

**FINAL**  
**STATE ENVIRONMENTAL IMPACT**  
**REPORT (SEIR)**

# US 301 (SR 43)

from SR 60 (Adamo Drive) to I-4 (SR 400)

Project Development and Environment Study



WPI Segment No. 430050-1

March 2018

# **State Environmental Impact Report**

## **US 301 (SR 43)**

### **Project Development and Environment Study**

#### **from State Road 60 (Adamo Drive) to I-4 (SR 400)**

#### **Hillsborough County, Florida**

ETDM No. 3097  
WPI Segment No. 430050-1

This roadway capacity improvement project involves widening US 301 from the existing four-lane divided arterial roadway to a divided six-lane arterial roadway to accommodate future travel demand in the study area. The study limits extend from the intersection with State Road 60 to south of the I-4/US 301 ramps in Hillsborough County. The total project length is 3.3 miles.

**Florida Department of Transportation**  
**District Seven**



Prepared By:  
**AIM Engineering & Surveying, Inc.**  
Tampa, Florida

March 2018

Florida Department of Transportation  
STATE ENVIRONMENTAL IMPACT REPORT

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**1. GENERAL INFORMATION**

Project Name: US 301 (SR 43) Project Development and Environment Study

Project Limits: From SR 60 (Adamo Drive) to Interstate 4 (SR 400)

ETDM Number: 3097

Financial Project Number: 430050 - 1

**2. PROJECT DESCRIPTION**

a. Existing Conditions: See Section 2.1

b. Proposed Improvements: See Section 2.2

**3. APPROVED FOR PUBLIC AVAILABILITY (BEFORE PUBLIC HEARING)**

  
District Secretary or Designee

2/2/16  
Date

A Public Hearing was held on

3/1/2016  
Date

**4. APPROVAL OF FINAL DOCUMENTATION (AFTER PUBLIC HEARING)**

  
District Secretary or Designee

3-15-2018  
Date

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**5. IMPACT EVALUATION**

Topical Categories	Sig	Min	None	NoInv	Basis for Decision*
<b>A. SOCIAL IMPACTS</b>					
1. Land Use Changes	[ ]	[ ]	[X]	[ ]	See <b>Section 3.1.1</b>
2. Community Cohesion	[ ]	[ ]	[X]	[ ]	See <b>Section 3.1.2</b>
3. Relocation Potential	[ ]	[ ]	[X]	[ ]	See <b>Section 3.1.3</b>
4. Community Services	[ ]	[ ]	[X]	[ ]	See <b>Section 3.1.4</b>
5. Title VI Considerations	[ ]	[ ]	[X]	[ ]	See <b>Section 3.1.5</b>
6. Controversy Potential	[ ]	[X]	[ ]	[ ]	See <b>Section 3.1.6</b>
7. Bicycles and Pedestrians	[ ]	[ ]	[X]	[ ]	See <b>Section 3.1.7</b>
8. Utilities and Railroads	[ ]	[X]	[ ]	[ ]	See <b>Section 3.1.8</b>
<b>B. CULTURAL</b>					
1. Historic Sites/Districts	[ ]	[ ]	[X]	[ ]	See <b>Section 3.2.1</b>
2. Archaeological Sites	[ ]	[ ]	[X]	[ ]	See <b>Section 3.2.2</b>
3. Recreation Areas	[ ]	[ ]	[X]	[ ]	See <b>Section 3.2.3</b>
<b>C. NATURAL</b>					
1. Wetlands	[ ]	[X]	[ ]	[ ]	See <b>Section 3.3.1</b>
2. Aquatic Preserves	[ ]	[ ]	[ ]	[X]	
3. Water Quality	[ ]	[ ]	[X]	[ ]	See <b>Section 3.3.2</b>
4. Outstanding FL Waters	[ ]	[ ]	[ ]	[X]	
5. Wild and Scenic Rivers	[ ]	[ ]	[ ]	[X]	
6. Floodplains	[ ]	[X]	[ ]	[ ]	See <b>Section 3.3.3</b>
7. Coastal Barrier Islands	[ ]	[ ]	[ ]	[X]	
8. Wildlife and Habitat	[ ]	[X]	[ ]	[ ]	See <b>Section 3.3.4</b>
9. Farmlands	[ ]	[ ]	[ ]	[X]	
10. Essential Fish Habitat	[ ]	[ ]	[ ]	[X]	
<b>D. PHYSICAL</b>					
1. Noise	[ ]	[X]	[ ]	[ ]	See <b>Section 3.4.1</b>
2. Air Quality	[ ]	[X]	[ ]	[ ]	See <b>Section 3.4.2</b>
3. Construction	[ ]	[X]	[ ]	[ ]	See <b>Section 3.4.3</b>
4. Contamination	[ ]	[X]	[ ]	[ ]	See <b>Section 3.4.4</b>
5. Navigation	[ ]	[ ]	[ ]	[X]	

\* Sig = Significant; Min = Minimal; None = None; NoInv = No involvement. Basis of decision is documented in the referenced attachment(s).

**E. PERMITS REQUIRED**

Environmental Resource Permit	SWFWMD
Section 404, Dredge and Fill Permit	USACE
Section 408, Permit	USACE
National Pollutant Discharge Elimination System Permit	FDEP

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### 6. COMMITMENTS AND RECOMMENDATIONS

#### *Commitments*

1. The FDOT is committed to the following measures to address surface water impacts for this project:
  - Practicable measures to avoid or minimize surface water impacts will be addressed during final design for the project.
  - Best Management Practices will be incorporated during construction to minimize surface water impacts to any off-site wetlands and surface waters that are affected by the proposed project.
  - While not currently anticipated to be required, unavoidable surface water impacts will be mitigated pursuant to S. 373.4137 F.S. to satisfy all mitigation requirements of Part IV, Chapter 373 F.S. and 33 U.S.C.s 1344 which includes purchase of mitigation bank credits or use of the FDOT wetland mitigation inventory program.
  
2. Based upon findings of the preliminary data collection, general corridor surveys, and ongoing coordination with the USFWS and FWC, the FDOT has established the following additional project commitments:
  - Gopher tortoise: Surveys for potentially affected gopher tortoise burrows will be conducted prior to construction, and permits to relocate tortoises and commensals as appropriate will be obtained from the FWC.
  - Eastern indigo snake: The standard FDOT Construction Precautions for the Eastern Indigo Snake will be adhered to during construction of the project.
  - Osprey: Surveys to update locations of active osprey nest sites will be conducted prior to construction, and permits will be acquired if impacts during construction are unavoidable. Coordination with FWC will take place, and a replacement nesting structure will be located in the immediate vicinity as appropriate.
  - Wood stork: Impacts to potential wood stork suitable foraging habitat will be evaluated during the design phase, and mitigation for unavoidable impacts will be provided as appropriate.
  - Bald eagle: Should a bald eagle nest be built prior to or during construction within 660 feet of the construction limits, further coordination will occur with the FWC and/or USFWS as appropriate.

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3. A land use review will be performed during the Design phase of the project to ensure that all noise-sensitive land uses that have received a building permit prior to the project's Date of Public Knowledge are evaluated.
4. The Department will coordinate with the Florida State Fairground the pedestrian crossing accommodation along US 301 within the design project limits.

#### ***Recommendations***

It is recommended that the primary engineering elements associated with the Preferred Build Alternative as described under Proposed Improvements in **Section 2** (Alternatives) be approved for advancement to future phases of project development (i.e. design, right-of-way acquisition, and construction) as funding becomes available.

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# 1.0 INTRODUCTION

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## 1.1 Project Description

The Florida Department of Transportation (FDOT) conducted a Project Development and Environment (PD&E) study to evaluate the proposed widening of US 301 (SR 43) to six lanes from SR 60 (Adamo Drive) to the southern end of the eastbound I-4 (SR 400) on- and off-ramps in Hillsborough County. The total project length is approximately 3.3 miles, and is illustrated in **Figure 1-1**. The purpose of this PD&E study is to document the need for additional capacity within the study corridor and to evaluate the costs and impacts associated with providing this additional capacity. Federal funds are not planned to be used for the project, so was conducted in accordance with the PD&E Manual, Part 1, Chapter 10, which addresses non-federal projects.

The proposed action involves widening US 301 from the existing four-lane divided roadway to a six-lane divided roadway. This improvement is necessary to provide additional capacity to accommodate the future travel demand that will be generated by the projected population and employment growth in eastern Hillsborough County. US 301 is a major north-south roadway that traverses all of Hillsborough County and provides connectivity to many of Florida's major roadways including SR 60, Lee Roy Selmon Expressway and I-4. This roadway is a vital link in the regional transportation network and also serves as an emergency evacuation route.

US 301 is functionally classified as an "Urban Other Principal Arterial" and has a posted speed limit of 50 miles per hour (mph) within the majority of the project limits. The posted speed limit is reduced to 45 mph approaching SR 60 and at the approaching on-ramp to eastbound I-4. Throughout most of the study corridor, US 301 exists as a four-lane divided roadway; however, three through lanes are provided in both the northbound and southbound directions in the vicinity of the intersection with SR 574 (Dr. Martin Luther King, Jr. Boulevard).

The existing right-of-way width ranges from 160 feet to 306 feet; however, a majority of the study corridor has a right-of-way width of 200 feet. Sidewalks as well as roadside ditches, where stormwater runoff is collected, were recently constructed along both the east and west sides of US 301 from SR 574 northward to I-4. Other sections of sidewalks exist intermittently from SR 60 to SR 574.

There are also seven bridges located within the project limits. Two bridges are located over the CSX Railroad's S-Line while two others are located over the CSX Railroad's A-Line and CR 574 (Broadway Avenue). There are also two bridges that cross over the Tampa Bypass Canal and one box culvert that crosses Bruce Creek.



Figure 1-1 Project Location Map

The project was evaluated through the FDOT's Efficient Transportation Decision Making (ETDM) process. This project is designated as ETDM project #3097. An ETDM *Programming Screen Summary Report* was published on January 9, 2013 containing comments from the Environmental Technical Advisory Team (ETAT) on the project's effects on various natural, physical and social resources.

## **1.2 Project Purpose and Need**

The purpose of this project is to relieve congestion on this portion of US 301 in unincorporated Hillsborough County. US 301 is a major north-south roadway facility in close proximity to the City of Tampa, which travels from the Sarasota-Bradenton-Venice Metropolitan Statistical Area across the state to the Jacksonville Metropolitan Statistical Area. US 301 serves regional travel and connects residential centers in the Brandon and South Shore area with employment centers along the I-75 Corridor. It provides regional connectivity with I-75, the Lee Roy Selmon Crosstown Expressway, and I-4. US 301 has been designated by Hillsborough County Emergency Management as an emergency evacuation route. In addition to increasing capacity, this project will add or enhance the multi-modal facilities in this corridor.

The need for this widening project is based on improving level of service through providing additional capacity to accommodate future travel demand and reduce congestion. The proposed improvements include accommodating both future traffic growth and enhancing safety.

The proposed widening of this portion of US 301 is expected to have positive mobility impacts. The Hillsborough County City-County Planning Commission's 2040 Long Range Transportation Plan socioeconomic projections (July 2014) contain both population and employment projections. These projections show Hillsborough County's population growing from 1,229,226 to 1,815,964 (a 48% increase) between 2010 and 2040. Employment is projected to grow from 711,400 to 1,112,059 (a 56% increase) between 2010 and 2040, mostly within the urban service area. Based on projected population and employment growth, the existing infrastructure would result in failing levels of service in the future.

Several Strategic Intermodal Systems (SIS) facilities are in close proximity to US 301, including the Port of Tampa, the Tampa Intercity Greyhound Bus Terminal, and the Port of Manatee. Emerging SIS facilities in the area include the Tampa Amtrak Station and the Tampa CSX Intermodal Terminal. After this project is constructed and congestion is decreased, travel to and from these intermodal facilities will become faster and easier. Additionally, this proposed project includes multi-modal improvements, including sidewalks and bicycle lanes. Currently, the Hillsborough Area Regional Transit (HART) system does not have buses running on this section of US 301.

Safety within the US 301 corridor is also expected to improve with an increase in capacity and a reduction in congestion. The US 301 corridor from SR 60 to I-4 had 637 crashes from 2007 through 2011. Most occurred at the intersections and were the result of rear end collisions. The addition and enhancement of multi-modal facilities will increase pedestrian and bicyclist safety along the corridor.

### **1.2.1 Capacity and Transportation Demand**

The purpose of this project is to increase the capacity of US 301 from SR 60 to south of I-4 to accommodate future traffic demand generated by population and employment growth in eastern Hillsborough County. The 2013 Annual Average Daily Traffic (AADT) volumes on US 301 range from 29,700 vehicles per day (vpd) to 36,200 vpd. All of the study corridor roadway segments are currently operating at Level of Service (LOS) C or better during the a.m. and p.m. peak hours.

Design year (2040) AADT volumes were developed using the 2035 Tampa Bay Regional Planning Model and then extrapolating using the existing (2013) and 2035 AADT volumes. The 2040 Build Alternative AADT volumes are projected to range between 55,500 vpd and 64,500 vpd. These future year AADT volumes are between 78% and 87% higher than the 2013 AADT volumes. Without the proposed widening to six lanes, the existing four-lane roadway will experience extremely high levels of congestion and vehicle delay.

### 1.2.2 System Linkage and Multimodal Relationships

US 301 is a major north-south arterial located in eastern Hillsborough County that serves regional travel and connects residential centers in the Brandon and South Shore area with employment centers along the I-75 corridor. This roadway provides access to many of the area's other major roadways, including I-4, I-75, US 92, Lee Roy Selmon Expressway and SR 60. Currently, the Hillsborough Area Regional Transit (HART) system does not provide bus service along the study corridor.

Several SIS facilities are in close proximity to US 301, including the Port of Tampa, the Tampa Intercity Greyhound Bus Terminal, and the Port of Manatee. After the project is constructed and congestion is decreased, travel to and from these intermodal facilities will become faster and easier. The US 301 improvements will also include bicycle lane enhancements and an extension of the existing sidewalk network.

### 1.2.3 Safety

To evaluate traffic safety in the study corridor, crash data for the five-year period between 2007 and 2011 (the latest available data when the study started) were obtained from FDOT Crash Analysis Reporting System. The data was analyzed to determine the characteristics of the crashes that occurred within the study corridor. Based on FDOT data, a total of 637 crashes occurred along the study corridor during this five-year period. These crashes resulted in 3 fatalities and 457 injuries. **Table 1-1** shows the total number of crashes, fatalities and injuries that occurred during each of the five years.

**Table 1-1 Total Number of Crashes from 2007 to 2011**

Year	Total No. of Crashes	No. of Fatality Crashes	No. of Injury Crashes	No. of Property Damage Crashes	Total No. of Fatalities	Total No. of Injuries
2007	135	1	48	86	1	74
2008	143	0	56	87	0	84
2009	131	0	58	73	0	99
2010	115	1	54	60	1	93
2011	113	1	51	61	1	107
<b>Total</b>	<b>637</b>	<b>3</b>	<b>267</b>	<b>367</b>	<b>3</b>	<b>457</b>

The three most prevalent types of crashes are rear end crashes (335), angle crashes (138) and sideswipe crashes (50). Combined, these three crash types comprise approximately 82% of the total crashes within the study corridor.

**Table 1-2** summarizes the geographical distribution of the crashes. Approximately 58.9% of the total crashes occurred at either the SR 574 intersection (200 crashes) or the SR 60 intersection (175 crashes). Approximately 58% of the total injuries and two of the three fatalities also occurred at these two signalized intersections. The next highest crash locations were in the vicinity of the

eastbound I-4 ramps (50 crashes), Sabal Industrial Boulevard (31 crashes), Elm Fair Boulevard (29 crashes), and Columbus Drive/Tampa E. Boulevard (27 crashes).

**Table 1-2 Crash Distribution**

Intersection/Mainline	Milepost Limits		Total No. of Crashes	Total No. of Fatalities	Total No. of Injuries
	From	To			
SR 60	22.415	22.620	175	1	121
Mainline	22.680	22.848	13	0	8
Old Hopewell Rd.	22.889	23.010	10	0	1
Stannum St./Massaro Blvd.	23.081	23.194	16	0	6
Columbus Dr./Tampa East Blvd.	23.254	23.357	27	0	23
Mainline	23.454	23.454	1	0	2
Centerpoint Business Park	23.510	23.581	6	0	5
E Meadow Blvd.	23.648	23.770	4	0	3
Mainline	23.827	23.956	4	0	1
Overpass Rd./21 <sup>st</sup> Ave. East	23.995	24.131	14	0	7
Sabal Industrial Blvd.	24.153	24.287	31	0	31
27 <sup>th</sup> Ave.	24.316	24.437	17	0	10
Mainline Dr.	24.495	24.627	6	0	1
SR 574 (Martin Luther King Jr. Blvd.)	24.716	24.911	200	1	144
Mainline	24.920	25.066	14	0	12
Oak Fair Blvd.	25.183	25.302	16	0	18
Mainline	25.316	25.316	4	0	2
Elm Fair Blvd.	25.326	25.526	29	1	21
I-4 Eastbound Ramps	25.548	25.726	50	0	41
<b>Total</b>			<b>637</b>	<b>3</b>	<b>457</b>

**Table 1-3** summarizes the actual crash rates (expressed in terms of crashes per million vehicle-miles of travel) for the period from 2007 through 2011 that were obtained from the State Safety Office. **Table 1-3** also provides the five-year average crash rates for four-lane and six-lane divided suburban arterials. A review of this table indicates that there are several segments of US 301 that have actual crash rates that are significantly higher than the statewide, FDOT District Seven district-wide, and Hillsborough County average crash rates. However, the six-lane divided segment is short in length (i.e., 0.14 miles) and includes a signalized intersection (i.e., SR 574) which skews the comparison. It should be noted that the total number of crashes included in **Table 1-3** is greater than the 637 crashes documented in **Table 1-1** and **Table 1-2** because the data provided by the State Safety Office covered a slightly longer total corridor length.

