

WEST BUSCH BOULEVARD

Corridor Alternatives & Strategies Report



DRAFT FINAL August 31, 2018

SR 580/Busch Blvd. from N. Dale Mabry Hwy. to N. Nebraska Ave.

Financial Project ID Number: 435908-1-22-01



Appendix E

Public Involvement



Section 1. Public Involvement Plan.....E-3

Section 2. PAG Meetings.....E-479

 2.1 PAG Meeting 1.....E-480

 2.2 PAG Meeting 2.....E-550

 2.3 PAG Meeting 3.....E-617

 2.4 PAG Meeting 4.....E-663

Section 3. Public Visioning Workshop.....E-748

Section 4. Alternatives Public Meeting.....E-815

Section 5. Local Coordination Meeting Summaries.....E-920

 5.1 City Summary.....E-921

 5.2 County Summary.....E-959

 5.3 MPO Summary.....E-992

Section 6. Newsletters.....E-1033

 6.1 Newsletter #1.....E-1034

 6.2 Newsletter #2.....E-1037

Section 7. Additional Public Comments.....E-1040

 7.1 Emailed Comments.....E-1041

 7.2 WikiMapping Comments.....E-1048

Section 1

Public Involvement Plan



SR 580/Busch Boulevard From
North Dale Mabry Highway to
North Nebraska Avenue

Hillsborough County, Florida

Corridor Study

Financial Project Number:
435908-1-22-01

May 2017

Public Involvement Plan



PUBLIC INVOLVEMENT PLAN

Project Name: SR 580/Busch Boulevard Corridor Study

Project Limits: From North Dale Mabry Highway to North Nebraska Avenue

County: Hillsborough County, Florida

Financial Project ID Number: 435908-1-22-01

Bridge Numbers: N/A

In accordance with Part 1, Chapter 11 of the Project Development and Environment (PD&E) Manual, this Public Involvement Plan is submitted to the District Environmental Management Office (DEMO) for review and approval.

Submitted by: _____
Carol Barker, P.E.
Project Manager
CH2M HILL

Date: _____

Approved by: _____
Kenneth Spitz, AICP
Transportation Planning Manager
Florida Department of Transportation District 7

Date: _____

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT.

Table of Contents

	<u>Page</u>
PUBLIC INVOLVEMENT PLAN SUMMARY.....	1
I. DESCRIPTION OF PROPOSED IMPROVEMENTS.....	2
II. BACKGROUND.....	3
III. PROJECT GOALS	4
IV. IDENTIFICATION OF AGENCIES AND AFFECTED PUBLIC.....	4
V. OUTREACH ACTIVITIES	8
VI. PUBLIC INVOLVEMENT STRATEGIES	11
VII. PUBLIC OUTREACH ACTIVITY SCHEDULE	15
VIII. PUBLIC MEETING FOLLOW UP	15
IX. EVALUATION OF PUBLIC INVOLVEMENT PROGRAM	16
X. PUBLIC INVOLVEMENT DURING THE PD&E AND / OR DESIGN PHASE	16

APPENDICES

Appendix A: Property Owner Mailing List – not included / Property Owner Boundary Map

Appendix B: Elected and Appointed Officials Mailing List

Appendix C: Agencies and Other Interested Parties Mailing List

Appendix D: Project Advisory Group Roster-not included

Public Involvement Plan Corridor Study

SR 580/Busch Boulevard
From North Dale Mabry Highway to North Nebraska Avenue
Financial Project ID: 435908-1-22-01
Federal Aid Project Number: N/A
Hillsborough County, Florida

Successful public involvement is based on building trust and understanding, as well as working to achieve public consensus on infrastructure investments. This requires a process that is characterized by technical competence, honesty and integrity, and good listening skills. These principles will create the framework for the public involvement that will occur for the SR 580/Busch Boulevard Corridor Study.

Activities implemented as part of this PIP will allow people living and working within the project study area, and other interested parties, to contribute to the decision-making process and to influence the choices that are made regarding proposed transportation improvements for SR 580/Busch Boulevard from North Dale



Mabry Highway to North Nebraska Avenue. The goal of the PIP is to generate interest in the project scope and schedule, and to outline the public involvement process. This will ensure that the general public, local communities, agencies and officials; and other interested parties understand the nature of the project and its benefits by providing an open, two-way line of communication, presenting project information in an easy-to-understand format, and by making project information easily accessible. Gaining community consensus among the varied stakeholders in the study area is essential to achieving a successful project outcome. The keys to gaining community consensus are:

- Include project stakeholders early in the study process and include their input to define the alternatives;
- Develop a transparent process to screen the alternatives and determine which viable alternative(s) to further evaluate through the public involvement process and environmental analysis; and
- Maintain regular communication with project stakeholders and provide multiple opportunities and methods for participating in the process and providing comments, in order to shape the future needs and financial viability of the selected alternative.

Public involvement efforts will seek to actively involve key stakeholders; to listen to ideas and concerns; and to document and incorporate input received during the study process. The PIP also will ensure that the study reflects the diverse values and needs of the communities this project is designed to benefit.

The fundamental objectives of the PIP are to ensure that the concerns and issues of those living and working within the study area, and those in close proximity who may be affected by the project, are identified; that stakeholders are given opportunities to review and comment on the findings of the alternatives analysis; and that stakeholder concerns are addressed in the analysis process. The PIP provides an outline for:

- Early and continuous involvement of stakeholders;
- Reasonable availability of technical and other project information;
- Collaborative input on alternative transportation improvements for the study area and the criteria against which they will be measured and evaluated;
- Open access to the decision-making process.



I. DESCRIPTION OF PROPOSED IMPROVEMENTS

Financial Project ID: 435908-1-22-01

Federal Aid Project Number: N/A

ETDM Number: N/A

Project Name: SR 580/Busch Boulevard Corridor Study

Project Limits: SR 580 from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles (see Figure 1 on the following page)

County: Hillsborough County, Florida

Proposed Activity: The Florida's Department of Transportation, District 7 (D7) has identified the need to analyze, evaluate, and document travel-related problems, needs and issues along SR 580/Busch Boulevard from North Dale Mabry Highway to North Nebraska Avenue. A study will be conducted to identify the cause and characteristics of the transportation issues within the corridor; these may include capacity, traffic operations, safety, access and egress, freight movements, transit, bicycles and pedestrian movements. The study will document both the current and forecasted-future travel conditions and needs and identify workable solutions for those needs.

Concurrent with the study, a Resurfacing, Restoration and Rehabilitation (RRR) design project will proceed under FPID 437530-1-32-01; the limits of the RRR design is SR 580 from East of Armenia Avenue to West of Florida Avenue, a distance of approximately 1.3 miles. The RRR improvements include: mill and resurfacing, replacement of sidewalk where deficient, and filling in missing gaps where existing right of way is available.

Figure 1: PROJECT LOCATION MAP



PROJECT CONTACT INFORMATION

Key project contacts for project information and public involvement activities are listed below:

Brian L. Shroyer
Multimodal Project Manager
Florida Department of Transportation
11201 N. McKinley Drive, MS 7-500
Tampa, FL 33612
813-975-6449
Brian.Shroyer@dot.state.fl.us

Kristen Carson
Public Information Officer
Florida Department of Transportation
11201 N. McKinley Drive, MS 7-110
Tampa FL 33612
813-975-6060
Kristen.Carson@dot.state.fl.us

II. PROJECT BACKGROUND

The SR 580/Busch Boulevard corridor is an east-west urban principal arterial connecting and serving local commuters and visitors between Dale Mabry Hwy and SR 583, a distance of 6.8 miles. . The SR 580/Busch Blvd corridor study area includes portions of unincorporated Hillsborough County and the City of Tampa and is the main route to the Busch Gardens Amusement Park (located approximately 2.15 miles east of the project limits at McKinley Drive). The corridor supports a variety of land uses and demographics, including schools, churches, large and small businesses with direct access to and from SR 580, transit services, pedestrians, bicyclists and many small communities some with limited English proficiency. The corridor also serves a large commuter population as a connection between Veterans Expressway to the west and I-75 to the east.

There is a CSX railroad line located south of and adjacent to the SR 580 corridor. The close proximity of the RR constrains any improvements to the south and contributes to the congestion during the daily freight train routes that impede north south movement across the SR 580 corridor. The congestion on the corridor continues to rise along with crashes, both minor and fatal. The purpose of the project is to study opportunities to address the decreasing level of service and operational inefficiencies, while balancing the use of the corridor by all modes of transportation, including vehicle, transit, pedestrian, bicycle, and rail. Finally, the study will document the vision for all future improvements in the corridor that address the travel-related issues.

III. PROJECT GOALS

The following goals and objectives have been identified for this study:

- Identify the travel related needs and issues within the corridor.
- Proactively engage with key stakeholders to establish the vision for the corridor and identify solutions to the corridor issues and improve operational efficiency within the SR 580 corridor.
- Document the vision for all future improvements within the SR 580/Busch Blvd corridor to maximize the “livability” for all users.



Efficient Transportation Decision Making

A portion of the project, from Dale Mabry Hwy to Florida Avenue was previously screened through the Efficient Transportation Decision Making (ETDM) screening process (ETDM 3096). There are two additional entries in the ETDM tool, but the screening was not completed and no ETDM number was initiated:

- As part of the LRTP, the MPO entered Busch Blvd from Florida Ave. to 56th St.
- A project from Dale Mabry Hwy to Nebraska was input into the tool as an “area of interest” in 2016 by D7, but no official screening was initiated.

Advance notification is not included in this study, but can be included as an optional service as needed.

IV. Identification of Agencies and Affected Public

The PIP includes conducting a formal, comprehensive community inventory to develop a stakeholder database. A variety of resources will be used to compile the database to include key community members and leaders who can provide information about the community, local, regional, state and federal agencies, elected and appointed officials, adjacent property owners, business owners and operators, chamber of commerce leaders, and neighborhood association presidents. A detailed list of the elected and appointed officials is provided in **Appendix B**. This list will be updated to reflect changes in interagency collaboration or as other public agency stakeholders are identified.

The following local, regional, state, or federal agencies having a concern in this project, due to jurisdictional review or expressed interest, have been identified. The project does not include the need for the AN process to be initiated, but it can be added as an optional service. As other concerned public agencies are identified throughout the study, they will also be listed and contacted.

State

Florida Department of Agriculture and Consumer Services – Division of Forestry
Florida Department of Economic Opportunity
Florida Department of Environmental Protection
Florida Department of Environmental Protection – Southwest Office
Florida Department of State, Division of Historical Resources
Florida Fish and Wildlife Conservation Commission
Florida Department of Transportation Environmental Management Office

Federal

Federal Aviation Administration – Airports District Office
Federal Emergency Management Agency
Federal Railroad Administration
U.S. Department of Transportation – Federal Highway Administration
U.S. Army Corps of Engineers
U.S. Department of Commerce – National Marine Fisheries Services, Habitat Conservation Division
U.S. Department of Health and Human Services – National Center for Environmental Health
U.S. Department of Housing and Urban Development
U.S. Department of Interior – Bureau of Land Management
U.S. Department of Interior – U.S. Fish and Wildlife Services
U.S. Department of Interior – National Park Services
U.S. Department of Interior – U.S. Geological Survey
U.S. Department of Interior – Bureau of Indian Affairs
U.S. Environmental Protection Agency
U.S. Forestry Services
Centers for Disease Control and Prevention

Regional

Southwest Florida Water Management District
Tampa International Airport
Tampa Bay Area Regional Transportation Authority
Tampa Bay Regional Planning Council
Hillsborough County City-County Planning Commission
CSX Railroad
The Hillsborough Area Transit Authority (HART)
Tampa Bay Area Regional Transportation Authority (TBARTA)
Hillsborough School Transportation

Native American Tribal Officials

Micosukee Tribe of Indians of Florida
Muscogee (Creek) Nation
Poarch Band of Creek Indians
Seminole Nation of Oklahoma
Seminole Tribe of Florida

ELECTED AND APPOINTED OFFICIALS

Federal Delegations

The Honorable Bill Nelson	U.S. Senator
The Honorable Marco Rubio	U.S. Senator
The Honorable Kathy Castor	U.S. Representative, District 14

Florida State Senators

The Honorable Dana D. Young	Florida State Senator, District 18
The Honorable Darry Ervin Rouson	Florida State Senator, District 19

