

Final Conceptual Stage Relocation Plan

SR 600 (US 92) Project Development & Environment Study Re-evaluation

From East of I-4 to East of County Line Road
Hillsborough County, Florida

Work Program Item Segment No.: 435749-1
Federal Aid Project No.: To Be Determined

The Florida Department of Transportation, District Seven, conducted a Project Development and Environment Study Re-evaluation for the purpose of widening State Road 600 (US 92) from east of Interstate 4 to east of County Line Road in Hillsborough County, Florida. The total project length is approximately 18.1 miles.

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT.

Prepared for:

Florida Department of Transportation
District Seven
11201 N. McKinley Drive
Tampa, Florida 33612
May 2017

Any reference contained herein to the Project Development & Environment Manual is referring to the 2016 revision.

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1 Description of Proposed Action

1.1 Project Description

FDOT, District Seven, is conducting a re-evaluation of a PD&E Study regarding the proposed widening of SR 600 (US 92) in Hillsborough County that was originally completed in March 1994. The limits of this project on US 92 are from east of I-4 to east of County Line Road, which is a distance of approximately 18.1 miles. The location and limits of this study are shown in the project location map as Figure 1-1.

The proposed action includes capacity improvements consisting of widening US 92 as well as intersection improvements and bicycle and pedestrian facilities. The project was divided into 11 evaluation segments based on changes in land use and the proposed typical section in comparison with the land use and typical sections from the original PD&E Study. The evaluation segment limits are shown in Figure 1-2. Five different typical section alternatives were recommended for the project in addition to the No-Build Alternative which was recommended for the portion of the project that passes through Plant City. The five typical section alternatives are listed below and the proposed action for each evaluation segment is described in Table 1-1.

Typical Section 1 - Typical section 1 is a suburban roadway with two 11-foot travel lanes and a seven-foot buffered bike lane in each direction. The travel lanes are separated by a 44-foot median with eight-foot inside shoulders. Type F curb and gutter is used along the outside lanes and curb inlets collect stormwater runoff which is then conveyed to stormwater retention ponds. A 17-foot border is provided along both sides of the roadway and accommodates five-foot sidewalks and five-foot tie down widths on both sides of the road. This typical section requires a minimum of 136 feet of right-of-way and complies with the FDOT minimum design speed of 45 mph.

Typical Section 2 - Typical Section 2 is a suburban roadway with two 11-foot travel lanes and a seven-foot buffered bike lane in each direction. The travel lanes are separated by a 40.5-foot median with eight-foot inside shoulders. Pier protection barrier is located between the bike lanes and the piers and six-foot sidewalks are located behind the piers on both sides of the roadway. Type F curb and gutter is used along the outside lanes and curb inlets collect stormwater runoff which is then conveyed to stormwater retention ponds. This typical section requires a minimum of 119.5 feet of right-of-way and complies with the FDOT minimum design speed of 45 mph but would require a design variation if the roadway is expanded to six lanes in the future.

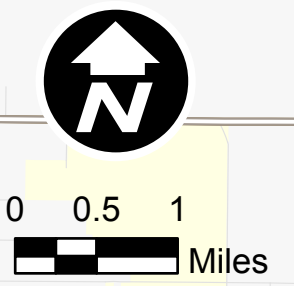
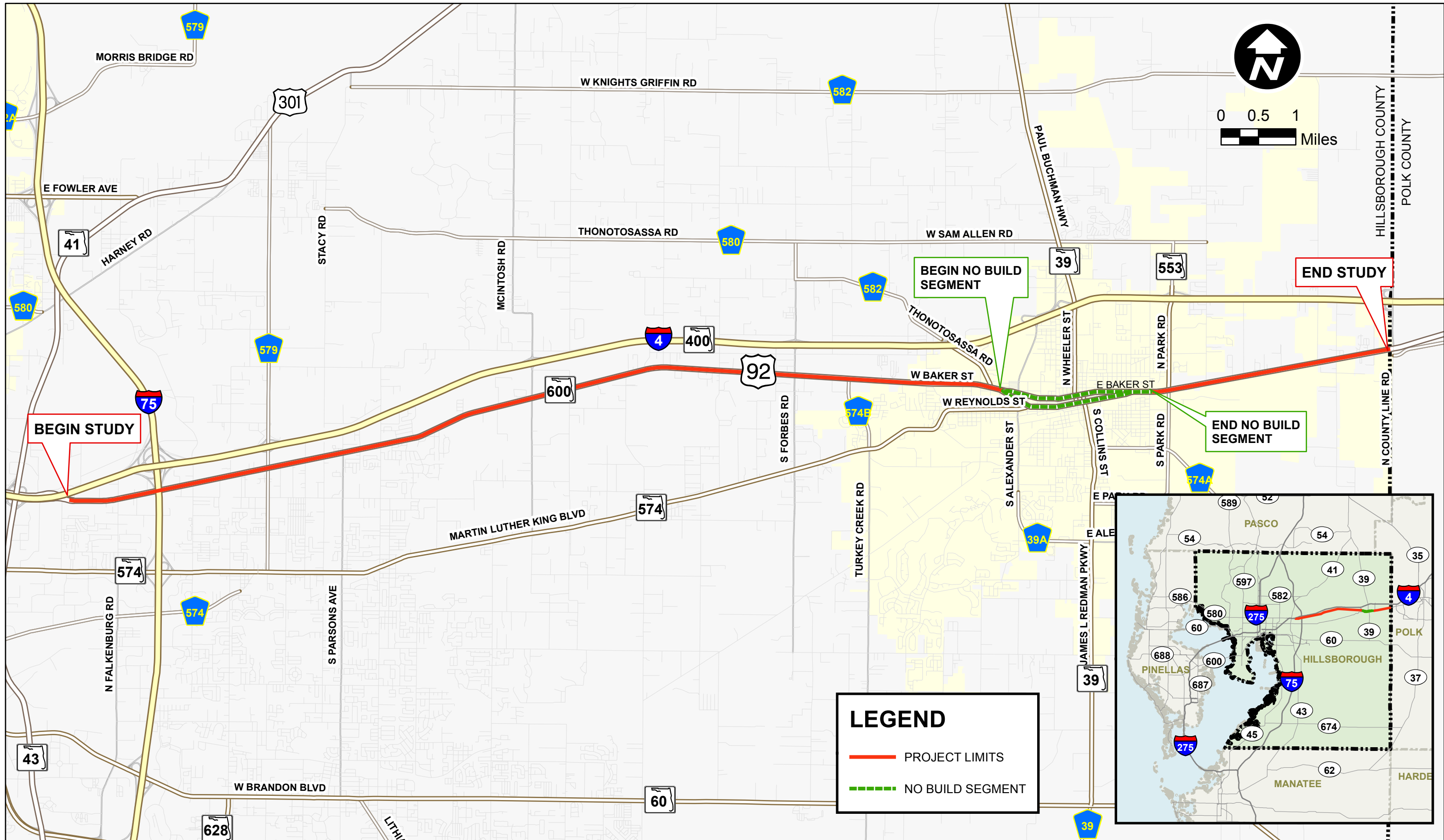
Typical Section 3 - Typical Section 3 is a high speed suburban roadway with two 12-foot travel lanes and a seven-foot buffered bike lane in each direction. The travel lanes are separated by a 54-foot median with eight-foot inside shoulders. Type E curb and gutter is used along the outside lanes and curb inlets collect stormwater runoff which is then conveyed to stormwater retention ponds. A 29-foot border is provided along both sides of the roadway and accommodates five-foot sidewalks on both sides of the road. This typical section requires a minimum of 160 feet of right-of-way and complies with the FDOT minimum design speed of 50 mph.

Typical Section 4 - Typical Section 4 is an urban roadway with two 11-foot travel lanes and a seven-foot buffered bike lane in each direction. The travel lanes are separated by a 22-foot median. Type E curb and gutter is along the inside and Type F curb and gutter is used along the outside lanes and curb inlets collect stormwater runoff which is then conveyed to stormwater retention ponds. A minimum 12-foot border is provided along both sides of the roadway and accommodates five-foot sidewalks on both sides of the road. This typical section requires a minimum of 114 feet of right-of-way and complies with the FDOT minimum design speed of 45 mph.

Typical Section 5 - Typical Section 5 is a high speed suburban roadway two 12-foot travel lanes, a five-foot sidewalk, and a seven-foot buffered bike lane in each direction separated by a 40-foot median with eight-foot inside shoulders with a design speed of 50 mph and is shown in Figure 1-7. A 24-foot border and a 24-foot clear zone are provided along both sides of the roadway. This four-lane typical section requires a minimum of 136 feet of right-of-way. A design variation would be required for border width. The typical section complies with clear zone criteria so no design variation or exception would be required for clear zone.

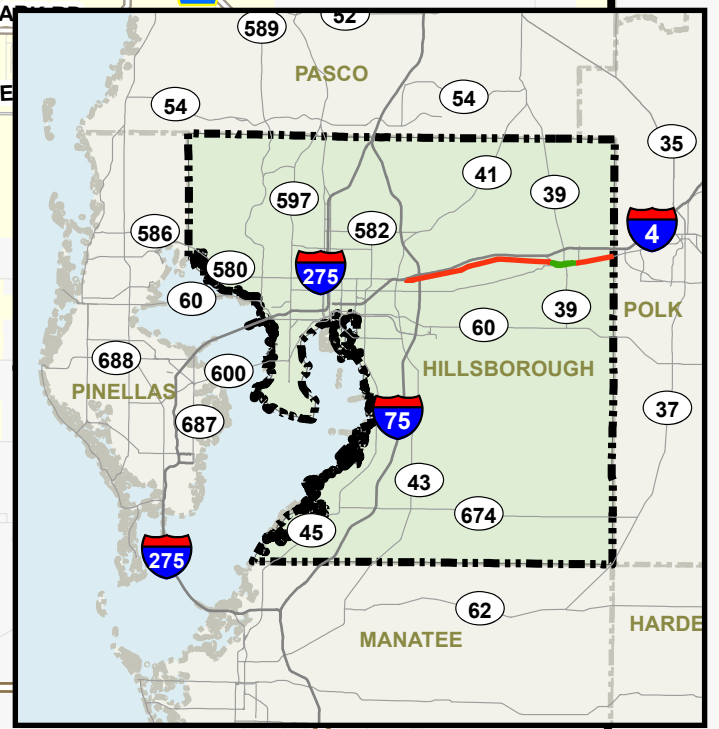
**Table 1-1
Proposed Actions by Evaluation Segment**

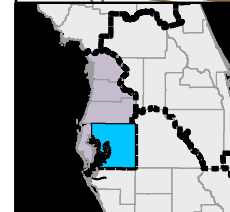
Evaluation Segment	Portion of Segment	Build Alternative
1	Garden Lane to west of I-75 & east of I-75 to west of Mango Road	Typical Section 1
	Just west of I-75 to Just east of I-75	Typical Section 2
2	All	Typical Section 1
3	All	Typical Section 1
4	All	Typical Section 1
5	All	Typical Section 3
6	All	Typical Section 3
7	All	Typical Section 3
8	All	Typical Section 3
9	East of Bethlehem Road to Edwards Street	Typical Section 3
	Edwards Street to Thonotosassa Road	Typical Section 4
10	All	No-Build
11	All	Typical Section 5



LEGEND

- PROJECT LIMITS
- - - NO BUILD SEGMENT



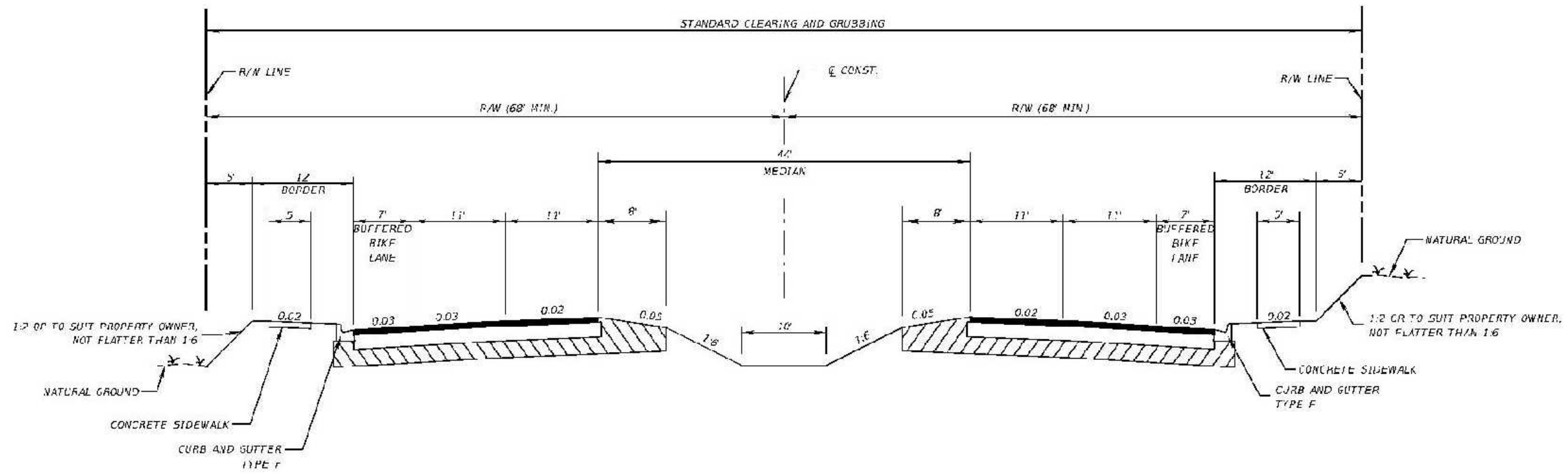


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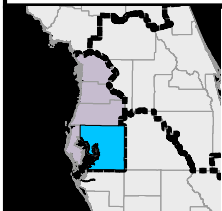
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EVALUATION SEGMENT LIMITS

Figure
1-2



FROM EAST OF GARDEN LANE TO WEST OF I-75 - SEGMENT 1
 FROM WEST OF CR 579 TO EAST OF CR 579 - SEGMENT 2
 FROM EAST OF CR 579 TO NORTH PARSONS AVENUE - SEGMENT 3
 FROM NORTH PARSONS AVENUE TO EAST OF CROW WING DRIVE - SEGMENT 4
 DESIGN SPEED = 45 MPH

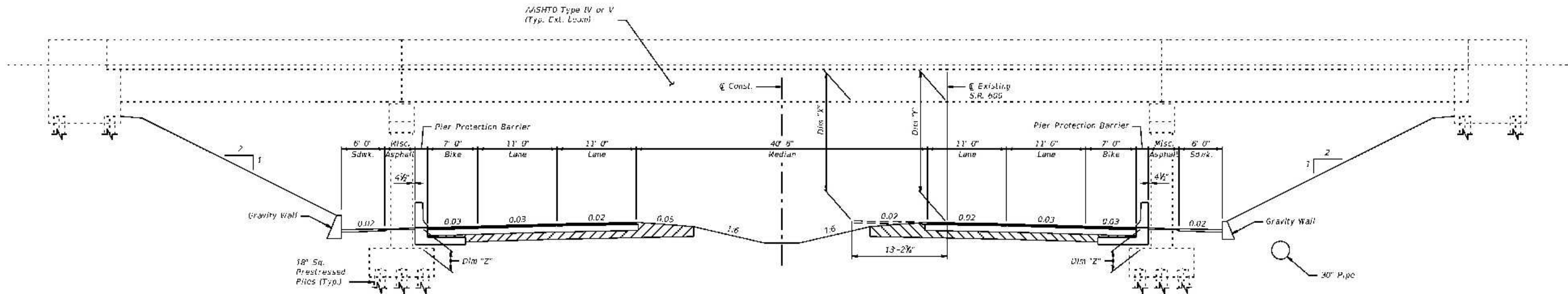


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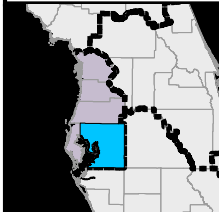
TYPICAL SECTION 1
PROPOSED FOUR LANE 45 MPH DESIGN SPEED
TYPICAL SECTION

Figure
1-3



UNDER I-75 BRIDGE
 FROM WEST OF I-75 TO EAST OF I-75 - SEGMENT 1
 DESIGN SPEED = 45 MPH

BRIDGE DATA TABLE				
BRIDGE NO.	DIM "X" (ft.)	DIM "Y" (ft.)	DIM "Z" (ft.)	
			PIER 2	PIER 3
100414	10.275	18.500	4.135	3.135
100415	19.069	19.333	2.552	2.552
100422	17.673	17.938	2.745	2.245
100424	16.038	16.302	3.438	3.438

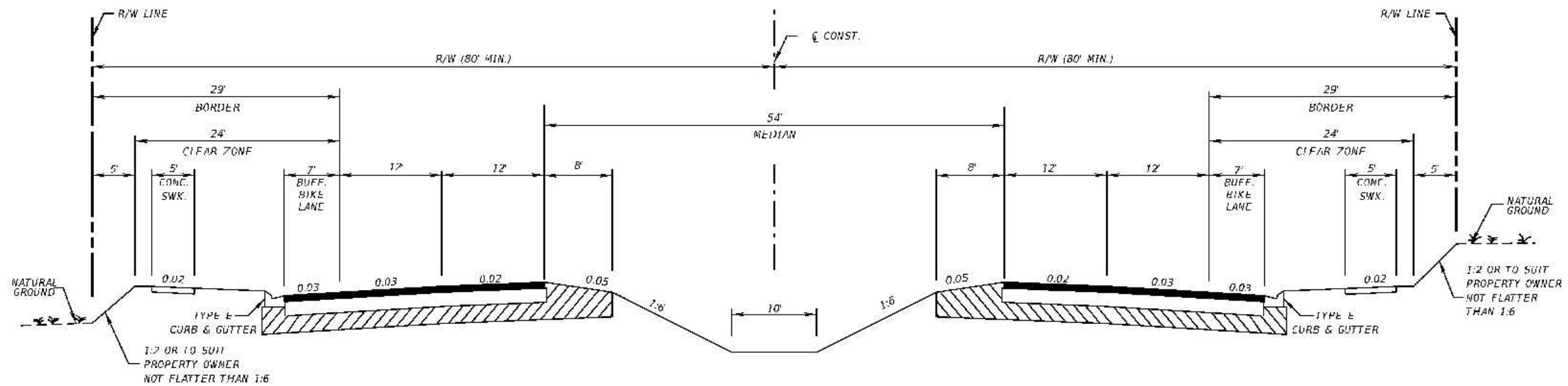


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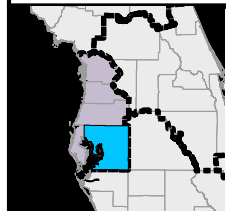
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TYPICAL SECTION 2
PROPOSED FOUR LANE 45 MPH DESIGN SPEED
TYPICAL SECTION UNDER I-75

Figure
1-4



FROM EAST OF CROW WING DRIVE TO CASTLEWOOD ROAD - SEGMENT 5
 FROM CASTLEWOOD ROAD TO WEST OF GALLAGHER ROAD - SEGMENT 6
 FROM WEST OF GALLAGHER ROAD TO LYNN OAKS CIRCLE - SEGMENT 7
 FROM LYNN OAKS CIRCLE TO EAST OF BETHLEHEM ROAD - SEGMENT 8
 FROM EAST OF BETHLEHEM ROAD TO EDWARDS STREET - SEGMENT 9
 DESIGN SPEED = 50 MPH



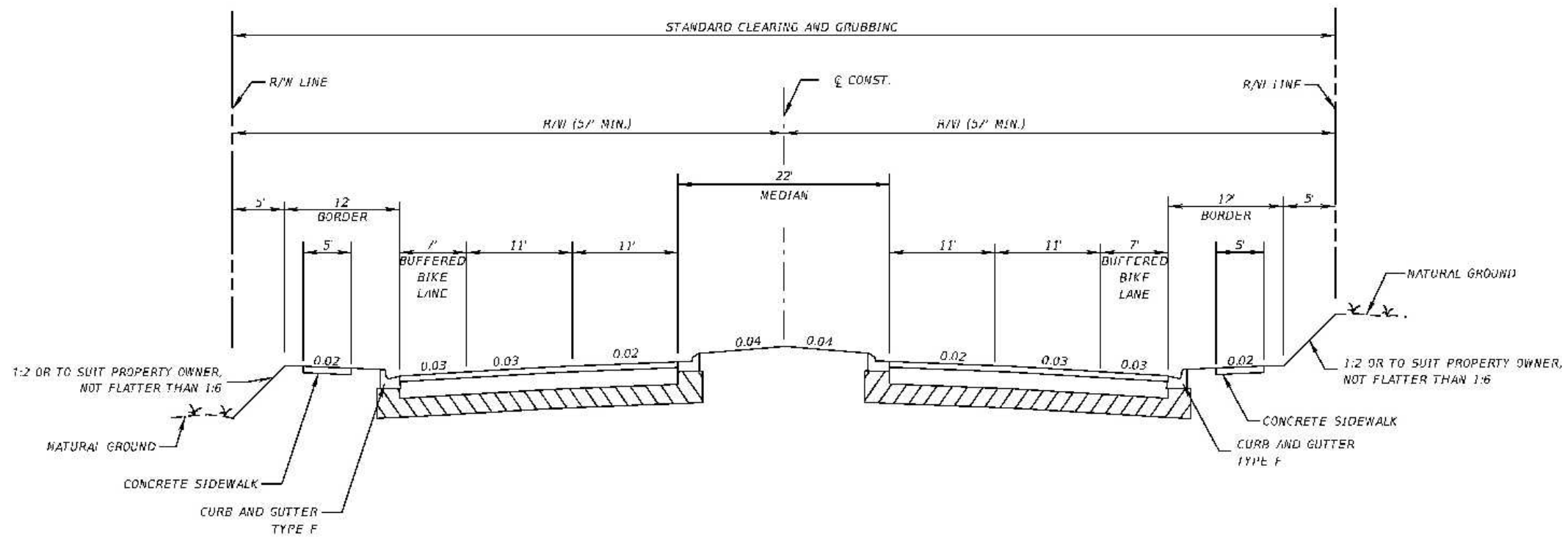
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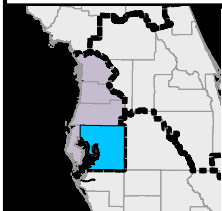
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TYPICAL SECTION 3
PROPOSED FOUR LANE 50 MPH DESIGN SPEED
TYPICAL SECTION

Figure
1-5



FROM EDWARDS STREET TO MOBLEY STREET - SEGMENT 9
 DESIGN SPEED = 45 MPH



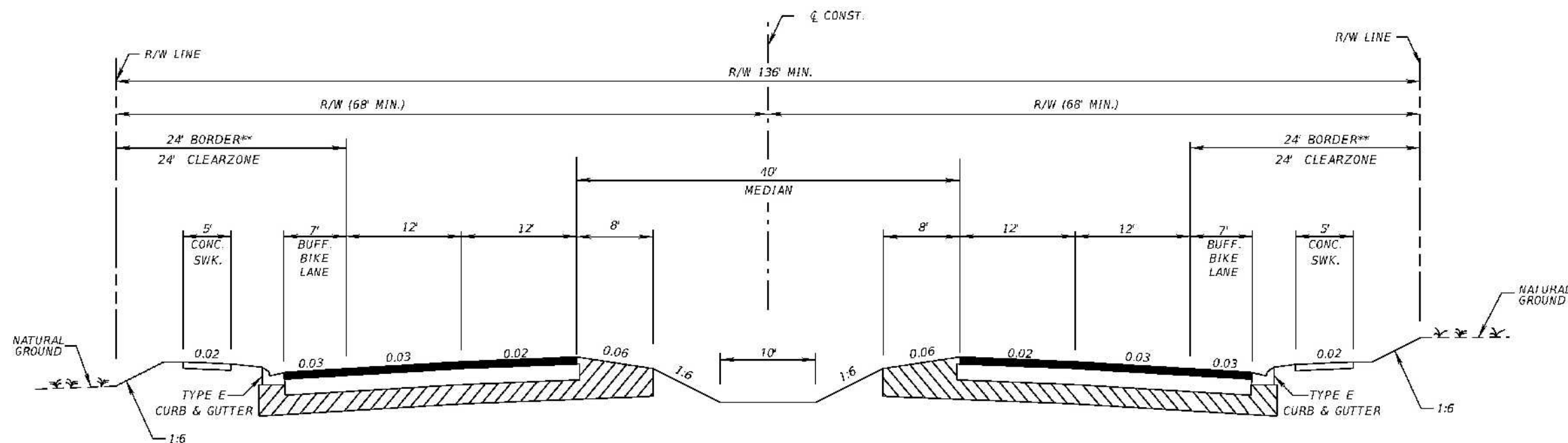
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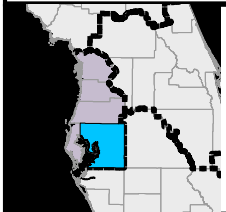
Work Program Item Segment No: 435749-1

TYPICAL SECTION 4
PROPOSED FOUR LANE 45 MPH DESIGN SPEED
EDWARDS STREET TO MOBLEY STREET

Figure
1-6



FROM PARK ROAD TO COUNTY LINE ROAD - SEGMENT 11
 DESIGN SPEED = 50 MPH



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TYPICAL SECTION 5
PROPOSED FOUR LANE 50 MPH DESIGN SPEED
PARK ROAD TO COUNTY LINE ROAD

Figure
1-7

1.2 Purpose

This Conceptual Stage Relocation Plan (CSRP) is submitted in accordance with Florida Statutes 339.09(2); 339.09(3); and 421.55;49 CFR Part 24 and 23 CFR Part 710 to comply with the Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970 (Public Law 91-646). The CSRP is being prepared as part of the PD&E Study to evaluate roadway improvements that will increase safety and mobility of all modes of transportation along US 92.