**Table 1-3 Actual and Average Crash Rates**

Milepost		Length (in miles)	Classification	Total No. of Crashes	Crash Rate (crashes per million vehicle-miles)			
From	To				Actual	Statewide Average	District Average	County Average
22.410	23.695	1.285	23-Suburban 4-5 Lanes Two-way Divided Raised	251	2.873	1.324	1.837	1.952
23.695	24.245	0.550	24-Suburban 4-5 Lanes Two-way Divided Paved	42	1.149	1.886	1.533	1.803
24.245	24.676	0.431	23-Suburban 4-5 Lanes Two-way Divided Raised	42	1.499	1.324	1.837	1.952
24.676	24.816	0.140	33-Suburban 6+ Lanes Two-way Divided Raised	160	17.581	2.019	2.611	2.945
24.816	25.731	0.915	23-Suburban 4-5 Lanes Two-way Divided Raised	270	4.679	1.324	1.837	1.952

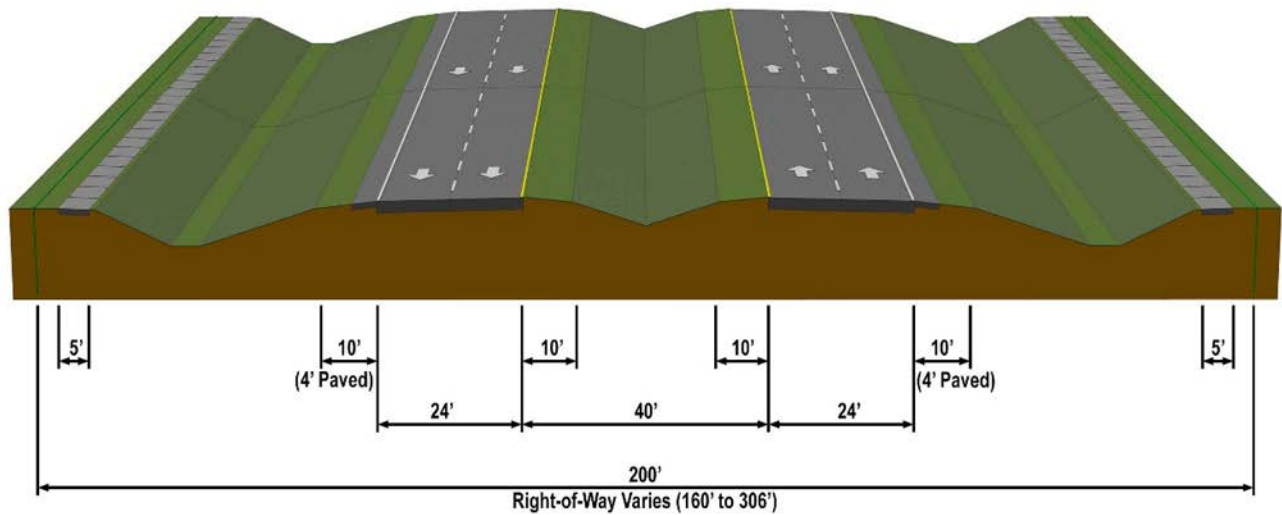
The proposed improvements primarily involve widening (six-laning) the US 301 mainline capacity, providing additional left-turn lanes and/or lengthening existing turn lanes, closing six existing median openings while converting five other existing full median openings to directional median openings, providing improved bike lanes and providing additional sidewalks in locations where they do not currently exist. These improvements may reduce the current crash rates in the corridor, and will provide a benefit to all users (includes drivers, bicyclists and pedestrians).

## 2.0 ALTERNATIVES

### 2.1 Existing Conditions

US 301 is a four-lane divided roadway throughout most of the study corridor with two 12-foot travel lanes in each direction as shown in **Figure 2-1**. A 40-foot grass median also exists throughout a majority of the study corridor. Stormwater runoff is collected in roadside ditches. US 301 transitions from a four-lane divided roadway to a six-lane divided roadway approximately 500 feet north of the SR 574 intersection and then transitions back to a four-lane divided roadway approximately 500 feet south of this intersection.

US 301 has a posted speed limit of 50 miles per hour (mph) within the majority of the project limits. The posted speed limit is reduced to 45 mph approaching SR 60 and approaching the on-ramp to eastbound I-4. The majority of the existing right-of-way (ROW) is 200 feet wide but portions vary from 160 to 306 feet wide.



**Figure 2-1 Existing Typical Section**

There are seven structures within the project limits. Four of these structures cross over active CSX Transportation rail lines (the S-Line and the A-Line). The S-Line is located between SR 60 and Old Hopewell Road, while the A-line is located just south of CR 574. There are also two structures that cross over the Tampa Bypass Canal. The remaining structure is a reinforced concrete double barrel bridge culvert that crosses Bruce Creek. Bruce Creek is located immediately south of Old Hopewell Road.

### 2.2 Proposed Improvements

#### 2.2.1 Typical Section and Design Speed

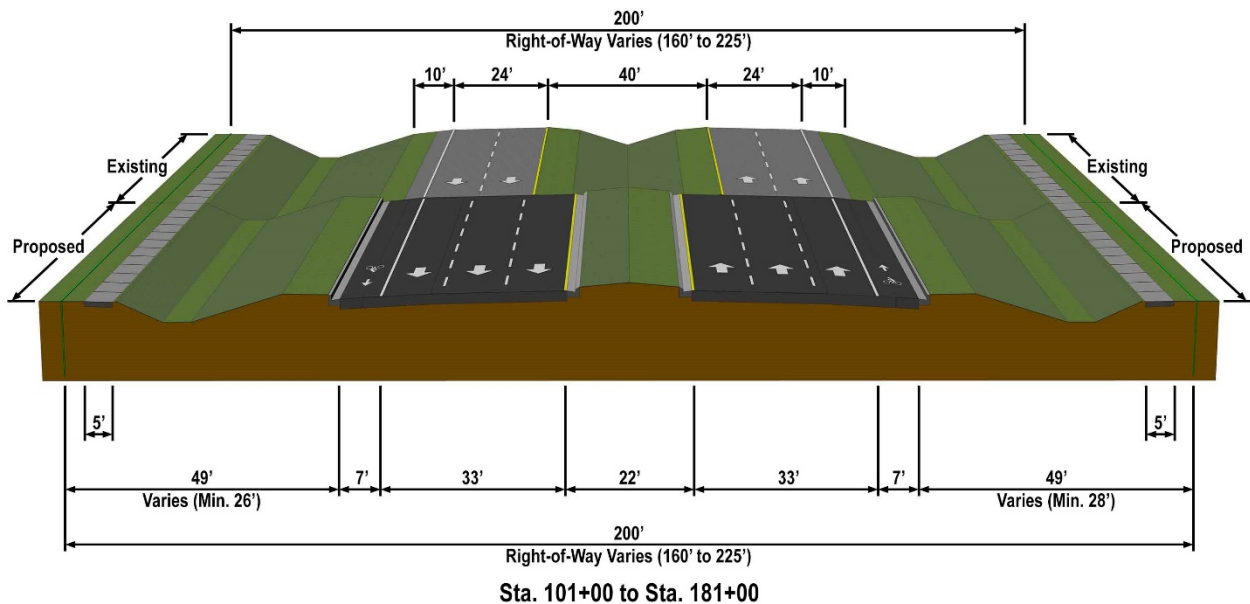
This section presents the primary engineering elements associated with the Preferred Build Alternative (Alternative 2). Alternative 2 consists of two typical sections for the widening of US 301. **Table 2-1** identifies the limits of the two typical sections.

**Table 2-1 Typical Section Limits**

Segment	Limits	Typical Section	Design Speed (mph)
1	From SR 60 to just north of Overpass Road/21 <sup>st</sup> Avenue	Urban	45 <sup>1</sup>
2	From just north of Overpass Road/21 <sup>st</sup> Avenue to SR 574	Suburban	50
3	From SR 574 to just south of the eastbound I-4 on-/off-ramps	Suburban	50

Note: 1. FDOT required the vertical alignment to be based on a design speed of 50 mph.

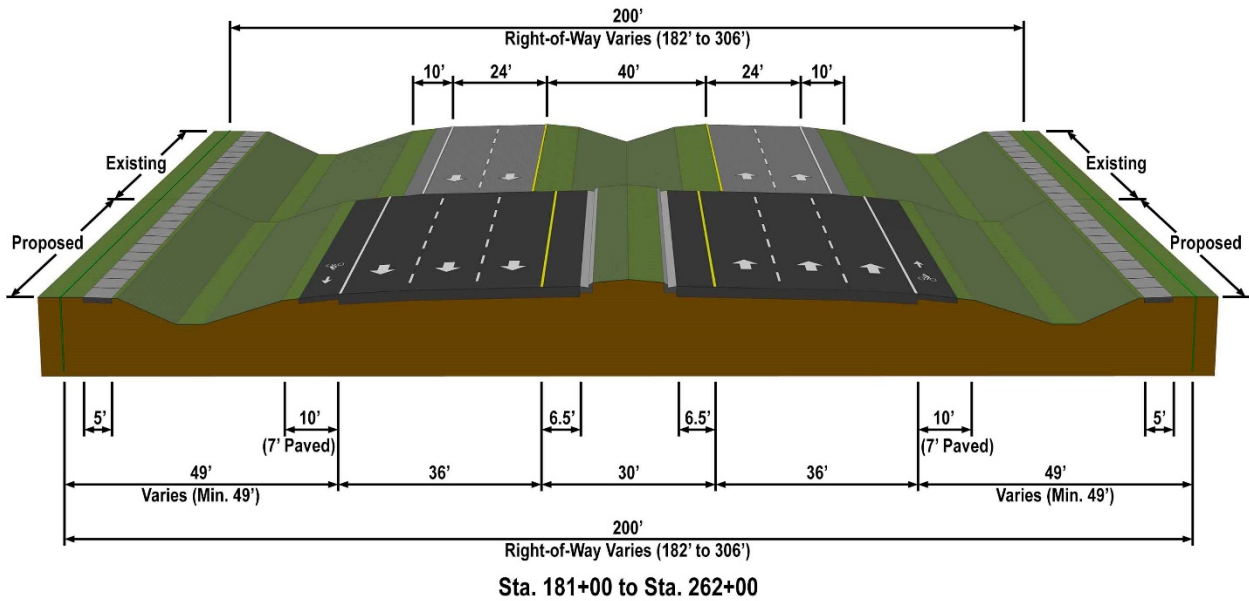
Typical section No. 1 is a 45 mph urban typical section that consists of six 11-foot travel lanes (three in each direction), 7-foot designated buffered bicycle lanes, a 22-foot raised median, and 5-foot sidewalks and curb and gutter on both sides. This urban typical section is illustrated in **Figure 2-2**.



**Figure 2-2 Proposed Urban Typical Section – Segment 1**

Typical section No. 2 is a 50 mph suburban typical section that consists of six 12-foot travel lanes (three in each direction), 6.5-foot paved inside shoulders, 10-foot outside shoulders (with 7 feet paved), a 30-foot raised median with curb and gutter in the median and 5-foot sidewalks on both sides. Typical section No. 2 is illustrated in **Figure 2-3**.





**Figure 2-3 Proposed Suburban Typical Section – Segments 2 & 3**

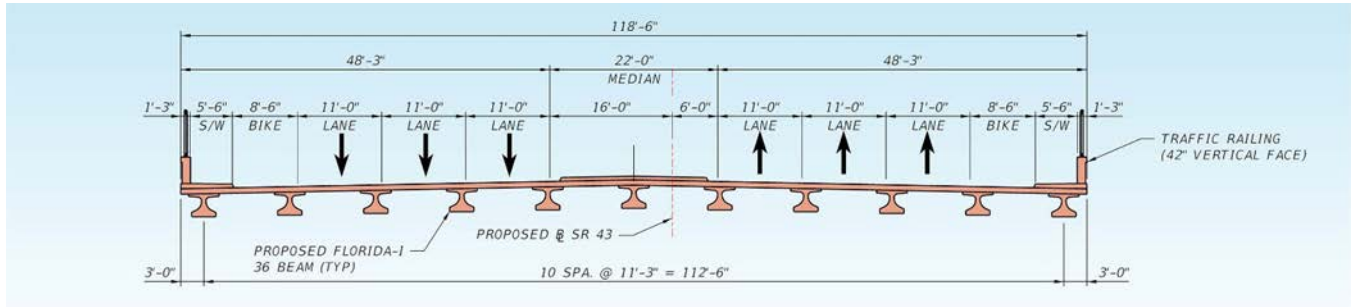
**2.2.2 Structures**

There are seven structures within the project limits and the proposed improvements are listed in **Table 2-2**. All four structures crossing the CSX Transportation rail lines will be replaced to provide the 23.5-foot minimum required vertical clearance. The proposed typical sections for the bridges over the CSX S-Line and CSX A-Line and CR 574 are shown in **Figure 2-4** and **Figure 2-5**, respectively.

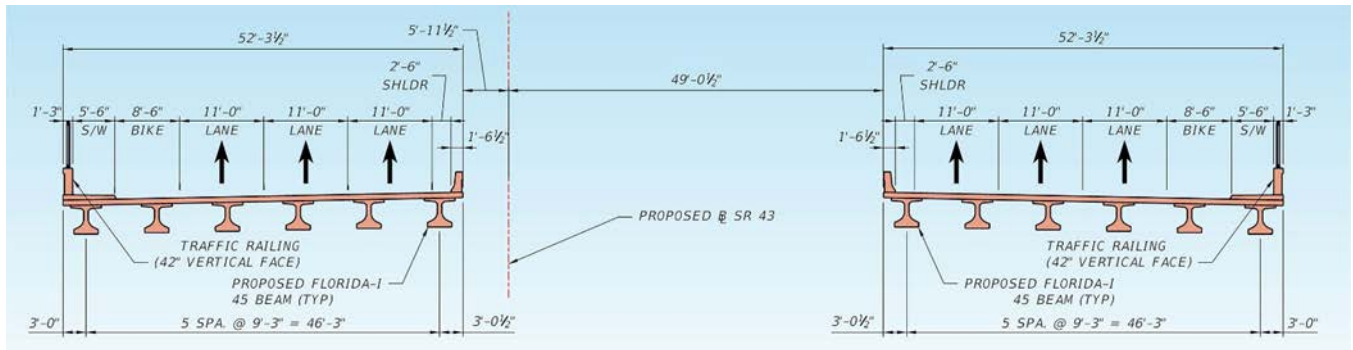
**Table 2-2 Bridge Improvements**

Bridge Number	Description	Year Built	Year Widening	Sufficiency Rating	Health Index	Proposed Improvement
100101 *	CSX S-Line (Northbound US 301)	1970	N/A	95.2	99.03	Replacement
100910 *	CSX S-Line (Southbound US 301)	1937	1971	95.2	99.54	Replacement
100574	Bruce Creek Double 10'x8' Culvert	1973	N/A	78.7	83.19	Extension
100102 *	CSX A-Line (Northbound US 301)	1970	N/A	95.2	98.00	Replacement
100011 *	CSX A-Line (Southbound US 301)	1931	1971	95.2	97.42	Replacement
100103	Tampa Bypass Canal (Northbound US 301)	1972	N/A	99.3	96.16	Widen
100012	Tampa Bypass Canal (Southbound US 301)	1972	N/A	99.2	96.52	Widen

\*All four bridges are considered to be functionally obsolete due to the minimum vertical clearance.

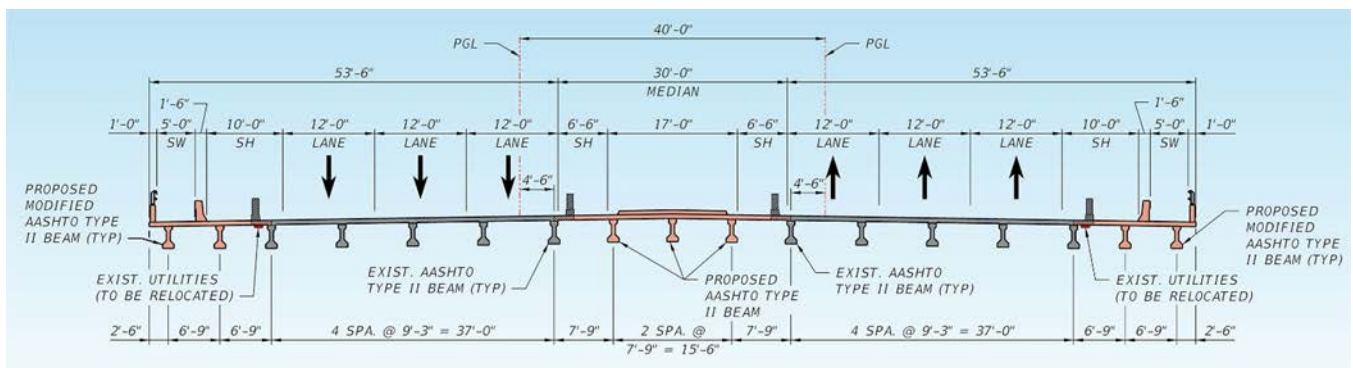


**Figure 2-4 Proposed Bridge Typical Section over CSX S-Line**



**Figure 2-5 Proposed Bridge Typical Section over CSX A-Line and CR 574**

The Bruce Creek bridge culvert will be extended and both Tampa Bypass Canal bridges will be widened. The open median between the two Tampa Bypass Canal bridges will be closed based on the proposed improvements shown in **Figure 2-6**.



**Figure 2-6 Proposed Bridge Typical Section over Tampa Bypass Canal**

### 2.2.3 Right-of-Way Needs and Relocations

The majority of the proposed US 301 mainline widening is located within the existing right-of-way. The additional right-of-way required is for corner clips resulting from the turning radius of the WB-62FL design vehicle and offsite stormwater facilities. Minor floodplain encroachments will be compensated for within the roadway right-of-way by steepening side slopes and excavating where

feasible. The additional roadway right-of-way needed for the US 301 improvements does not include any business or residential relocations.

**2.2.4 Cost Estimates**

The project costs estimated for the recommended alternative are summarized in **Table 2-3**. Construction costs were estimated in June 2015 using FDOT’s Long Range Estimate (LRE). The cost for design and construction engineering and inspection is estimated as 10% of the total construction cost.