Florida State Representatives

The Honorable Shawn Harrison	Florida State Representative, District 63
The Honorable Sean Shaw	Florida State Representative, District 61

LOCAL GOVERNMENT

Hillsborough County

Sandra Murman	Commissioner, District 1, Vice Chair
Victor Christ	Commissioner, District 2
Lesley "Les" Miller, Jr.	Commissioner, District 3, Chaplain
Stacy White	Commissioner, District 4, Chairman
Ken Hagan	Commissioner, District 5
Pat Kemp	Commissioner, District 6
Al Higginbotham	Commissioner, District 7
Mike Merrill	County Administrator
Pat Frank	Clerk of the Circuit Court & Comptroller
John Lyons	Public Works Director
Sheriff David Gee	Hillsborough County Sheriff
Jeff Eakins	Hillsborough County School Superintendent
Adam Gormly	Development Services Director
Dennis Jones	Hillsborough County Fire Rescue Fire Chief
Brandon Wagner	Operations & Legislative Affairs
Preston Cook	Emergency Management
Wanda Sloan	Neighborhood Relations
Forest Turbiville	Conservation & Environmental Lands Director
Eric Johnson	Strategic Planning & Grants
Beth Alden, AICP	Metropolitan Planning Organization Director
George Cassidy	Public Utilities

Tampa

Bob Buckhorn	Mayor
Dennis Rogero	Chief of Staff
Mike Suarez	Council Member, District 1
Charlie Miranda	Council Member, District 2
Yvonne Yolie Capin	Council Member, District 3
Harry Cohen	Council Member, District 4
Frank Reddick	Council Member, District 5
Guido Maniscalco	Council Member, District 6
Luis Viera	Council Member, District 7
Shirley Foxx- Knowles	City Clerk
Jean W. Duncan	Transportation & Stormwater Services
Douglas Lewis	City Manager
Brad Baird	Administrator, Public Works & Utility Services
Michael L. Haworth	Police Chief
Eric Ward	Fire Chief

Other Interested Parties:

Civic and Business Organizations

Hillsborough County Tourist Development Council
Greater Tampa Chamber of Commerce
Tampa Bay Hispanic Chamber of Commerce
Tampa Bay Black Chamber of Commerce
Tampa Bay Cycle
Visit Tampa Bay

Area Schools, Library and Organizations/Associations

Chamberlain High School Busch Gardens
Chamberlain High School
Adams Middle School
Forest Hills Elementary School
Forest Hills Youth Baseball
Hillsborough County Library – Northwest Library
Forest Hills Village Home Owners Association
Forest Hills Home Owners Association
Forest Hills Neighborhood Association
Department of Labor Career Source of Tampa Bay
Sulphur Springs Community Center
Eckerd Community Center
Department of Children and Families

Environmental Non-Government Agencies

Audubon of Florida
Audubon of the Western Everglades
Defenders of Wildlife – Florida
Gopher Tortoise Council
Florida Trail Association
The Nature Conservancy
Sierra Club, Regional Office
Rails to Trails Conservancy
Florida Audubon Society-Florida State Office
1000 Friends of Florida
Defenders of Wildlife

As other organizations, public/special interest parties are identified or express an interest in the study, they will be added to the contact list.

Additionally, those individuals whose property lies, in whole or in part, within 300 feet of the centerline of each project alternative and property owners adjacent to the project will be included in a separate property owners' mailing list.

V. OUTREACH ACTIVITIES

The use of multiple media services will be used to carry public notices, news releases, public service announcements, news items, and interviews regarding the proposed transportation improvement(s) and to solicit public input into the project development process. Additional media contacts will be coordinated through the FDOT's Public Information Office (PIO).

Newspapers	
Tampa Tribune - TBO 200 S. Parker Street Tampa, FL 33606 813-259-8225 / 1-800-527-2773 Mike Salinero: MSalinero@tampatrib.com Ted Jackovics: TJackovics@tampatrib.com	CENTRO Tampa / (Hispanic) 202 S. Parker Street Tampa, FL 33603 813-259-8037 Myriam Silva-Warren mwarren@centrotampa.com
Tampa Bay Times 1000 N. Ashley Drive, Suite 104 Tampa, FL 33602 813-226-3366 / 727-893-8785 Mike Brassfield: brassfield@tampabay.com local@tampabay.com	Tampa Bay Newspapers (TBN) 9911 Seminole Blvd. Seminole, FL 33772 727-397-5563, ext. 306 Tiffany Razzaro: trazzano@tbnweekly.com
Tampa Bay Business Journal 4890 W. Kenned Boulevard, Ste. 850 Tampa, FL 33609 813-873-8225 tampabay@bizjournals.com	The Weekly Challenger (African American) P.O. Box 35130 St. Petersburg, FL 33705 727.896.2922 contactus@theweeklychallenger.com

Television	
Bay News 9 (Brighthouse only) 700 Carillon Parkway, Suite 9 St. Petersburg, FL 33716 727-329-2400 desk@baynews9.com	WFLA, Channel 8 (NBC) 200 S. Parker Street Tampa, FL 33606 813-221-5788 (newsroom) news@wfla.com
City of Tampa Television (CTTV Home) Ch. 15 202 West 7th Avenue Tampa, FL 33602 813-274-7028	WTSP, Channel 10 (CBS) 11450 Gandy Boulevard N. St. Petersburg, FL 33702 727-577-1010 desk@tampabay10.com
WTVT Channel 13 (FOX) 3213 W. Kennedy Boulevard Tampa, FL 33609 813-876-1313 (main) 813-870-9630 (newsroom) news@wvtv.com	WEDU, Channel 3 (PBS) Berkman Family Broadcast Center 1300 North Boulevard Tampa, FL 33607 813-254-9338 outreach@wedu.org
WTOG, Channel 44 (CW) 365 105 th Terrace NE St. Petersburg, FL 33716 Public Affairs: 727-570-4458	WUSF, Channel 16 (PBS) 4202 E. Fowler Avenue, TVB 100 Tampa, FL 33620 813-974-8700 news@wusf.org
WRMD, Channel 49 (Telemundo)	WVEA, Channel 62 (Univision)

4107 Spruce Street, Suite 250 Tampa, FL 33607 813-319-4949	2610 W. Hillsborough Avenue Tampa, FL 33614 813-998-3660 noticiastampa@entravision.com
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Radio	
CBS Radio Tampa Bay 92.5FM WYUU (Spanish), 94.1FM WLLD, 94.1FM-HD2 WSJT, 98.7FM WHFS, 99.5FM WQYK, 104.7FM WRBQ, 1010AM WHFS 5510 W. Gray Street, Suite 130 Tampa, FL 33619 727-579-1925	Clear Channel Communications Tampa Bay 93.3FM WFLZ, 95.7FM WBTP, 97.9FM WXTB, 100.7FM WMTX, 103.5FM WFUS, 105.9FM WMTX, 106.5FM WCTQ, 620AM WDAE, 970AM WFLA, 1250AM WHNZ 4002 W. Gandy Boulevard Tampa, FL 33611 813-832-1000
Community Radio 88.5 WMNF 1210 Dr. Martin Luther King Jr. Boulevard Tampa, FL 33619 813-238-8001	Cox Media Group Tampa 94.9FM WWRM, 97.1FM WSUN, 101.5FM WPOI, 102.5FM WHPT, 105.5FM WDUV, 107.3FM WXGL 11300 4th Street N St. Petersburg, FL 33716 727-579-2000
Davidson Media Group 96.1 FM / 1150 AM WTMP (African American) 407 Howard Avenue, Suite 200 Tampa, FL 33606 813-259-9867	Genesis Communications (News Talk Florida) 820AM WWBA, 1040AM WHBO, 1060AM WIXC, 1190AM WAMT, 1470AM WMGG (Spanish) 800 8th Avenue SE Largo, FL 33771
Genesis 680 680AM WGES (Spanish) 2005 Pan Am Circle, Suite 250 Tampa, FL 33607 813-637-8000	Radio Tropical Inc. 1300AM WQBN (Spanish) 4023 N. Armenia Avenue, Suite 400 Tampa, FL 33607 813-871-1333
Salem Communications 570AM WTNB, 910AM WTWD, 760AM WLCC (Spanish), 860AM WGUL, 930AM WLSS 5211 W Laurel Street, Suite 101 Tampa, FL 33607	Tan Talk Radio Network 1340 AM WTAN 706 N. Myrtle Avenue Clearwater, FL 33755 727-441-3311
WUSF Public Media 89.7FM WUSF 4202 E. Fowler Avenue TVB100 Tampa, FL 33620 813-974-8700	1590AM WRXB 3551 42nd Avenue S, Suite B-106 St. Petersburg, FL 33711 727-865-1591

VI. PUBLIC INVOLVEMENT STRATEGIES

In addition to working with the media, a number of different techniques will be used throughout the study. A brief description of these techniques is listed below.

Elected Officials and Agency Kickoff

An important part of the study process is the coordination with environmental resource agencies with permitting and/or approval authority for the project, and with other governmental and non-governmental organizations with an interest in the project. The purpose of the Elected Officials and Agency Kickoff meeting is to provide an opportunity for the study team and the agencies to work cooperatively together to develop a transportation improvement that meets the needs of the area while being a responsible steward of the natural and human environment.

Elected Officials and Agency Kickoff Meeting Site

It is anticipated that the Agency Kickoff Meeting will be held at the Hillsborough County Library, North Tampa Branch, 8916 North Boulevard, Tampa, Florida 33604.



Bilingual Information

There are communities within the project area that are Spanish speaking. To accommodate these stakeholders, the notification cards for the Corridor Visioning Workshop and Alternatives Public Workshop will be bilingual, the newsletters will include a note directing Spanish speakers to the FDOT, and there is an opportunity for bilingual meeting handouts. A Spanish translator will be available at public meetings to listen to comments and concerns, answer questions, and provide project information.

Newsletters

Informational and easy-to-understand newsletters will be distributed via email and/or mail to elected and appointed officials, agencies, property owners within 300 feet of the project and interested persons. The newsletters will give a detailed overview of the project, notice of public meetings and other study activities, a summary of the findings, and the next steps of the project development. A note will be added to the newsletter, in Spanish, directing all Spanish speakers that need additional information to contact the FDOT. It is anticipated that there will be four newsletters distributed for this study at the following project milestones:

1. After the Officials/Agencies Kick-Off Meeting
2. Prior to the Corridor Visioning Public Workshop
3. Prior to the Alternatives Public Workshop
4. After the Alternatives Public Workshop

Newsletters will also be submitted to FDOT PIO for posting on the project website.

News/Press Releases:

News/press releases notifying the public of meetings and other study activities will be submitted to the FDOT Public Information Office no less than seven (7) days in advance of public meetings.

Public Announcements:

In order to distribute study information to a broader segment of the community, newsletters can be made available to homeowners associations, cultural and civic groups, business organizations, churches, schools

and others to publish in existing newsletters and on websites. Any supplemental correspondence will first be reviewed and approved by the District's Public Information Office.

Direct Mail/Email List

The following will be contacted by either direct mail or email in order to obtain input throughout the project development process and/or to provide project information. The mailing list will be updated at regular intervals throughout the Corridor Study.

- Those people whose property lies in whole or in part within 300 feet on either side of the right-of-way line of any alternative for the proposed project will be notified, as required by Section 339.155, F.S., as well as other local citizens who may be impacted by the construction of this project. This list will be compiled utilizing current Geographical Information System (GIS) data obtained from the Hillsborough County Property Appraisers Office.
- Local elected and appointed officials or individuals who request to be placed on the mailing list for this project.
- Notice will be sent by mail and/or email to public and private groups, organizations, businesses and/or individuals who request to be placed on the mailing list for this project.

Project Website

A project website will be maintained by FDOT throughout the Corridor Study. Project information including the project schedule, project location map, meeting announcements and presentations, and project documents will be posted on the site. The site, www.fdotd7studies.com, will include contact information for the FDOT Project Manager and Public Information Director for the study. Links will be provided to allow the public to submit comments and sign up to be added to the mailing list. Newsletters and other public notices will be available. All materials to be uploaded on the website will first be provided to the FDOT for review and approval.