1.3 Planning Consistency

The US 92 road segments from Park Road to County Line Road and from US 301 to Mango Road are consistent with Hillsborough MPO's Fiscal Year (FY) 2016/17-2020/21 Transportation Improvement Program (TIP), the approved FDOT State Transportation Improvement Program (STIP) for FY 2017-2020, and the Hillsborough MPO's adopted Imagine 2040 Long Range Transportation Plan (LRTP).

Both project segments are currently funded for design using state funding sources. The right-of-way and construction phases are not currently funded in FDOT's Adopted Five-Year Work Program or the approved STIP or TIP but are included in the Hillsborough MPO Imagine 2040 LRTP Cost Feasible Plan. Right-of-way and construction of US 92 from Park Road to County Line Road is designated for funding in FY 2021-2025 and the western segment from US 301 to Mango Road is designated for funding in FY 2021-2025. Documentation of funding for this project can be found in the adopted Hillsborough MPO TIP for FY 2016/17 to 2020/21, the FDOT STIP for FY 2017-2020, and the Hillsborough MPO LRTP.

There is no current or future funding programmed for the segment between Mango Road and Park Road.

It appears that planning consistency requirements have been met for this project as the design phase for two segments of the PD&E Study is reflected in the STIP/TIP, and the right-of-way and construction phases for each of these segments are included in the Hillsborough MPO's Imagine 2040 LRTP Cost Feasible Plan.

2 Recommended Alternatives

Based on a comparative evaluation of the No-Build and Build Alternative impacts and ability to meet the purpose and need for this project as well as public input and agency coordination, the recommended alternative for each of the 11 segments has been determined as described below.

Segment 1 from east of I-4 (Garden Lane) to west of Mango Road

From Garden Lane to west of I-75 and from just east of I-75 to west of Mango Road, the recommended typical section is an urban roadway with two 11-foot travel lanes and a seven-foot buffered bike lane in each direction. The travel lanes are separated by a 44-foot median with eight-foot inside shoulders. Type F curb and gutter is used along the outside lanes and curb inlets collect stormwater runoff which is then conveyed to stormwater retention ponds. A 17-foot border is provided along both sides of the roadway and accommodates five-foot sidewalks and five-foot tie down widths on both sides of the road. This typical section requires

a minimum of 136 feet of right-of-way and complies with the FDOT minimum design speed of 45 mph. The recommended typical section for Segment 1 is shown in Figure 1-3.

From just west of I-75 to just east of I-75, the recommended typical section is constrained by the piers for I-75. The recommended typical section under I-75 is an urban roadway with two 11-foot travel lanes and a seven-foot buffered bike lane in each direction. The travel lanes are separated by a 40-foot six-inch median with eight-foot inside shoulders. Pier protection barrier is located between the bike lanes and the piers and six-foot sidewalks are located behind the piers on both sides of the roadway. Type F curb and gutter is used along the outside lanes and curb inlets collect stormwater runoff which is then conveyed to stormwater retention ponds. This typical section complies with the FDOT minimum design speed of 45 mph. This recommended typical section is shown in Figure 1-4.

The recommended alignment for Segment 1 from Garden Lane to west of Mango Road follows the preferred alignment from the original PD&E Study. From Garden Lane to Falkenburg Road, the recommended alignment is a north alignment with right-of-way to be acquired from the north side of the roadway. From Falkenburg Road to just west of Mango Road, the alignment shifts to a south alignment with right-of-way to be acquired from the south side of the roadway.

Segment 2 from west of Mango Road to east Mango Road

The recommended typical section for this segment is the same as for the major portion of Segment 1 (exclusive of the portion from just west of I-75 to just east of I-75) and is shown in Figure 1-3.

North, center and south alignments were evaluated due to additional development which has occurred in the vicinity of the Mango Rd intersection. The recommended alignment for Segment 2 from west of Mango Road to east Mango Road is the north alignment. This alignment was selected to minimize impacts to the Seffner Christian Academy in the southwest quadrant of the US 92 and Mango Road intersection, and to minimize impacts to the Hardees Restaurant in the southeast quadrant of the intersection. This alignment begins as a south alignment adjacent to Segment 1 and then transitions to the north side of US 92 through the intersection, and then transitions to a south alignment at the beginning of Segment 3.

Segment 3 from east of Mango Road to North Parsons Avenue

The recommended typical section for this segment is the same as for the major portion of Segment 1 (exclusive of the portion from just west of I-75 to just east of I-75) and is shown in Figure 1-3.

The recommended alignment for Segment 3 from east of Mango Road to North Parsons Avenue follows the recommended alignment from the original PD&E Study and is a south alignment.

Segment 4 from North Parsons Avenue to east of Crow Wing Drive

The recommended typical section for this segment is the same as for the major portion of Segment 1 (exclusive of the portion from just west of I-75 to just east of I-75) and is shown in Figure 1-3.

North, south, and center alignments were evaluated due to additional developments which have occurred on the north side of the road. The recommended alignment for Segment 4 follows the preferred alignment from the original PD&E Study and is a north alignment.

Segment 5 from east of Crow Wing Drive to Castlewood Road

The recommended typical section for this segment is a high speed suburban roadway with two 12-foot travel lanes and a seven-foot buffered bike lane in each direction. The travel lanes are separated by a 54-foot median with eight-foot inside shoulders. Type F curb and gutter is used along the outside lanes and curb inlets collect stormwater runoff which is then conveyed to stormwater retention ponds. A 29-foot border is provided along both sides of the roadway and accommodates five-foot sidewalks on both sides of the road. This typical section requires a minimum of 160 feet of right-of-way and complies with the FDOT minimum design speed of 50 mph. The recommended typical section for Segment 5 is shown in Figure 1-5.

The recommended alignment for Segment 5 from east of Crow Wing Drive to Castlewood Road follows the preferred alignment from the original PD&E Study and is a north alignment.

Segment 6 from Castlewood Road to west of Gallagher Road

The recommended typical section for this segment is the same as for Segment 5 and is shown in Figure 1-5.

North, center, and south alignments were evaluated due to additional developments which have occurred in the vicinity of McIntosh Road. The recommended alignment for Segment 6 from Castlewood Road to west of Gallagher Road is the south alignment. The south alignment was selected because the estimated cost estimate is less than the north alignment and to minimize impacts to Driscoll's of Florida and Independence Academy.

Segment 7 from west of Gallagher Road to Lynn Oaks Circle

The recommended typical section for Segment 7 is the same as for Segment 5 and is shown in Figure 1-5.

The recommended alignment for Segment 7 from west of Gallagher Road to Lynn Oaks Circle follows the preferred alignment from the original PD&E Study and is a south alignment.

Segment 8 from Lynn Oaks Circle to east of Bethlehem Road

The recommended typical section for Segment 8 is the same as for Segment 5 and is shown in Figure 1-5.

North, center, and south alignments were evaluated due to a reduction in the proposed typical section width. The recommended alignment for Segment 8 from Lynn Oaks Circle to east of

Bethlehem Road follows the preferred alignment from the original PD&E Study and is a centered alignment.

Segment 9 from east of Bethlehem Road to Mobley Street

Segment 9 was further divided into several portions. The recommended typical section for the portion of Segment 9 from east of Bethlehem Road to Edwards Street is the same as for Segment 5 and is shown in Figure 1-5.

The recommended typical section for the portion of Segment 9 from Edwards Street to Mobley Street is an urban roadway with two 11-foot travel lanes and a seven-foot buffered bike lane in each direction. The travel lanes are separated by a 22-foot median. Type E curb and gutter is along the inside and Type F curb and gutter is used along the outside lanes and curb inlets collect stormwater runoff which is then conveyed to stormwater retention ponds. A minimum 12-foot border is provided along both sides of the roadway and accommodates five-foot sidewalks on both sides of the road. This typical section requires a minimum of 114 feet of right-of-way and complies with the FDOT minimum design speed of 45 mph. The recommended typical section for Segment 9 from Edwards Street to Mobley Street is shown in Figure 1-6.

The recommended alignment for Segment 9 from east of Bethlehem Road to Woodrow Wilson follows the preferred alignment from the original PD&E Study and is a south alignment from east of Bethlehem Road to Turkey Creek Road and then transitions to a north alignment from Turkey Creek Road to Woodrow Wilson Street. From Woodrow Wilson Street to Mobley Street, the recommended alignment is a centered alignment due to geometric constraints at the Thonotosassa Road intersection and the Baker Street (US 92) intersection.

Segment 10 from Mobley Street to Park Road

The preferred alternative for this segment from the original PD&E Study is No-Build with the exception that the section of Baker Street between Mobley Street and Whitehall Street be converted from a rural to urban roadway in order to provide sidewalks. Improvements have been completed in this section of the roadway which meet the intent of the original PD&E Study recommendation for this segment of the project. Therefore, the recommendation for this segment is No-Action.

Segment 11 from Park Road to just east of County Line Road

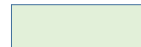
The recommended typical section for Segment 11 consists of two 12-foot travel lanes, a five-foot sidewalk, and a seven-foot buffered bike lane in each direction separated by a 40-foot median with eight-foot inside shoulders with a design speed of 50 mph and is shown in Figure 1-7. A 24-foot border and a 24-foot clear zone are provided along both sides of the roadway. This four-lane typical section requires a minimum of 136 feet of right-of-way. A design variation would be required for border width. The typical section complies with clear zone criteria so no design variation or exception would be required for clear zone.

The recommended alignment for Segment 11 from Park Road to County Line Road follows the preferred alignment from the original PD&E Study and is a north alignment.

Table 2-1 - US 92 Alternatives Evaluation Matrix

US 92 Matrix Evaluation																			
From I-4 to County Line Road																			
Evaluation Criteria	1994 PD&E Preferred Alignment	Segment 1	Segment 2			Segment 3	Segment 4			Segment 5	Segment 6		Segment 7	Segment 8			Segment 9	Segment 10	Segment 11
		From east of I-4 (Garden Lane to west of CR 579)	from west of CR 579 to east CR 579			from east of CR 579 to North Parsons Avenue	from North Parsons Avenue to east of Crow Wing Drive			from east of Crow Wing Drive to Castlewood Road	from Castlewood Road to west of Gallagher Road		from west of Gallagher Road to Lynn Oaks Circle	from Lynn Oaks Circle to east of Bethlehem Road			from east of Bethlehem Road to Mobley Street	from Mobley Street to Park Road	from Park Road to just east of County Line Road
		Original PD&E Study Alignment North - South	North	Centered (Original PD&E Study Alignment)	South	Original PD&E Study Alignment - South	North (Original PD&E Study Alignment)	South	Original PD&E Study Alignment - North	North (Original PD&E Study Alignment)	South	Original PD&E Study Alignment - South	North	Centered (Original PD&E Study Alignment)	South	Original PD&E Study Alignment South - North	No-Build (Original PD&E Study Alignment)	Original PD&E Study Alignment - North	
Business Impacts																			
Number of Businesses Relocations	50	26	5	4	5	9	1	1	1	2	4	1	2	2	0	36	0	25	
Residential Impacts																			
Number of Residential Relocations	91	8	2	2	2	0	19	9	5	3	3	7	11	10	23	22	0	44	
Potential Environmental Effects																			
Archaeological/Historical Sites	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
Noise ¹	244	TBD	TBD	TBD	TBD	TBD	TBD	TBD	TBD	TBD	TBD	TBD	TBD	TBD	TBD	TBD	0	TBD	
Wetlands (acres)	21.98	0.68	0	0	0	0	1.28	1.38	0.14	1.28	0.28	0	2.19	2.09	1.5	6.7	0	0.16	
Floodplains (acre feet)	5.41	1.29	0	0	0	0		0	15.69	1.42	1.42	0.72	6.52	6.52	6.52	31.63	0	0	
Threatened and Endangered Species	Low	Low	Low	Low	Low	Low	Low	Low	Low	Low	Low	Low	Low	Low	Low	Low	0	Low	
Contamination Sites (high/medium)	43	10	1	2	2	3	0	1	0	2	3	0	0	1	1	6	0	14	
Right-of-Way Needs																			
Right-of-Way to be Acquired for Roadway Improvements (ac.)	137.5	15.1	5.1	4.5	5.4	4.6	10.3	10.4	9.6	9	8.5	11.1	14	13.6	13.1	32.1	0	33.8	
Right-of-Way to be Acquired for Stormwater Facilities (ac.)		15.4	0	0	0	1.2	1.7	1.7	8.66	4.8	4.8	1.5	1.8	1.8	1.8	13.4	0	7.3	
Right-of-Way to be Acquired for Floodplain Compensation (ac.)		1.4	0	0	0	0	0	0	0	15.7	1.4	1.4	0.7	6.5	6.5	6.5	31.6	0	0
Estimated Total Project Costs (2015 Cost)																			
Design ²	\$2,410,000	\$1,645,000	\$546,000	\$546,000	\$546,000	\$504,000	\$987,000	\$987,000	\$648,000	\$619,200	\$619,200	\$864,000	\$1,036,800	\$1,036,800	\$1,036,800	\$4,491,000	\$0	\$2,534,400	
Wetland Mitigation ³	N/A	\$102,000	\$0	\$0	\$0	\$0	\$192,000	\$207,000	\$21,000	\$192,000	\$42,000	\$0	\$328,500	\$313,500	\$225,000	\$1,005,000	\$0	\$24,000	
Road Right of Way	\$52,740,000	\$28,906,000	\$10,117,800	\$11,216,800	\$8,817,400	\$6,968,600	\$10,781,200	\$10,786,100	\$3,163,000	\$8,964,100	\$8,862,900	\$11,207,100	\$16,294,600	\$17,494,900	\$16,128,500	\$41,953,600	\$0	\$41,193,000	
Pond Right of Way		\$5,612,000	\$2,586,000	\$2,586,000	\$2,586,000	\$493,000	\$1,066,000	\$1,066,000	\$8,218,000	\$1,610,000	\$1,610,000	\$1,597,000	\$4,477,000	\$4,477,000	\$4,477,000	\$19,351,000	\$0	\$5,230,000	
Roadway Construction	\$23,990,000	\$16,450,000	\$5,460,000	\$5,460,000	\$5,460,000	\$5,040,000	\$9,870,000	\$9,870,000	\$6,480,000	\$6,192,000	\$6,192,000	\$8,640,000	\$10,368,000	\$10,368,000	\$10,368,000	\$44,910,000	\$0	\$25,344,000	
Construction Engineering & Inspection ⁴	\$2,410,000	\$1,645,000	\$546,000	\$546,000	\$546,000	\$504,000	\$987,000	\$987,000	\$648,000	\$619,200	\$619,200	\$864,000	\$1,036,800	\$1,036,800	\$1,036,800	\$4,491,000	\$0	\$2,534,400	
Preliminary Estimate of Total Project Cost (2015 Cost)	\$81,550,000	\$54,360,000	\$19,255,800	\$20,354,800	\$17,955,400	\$13,509,600	\$23,883,200	\$23,903,100	\$19,178,000	\$18,196,500	\$17,945,300	\$23,172,100	\$33,541,700	\$34,727,000	\$33,272,100	\$116,201,600	\$0	\$76,859,800	

1. Number of noise sensitive sites that meet or exceed FHWA NAC.
 2. Design cost is estimated at 10% of the total construction cost.
 3. Mitigation Costs are estimated based on \$150,000 per acre of impacted wetlands. Final mitigation costs will be determined through consultation with environmental agencies.
 4. Construction Engineering & Inspection Costs are estimated at 10% of the total construction cost.



Recommended Alternative

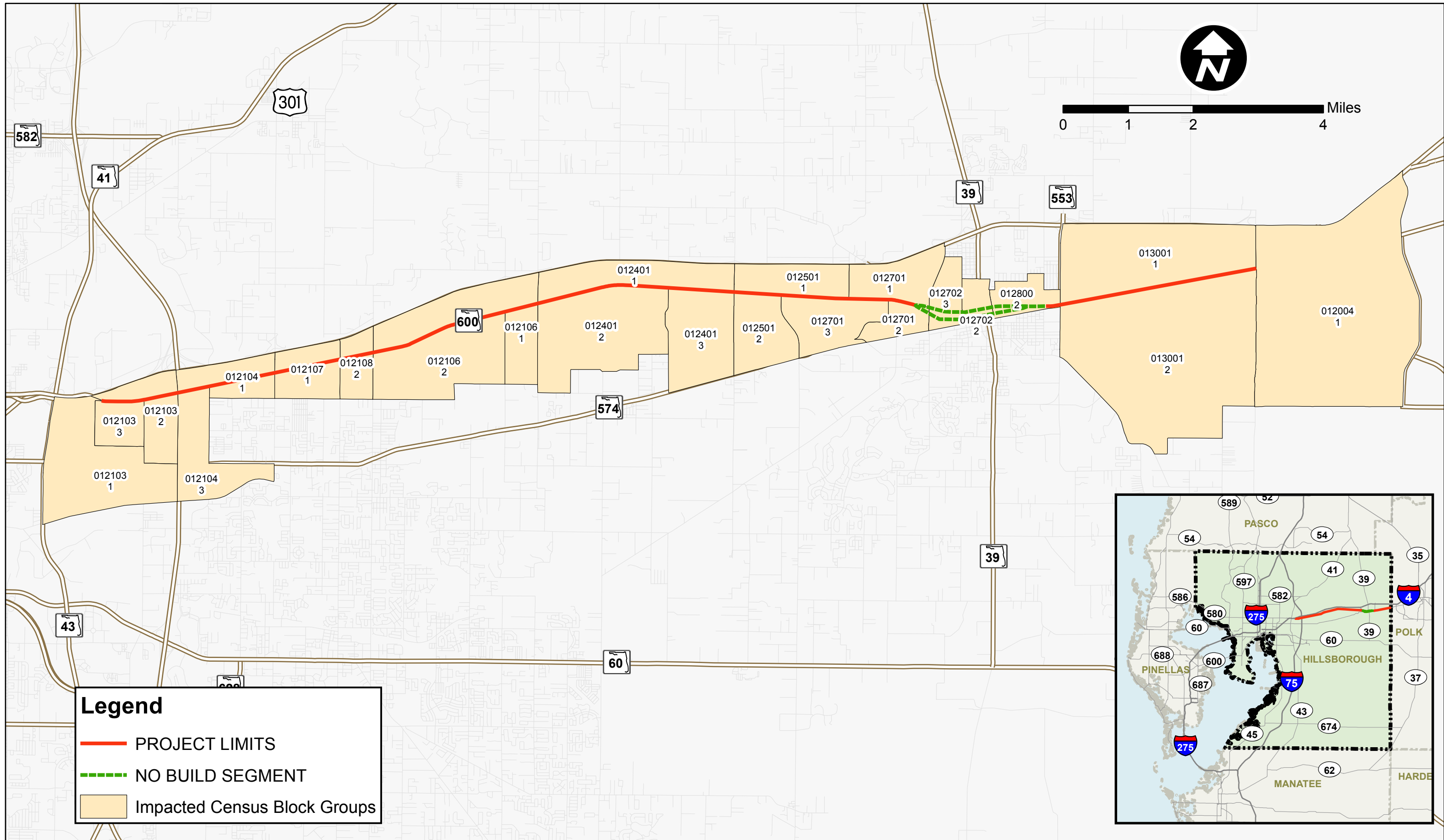
3 Project Approach

The purpose of the Final CSR is to evaluate and address any potential effects that the project may have on the communities surrounding US 92 within the project limits and their resources. The Final CSR focuses on the residences and businesses that will be directly impacted by the recommended alignment and will require special relocation needs.

The characteristics of the communities that surround the project location have been determined through the analysis of demographic data. Baseline data was collected from census block groups that intersect the project alignment. These baseline characteristics are then compared with state and county census data to determine the demographic characteristics of the impacted communities relative to the surrounding geography.

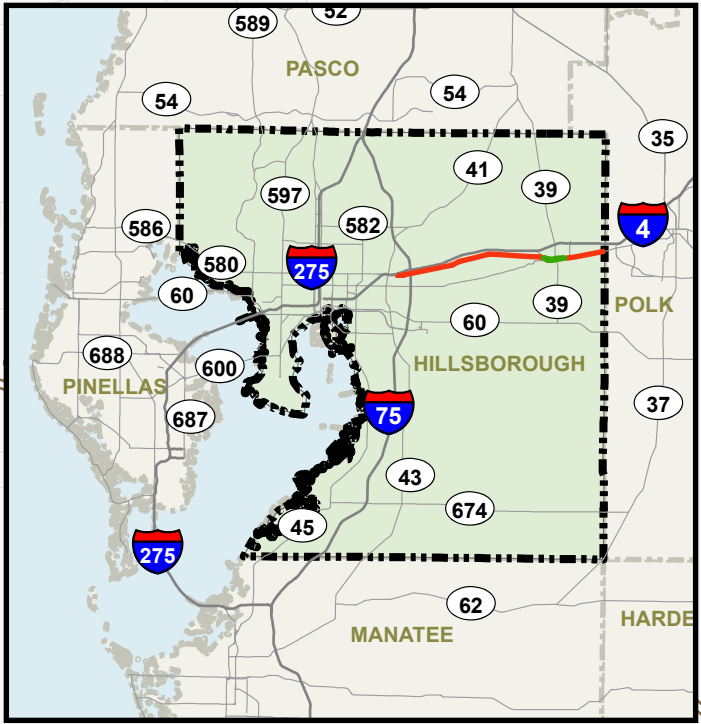
4 Demographics

This section outlines the demographic conditions of the Census block groups that are directly impacted by the recommended alignment of US 92. Using data from the most recent 5-year American Community Survey (ACS 2010-2014) retrieved from the U.S Census Bureau, GIS analysis was performed to identify 23 Census block groups which intersect with the recommended roadway alignment. The impacted block groups are shown in Figure 4-1. Major demographic indicators such as, population, housing, and income statistics were evaluated for these impacted block groups as well as Hillsborough County and the State of Florida in order to determine if the project will have a disproportionate impact on low income and/or minority populations.



Legend

- PROJECT LIMITS
- - - NO BUILD SEGMENT
- Impacted Census Block Groups



4.1 Population

Table 4-1 summarizes the total population of the State of Florida, Hillsborough County and the 23 block groups in which the preferred alignment of US 92 passes through. Compared to 2010 Census estimates the population of Hillsborough County has increased by approximately 50,000 residents (4.1%), population growth statewide was approximately 3% during this same period of time.

**Table 4-1
Population**

Impacted Block Groups	Hillsborough County	State of Florida
38,239	1,279,668	19,361,729

Source: US Census, 2010-2014 ACS 5 Year Estimates, Table B01001

4.1.1 Population by Age and Race

The population impacted by the recommended alignment of US 92 is predominantly white. All block groups have a majority white population with the exception of Census Tract 012103 - Block Group 2 which has a majority Asian Alone population of 36.5%. Table 4-2 summarizes the population of the impacted block groups by race.

Overall the White Alone population accounts for approximately 73% of the impacted area. The African American Alone population accounts for approximately 12% of the total population, Some Other Race Alone accounts for approximately 8%, and the remainder of the population is composed of American Indian or Native Alaskan Alone, Native Hawaiian or Pacific Islander Alone, Asian Alone and Two or More Races.

According to ACS 2010 - 2014 data, approximately 10% of the total population of the block groups impacted by the preferred alignment of US 92 is over 65, and there are 2,780 households (approximately 21 percent) in the impacted block groups that contain one or more person over the age of 65.