**Table 2-3 Preliminary Project Cost Estimate**

<b>Cost Component</b>	<b>Alternative 2</b>
Design <sup>1</sup>	\$5,208,400
Mitigation Cost <sup>2</sup>	\$0
Total ROW cost <sup>3</sup>	\$8,594,900
Total construction cost	\$52,083,500
Construction Engineering & Inspection (CEI) <sup>4</sup>	\$5,208,400
<b>Preliminary Estimate of Total Project Cost (2015 Cost)</b>	<b>\$71,095,200</b>

Notes:

1. Design cost is estimated at 10% of the total construction cost.
2. Mitigation cost will be determined through consultation with environmental agencies.
3. The additional right-of-way required is for corner clips resulting from the turning radius of the WB-62FL design vehicle and offsite stormwater facilities.
4. CEI is estimated at 10% of the total construction cost.

## **3.0 IMPACT EVALUATION**

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The US 301 PD&E study has been developed in compliance with the FDOT's *PD&E Manual*. Potential project impacts to the social, cultural, natural and physical categories of the environment were evaluated and described in the following sections.

### **3.1 Social Impacts**

#### **3.1.1 Land Use Changes**

It is important to consider the potential impacts a roadway alignment might have on the existing and future land uses in the vicinity of the study corridor. A review of Hillsborough County's existing land use map, in addition to field reviews conducted during the early stages of the study, indicates that the existing land uses in the study corridor consist of primarily light industrial and commercial properties, with scattered residential. Two notable land uses include the Veteran's Memorial Park and the Florida State Fairgrounds, both owned by Hillsborough County. **Figure 3-1** depicts the existing land uses within the study area. There are no schools or churches along the project study corridor. A review of Hillsborough County's adopted Future Land Use Map indicates that the study corridor land uses will consist of light industrial, research corporate park and urban mixed use. **Figure 3-2** illustrates the future year land uses in the vicinity of the study corridor.

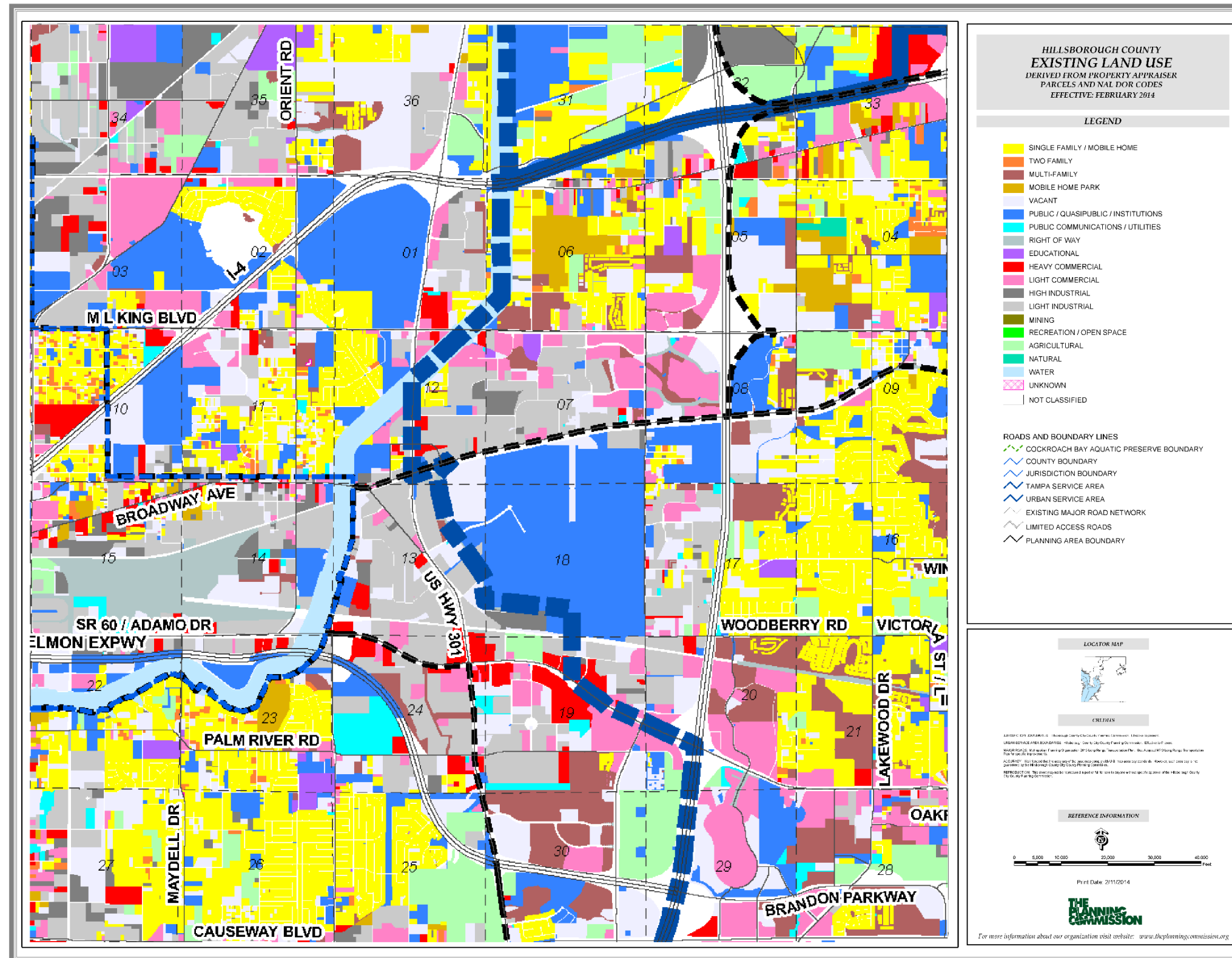


Figure 3-1 Existing Land Use Map

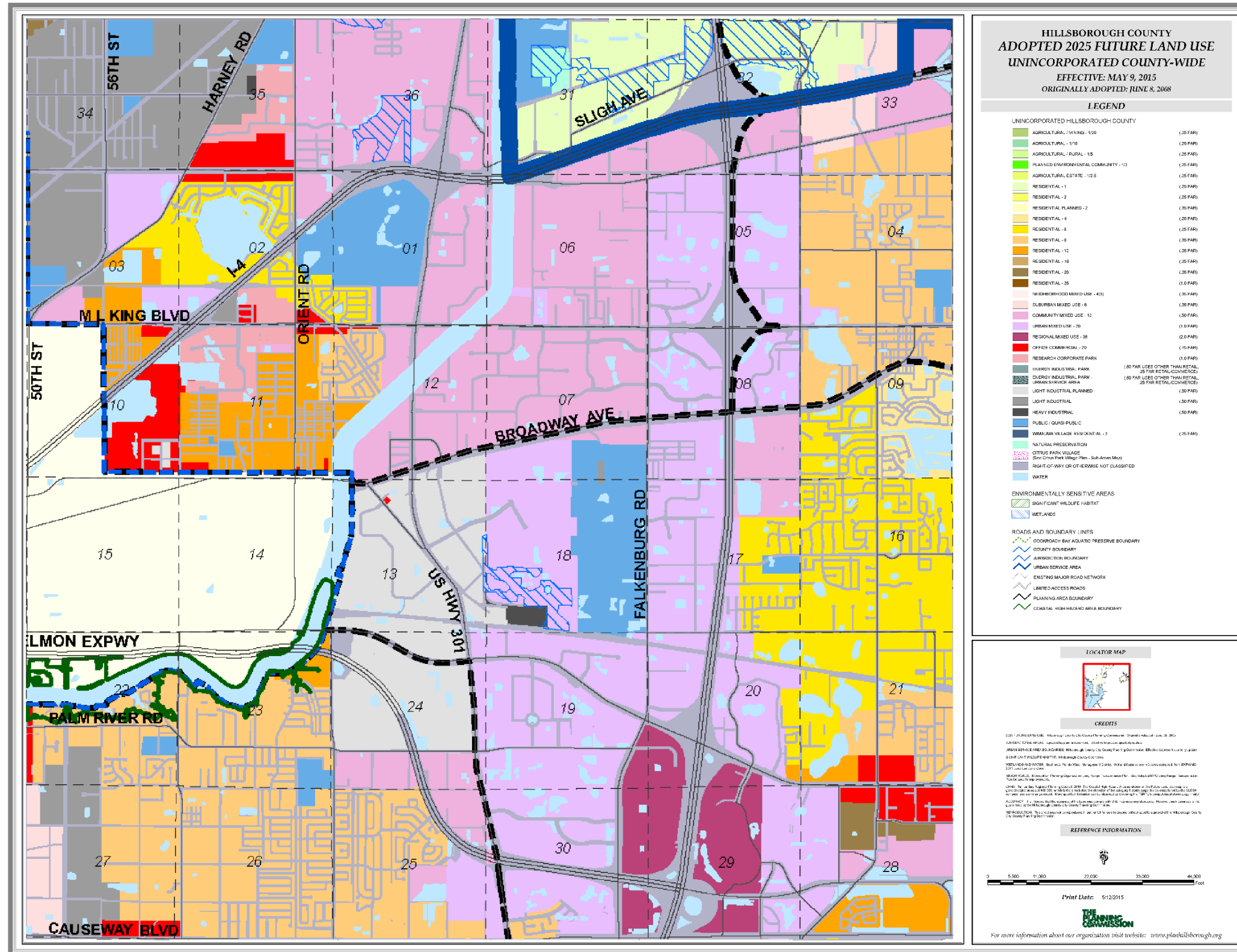


Figure 3-2 Future Land Use Map

Hillsborough County Geographic Information System (GIS) land use data was analyzed to determine what, if any, changes could be expected to the land uses surrounding US 301. Based on the available data, the area immediately adjacent to US 301 is anticipated to experience minimal changes in land use. The proposed roadway improvements should have no impact on changes to future land use patterns along the project corridor. Therefore, this category has been designated as **NONE** on the Summary of Environmental Impacts Checklist.

### **3.1.2 Community Cohesion**

The proposed project involves widening an existing facility that traverses through an existing urbanized area and will not cause splitting or isolation of any existing neighborhoods, industrial parks, or commercial areas. Additionally, this project is not anticipated to adversely impact elderly persons, handicapped individuals, non-drivers/transit-dependent individuals, or minorities. The proposed improvements will improve the connectivity and traffic flow within the community, potentially making the facility safer for vehicular, pedestrian, and bicycle movements along US 301. Therefore, this category has been designated as **NONE** on the Summary of Environmental Impacts Checklist.

### **3.1.3 Relocation Potential**

There are no anticipated residential or business relocations associated with the widening of US 301 since a majority of the roadway improvements can be constructed within the existing ROW. The additional ROW required is for corner clips resulting from the turning radius of the WB-62FL design vehicle and offsite stormwater facilities. Therefore, this category has been designated as **NONE** on the Summary of Environmental Impacts Checklist.

### **3.1.4 Community Services**

Community services typically serve the needs of the surrounding area and provide a focal point for adjacent neighborhoods and communities. Community services include churches, cemeteries, cultural centers, civic centers, clinics, social service centers, schools, parks, recreational facilities, and public buildings and facilities. Parks and recreational facilities are discussed in **Section 3.2.3** of this document. A review of the GIS data generated as part of the ETDM programming screen identified the following community services as being located adjacent to US 301 within, or close proximity to, the project study area limits.

- Fortis College
- The Creative Garden Social Service Facility
- Mental Health Care Family Support and Preservation
- The Good Sam Club Community Center
- Tampa Bay Horse Show Association Community Center
- Cracker Country Museum at the Florida State Fairgrounds

There would be no adverse impacts to neighborhoods, services and/or community facilities as a result of project implementation. It is anticipated that the widening of the existing four-lane facility, will reduce traffic congestion and improve traffic flow along US 301. This could improve emergency services by potentially reducing the emergency response times in the community. Therefore, this category has been designated as **NONE** on the Summary of Environmental Impacts Checklist.

### **3.1.5 Title VI Considerations**

In February 1994, the President of the United States issued Executive Order 12898 (Environmental Justice) requiring federal agencies to analyze and address, as appropriate,

disproportionately high adverse human health and environmental effects of federal actions on ethnic and cultural minority populations and low income populations, when such analysis is required by the National Environmental Policy Act of 1969 (NEPA).

An adverse effect on minority and/or low income populations occurs when:

1. The adverse effect occurs primarily to a minority and/or low income population; or
2. The adverse effect suffered by the minority and/or low-income population is more severe or greater in magnitude than the adverse effect suffered by the non-minority and /or non-low-income populations.

An evaluation of environmental, public health and interrelated social and economic effects of the proposed project on minority and/or low-income populations is required. All proposed projects should include measures to avoid, minimize and/or mitigate disproportionately high and adverse impacts and provide offsetting benefits and opportunities to enhance communities, neighborhoods, and individuals affected by these activities.

In addition to compliance with Executive Order 12898, any proposed federal project must comply with the provisions of Title VI of the Civil Rights Act of 1964, as amended by Title VIII of the Civil Rights Act of 1968. Title VI of the 1964 Civil Rights Act provides that no person will, on the grounds of race, color, religion, sex, national origin, marital status, disability, or family composition be excluded from participation in, be denied the benefits of, or be otherwise subject to discrimination under any program of the federal, state, or local government. Title VIII of the 1968 Civil Rights Act guarantees each person equal opportunity in housing.

In August 2000, the President of the United States issued Executive Order 13166 (Improving Access to Service for Persons with Limited English Proficiency), to clarify Title VI of the Civil Rights Act of 1964. Its purpose was to ensure accessibility to programs and services for eligible persons who are not proficient in the English language.

This project has been developed in accordance with the Civil Rights Act of 1964, as amended by the Civil Rights Act of 1968, and in accordance with Executive Order 12898. The adjacent communities, including Orient Park, contain a high percentage of minorities, greater than 30%, and as a result these areas have people exhibiting Limited English Proficiency (LEP). This LEP percentage varies depending on the specific location within the project corridor, but ranges from 2% to over 25%. Providence Pointe, a Habitat for Humanity community currently under construction (located on the east side of US 301 north of 27<sup>th</sup> Avenue), affords lower-income families who are willing to participate in their program an opportunity to move into a new home.

Many aspects of this project will be enhancements to the standard of living for residents in the study area, minority or otherwise, and users of surrounding facilities. There will be improvements for pedestrians and bicyclists with the provision of 5-foot sidewalks and 7-foot bicycle lanes with the recommended alternative. All proposed pedestrian amenities will include Americans with Disabilities Act (ADA) accessible features to the extent required by *ADA Accessibility Guidelines for Buildings and Facilities* and FDOT's design standards. Also, the project will improve mobility throughout the corridor for all users.

FDOT does not anticipate that the proposed project will result in any disproportionate adverse impacts to any distinct minority, ethnic, elderly or handicapped group, and/or low-income households since the majority of roadway improvements will be conducted within the existing ROW. The additional ROW required is for corner clips resulting from the turning radius of the WB-62FL design vehicle and offsite stormwater facilities. Therefore, this category has been designated as **NONE** on the Summary of Environmental Impacts Checklist.



### **3.1.6 Controversy Potential**

A Public Involvement Program (PIP) was developed for this project in compliance with the FDOT *PD&E Manual*, Part 1, Chapter 11; Section 339.155, Florida Statutes (F.S.); Executive Orders 11990 and 11988; Council on Environmental Quality Regulations for Implementing the Procedural Provisions of the National Environmental Policy Act (NEPA), 23 Code of Federal Regulations (CFR) 771, and the Americans with Disabilities Act (ADA). The purpose of the PIP is to assist FDOT in providing information to, and obtaining input from concerned citizens, agencies, private groups (residential/business), and governmental entities. The overall goal of this program is to help ensure that the study reflects the values and needs of the communities it is designed to benefit.

In 2012, this project was entered into the Programming Screen phase of the ETDM Environmental Screening Tool (EST) for agency review (ETDM #3097). The ETAT, comprised of agency representatives, reviewed this information and their comments are documented in the ETDM *Final Programming Screen Summary Report* (published January 9, 2013). There were no Substantial Degree of Effects assigned for any issues, therefore no Dispute Resolution activities were required.

The only public comments received to date involve the preliminary access management plan that was developed. This preliminary plan closes four of the seven existing median openings between SR 574 and I-4. Only the existing full median openings at SR 574 (existing signal) and Oak Fair Boulevard are proposed to remain as full median openings. The existing full median opening serving BP, Red Roof Inn, and Ker's WingHouse Bar & Grill will be converted to a southbound directional median opening. The traffic separator for this median opening will be extended to the I-4 ramp gore to preclude I-4 traffic from utilizing this median opening. With this access management plan, the primary entrance and exit for the Florida State Fairgrounds would be relocated further south to the existing Oak Fair Boulevard intersection. FDOT met with representatives of the Florida State Fairgrounds Authority and several business owners from the east side of US 301 regarding this plan and do not anticipate any controversy. As a result of the coordination with the public and local business community there has been very little controversy associated with the proposed improvements. Therefore, this category has been designated as **MINIMAL** on the Summary of Environmental Impacts Checklist.

### **3.1.7 Bicycles and Pedestrians**

Designated bicycle lanes and sidewalks exist from just south of SR 574 northward to the I-4 interchange. Other small portions of sidewalk exist between SR 60 and SR 574. Additionally, south of the Tampa Bypass Canal, bicycle lanes are provided on the shoulder but do not currently have any type of pavement markings associated with them. The No-Build Alternative does not change the pedestrian and bicycle facilities currently available. The recommended build alternative includes improvements for pedestrians and bicycles with the provision of a 5-foot sidewalk and a 7-foot bicycle lane on both sides of US 301 for both the urban and suburban typical sections.

A Recreational Property Inventory was conducted for this project. The inventory revealed there is one recreational property in the study corridor. Veteran's Memorial Park located on the west side of US 301 just north of the Tampa Bypass Canal, includes memorials, walking trails, exhibits and a picnic area. Additionally, the proposed Tampa Bypass Canal Trail is a future multi-use trail that would connect the Flatwoods Park in New Tampa through Wilderness and Trout Creek Parks and extend south to the McKay Bay Trail, the Selmon Greenway and the South County Trail. No long term impacts are expected for either of these recreational areas. Therefore, this category has been designated as **NONE** on the Summary of Environmental Impacts Checklist.

### 3.1.8 Utilities and Railroads

#### 3.1.8.1 Utilities

In order to evaluate potential surface and subsurface utility conflicts associated with the proposed project, information was obtained concerning the location and characteristics of the existing utilities within the US 301 study area. As shown in **Table 3-1**, a list of the utility agency owners (UAOs) in the vicinity of US 301 was obtained from the Sunshine State One Call of Florida service.

