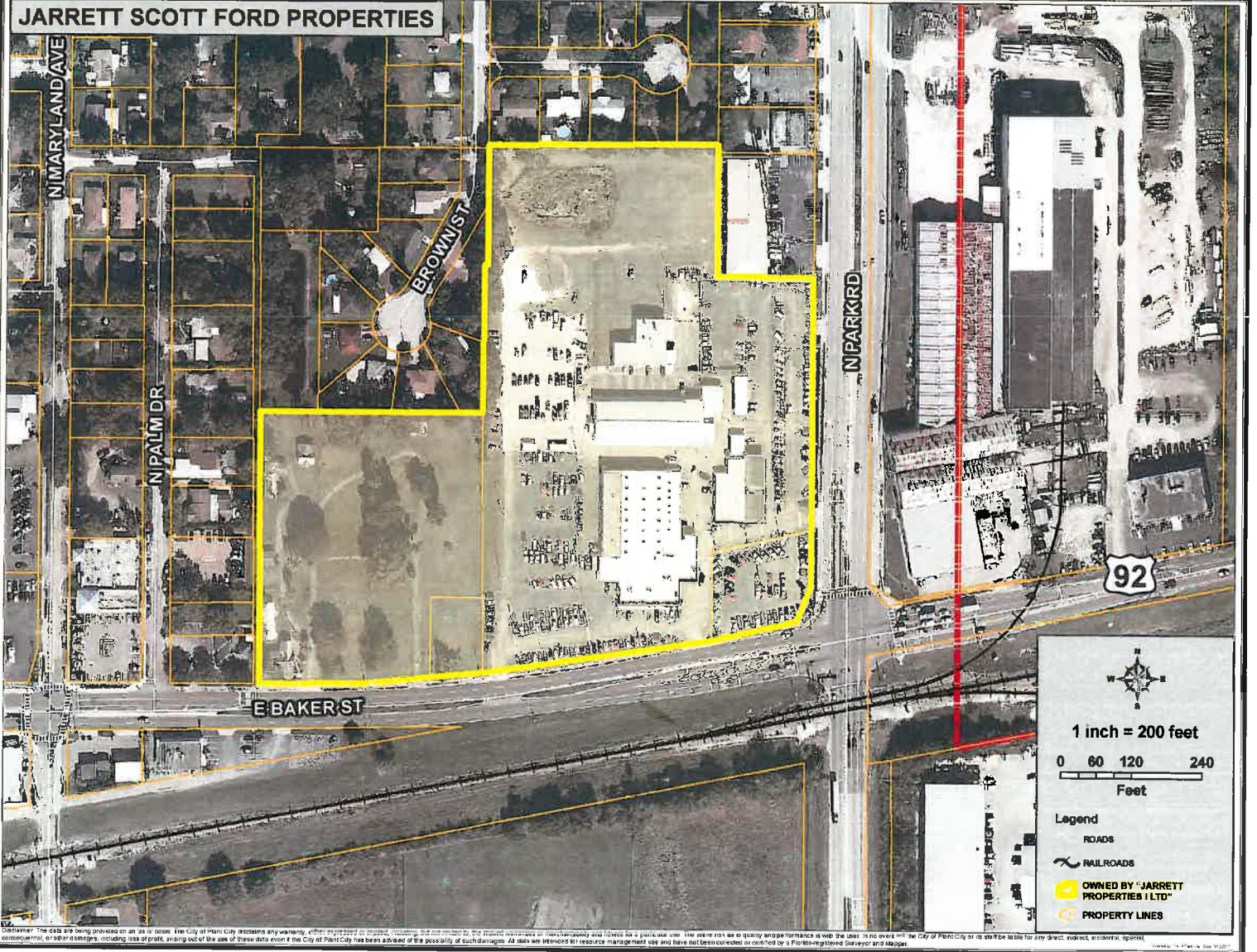
Sincerely,



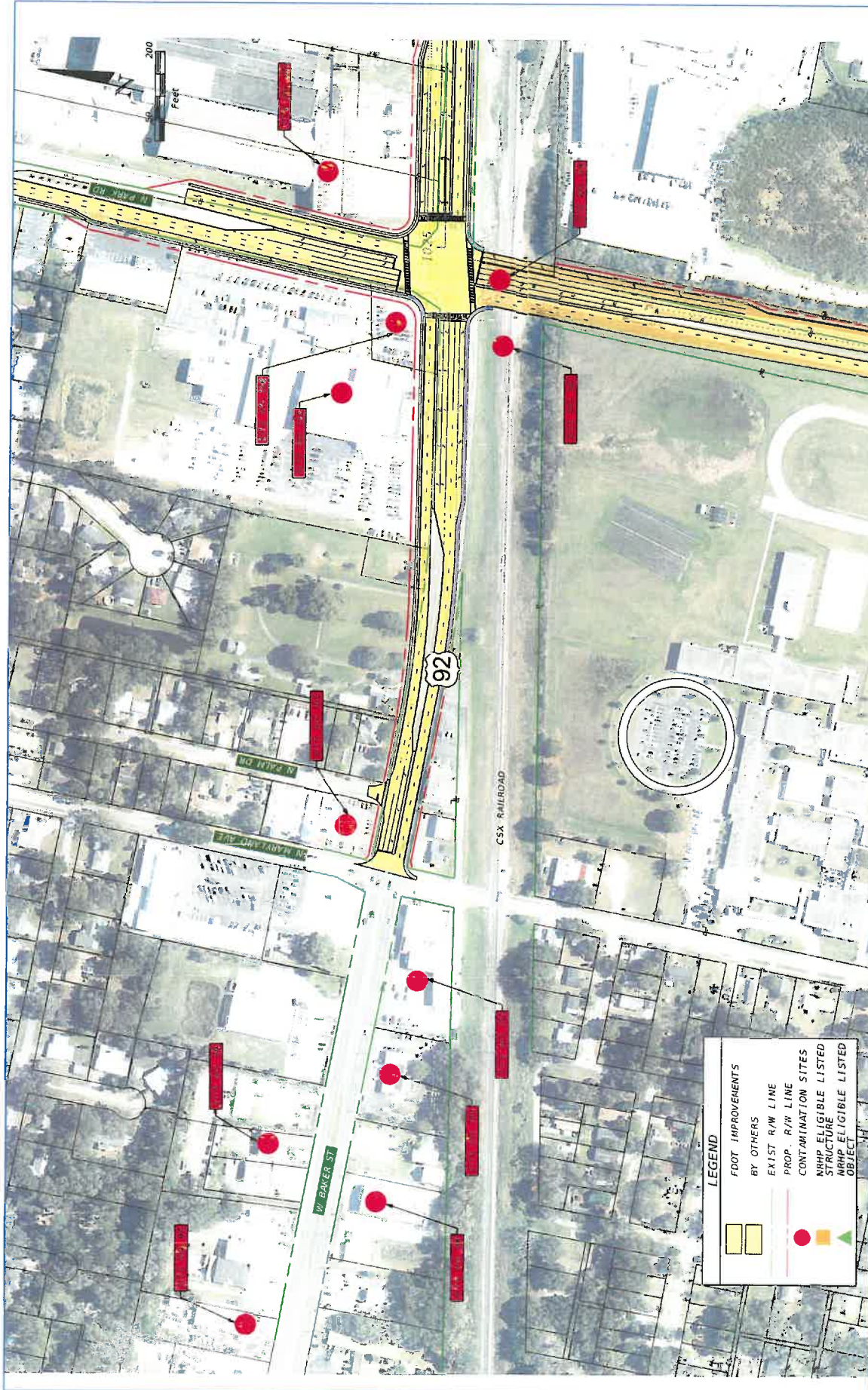
Michael A. Schenk, P.E.
City Engineer

Cc: Mike Herr, City Manager
Jim Scott, Jarrett Scott Ford

JARRETT SCOTT FORD PROPERTIES



Disclaimer: The data are being provided on an "as is" basis. The City of Palm City disclaims any warranty, either expressed or implied, including the accuracy, completeness, reliability, and fitness for a particular use. The user assumes all responsibility for the use of the data. The City of Palm City or its staff will not be liable for any direct, indirect, incidental, special, consequential, or other damages, including loss of profit, arising out of the use of the data even if the City of Palm City has been advised of the possibility of such damages. All data are intended for resource management use and have not been collected or certified by a Florida-Registered Surveyor and Mapper.



LEGEND

- FDOT IMPROVEMENTS
- BY OTHERS
- EXIST R/W LINE
- PROP. R/W LINE
- CONTAMINATION SITES
- NRHP ELIGIBLE LISTED STRUCTURE