Disability data is not reported at the block group level but it is anticipated that the percentage of disabled population within the impacted block groups is similar to that of Hillsborough County. Overall, the percentage of disabled residents within Hillsborough County is approximately 12 percent, which is less than the percentage of disabled residents in the State of Florida (approximately 13 percent). Overall, the population characteristics of the study area follow the same trends as the greater population of Hillsborough County.

4.1.1.1 Hispanic or Latino Population

Within the 23 impacted block groups, approximately 30 percent of the total population identifies as having Hispanic or Latino origins. However, within individual block groups the percentages range from approximately 69 percent to 8.1 percent of the population. Table 4-3 summarizes the population of the impacted block groups that identify as having Hispanic or Latino Origins.

**Table 4-2
Population by Race**

Census Tract	Block Group Number	Race of Population														Block Group Totals
		White alone		African American Alone		American Indian or Native Alaskan Alone		Asian Alone		Native Hawaiian or Pacific Islander Alone		Some Other Race Alone		Two or More Races		
		No.	%	No.	%	No.	%	No.	%	No.	%	No.	%	No.	%	
012004	Block Group 1	1963	83.9%	124	5.3%	0	0.0%	59	2.5%	0	0.0%	195	8.3%	0	0.0%	2,341
012701	Block Group 1	1118	82.1%	142	10.4%	0	0.0%	69	5.1%	0	0.0%	9	0.7%	23	1.7%	1,361
012701	Block Group 2	707	64.3%	21	1.9%	12	1.1%	3	0.3%	0	0.0%	350	31.8%	7	0.6%	1,100
012701	Block Group 3	3621	77.6%	435	9.3%	0	0.0%	0	0.0%	0	0.0%	537	11.5%	73	1.6%	4,666
012401	Block Group 1	962	81.6%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	161	13.7%	56	4.7%	1,179
012401	Block Group 2	1058	99.2%	9	0.8%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	1,067
012401	Block Group 3	574	69.5%	104	12.6%	121	14.6%	0	0.0%	0	0.0%	0	0.0%	27	3.3%	826
012501	Block Group 1	457	71.2%	11	1.7%	0	0.0%	0	0.0%	0	0.0%	151	23.5%	23	3.6%	642
012501	Block Group 2	1329	85.4%	181	11.6%	7	0.4%	0	0.0%	0	0.0%	0	0.0%	40	2.6%	1,557
012800	Block Group 2	1144	79.1%	29	2.0%	0	0.0%	0	0.0%	0	0.0%	198	13.7%	76	5.3%	1,447
012104	Block Group 3	1201	44.9%	847	31.7%	0	0.0%	0	0.0%	0	0.0%	591	22.1%	36	1.3%	2,675
012104	Block Group 1	831	86.5%	105	10.9%	25	2.6%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	961
012103	Block Group 2	542	35.4%	176	11.5%	0	0.0%	558	36.5%	135	8.8%	118	7.7%	0	0.0%	1,529
012106	Block Group 1	1057	77.0%	119	8.7%	0	0.0%	82	6.0%	0	0.0%	69	5.0%	46	3.4%	1,373
012106	Block Group 2	1851	78.1%	188	7.9%	0	0.0%	167	7.0%	0	0.0%	9	0.4%	156	6.6%	2,371
012106	Block Group 1	810	81.8%	128	12.9%	0	0.0%	0	0.0%	0	0.0%	52	5.3%	0	0.0%	990
012103	Block Group 1	1146	52.6%	545	25.0%	0	0.0%	69	3.2%	0	0.0%	29	1.3%	391	17.9%	2,180
012108	Block Group 2	1026	79.0%	231	17.8%	0	0.0%	26	2.0%	0	0.0%	16	1.2%	0	0.0%	1,299
012702	Block Group 2	1099	98.0%	23	2.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	1,122
012702	Block Group 3	919	77.2%	102	8.6%	0	0.0%	0	0.0%	0	0.0%	147	12.3%	23	1.9%	1,191
013001	Block Group 1	2540	79.6%	238	7.5%	0	0.0%	0	0.0%	0	0.0%	272	8.5%	142	4.4%	3,192
013001	Block Group 2	618	100.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	0	0.0%	618
012103	Block Group 3	1362	53.4%	645	25.3%	45	1.8%	239	9.4%	0	0.0%	83	3.3%	178	7.0%	2,552
Total		27,935	73.1%	4,403	11.5%	210	0.5%	1,272	3.3%	135	0.4%	2,987	7.8%	1,297	3.4%	38,239

Source: US Census, 2010-2014 ACS 5 Year Estimates, Table B02001

**Table 4-3
Hispanic or Latino Origin Summary**

Census Tract	Block Group Number	Total Population	Population with Hispanic or Latino Origin	Percent with Hispanic or Latino Origin
012004	Block Group 1	2,341	288	12.3%
012103	Block Group 2	1,529	383	25.0%
012103	Block Group 1	2,180	193	8.9%
012103	Block Group 3	2,552	734	28.8%
012104	Block Group 3	2,675	997	37.3%
012104	Block Group 1	961	128	13.3%
012106	Block Group 1	1,373	529	38.5%
012106	Block Group 2	2,371	369	15.6%
012107	Block Group 1	990	124	12.5%
012108	Block Group 2	1,299	153	11.8%
012401	Block Group 1	1,179	291	24.7%
012401	Block Group 2	1,067	137	12.8%
012401	Block Group 3	826	198	24.0%
012501	Block Group 1	642	269	41.9%
012501	Block Group 2	1,557	486	31.2%
012701	Block Group 1	1,361	173	12.7%
012701	Block Group 2	1,100	564	51.3%
012701	Block Group 3	4,666	3,227	69.2%
012702	Block Group 2	1,122	434	38.7%
012702	Block Group 3	1,191	355	29.8%
012800	Block Group 2	1,447	584	40.4%
013001	Block Group 1	3,192	656	20.6%
013001	Block Group 2	618	50	8.1%
Totals		38,239	11,322	29.6%

Source: US Census, 2010-2014 ACS 5 Year Estimates, Table B03002

4.2 Housing and Income

Within the impacted block groups there are 15,357 total housing units available. Of these units, 13,513 are occupied and 1,844 are vacant, giving the project area a 12% vacancy rate which is consistent with the vacancy rate seen in greater Hillsborough County. Some of these vacant units in the impacted block groups could potentially be utilized for the relocation of displaced residents.

Of the occupied housing units 51% are owner occupied and approximately 49% are renter occupied units. Although the majority of housing in the project area is owner occupied, the percentage is lower than the owner-occupied housing rates seen in Hillsborough County (59.3%) and the state of Florida (66.1%) for the years 2010-2014.

Of the occupied housing units in the project area there are 1,581 households that include five persons or more. This is about 12% of all occupied housing units in the impacted block

groups. This is slightly higher than the 7.4% of households which hold over five persons countywide.

**Table 4-4
Housing Vacancy and Occupancy**

Housing	Impacted Block Groups		Hillsborough County	
	No. of Units	%	No. of Units	%
Occupied Housing Units	13,513	88.0	477,472	87.7
Vacant Housing Units	1,844	12.0	66,787	12.3
Total Housing Units	15,357	100.0	544,259	100.0
Occupancy Type	No. of Units	%	No. of Units	%
Owner Occupied	6,943	51.4	283,064	59.3
Renter Occupied	6,570	48.6	194,408	40.7

Source: US Census, 2010-2014 ACS 5 Year Estimates, Tables B25001, B25002, B25003

The majority of existing housing in the impacted block groups was built within the past 40 years. Almost 30% of the existing housing stock was built between the years 2000 and 2009, and approximately 35% was built from 1980-1999. Around 22% of the total housing stock was built in, or before 1969.

**Table 4-5
Housing Stock Build Dates**

Housing- Year Built	Impacted Block Groups		Hillsborough County	
	No. of Units	%	No. of Units	%
2010 or Later	231	1.5	9,303	1.7
2000-2009	4458	29.0	122,860	22.6
1990-1999	2478	16.1	96,236	17.7
1980-1989	2755	17.9	111,640	20.5
1970-1979	1969	12.8	83,441	15.3
1960-1969	1614	10.5	50,168	9.2
1950-1959	1057	6.9	41,470	7.6
1940-1949	324	2.1	12,465	2.3
1939 and earlier	471	3.1	16,676	3.1
Total	15,357	100.0%	544,259	100.0

Source: US Census, 2010-2014 ACS 5 Year Estimates, Table B25034

The average Median Household income of the block groups that will be impacted by the recommended alignment is \$44,326. Median household incomes of individual block groups range from as low as \$15,904 to as high as \$72,100 per year within impacted area. Average median household incomes in the impacted block groups are approximately \$6,000 less than the average median household income in Hillsborough County in 2014, and approximately \$3,000 less than the median household income of the state of Florida during that same time period.

**Table 4-6
Median Household Income by Geography**

Median Household Income (\$)		
Impacted Block Groups	Hillsborough County	State of Florida
44,326	50,122	53,482

Source: US Census, 2010-2014 ACS 5 Year Estimates, Table B19013 and US Census QuickFacts

With the exception of a slightly smaller percentage of owner occupied housing, the demographic characteristics seen along the project corridor follow the same general trends that can be found in surrounding Hillsborough County. The demographic data analysis for the block groups that are impacted by the recommended alignment reveals that these areas do not contain a significant population of minority and/or low income residents that will be disproportionality affected by the relocations.

5 Relocation Impacts

This section of the Final CSRP will identify all potential ROW impacts and residential and commercial displacements that are projected due to the recommended build alternatives of US 92. Table 5-1 summarizes all potential relocations associated with the recommended alignment of US 92.

**Table 5-1
US 92 Potential Relocations**

Affected Property	Number of Relocations
Residential	120
Business	110
Public	0
Signs	N/A

5.1 Potential Residential and Business Relocations

The recommended alignment of US 92 acquires ROW from 477 parcels throughout the project corridor, however not all of these properties will be displaced. It is estimated that the roadway improvements will require the relocation of 230 total properties. Of these, 120 are residences and 110 are businesses. Table 5-2 lists all 110 potential business relocations and an estimated number of employees.

Although there will be numerous residential and commercial relocations, the potential construction of this alternative is not expected to subdivide neighborhoods, negatively impact residential neighborhood identity, separate residences from community facilities, such as places of worship, schools, shopping areas, or civic or cultural facilities, and business relocations are not expected to impact the economy of the adjacent community. The relocations are not expected to disproportionately affect or contribute to social isolation of any special populations of elderly, handicapped, minority, or transient dependents.

The Greater Plant City Chamber of Commerce was contacted by phone on October 25, 2016 for guidance regarding potential business relocation strategies. The request was forwarded to Jake Austin, President and CEO of the Plant City Economic Development Corporation; however, no response was received. Other resources for displaced business owners can be found through the District right-of-way office, which maintains a list of small business financial referrals available upon the request of displacees.

Special Relocation Assistance

Through analyzing Census data patterns for the impacted block groups and Hillsborough County a rough estimate of households that may require special relocation assistance can be developed. Approximately 21 percent of the households in the impacted block groups contain one or more person over the age of 65, approximately 12 percent of households contain five or more family members, and approximately 12 percent of the population identify as having some type of disability. By applying these rates to the estimated number of residential relocations it is anticipated that there will be approximately 25 households that contain at least one person over the age of 65, and approximately 14 households containing over 5 family members among the relocations. Additionally, approximately 4,589 residents that identify as disabled are likely to be living within the study area.

**Table 5-2
Potential Business Relocations**

Business Relocation Name	Address	Type	No. of Employees*
A Cantu Fresh Produce	2904 East Baker Street, Plant City FL 33566	Produce Stand	4
A Lot Car Lot Inc.	10805 US 92, Tampa FL 33610	Automotive Service / Sales	5
Absolute Auto Repair	13095 US 92, Dover FL 33572	Automotive Service / Sales	5
American Legion Post 26	2207 W Baker Street, Plant City FL 33563	Food / Restaurant	8
American Lighting and Signalization	12720 US 92, Seffner FL 33584	Manufacturing	8
Anna's Restaurant	3412 W Baker Street, Plant City FL 33563	Food / Restaurant	8

Business Relocation Name	Address	Type	No. of Employees*
Antiques and Vintage Store	4815 US 92, Plant City FL 33563	Retail	4
ATM Express	930 US 92, Seffner FL 33584	Gas / Convenience Store	9
Auto Search One	3726 US 92, Plant City FL 33566	Automotive Service / Sales	5
Bills Bait and Tackle	9806 US 92, Tampa FL 33610	Gas / Convenience Store	9
Boost Mobile	4288 W Hillsborough Ave, Plant City FL 33563	Retail	4
BP Gas Station	10907 US 92, Seffner FL 33584	Gas / Convenience Store	9
Bravo Supermarket	3412 W Baker Street, Plant City FL 33563	Food / Restaurant	30
Broadway Used Tire and Salvage	3502 W Baker Street, Plant City FL 33563	Automotive Service / Sales	1
Broke and Poor Surplus Building Materials Inc.	2670 US 92, Plant City FL 33566	Retail	8
Car Mex Auto Sales	103 Son Keen Road, Plant City FL 33566	Automotive Service / Sales	2
Carousel Village Mobile Home Park	10016 East Hillsborough Ave	Office	1
Child Abuse for Children Medical	3108 W Azele Street, Plant City FL 33563	Medical	6
Circle K	706 US 92, Seffner FL 33584	Gas / Convenience Store	9
Citgo Foodmart	11305 US 92, Seffner FL 33584	Gas / Convenience Store	9
Computers	4288 W Hillsborough Ave, Plant City FL 33563	Retail	4
Country Deli and Grocery	13606 US 92, Dover FL 33572	Food / Restaurant	8
County Line Road Auction	3706 US 92, Plant City FL 33566	Automotive Service / Sales	3
Dave's Discount Auto Parts	10830 US 92, Tampa FL 33610	Automotive Service / Sales	5
DiversiMed, Inc.	9913 E Hillsborough Ave, Tampa FL 33610	Medical	6

Business Relocation Name	Address	Type	No. of Employees*
Dolphin Tanker Systems	2550 US 92, Plant City FL 33566	Manufacturing	8
Excellence Organic Nail Spa	12019 US 92, Seffner FL 33584	Service	3
Family Motors	5413 US 92, Plant City FL 33566	Automotive Service / Sales	5
Game Gear Designer	11201 US 92, Seffner FL 33584	Service	2
Git N Go Citgo	5320 N Falkenburg Road, Tampa FL 33610	Gas / Convenience Store	9
Golden Rule Child Care	105 Thrasher Road, Plant City FL 33566	Education / Child and Elderly Care	4
Green Goddess Smoke Shop	10907 US 92, Seffner FL 33584	Retail	4
GTO Auto Sales	3808 US 92, Plant City FL 33566	Automotive Service / Sales	5
Happy Homes Mobile Home Park	102 Happy Homes Lane, Plant City PL 33566	Office	1
Harwell Farms Produce Stand	4904 W 92, Plant City FL 33563	Produce Stand	4
Hooker John D Attorney LLC	13610 US 92, Dover FL 33572	Office	2
Horizon Realty	4306 US 92, Plant City FL, 33563	Office	4
Hungry Howies Pizza	2424 US 92, Plant City FL 33563	Food / Restaurant	8
Ibaw Local Union 108	5410 N Falkenburg Rd, Tampa FL 33610	Service	5
Ice Cream Shop	12019 US 92, Seffner FL 33584	Food / Restaurant	6
Iglesia De Dios Torre Fuerte Church	5314 Lenoir Court, Plant City FL 33566	Religious	5
Jarrett Scott Ford	2000 E Baker Street, Plant City FL 33563	Automotive Service / Sales	75
Jay & R Wholesale Exchange	11558 US 92, Seffner FL 33584	Service	1
Kelly's Hide Away Lounge	1425 US 92, Seffner FL 33584	Food / Restaurant	10
Kennedy Hill Pub	11801 US 92, Seffner FL 33584	Food / Restaurant	10
Labor Ready	10907 US 92, Seffner FL 33584	Office	4

Business Relocation Name	Address	Type	No. of Employees*
Las Tres Mexican Food	3300 US 92, Plant City FL 33566	Food / Restaurant	8
Laundromat	3504 W Baker Street, Plant City FL 33563	Service	1
Laundromat	3412 W Baker Street, Plant City FL 33563	Service	1
Light House Ministries Thrift Shop	2330 W Baker Street, Plant City FL 33563	Retail	4
Lorena's Decorations	4288 W Hillsborough Ave, Plant City FL 33563	Retail	4
Maintain A Plant Nursery	13009 US 92, Dover FL 33572	Retail	4
Manny's Barber Shop	3412 W Baker Street, Plant City FL 33563	Retail	4
Marathon Gas Station	12901 US 92, Dover FL 33527	Gas / Convenience Store	9
Marcos Y Mauricio	4288 W Hillsborough Ave, Plant City FL 33563	Office	1
Martha's Family Restaurant	12019 US 92, Seffner FL 33584	Food / Restaurant	10
Metro PCS	10907 US 92, Seffner FL 33584	Service	5
Metro PCS	3412 W Baker Street, Plant City FL 33563	Service	2
Miguel's Automotive	5415 Baptist Church Rd, Tampa FL 33610	Automotive Service / Sales	5
Miss Ginny's Childcare Inc.	11825 US 92, Seffner FL 33584	Education / Child and Elderly Care	4
Mr. Sub X press	10907 US 92, Seffner FL 33584	Food / Restaurant	8
Norbs Automotive	10341 US 92, Tampa FL 33610	Automotive Service / Sales	8
One Accord Church	2203 W Baker Street, Plant City FL 33563	Religious	5
Parkesdale Farmers Market	3207 W Baker Street, Plant City FL 33563	Produce Stand	10

Business Relocation Name	Address	Type	No. of Employees*
Pay Day Advance	3514 W Baker Street, Plant City FL 33563	Service	1
PC Motors	4306 US 92, Plant City FL, 33563	Automotive Service / Sales	5
Penske Truck Rental	4288 W Hillsborough Ave, Plant City FL 33563	Automotive Service / Sales	1
Plant City Citgo	5117 US 92, Plant City FL 33566	Gas / Convenience Store	9
Plant City Discount Factory Outlet	2312 US 92, Plant City FL 33563	Manufacturing	8
Plant City Restaurant	2005 W Baker Street, Plant City FL 33563	Food / Restaurant	8
Plant City Steel Company	2020 US 92, Plant City FL 33563	Manufacturing	NA
Power Source Marine	10909 US 92, Seffner FL 33584	Retail	8
Priority Motors LLC	9912 E Hillsborough Ave, Tampa FL 33610	Automotive Service / Sales	5
Produce Stand	5117 US 92, Plant City FL 33566	Produce Stand	4
Property Damage Appraisers	5437 Comfort Drive, Tampa FL 33610	Office	4
R&W Auto Sales	3602 US 92, Plant City FL 33566	Automotive Service / Sales	2
RA Wholesale Inc.	4288 W Hillsborough Ave, Plant City FL 33563	Office	1
Rainbow Rock Mini Storage	4288 W Hillsborough Ave, Plant City FL 33563	Service	2
RC Towing and Recovery	9718 US 92, Tampa FL 33610	Automotive Service / Sales	5
Right at Home Senior Care	1905 W Baker Street, Plant City FL 33563	Education / Child and Elderly Care	6
Roll Your Own Tobacco Store	12019 US 92, Seffner FL 33584	Retail	4
Roosters Restaurant	1024 US 92, Seffner FL 33584	Food / Restaurant	10
Salon J	12019 US 92, Seffner FL 33584	Service	3

Business Relocation Name	Address	Type	No. of Employees*
Shell Gas	706 US 92, Seffner FL 33584	Gas / Convenience Store	9
Showgirls Gentlemen's Club	4210 US 92, Plant City FL 33566	Service	10
Smart Kids Academy Pre School	3512 W Baker Street, Plant City FL 33563	Education / Child and Elderly Care	4
Speed Wash Coin Laundry	5449 Mobile Drive, Seffner FL 33584	Service	1
Speedway Gas Station	12877 US 92, Dover FL 33527	Gas / Convenience Store	9
Star Motel	10809 US 92, Tampa FL 33610	Hotel / Lodging	5
Stoneworks of Tampa	3404 US 92, Plant City FL 33566	Manufacturing	8
Strawberry Country Fruit Stand	3207 W Baker Street, Plant City FL 33563	Produce Stand	4
Suites Motel	11507 US 92, Seffner FL 33584	Hotel / Lodging	5
Sun Auto Group	2290 US 92, Plant City FL 33563	Automotive Service / Sales	2
Sunoco Gas Station	3501 W Baker Street, Plant City FL 33563	Gas / Convenience Store	9
Superior Crane and Rigging Inc.	10929 US 92, Seffner FL 33584	Service	10
SurvTech Solutions Inc	10219 US 92, Tampa FL 33610	Office	4
T and J Gonzaless Property Management	11817 US 92, Seffner FL 33584	Office	4
Tampa East RV Resort	12720 US 92, Seffner FL 33584	Office	1
Team Fischel Construction	2021 US 92, Plant City FL 33563	Service	10
Thrift Store	10907 US 92, Seffner FL 33584	Retail	4
Tooth Caboose Dental	2005 Thonotosassa Rd., Plant City FL 33563	Medical	6
Torrissi and Sons Gas Station	10615 E Hillsborough Ave., Tampa FL 33610	Gas / Convenience Store	9

Business Relocation Name	Address	Type	No. of Employees*
Tortilleria La Sabrosa	3412 W Baker Street, Plant City FL 33563	Food / Restaurant	8
TR Drum and Freight	3910 US 92, Plant City FL 33566	Manufacturing	18
U Haul Neighborhood dealer	301 US 92, Seffner FL	Retail	4
Unified Associate Signs	2610 East Baker Street, Plant City FL 33566	Manufacturing	8
Used Car Sales (Owner: Ramon Pabon)	11225 US 92, Seffner FL 33584	Automotive Service / Sales	5
Vacant Office	9913 E Hillsborough Ave, Tampa FL 33610	Office	NA
Vacant Office	9913 E Hillsborough Ave, Tampa FL 33610	Office	NA
X Press Mart	2424 US 92, Plant City FL 33563	Gas / Convenience Store	9

*Employee estimates were made by evaluating general business types and size of space

5.2 Potential Sign Relocations

The recommended alignment of US 92 will not require the relocation of any signage throughout the project corridor. FDOT District 7 does not relocate signage, instead, impacted signs are purchased from property owners as part of the ROW acquisition process.

5.3 Potential Public Land Relocations

There will be ROW impacts to 11 publicly owned properties throughout the project corridor. Two of these properties are owned by Hillsborough County and the remaining nine properties are owned by FDOT.

Right-of-Way will need to be acquired from the Hillsborough County Fire Station #4, located on the corner of US 92 and Peach Avenue. Although ROW will be acquired there is no relocation anticipated for this location. The second Hillsborough County property is a 0.02 acre corner clip in the northeast corner of US 92 and Gallagher road. There will also be ROW acquired from an FDOT property located at 5825 US 92 in Plant City. This property is vacant land and will not require any relocations. The remaining public properties impacted by the recommended alignment are existing ROW and easement parcels and will not require any relocations.

5.4 Potential Contamination Site Impacts

A level 1 contamination evaluation was conducted and documented in a Final Contamination Evaluation Screening Report (CSER) (Tierra, Inc., April 2017) for this project. The report identified nine (9) high risk, and 50 medium risk sites that have the potential to contain hazardous waste or petroleum contamination. Several of these sites correspond with potential residential and business relocation sites. In accordance with FDOT guidelines, all sites that are determined to be impacted by the new alignments during the design phase will be subject to a Level 2 field screening as they have been determined to contain contaminants that could impact the proposed roadway improvements. Other technical studies may be required to determine the level of contamination on individual sites prior to project construction.

6 Acquisition and Relocation Assistance Program

In order to minimize the unavoidable effects of Right of Way acquisition and displacement of people, the Florida Department of Transportation (FDOT) will carry out a Right of Way Acquisition and Relocation Assistance Program in accordance with Florida Statute 339.09 and the Uniform Relocation Assistance and Real Property Acquisitions Policies Act of 1970 (Public Law 91-646 as amended by Public Law 110-17).

The Florida Department of Transportation provides advance notification of impending Right of Way acquisition. Before acquiring Right of Way, all properties are appraised on the basis of comparable sales and land use values in the area. Owners of property to be acquired will be offered and paid fair market value for their property rights.