**Table 3-1 Utility Contact List**

Utility	Contact	Address	Phone Number	Facility within Study Area
AT&T Transmission	Greg Jacobson	6015 Benjamin Road Suite 306 Tampa FL 33634	(813) 342-0512	Fiber optic cables (FOC) within US 301 and CSX R/W – A Line and S Line
Bright House Networks	Randy Lyle	4145 S Falkenburg Road, Suite 4 Riverview FL 33578	(813) 684-6100 Ext. 32143	Coaxial & fiber optic cables, overhead TV (OTV) and buried TB (BTV) within US 301 R/W
Central Florida Pipeline/ Kinder Morgan	Mark Clark	2101 GATX Drive Tampa FL 33605	(813) 781-1718	Gas pipeline within CSX R/W – A Line
CenturyLink / Qwest	Mike Fitzgerald	5908 Hampton Oaks Parkway, Suite A Tampa FL 33610	(941) 661-7557	FOC within US 301 and CSX R/W – A Line and S Line
City of Tampa Wastewater Department	Dallas Pryor	306 E Jackson Street 6 North Tampa FL 33602	(813) 274-8936	Wastewater force main (FM) within US 301 R/W
City of Tampa Water Department	Janice Davis	306 E Jackson Street 5 East Tampa FL 33602	(813) 274-7096	Water Mains (WM) within US 301 R/W
FiberLight, LLC	Tim Greene	4023 N Armenia Avenue, Suite 200 Tampa FL 33607	(813) 877-7183	FOC (it has not been verified if FiberLight is within the R/W of US 301)
FPL Fibernet	Danny Haskett	9250 W Flagler Street FN/GO Miami FL 33174	(305) 552-2931	FOC within US 301 R/W
Hillsborough County Public Utilities	Doris Loughlin	925 E Twigg Street Tampa FL 33602	(813) 209-3041	No facilities within study area
Hillsborough County Sheriff's Office	Craig McEntyre	10140 Windhorst Road Tampa FL 33619	(813) 290-2222	FOC within US 301 R/W
Level 3 Communications, Inc.	Kelli Whitehead	1025 Eldorado Boulevard Broomfield CO 80021	(512) 742-1479	FOC with US 301 and CXS R/W – crossing S Line, parallel with A Line
MCI/Verizon Business	Charles Brunick	813 Ohio Avenue Lynn Haven FL 32444	(850) 265-3652	FOC within US 301 and CSX R/W – crossing S line, parallel with A Line
Pluris/Utility Partners, LLC	Joseph Kuhns	6608 Walton Way Tampa FL 33610	(813) 927-5798	No facilities within study area
Sprint/Ericsson Services	Mark Caldwell	201 E Pine Street Suite 1306 Orlando FL 32801	(321) 287-9942	FOC within US 301 R/W
Tampa Bay Water	Jon Kennedy	2575 Enterprise Road Clearwater FL 33763	(727) 796-2355	No facilities within study area
Tampa Electric Company	Heather Vitrano	2200 E Sligh Avenue Tampa FL 33610	(813) 275-3433	Electric within US 301 R/W and private easements
TECO Peoples Gas	Frank Kistner	1400 Channelside Drive Tampa FL 33605	(813) 275-3731	Gas mains (GM) within US 301 R/W
TW Telecom	James McVeigh	3030 N Rocky Point Drive Suite 850 Tampa FL 33607	(813) 316-7763	Coaxial Cables within US 301 R/W
Verizon Florida, LLC	Daniel Collings	7701 E Telecom Parkway MC FLTDSA3 Tampa FL 33637	(813) 978-2158	FOC, buried telephone (BT) within US 301 R/W
XO Communications	Jeff Sbrocco	5904 Hampton Oaks Parkway, Suite A Tampa FL 33610	(813) 301-4047	FOC within US 301 R/W

The proposed improvements may require the relocation of some or all of these utilities depending on their location and depth. A detailed description of the existing facilities and estimated relocation costs are summarized in the Preliminary Engineering Report for this project.

**3.1.8.2 Utility Mitigation Recommendations**

Most of the UAOs have the capability to adjust their services without causing major difficulties to their customers. The City of Tampa Water Department’s 36” water main provides water for a large number of customers within the City’s service area and is not easily adjusted. Shutting down this main, even for a brief time, would cause major disruption to the City’s customers.

Mitigation measures for this project should include minimizing service disruptions, allowing service disruptions only during periods of minimum usage and installing alternative or new services before disconnecting the existing service.

**3.1.8.3 Railroads**

The existing US 301 alignment crosses over two CSX railroad lines (the A-line and the S-line) at two different locations. The northbound and southbound bridges over the CSX A-line also cross over the adjacent CR 574 south of the Tampa Bypass Canal. The northbound and southbound bridges over the CSX S-line are located just to the north of SR 60. **Table 3-2** provides crossing information for both railroad locations.

**Table 3-2 Railroad Crossing Data**

Line Segment	DOT Crossing Inventory Number	Railroad Milepost	Maximum Timetable Speed	Total Number of Trains Crossing			Quiet Zone
				Day	Night	Switching	
S	624463X	838.92	50	3	2	12	No
A	624364A	876.21	79	3	4	2	No

Considering the utility mitigation measures summarized in **Section 3.1.8.2** and no anticipated disruption to the CSX railroad lines as a result of the proposed project improvements, the Utilities and Railroads category has been designated as **MINIMAL** on the Summary of Environmental Impacts Checklist.

**3.2 Cultural Impacts**

A Cultural Resource Assessment Survey (CRAS) was conducted to comply with the revised Chapter 267, Florida Statutes. All work was carried out in conformity with Part 2, Chapter 12 of FDOT’s *PD&E Manual*, and the standards contained in *The Historic Preservation Compliance Review Program of the Florida Department of State, Division of Historic Resources Manual* and the *Cultural Resource Management Standards and Operational Manual*. In addition, this study meets the specifications set forth in Chapter 1A-46, Florida Administrative Code (FAC).

The purpose of the CRAS was to locate and identify any cultural resources within the project Area of Potential Effects (APE) and to assess their significance in terms of eligibility for listing in the National Register of Historic Places (NRHP).

**3.2.1 Historic Sites/Districts**

The historical APE utilized in the CRAS was defined as the existing ROW as well as all immediately adjacent properties within 250 feet. The preliminary background research revealed that four previously recorded historic resources are located in the APE. These include two Frame

Vernacular style buildings (8HI06547A and 8HI06547B) and two linear resource groups (8HI11335 and 8HI11481).

Neither linear resource was evaluated by the State Historic Preservation Officer (SHPO). Historical/architectural field surveys of the US 301 PD&E study project APE were conducted from March 21, 2013 to May 20, 2013 and resulted in the identification and evaluation of 15 historic resources; including one bridge (8HI12133), two building complex resource groups (8HI12134 and 8HI12136), four linear resource groups (8HI11335, 8HI11481, 8HI12135, and 8HI12137), and eight buildings (8HI06547A, 8HI06547B, and 8HI12138 through 8HI12143). Four of these 15 historic resources were previously recorded in the Florida Master Site File (FMSF) and 11 were newly identified as a result of this survey. None of the historic buildings are considered potentially eligible for listing in the NRHP due to their commonality of style and lack of significant historical associations. Similarly, each building complex resource group is comprised of undistinguished examples of their respective types and styles and, therefore, does not meet the criteria of eligibility for listing in the NRHP. Further, there is no potential for historic districts within the APE. The segment of US 301 (8HI12137) contained within the project APE is not considered potentially eligible for NRHP listing because of its lack of physical historic integrity.

In conclusion, given the results of background research and archaeological and historical/architectural field surveys, with the exception of the three unevaluated linear resources (8HI11335, 8HI11481, and 8HI12135), project development will have no effect on any archaeological sites or historic resources that are listed, determined eligible, or considered potentially eligible for listing in the NRHP, or otherwise of historical or archaeological value. The SHPO found the CRAS complete and sufficient and concurred with the recommendations and findings for SHPO/DHR Project file number 2015-1775 on April 20, 2015. The SHPO concurrence letter is in **Appendix A**.

The Historic Sites/Districts category has been designated as **NONE** on the Summary of Environmental Impacts Checklist.

### **3.2.2 Archaeological Sites**

The archaeological APE for mainline improvements utilized in the CRAS was defined as within the existing ROW. A review of the FMSF and the NRHP indicated that 22 previously recorded archaeological sites are located within one mile of the study corridor. Site 8HI05048 (the US 301 Cloverleaf Site), a culturally indeterminate lithic scatter determined ineligible for listing in the NRHP by SHPO, is located within the project APE. The background research suggested a generally low potential for archaeological sites due to the poorly drained nature of the soils and lack of permanent water sources, as well as the extensively altered condition of the ROW. No new archaeological sites were discovered as the result of field survey and no evidence of 8HI05048 was found.

In conclusion, given the results of the background research and archaeological field surveys, project development will have no effect on any existing archaeological sites or other areas of archaeological value. The SHPO found the CRAS complete and sufficient and concurred with the recommendations and findings for SHPO/DHR Project file number 2015-1775 on April 20, 2015. The SHPO concurrence letter is in **Appendix A**. Therefore, the Archaeological Sites category has been designated as **NONE** on the Summary of Environmental Impacts Checklist as stated in the conclusion section of the CRAS.

### **3.2.3 Recreation Areas**

A Recreational Property Inventory was conducted for this project. The inventory revealed there is one recreational property in the study corridor. Veteran's Memorial Park located on the west side

of US 301 just north of the Tampa Bypass Canal, includes memorials, walking trails, exhibits and a picnic area. Additionally, the proposed Tampa Bypass Canal Trail is a future multi-use trail that would connect the Flatwoods Park in New Tampa through Wilderness and Trout Creek Parks and extend south to the McKay Bay Trail, the Selmon Greenway and the South County Trail. No long term impacts are expected for either of these recreational areas, therefore, this category has been designated as **NONE** on the Summary of Environmental Impacts Checklist.

### **3.3 Natural Impacts**

#### **3.3.1 Wetlands**

Pursuant to Presidential Executive Order 11990 entitled "Protection of Wetlands" (May 23, 1977), the United States Department of Transportation (USDOT) has developed a policy, (USDOT Order 5660.1A), Preservation of the Nation's Wetlands, dated August 24, 1978, which requires all federally funded highway projects to protect wetlands to the fullest extent possible. In accordance with this policy, as well as Part 2, Chapter 18 - Wetlands of the FDOT's *PD&E Manual*, two project alternatives were evaluated within the proposed US 301 study corridor to determine the potential wetland impacts associated with construction of this project. This evaluation assesses potential impacts of the alternative roadway alignments studied and efforts to avoid, minimize, or mitigate those impacts to the greatest extent possible. The Final Wetland Evaluation and Biological Assessment Report (WEBAR) prepared as part of this PD&E study documents the findings of the evaluation.

On May 1 and 14, 2013, 6.54 acres of surface waters were identified and mapped along the project corridor. No wetlands were identified within the project ROW. Surface waters identified for impact consist primarily of ditches that are located within the existing ROW. They have been previously disturbed by roadway construction, maintenance activities, and the invasion of nuisance and exotic species. A description of the dominant floral species, soil types, land use, and other pertinent remarks are provided in the Final WEBAR. Since no wetlands were identified within the project ROW, the Uniform Mitigation Assessment Methodology (UMAM) analysis was not necessary. Final determination of jurisdictional boundaries, in addition to mitigation requirements, will be coordinated between FDOT and the permitting agencies during the final design stage of the project.

The results of this PD&E study indicate there are no practicable alternatives to the anticipated impacts due to the need to increase roadway capacity. Furthermore, all wetland impacts have been avoided or minimized to the greatest degree possible and have been limited to those areas of previous disturbance. Therefore, the Wetlands category has been designated as **MINIMAL** on the Summary of Environmental Impacts Checklist.

#### **3.3.2 Water Quality**

The project is located in an area that is dominated by industrial and commercial land uses with minimal recreational lands, streams and waterways and residential areas interspersed throughout. Additionally, the proposed improvement will cross the Tampa Bypass Canal Tributary 2 (Bruce Creek) and the Tampa Bypass Canal (Six Mile Creek). The addition of impervious surface within the project corridor will increase stormwater runoff. The project area resides within four waterbodies as defined by the Florida Department of Environmental Protection (FDEP) and these include WBID 1536A (South Tampa Canal), 1536B (Six Mile Creek/Tampa Bypass Canal), 1536F (Six Mile Creek/Tampa Bypass Canal), and 1576 (Mango Drain). All four waterbodies are listed as impaired, however WBID 1536A is listed as impaired for Fecal Coliform which is not a pollutant of concern for FDOT. Pollutant loading removal calculations are to be included in the

Southwest Florida Water Management District (SWFWMD) permitting for the project and were performed for all basins proposed in the Pond Sizing Technical Memorandum.

A Water Quality Impact Evaluation (WQIE) has been completed for the PD&E study. The proposed stormwater facility design will include, at a minimum, the water quantity design requirements for water quality impacts as required by the SWFWMD in Rule 62-330 FAC and *Applicants Handbook* Volumes 1 and 2. The project will be designed to treat all stormwater runoff generated from the additional impervious surface area and will be designed to meet the SWFWMD criteria.

Proper Best Management Practices (BMPs) will be utilized during construction of the project to reduce or eliminate turbidity, erosion, and sedimentation into adjacent wetlands and surface waters found along the project corridor. The BMPs will prevent water quality degradation to surrounding or nearby waters during construction activities. Therefore, this category has been designated as **NONE** on the Summary of Environmental Impacts Checklist.

### **3.3.3 Floodplains**

In accordance with Executive Order 11988, "Floodplain Management," DOT Order 5650.2, "Floodplain Management and Protection," and Chapter 23, CFR, Part 650A, encroachment into floodplains from the construction of the proposed project was considered. A Final Location Hydraulics Memorandum was prepared to comply with 23 CFR 650 and 23 CFTR 771. Federal Emergency Management Agency (FEMA) National Flood Insurance Program (NFIP) Flood Insurance Rate Maps (FIRMs) were utilized to determine locations of highway encroachments on 100-year floodplains within or adjacent to the study corridor.

The latest revision of the FEMA Flood Insurance Study for Hillsborough County was adopted in 2013. Portions of the US 301 PD&E study area are located within the floodplain limits shown on FIRM Map Numbers 12057C0378J and 12057C0380J (both maps were revised September 27, 2013). Two locations along the study corridor are contiguous or situated within areas of Zone AE, which have base flood elevations determined from floodplain analyses of the 100-year frequency storm event. The effected floodplains are associated with the Tampa Bypass Canal, a U.S. Army Corps of Engineers project that alleviates major flooding along the Hillsborough River within Hillsborough County and the City of Tampa. The Tampa Bypass Canal is operated and maintained by SWFWMD.

The US 301 PD&E study corridor crosses the Tampa Bypass Canal Tributary 2, also known as Bruce Creek, just south of Old Hopewell Road. Bruce Creek has a base flood elevation (BFE) of 17.0 (NAVD 1988) on the downstream (west) side of US 301 and a BFE of 18.0 (NAVD 1988) on the upstream (east) side. Additionally, the study corridor crosses the Tampa Bypass Canal, also known as Six Mile Creek, with a BFE of 11.0 at both the upstream and downstream sides of the bridge.

The two floodplain crossings which occur along the existing US 301 alignment are short, transverse encroachments of freshwater or riverine floodplains. The floodplain encroachments will be minimal due to the proposed roadway alignment following the same alignment as the existing roadway and headwaters staying within the channel banks. Floodplain compensation for any freshwater encroachments may be required by SWFWMD. It is anticipated that compensation for the minor floodplain encroachments will be provided within the roadway right-of-way by steeping side slopes and excavating where feasible. Bruce Creek and the Tampa Bypass Canal are regulated floodways and will require preparation of "No Rise" Certifications during design.

Existing US 301 cross drains along the alignment include a double 10' x 8' bridge culvert at Bruce Creek, the Tampa Bypass Canal bridges, (which are 675 feet in length), and four other

cross drains. FDOT District Seven Tampa Maintenance Yard has not reported any flooding problems due to inadequately sized cross drains. Maintenance staff noted several drainage issues that are not related to the cross drains and would typically be addressed by roadway and drainage during the final design phase of the widening project.

All cross drain structures will have to be longer to accommodate the requirements of the widened roadway. Based upon visual observations it appears that the existing cross drains, if hydraulically suitable, are candidates for extension. However, it is recognized that some culverts may need to be replaced with hydraulically equivalent structures when they are analyzed in more detail (hydraulically and structurally) in the final design phase.

The existing corridor is already heavily developed. The proposed project will not encourage additional floodplain development due to local FEMA floodplain and SWFWMD regulations. The project drainage design will be consistent with local FEMA, FDOT, and SWFWMD design criteria. Therefore, no significant change in the base flood elevation or limits will occur. The proposed roadway will follow the same general alignment as the existing roadway. Therefore, no natural or beneficial floodplain values will be significantly affected.

Based on the information collected during this study, the proposed improvements can be categorized as STATEMENT 3: PROJECTS INVOLVING MODIFICATIONS TO EXISTING DRAINAGE STRUCTURES, as defined in Chapter 24 of the FDOT's *PD&E Manual*, Part 2, Figure 24.1.

The modifications to drainage structures included in this project will result in an insignificant change in their capacity to carry floodwater. This change will cause minimal increases in flood heights and flood limits. These minimal increases will not result in any significant adverse impacts on the natural and beneficial floodplain values or any significant change in flood risks or damage. There also will not be a significant change in the potential for this emergency evacuation route to become flooded and unavailable for use during an evacuation event. Therefore, the Floodplains category has been designated as **MINIMAL** on the Summary of Environmental Impacts Checklist.

### **3.3.4 Wildlife and Habitat**

The project was evaluated for potential impacts to wildlife and habitat resources, including protected species in accordance with 50 CFR Part 402, the Endangered Species Act of 1973, as amended, Chapters 5B- 40: *Preservation of Native Flora of Florida* and 68A-27 Florida Administrative Code (FAC) *Rules Relating to Endangered or Threatened Species*, and Part 2, Chapter 27 - Wildlife and Habitat Impacts of the FDOT's *PD&E Manual*.