Presentations to Local Officials:

Presentations will be given to local officials and agencies, such as the Hillsborough Metropolitan Planning Organization (MPO), HART, CSX, Hillsborough County and the City of Tampa to apprise local officials of the project status, specific location and design concepts, and to receive their comments.

Public Hearing: *(Not applicable - Optional Service)*

Project Advisory Group (PAG)

A Project Advisory Group (PAG), comprised of select agency and community representatives, will be formed to: provide a forum for local experts to share their local knowledge about ongoing planning efforts; past successes and failures that may impact this study; and existing issues and concerns along the project corridor. Once finalized, a listing of the PAG members will be included in **Appendix D**. The PAG will meet four (4) times over the course of the study. The PAG will serve as an advisory board to help the FDOT and the study team gather local input and then disseminate information back to the impacted stakeholders and communities.

Corridor Visioning Workshop:

A Corridor Visioning Workshop will be held to solicit input from the public on the goals and objectives of the study, and to receive input on the full range of potential improvements to be evaluated.

Alternatives Public Workshop:

The Alternatives Public Workshop will present to the public the results of the study to date and allow the public to express concerns, ask questions, and make comments to be included in the project files. The Workshop will follow an informal, open house format. The meeting handout will encourage the public to review the displays, ask questions, and discuss issues with the project representatives.

The Consultant will mail or email newsletters announcing the Alternatives Public Workshop to elected and appointed officials, agencies, property owners and interested citizens on the mailing list a minimum of 14 days prior to the Workshop. Members of the PAG and other project stakeholders may also be asked to post notification of the Workshop on their websites and social media pages.

Informal Meetings:

In addition to scheduled meetings, participation in other meetings with the public, elected officials, special interest groups, or public agencies is expected to occur. The purpose of these meetings will be to apprise the attendees of the project status, specific location and design concepts, and to receive input.

Meeting Facilities Evaluation

Public meeting facilities will be convenient to the study area in locations where people normally congregate, and which are accessible to public transportation. Potential meeting sites will be visited and reviewed for suitability, including whether they are appropriate to the meeting purpose and format, and whether they comply with the requirements of the *Americans with Disabilities Act (ADA)*.

Special considerations will also be given to ensure the facilities comply with the *2005 Jessica Lunsford Act*. Public school facilities, grades K-12, will not be used for public meetings while students are present. School facilities may be used, however, with authorization from an appropriate school principal when no students are present. Alternate facilities to be considered include community centers, churches, colleges, universities, and private schools.

A summary of potential meeting sites will be prepared and presented to the FDOT for review and approval. Sites that are identified during the course of the study that are suitable for meetings will be listed below during subsequent updates to this document.

Meetings will be held in compliance with the "Project Development and Environment Manual," 23 CFR 771 and Section 339.155, F.S.

Public Meeting Site

Public meetings will be held at an appropriate facility convenient to the study area and meeting all of the other aforementioned meeting facility requirements. Prior to site selection, the meeting location will be inspected for size, adequate parking, transit access and compliance with ADA requirements. Several potentials have been identified and reflected in Table 2. These locations will require on-site inspections and additional coordination before making a final selection.



Potential Public Meeting Sites

Name and Location	Distance from Project	Phone	Comments
North Tampa Branch Library 8916 North Boulevard Tampa, FL 33604	.1 Mile	813-273-3652	Large meeting room for 80, plus smaller rooms, ample parking, security, transit access, ADA compliant, easy to find.
Grace Lutheran Church 3714 W Linebaugh Avenue Tampa, FL 33618	.1 Mile	813-961-8747	Large meeting room for 100, plus smaller room, ample parking, transit access, ADA compliant, easy to find.
Florida Department of Transportation 11201 N. McKinley Drive Tampa, FL 336612	4.4 Miles	813-975-6000	Large meeting room for 150, ample parking, transit access, ADA compliant, easy to find, after hours meeting would require security.

Non-Discrimination Laws and Americans with Disabilities Act Compliance

Title VI and Americans with Disabilities Act Compliance of 1964: In accordance with Title VI of the Civil Rights Act of 1964, all reasonable steps will be taken to ensure that persons with limited English proficiency receive the “language assistance necessary to afford them meaningful access to programs and services”. Upon request, arrangements will be made to provide an appropriate interpreter and translation services free of charge. Based on FDOT guidance dated March 21, 2013, the non-discrimination language shown below will be added to all meeting notices, letters ads, newsletters, boards, and other project-related materials.

FDOT Approved Non-Discrimination Language – March 21, 2013

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Christopher Speese, Public Involvement Coordinator, at Christopher.Speese@dot.state.fl.us or call (813) 975-6405 or (800) 226-7220 at least seven (7) working days in advance of the meeting.

Assistance for Spanish Speaking Attendees: Persons who require translation services (free of charge) should contact Brian Shroyer, Environmental Management Office Project Manager, at brian.shroyer@dot.state.fl.us, or call (813) 975-6445 or (800) 226-7220 at least seven (7) working days in advance of the meeting. Spanish-speaking personnel will be on hand to assist those in need.

Americans with Disabilities Act of 1990: Notification of FDOT’s intent to comply with the Americans with Disabilities ACT (ADA) will be provided in the public meeting announcements, invitations, newsletters and by the election of an ADA compliant public meeting site.

NEPA Assignment Language - December 16, 2016

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT.

VII. Public Outreach Activity Schedule

SR 580/Busch Boulevard from North Dale Mabry Highway to North Nebraska Avenue Corridor Study																			
PUBLIC INVOLVEMENT PLAN SCHEDULE																			
ACTIVITY	2017										2018								
	Mar	Apr	May	June	July	Aug	Sept	Oct	Nov	Dec	Jan	Feb	Mar	April	May	June	July	Aug	Sept
Notice to Proceed - Project Begins	★																		
Develop Public Involvement Plan																			
Mailing List																			
Elected Officials/Agency Kick-off Meeting																			
Project Advisory Group (PAG) Meetings																			
Corridor Visioning Workshop																			
Alternatives Public Meeting																			
Final Corridor Alternatives & Strategies Report																			

The study team will also conduct two briefing meetings prior to public meetings to review materials. The first briefing will be conducted with FDOT planning staff and project manager in order to fine-tune the meeting materials. The second briefing will be conducted with FDOT executive staff, planning staff and project manager to gain final approval of meeting materials.

VIII. PUBLIC MEETING/WORKSHOP FOLLOW-UP

The following procedures will occur after the public meetings/workshops:

Responses

Responses to all written comments received as a result of public meetings, and questions and comments not answered during the public meeting, will be made in writing.

Recommendation Notice

Not Applicable

Comments and Coordination Report

A *Comments and Coordination Report* will be produced and submitted at the conclusion of the study, in a bound booklet with a cover, containing, at a minimum, all documentation regarding public participation performed throughout the study period. This report shall include all comments and responses received from the public, as well as records of coordination with local officials and agencies, and public meetings; the verbatim transcript from the public hearing (optional service); proof of publication of legal ads; public hearing certification (optional service); newsletters; sign-in sheets; comment forms; public meeting materials; presentations and displays; and all public correspondence. The *Comments and Coordination Report* will be submitted with the final engineering documents. A summary of the public involvement efforts shall be included with the final engineering document.

Public Meeting Summary

A document containing 11"x17" reproductions of public meeting display boards, concept plans, presentation materials, newsletters, and sign-in sheets will be prepared. The summary will also include a cover page with a project description, FPID No.; a project location map; information about the meeting, such as the date and location, as well as the total number of people in attendance; all the alternatives that were shown; a list of all reports that were displayed; and all public correspondence. A CD containing the electronic files in PDF format will also be included.

IX. EVALUATION OF PUBLIC INVOLVEMENT PLAN

A public involvement evaluation process will be developed to assess the effectiveness of the public involvement efforts utilized throughout the Corridor Study. This process will include identification of the public involvement tools, establishment of performance measures, performance evaluations, identification of improvement strategies and a survey to be filled out by the public to measure the effectiveness and quality of the graphics, presentations and overall public involvement activities.

X. PUBLIC INVOLVEMENT DURING THE PD&E AND / OR DESIGN

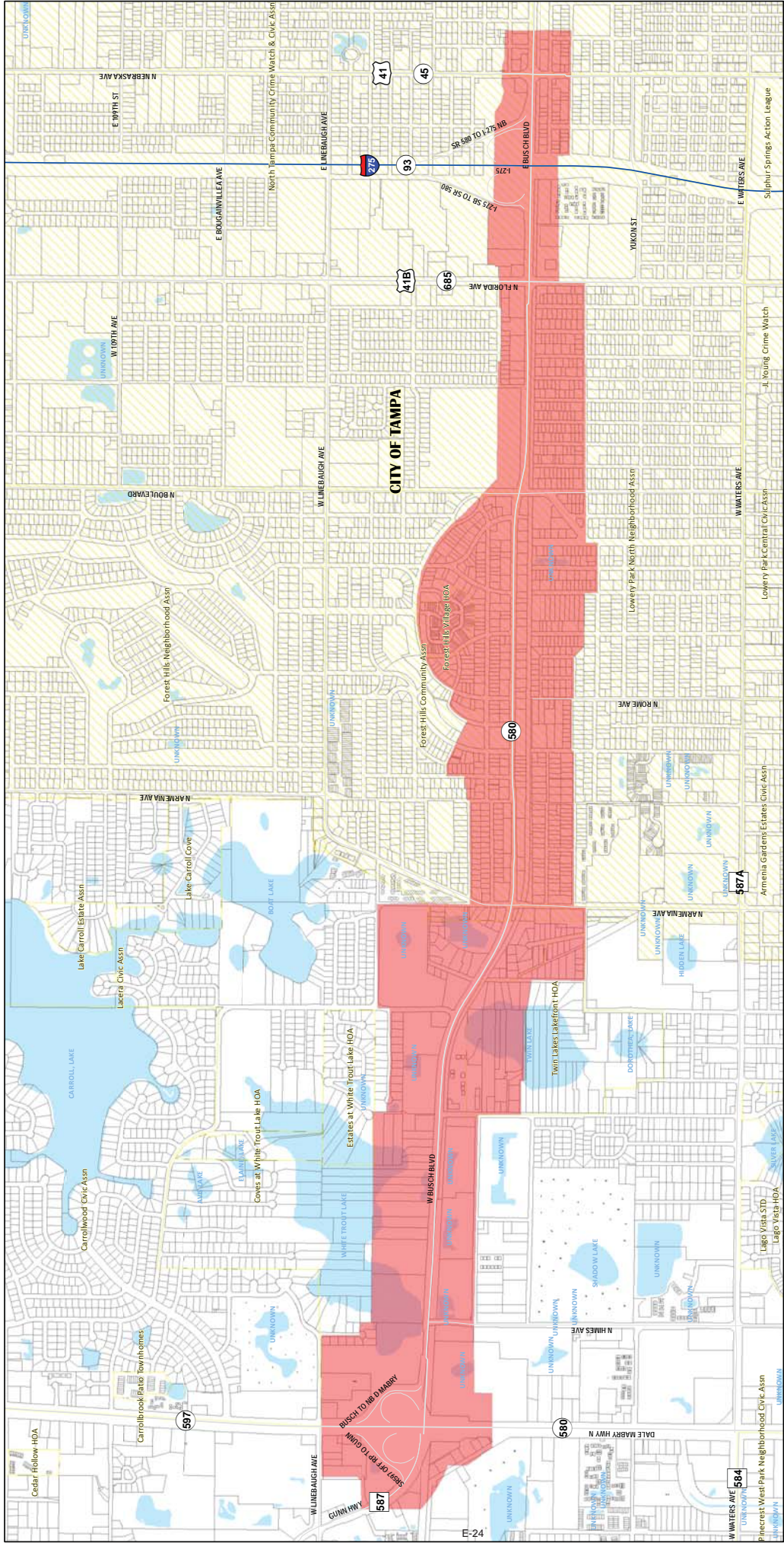
It is anticipated that the Design Phase Project Manager will maintain the appropriate level of public involvement activities throughout the design process. These public involvement activities may include additional coordination meetings with local government and environmental permitting agencies, work sessions, small group meetings, and public meetings, as directed by the FDOT.