No person lawfully occupying real property will be required to move without at least 90 days written notice of the intended vacation date, and no occupant of a residential property will be required to move until decent, safe, and sanitary replacement housing is made available. "Made available" means that the affected person has either by himself obtained and has the right of possession of replacement housing, or that the Florida Department of Transportation has offered the relocatee decent, safe and sanitary housing which is within his financial means and available for immediate occupancy.

At least one relocation specialist is assigned to each highway project to carry out the relocation assistance and payments program. A relocation specialist will contact each person to be relocated to determine individual needs and desires, and to provide information, answer questions, and give help in finding replacement property. Relocation services and payments are provided without regards to race, color, religion, sex, or national origin.

All tenants and owner-occupant displaces will receive an explanation regarding all options available to them, such as; (1) varying methods of claiming reimbursement for moving expenses; (2) rental replacement housing, either privately or publicly subsidized; (3) purchase of replacement housing; and (4) moving owner-occupied housing to another location.

Financial assistance is available to the eligible relocatee to:

1. Reimburse the relocatee for the actual reasonable costs of moving from homes, businesses, and farm operations acquired for a highway project.

2. Make up the difference, if any, between the amount paid for the acquired dwelling and the cost of comparable decent, safe and sanitary dwelling available on the private market, as determined by the Department.
3. Provide reimbursement of expenses, incidental to the purchase of a replacement dwelling.
4. Make payment for eligible increased interest cost resulting from having to get another mortgage at a higher interest rate. Replacement housing payments, and closing costs are limited to \$31,000 combined total.

A displaced tenant may be eligible to receive payment, not to exceed \$7,200, to rent a replacement dwelling or room, or to use as a down payment, including closing costs, on the purchase of a replacement dwelling.

Brochures that describe the Florida Department of Transportation's Relocation Assistance Program and Right of Way acquisition program are;

- Residential Relocation Under the Florida Relocation Assistance Program
- Relocation Assistance, Business, Farms and Non-profit Organizations
- Sign Relocation Under the Florida Relocation Assistance Program
- Mobile Home Relocation Assistance
- Relocation Assistance Program Personal Property Moves

All of these brochures are distributed at all public hearings and made available upon request to any interested persons.

7 Relocation Resources

Using GIS parcel data for Hillsborough County an analysis of sale prices of the 477 properties impacted by the ROW of the roadway alignment was performed. The analysis revealed that the average sale price of these properties was approximately \$137,000. Using this as guidance, searches for available property were limited to properties with listing prices of \$200,000 and under.

Properties included in this section were retrieved from online real estate sites on September 27, 2016. The exact properties are not likely to still be available at the time relocations will take place, however, they represent the types of properties that may be available at any given time in the areas surrounding US 92.

7.1 Available Residential Property

A search for residential properties for sale and for rent in the six zip codes that encompass the project area was performed using the online real estate site Zillow. The search revealed a total of 239 properties for sale, and another 94 properties available for rent at this time. The properties ranged in price from \$10,000 to the search limit of \$200,000 and included homes with 0 (studio) to 4 bedrooms. The available residential properties are composed of various architectural styles including manufactured homes. Rental properties found in the search displayed similar physical characteristics to the "for sale" homes and ranged in monthly rents from \$500-\$2,500. There was no maximum monthly rent price used to filter the rental property

search. Table 7-1 summarizes the available residential properties in each zip code surrounding the project location.

**Table 7-1
Available Residential Property Summary**

Project Zip Code	City	For Sale	For Rent	Total
33563	Plant City	57	12	69
33566	Plant City	30	10	40
33567	Plant City	16	3	19
33584	Seffner	57	26	83
33610	Tampa	68	39	107
33527	Dover	11	4	15
Total		239	94	333

Source: Zillow.com, September 26, 2016

7.2 Housing of Last Resort

Comparable Replacement housing for sale and rent is available in the Seffner/Plant City area of Hillsborough County, Florida. However, there may be some last resort rent supplements and last resort replacement housing payments necessary. Whenever the maximum allowable replacement housing payment for purchase or rent would be insufficient to ensure that a comparable replacement dwelling will be available on a timely basis to a displacee, the Department will provide additional or alternative assistance under Last Resort Housing Provisions. For additional information regarding Last Resort Housing Provisions, refer to the FDOT Right-of-Way Manual, Section 9.6, Last Resort Housing. Last resort housing payments would be used in order to place the relocates in decent, safe, and sanitary housing, if necessary. Should last resort housing be constructed, the housing would be available before the displaces are required to vacate their dwellings. There are numerous residential lots available for new construction within the Seffner/Plant City area. Lot sizes range from 3,500 sf to 95 acres and are priced from \$3,700 to \$1.9 million.

7.3 Available Commercial Property

A similar search for commercial properties in the six project zip codes was performed using the online commercial real estate site LoopNet. The search revealed a total of 47 commercial properties for sale, and 73 for lease in the six project zip codes. The for sale properties ranged in price from \$35,000 to \$3.6 million. There was no maximum property price or lease price used to filter this search, however, the search was filtered to exclude commercial property without an existing building. The rents for the spaces found in the project area ranged from \$2.31/sf to \$20.00/sf per year, with many rents listed as “negotiable”.

With 110 business relocations associated with the project and only 120 commercial spaces available in the surrounding zip codes at this time, finding comparable relocation sites may require increased assistance and coordination with business owners throughout the relocation process to ensure profitable business operations after the relocation.

There are several impacted businesses that may have special relocation needs due to the business type, size of the property needed for relocation, and the potential impact that relocation could have on the surrounding communities. Jarrett Scott Ford, and the Iglesia de Dios Torre Fuerte Church are two businesses that could potentially create a large adverse impact to the community if suitable relocation sites are not found for them. These cases are discussed further in the following sections. Other businesses that may require increased coordination in relocating are the numerous gas stations at key intersections throughout the project that rely on traffic from US 92 to support their business and one adult entertainment club that may require a commercial space with specific zoning needs to continue to operate. Table 7-2 summarizes the available commercial properties in each zip code surrounding the project location.

7.3.1 Iglesia de Dios Torre Fuerte Church

The Iglesia de Dios Torre Fuerte Church is located at 5314 Lenoir Court along US 92 in Plant City and is made up of a predominantly Spanish speaking congregation. The church is located in Census tract 012401, block group 3 (012401-3) which has a Hispanic and Latino Origin population of approximately 24 percent. However, the adjacent block groups 012501-1 and 012501-2 have higher Hispanic and Latino populations of approximately 41 percent and 31 percent, respectively. The location is also less than one-mile west of block group 012701-3 which has a majority Hispanic and Latino population of 69.2 percent.

Not only is Iglesia de Dios Torre Fuerte's successful operation after relocation dependent on having a surrounding population with similar cultural characteristics to the area where it is currently located, but the population that the church currently serves may be left without a viable alternative for worship in its absence as well. Spanish speaking churches that could provide for an alternative place of worship in the Plant City area include; Iglesia de Dios Ebenezer (approximately 8 miles away from Iglesia de Dios Torre Fuerte), Iglesia Dios Pentecostal (approximately 4 miles away), and Primera Iglesia Bautista La (approximately 4 miles away). Although these are all potential options, none of these three churches seem to be on the same scale of Iglesia de Dios Torre Fuerte in respect to overall size. Additionally, transportation to these locations may be more difficult to some of the population currently being served by Iglesia de Dios Torres Fuerte. A relocation that would serve the existing congregation should be a priority; however, if that is not an option at the time relocation occurs, coordination with local transit providers should be considered in order to connect the churches existing congregation with the new location.

7.3.2 Jarrett Scott Ford

The Jarrett Scott Ford dealership is located in the northwest quadrant of the intersection of US 92 and North Park Road in Plant City. The dealership occupies a 9.4-acre lot according to the Hillsborough County Property Appraiser, and is likely to have around 75 employees according to the Plant City Economic Development Corporation. Since an existing facility that matches Jarrett Scott Ford's need may not be available for sale or rent at the time of relocations, a new facility located on vacant land that is close to the existing customer base and easily accessible, may need to be considered.

A property search in the surrounding areas revealed one property located approximately 1.7 miles north of the existing dealership on N Park Road that could be a potential relocation site.

The available property is an approximately 13-acre lot with commercial zoning, over 660 feet of road frontage and is located approximately 2,000 feet from an interchange with I-4. The property is also located in close proximity to Stingray Chevrolet, an existing auto dealership. Many types of businesses, including auto dealers, can benefit from locating close to competitors in order to increase their customer base that may be traveling from far away to make their purchases. Planning and coordination for relocation of this scale must begin early on in the process to ensure that an available property is identified and, if necessary, construction of a new facility will be completed at the time that relocations will occur. No loss of jobs or adverse impact to the local economy is anticipated if the dealership can continue to operate effectively throughout the relocation process, and the relocation site remains within the Plant City limits. Appendix B provides a map showing the general location, and features of the potential property located on N Park Road.

**Table 7-2
Available Commercial Property Summary**

Project Zip Code	City	For Sale	For Rent	Total
33563	Plant City	19	17	36
33566	Plant City	3	9	12
33567	Plant City	0	2	2
33584	Seffner	5	4	9
33610	Tampa	19	39	58
33527	Dover	1	2	3
Total		47	73	120

Source: LoopNet.com, September 26, 2016

8 Relocation Resources and Assurances

Several resources are available within Hillsborough County to assist in finding new locations for the displaced residences and businesses, as well as helping property owners acquire financing for new property. These resources and their contact information are provided below.

U.S Dept. of Housing and Urban Development Region 4
Jacksonville Field Office
Charles E. Bennet Federal Building
400 W. Bay Street, Suite 1015
Jacksonville, FL 32202
Phone: 904-232-3759

Plant City Chamber of Commerce
106 North Evers Street
Plant City, Florida 33564
Phone: 813-754-3707
President: Christine Miller
Email: christine@plantcity.org

Plant City Community Development Corporation
620 S Collins Street
Plant City, FL 33563
Phone: 813-659-3100
Deputy Executive Director: Issac Barefield
Email: Nbarefield@plantcitycdc.org

Hillsborough County Affordable Housing Services
601 E. Kennedy Boulevard
24th Floor
Tampa, FL 33602
Phone: 813-612-5397
Email: TamesG@HCFLGov.net
Housing Finance Authority of Hillsborough County, Florida

Catholic Charities CDC
1213 16th Street
St. Petersburg, FL 33705
Phone: 727-893-1313
Email: catholic.charities@ccdosp.org

SBA Tampa Office
501 East Kennedy Boulevard, Suite 1210
Tampa, FL 33602
P: 813-228-2100 ext. 24
Robert Chavarria, SBA Senior Area Manager
robert.chavarria@sba.gov

USF Small Business Development Center
Port Authority Building
1101 Channelside Drive, Suite 210
Tampa, FL 33602
813-905-5800

USF Connect Building
3802 Spectrum Boulevard, Suites 109 and 111
Tampa, FL 33612-9220
Field Office (813) 974-9080 or (813) 974-5238

FSBDC at Hillsborough County
Entrepreneur Collaborative Center
2101 East Palm Avenue
Tampa, FL 33605-3915
Telephone: 813-204-YBOR (9267)
Fax: (813) 277-1387

US Housing & Urban Development Department
Housing Authority
Address: 500 E Zack Street, # 402
Tampa, FL 33602
Phone :(813) 228-2026

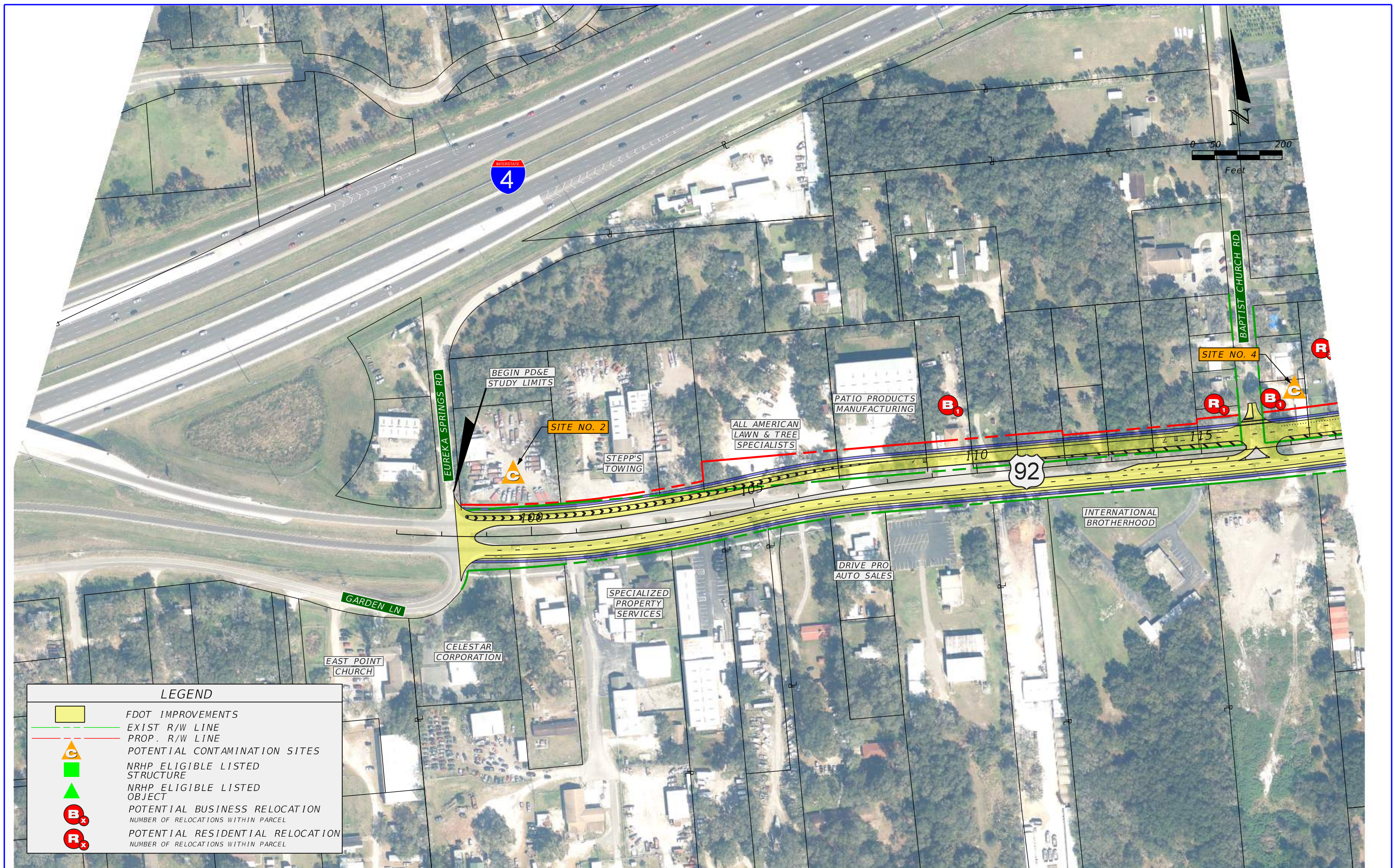
BBIF- Business Financing for Minority Owned Businesses
301 East Pine Street, Suite 175
Orlando, Florida 32801
Telephone: (407) 649-4780
Fax: (407) 649-8688
info@bbif.com
www.bbifflorida.com

9 References

- American Community Survey, 5-Year Estimates- Geodatabase Format, U.S. Census Bureau. <https://www.census.gov/geo/maps-data/data/tiger-data.html>
- American Fact Finder, U.S. Census Bureau. Various Tables. <http://factfinder.census.gov/faces/nav/jsf/pages/index.xhtml>
- Final Contamination Screening Evaluation Report- Tierra, Inc., April 2017
- Final Preliminary Engineering Report SR 600 (US 92) PD&E Study Re-Evaluation- Inwood Consulting Engineers, May 2017.
- FDOT. Project Development and Environment Manual. Part 2, Chapter 9, Section 9.3.4, Considerations for Evaluating Relocation Effects. August 25, 2016.
- FDOT. Right-of-Way Manual. Section 9.1, Relocation Assistance Program. April 15, 1999, Revised October 1, 2014.
- Hillsborough MPO, Imagine 2040 Long Range Transportation Plan
- Hillsborough MPO, Transportation Improvement Program Fiscal Years 2016/17-2020/21. Effective 10/01/16 - 09/30/17
- LoopNet.com [Online]. <http://www.loopnet.com/> . (September 27, 2016) and (October 26, 2016)
- Zillow.com [Online]. <http://www.zillow.com/homes/> . (September 27, 2016)

Appendix A

Preliminary Concept Plans



LEGEND	
	FDOT IMPROVEMENTS
	EXIST R/W LINE
	PROP. R/W LINE
	POTENTIAL CONTAMINATION SITES
	NRHP ELIGIBLE LISTED STRUCTURE
	NRHP ELIGIBLE LISTED OBJECT
	POTENTIAL BUSINESS RELOCATION NUMBER OF RELOCATIONS WITHIN PARCEL
	POTENTIAL RESIDENTIAL RELOCATION NUMBER OF RELOCATIONS WITHIN PARCEL

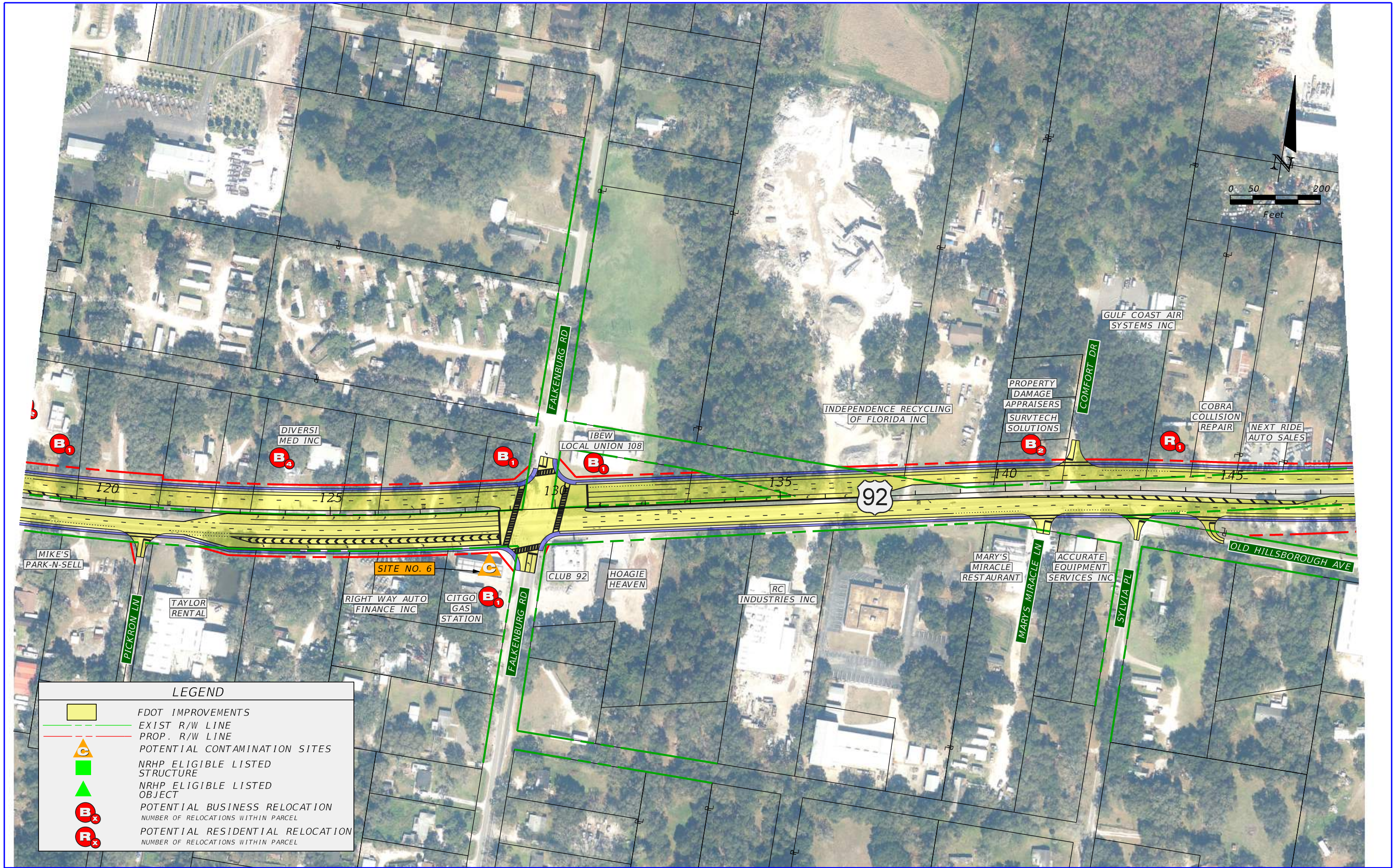
REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

ENGINEER OF RECORD
 Derek D. Dean, PE
 PE No. 51905
 Inwood Consulting Engineers, Inc.
 Certificate of Authorization No. 7074
 3000 Dovera Drive, Suite 200, Oviedo, Florida 32765
 P 407.971.8850

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
600	HILLSBOROUGH	435749-1-22-01