Field surveys and database searches for protected species were conducted in 2013. One federally protected species, the wood stork (*Mycteria americana*), was determined to be present or have a high likelihood for using project habitats. The bald eagle (*Haliaeetus leucocephalus*), which receives protection under the Migratory Bird Treaty Act (MBTA) and the Bald and Golden Eagle Protection Act (BGEPA), and the osprey (*Pandion haliaetus*), which receives protection under the MBTA, also have the potential to occur within the project area. FDOT has detailed commitments to protect the federally-threatened eastern indigo snake (*Drymarchon corais couperi*), and state-threatened gopher tortoise (*Gopherus polyphemus*) which were both determined to have a low probability of occurrence within project habitats. One state-listed wildlife species (Florida Sandhill Crane), described below, was observed during field surveys.

The **wood stork** is designated as threatened by the U.S. Fish and Wildlife Service (USFWS). The project corridor is located within the Core Foraging Area (CFA) of six documented wood stork colonies. No wood storks were observed during field reviews; however, suitable foraging habitat exists within roadside ditches along the corridor. A foraging habitat assessment procedure may

be required to quantify impacts to suitable foraging habitat. However, because loss of these areas will either be mitigated or replaced, the project “may affect but is not likely to adversely affect” this species.

The **eastern indigo snake** is designated as threatened by the USFWS. This species typically inhabits a variety of natural areas including forested uplands and wetlands as well as wet and dry prairies. There is limited suitable habitat for this species near the highly urbanized project corridor and FDOT will commit to the standard protection measures detailed in the USFWS eastern indigo snake protection/education plan (August 12, 2013). Therefore, the project “may affect but is not likely to adversely affect” this species.

The **Florida sandhill crane** (*Grus canadensis pratensis*) is listed as threatened by the Florida Fish and Wildlife Conservation Commission (FWC). Adult sandhill cranes were observed in one area of the project corridor. Current FWC protection measures specify that no construction activities may occur within 125 meters of nest sites during the breeding season (January through August).

The **gopher tortoise** is listed as threatened by the FWC and is a candidate species for listing by the USFWS. Gopher tortoises thrive in xeric areas with sandy soils and open canopy with low groundcover. This habitat is largely absent from the project area. FDOT will commit to conducting comprehensive surveys for gopher tortoises and their burrows during the project’s final design phase. Until field surveys indicate otherwise, it has been determined that the project “may affect but is not likely to affect” the gopher tortoise.

In addition to faunal surveys, appropriate habitats were surveyed for protected flora. No federal or state-listed plant species were observed within the project area. This project proposes minimal impacts to undisturbed natural habitat and FDOT will be committed to coordination with the Florida Department of Agricultural and Consumer Services (FDACS) if protected plant species are observed within the proposed impact areas during the final design phase. Based on the results of the floral surveys, the project is not anticipated to adversely affect protected plant species.

Commitments to protect these species and habitat are provided and detailed in the Final WEBAR prepared as part of this PD&E study. These commitments include, but are not limited to, protection measures employed during design and construction phases. Standard operating measures such as providing compensatory mitigation measures for impacts to foraging habitat and resurveying of suitable habitat areas prior to construction will also provide protection for species and habitat. If protected species are identified, coordination with the USFWS, FWC and/or the FDACS - Division of Plant Industry will be initiated to determine permit requirements or modifications to construction activities that may be required. Consistent with the information provided in this section, the Wildlife and Habitat category has been designated as **MINIMAL** on the Summary of Environmental Impacts Checklist.

### **3.4 Physical Impacts**

#### **3.4.1 Noise**

A traffic noise evaluation was conducted and documented in a Final Noise Study Report (NSR) as a part of the PD&E study. The evaluation included an analysis of predicted traffic noise for noise sensitive areas along the Recommended Build Alternative alignment. The traffic noise analysis was performed following FDOT procedures that comply with Title 23 Code of Federal Regulations (CFR), Part 772, *Procedures for Abatement of Highway Traffic Noise and Construction Noise* (July 2010). In addition, Chapter 335.17, *Florida Statute*, requires the use of



23 CFR 772 in the noise impact assessment process, regardless of funding. The evaluation used methodologies established by FDOT and documented in the *PD&E Manual*, Part 2, Chapter 17 (May 2011). The prediction of traffic noise levels with and without the roadway improvements was performed using Version 2.5 of FHWA's Traffic Noise Model (TNM).

The purpose of the noise study is to identify noise sensitive sites that could be impacted with the proposed project, evaluate abatement measures at impacted noise sensitive sites, and determine where noise abatement (i.e., noise barriers) needs to be given further consideration during the final design phase of the project.

Abatement is evaluated for all noise sensitive sites predicted to approach/exceed the noise abatement criteria (NAC) or experience a substantial increase in traffic noise caused by the proposed project. Abatement measures considered include traffic management, alignment modifications, buffer zones, and noise barriers. Traffic management and alignment modification were determined to not be reasonable abatement measures.

Of the 18 evaluated noise sensitive receptors, nine were located at residences, three were restaurants with outdoor dining areas (Five Guys, Joe's Sandwich Shop, and 301 Family Restaurant), and three were evaluated as exterior uses associated with the Comfort Inn, La Quinta, and Holiday Inn hotels. A trail within Veteran's Memorial Park and an office complex (Centerpoint Business Park) with two exterior uses were also evaluated.

Existing (2013) traffic noise levels are predicted to range from 51.2 to 70.6 decibels on the "A" weighted scale (dB(A)) at the 18 receptors evaluated. In the future, without the proposed improvements (2040 No-Build), traffic noise levels are predicted to range from 53.1 to 70.8 dB(A) at these receptors. With the recommended improvements (2040 Build), traffic noise levels are predicted to range from 55.4 to 73.2 dB(A) with levels approaching, meeting, or exceeding the NAC at six of the receptors. When compared to the existing condition, traffic noise levels with the improvements are not predicted to increase more than 5 dB(A). Therefore, the project would not substantially increase traffic noise (i.e., cause an increase in traffic noise of 15 dB(A) or more with an improvement when compared to an existing level).

Noise abatement measures were considered for the six noise sensitive receptors where traffic noise levels are predicted to approach, meet, or exceed the NAC. The measures were traffic management, alternative roadway alignments, buffer zones, and noise barriers. The results of the analysis indicate that although feasible, traffic management and alternative roadway alignments are not reasonable methods of reducing predicted traffic noise levels at the impacted receptors. Additionally, providing a buffer between the highway and noise sensitive land uses is only reasonable for locating future noise sensitive uses and should be considered as part of the local land use planning process. The results of the analysis also indicate that noise barriers do not appear to be a potentially reasonable and feasible method of reducing predicted traffic noise levels for any of the impacted noise sensitive receptors should the project be implemented in the future.

Because the consideration of abatement measures did not indicate there are any measures that would be both feasible and reasonable, there is no commitment to further consider any measure during the project's final design phase. However, there is a commitment to perform a land use review at that time to ensure that all noise sensitive land uses that received a building permit prior to the project's Date of Public Knowledge (i.e., the date the SEIR is approved) have been evaluated. Notably, there was no construction or posted permits observed within the project limits when the land uses were surveyed on November 13, 2014.

Construction of the proposed roadway improvements could result in temporary construction related noise and/or vibration impacts. It is anticipated that the application of *FDOT Standard Specifications for Road and Bridge Construction* will minimize or eliminate potential construction noise and/or vibration impacts. Should noise or vibration issues arise during the construction process, the Project Engineer, in coordination with the District Noise Specialist and the Contractor, will investigate additional methods of controlling these impacts.

Land uses such as residences, offices, and parks are considered incompatible with highway noise levels exceeding the NAC. In order to reduce the possibility of new noise-related impacts, noise level contours were developed for the future improved roadway facility. These contours delineate the distance from the improved roadway's edge-of-travel lane where traffic noise levels of 56, 66, and 71 dB(A) (FDOT's NAC for Activity Categories A, B/C, and E, respectively) are expected to occur in the year 2040 with the proposed improvements. Local officials will be provided a copy of the final NSR to promote compatibility between land development and the construction of the proposed US 301 project.

The Noise category has been designated as **MINIMAL** on the Summary of Environmental Impacts Checklist. A detailed review of the predicted noise levels for each noise sensitive site, the predicted reduction of noise levels associated with barrier analyses at impacted receptor locations, and the distances of noise contours expected with the proposed improvements can be found in the Final NSR.

### **3.4.2 Air Quality**

The project study area is located in Hillsborough County, an area currently designated by the US Environmental Protection Agency (EPA) as "attainment" for all of the criteria air pollutants. Because the project is in an attainment area and designed to reduce congestion, it is not likely that the proposed improvements will have an impact on local or regional air pollutant/pollutant precursor emissions or concentrations. As required by FDOT, the project was subjected to a localized carbon monoxide (CO) screening analysis.

The project Build and No-Build Alternatives were evaluated for the opening year of the project (2020) and the project's design year (2040) using FDOT's air quality screening model, CO Florida 2012 (approved by the FHWA on April 12, 2013). CO Florida 2012 produces estimates of one- and eight-hour concentrations of CO at default air quality receptor locations. These concentrations can be directly compared to the one- and eight-hour National Ambient Air Quality Standards (NAAQS) for CO (35 and 9 parts per million [ppm], respectively).

The intersections projected to have the highest approach traffic volume in the years 2020 and 2040 with the No-Build and Build Alternatives are the SR 60 and SR 574 intersections. Based on the results of the screening model summarized in **Table 3-3**, the highest predicted one- and eight-hour CO concentrations would not exceed the NAAQS for this pollutant regardless of intersection, alternative, or year of analysis (because the intersection with the highest approach volume passed the screening test). Therefore, the project also "passes" the screening test.

**Table 3-3 Air Quality Intersection CO Screening Results**

Year	Alternative	Maximum CO Levels (parts per million)		Passes Screening Test
		Project one-hour	Project eight-hour	
2020 (Opening Year)	No-Build	5.7	3.4	Yes
	Build	5.7	3.4	Yes
2040 (Design Year)	No-Build	5.5	3.3	Yes
	Build	5.5	3.3	Yes

Any air quality impacts will be temporary and will primarily be in the form of emissions from diesel-powered construction equipment and dust from grading and embankment areas. Air pollution associated with the creation of airborne particles will be effectively controlled through the use of watering or the application of other controlled materials in accordance with FDOT’s *Standard Specifications for Road and Bridge Construction* as directed by the FDOT Project Engineer. The Air Quality category has been designated as **MINIMAL** on the Summary of Environmental Impacts Checklist as a result of the project “passing” the screening test.

**3.4.3 Construction**

Construction activities for the project may have short-term air, noise, vibration, water quality, traffic flow, and visual effects for those residents and travelers within the immediate vicinity of the project.

Noise and vibration effects will be from the heavy equipment movement and construction activities such as pile driving and vibratory compaction of embankments. Noise control measures will include those contained in FDOT’s *Standard Specifications for Road and Bridge Construction*. Specific noise level problems that may arise during construction of the project will be addressed by the FDOT’s construction engineer in cooperation with the appropriate District Environmental specialist. The Construction category therefore has been designated as **MINIMAL** on the Summary of Environmental Impacts Checklist.

**3.4.4 Contamination**

In accordance with FDOT policy and the FHWA requirements, a contamination screening evaluation was performed to evaluate potential project impacts from contaminated sites. A Final Contamination Screening Evaluation Report (CSER) was prepared pursuant to the FHWA’s Technical Advisory T 6640.8A and the FDOT’s *PD&E Manual*, Part 2, Chapter 22. Risk ratings were assigned after reviewing data obtained from on-site reviews of the parcels, a review of historical land use, hazardous/petroleum site lists, and other data.

All sites along or in close proximity to the US 301 study corridor were evaluated through review of historical resources such as aerial photography and city directories, regulatory sources at the county and state levels, site reconnaissance, literature review and when necessary, personal interviews of individuals and business owners within the limits of the project. Sixty-eight (68) mainline sites were investigated for facilities or operations that may present the potential for finding petroleum contamination or hazardous materials, and therefore may impact the proposed improvements for this project.

The specific project study area included the limits of the mainline project and an approximate 300-foot area extending beyond those boundaries. Of the 68 mainline sites investigated, the following

risk ratings for potential contamination concerns have been applied: 5 “High” sites, 9 “Medium” sites, 33 “Low” sites, and 21 “No” sites.

For the sites rated “No” for potential contamination, no further action is planned. These sites have been evaluated and determined not to have any potential environmental risk to the study area at this time.

For sites rated “Low” for potential contamination, no further action is required at this time. These sites/facilities have the potential to impact the study area, but based on select variables these sites have been determined to have low risk to the corridor at this time. Variables that may change the risk rating include a facility’s non-compliance to environmental regulations, new discharges to the soil or groundwater, and modifications to current permits. Should any of these variables change, additional assessment of the facilities would be conducted.

For those locations with a risk rating of “Medium” or “High”, Level II field screening will be conducted during the final design phase of the project. These sites have been determined to have potential contaminants, which may impact the project’s construction activities. Additional information may become available or site-specific conditions may change from the time the Final CSER was prepared and will be considered prior to proceeding with roadway construction. The Contamination category has been designated as **MINIMAL** on the Summary of Environmental Impacts Checklist.

### **3.5 Permits Required**

The U.S. Army Corps of Engineers (USACE) and SWFWMD regulate wetlands within the project area. The USFWS, United States Environmental Protection Agency (EPA), National Marine Fisheries Service (NMFS), and the FFWCC review and comment on wetland permit applications. It is currently anticipated that the following permits will be required for this project:

- Environmental Resource Permit – SWFWMD
- Section 404, Dredge and Fill Permit – USACE
- Section 408, Permit – USACE
- National Pollutant Discharge Elimination System Permit – FDEP

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## 4.0 PUBLIC INVOLVEMENT

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Public involvement is extremely important in the development of any transportation project.

### 4.1 Public Involvement Program

A Public Involvement Program (PIP) was created to identify stakeholders, agencies and other interested parties that should be included on the project mailing list. The PIP also documented numerous outreach techniques including a project web site, newsletters, small group meetings and a public hearing. A Comments and Coordination Report was prepared at the end of the PD&E study to document the results of the PIP. This section summarizes the results of the PIP that occurred during the alternative analysis process for this project's PD&E study.

### 4.2 Efficient Transportation Decision Making

As stated earlier, this project was entered into the Programming Screen phase of the ETDM Environmental Screening Tool (EST) in 2012 for agency review. At that time, preliminary information was entered including the draft purpose and need as well as the study area limits. The ETAT, comprised of agency representatives, reviewed this information and their comments are documented in the *ETDM Final Programming Screen Summary Report* (published January 9, 2013). The comments were reviewed and a degree of effect was identified for each of the environmental issues. The degrees of effect are summarized in the *ETDM Final Programming Screen Summary Report*.

### 4.3 Agency Meetings

At the beginning of the project, numerous agencies that would have an interest in the project were identified. The agency mailing list contained representatives from the ETAT, including federal and state government, as well as state permitting agencies. On January 22, 2014 a preliminary meeting was held with SWFWMD to discuss the project. The file number assigned to this project is PA 400766. During this meeting it was discussed that attenuation of the 25-year, 24-hour design storm event is not required for ponds discharging to the Tampa Bypass Canal, and that SWFWMD will acknowledge compensatory treatment to offset pollutant loads associated with portions of the project that cannot be physically treated. This includes the bridges over the Tampa Bypass Canal which are flat and are proposed for widening rather than replacement.

### 4.4 Stakeholder Meetings

At the beginning of the study, numerous stakeholders that could have an interest in the project were identified. The stakeholder mailing list included representatives from the various local governments, chambers of commerce, civic organizations, environmental groups and local businesses. The mailing list associated with this project is contained in **Appendix B**.

Small group presentations were incorporated into the public involvement program to provide a communication exchange in a one-on-one setting. Presentations were also made upon special request. A list of the small group meetings held during the study is shown in **Table 4-1**.

**Table 4-1 Small Group Meetings**

Date	Organization/Company Attending	Location
03-21-2014	Florida State Fair Authority, FDOT, and AIM	Florida State Fairgrounds Administration Office
09-16-2014	Ker's Winghouse Bar & Grill, Red Roof Inn, Holiday Inn Express, BP, Five Guys, Duke Realty, Cardinal Point Management, La Quinta Inn and Suites, FDOT, and AIM	Ker's Winghouse Bar & Grill
09-17-2014	Sims Crane & Equipment, FDOT, and AIM	Sims Crane & Equipment Office
09-19-2014	Florida State Fair Authority, FDOT, and AIM	Florida State Fairgrounds Administration Office

The study team met with the Florida State Fair Authority (FSFA) and business operators adjacent to US 301. The primary interest and topic of discussion with these groups was the proposed changes to the existing median openings and adjacent property access depicted in the US 301 improvement concepts.

The study team met with FSFA's Executive Director and staff on March 21, 2014. FDOT presented the preliminary improvement concept and discussed the access management plan that was developed for the portion of US 301 from SR 574 to just south of the eastbound I-4 ramps. A colored 1" = 100' scale concept drawing depicting the six-laning of US 301 and the proposed median openings was used to facilitate this discussion. With this access management plan, the primary entrance and exit for the Florida State Fairgrounds would be relocated further south to the existing Oak Fair Boulevard intersection. The proposed concept would still provide dual right-turn lanes on southbound US 301 at the Fairgrounds entrance; however, the length of these dual right-turn lanes would be increased significantly (from 600 feet to 1,500 feet). This would provide more queue storage on US 301 for vehicles entering the Fairgrounds and reduce the vehicle backups that currently occur on the exit ramp from eastbound I-4 to southbound US 301 during Florida State Fairgrounds events and amphitheater concerts.