- NRHP ELIGIBLE LISTED OBJECT

DATE	DESCRIPTION	REVISIONS	DATE	DESCRIPTION

ENGINEER OF RECORD	STATE OF FLORIDA
DR. J. D. DAVIS, PE	DEPARTMENT OF TRANSPORTATION
Innovative Consulting Engineers, Inc.	
3000 DWS Drive, Suite 200, Oviedo, Florida 32765	ROAD NO. 600
P. 407.971.8855	COUNTY HILLSBOROUGH
	FINANCIAL PROJECT ID 435749-1-22-01

US 92 / SR 600	SHEET NO.
PLAN SHEETS	

9.16.2016 3:26:21 PM C:\P\105077-002-01\Drawings\92SR600\105077.dwg



Florida Department of Transportation

RICK SCOTT
GOVERNOR

605 Suwannee Street
Tallahassee, FL 32399-0450

MIKE DEW
SECRETARY

June 23, 2017

Michael Schenk, P.E.
City of Plant City Engineering Division
P.O. Box C
Plant City, FL 33564

Re: WPI Segment No. 435749-1/US 92 (SR 600) PD&E Study Reevaluation

Dear Michael Schenk,

This letter is in response to your comments about the proposed future widening of US 92, from east of I-4 to east of County Line Road. More specifically, in response to your letter dated January 23, 2017 regarding the median openings adjacent to the Jarrett Scott Ford business entrances.