APPENDICES

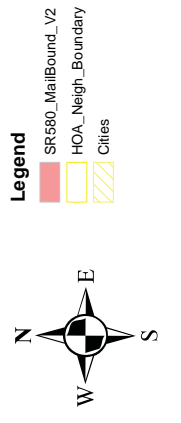
Appendix A:

Property Owner Boundary Map

Property Owner Mailing List



SR 580/Busch Blvd from N Dale Mabry to N Nebraska
Hillsborough County, Florida
Mailing Boundary



Property Owner Mailing List will be inserted once available.

Appendix B:

Elected and Appointed Officials

Mailing List

First Name	Last Name	Title	Agency	Address	City	State	Zip Code	Phone	E-mail
Federal Elected Officials									
Marco	Rubio	Senator	US Senate	402 South Monroe Street Suite 2105E	Tallahassee	FL	32399	(850) 599-9100	Ryan_Patmintra@rubio.senate.gov
Bill	Nelson	Senator	US Senate	801 N. Florida Ave. 4th Floor 4144 N. Armenia Ave. Suite 300	Tampa	FL	33602	(813) 225-7040	brandon_kaufman@billnelson.senate.gov
Kathy	Castor	Congresswoman	US Representative, District 14		Tampa	FL	33607	(813) 871-2817	kathy.castro@mail.house.gov
State Elected Officials									
Rick	Scott	Governor	State of Florida	The Capitol, 400 S. Monroe St.	Tallahassee	FL	32399	(850) 488-7146	Rick.Scott@eog.myflorida.com
Dana D.	Young	Senator	Florida Senate, District 18	1211 N. Westshore Blvd. Suite 409	Tampa	FL	33607	(813) 281-5507	young.dana@flsenate.gov
Darryl Ervin	Rouson	Senator	Florida Senate, District 19	535 Central Avenue Suite 302	St. Petersburg	FL	33701	(727) 822-6828	rouson.darryl@flsenate.gov
Shawn	Harrison	Representative	Florida House of Representatives, District 63	15310 Amberly Drive Suite 215	Tampa	FL	33647	(813) 910-3277	shawn.harrison@myfloridahouse.gov
Sean	Shaw	Representative	Florida House of Representatives, District 61	508 West Dr. Martin Luther King Jr. Boulevard	Tampa	FL	33603	(813) 224-1947	sean.shaw@myfloridahouse.gov

Salutation	First Name	Last Name	Title	Agency	Address	City	State	Zip Code	Phone	E-mail
City of Tampa Elected Officials										
The Honorable	Bob	Buckhorn	Mayor	City of Tampa	306 East Jackson Street	Tampa	FL	33602	(813) 274-8251	bob.buckhorn@tampagov.net
The Honorable	Mike	Suarez	Council Member, District 1	City of Tampa	315 East Kennedy Boulevard	Tampa	FL	33602	(813) 274-7072	mike.suarez@tampagov.net
The Honorable	Charlie	Miranda	Council Member, District 2	City of Tampa	315 East Kennedy Boulevard	Tampa	FL	33602	(813) 274-7074	charlie.miranda@tampagov.net
The Honorable	Yvonne	Yolie	Council Member, District 3	City of Tampa	315 East Kennedy Boulevard	Tampa	FL	33602	(813) 274-8133	yvonne.capin@tampagov.net
The Honorable	Harry	Cohen	Council Member, District 4	City of Tampa	315 East Kennedy Boulevard	Tampa	FL	33602	(813) 274-8134	harry.cohen@tampagov.net
The Honorable	Frank	Reddick	Council Member, District 5	City of Tampa	315 East Kennedy Boulevard	Tampa	FL	33602	(813) 274-8189	frank.reddick@tampagov.net
The Honorable	Guido	Maniscalco	Council Member, District 6	City of Tampa	315 East Kennedy Boulevard	Tampa	FL	33602	(813) 274-7071	guido.maniscalco@tampagov.net
The Honorable	Luis	Viera	Council Member, District 7	City of Tampa	315 East Kennedy Boulevard	Tampa	FL	33602	(813) 274-7073	Luis.Viera@tampagov.net

Salutation	First Name	Last Name	Title	Agency	Address	City	State	Zip Code	Phone	E-mail
Hillsborough County Elected Officials										
The Honorable	Sandra	Murman	Commissioner- District 1	Hillsborough County	601 E. Kennedy Blvd.	Tampa	FL	33602	(813) 272-5470	murmans@hillsboroughcounty.org
The Honorable	Victor	Christ	Commissioner- District 2	Hillsborough County	601 E. Kennedy Blvd.	Tampa	FL	33602	(813) 272-5452	christv@hillsboroughcounty.org
The Honorable	Lesley Las*	Miller Jr.	Commissioner- District 3	Hillsborough County	601 E. Kennedy Blvd.	Tampa	FL	33602	(813) 272-5720	llemj@hillsboroughcounty.org
The Honorable	Stacy	White	Commissioner- District 4	Hillsborough County	601 E. Kennedy Blvd.	Tampa	FL	33602	(813) 272-5740	whites@hillsboroughcounty.org
The Honorable	Ken	Hagan	Commissioner- District 5	Hillsborough County	601 E. Kennedy Blvd.	Tampa	FL	33602	(813) 272-5725	hagan@hillsboroughcounty.org
The Honorable	Pat	Kemp	Commissioner- District 6	Hillsborough County	601 E. Kennedy Blvd.	Tampa	FL	33602	(813) 272-5730	kemp@hillsboroughcounty.org
The Honorable	Al	Higginbotham	Commissioner- District 7	Hillsborough County	601 E. Kennedy Blvd.	Tampa	FL	33602	(813) 272-5735	higginbotham@hillsboroughcounty.org
The Honorable	Pat	Frank	Clerk of the Circuit Court & Comptroller	Hillsborough County	601 E. Kennedy Blvd.	Tampa	FL	33602	(813) 272-5470	frankp@hillsboroughcounty.org
The Honorable	David	Gee	Hillsborough County Sheriff	Hillsborough County	2008 E. 8th Ave	Tampa	FL	33605	(813) 247-8060	dgee@hcsa.tampa.fl.us
The Honorable	Susan	Valdes	School Board- District 1	Hillsborough County School Board	901 East Kennedy Blvd.	Tampa	FL	33602	(813) 272-4052	YamiliDiaz@sdhcs.k12.fl.us
The Honorable	Sally	Harris	School Board- District 2	Hillsborough County School Board	901 East Kennedy Blvd.	Tampa	FL	33602	(813) 272-4052	YamiliDiaz@sdhcs.k12.fl.us
The Honorable	Chndy	Stuart	School Board- District 3	Hillsborough County School Board	901 East Kennedy Blvd.	Tampa	FL	33602	(813) 272-4052	YamiliDiaz@sdhcs.k12.fl.us
The Honorable	Melissa	Snively	School Board- District 4	Hillsborough County School Board	901 East Kennedy Blvd.	Tampa	FL	33602	(813) 272-4052	YamiliDiaz@sdhcs.k12.fl.us
The Honorable	Tamara	Shamburger	School Board- District 5	Hillsborough County School Board	901 East Kennedy Blvd.	Tampa	FL	33602	(813) 272-4052	YamiliDiaz@sdhcs.k12.fl.us
The Honorable	April	Griffin	School Board- District 6	Hillsborough County School Board	901 East Kennedy Blvd.	Tampa	FL	33602	(813) 272-4052	YamiliDiaz@sdhcs.k12.fl.us
The Honorable	Lynn	Gray	School Board- District 7	Hillsborough County School Board	901 East Kennedy Blvd.	Tampa	FL	33602	(813) 272-4052	YamiliDiaz@sdhcs.k12.fl.us
The Honorable	Lesley Las*	Miller Jr.	Chair	Hillsborough (MPO)- Board of Directors	601 E Kennedy Blvd. 18th floor	Tampa	FL	33602	(813) 272-5940	llemj@hillsboroughcounty.org
The Honorable	Henry	Cohen	Vice Chair	Hillsborough (MPO)- Board of Directors	601 E Kennedy Blvd. 18th floor	Tampa	FL	33602	(813) 272-5940	henry.cohen@tampapov.net
The Honorable	Ken	Hagan	Board Member	Hillsborough (MPO)- Board of Directors	601 E Kennedy Blvd. 18th floor	Tampa	FL	33602	(813) 272-5940	hagan@hillsboroughcounty.org
The Honorable	Sandra	Murman	Board Member	Hillsborough (MPO)- Board of Directors	601 E Kennedy Blvd. 18th floor	Tampa	FL	33602	(813) 272-5940	murmans@hillsboroughcounty.org
The Honorable	Stacy	White	Board Member	Hillsborough (MPO)- Board of Directors	601 E Kennedy Blvd. 18th floor	Tampa	FL	33602	(813) 272-5940	whites@hillsboroughcounty.org
The Honorable	Guido	Maniscalco	Board Member	Hillsborough (MPO)- Board of Directors	601 E Kennedy Blvd. 18th floor	Tampa	FL	33602	(813) 272-5940	guido.maniscalco@tampapov.net
The Honorable	Luis	Viera	Board Member	Hillsborough (MPO)- Board of Directors	601 E Kennedy Blvd. 18th floor	Tampa	FL	33602	(813) 272-5940	Luis.Viera@tampapov.net
The Honorable	Kim	Leinbach	Board Member	Hillsborough (MPO)- Board of Directors	601 E Kennedy Blvd. 18th floor	Tampa	FL	33602	(813) 272-5940	kleinbach@templeterrace.com
The Honorable	Rick	Lott	Board Member	Hillsborough (MPO)- Board of Directors	601 E Kennedy Blvd. 18th floor	Tampa	FL	33602	(813) 272-5940	rlott@penicly.gov.com
The Honorable	Wallace	Bowers	Board Member	Hillsborough (MPO)- Board of Directors	601 E Kennedy Blvd. 18th floor	Tampa	FL	33602	(813) 272-5940	wallace.bowers1@verizon.net
The Honorable	Joe	Lopano	Board Member	Hillsborough (MPO)- Board of Directors	601 E Kennedy Blvd. 18th floor	Tampa	FL	33602	(813) 272-5940	jo.pano@tampaaiport.com
The Honorable	Paul	Anderson	Board Member	Hillsborough (MPO)- Board of Directors	601 E Kennedy Blvd. 18th floor	Tampa	FL	33602	(813) 272-5940	panderson@tampaport.com
The Honorable	Joseph	Wagner	Board Member	Hillsborough (MPO)- Board of Directors	601 E Kennedy Blvd. 18th floor	Tampa	FL	33602	(813) 272-5940	joel@tampa-xway.com
The Honorable	Trent	Green	Board Member	Hillsborough (MPO)- Board of Directors	601 E Kennedy Blvd. 18th floor	Tampa	FL	33602	(813) 272-5940	trentgreen@uf.edu
The Honorable	Chndy	Stuart	Board Member	Hillsborough (MPO)- Board of Directors	601 E Kennedy Blvd. 18th floor	Tampa	FL	33602	(813) 272-5940	YamiliDiaz@sdhcs.k12.fl.us

Appendix C:

Agencies and Other Interested Parties

Mailing List

First Name	Last Name	Title	Agency	Address	City	State	Zip Code	Phone	E-mail
City of Tampa Agency Representatives									
Dennis	Rogero	Chief of Staff	City of Tampa	307 East Jackson Street	Tampa	FL	33602	(813) 274-7360	dennis.rogero@tampagov.net
Shirley	Fox-Knowles	City Clerk	City of Tampa	315 East Kennedy Boulevard	Tampa	FL	33602	(813) 274-8397	shirley.foxknowles@tampagov.net
Jean W.	Duncan	Transportation & Stormwater Services	City of Tampa	306 East Jackson Street Tampa, FL 33602	Tampa	FL	33602	(813) 274-8333	jean.duncan@tampagov.net
Brad	Baird	Administrator, Public Works & Utility Services	City of Tampa	306 East Jackson Street	Tampa	FL	33602	(813) 274-7883	brad.baird@tampagov.net
Eric	Ward	Police Chief	City of Tampa	411 North Franklin Street	Tampa	FL	33602	(813) 276-3200	eric.ward@tampagov.net
Thomas	Forward	Fire Chief	City of Tampa	808 East Zack Street	Tampa	FL	33602	(813) 274-7011	thomas.forward@tmapagov.net