FSFA staff asked if the existing full median opening located to the north of SR 574 could be maintained. The Fairgrounds purchased the property that was formerly owned by Jim Walters Corporation several years ago and currently uses this roadway to direct vehicles that originate from the southern portions of Hillsborough County into and out of the Fairgrounds during peak periods (to alleviate some of the traffic congestion at the main entrance/exit). The FSFA Executive Director stated that the Fairgrounds was currently working with a developer and exploring the possibility of developing some of the vacant land in the southeast portion of their property. The Fairgrounds felt that this existing median opening and entrance/exit roadway could be used to separate Fairgrounds event traffic from non-Fairgrounds event traffic and increase the potential viability of developing this area. FDOT explained that this full median opening was too close to the signalized full median opening at the SR 574 intersection and the spacing between these two existing median openings did not meet FDOT's minimum spacing standards. It was also pointed out that the proposed US 301 improvement concept provided triple left-turn lanes on the southbound US 301 approach at SR 574 and the length of these left-turn lanes precludes the ability to provide a northbound left-turn lane at this existing Fairgrounds access point. A follow-up meeting was held with FSFA's Executive Director and staff on September 19, 2014 and FDOT's US 301 improvement concept and FSFA's development plans were discussed in more detail. On

September 28, 2014, the governing board of the FSFA voted unanimously to reject a proposal from Republic Land Development to develop the southeast portion of the Fairgrounds property.

On September 16, 2014, the study team met with the owners/operators of the businesses located on the east side of US 301 from Oak Fair Boulevard to north of Elm Fair Boulevard, just south of the I-4 interchange. The meeting was held at Ker's WingHouse Bar & Grill of Brandon at 5003 US 301. Representatives from the following businesses attended: Ker's Winghouse Bar & Grill, Red Roof Inn, Holiday Inn Express, BP, Five Guys, Duke Realty, Cardinal Point Management, and La Quinta Inn and Suites. FDOT presented the preliminary improvement concept and discussed the access management plan that was developed for the portion of US 301 from SR 574 to just south of the eastbound I-4 ramps. The proposed access management plan closed the existing full median opening at Elm Fair Boulevard and the existing full median opening located just south of the I-4 interchange. These median opening closures would require southbound traffic to use the full median opening at Oak Fair Boulevard to access the businesses. The owners/operators of the businesses located between Elm Fair Boulevard and the I-4 interchange voiced their concern about customers not being willing to make a U-turn at Oak Fair Boulevard to access their businesses and choosing to patronize one of the businesses with more convenient access at Oak Fair Boulevard. Consequently, it was requested that FDOT reconsider the closure of the full median opening located just south of the I-4 interchange and provide a directional median opening that would allow southbound US 301 vehicles to turn left and access the northern businesses just south of the I-4 interchange. FDOT subsequently modified the proposed improvement's concept to provide this southbound directional median opening.

On September 17, 2014, the study team met with Steven Stodgill, president of Sims Crane & Equipment at Sims Crane's headquarters adjacent to US 301 at 1219 US 301. FDOT presented the preliminary improvement concept and discussed the median opening at Massaro Boulevard/Stannum Street. The proposed concept includes modifying this existing full median opening to provide a dual directional median opening. The directional median opening would prohibit vehicles from exiting Sims Crane and turning left onto southbound US 301. Mr. Stodgill was not opposed to the directional median opening at Massaro Boulevard/Stannum Street and stated that he thought it would be safer. Mr. Stodgill indicated that he has instructed Sims Crane's employees to turn right rather than left when exiting the property. Mr. Stodgill was concerned about the high vehicle speeds on northbound US 301 south of his entrance/exit and asked FDOT to consider providing an acceleration lane north of his driveway to help heavy trucks merge into the northbound US 301 traffic.

The existing posted speed limit in this area is 50 mph and with the proposed widening the posted speed limit will be reduced to 45 mph. The acceleration length from a stop condition to 45 mph is 560 feet based on AASHTO's A Policy on Geometric Design of Highways and Streets, 2004. The proposed US 301 improvement concept provides an exclusive northbound right-turn lane at Columbus Drive that extends back to south of the Southern Equipment Corporation driveway. The Southern Equipment Corporation driveway is located approximately 450 feet north of Stannum Street. If an acceleration lane was provided from Stannum Street northward to Columbus Drive, there could be potential operational problems in the shared acceleration lane/right-turn deceleration lane due to excessive vehicle weaving/lane changing. In addition, if the acceleration lane was provided at Stannum Street additional right-of-way would be required from the Southern Equipment Corporation property. The crash data was reviewed for this area and there were no rear-end crashes recorded. Based on these considerations an acceleration lane was not included at this location.

#### **4.5 Public Hearing**

The FDOT held a Public Hearing for the PD&E Study for the proposed improvements to US 301 on March 1, 2016 at the Sheraton Tampa East Hotel from 5:30 p.m. to 7:30 p.m. Draft project documents, including a draft of this SEIR, along with other project-related materials were on display as well as a project video presentation that ran continuously. The formal presentation began at 6:30 p.m. and discussed the project in detail. These details included the PD&E process, description of the Recommended Build Alternative, and anticipated right-of-way acquisition. The public was then invited to make formal oral comments following the formal portion of the public hearing, submit written comments at the hearing, or to mail/email comments following the hearing. A court reporter was also available at the hearing to receive comments in a one-on-one setting.

One formal oral comment was provided by a representative of Veteran's Memorial Park and Museum Complex. The formal portion of the public hearing concluded at 6:41 p.m. and the open house portion of the public hearing concluded at 7:30 p.m. The one formal oral comment included concerns about the US 301 improvements affecting the footprint of the Veteran's Memorial Park and Museum Complex and if the proposed access to the property would accommodate all sizes of vehicles. One additional comment was received through the mail on March 4, 2016 and included a critique of the selected location of the public hearing venue and the difficulty navigating through traffic in an effort to attend the hearing.



## **Appendix A – State Historic Preservation Officer Concurrence Letter**



## Florida Department of Transportation

RICK SCOTT  
GOVERNOR

11201 N. McKinley Drive  
Tampa, FL 33612-6456

JIM BOXOLD  
SECRETARY

April 14, 2015

Mr. Robert F. Bendus  
State Historic Preservation Officer  
Florida Division of Historical Resources  
500 South Bronough Street  
Tallahassee, Florida 32399-0250

Attention: Transportation Compliance Review Program

RE: US 301 (SR 43) from SR 60 (Adamo Drive) to I-4 (SR 400)  
Work Program Segment No.: 430050-1  
Hillsborough County, Florida

RECEIVED  
BUREAU OF  
HISTORIC PRESERVATION  
2015 APR 15 10 41 51

Dear Mr. Bendus:

The Florida Department of Transportation (FDOT), District Seven is preparing a state funded Project Development and Environment (PD&E) Study to evaluate roadway capacity improvements to US 301. Within the project limits, the existing roadway is a four-lane divided arterial roadway and the proposed improvements will expand it to a six-lane divided arterial roadway within the existing right-of-way (ROW). The total project length is 3.3 miles. A *State Environmental Impact Report* (SEIR) is being prepared for this study.

Enclosed is one (1) copy of the Cultural Resource Assessment Survey (CRAS) (March 2015) that was prepared for the above referenced project. Also enclosed are 17 Florida Master Site File (FMSF) forms (8HI333, 8HI1058, 8HI6547A, 8HI6547B, 8HI11335, 8HI11481, 8HI12133 through 8HI12143); a CD containing the FMSF photographs and pdf files of the FMSF forms and CRAS; and a Survey Log Sheet.

The CRAS included background research and a field survey. The purpose was to locate and identify any archaeological sites and historic resources located within the project area of potential effect (APE) and to assess their significance in terms of eligibility for listing in the National Register of Historic Places (NRHP). The archaeological Area of Potential Effect (APE) was defined as the existing ROW; the historical APE includes the existing ROW as well as immediately adjacent properties within 250 feet. Proposed pond and floodplain compensation sites were not identified in the PD&E Study and will be evaluated later during design.

Mr. Robert F. Bendus  
US 301 (SR 43) from SR 60 (Adamo Drive) to I-4 (SR 400)  
Work Program Segment No.: 430050-1  
April 14, 2015  
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Background research indicated that two (2) previously recorded archaeological sites (8HI333 and 8HI1058) are located within or near the project APE. The background research suggested a generally low potential for archaeological sites. As a result of field survey, no new archaeological sites were discovered and no evidence of the two (2) previously recorded sites was found.

Background research revealed that four (4) previously recorded historic resources are located within the project APE. Historical/architectural field survey resulted in the identification and evaluation of 15 historic resources. This includes one (1) bridge (8HI12133); two (2) building complex resource groups (8HI12134 and 8HI12136); four (4) linear resource groups (8HI11335, 8HI11481, 8HI12135, and 8HI12137); and eight (8) buildings (8HI06547A, 8HI06547B, and 8HI12138 through 8HI12143). Of these 15 historic resources, four (4) were previously recorded in the FMSF and 11 were newly identified as a result of this survey. None of the historic buildings is considered potentially eligible for listing in the NRHP due to their commonality of style and lack of significant historical associations. Similarly, each building complex resource group is comprised of undistinguished examples of their respective types and styles and, therefore, does not meet the criteria of eligibility for listing in the NRHP. Further, there is no potential for historic districts within the APE. There is insufficient information to determine the NRHP eligibility of the Seaboard Railway (8HI11335), the Atlantic Coast Line Railroad (8HI11481), and the Tampa Bypass Canal (8HI12135), because only short segments of these linear resource groups are located within the US 301 project APE. The segment of US 301 (8HI12137) contained within the project APE is not considered potentially eligible for NRHP listing because of its lack of physical historic integrity.

Based on the results of background research and field surveys, with the exception of the three unevaluated linear resources (8HI11335, 8HI11481, and 8HI12135), there are no NRHP listed or eligible resources associated with this project.

This information is being provided in accordance with the provisions contained in the revised Chapter 267, Florida Statutes. Provided you approve the recommendations and findings in the enclosed cultural resource document, please sign below for concurrence. If you have any questions, please contact me at (813) 975-6456 or [todd.bogner@dot.state.fl.us](mailto:todd.bogner@dot.state.fl.us) or Rebecca Spain Schwarz at (813) 281- 8308 or [rebecca.spain-schwarz@atkinsglobal.com](mailto:rebecca.spain-schwarz@atkinsglobal.com).

Sincerely,



Todd L. Bogner  
Environmental Specialist III  
Cultural Resource Coordinator

Enclosure


cc: Roy Jackson (FDOT SEMO)      Robin Rhinesmith (FDOT)      Stephanie Pierce (FDOT)  
Sean Donahoo (AIM)      Rebecca Spain Schwarz (Atkins/GEC)

Mr. Robert F. Bendus  
US 301 (SR 43) from SR 60 (Adamo Drive) to I-4 (SR 400)  
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The Florida State Historic Preservation Officer finds the attached Cultural Resource Assessment Survey complete and sufficient and concurs with the recommendations and findings provided in this cover letter for SHPO/DHR Project File Number 2015-1775. Or, the SHPO finds the attached Technical Memorandum contains \_\_\_ insufficient information.

SHPO Comments:


/s/   
Robert F. Bendus, Director  
Florida Division of Historical Resources  
and State Historic Preservation Officer

4/20/15  
Date

## **Appendix B – Stakeholder Mailing List**

FPID: 430050-1-22-01

Description: US 301 from SR 60 to I-4  
Hillsborough County

**Property Owners Site Addresses List**

	OWNER	MAIL_ADDR_1	MAIL_ADDR_2	MAIL_CITY	MAIL_STATE	MAIL_ZIP	COUNTRY	SUB
1	ARRAHAM P CHACKO	2535 BRIMHOLLOW DR		VALRICO	FL	33596-5744		1NK
1	BIG BEND CENTERS LLC	1706 S KINGSWAY RD		SEFFNER	FL	33584-5348		ZZZ
1	BRENDA ST CLAIR ELTING	PO BOX 1425		BRANDON	FL	33509-1425		1NV
1	CARGO GASOLINE CO	C/O MR L M HUGHHEY	205 S HOOVER BLVD STE 400	TAMPA	FL	33609-3591		1NK
1	CARGO GASOLINE CO	C/O MR L M HUGHHEY	205 S HOOVER BLVD STE 400	TAMPA	FL	33609-3591		1NK
1	CORNETTE PROPERTIES INC	PO BOX 456		ODESSA	FL	33556-0456		1NV
1	CSX TRANSPORTATION INC	TAX DEPT (C-910)	500 WATER ST	JACKSONVILLE	FL	32202-4445		
1	CSX TRANSPORTATION INC	TAX DEPT (C-910)	500 WATER ST	JACKSONVILLE	FL	32202-4445		
1	DUKES SECURED FINANCING 2009-1A1Z LLC	600 E 96TH ST STE 100		INDIANAPOLIS	IN	46240-3786		
1	DUKES SECURED FINANCING 2009-1A1Z LLC	600 E 96TH ST STE 100		INDIANAPOLIS	IN	46240-3786		
1	DUKES SECURED FINANCING 2009-1A1Z LLC	600 E 96TH ST STE 100		INDIANAPOLIS	IN	46240-3786		
1	DUKES SECURED FINANCING 2009-1A1Z LLC	600 E 96TH ST STE 100		INDIANAPOLIS	IN	46240-3786		
1	DUKES SECURED FINANCING 2009-1A1Z LLC	600 E 96TH ST STE 100		INDIANAPOLIS	IN	46240-3786		
1	DUKES SECURED FINANCING 2009-1A1Z LLC	600 E 96TH ST STE 100		INDIANAPOLIS	IN	46240-3786		
1	DUKES SECURED FINANCING 2009-1A1Z LLC	600 E 96TH ST STE 100		INDIANAPOLIS	IN	46240-3786		
1	EASTMADDOX DISTRIBUTION CENTER PHASE II L P	PO BOX 1368		CARLSBAD	CA	92018-1368		ZZZ
1	FLORIDA STATE FAIR FOUNDATION INC	4800 N US HIGHWAY 301		TAMPA	FL	33610-7350		ZZZ
1	GARY BARNHART	11311 N EDISON AVE		TAMPA	FL	33612-5108		ZZZ
1	GARY L BARNHART	PO BOX 280001		TAMPA	FL	33682-0001		ZZZ
1	HIGHWAY 60 AND 301 CENTER INC	105 S US HIGHWAY 301 STE 110		TAMPA	FL	33619-3534		663
1	HILLSBOROUGH COUNTY	REAL ESTATE DEPT	PO BOX 1110	TAMPA	FL	33601-1110		ZZZ
1	HILLSBOROUGH COUNTY	REAL ESTATE DEPT	PO BOX 1110	TAMPA	FL	33601-1110		ZZZ
1	HILLSBOROUGH COUNTY	REAL ESTATE DEPT	PO BOX 1110	TAMPA	FL	33601-1110		ZZZ
1	IBP FLEXSPACE 2 LTD	1400 NW 107TH AVE FL 5		DORAL	FL	33172-2746		
1	INTERSTATE BUSINESS PARK LLC	74 W PARK PL		STAMFORD	CT	06901-2209		
1	LEVANT ENTERPRISES LLC	C/O LEE A LEVANT	PO BOX 75301	TAMPA	FL	33675-0301		1NV
1	LEVANT ENTERPRISES LLC	2803 ORIENT RD		TAMPA	FL	33619-2503		1NV
1	METROPOLITAN LIFE INS	C/O REAL ESTATE TAX SERVICE	101 E KENNEDY BLVD STE 2330	TAMPA	FL	33602-5147		ZZZ
1	METROPOLITAN LIFE INS	C/O REAL ESTATE TAX SERVICE	101 E KENNEDY BLVD STE 2330	TAMPA	FL	33602-5147		ZZZ
1	METROPOLITAN LIFE INS CO	C/O REAL ESTATE TAX SERVICE	101 E KENNEDY BLVD STE 2330	TAMPA	FL	33602-5147		ZZZ
1	PARK OF COMMERCE ASSOCIATION INC	REAL ESTATE TAX ADVISORS LLC	PO BOX 40509	INDIANAPOLIS	IN	46240-0509		
1	PARK OF COMMERCE ASSOCIATION INC	REAL ESTATE TAX ADVISORS LLC	PO BOX 40509	INDIANAPOLIS	IN	46240-0509		
1	RYAN GERMAK	4214 GARDEN LN		TAMPA	FL	33610-7320		
1	SOUTHWEST FLORIDA WATER MANAGEMENT DISTRICT	2379 BROAD ST		BROOKSVILLE	FL	34604-6899		ZZZ
1	SOUTHWEST FLORIDA WATER MANAGEMENT DISTRICT	2379 BROAD ST		BROOKSVILLE	FL	34604-6899		ZZZ
1	SOUTHWEST FLORIDA WATER MANAGEMENT DISTRICT	2379 BROAD ST		BROOKSVILLE	FL	34604-6899		ZZZ
1	SOUTHWEST FLORIDA WATER MANAGEMENT DISTRICT	2379 BROAD ST		BROOKSVILLE	FL	34604-6899		ZZZ
1	SPRINGHILL MISSIONARY BAPTIST CHURCH OF TAMPA INC	8119 E DR MARTIN LUTHER KING JR BLVD		TAMPA	FL	33619-1242		1NK
1	BOOG AND JULSTER LLC	10000 LINDELAAN DR		TAMPA	FL	33618-4208		ZZZ
1	BOOG AND JULSTER LLC	10000 LINDELAAN DR		TAMPA	FL	33618-4208		ZZZ
1	C & O PROPERTIES LTD	2905 PREMIERE PKWY STE 300		DULUTH	GA	30097-5240		ZZZ
1	G & VII HOPEWELL LLC	220 E 42ND ST FL 27		NEW YORK	NY	10017-5819		ZZZ
1	LEVANT ENTERPRISES LLC	2803 ORIENT RD		TAMPA	FL	33619-2503		1NV
1	C&O PROPERTIES LTD	2905 PREMIERE PKWY STE 300		DULUTH	GA	30097-5240		ZZZ
1	CAROL ANNE METZMEIER	8107 E 19TH AVE		TAMPA	FL	33619-2201		1NV
1	CITY OF TAMPA	ATTN REAL ESTATE DIVISION	308 E JACKSON ST	TAMPA	FL	33602-5223		1NV
1	CORNETTE PROPERTIES INC	PO BOX 456		ODESSA	FL	33556-0456		1NV