US 92 / SR 600
 PLAN SHEETS SEG 1
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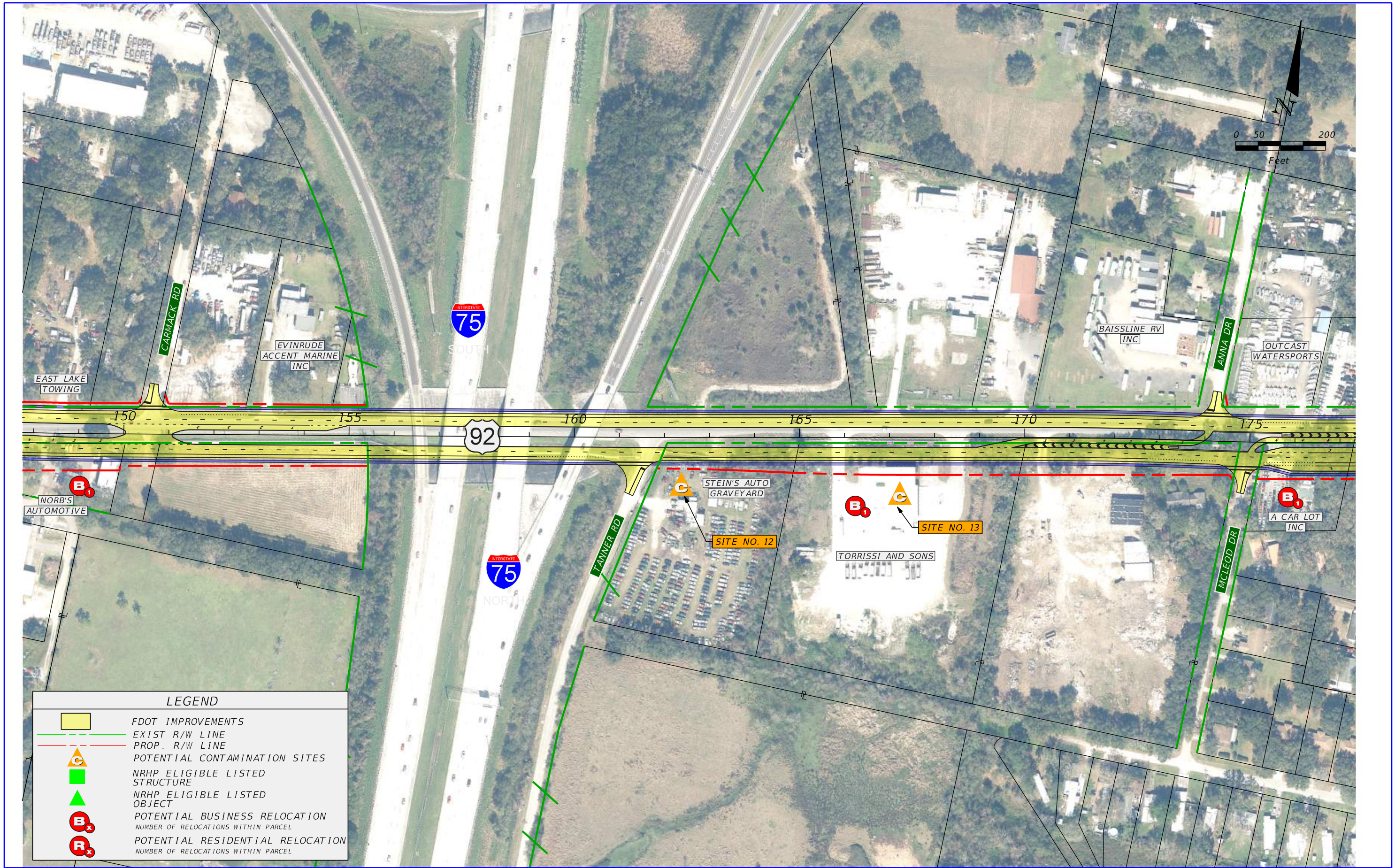
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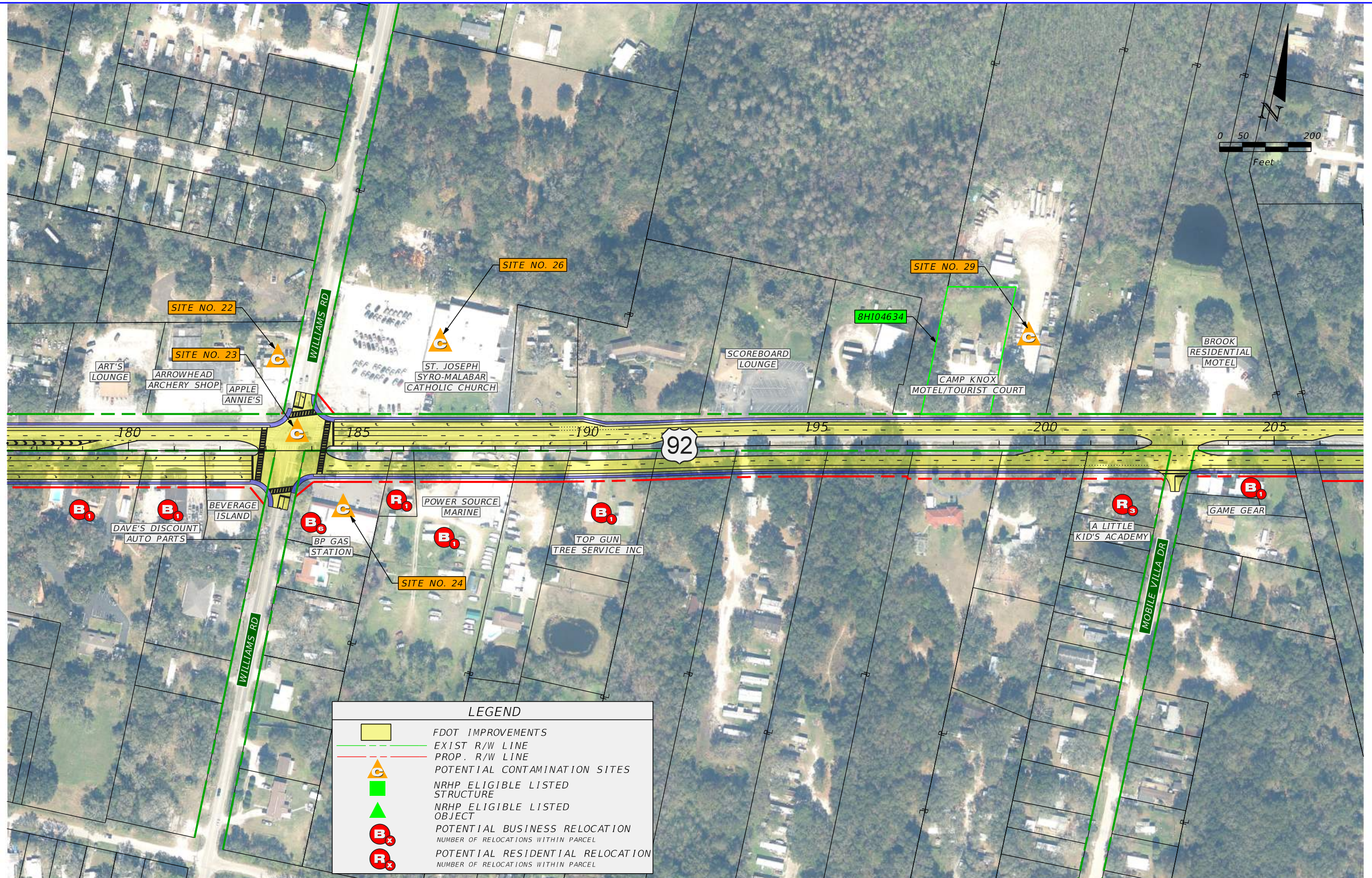
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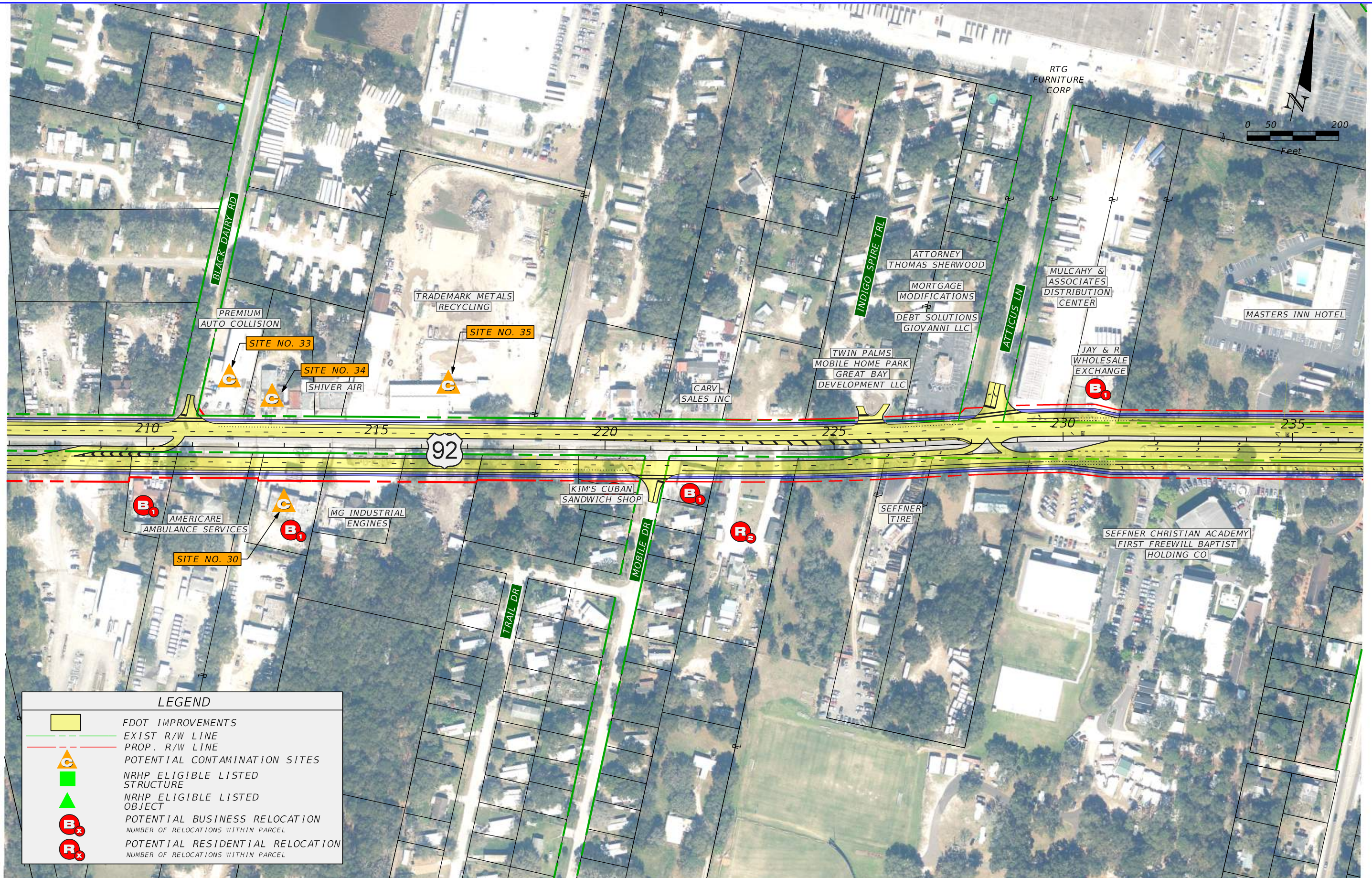
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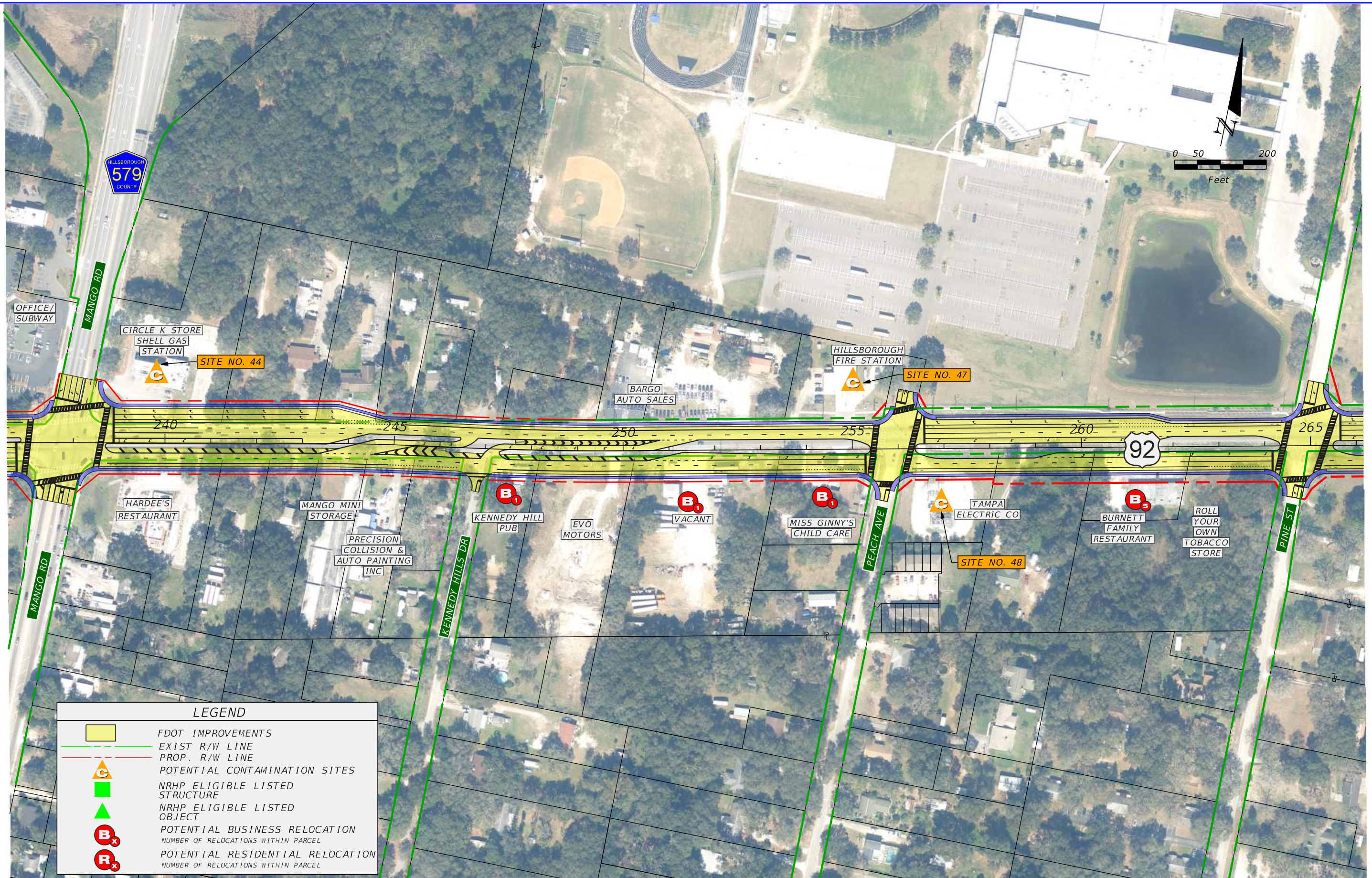
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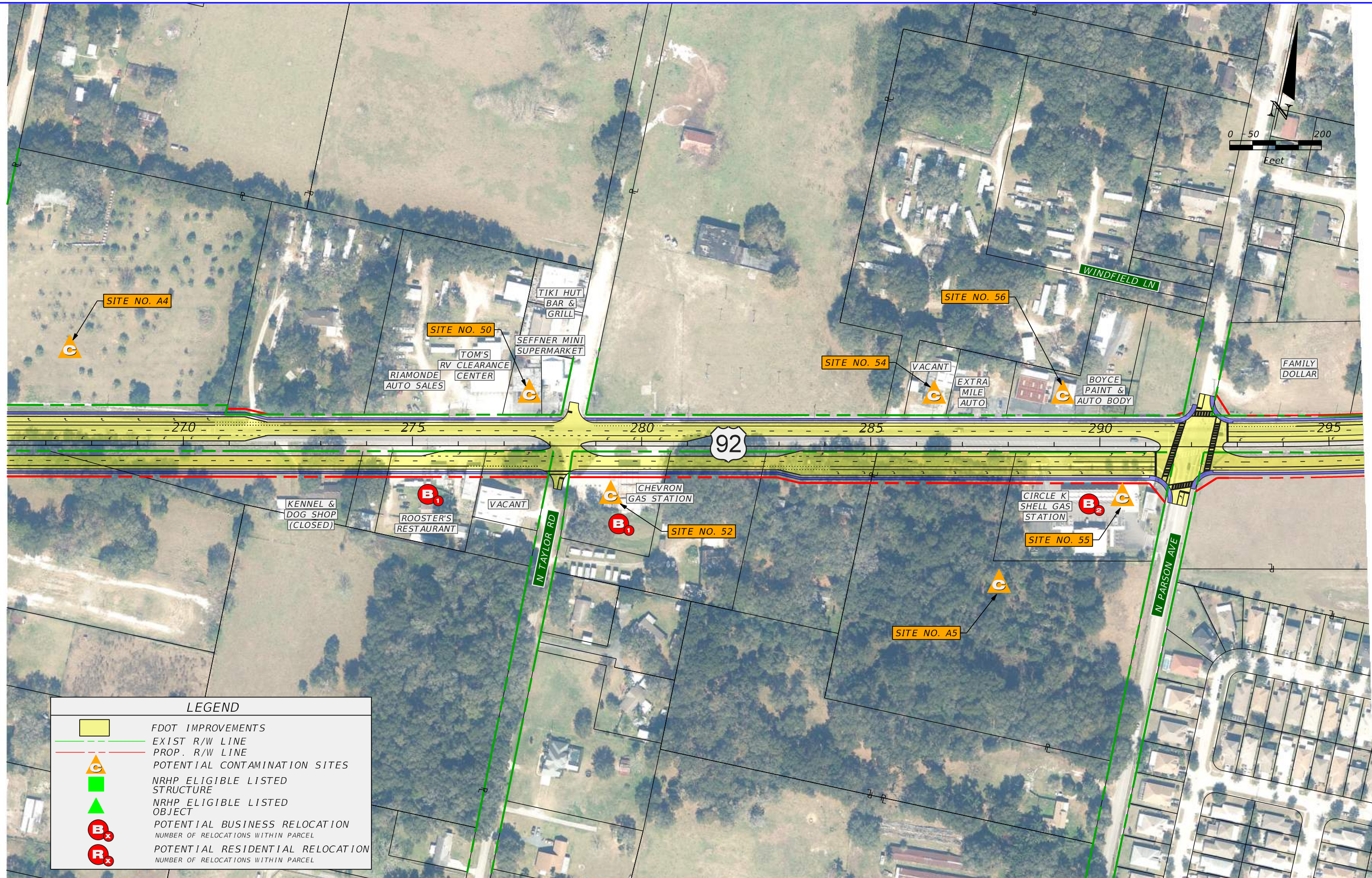
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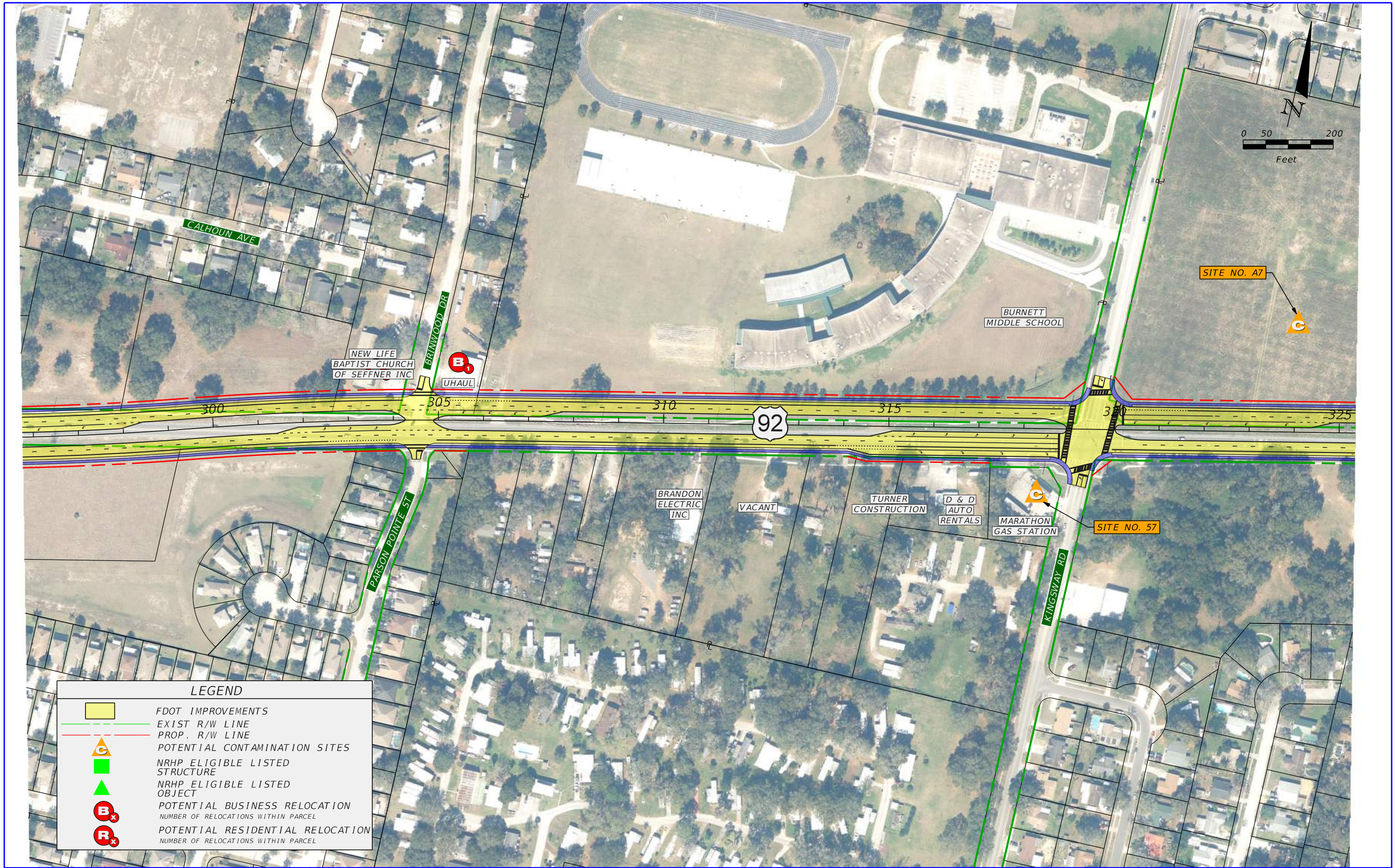
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- R_x POTENTIAL RESIDENTIAL RELOCATION
NUMBER OF RELOCATIONS WITHIN PARCEL

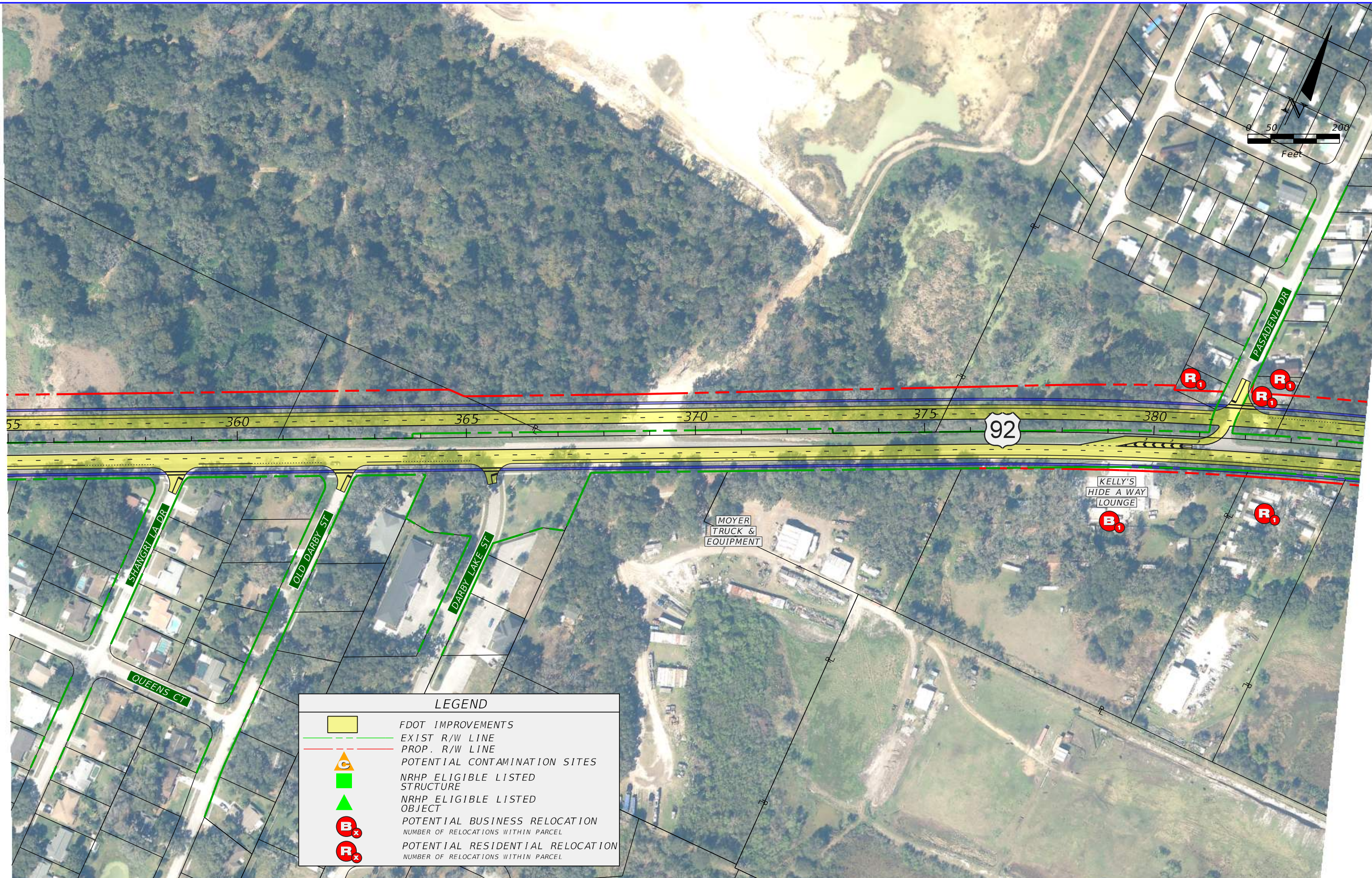
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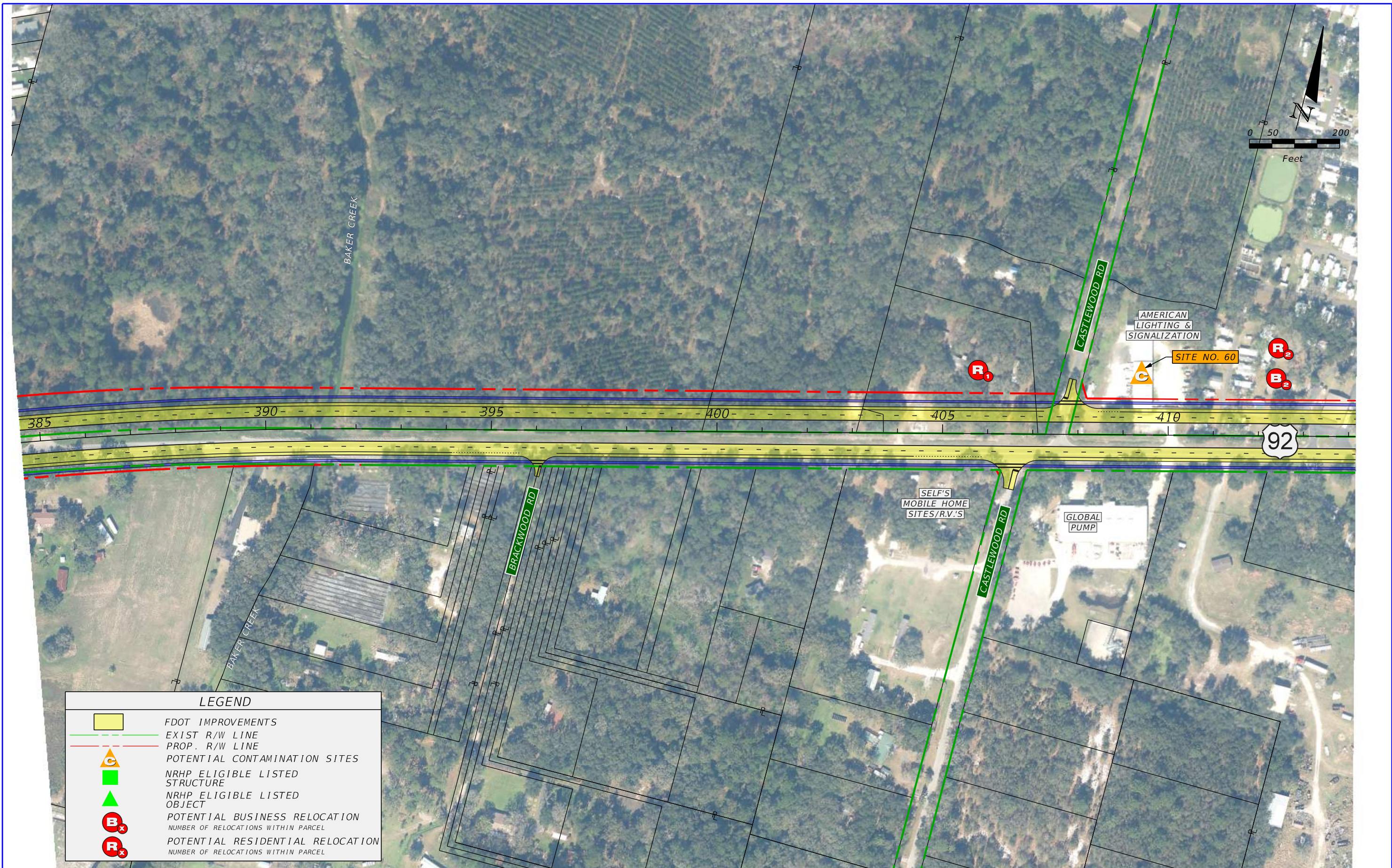
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600	HILLSBOROUGH	435749-1-22-01