First Name	Last Name	Title	Agency	Address	City	State	Zip Code	Phone	E-mail
Hillsborough County Agency Representatives									
Mike	Merrill	County Administrator	Hillsborough County	601 E. Kennedy Blvd.	Tampa	FL	33602	(813) 272-5750	merrillm@hillsboroughcounty.org
John	Lyons	Public Works Director	Hillsborough County	601 E. Kennedy Blvd.	Tampa	FL	33602	(813) 307-1868	lyonsj@hillsboroughcounty.org
Adam	Gormly	Development Services Director	Hillsborough County	601 E. Kennedy Blvd.	Tampa	FL	33602	(813) 272-5600	gormlya@hillsboroughcounty.org
Dennis	Jones	Hillsborough County Fire Rescue Fire Chief	Hillsborough County	9450 E. Columbus Dr.	Tampa	FL	33619	(813) 272-6600	patriciom@hillsboroughcounty.org
Brandon	Wagner	Operations & Legislative Affairs	Hillsborough County	601 E. Kennedy Blvd.	Tampa	FL	33602	(813) 276-2640	wagnerb@hillsboroughcounty.org
Preston	Cook	Emergency Management	Hillsborough County	9450 E. Columbus Dr.	Tampa	FL	33619	(813) 236-3800	cook@hillsboroughcounty.org
Wanda	Sloan	Neighborhood Relations	Hillsborough County	601 E. Kennedy Blvd.	Tampa	FL	33602	(813) 272-5860	SloanW@hillsboroughcounty.org
Forest	Turbville	Conservation & Environmental Lands Director	Hillsborough County	10940 McMullen Road Bell Creek Preserve	Riverview	FL	33569	(813) 672-7876	glyardy@hillsboroughcounty.org
Beth	Alden, AICP	Metropolitan Planning Organization Director	Hillsborough County	601 E. Kennedy Blvd.	Tampa	FL	33602	(813) 273-3774 x318	aldenb@plancom.org
George	Cassady	Public Utilities	Hillsborough County	601 E. Kennedy Blvd.	Tampa	FL	33602	(813) 209-3009	cassadyg@hcf.gov.net

Appendix D:

Project Advisory Group Roster

Project Advisory Roster will be inserted once available.



Florida Department of Transportation
District 7
11201 N. McKinley Drive
Tampa, Florida 33612



ELECTED OFFICIALS / AGENCY KICK-OFF MEETING SUMMARY

THURSDAY, JUNE 29, 2017 from 10:00 AM to 12:00 PM

At

North Tampa Branch Library - 8916 North Boulevard Tampa, Florida

The combined Elected Officials and Agency Kick-Off Meeting for the West Busch Boulevard (SR 580) Corridor Study was held on Thursday, June 29, 2017, from 10:00 a.m. to 12:00 p.m. at the North Tampa Branch Library, 8916 North Boulevard, Tampa.

Notification of Meeting

Meeting invitation letters were sent by email to elected officials, government officials, and local agencies. The invitation letters included the meeting date, time and location, and a summary of the project.

A Florida Administrative Register (FAR) ad was published in Volume 43/No. 117 227 on June 16, 2017. The Florida Department of Transportation posted meeting information on the FDOT Public Notices website.

Copies of the invitation letter, FAR ad, and FDOT Public Notice website ad are included in Appendix A.

Meeting Handouts

A copy of the meeting agenda, Project Advisory Group (PAG) handout and a comment form were distributed to the attendees at the sign-in table. Participants were encouraged to discuss their thoughts and concerns regarding the corridor study with project team members.

A copy of the meeting agenda and the PAG Overview handout is available in Appendix B

Meeting Information and Materials

The Kick-Off Meeting began at 10:00 a.m. Parking was available to accommodate all of the attendees, including the disabled. Directional signs were placed along West Busch Boulevard and North Boulevard to assist with way-finding to the meeting site.

The meeting was conducted in an informal forum. Project staff followed a meeting agenda that was distributed to attendees. After the introductions, the goals of the meeting were discussed and the project team provided an overview of the project, including the project schedule and brief discussion of other on-going and concurrent projects in the corridor. This was followed by

a working break for the meeting attendees to examine graphics of the corridor; these included an aerial plot of the West Busch Boulevard project corridor (one was mounted, the other large scale plot was laid out on a table) and a mounted graphic depicting the context classifications of the corridor. During this session, attendees were able to identify known issues and interact with the members of the project team to discuss the project and answer questions.

After the working break, the project team facilitated a session for the attendees to provide input on the existing character of the corridor, what are the most significant challenges and opportunities for improvement within the project limits.

The meeting was closed with an invitation to meeting attendees to volunteer for the Project Advisory Group (PAG). It was explained that the PAG participants will play a key role in developing a vision for the corridor that will guide proposed short and long-term recommendations.

Copies of the display boards, meeting photos, and flip-chart ideas captured during the meeting are included in Appendix C.

Meeting Attendance

Nine (9) people attended the meeting. Also attending were two (2) members of the project team from the FDOT District Seven Office and six (6) project consultants. A copy of the sign-in sheets is included in Appendix D.

Project Advisory Group (PAG)

Nine (9) meeting attendees signed-up to participate in the PAG. A copy of the PAG sign-up sheet is available in Appendix E.

Attendee Comments

Three comment forms were received at the meeting. A copy of the comments received is included in Appendix F.

Summary

The following provides a summary of the items discussed at the meeting:

- Brian Shroyer of the Florida Department of Transportation (FDOT) opened the meeting by thanking attendees and presented an overview of the project.
- FDOT, Officials, Agency personnel and consultant team were introduced. Carol Barker, CH2M Hill, discussed the goals off the Kick-Off Meeting and the corridor study.

- Carol Barker gave a project overview and briefly discussed the design RRR project that is concurrent and is a subset of the corridor study limits.
- Meeting attendees were given 15 minutes to view the board and roll plot. Meeting staff were available to answer questions. Post-it notes were available on the table so meeting attendees could write notes and place them on the roll plot. The comments provided on the post-it notes have been uploaded to the [WikkiMapping website](#) for the corridor study.
- Attendees participated in a Corridor Concerns and Ideas Exercise. Attendees were asked three (3) questions and their answers were shared on a flip chart. (1) How would you describe the existing West Busch Boulevard Corridor, in one or two words? (2) What is the most significant challenge in the corridor? (3) One or two ideas you have to address the challenges? Participant responses can be found in Appendix C.
- Carol Barker explained the role of the PAG and attendees were encouraged to join.
- Carol Barker discussed upcoming PAG meetings and public meetings.
- The project website and WikiMapping were discussed.
- Brian Shroyer, FDOT, provided closing remarks. The meeting ended at 12:00 p.m.

Discussion

Stakeholder concerns noted during discussion included:

- Traffic concerns regarding the congestion on West Busch Boulevard;
- Operational changes to give pedestrians more time to cross;
- Pedestrian and bike safety concerns;
- Poor lighting at West Busch Boulevard and North Boulevard. It is dark when school kids cross before 7:00 a.m. and during Friday night football games;
- Access management for businesses;
- Vehicles speeding on West Busch Boulevard;
- Driveway size considerations for emergency vehicles, specifically fire trucks;
- Right of Way issues and constraints;
- Identifying additional PAG members;
- HART bus stop locations.

END OF MEETING SUMMARY

This meeting summary was prepared by Karen Harrell, Public Involvement Coordinator. If you feel clarifications are necessary, or if this differs from your understanding, please notify Karen by telephone at 352-257-1651 or by e-mail at Karen.Harrell@qcausa.com within (5) working days upon receipt of this summary.

Appendices

Appendix A:

- Elected Official Invitation Letter
- Agency Invitation Letter
- Florida Administrative (FAR) Ad
- FDOT Public Notices Website Ad

Appendix B:

- Project Advisory Group (PAG) Handout
- Meeting Agenda

Appendix C:

- Welcome Board
- Title VI English / Spanish
- Context Classifications
- Project Aerial
- Meeting Photos
- Group Questions and Answers

Appendix D:

- Elected Officials / Agency Stakeholder Sign-In
- Project Team Sign-In

Appendix E:

- Project Advisory Group Sign-Up

Appendix F:

- Completed Comment Forms

Appendix A

Date: June 06, 2017

Subject: Officials/Agencies Kick-Off Meeting
West Busch Boulevard (SR 580) Corridor Study
from North Dale Mabry Highway to North Nebraska Avenue
Hillsborough County, Florida
FDOT Financial Project ID Number: 435908-1-22-01

Dear Elected / Appointed Official:

The Florida Department of Transportation (FDOT), District Seven has scheduled an officials/agencies kick-off meeting to discuss the above referenced project. The West Busch Blvd Corridor study limits extend from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The study area includes portions of incorporated Hillsborough County and the City of Tampa. This notice has been sent to other public officials and agency representatives interested in the project. The meeting is scheduled for June 29, 2017, from 10:00 AM to Noon at the **North Tampa Branch Library, 8916 North Boulevard, Tampa, Florida 33604.**

The purpose of the Corridor Study is to work with the community, stakeholders and a Project Advisory Group (PAG) to develop a vision and identify needs and issues in the corridor. At the kick-off meeting, we hope to identify PAG participants from the area to help develop a vision for the corridor that will guide proposed short and long-term recommendations. This may include issues related to capacity, traffic operations, safety, access and egress, mobility, freight movemetns, transit, bicycles, and pedestrian movements.

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220 at least seven (7) working days in advance of the meeting.

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT.

If you require further information concerning this project, please contact Brian Shroyer, Florida Department of Transportation Project Manager at 813-975-6449 or by email at Brian.Shroyer@dot.state.fl.us.

Sincerely,
Paul Steinman, P.E.
District Seven Secretary
Florida Department of Transportation

Date: June 06, 2017

Subject: Officials/Agencies Kick-Off Meeting
West Busch Boulevard (SR 580) Corridor Study
from North Dale Mabry Highway to North Nebraska Avenue
Hillsborough County, Florida
FDOT Financial Project ID Number: 435908-1-22-01

Dear Agency Partner:

The Florida Department of Transportation (FDOT), District Seven has scheduled an officials/agencies kick-off meeting to discuss the above referenced project. The West Busch Blvd Corridor study limits extend from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The study area includes portions of incorporated Hillsborough County and the City of Tampa. This notice has been sent to other public officials and agency representatives interested in the project. The meeting is scheduled for June 29, 2017, from 10:00 AM to Noon at the **North Tampa Branch Library, 8916 North Boulevard, Tampa, Florida 33604.**

The purpose of the Corridor Study is to work with the community, stakeholders and a Project Advisory Group (PAG) to develop a vision and identify needs and issues in the corridor. At the kick-off meeting, we hope to identify PAG participants from the area to help develop a vision for the corridor that will guide proposed short and long-term recommendations. This may include issues related to capacity, traffic operations, safety, access and egress, mobility, freight movemetns, transit, bicycles, and pedestrian movements.

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220 at least seven (7) working days in advance of the meeting.

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT.

If you require further information concerning this project, please contact Brian Shroyer, Florida Department of Transportation Project Manager at 813-975-6449 or by email at Brian.Shroyer@dot.state.fl.us.

Sincerely,
Brian Shroyer
Multimodal Project Manager
Florida Department of Transportation

Florida Administrative Register Ad:

The Florida Department of Transportation (FDOT), District Seven, is holding a West Busch Boulevard (SR 580) Corridor Study Kick-Off Meeting for elected officials and agency representatives.

DATE AND TIME: June 29, 2017, 10:00 AM to Noon.

PLACE: North Tampa Branch Library, 8916 North Boulevard, Tampa, FL 33604

GENERAL SUBJECT MATTER TO BE CONSIDERED: The Florida Department of Transportation, District Seven, is holding a West Busch Boulevard (SR 580) Corridor Study Kick-Off Meeting for elected officials and agency representatives within the study limits. The limits of the project extend from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The corridor study area includes portions of unincorporated Hillsborough County and the City of Tampa.

The purpose of the study is to work with the community, stakeholders and a Project Advisory Group (PAG) to develop a vision and identify needs and issues. At the kick-off meeting, we hope to identify PAG participants from the area to help develop a vision for the corridor that will guide proposed short and long-term recommendations. This may include issues relating to capacity, traffic operations, safety, access and egress, mobility, freight movements, transit, bicycles, and pedestrian movements. The FDOT Financial Project ID Number for the project is 435908-1-22-01.

A copy of the agenda may be obtained by contacting: Mr. Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us.