Currently this segment of the project corridor is not funded for design, right of way acquisition, and/or construction. Of course, once the project enters the design phase, impacts to properties along the corridor and access management will be reviewed further during the design phase with the most current survey data and design standards to determine the exact location of the roadway improvements.

Thank you for your continued coordination with FDOT and your interest in this reevaluation. Please do not hesitate to contact me with any future questions or concerns.

Sincerely

A handwritten signature in black ink, appearing to read "L. Escalera". The signature is fluid and cursive.

Lilliam Escalera
EMO Project Manager
Lilliam.Escalera@dot.state.fl.us
(813) 975-6445 / (800) 226-7220 x6445



Hillsborough MPO Metropolitan Planning for Transportation

February 8, 2017

Commissioner
Lesley "Les" Miller, Jr.
Hillsborough County
MPO Chairman

Councilman Harry Cohen
City of Tampa
MPO Vice Chairman

Paul Anderson
Tampa Port Authority

Wallace Bowers
HART

Trent Green
Planning Commission

Commissioner Ken Hagan
Hillsborough County

Commissioner
Pat Kemp
Hillsborough County

Mayor Kim Leinbach
Temple Terrace

Joe Lopano
Hillsborough County
Aviation Authority

Mayor Rick A. Lott
City of Plant City

Councilman
Guido Maniscalco
City of Tampa

Commissioner
Sandra Murman
Hillsborough County

Cindy Stuart
Hillsborough County
School Board

Councilman
Luis Viera
City of Tampa

Joseph Waggoner
Expressway Authority

Commissioner
Stacy R. White
Hillsborough County

Beth Alden, AICP
Executive Director

Lilliam Escalera, Project Manager
FDOT District Seven
11201 N. Malcolm McKinley Drive, MS 7-500
Tampa, Florida 33612-6403

Subject: US 92 PD&E Study Comments

Dear Ms. Escalera,

Thank you for briefing the Hillsborough MPO and its advisory committees about proposed capacity improvements on US Highway 92 between Interstate 4 and County line. The segments near I-75 and near County Line Road are forecast to be highly congested, and the MPO's adopted long range transportation plan identifies them as a priority for funding. We appreciate the Department's work to move forward.

Having reviewed the PD&E recommended build alternative, the MPO's Bicycle and Pedestrian Advisory Committee (BPAC) approved a motion at its January meeting requesting the following two items:

1. That FDOT consider a separated Multi-Use Trail facility along the entire corridor (except in the 'no build' segment through Plant City). Such a facility would help make an important connection between the SUN Southwest Coast Corridor and other trail facilities of statewide significance such as the Van Fleet Trail in the Lakeland area.
2. If a Multi-Use Trail cannot be accommodated, the committee requests that where bicycle facilities pass under bridges, they be protected behind bridge structural supports, rather than being located on the vehicular travel side of the bridge supports as currently shown.

The MPO Board supports this request. Thank you for your consideration, and please contact me or Michele Ogilvie of my staff if you have any questions.

Sincerely,

Beth Alden, AICP
Executive Director

Cc: Stephen Benson, District 7 Local Liaison Administrator
BPAC Chair Patrick Thorpe



Plan Hillsborough
planhillsborough.org
planner@plancom.org
813 - 272 - 5940
601 E Kennedy Blvd
18th Floor
Tampa, FL 33602



Florida Department of Transportation

RICK SCOTT
GOVERNOR

605 Suwannee Street
Tallahassee, FL 32399-0450

MIKE DEW
SECRETARY

June 23, 2017

Beth Alden, AICP
Hillsborough County Metropolitan Planning Organization
601 E Kennedy Blvd – 18th Floor
Tampa, FL 33602

Re: WPI Segment No. 435749-1/US 92 PD&E Study Reevaluation

Dear Ms. Alden,

This letter is in response to your letter dated February 8, 2017 regarding the subject project and proposed future widening of US 92, from east of I-4 to east of County Line Road. As countywide trail planning activities move forward, any new bicycle/pedestrian facilities requested along a state highway corridor beyond the FDOT standard must be added to the Hillsborough County Greenways and Trails Plan map indicating the proposed continuous route and its relationship to the surrounding trail network.

The subject segment of the project is not funded for design, right of way acquisition, or construction. It is critical that these features be discussed jointly between the MPO, Hillsborough County, and the Department, during the design phase of the project. FDOT will coordinate with the MPO and Hillsborough County regarding the opportunity to include a trail along US 92 from Park Road to County Line Road, consistent with current FDOT trail maintenance policies. We will reach out to you to continue this discussion once the design phase is moved into the Five Year Work Program.

Thank you for your continued partnership with FDOT and for your interest in improving safety for bicyclists and pedestrians. Please do not hesitate to contact me with any future questions or concerns.

Sincerely

A handwritten signature in black ink that reads "L. Escalera".

Lilliam Escalera
EMO Project Manager

Lilliam.Escalera@dot.state.fl.us

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