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	NRHP ELIGIBLE LISTED STRUCTURE
	NRHP ELIGIBLE LISTED OBJECT
	POTENTIAL BUSINESS RELOCATION NUMBER OF RELOCATIONS WITHIN PARCEL
	POTENTIAL RESIDENTIAL RELOCATION NUMBER OF RELOCATIONS WITHIN PARCEL

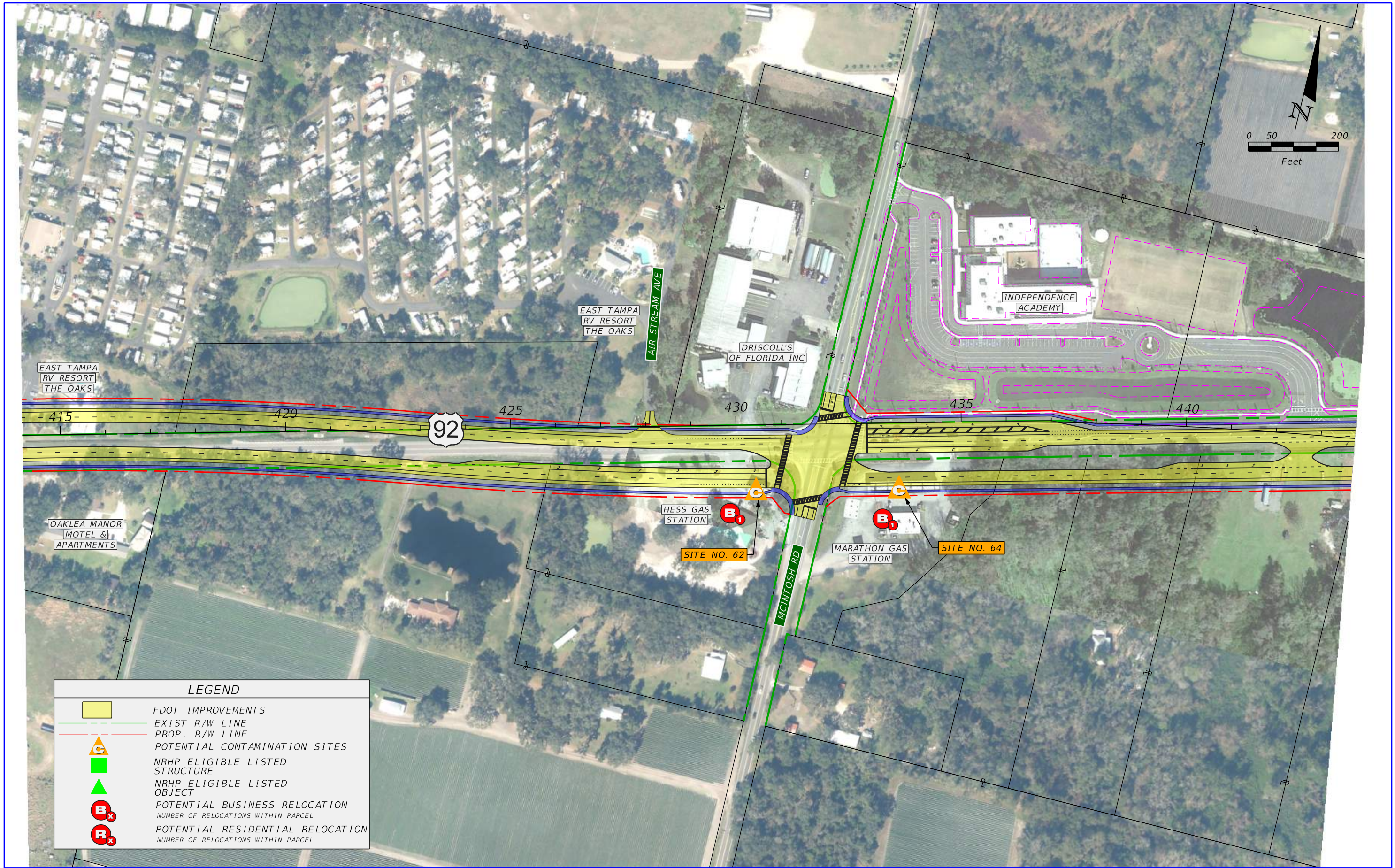
REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

ENGINEER OF RECORD
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 3000 Dovera Drive, Suite 200, Oviedo, Florida 32765
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STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
600	HILLSBOROUGH	435749-1-22-01

US 92 / SR 600
 PLAN SHEETS SEG 6
 PREFERRED ALIGNMENT

SHEET NO.
 11



LEGEND	
	FDOT IMPROVEMENTS
	EXIST R/W LINE
	PROP. R/W LINE
	POTENTIAL CONTAMINATION SITES
	NRHP ELIGIBLE LISTED STRUCTURE
	NRHP ELIGIBLE LISTED OBJECT
	POTENTIAL BUSINESS RELOCATION NUMBER OF RELOCATIONS WITHIN PARCEL
	POTENTIAL RESIDENTIAL RELOCATION NUMBER OF RELOCATIONS WITHIN PARCEL

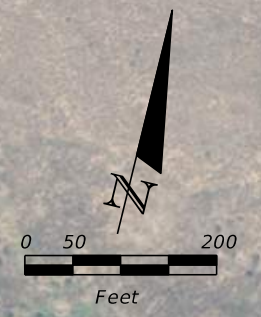
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600	HILLSBOROUGH	435749-1-22-01

US 92 / SR 600
 PLAN SHEETS SEG 6
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SHEET NO.
 12



LEGEND	
	FDOT IMPROVEMENTS
	EXIST. R/W LINE
	PROP. R/W LINE
	POTENTIAL CONTAMINATION SITES
	NRHP ELIGIBLE LISTED STRUCTURE
	NRHP ELIGIBLE LISTED OBJECT
	POTENTIAL BUSINESS RELOCATION NUMBER OF RELOCATIONS WITHIN PARCEL
	POTENTIAL RESIDENTIAL RELOCATION NUMBER OF RELOCATIONS WITHIN PARCEL

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600	HILLSBOROUGH	435749-1-22-01

US 92 / SR 600
 PLAN SHEETS SEG 6
 PREFERRED ALIGNMENT

SHEET NO.
 13



LEGEND	
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	PROP. R/W LINE
	POTENTIAL CONTAMINATION SITES
	NRHP ELIGIBLE LISTED STRUCTURE
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	POTENTIAL BUSINESS RELOCATION NUMBER OF RELOCATIONS WITHIN PARCEL
	POTENTIAL RESIDENTIAL RELOCATION NUMBER OF RELOCATIONS WITHIN PARCEL

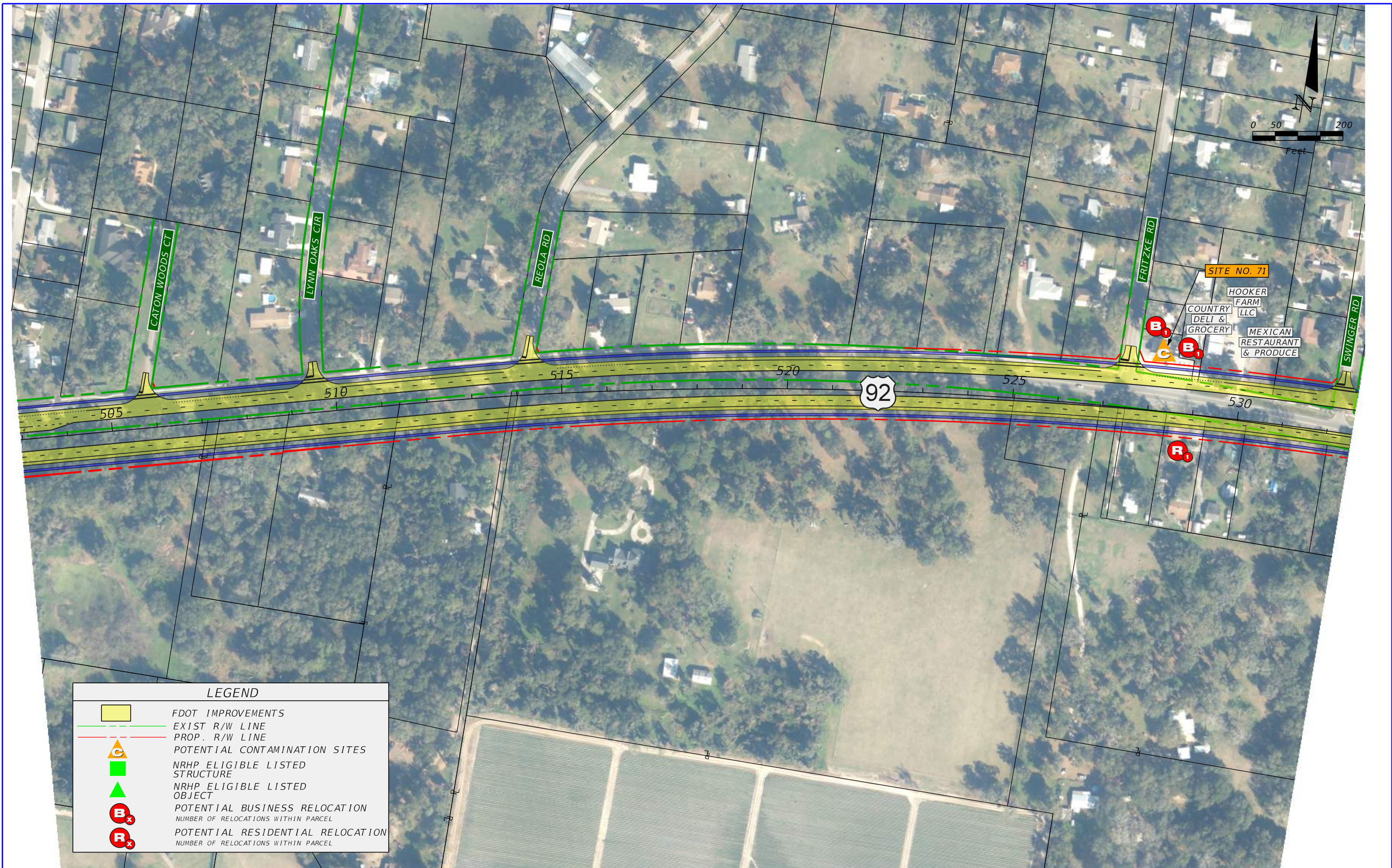
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600	HILLSBOROUGH	435749-1-22-01

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 PLAN SHEETS SEG 7
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SHEET NO.
 14



LEGEND	
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	EXIST R/W LINE
	PROP. R/W LINE
	POTENTIAL CONTAMINATION SITES
	NRHP ELIGIBLE LISTED STRUCTURE
	NRHP ELIGIBLE LISTED OBJECT
	POTENTIAL BUSINESS RELOCATION
	NUMBER OF RELOCATIONS WITHIN PARCEL
	POTENTIAL RESIDENTIAL RELOCATION
	NUMBER OF RELOCATIONS WITHIN PARCEL

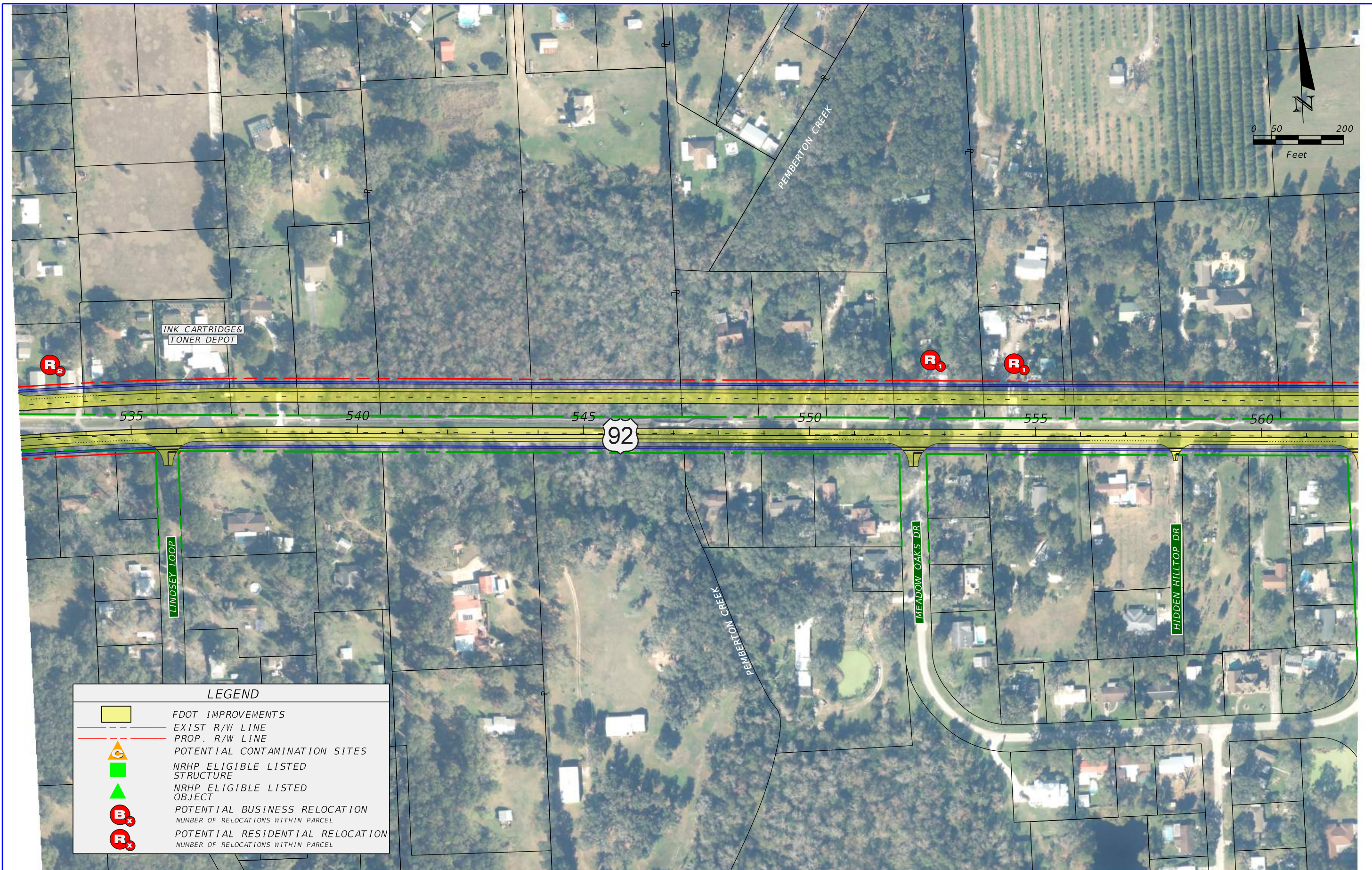
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ROAD NO.	COUNTY	FINANCIAL PROJECT ID
600	HILLSBOROUGH	435749-1-22-01

US 92 / SR 600
 PLAN SHEETS SEG 8
 PREFERRED ALIGNMENT

SHEET NO.
 15



LEGEND

- FDOT IMPROVEMENTS
- EXIST R/W LINE
- PROP. R/W LINE
- C POTENTIAL CONTAMINATION SITES
- NRHP ELIGIBLE LISTED STRUCTURE
- NRHP ELIGIBLE LISTED OBJECT
- B_x POTENTIAL BUSINESS RELOCATION
NUMBER OF RELOCATIONS WITHIN PARCEL
- R_x POTENTIAL RESIDENTIAL RELOCATION
NUMBER OF RELOCATIONS WITHIN PARCEL

REVISIONS	
DATE	DESCRIPTION

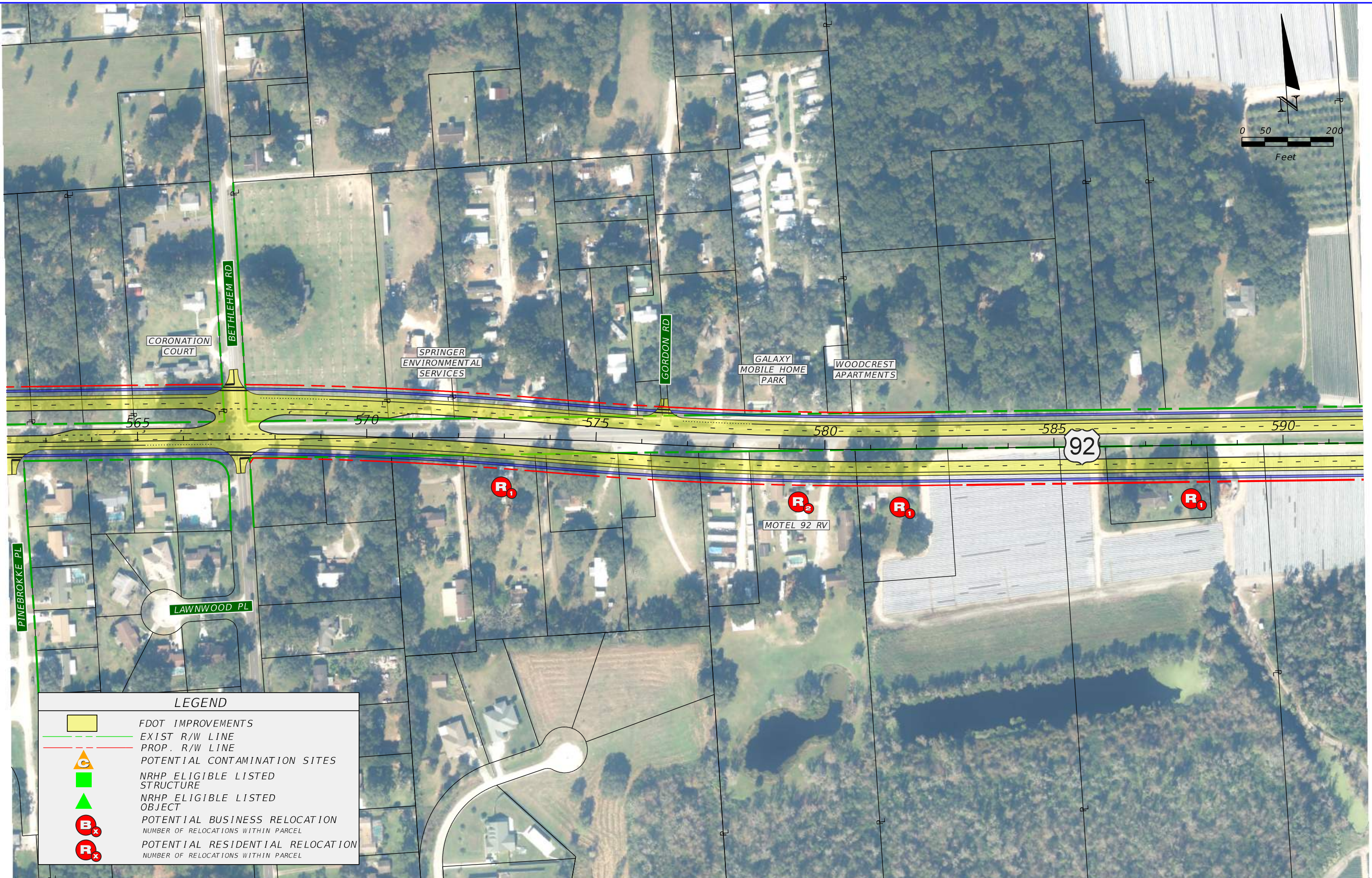
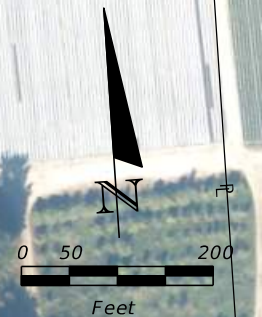
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STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
600	HILLSBOROUGH	435749-1-22-01

**US 92 / SR 600
 PLAN SHEETS SEG 8
 PREFERRED ALIGNMENT**

SHEET
NO.