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220 at least seven (7) working days in advance of the meeting.

Comuniquese Con Nosotros

Nos importa mucho la opinión del público sobre el proyecto. Si usted tiene preguntas o comentarios, o si simplemente desea más información, por favor comuniquese con nuestra representante, Lilliam Escalera, al (813) 975-6403, Departamento de Transportación de Florida, 11201 N. McKinley Dr., Tampa, FL 33612, Lilliam.Escalera@dot.state.fl.us

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT.

FDOT Public Notices Website:

KICK-OFF MEETING FOR ELECTED OFFICIALS AND AGENCY REPRESENTATIVES

District: Seven

Meeting Type: Meeting

Date: June 29, 2017

Time: 10 AM to Noon

Location Name: North Tampa Branch Library

Street Address: 8916 North Boulevard

City: Tampa, FL 33604

Purpose: The Florida Department of Transportation, District Seven, is holding a West Busch Boulevard (SR 580) Corridor Study Kick-Off Meeting for elected officials and agency representatives within the study limits. The limits of the project extend from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The corridor study area includes portions of unincorporated Hillsborough County and the City of Tampa.

The purpose of the study is to work with the community, stakeholders and a Project Advisory Group (PAG) to develop a vision and identify needs and issues. At the kick-off meeting, we hope to identify PAG participants from the area to help develop a vision for the corridor that will guide proposed short and long-term recommendations. This may include issues relating to capacity, traffic operations, safety, access and egress, mobility, freight movements, transit, bicycles, and pedestrian movements. A copy of the agenda may be obtained by contacting: Mr. Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us.

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220 at least seven (7) working days in advance of the meeting.

Comuniquese Con Nosotros

Nos importa mucho la opinión del público sobre el proyecto. Si usted tiene preguntas o comentarios, o si simplemente desea más información, por favor comuniquese con nuestra representante, Lilliam Escalera, al (813) 975-6403, Departamento de Transportación de Florida, 11201 N. McKinley Dr., Tampa, FL 33612, Lilliam.Escalera@dot.state.fl.us

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT.

Primary Contact: Brian Shroyer, FDOT Project Manager

Primary Phone: (813) 975-6449

Primary E-mail: Brian.Shroyer@dot.state.fl.us

Additional Contact: Christopher Speese, Public Involvement Coordinator

Additional Phone: (813) 975-6405

Additional E-mail: Christopher.Speese@dot.state.fl.us

Appendix B



PROJECT ADVISORY GROUP OVERVIEW

1. What is a Project Advisory Group?

A Project Advisory Group (PAG) is established early in a corridor study and is composed of key stakeholders representing local cities, counties, agencies, jurisdictions and organizations within the study area that can help ensure that the explicit needs and concerns of the corridor are addressed. This important group serves as a special advisory resource to the Florida Department of Transportation (FDOT) and the project team; they will gather local input and then disseminate information back to the impacted stakeholders and communities to ensure that any improvements within the corridor continue to serve as vital elements of the community's transportation system. The PAG will also provide policy guidance and technical assistance to FDOT and the project team as the common goals and objectives of the study are defined, issues are identified, and potential solutions are developed.

The West Busch Blvd Corridor Study PAG will meet four (4) times over the course of the study and provides an opportunity for the study team to share project information and receive critical input on a regular basis.

2. What is the Role of the PAG?

The PAG is not an independent decision-making board, but is advisory in nature and will assist the FDOT and the consultant team by providing jurisdictional, policy, technical and administrative guidance throughout the study with the following goals in mind:

- Identify the interests of the stakeholders within the project limits.
- Establish relationships and open the lines of communication.
- Work collectively to bring diverse ideas and approaches together.
- Work as a team to achieve the common goals identified during the study.

To maximize success, PAG members should be honest, open-minded, responsive, cooperative and respectful of all group members.

3. What is the Purpose of PAG Meetings?

The purpose of PAG meetings is to bring together the FDOT, the consultant team, and the PAG members to establish an effective project planning or "partnering" team. The PAG will serve as an advisory board to help the FDOT and the study team gather local input and then disseminate information back to the impacted stakeholders and communities. This flow of information will aid in identifying issues, needs, and potential solutions that can be evaluated and documented the final Corridor Study.

4. What is a Corridor Study?

In general, corridor studies are a planning process used within the engineering community to assess existing conditions, identify transportation-related needs for a corridor and develop a preferred future condition. Corridor studies are used to:

- Define acceptable levels of access and mobility,
- Determine transportation system needs to support surrounding land uses,

- Consolidate and control access points,
- Identify safety concerns on the corridor, and
- Identify operational deficiencies and promote operational efficiency

Potential benefits of the West Busch Blvd Corridor Study may include:

- Maximizing efficiency and life-span of existing infrastructure,
- Improved safety conditions,
- Development of coordinated land uses,
- Access or mobility improvements,
- Improvements to traffic operations,
- Pedestrian, bicycle and transit enhancements or
- Air quality improvements through congestion reduction

Components of the West Busch Blvd Corridor Study

Visioning and Consensus – FDOT will work with project stakeholders to develop a shared vision that guides project goals and objectives. A comprehensive understanding of the SR 580 corridor needs and community endorsement are vital to the effectiveness of the planning, design and implementation processes.

Planning and Conceptual Design – Communities leverage local resources and knowledge to assist in guiding project activities to best meet the needs of their community. Tailoring best practices to match up with local conditions and desires will assist in developing a successful and implementable planning study.

5. Public Involvement for the Corridor Study

Several meetings will be held with community groups during the Corridor Study.

Elected Officials and Agency Kickoff Meeting: This meeting will be held in June 2017. The purpose of the Elected Officials and Agency Kickoff meeting is to provide an opportunity for the FDOT and the study team to introduce the West Busch Blvd Corridor Study and to solicit participation on the PAG.

Corridor Visioning Workshop: This meeting will be held in November 2017 to solicit input from the public on the goals and objectives of the study, and to receive input on the full range of potential improvements to be evaluated.

Alternatives Public Workshop: This meeting will be held in March 2018 to present the public the results of the study to date and provide an opportunity to express concerns, ask questions, state preferences, and make comments that will be included in the project files.

Project Advisory Group (PAG): This group is comprised of agency and community representatives, providing a forum for local experts to share their local knowledge about ongoing planning efforts; past successes and failures that may impact this study; and existing issues and concerns along the project corridor. The PAG will meet at key points throughout the study to collaborate with the project team to ensure that the corridor vision and improvement alternatives are well-coordinated with local planning efforts and community goals.

Informal Meetings: Informal meetings will be held in addition to scheduled meetings, where participation in other meetings with the public, elected officials, special interest groups or public agencies is expected to occur. The purpose of these meetings will be to apprise the attendees of the project status, specific location and design concepts, and to receive input.

6. Government and Agency Coordination

Local, regional, state and federal governments, agencies, businesses and organizations having potential impacts or concerns due to jurisdictional review or expressed interest have been identified and contacted. Coordination will continue with these groups throughout the project process.

7. Contact Information

Brian L. Shroyer
Multimodal Project Manager
Florida Department of Transportation
11201 N. McKinley Drive, MS 7-500
Tampa, FL 33612
813-975-6449
Brian.Shroyer@dot.state.fl.us

Kristen Carson
Public Information Officer
Florida Department of Transportation
11201 N. McKinley Drive, MS 7-110
Tampa, FL 33612
813-975-6060
Kristen.Carson@dot.state.fl.us

8. Project Facts

A. West Busch Blvd Corridor Study and Project Description

The Florida Department of Transportation, District Seven (D7) has identified the need to analyze, evaluate, and document travel-related problems, needs and issues along SR 580/Busch Boulevard from North Dale Mabry Highway to North Nebraska Avenue. The West Busch Blvd Corridor Study will be conducted to identify the cause and characteristics of the transportation issues within the corridor; these may include capacity, traffic operations, safety, access and egress, freight movements, transit, bicycles and pedestrian movements. The study will document both the current and forecasted-future travel conditions and needs and identify workable solutions for those needs.

Concurrent with the study, a Resurfacing, Restoration and Rehabilitation (RRR) design project will proceed under FPID 437530-1-32-01; the limits of the RRR design is SR 580 from East of Armenia Avenue to West of Florida Avenue, a distance of approximately 1.3 miles. The RRR improvements include milling and resurfacing, replacement of sidewalk where deficient and filling in missing gaps where existing right of way is available.

B. Project Location Map



C. West Busch Blvd Corridor Study Purpose and Need

The purpose of this study is to:

1. Determine and document the existing challenges in the corridor, including: existing and projected development, travel-related problems, congestion, access, operational efficiency, safety, pedestrian and bicycle connectivity. This includes soliciting input from the local users and stakeholders.
2. Develop a long-term vision for the corridor that defines the goals and objectives and policy requirements that harmonize with Complete Streets and Vision Zero.
3. Address near-term needs by identifying workable solutions for travel needs, consistent with the long-term vision that is established.

Improvements to West Busch Boulevard (SR 580) are needed to: address congestion in the corridor; improve the level of service at the intersections; reduce the number vehicle/pedestrian/bicycle crashes; and support accessibility to transit. This effort to improve mobility and enhance safety in the corridor will be achieved by developing several alternatives, including near-term improvements such as addition of bike lanes and closing side-walk gaps, where feasible; evaluating turning movement efficiency and access management. Long-term solutions could include addition of continuous sidewalks and bike lanes, if feasible within the context of the corridor.

D. Project Schedule

The West Busch Boulevard (SR 580) study began in March 2017 and is anticipated to be completed in 18 months. The project activities include collecting information, defining a vision for the corridor and developing roadway improvement alternatives. Please see the project schedule below:

SR 580/Busch Boulevard from North Dale Mabry Highway to North Nebraska Avenue Corridor Study																			
PUBLIC INVOLVEMENT PLAN SCHEDULE																			
ACTIVITY	2017											2018							
	Mar	Apr	May	June	July	Aug	Sept	Oct	Nov	Dec	Jan	Feb	Mar	April	May	June	July	Aug	Sept
Notice to Proceed - Project Begins	★																		
Develop Public Involvement Plan																			
Mailing List																			
Elected Officials/Agency Kick-off Meeting					★														
Project Advisory Group (PAG) Meetings																			
Corridor Visioning Workshop																			
Alternatives Public Meeting																			
Final Corridor Alternatives & Strategies Report																			



ELECTED OFFICIALS / AGENCY KICK-OFF MEETING

AGENDA

West Busch Boulevard (SR 580) Corridor Study
from North Dale Mabry Highway to North Nebraska Avenue
Hillsborough County, Florida
FDOT Financial Project ID Number: 435908-1-22-01
North Tampa Branch Library
June 29, 2017 – 10:00 am to 12:00 pm

1. Welcome
 - a. Florida Department of Transportation Team Introductions
 - b. Officials and Agency Personnel Introductions
 - c. Consultant Team Introductions
2. Goals for the Kick-Off Meeting
 - a. Provide Officials and Agency partners information on the project
 - b. Solicit input on issues and concerns in the corridor
 - c. Invite participation in the Project Advisory Group
3. Project Overview
 - a. Description
 - b. Formal Tasks
 - c. Schedule
4. Open Discussion: Corridor Concerns and Ideas
5. Next Steps
6. Opportunities to Participate
7. Project Advisory Group (PAG) Formation
8. Closing Remarks and Adjourn

Appendix C



TITLE VI



The proposed project is being developed in accordance with the Civil Rights Act of 1964, as amended. Under Title VI of the Civil Rights Act, public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status.

To express concern(s), please contact the following:

Christopher Speese

Title VI Coordinator | Florida Department of Transportation, District Seven
11201 N. McKinley Drive | Tampa, Florida 33612
Christopher.Speese@dot.state.fl.us
(813) 975-6405

-- or --

Jacqueline Paramore

Statewide Title VI Coordinator | Equal Opportunity Office
605 Suwannee Street, Room 260 | Mail Station 65
Tallahassee, Florida 32399-0450
Jacqueline.Paramore@dot.state.fl.us
(850) 414-4753



TITULO VI



Este proyecto se está diseñando de acuerdo con la Ley de Derechos Civiles de 1964 y 1968, según enmendada. Bajo el Título VI de la Ley de Derechos Civiles, la participación pública es solicitada sin distinción de raza, color, origen nacional, edad, sexo, incapacidad o estado familiar.