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CSX TRANSPORTATION INC-ASSESSED BY DEPT OF REVENUE	TAX DEPT (C-910)	500 WATER ST	JACKSONVILLE	FL	32202-4423	ZZZ
DIANNE STACKPOLE POWELL/LIFE ESTATE	8110 CARROLL BLVD		TAMPA	FL	33619-2216	1NW
13940 WILLARD ROAD LLC	4539 OAK FAIR BLVD STE A		TAMPA	FL	33610-7304	
3910 ENTERPRISES LLC	C/O RICHARD KINGSLAND	7410 E COLONIAL DR	ORLANDO	FL	32807-6316	ZZZ
8610 N HIGHWAY 301 ELM FAIR BOULEVARD HOLDINGS LLC	C/O CWCAPITAL	7501 WISCONSIN AVE STE 5003	BETHESDA	MD	20814-6581	1M1
A D MACKINNON FAMILY LIMITED PARTNERSHIP LLLP	2230 N US HIGHWAY 301		TAMPA	FL	33619-2646	1N7
ABRAHAM P CHACKO AND ALEYAMMA P ABRAHAM ET AL	2535 BRIMHOLLOW DR		VAIRICO	FL	33596-5744	1NK
ACE HARDWARE	ATTN TAX DEPT	2200 KENSINGTON CT	OAK BROOK	IL	60523-2103	ZZZ
AIRGAS-SOUTH INC	2015 VAUGHN RD NW STE 400		KENNESAW	GA	30144-7802	ZZZ
ALFREDO PEREZ AND SURELLA LOPEZ	8412 RADIO LN		TAMPA	FL	33619-1320	ZZZ
AMERADA HESS FACILITIES BUSINESS TRUST NO 1998-1	PO BOX 696419		SAN ANTONIO	TX	78269-6419	ZZZ
ANTHONY BP INC	4911 N US HIGHWAY 301		TAMPA	FL	33610-7373	ZZZ
ARM PROPERTIES	8405 N EDISON AVE		TAMPA	FL	33604-1210	ZZZ
ARMANDI LLC	1341 MASSARQ BLVD		TAMPA	FL	33619-3063	ZZZ
ASPEN REMEDIAL CORP	PO BOX 7106		WESLEY CHAPEL	FL	33545-0101	ZZZ
B A S DEVELOPMENT INC	11311 N EDISON AVE		TAMPA	FL	33612-5108	ZZZ
BARLOWORLD HANDLING	BRUNGART PROPERTIES	2849 NE 35TH CT	FORT LAUDERDALE	FL	33308-5815	ZZZ
BARNEY M AND JANVALUK GILEWITZ	4112 GARDEN LN		TAMPA	FL	33610-7318	
BEATRICE J BUSH	8110 E 19TH AVE		TAMPA	FL	33619-2702	1NW
BENVENUTI PROPERTIES LLC	1920 TAMPA EAST BLVD		TAMPA	FL	33619-3024	ZZZ
BIG BEND CENTERS LLC	1706 S KINGSWAY RD		SEFFNER	FL	33584-5348	ZZZ
BOOG AND JULSTER LLC	10000 LINDELAN DR		TAMPA	FL	33618-4208	ZZZ
BRE/LO FL PROPERTIES LLC	909 HIDDEN RDG STE 600		IRVING	TX	75038-3822	1M1
BRENDA ST CLAIR ELTING	PO BOX 1425		BRANDON	FL	33509-1425	1NW
CARBONIC PROPERTIES	1610 S DIVISION AVE		ORLANDO	FL	32805-4726	1NW
CARGO GASOLINE CO	C/O MR L M HUGHLEY	205 S HOOVER BLVD STE 400	ORLANDO	FL	32805-4726	1NW
CORRUGATED INDUSTRIES OF FLORIDA INC	1920 N US HIGHWAY 301		TAMPA	FL	33619-2640	1NX
DAVID E HOMZA	4220 GARDEN LN		TAMPA	FL	33610-7320	ZZZ
DAWN M HOUGH AND RONALD D HOUGH	4202 GARDEN LN		TAMPA	FL	33610-7320	
DH TAMPA LLC	600 E 96TH ST STE 100		INDIANAPOLIS	IN	46240-3786	
DORIS A ARGUELLES	8116 E 19TH AVE		TAMPA	FL	33619-2702	1NW
DUKES SECURED FINANCING 2009-JA1Z LLC	600 E 96TH ST STE 100		INDIANAPOLIS	IN	46240-3786	1M1
EASTGROUP PROPERTIES LP	2966 COMMERCE PARK DR STE 450		ORLANDO	FL	32819-8616	ZZZ
EASTMEADOW DISTRIBUTION CENTER LP #2313	PO BOX 1368		CARLSBAD	CA	92018-1368	1N7
EFFICIENCY LEASING OF FLORIDA LLC	8509 E DR MARTIN LUTHER KING JR BLVD		TAMPA	FL	33610-7303	9RL
EVELYN M ALLEN	3723 N 56TH ST		TAMPA	FL	33619-1427	
EXTER 8800 ADAMO LLC	C/O EXETER PROPERTY GROUP	140 WEST GERMANTOWN PIKE	PLYMOUTH MEETING	PA	19462-1434	ZZZ
FIRST TAMPA 301 LLC	C/O5-G MALHI LLC	11821 SHIRE WYCLIFFE CT	TAMPA	FL	33626-3330	1M1
FLORIDA STATE FAIR AUTHORITY	PO BOX 11766		TAMPA	FL	33680-1766	ZZZ
FMW RRI LLC	5847 SAN FELIPE ST STE 4650		HOUSTON	TX	77057-3277	ZZZ
FRANK E AND ANGELA S HINELINE	8117 E 19TH AVE		TAMPA	FL	33619-2201	1NW
G&I TAMPA EAST LLC	220 E 42ND ST FL 27		NEW YORK	NY	10017-5819	ZZZ
GARY BARNHART	11311 N EDISON AVE		TAMPA	FL	33612-5108	ZZZ
GEORGE AND SOPHY THOMAS	5804 TULIP FLOWER DR		RIVERVIEW	FL	33578-3735	
GEORGE F YOUNG OF FLORIDA INC	299 DR MARTIN LUTHER KING JR ST N		SAINTE PETERSBURG	FL	33701-3126	ZZZ
GEORGE N BUKOTAJ TRUSTEE	577 ROSARIO AVE NE		RENTON	WA	98059-4553	1NX
GREEN LABEL VENTURES LLC	PO BOX 2618		BRANDON	FL	33509-2618	1NW
GREGORY HERMAN	2903 LESLIE RD		TAMPA	FL	33619-2225	1NW
HARRIET ST CLAIR WATSON TRUSTEE	PO BOX 1425		BRANDON	FL	33509-1425	1NW
HARRIS-MCBURNEY COMPANY	2120 N US HIGHWAY 301		TAMPA	FL	33619-2654	ZZZ

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HIGHWAY 60 AND 301 CENTER INC	105 S US HIGHWAY 301 STE 110	TAMPA	FL	33619-3534	ZZZ
HILLSBOROUGH COUNTY	REAL ESTATE DEPT	TAMPA	FL	33601-1110	ZZZ
HOLY CHURCH OF GOD INC	3801 DANNY BRYAN BLVD	TAMPA	FL	33619-1274	INX
HIS-TAMPA FL LLC	HUGHES SUPPLY TAX DEPT	ORLANDO	FL	32802-1713	ZZZ
IBP FLEXSPACE 2 LTD	1400 NW 107TH AVE FL 5	DORAL	FL	33172-2746	804
ILARIA IPPOLITO/ TRUSTEE	20423 AYERS RD	BROOKSVILLE	FL	34604-7064	ZZZ
IMC IMMO TAMPA LLC	LE PLANCHET	GREZIEU LE MARCHE 69610	FRANCE	00000-0000	INW
INTERPHASE INC	2535 SUCCESS DR	ODESSA	FL	33556-3401	ZZZ
INTERSTATE BUSINESS PARK LLC	74 W PARK PL	STAMFORD	CT	06901-2209	1M1
J S AND MARTHA CREWS	8103 E 19TH AVE	TAMPA	FL	33619-2201	INW
JAMES M MARSICANO	2803 LESLIE RD	TAMPA	FL	33619-2319	INW
JEFFREY C SWERDLOW AND JEFFREY-ALLEN INC	2808 29TH AVE E	BRADENTON	FL	34208-7415	ZZZ
JFL 301 LLC	1306 W KENNEDY BLVD	TAMPA	FL	33606-1849	ZZZ
JIMMY A COLVIN	8421 E 27TH AVE	TAMPA	FL	33619-1305	INX
JINUPS! LLC ET AL	UPS CALL CENTER ATTN: SEBBY RUSSO 35 GLENLAKE PKWY NE PMB 281	ATLANTA	GA	30328-3475	663
JOCK BRUCE HALL	291 CHANCE RD	VONORE	TN	37885-3135	1N1
JORGE H RUIZ	3804 N US HIGHWAY 301	TAMPA	FL	33619-1257	INX
JPV INVESTMENT INC	PO BOX 1118	PLANT CITY	FL	33564-1118	ZZZ
KIM HINELINE	8119 E 19TH AVE	TAMPA	FL	33619-2201	INW
KOUKAP LLC	288 WILLSTONE DR	PALM HARBOR	FL	34683-4836	INX
KRAUSS PORTFOLIO LIMITED #2313	PO BOX 1368	CARLSBAD	CA	92018-1368	INZ
LEVANT ENTERPRISES LLC	2803 ORIENT RD	TAMPA	FL	33619-	INW
MARSHALL HAND BESSIE LEE LEDBETTER	3805 N US HIGHWAY 301	TAMPA	FL	33619-1256	ZZZ
MARY GOLDSTEIN AND EDWARD ZWAK	8111 E 19TH AVE	TAMPA	FL	33619-2201	INW
METROPOLITAN LIFE INS	C/O REAL ESTATE TAX SERVICE	TAMPA	FL	33602-5147	ZZZ
MRC HOLDINGS INC	BUSINESS REPLY'S 17C	BALTIMORE	MD	21202-2120	ZZZ
OSMAR SAUMELL AND EUODIA SAUMELL	216 LAKEWAY LN	APOLLO BEACH	FL	33572-2694	INW
PARK OF COMMERCE ASSOCIATION INC	REAL ESTATE TAX ADVISORS LLC	INDIANAPOLIS	IN	46240-0509	1M1
PERFECTION FASTNERS INC	PO BOX 21242	TAMPA	FL	33622-1242	ZZZ
PRAXAIR INC	PO BOX 1118	PLANT CITY	FL	33564-1118	ZZZ
R & B DEVELOPMENT LLC	39 OLD RIDGEBURY RD STE 7	DANBURY	CT	06810-5100	ZZZ
R AND R 2005 LLC	1923 N WEST BYP	SPRINGFIELD	MO	65803-2206	ZZZ
R HANNA INVESTMENT LLC	1923 TAMPA EAST BLVD	TAMPA	FL	33619-3023	ZZZ
R T OAKES INC	3002 N US HIGHWAY 301	TAMPA	FL	33619-2242	ZZZ
RADIANT GROUP LLC	2294 JACOBSON RD	BROOKSVILLE	FL	34601-4807	ZZZ
RAYMOND H GIBSON	1320 E 9TH AVE STE 100	TAMPA	FL	33605-3602	ZZZ
REALTY L C SVO	C/O NATIONAL FACILITIES CORP	SARASOTA	FL	34242-1781	ZZZ
REGIONAL FINANCE COMPANY	PO BOX 163	SANTA MONICA	CA	90403-5708	ZZZ
RING POWER CORPORATION	500 WORLD COMMERCE PKWY	PLANT CITY	FL	33564-0163	ZZZ
ROBERT B LINSEMEYER/ TRUSTEE	513 W MONTEBELLO AVE	PHOENIX	AZ	85013-1848	ZZZ
ROBERT G LOFLEY	PO BOX 1132	MANGO	FL	33550-1132	ZZZ
ROBERT JEFFERSON AND BRENDA MILNER	9704 LORRAINE RD	RIVERVIEW	FL	33578-5010	INX
ROBERT K AND HANITA B CREWS	8101 E 19TH AVE	TAMPA	FL	33619-2201	INW
ROBIN CALLEN	1507 W RIVER LN	TAMPA	FL	33603-2923	ZZZ
RODANTHI K AND PANTELIS KOUMOUNDOUROIS	3407 N US HIGHWAY 301	TAMPA	FL	33619-2249	INX
RONALD DAVID HOUGH	5020 GARDEN LN	TAMPA	FL	33610-5812	INX
RUTH R LINDEMEYER ESTATE OF	C/O NADINE MILLER	BRANDON	FL	33510-3235	ZZZ
RYAN CERMAK	4214 GARDEN LN	TAMPA	FL	33610-7320	ZZZ
SABAL INDUSTRIAL PARK ASSOC INC	C/O CONDOMINIUM ASSOCIATES	TAMPA	FL	33602-5744	ZZZ



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SAN ANN FOOD STORES INC	C/O MR. L M HUGHES	205 S HOOVER BLVD STE 400	TAMPA	FL	33609-3591	INX
SHOOT STRAIGHT TAMPA HOLDING CO	1349 S ORANGE BLOSSOM TRL		APOPKA	FL	32703-7605	ZZZ
SHOOT STRAIGHT TAMPA HOLDING CO	3809 N US HIGHWAY 301		TAMPA	FL	33619-1258	ZZZ
SIMS CRANE & EQUIPMENT CO	PO BOX 11825		TAMPA	FL	33680-1825	ZZZ
SOLOMON PARTNERS INC	205 S HOOVER BLVD STE 101		TAMPA	FL	33609-3594	ZZZ
SOUTHLAND CORPORATION	ATTN: TAX DEPT 26339	1722 ROUTH ST STE 1000	DALLAS	TX	75201-2504	ZZZ
SOUTHWEST FLORIDA WATER MANAGEMENT DISTRICT	2379 BROAD ST		BROOKSVILLE	FL	34604-6899	ZZZ
SPRINGHILL MISSIONARY BAPTIST CHURCH OF TAMPA INC	8119 E DR MARTIN LUTHER KING JR BLVD		TAMPA	FL	33619-1242	INX
STANA AND MILOS ILIAS	4222 GARDEN LN		TAMPA	FL	33610-7320	ZZZ
STAR 103 DEVELOPMENT INC TRUSTEE	9625 WES KEARNEY WAY		RIVERVIEW	FL	33578-0506	ZZZ
STRUBE HOLDINGS INC	784 RUGBY ST		ORLANDO	FL	32804-4969	ZZZ
SUNBEL RENTALS INC	ATTN BONNIE CARTER	PO BOX 410328	CHARLOTTE	NC	28241-0328	ZZZ
TAMPA BAY HOTELS LLC	10610 LOW OAK TER		THONOTOSASSA	FL	33592-3933	1M1
TAMPA ELECTRIC CO	TECO ENERGY CORP TAX DEPT	PO BOX 111	TAMPA	FL	33601-0111	ZZZ
TAMPA IBP LLC	11300 4TH ST N SUITE 250		ST PETERSBURG	FL	33716-2918	1M1
TESHA LLC	3411 N US HIGHWAY 301		TAMPA	FL	33619-2213	ZZZ
TOULA KAPLANERIS	3407 N US HIGHWAY 301		TAMPA	FL	33619-2249	ZZZ
W R B ENTERPRISES INC	1414 W SWANN AVE STE 201		TAMPA	FL	33606-2592	ZZZ
W W GRAINGER INC	C/O MARVIN F POER & CO	3520 PIEDMONT RD NE STE 411	ATLANTA	GA	30305-1512	ZZZ
WATSON COMMERCIAL PROPERTIES LLC	11715 N FLORIDA AVE		TAMPA	FL	33612-5217	ZZZ
WILLIAM BAY CORNWELL	4108 GARDEN LN		TAMPA	FL	33610-7318	ZZZ
WINGHOUSE XV LLC	7491 ULMERTON RD STE 3B		LARGO	FL	33771-4504	ZZZ
ZINK FAMILY LIMITED PARTNERSHIP	5141 FAIRWAY ONE DR		VAIRICO	FL	33596-8229	ZZZ

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SITE_ADDR	SITE_CITY	SITE_ZIP	DBA	STRAP
105 S US HIGHWAY 301	TAMPA	33619	BRANDON PERFORMANCE CENTER	1929.4663000001526800U
0				1929.4222000001525900U
2120 N US HIGHWAY 301	TAMPA	33619	HARRIS MCBURNIEY CO.	1929.13272000001477100U
3602 N US HIGHWAY 301	TAMPA	33619	VETERAN MEMORIAL PARK	1929.12272000001469900U
0 BYPASS CANAL	TAMPA	33619		1929.12272000001468900U
8422 E 27TH AV	TAMPA	33619		1929.121N40000000000030U
3910 N US HIGHWAY 301	TAMPA	33619	EASTSHORE BUSINESS CENTER	1929.12272000001469700U
8118 CARROLL BV	TAMPA	33619		1929.121NW000002000260U
3002 N US HIGHWAY 301	TAMPA	33619	BP AMOCO GAS/CONV STORE	1929.12272000001471200U
3407 N US HIGHWAY 301	TAMPA	33619	MOTEL 301 RESTAURANT	1929.12272000001469100U
8417 E 27TH AV	TAMPA	33619		1929.121NK000000000330U
3411 N US HIGHWAY 301	TAMPA	33619	MOTEL 301	1929.12272000001477400U
3804 N US HIGHWAY 301	TAMPA	33619	DOMINO SELF STORAGE	1929.121NK000002000070U
3805 N US HIGHWAY 301	TAMPA	33619	US 301 MIXED USE AUTO	1929.12272000001467400U
1930 N US HIGHWAY 301	TAMPA	33619	PRAXAIR	1929.12272000001477400U
3901 N US HIGHWAY 301	TAMPA	33619	PARKING FOR SHOOT STRAIGHT	1929.12272000001467300U
1401 N US HIGHWAY 301	TAMPA	33619	SOUTHERN EQUIPMENT COPP	1929.13272000001485600U
8111 E 19TH AV	TAMPA	33619		1929.121NW000002000070U
3939 US HWY 301	TAMPA	33619	CITGO	1929.129R10000000000010U
4811 N US HIGHWAY 301	TAMPA	33610	LA QUINTA INN	1929011M10000000000030U
8800 E ADAMO DR	TAMPA	33619	ADAMO WAREHOUSE	1929.13272000001486400U
1820 TAMPA EAST BV	TAMPA	33619	GRAINGER INDUSTRIAL SUPPLIES	1929.1322000001480400U
3922 N US HIGHWAY 301	TAMPA	33619		1929.121NK000001000011U
3934 N US HIGHWAY 301	TAMPA	33619		1929.121NK000001000012U
1911 N US HIGHWAY 301	TAMPA	33619	CENTER POINT BUSINESS PARK	1929.13272000001485800U
8606 ELM FAIR BLVD	TAMPA	33610	FIVE GUYS	1929011M1000000000121U
0	TAMPA	33610		1929.12272000001466400U
0 N US 301 HY	TAMPA	33619		1929.13272000001487100U
0	TAMPA	33619		1929.13272000001487000U
8108 E 19TH AV	TAMPA	33619		1929.121NW000003000160U
8111 E 20TH AV	TAMPA	33619		1929.121NW000003000090U
1928 TAMPA EAST BV	TAMPA	33619	COMMERCIAL PLASTIC RECYCLING	1929.13272000001479200U
4202 GARDEN LN	TAMPA	33610		1929011M1000000000231U
4210 GARDEN LN	TAMPA	33610		1929011M1000000000220U
4214 GARDEN LN	TAMPA	33610		1929011M1000000000211U
4216 GARDEN LN	TAMPA	33610		1929011M1000000000210U
4220 GARDEN LN	TAMPA	33610		1929011M1000000000201U
4531 OAK FAIR BV	TAMPA	33610	RALPH'S TRANSFER CO.	1929011M10000000000030U
3315 N US HIGHWAY 301	TAMPA	33619	REDNECK TRAILER SUPPLIES	1929.12272000001473600U
1620 TAMPA EAST BV	TAMPA	33619	AIRGAS	1929.13272000001480300U
8108 KRAUSS BV 103-110	TAMPA	33619	KRAUSS WAREHOUSE	1929.13272000001482600U
8119 E 19TH AV	TAMPA	33619		1929.121NW000002000010U
1217 N US HIGHWAY 301	TAMPA	33619	US HIGHWAY 301 WAREHOUSE	1929.13272000001484700U
1201 OLD HOPEWELL RD	TAMPA	33619	OLD HOPEWELL WAREHOUSE	1929.13272000001486100U
0 BYPASS CANAL	TAMPA	33619		1929.12272000001468800U
0	TAMPA	33619		1929.12272000001470000U