16



LEGEND

- FDOT IMPROVEMENTS
- EXIST R/W LINE
- PROP. R/W LINE
- C POTENTIAL CONTAMINATION SITES
- NRHP ELIGIBLE LISTED STRUCTURE
- ▲ NRHP ELIGIBLE LISTED OBJECT
- B_x POTENTIAL BUSINESS RELOCATION
NUMBER OF RELOCATIONS WITHIN PARCEL
- R_x POTENTIAL RESIDENTIAL RELOCATION
NUMBER OF RELOCATIONS WITHIN PARCEL

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ROAD NO.	COUNTY	FINANCIAL PROJECT ID
600	HILLSBOROUGH	435749-1-22-01

**US 92 / SR 600
 PLAN SHEETS SEG 8
 PREFERRED ALIGNMENT**

SHEET NO.
17



LEGEND	
	FDOT IMPROVEMENTS
	EXIST R/W LINE
	PROP. R/W LINE
	POTENTIAL CONTAMINATION SITES
	NRHP ELIGIBLE LISTED STRUCTURE
	NRHP ELIGIBLE LISTED OBJECT
	POTENTIAL BUSINESS RELOCATION NUMBER OF RELOCATIONS WITHIN PARCEL
	POTENTIAL RESIDENTIAL RELOCATION NUMBER OF RELOCATIONS WITHIN PARCEL

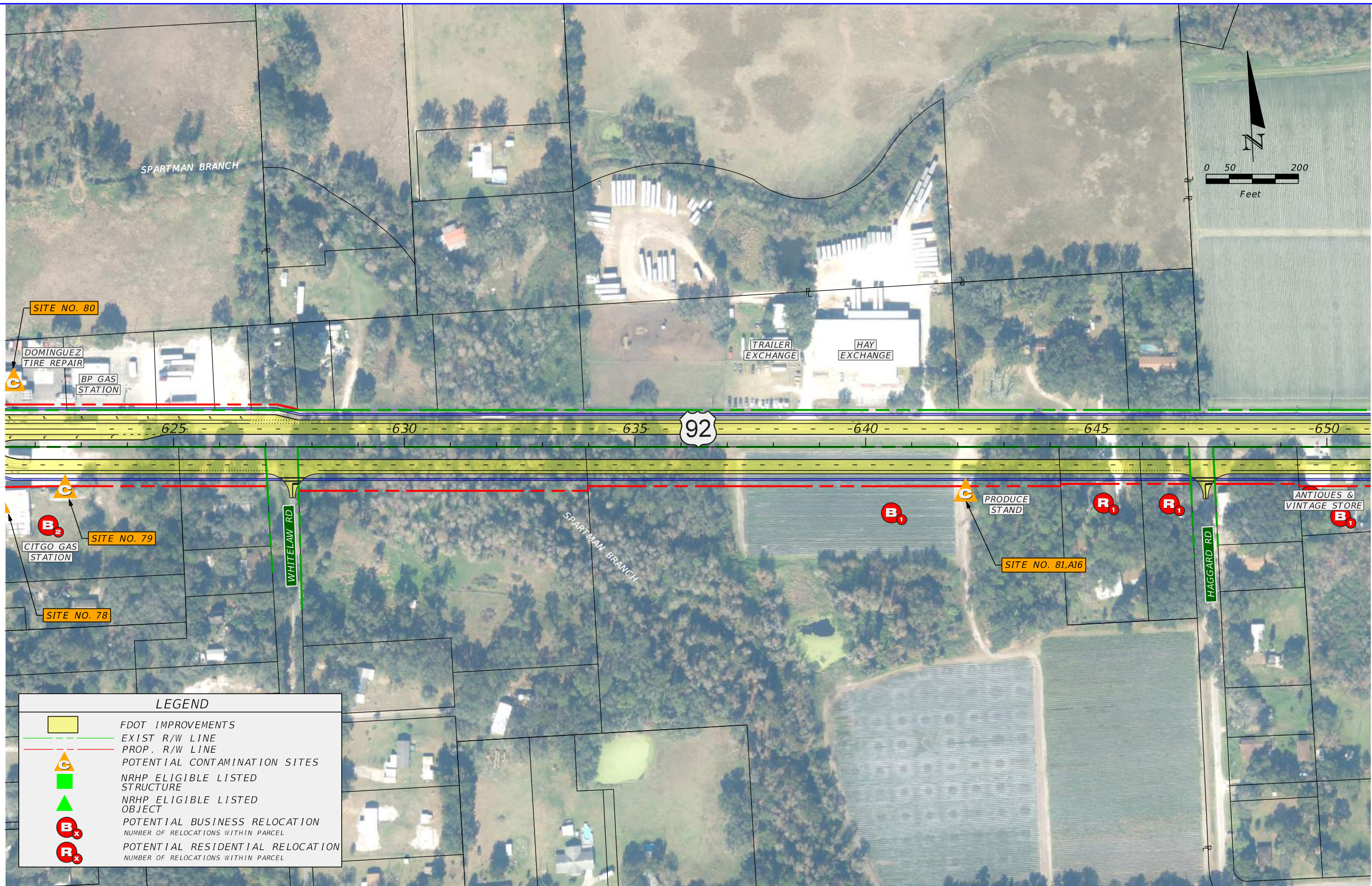
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ROAD NO.	COUNTY	FINANCIAL PROJECT ID
600	HILLSBOROUGH	435749-1-22-01

US 92 / SR 600
 PLAN SHEETS SEG 9
 PREFERRED ALIGNMENT

SHEET NO.
 18



LEGEND	
	FDOT IMPROVEMENTS
	EXIST R/W LINE
	PROP. R/W LINE
	POTENTIAL CONTAMINATION SITES
	NRHP ELIGIBLE LISTED STRUCTURE
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	POTENTIAL BUSINESS RELOCATION NUMBER OF RELOCATIONS WITHIN PARCEL
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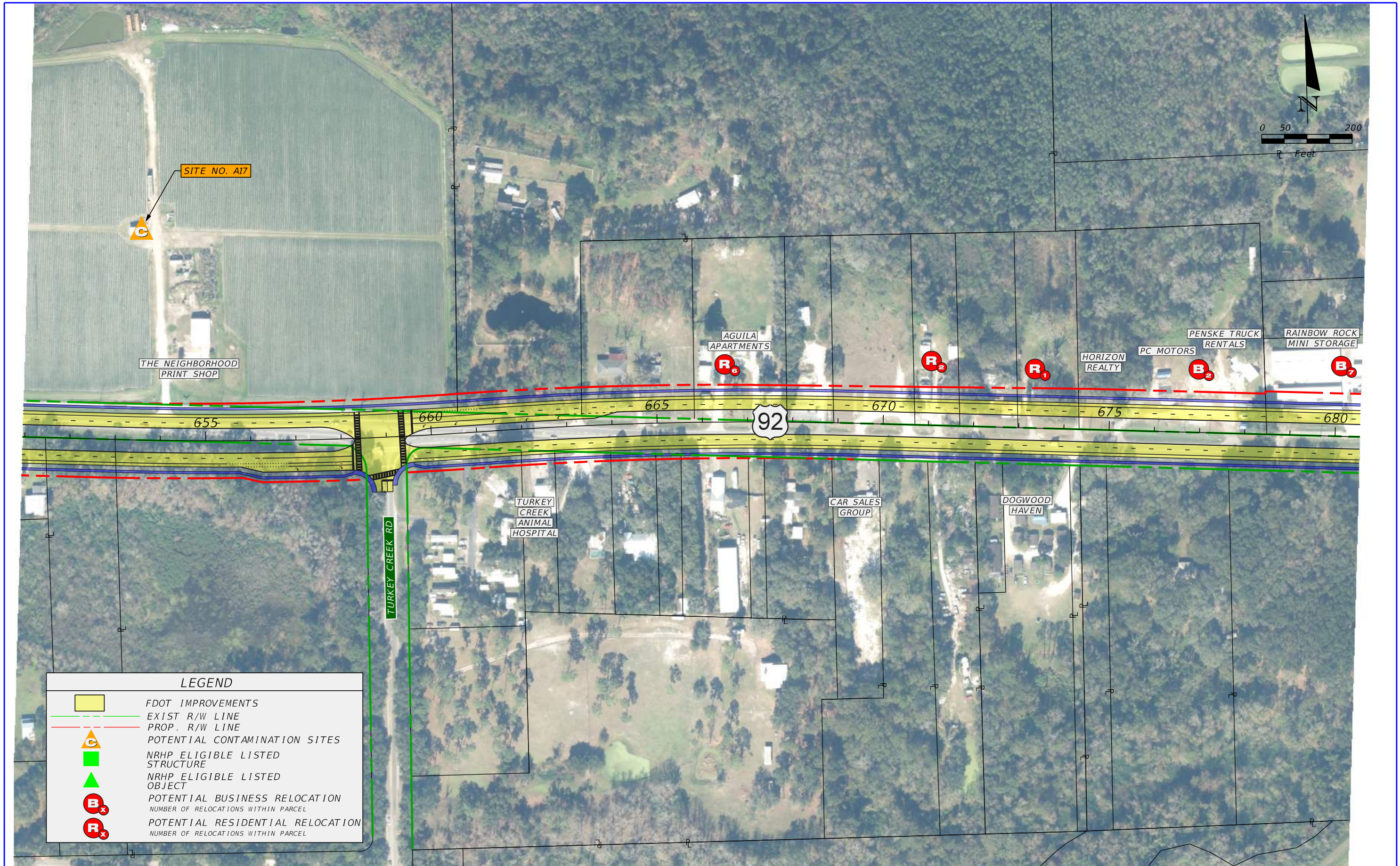
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STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
600	HILLSBOROUGH	435749-1-22-01

US 92 / SR 600
 PLAN SHEETS SEG 9
 PREFERRED ALIGNMENT

SHEET NO.
 19



LEGEND	
	FDOT IMPROVEMENTS
	EXIST R/W LINE
	PROP. R/W LINE
	POTENTIAL CONTAMINATION SITES
	NRHP ELIGIBLE LISTED STRUCTURE
	NRHP ELIGIBLE LISTED OBJECT
	POTENTIAL BUSINESS RELOCATION NUMBER OF RELOCATIONS WITHIN PARCEL
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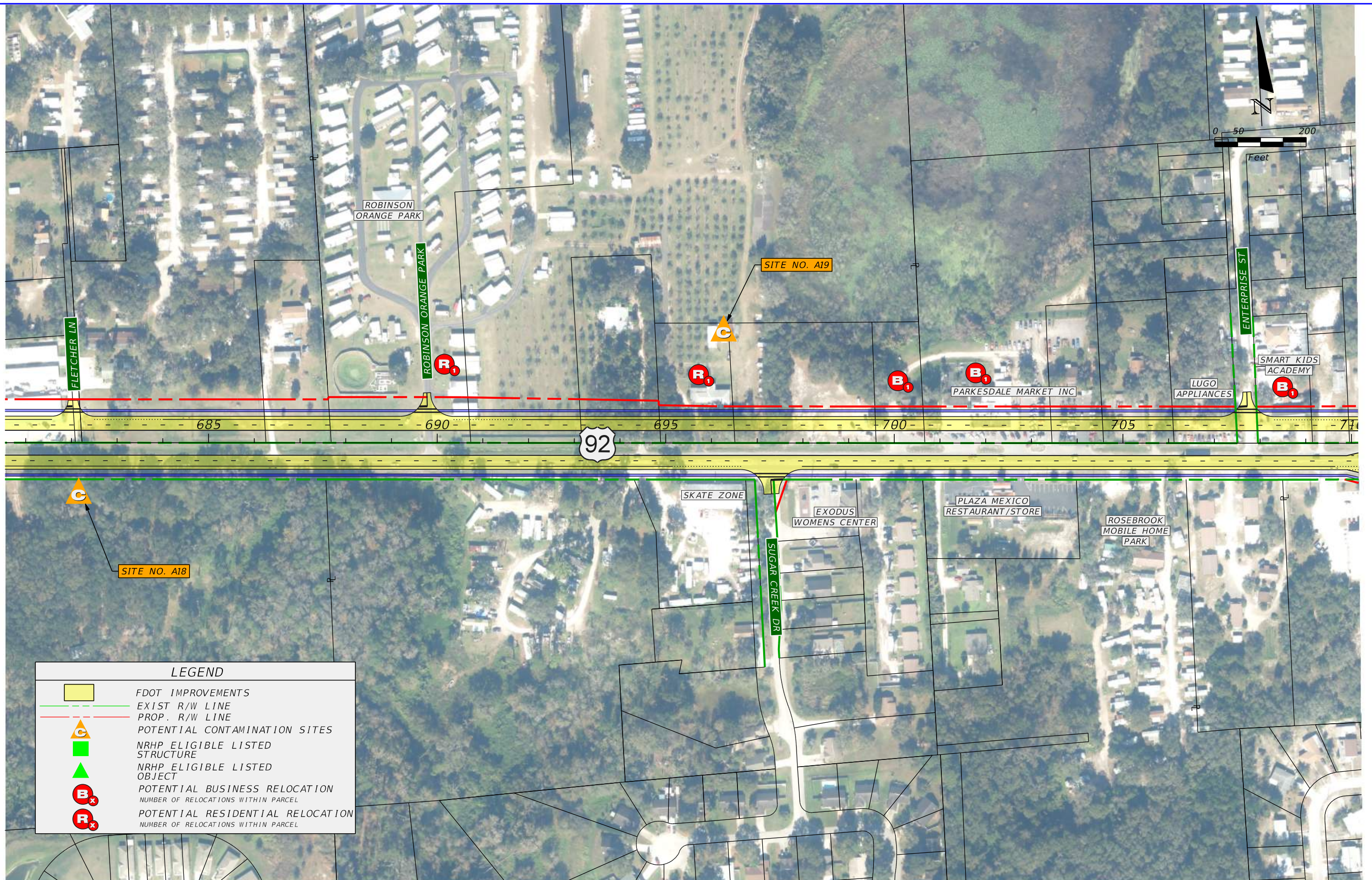
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600	HILLSBOROUGH	435749-1-22-01

US 92 / SR 600
 PLAN SHEETS SEG 9
 PREFERRED ALIGNMENT

SHEET NO.
 20



LEGEND	
	FDOT IMPROVEMENTS
	EXIST R/W LINE
	PROP. R/W LINE
	POTENTIAL CONTAMINATION SITES
	NRHP ELIGIBLE LISTED STRUCTURE
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	POTENTIAL BUSINESS RELOCATION NUMBER OF RELOCATIONS WITHIN PARCEL
	POTENTIAL RESIDENTIAL RELOCATION NUMBER OF RELOCATIONS WITHIN PARCEL

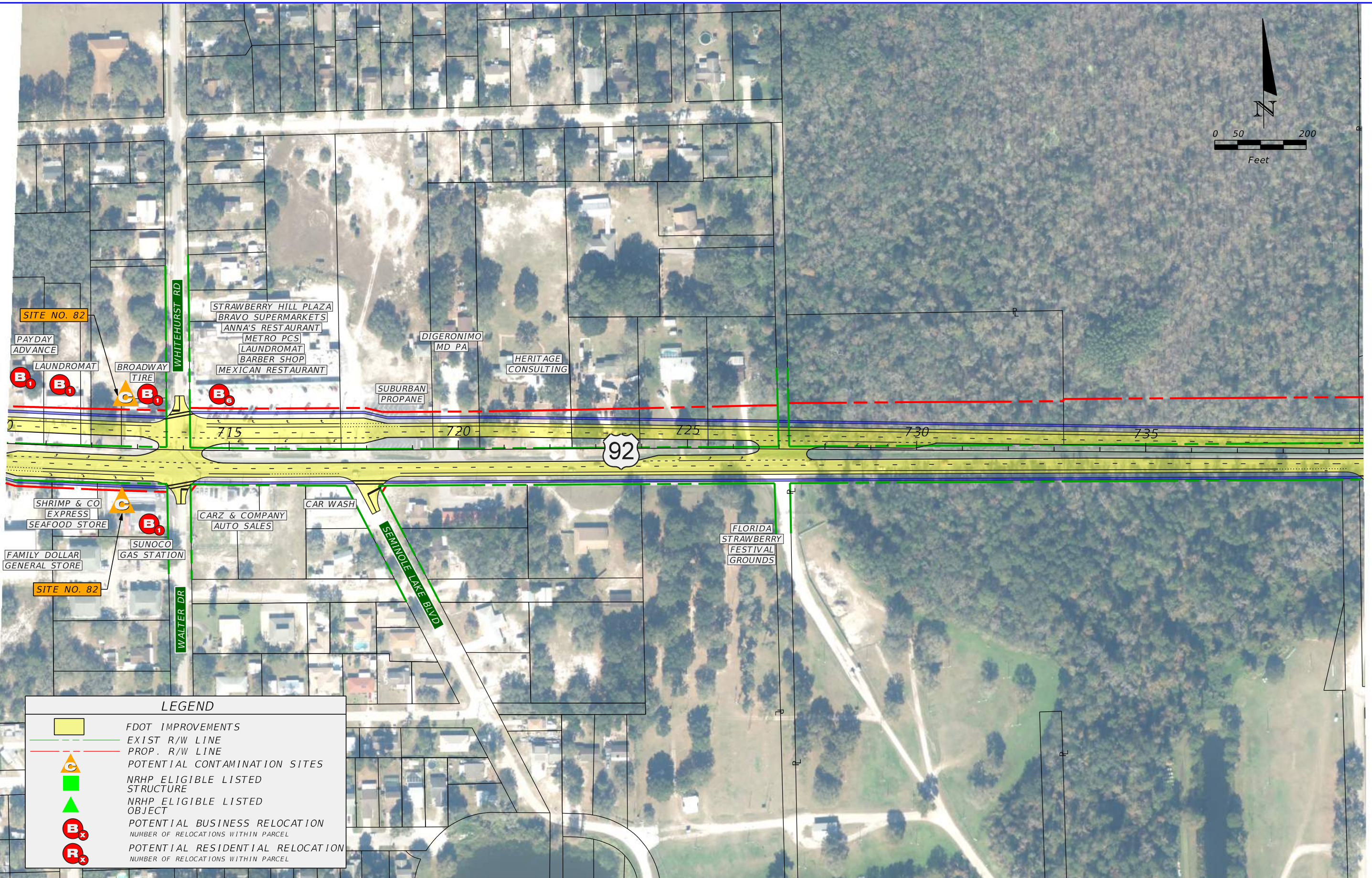
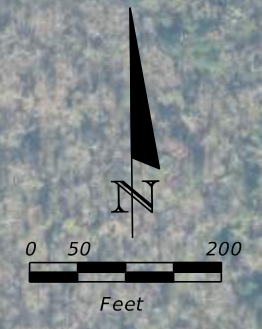
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ROAD NO.	COUNTY	FINANCIAL PROJECT ID
600	HILLSBOROUGH	435749-1-22-01

US 92 / SR 600
 PLAN SHEETS SEG 9
 PREFERRED ALIGNMENT

SHEET NO.
 21



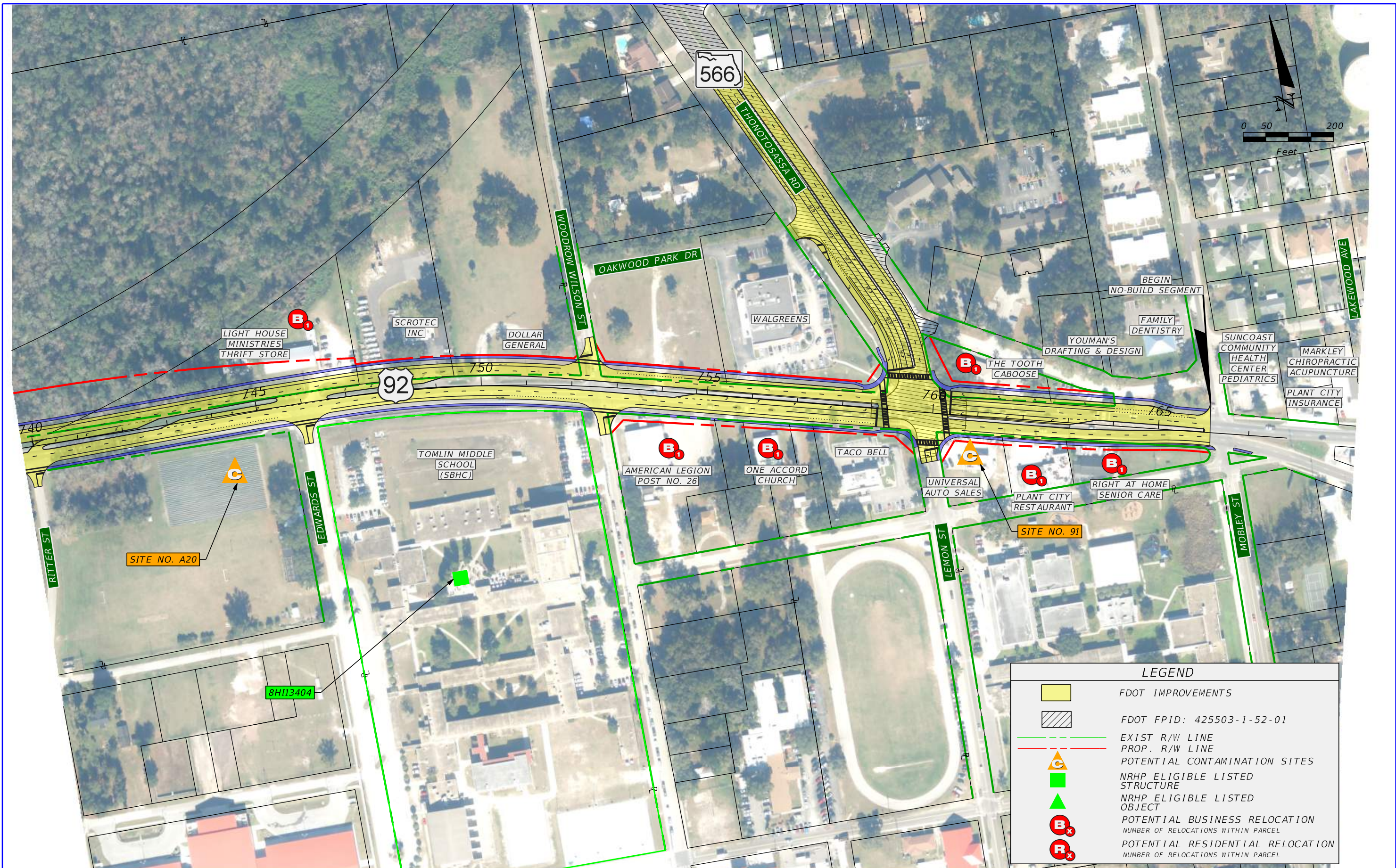
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600	HILLSBOROUGH	435749-1-22-01

US 92 / SR 600
 PLAN SHEETS SEG 9
 PREFERRED ALIGNMENT

SHEET NO.
 22



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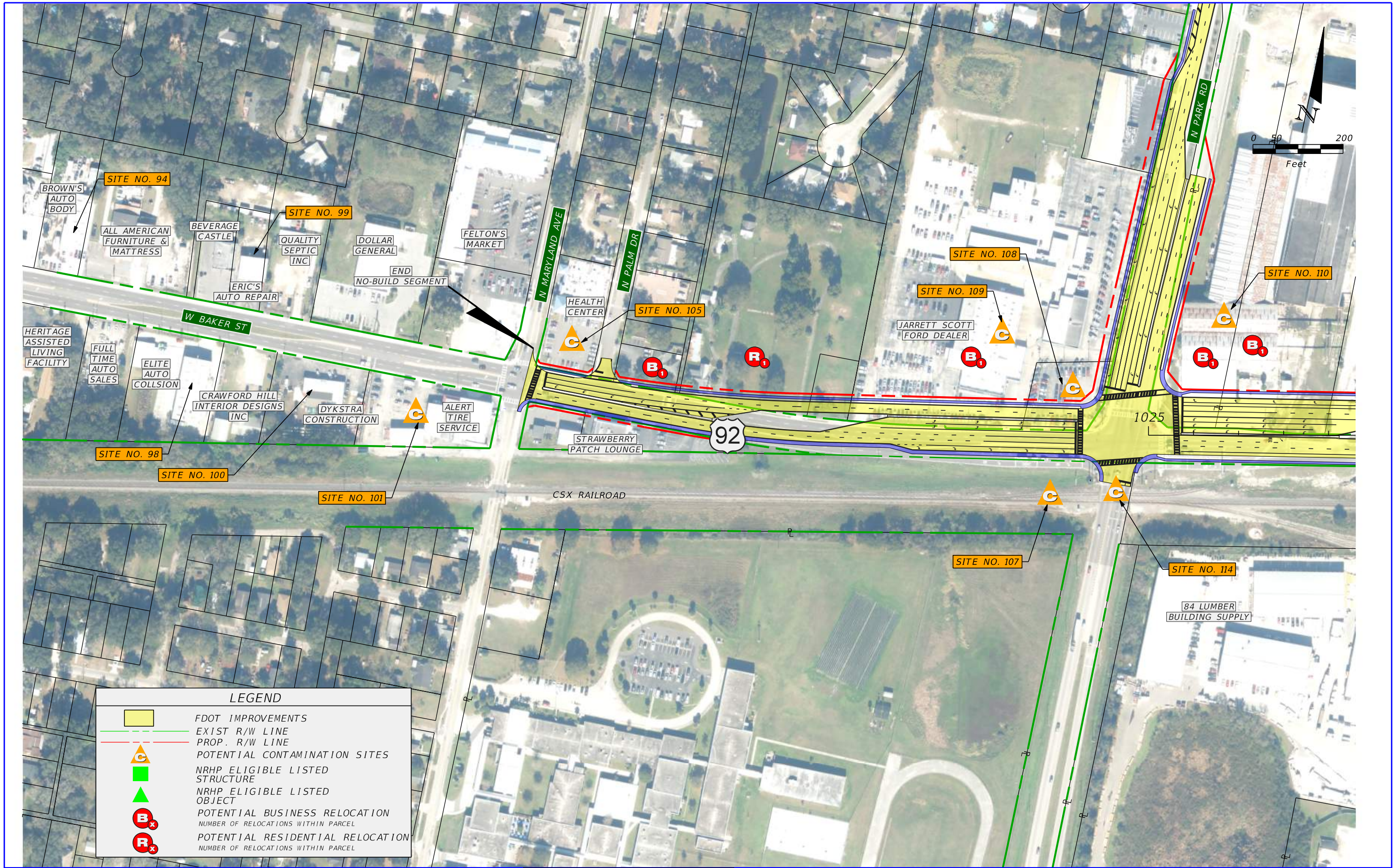
REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