Para expresar alguna preocupación, comuníquese con:

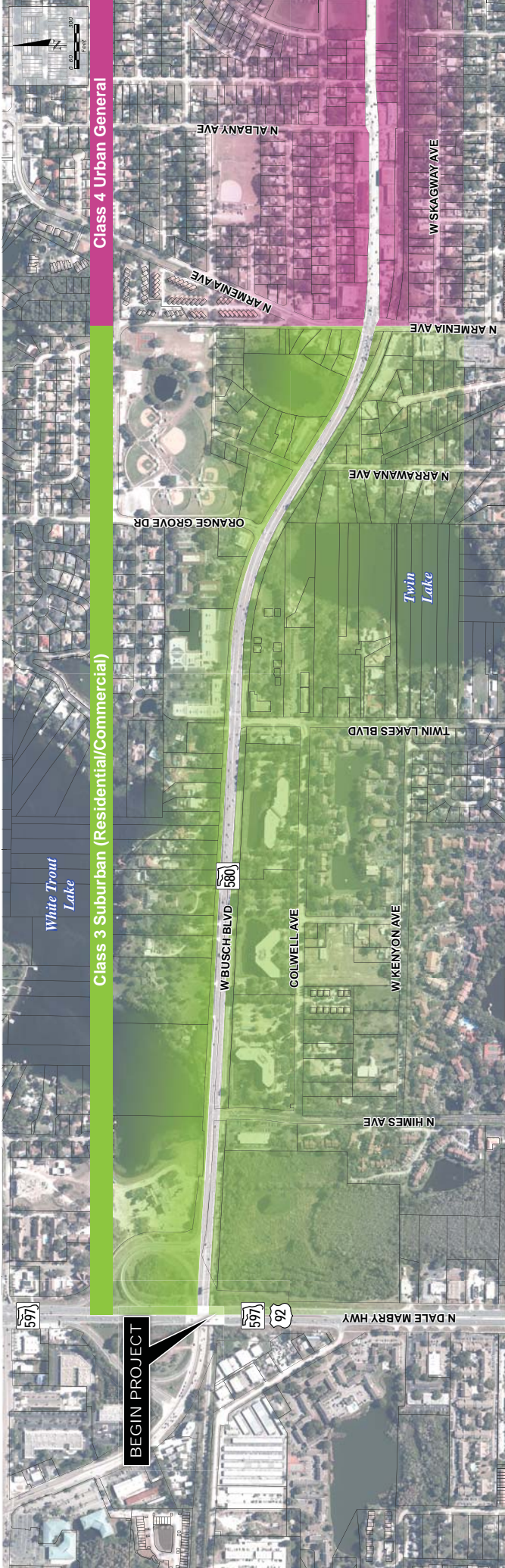
Lilliam Escalera

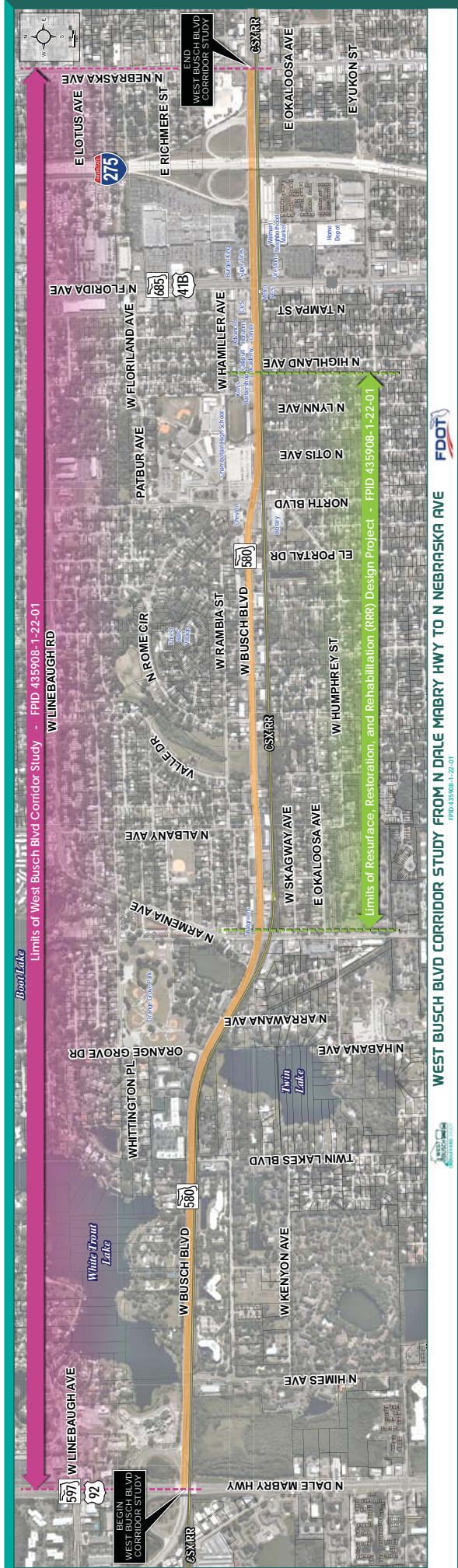
Florida Department of Transportation, District Seven
11201 N. McKinley Drive | Tampa, Florida 33612
Lilliam.Escalera@dot.state.fl.us
(813) 975-6445

-- or --

Jacqueline Paramore

Statewide Title VI Coordinator | Equal Opportunity Office
605 Suwannee Street, Room 260 | Mail Station 65
Tallahassee, Florida 32399-0450
Jacqueline.Paramore@dot.state.fl.us
(850) 414-4753





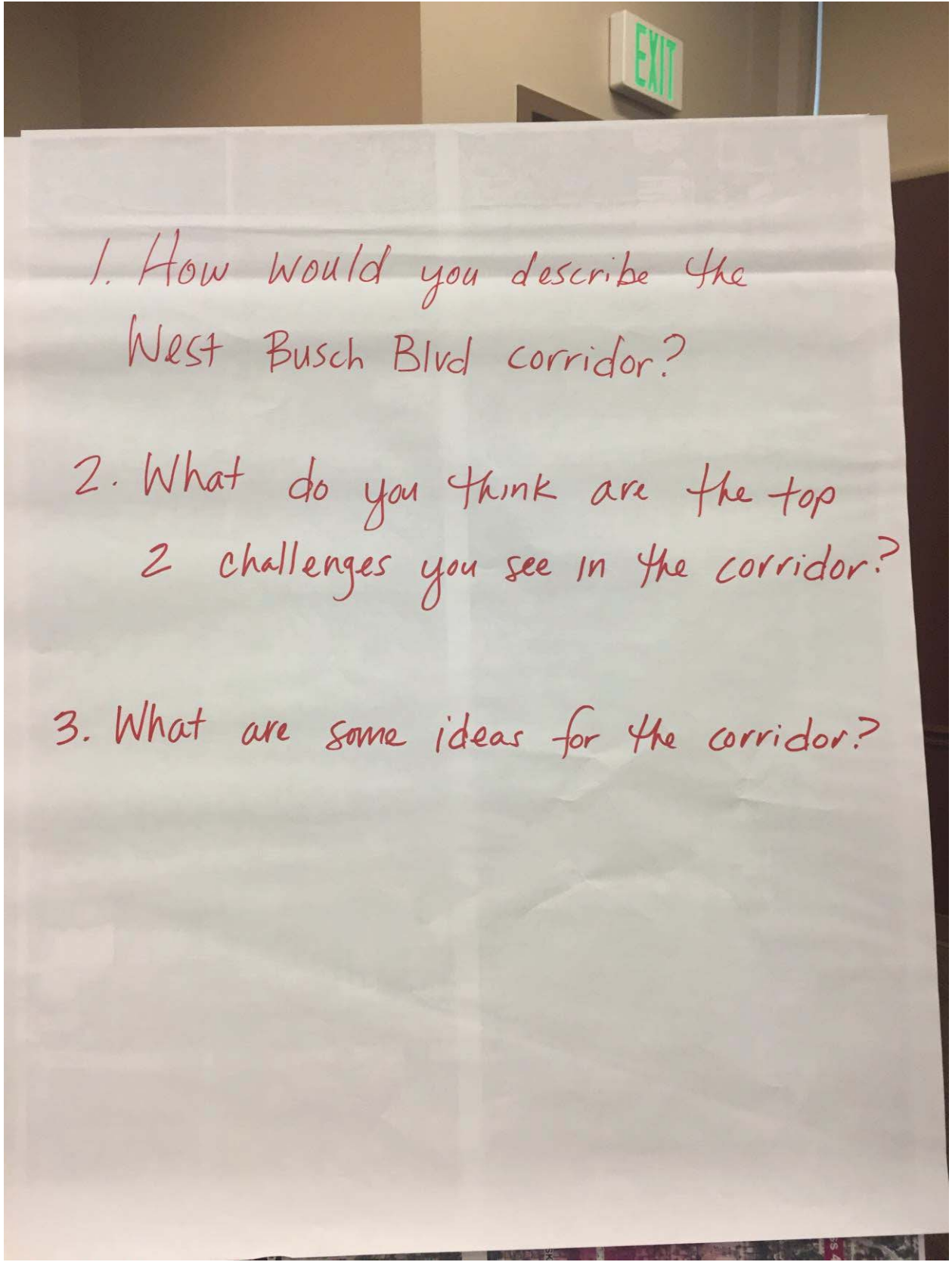
WEST BUSCH BLVD CORRIDOR STUDY FROM N DALE MABRY HWY TO N NEBRASKA AVE



FPID 435908-1-22-01

KICK-OFF MEETING PICTURES



- 
1. How would you describe the West Busch Blvd corridor?
 2. What do you think are the top 2 challenges you see in the corridor?
 3. What are some ideas for the corridor?



POST-IT SUPERSTICKY EASELPAD
TABLEAU A FEUILLES MOBILES SUPERCOLLANTES
SUPERADHESIVAS BLOC DE HOJAS REPOSICIONABLES



30

25 IN/PO x 30 IN/PO
63,5 cm x 76,2 cm
5.2 SQ FT/PI² (0,48 m²)

3M

Too Fast (2)
Too Many Cars/Car
Centric
Congested

Through Corridor
Outdated
Violent

Depressing



SUPERSTICKY EASELPAD
TABLEAU A FEUILLES MOBILES SUPERCOLLANTES
SUPERADHESIVAS BLOC DE HOJAS REPOSICIONABLES



30

24 IN/PO x 30 IN/PO
63,5 cm x 76,2 cm
5.2 SQ. FT/PI² (0,48 m²) 3M

2 - BIKE/PED EDUCATION

- CSX INCLUSION - TWLTL USED AS TRAVEL LANE
- TRAFFIC VOLUME / SPEED OF TRAFFIC
- # ~~YOUNG~~ YOUNG DRIVERS ON CORRIDOR / EDUCATION
- UNSAFE FOR NON-MOTORIZED
(MULTIMODAL EQUITY) - LAND USE - DW GRADES
SIGHT DISTANCE, SIGNING
- PED XING NEAR SCHOOL LACK OF WARNING
NOT SCHOOL ZONE (FL BEACON)
- ACCESS MGMT / BUSINESS CONSIDERATIONS
- BEAUTIFICATION
- ACCESSIBILITY
- R/W
- CSX PROXIMITY TO CORRIDOR



SMARTSTICKY EASELPAD
TABLEAU A FEUILLES MOBILES SUPERCOLLANTES
SUPERADHESIVAS BLOCC DE HOJAS REPOSICIONABLES



30

24 IN/PO x 30 IN/PO
63,5 cm x 76,2 cm
5,2 SQ FT/PI² (0,48 m²)

3M

#3

FHWA - WARRANT 17 & 19 - IMPLEMENT
ALL COUNTERMEASURES

- IMPROVE ACCESSIBILITY (EMS)
- BACKUPS / ENTRANCE TO LIMITED ACCESS FACILITIES
- HARD STOP LOCATIONS / RELATED TO INTERSECTIONS
- MID BLOCK XING
- BUY R/W BETWEEN BUSCH / CSX
- LIGHTING
- ACC. MGMT.
- S/W CONNECTIVITY
- TRAFFIC CALMING - RRFB
- CSX COORDINATION - MOVE S/W / FENCE
- CORRIDOR THEME
- ELEVATED L/A