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8116 CARROLL BV	TAMPA	33619	1929121NW000002000240U
9205 E ADAMO DR	TAMPA	33619	202919ZTZ00000025183000U
9204 E ADAMO DR	TAMPA	33619	192924ZTZ00000015254000U
9204 E ADAMO DR	TAMPA	33619	192924ZTZ00000015256000U
9208 E ADAMO DR	TAMPA	33619	192924ZTZ00000015255000U
9017 E ADAMO DR	TAMPA	33619	192924ZTZ00000015269000U
4506 OAK FAIR BV	TAMPA	33610	1929011M10000000000007U
4508 OAK FAIR BV	TAMPA	33610	1929011M100000000000020U
8116 E 21ST AV	TAMPA	33619	192912ZZZ00000014713000U
8112 E 21ST AV	TAMPA	33619	192912ZZZ00000014716000U
1910 N US HIGHWAY 301	TAMPA	33619	192913ZZZ00000014775000U
3015 N US HIGHWAY 301	TAMPA	33619	192912ZZZ00000014737000U
4311 N US HIGHWAY 301	TAMPA	33610	192901ZZZ00000014318000U
9201 E ADAMO DR	TAMPA	33619	192924ZTZ00000015257000U
1333 MASSARO BV	TAMPA	33619	192913ZZZ00000014808000U
3909 N US HWY 301	TAMPA	33619	192912ZZZ00000014668000U
0	TAMPA	33610	1929018040000000000010U
1211 OLD HOPEWELL RD	TAMPA	33619	192924ZTZ0000001525901U
8406 RADIO LN	TAMPA	33619	192912ZZZ0000001469000U
2914 N US HIGHWAY 301	TAMPA	33619	192913ZZZ00000014844000U
110 S US 301	TAMPA	33619	1929121NW00000004000000U
1920 N US HIGHWAY 301	TAMPA	33619	192913ZZZ00000014855000U
0	TAMPA	33619	192912ZZZ00000014714000U
8116 E 21ST AV 1/2	TAMPA	33619	192913ZZZ00000014777000U
2230 N US HIGHWAY 301	TAMPA	33619	1929131NZ00000000000110U
0 US 301	TAMPA	33619	1929121NW00000000000000U
8420 E 27TH AV	TAMPA	33619	1929121NW00000000000020U
4401 N US HIGHWAY 301	TAMPA	33610	1929121NW000000014317000U
8419 E 27TH AV	TAMPA	33619	1929121NW00000000000320U
9203 E ADAMO DR	TAMPA	33619	192924ZTZ00000015258000U
1325 MASSARO BV	TAMPA	33619	192913ZZZ00000014807000U
4322 N US HIGHWAY 301	TAMPA	33619	192913ZZZ00000014770000U
1504 TAMPA EAST BV	TAMPA	33619	192901ZZZ00000014315030U
8224 E BROADWAY AV	TAMPA	33619	192901ZZZ00000014308000U
5001 N US HIGHWAY 301	TAMPA	33610	192913ZZZ00000014825000U
1926 N US HIGHWAY 301	TAMPA	33610	192912ZZZ00000014745000U
5003 N US HIGHWAY 301	TAMPA	33610	192901ZZZ00000014306000U
8116 E 19TH AV	TAMPA	33619	192913ZZZ00000014778000U
8117 E 19TH AV	TAMPA	33619	1929121NW00000003000190U
8412 RADIO LN	TAMPA	33619	1929121NW00000002000050U
8421 E 27TH AV	TAMPA	33619	1929121NW00000001468600U
4821 N US HIGHWAY 301	TAMPA	33610	1929121NW00000000000310U
8423 E 27TH AV	TAMPA	33619	1929121NW00000000000360U
8406 E DR MARTIN LUTHER KING	TAMPA	33610	192901ZZZ0000001481801U
1220 N US HIGHWAY 301	TAMPA	33619	192913ZZZ00000014816000U
1902 N US HIGHWAY 301	TAMPA	33619	192913ZZZ00000014776000U
3915 N US HIGHWAY 301	TAMPA	33619	192912ZZZ00000014669000U
8204 SABAL INDUSTRIAL	TAMPA	33619	192912ZZZ00000014723000U

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Description: US 301 from SR 60 to I-4  
Hillsborough County

**Property Owners Site Addresses List**

8610 ELM FAIR BLVD	TAMPA	33610	HOLIDAY INN EXPRESS	1929011M1000000000120U
8100 E BROADWAY AV	TAMPA	33619	BROADWAY WAREHOUSE	1929121NW000001000001U
8327 E DR MARTIN LUTHER KING	TAMPA	33619		1929121NX0000000000010U
8327 E DR MARTIN LUTHER KING	TAMPA	33619	TRUCK REPAIRS	1929121NX0000001000010U
9080 E ADAMO DR	TAMPA	33619	BRANDON FORD	192924Z7Z0000001526200U
1907 N US HIGHWAY 301	TAMPA	33619	CENTER POINT BUSINESS PARK	192913Z7Z000001484900U
1909 N US HIGHWAY 301	TAMPA	33619	CENTER POINT BUSINESS PARK	192913Z7Z000001485100U
1913 N US HIGHWAY 301	TAMPA	33619	CENTER POINT BUSINESS PARK	192913Z7Z000001485700U
2910 OVERPASS RD	TAMPA	33619	OVERPASS WAREHOUSE	192912Z7Z000001474400U
8210 SABAL INDUSTRIAL BLVD	TAMPA	33619	WHITE CAP CONSTRUCTION SUPPLY	192912Z7Z000001472500U
3402 N US HIGHWAY 301	TAMPA	33619	DALLAS BULL PARKING / CELL SITE/SBA TOWERS	192912Z7Z000001470200U
3322 N US 301 HWY	TAMPA	33619	THE DALLAS BULL	192912Z7Z000001470302U
5001 ORIENT RD	TAMPA	33610	FLORIDA STATE FAIR	192901Z7Z000001431300U
1219 N US HIGHWAY 301	TAMPA	33619	SIMS CRANE AND EQUIPMENT	192913Z7Z000001484500U
1206 N US HIGHWAY 301	TAMPA	33619	EASTMEADOW DIST CENTER	192913Z7Z000001486200U
2601 TAMPA EAST BV	TAMPA	33619	COMMONWEALTH - ALTADIS, INC.	1929131NZ0000000000001U
2441 E MEADOW BV	TAMPA	33619	MEADOW WAREHOUSE	1929131NZ0000000000002U
3010 N US HIGHWAY 301	TAMPA	33619	RIVERGATE / FLEX SERVICE	192912Z7Z000001470300U
2122 N US HIGHWAY 301	TAMPA	33619	BRAHMA FASTENERS	192913Z7Z000001477300U
8119 20TH AVE	TAMPA	33619	CHAMPION TRAILERS SALES & SERVICE	1929121NW000003000010U
8113 E 20TH AV	TAMPA	33619	RV & TRUCK DETAILING	1929121NW000003000040U
8350 DR MARTIN LUTHER KING	TAMPA	33619	HESS EXPRESS / QUIPNOS SANDWICH SHOP	192901Z7Z000001431501U
0 STANNUM ST	TAMPA	33619		192913Z7Z000001486700U
3610 N US HIGHWAY 301	TAMPA	33619		192912Z7Z000001469800U
4820 OAK FAIR BLVD	TAMPA	33610	TECO SUBSTATION	1929011M10000000000080U
4343 N US HIGHWAY 301	TAMPA	33610	TELETECH CALL CENTER	192924663000001527800U
111 S US HIGHWAY 301	TAMPA	33610	PUMP STATION #202	1929011M10000000000005U
0 OAK FAIR BLVD	TAMPA	33610	J.W. WATSON TRUCKING	1929121NW000003000100U
2907 LESLIE RD	TAMPA	33619		1929121NW000002000150U
2805 LESLIE RD	TAMPA	33619		1929121NW000002000130U
8101 19TH AV	TAMPA	33610	ELM FAIR FLEX SERVICE	1929011M10000000000031U
8633 ELM FAIR BLVD	TAMPA	33610		1929121NW000002000230U
8110 CARROLL BV	TAMPA	33619		1929121NW000003000130U
8112 CARROLL BV	TAMPA	33619		1929121NW000002000110U
8107 E 19TH AV	TAMPA	33619		1929121NW000003000130U
8119 E DR MARTIN LUTHER KING	TAMPA	33619	SPRINGHILL MISSIONARY BAPTIST	1929121NX000002000030U
8119 E DR MARTIN LUTHER KING	TAMPA	33619	SPRINGHILL MISSIONARY BAPTIST CHURCH	1929121NX000001000030U
8311 E DR MARTIN LUTHER KING	TAMPA	33619		1929121NX000001000020U
8110 E 19TH AV	TAMPA	33619		1929121NW000003000170U
8108 20TH AV	TAMPA	33619	J.W. WATSON TRUCKING.	1929121NW000002000170U
2803 LESLIE RD	TAMPA	33619		1929121NW000005000010U
3801 DANNY BRYAN BV	TAMPA	33610	HOLY CHURCH OF GOD	1929011M10000000000002U
8402 INTERSTATE BUSINESS PW	TAMPA	33610	INTERSTATE BUSINESS FLEX SERVICE	192912Z7Z000001471700U
8102 E 21ST AV	TAMPA	33619		192913Z7Z000001471500U
8108 E 21ST AV	TAMPA	33619	GULF COAST PLUMBING	192913Z7Z000001486800U
1923 TAMPA EAST BV	TAMPA	33619		192913Z7Z000001486800U
9220 STANNUM ST	TAMPA	33610		1929011M1000000000221U

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Hillsborough County

**Property Owners Site Addresses List**

0	TAMPA	33610	19290180T0000000000020U
0	TAMPA	33610	1929019NA0000000000060U
0	TAMPA	33610	1929019NA00000000000160U
4002 GARDEN LN	TAMPA	33610	GREEN SOLUTIONS LAWN CARE & PEST CONTROL
4006 GARDEN LN	TAMPA	33610	1929011M100000000000233U
4108 GARDEN LN	TAMPA	33610	1929011M100000000000232U
4112 GARDEN LN	TAMPA	33610	1929011M100000000000230U
4222 GARDEN LN	TAMPA	33610	1929011M100000000000200U
4539 OAK FAIR BLVD	TAMPA	33610	PURNELL FURNITURE SERVICES
4543 OAK FAIR BV	TAMPA	33610	INTERSTATE BUSINESS PARK WAREHOUSE
4661 OAK FAIR BLVD	TAMPA	33610	INTERSTATE BUSINESS PARK WAREHOUSE
4701 OAK FAIR BLVD	TAMPA	33610	INTERSTATE BUSINESS PARK WAREHOUSE
4720 OAK FAIR BLVD 1	TAMPA	33610	INTERSTATE BUSINESS PARK WAREHOUSE
4720 OAK FAIR BLVD 2	TAMPA	33610	INTERSTATE BUSINESS PARK WAREHOUSE
4727 OAK FAIR BLVD	TAMPA	33610	INTERSTATE BUSINESS PARK WAREHOUSE
4758 OAK FAIR BLVD	TAMPA	33610	INTERSTATE BUSINESS PARK WAREHOUSE
5014 GARDEN LN	TAMPA	33610	INTERSTATE BUSINESS PARK
5016 GARDEN LN	TAMPA	33610	NOAHS PRESCHOOL
5020 GARDEN LN	TAMPA	33610	1929011M1E000000000121U
8001 E BROADWAY AV	TAMPA	33619	1929019NA0000000000050U
8407 LAUREL FAIR CIR	TAMPA	33610	19291327Z0000001482700U
8460 ELM FAIR BLVD	TAMPA	33610	192901804000000000014U
			INTERSTATE BUSINESS PARK WAREHOUSE

## **Appendix C – USFWS Concurrence Letter**



United States Department of the Interior

U. S. FISH AND WILDLIFE SERVICE

7915 BAYMEADOWS WAY, SUITE 200  
JACKSONVILLE, FLORIDA 32256-7517

IN REPLY REFER TO:

FWS Log No. 04EF1000-2016-I-0189

February 3, 2016

Nicole Selly  
District 7 Environmental Specialist  
Florida Department of Transportation  
11201 N. McKinley Drive  
Tampa, Florida 33612-6456

**RE: US 301 (SR43) from SR 60 (Adam Drive) to I-4 (SR400) Project Development and Environment Study  
Hillsborough County, Florida.  
WPI No.: 430050-1**

Dear Ms. Selly:

The U.S. Fish and Wildlife Service (Service) has completed its review of the Draft Wetland Evaluation and Biological Assessment Report (WEBAR) for the Project Development and Environment (PD&E) study to evaluate the proposed widening of US 301 (SR 43) to six lanes from SR 60 (Adamo Drive) to the southern end of the eastbound I-4 (SR 400) on- and off-ramps in Hillsborough County. Project goals are to document the need for additional capacity within the study corridor and to evaluate the costs and impacts associated with providing additional capacity for which federal permits will be needed. The Service provides the following comments in accordance with section 7 of the Endangered Species Act of 1973 (Act), as amended (16 U.S.C. 1531 et seq.).

The Service received a request from the Florida Department of Transportation (FDOT) on January 11, 2016, for review of the draft WEBAR for the proposed project. The draft document includes determinations of "may affect, not likely to adversely affect" for the wood stork (*Mycteria americana*) and the eastern indigo snake (*Drymarchon couperi*) and a no effect determination for the West Indian Manatee (*Trichechus manatus*). The proposed project crosses the Tampa Bypass Canal which contains a weir that excludes manatees from accessing the canal,

consequently no direct impacts this species are expected. It is our understanding that wetland impacts to suitable wood stork foraging areas will be re-evaluated and compensation for unavoidable impacts will be provided within a Service approved mitigation or conservation bank during the permitting process. The Service has reviewed the information provided and FDOT's effects determinations for potential impacts to species listed under the Endangered Species Act and provide the following comments.

**Eastern Indigo Snake (*Drymarchon couperi*)**

A 'may affect, but not likely to adversely affect' determination for the eastern indigo snake was made due to the fact that suitable habitat present in the project area is minimal, eastern indigo snakes were not been observed during field surveys within the project study area, area of impact is less than 25 acres of xeric habitat supporting less than 25 occupied gopher tortoise burrows and FDOT's commitment to implementing the Service's Standard Protection Measures for the Indigo Snake during construction of the project. The Service would like to request that if or when an eastern indigo snake is observed at the project site that the Service is contacted within 24 hours before work continues at the project site. The proposed project is within a highly urbanized area where impacts to the species habitat have already taken place. Based on our review of the information provided, our records for eastern indigo snake observations, and FDOT's commitment to implement the Standard Protection Measures for the Eastern Indigo Snake the Service concurs with a 'may affect, but not likely to adversely affect' determination for the Eastern indigo snake.

**Wood Stork (*Mycteria americana*)**


Suitable foraging habitat (SFH) for woods storks is present within the proposed project study area. The draft WEBAR identified one (1) active colony sites within a 15-mile radius of the proposed project site, the project is more than 2,500 feet from a colony site, and estimated project impacts are greater than 0.5 acres of SFH. To reach a "may affect, not likely to adversely affect" determination for the wood stork, FDOT commits to re-initiating informal Section 7 consultation prior to construction and compensate for the loss of suitable foraging habitat within the core foraging areas (CFA). The Service recommends and prefers that mitigation for this species is "like-for-like" habitat within the same ecological CFA. The Service has reviewed the information provided and FDOT's commitments, as well as available observation and species presence data and concurs with a 'may affect, but not likely to adversely affect' determination for this species.

Thank you for considering the effects of your proposed project on fish and wildlife, and the ecosystems upon which they depend. Although this does not represent a biological opinion as described in Section 7 of the Act, it does fulfill the requirements of the Act. Should changes to



the proposed project occur or new information regarding fish and wildlife resources become available, further consultation with the Service should be initiated to assess any or further potential impacts. If you have any questions, please contact Lourdes Mena at (904)731-3119.

Sincerely,



Jay B. Herrington  
Field Supervisor