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ROAD NO.	COUNTY	FINANCIAL PROJECT ID
600	HILLSBOROUGH	435749-1-22-01

**US 92 / SR 600
 PLAN SHEETS SEG 9
 PREFERRED ALIGNMENT**

SHEET NO.
23



LEGEND	
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	PROP. R/W LINE
	POTENTIAL CONTAMINATION SITES
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	POTENTIAL RESIDENTIAL RELOCATION NUMBER OF RELOCATIONS WITHIN PARCEL

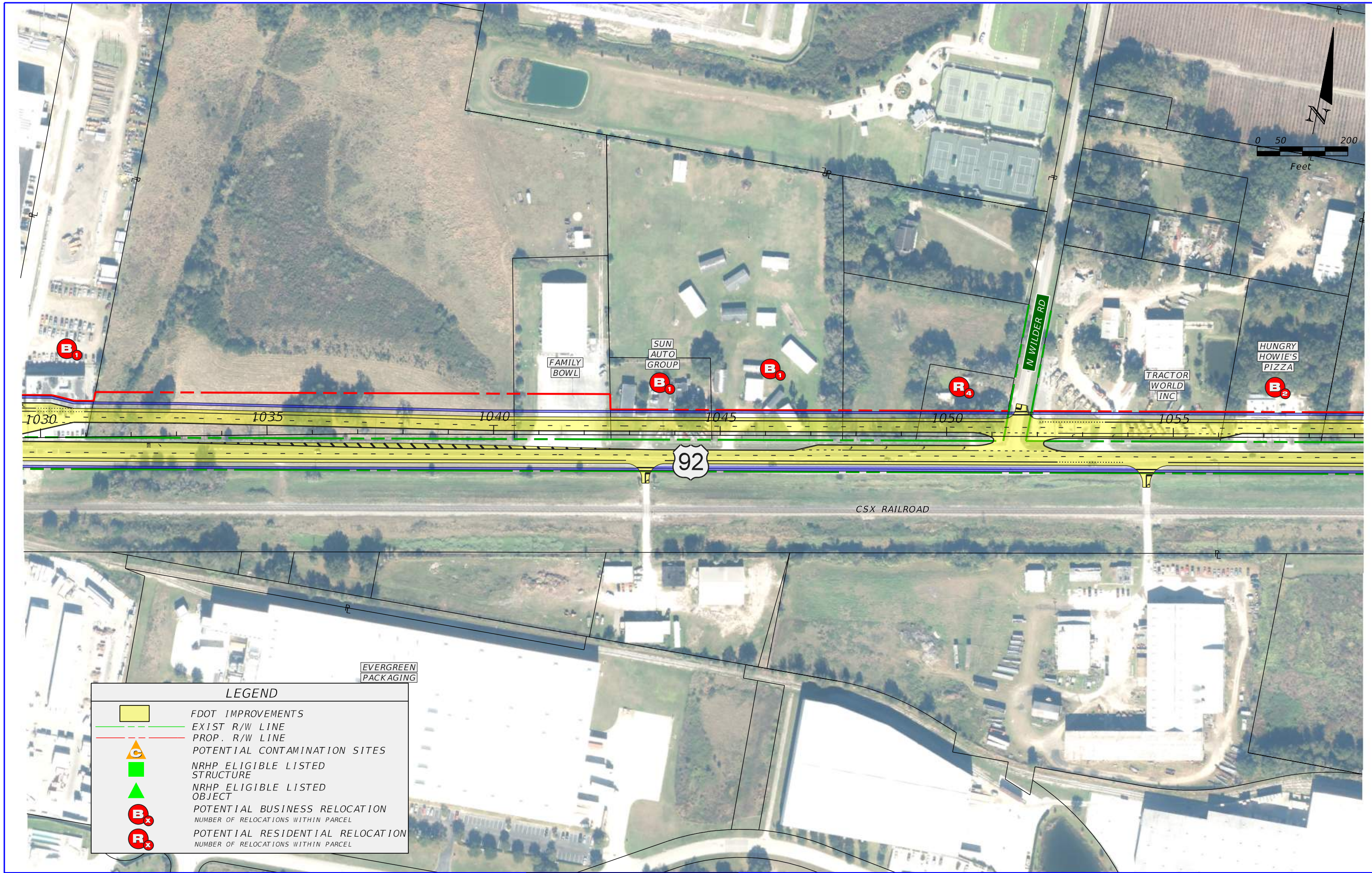
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ROAD NO.	COUNTY	FINANCIAL PROJECT ID
600	HILLSBOROUGH	435749-1-22-01

**US 92 / SR 600
PLAN SHEETS**

SHEET NO.
24



LEGEND	
	FDOT IMPROVEMENTS
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	PROP. R/W LINE
	POTENTIAL CONTAMINATION SITES
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	NRHP ELIGIBLE LISTED OBJECT
	POTENTIAL BUSINESS RELOCATION NUMBER OF RELOCATIONS WITHIN PARCEL
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REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

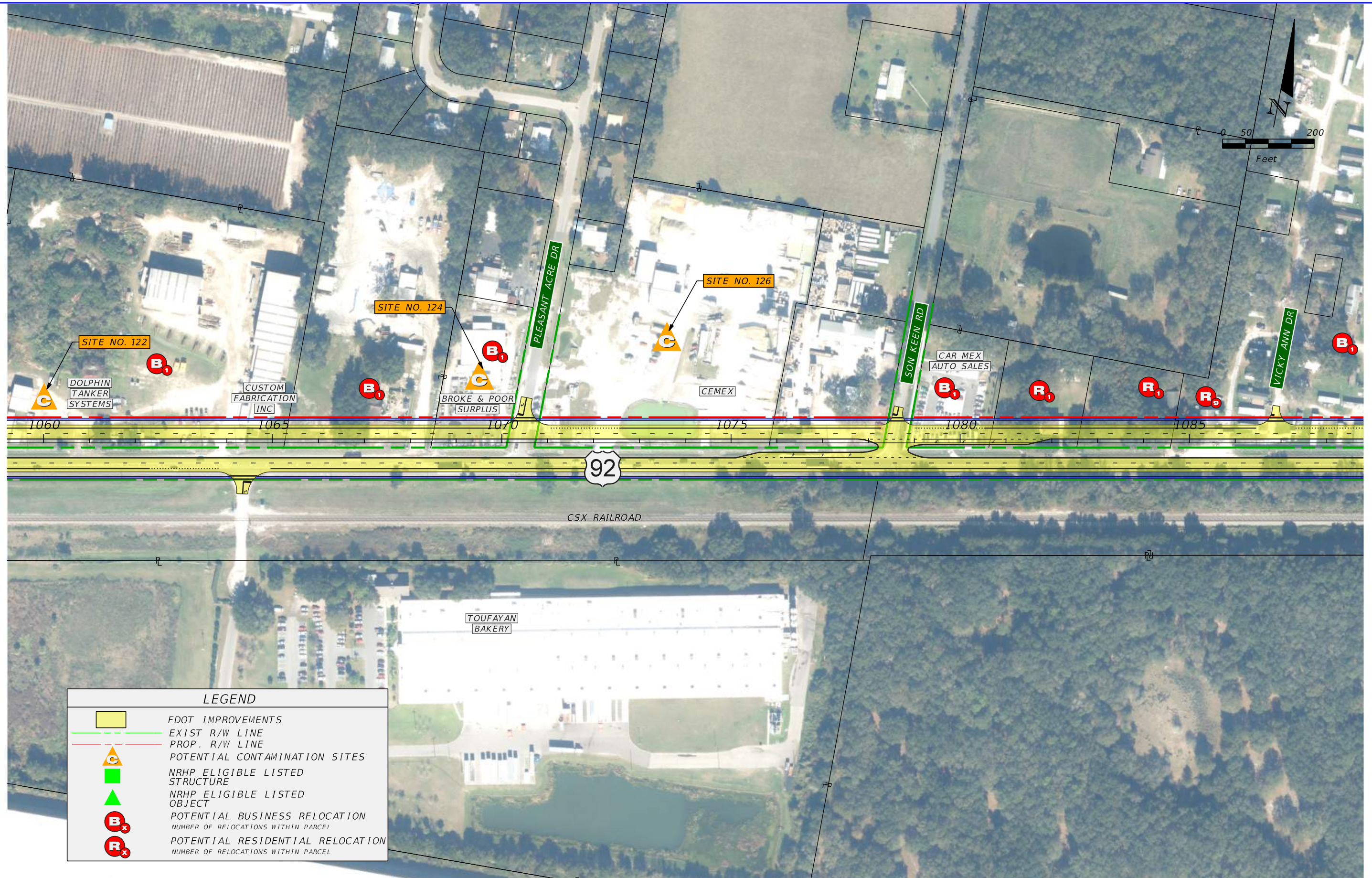
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ROAD NO.	COUNTY	FINANCIAL PROJECT ID
600	HILLSBOROUGH	435749-1-22-01

**US 92 / SR 600
PLAN SHEETS**

SHEET
NO.

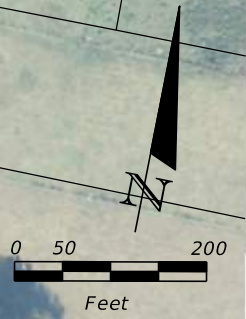
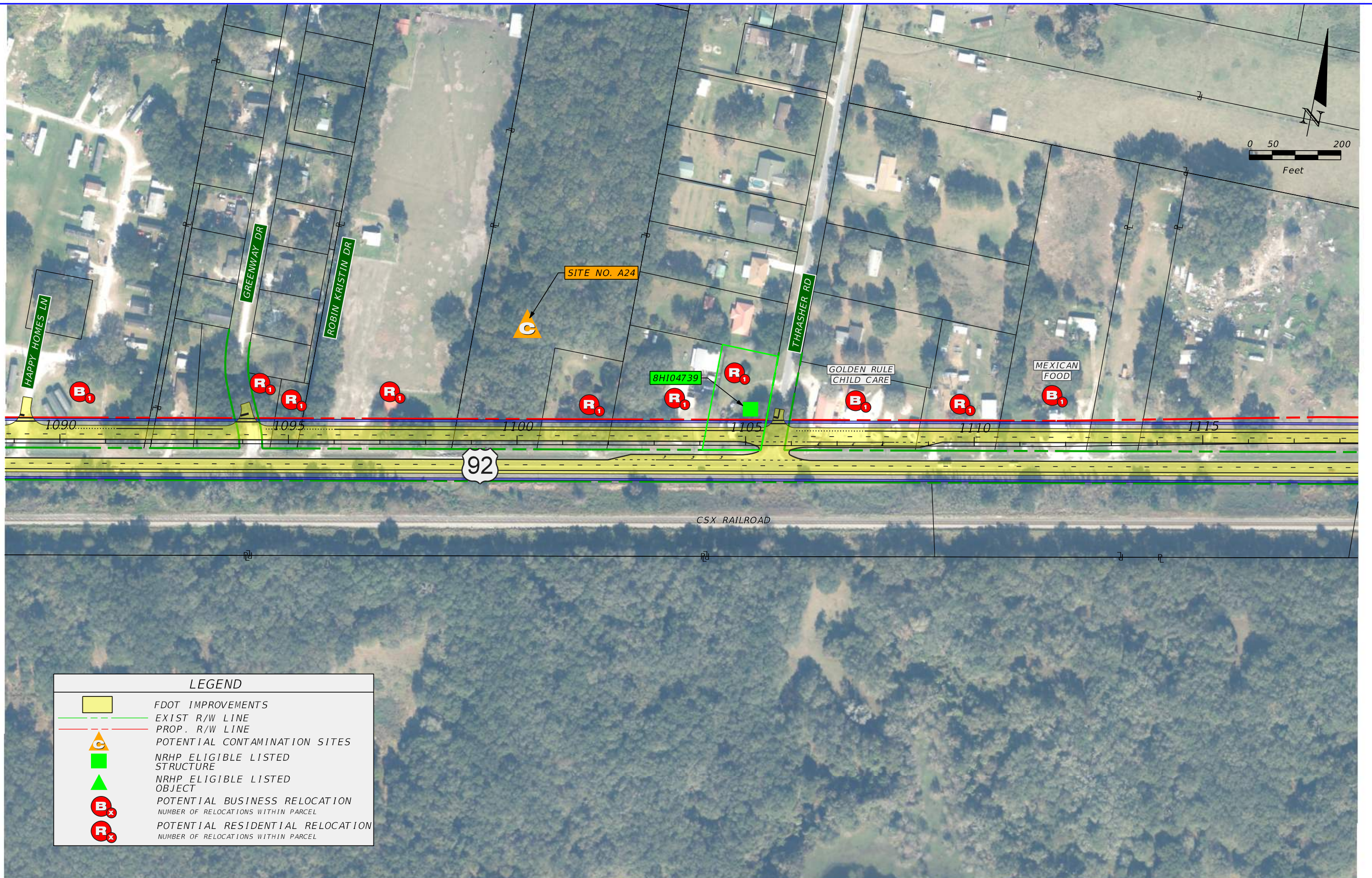
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LEGEND	
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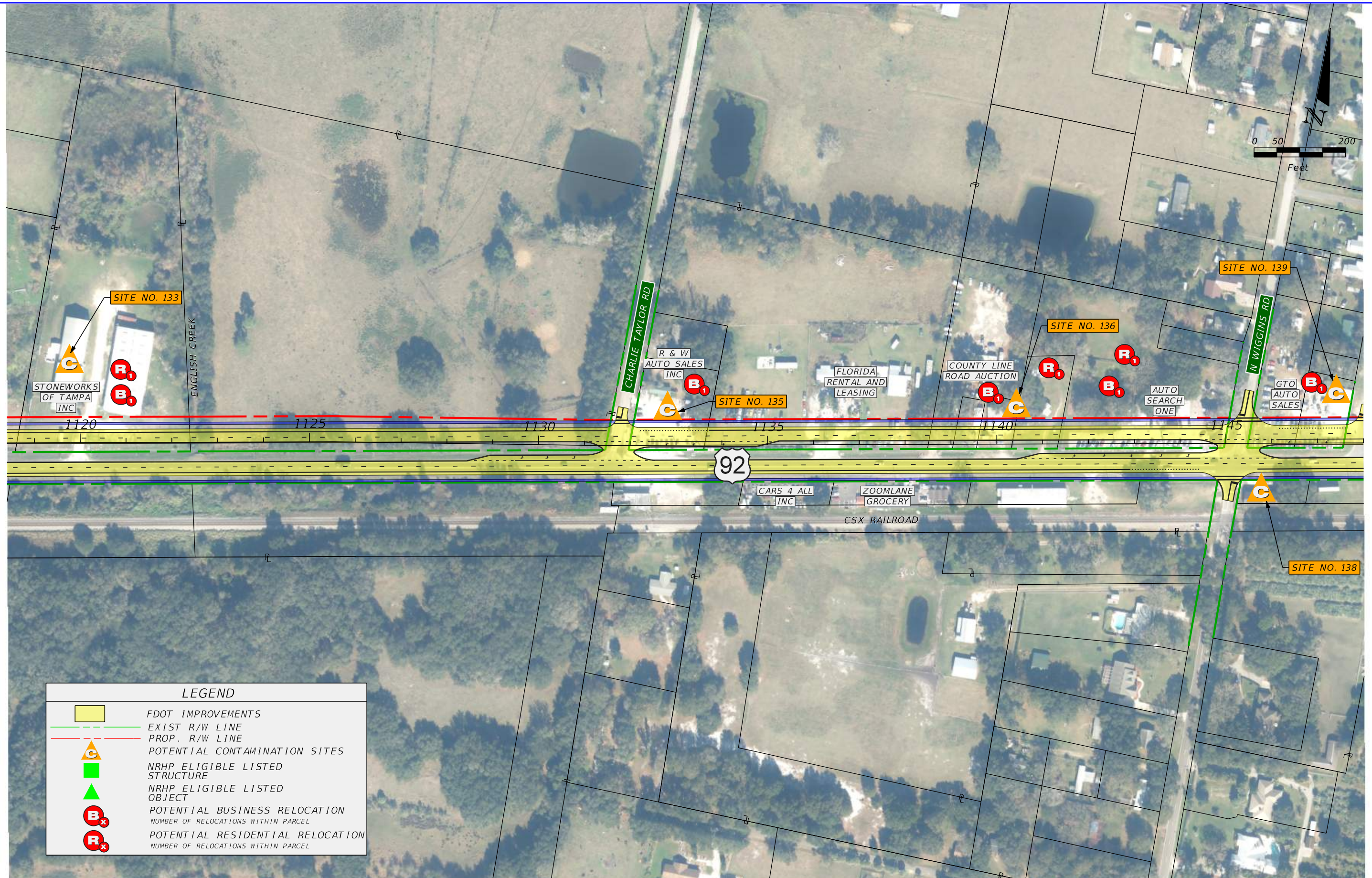
REVISIONS				ENGINEER OF RECORD Derek D. Dean, PE PE No. 51905 Inwood Consulting Engineers, Inc. Certificate of Authorization No. 7074 3000 Dovera Drive, Suite 200, Oviedo, Florida 32765 P 407.971.8850	STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			SHEET NO. 26
DATE	DESCRIPTION	DATE	DESCRIPTION		ROAD NO.	COUNTY	FINANCIAL PROJECT ID	
					600	HILLSBOROUGH	435749-1-22-01	

**US 92 / SR 600
PLAN SHEETS**



LEGEND	
	FDOT IMPROVEMENTS
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	PROP. R/W LINE
	POTENTIAL CONTAMINATION SITES
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	POTENTIAL BUSINESS RELOCATION NUMBER OF RELOCATIONS WITHIN PARCEL
	POTENTIAL RESIDENTIAL RELOCATION NUMBER OF RELOCATIONS WITHIN PARCEL

REVISIONS				ENGINEER OF RECORD Derek D. Dean, PE PE No. 51905 Inwood Consulting Engineers, Inc. Certificate of Authorization No. 7074 3000 Dovera Drive, Suite 200, Oviedo, Florida 32765 P 407.971.8850	STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			SHEET NO. 27
DATE	DESCRIPTION	DATE	DESCRIPTION		ROAD NO.	COUNTY	FINANCIAL PROJECT ID	
					600	HILLSBOROUGH	435749-1-22-01	



LEGEND	
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	PROP. R/W LINE
	POTENTIAL CONTAMINATION SITES
	NRHP ELIGIBLE LISTED STRUCTURE
	NRHP ELIGIBLE LISTED OBJECT
	POTENTIAL BUSINESS RELOCATION NUMBER OF RELOCATIONS WITHIN PARCEL
	POTENTIAL RESIDENTIAL RELOCATION NUMBER OF RELOCATIONS WITHIN PARCEL

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STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
600	HILLSBOROUGH	435749-1-22-01

**US 92 / SR 600
PLAN SHEETS**

SHEET NO.
28



LEGEND	
	FDOT IMPROVEMENTS
	BY CITY OF LAKELAND FPID: 434531-2-58-01
	EXIST R/W LINE
	PROP. R/W LINE
	POTENTIAL CONTAMINATION SITES
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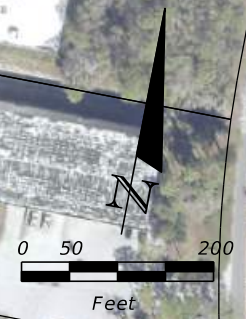
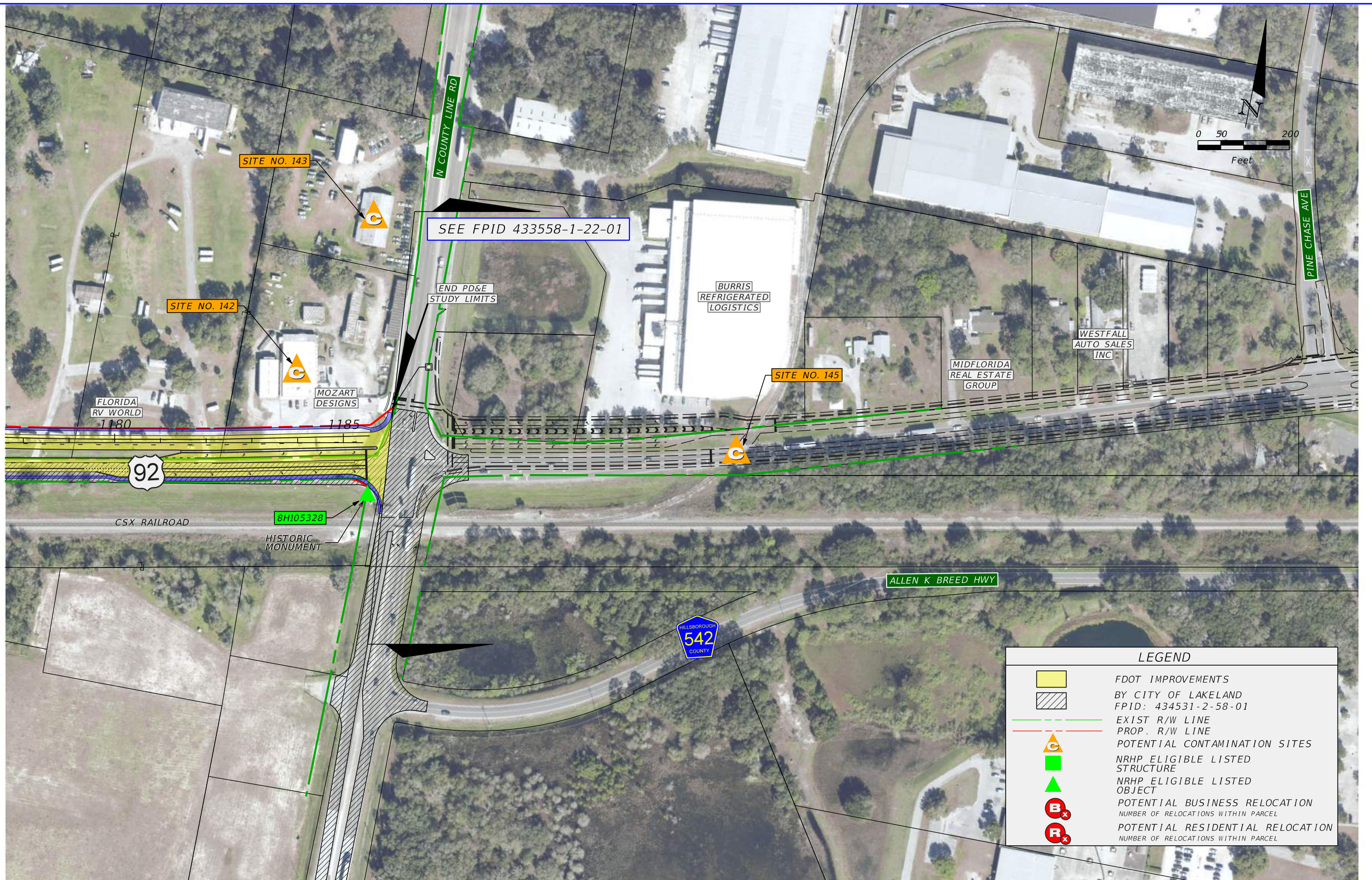
REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

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ROAD NO.	COUNTY	FINANCIAL PROJECT ID
600	HILLSBOROUGH	435749-1-22-01

US 92 / SR 600
 PLAN SHEETS

SHEET NO.
 29



LEGEND	
	FDOT IMPROVEMENTS BY CITY OF LAKE LAND FPID: 434531-2-58-01
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	PROP. R/W LINE
	POTENTIAL CONTAMINATION SITES
	NRHP ELIGIBLE LISTED STRUCTURE
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	POTENTIAL BUSINESS RELOCATION NUMBER OF RELOCATIONS WITHIN PARCEL
	POTENTIAL RESIDENTIAL RELOCATION NUMBER OF RELOCATIONS WITHIN PARCEL

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600	HILLSBOROUGH	435749-1-22-01

**US 92 / SR 600
PLAN SHEETS**

SHEET
NO.
30

Appendix B

Potential Jarrett Scott Ford Relocation Property

Google earth

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To Tampa

E SAM ALLEN RD

NORTH PARK ISLES (PUD) 1,204 DU

Knights Griffin RD

N Park RD

Stingray Chevrolet

S Park RD

Plant City

N Wilder RD

VARREA (PUD) 2,640 DU

N Charlie Taylor RD

To Orlando

SITE

DONCO REALTY, INC.
 P. O. Box 1569
 Plant City, Florida 33561-1569

SITE
 NWQ N. Park RD and I-4
 Plant City, FL

VIEW NORTH
 12.67 Acres
 Commercial Zoning
 663 Ft. ± Park RD Frontage

3000 ft