Appendix D



ELECTED OFFICIALS / AGENCY KICK-OFF MEETING
West Busch Boulevard (SR 580) Corridor Study
 From North Dale Mabry Highway to North Nebraska Avenue
 Financial Project ID Number: 435908-1-22-01



Thursday, June 29, 2017 from 10:00 am to 12:00 pm ■ North Tampa Branch Library, 8916 North Boulevard, Tampa, FL 33604

Sign-In Sheet

NAME (PLEASE PRINT)	AGENCY / ORGANIZATION	TITLE	PHONE	EMAIL
Heath Beauregard	HCPs	Principal	975-7670	heath.beauregard@stlcc.k12.fl.us
Calvin Thresholt	COT	Engineer	274-7884	Calvin.Thresholt@tampabaygov.net
Eric Lawson	Tampa IP	COO		eric@tampainnovation.com
Nick Loliccio	Tampa Fire, Rescue	Asst. Ch	274-7535	nick.loliccio@tampabaygov.net
Gena Torres	Hillsborough MPO	Exec. Planner	813-273-3774x357	torresg@plancom.org
Luis Viera	Tampa City Council	City Council	892564	
Lissa Silva	Hills MPO	Prinl Planner	813-272-5940	silva@plancom.org
Lisa Montelione	CDC of Tampa	Employer Relations	813-695-5030	Lisa.Montelione@cdcotampa.org

Appendix E

Appendix F



ELECTED OFFICIALS / AGENCY KICK-OFF MEETING

West Busch Boulevard (SR 580) Corridor Study

From North Dale Mabry Highway to North Nebraska Avenue

Financial Project ID Number: 435908-1-22-01



Thursday, June 29, 2017 • North Tampa Branch Library, 8916 North Boulevard, Tampa, Florida 33604

Please provide your comments below. If more space is needed, please use the back of this sheet or an additional sheet of paper. You may place your comments in the "Comment Box" provided at the meeting, or send to Brian Shroyer, Project Manager at the address on the back of this comment form, by July 10, 2017. Emailed comments are also acceptable to Brian.Shroyer@dot.state.fl.us

*All comments are part of the project record and are available for review by the public and the media.

- Work w/ HART to RELOCATE BUS STOPS AROUND Chamberlain HS, back farther away from the intersection of W. BLVD, or Betty, locate a "Super Stop" on Private Property like they have @ Brandon Mall & @ NETPARK
- ADD MID BLOCK CROSSINGS w/ RRTB's
- Lengthen turning Lane to access I-275 & N. Dale Mabry West Bound
- Timing of lights synch w/ each other during Rush Hour @ Nebraska, Florida, & Linebaugh & Armenia
- ADD pedestrian height street lighting
- ADD Landscaping
- Eliminate flat concrete medians, because they ARE USED AS if they are sidewalks
- Wider side walks
- Lengthen turning Lane @ Armenia

CONTACT INFORMATION (PLEASE PRINT)

PLEASE RETURN COMMENTS BY JULY 10, 2017 TO:

Name LISA Montelione

Title CDC of Tampa

Agency / Organization Job Developer, Employer

Phone Number 813-695-5030 Relations

Email LISA.Montelione@cdc of tampa.org

Brian L. Shroyer
Multimodal Project Manager
Florida Department of Transportation
11201 N. McKinley Drive, MS 7-500
Tampa, FL 33612
Brian.Shroyer@dot.state.fl.us

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ELECTED OFFICIALS / AGENCY KICK-OFF MEETING

West Busch Boulevard (SR 580) Corridor Study

From North Dale Mabry Highway to North Nebraska Avenue

Financial Project ID Number: 435908-1-22-01



Thursday, June 29, 2017 • North Tampa Branch Library, 8916 North Boulevard, Tampa, Florida 33604

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2 MPO studies - Busch Beautification easements Accessible Entry July 4/06 to prary

MPO Livable Roadways Committee acquire remnants lots on N side of CSX

Busch Gardens Pop = Colleen Roop add raised medians near N. Blvd. Armenia to Busch

CSX rep. Gateway - said could shift fence replace

Innovation Gateway plan hillsborough.org innovation-gateway Community Aesthetics Feature application

contact large property owner @ NE quadrant of Dale Mabry & Busch

West bound HART Bus stop

CONTACT INFORMATION (PLEASE PRINT)

PLEASE RETURN COMMENTS BY JULY 10, 2017 TO:

Name Lisa Silva Title MPO Agency / Organization MPO Phone Number 272-5940 Email lsilva@plancom.org

Brian L. Shroyer Multimodal Project Manager Florida Department of Transportation 11201 N. McKinley Drive, MS 7-500 Tampa, FL 33612 Brian.Shroyer@dot.state.fl.us

better near stadium

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get school bus stops data from MPO



ELECTED OFFICIALS / AGENCY KICK-OFF MEETING

West Busch Boulevard (SR 580) Corridor Study

From North Dale Mabry Highway to North Nebraska Avenue

Financial Project ID Number: 435908-1-22-01



Thursday, June 29, 2017 • North Tampa Branch Library, 8916 North Boulevard, Tampa, Florida 33604

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*All comments are part of the project record and are available for review by the public and the media.

Please consider slightly delaying TRB project to incorporate solutions that may be generated at the end of corridor study and more feasibly built.

Kudos for leading pedestrian intervals consideration

Please reach out to the community for a leader who can represent the citizens. Coming to them with recommendations or close to final recommendations doesn't provide a transparent process

A target speed should be decided on and engineering solutions to ensure the design matches

* Visit the site in the evening, walk the corridor, bicycle the corridor

businesses losing access

- 1. violent
- 2. slowing speed
- 3. narrowing midblock lights landscaping

CONTACT INFORMATION (PLEASE PRINT)

Name Gena Torres
 Title Exec. Planner
 Agency / Organization Hillsborough MPD
 Phone Number (813) 273-3774 x357
 Email torresg@plancom.org

PLEASE RETURN COMMENTS BY JULY 10, 2017 TO:

Brian L. Shroyer
 Multimodal Project Manager
 Florida Department of Transportation
 11201 N. McKinley Drive, MS 7-500
 Tampa, FL 33612
 Brian.Shroyer@dot.state.fl.us

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ELECTED OFFICIALS / AGENCY KICK-OFF MEETING

West Busch Boulevard (SR 580) Corridor Study

From North Dale Mabry Highway to North Nebraska Avenue

Financial Project ID Number: 435908-1-22-01



Thursday, June 29, 2017 ■ North Tampa Branch Library, 8916 North Boulevard, Tampa, Florida 33604

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*All comments are part of the project record and are available for review by the public and the media.

Evaluate All intersection with FHWA
IA-19 Signal Warrant 7 for Cesh. Also
look at all FHWA Instrument Approval
for application in the corridor

CONTACT INFORMATION (PLEASE PRINT)

PLEASE RETURN COMMENTS BY JULY 10, 2017 TO:

Name _____
Title _____
Agency / Organization _____
Phone Number _____
Email _____

Brian L. Shroyer
Multimodal Project Manager
Florida Department of Transportation
11201 N. McKinley Drive, MS 7-500
Tampa, FL 33612
Brian.Shroyer@dot.state.fl.us

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Project Advisory Group Meeting #1

Date: Tuesday, August 29, 2017
Time: 10:00 a.m. to 12:00 p.m.
Location: North Tampa Branch Library
8916 North Boulevard
Tampa, Florida 33604
Attendees: See Appendix A

Notification of Meeting

Meeting invitation letters were sent by email to elected officials, government officials, local agencies and other interested parties. The invitation letters included the meeting date, time and location, and a summary of the project.

A Florida Administrative Register (FAR) ad was published in Volume 43/161 on August 18, 2017. The Florida Department of Transportation posted meeting information on the FDOT Public Notices website.

Copies of the invitation letter, FAR ad and FDOT Public Notice website ad are included in Appendix B.

Meeting Handouts

A copy of the meeting PowerPoint, Project Advisory Group (PAG) handout and Newsletter #1 were distributed to the attendees. PAG participants were encouraged to discuss their thoughts and concerns regarding the corridor study.

A copy of the meeting handouts is available in Appendix C.

Attendee Comments

One written comment form was received. A copy of the comment received is included in Appendix D.

Summary

The following provides a summary of the items discussed at the meeting:

- Brian Shroyer of the Florida Department of Transportation (FDOT) opened the meeting by thanking attendees for their participation.

- Consultant Project Manager, Carol Barker, introduced the project team members and asked everyone to introduce themselves and describe their affiliations.
- Carol provided a project overview and explained the role of PAG (See Appendix C).
- Carol guided the PAG through a PowerPoint presentation (See Appendix C).
- Attendees participated in a Color Dot Survey Exercise. They were given green, yellow, blue and red dot stickers and were asked to rank what is most important to least important to them for the SR 580 Corridor. Responses can be found in Appendix E.
- A Draft User Preference Survey was distributed to attendees. Carol went through each potential survey question and asked the attendees for suggestions. Participant's responses can be found in Appendix F. The consultant will revise the User Preference Survey and show the updated version at PAG Meeting #2.
- Carol discussed upcoming PAG meetings and public meetings.
- Carol encouraged the attendees to visit the [FDOT project website](#) and [WikkiMapping website](#).
- At the conclusion of the meeting, attendees were provided copies of the newsletters to distribute to other interested parties.
- Carol provided closing remarks. The meeting ended at 12:00 p.m.
- After the PAG meeting, the consultant team distributed newsletters to businesses along the project corridor.

Discussion

Project Advisory Group (PAG) questions during discussion included:

Attendee: Have you looked into the causes of crashes along the corridor?

Carol Barker: Carol advised "Yes" and discussed the following PowerPoint slides: Crash History Data, Crash Prone Areas Heat Map and Latest Crash History by Year.

Attendee: I was advised a School Zone Safety Study has not been done. Can you please, let us know if you hear different?

Carol Barker: I am not aware of a current School Zone Safety Study but there was a Mid-Block crossing study. I will let you know at the next PAG meeting if I hear any information

Attendee: The MPO provides translation for specific Latino groups such as Puerto Rican and Cuban. Will the User Preference Survey be in Spanish also?

Carol Barker: The User Preference Survey can be translated into Spanish if the need arises.

Attendee: Maybe project information can be pushed out through the Parent Link at local schools?

Carol Barker: That sounds like a great idea. I will discuss with the PAG school representatives.

Attendee: Has there been any discussions with CSX?

Carol Barker: I will be speaking with Brian Shroyer/FDOT, about setting up an individual meeting with Bob O'Malley (CSX), in Bob's Orlando Office.

Attendee: Will you be meeting with the MPO?

Carol Barker: Yes, we will be meeting with the MPO.

END OF MEETING SUMMARY

This meeting summary was prepared by Karen Harrell, Public Involvement Coordinator. If you feel clarifications are necessary, or if this differs from your understanding, please notify Karen by telephone at 352-257-1651 or by e-mail at Karen.Harrell@qcausa.com within (5) working days upon receipt of this summary.

Appendix A



PROJECT ADVISORY GROUP MEETING #1
West Busch Boulevard (SR 580) Corridor Study
 From North Dale Mabry Highway to North Nebraska Avenue
 Financial Project ID Number: 435908-1-22-01



Tuesday, August 29, 2017 from 10:00 am to 12:00 pm ■ North Tampa Branch Library, 8916 North Boulevard, Tampa, FL 33604

Project Advisory Group (PAG) Sign-In Sheet

NAME (PLEASE PRINT)	AGENCY / ORGANIZATION	TITLE	PHONE	EMAIL
Lindaw Walker	HART	Planner II	813-223-6831	walkerl@ghart.org
Colvin Thambel	City of Tampa	Engineer	913-274-2854	Colvin.Thambel@Tampabay.NA
Carol Baker	CH2M	PM	407-650-2175	
Lish Silva	MPO			Silval@plancom.org
Wade Reynolds	MPO		272-5940	ReynoldsW@plancom.org
Johnny Wiley	MPO			wjw@plancom.org
RUSSELL WATKINS	MPO-HILLS	Principal	272-5940	RUSSELL.WATKINS@plancom.org
Rafael Parra	TPL		913-625-8020	parra-r@planum.org
Hazel Berglund	HGPS	Principal	813-975-7470	hazel.berglund@sdsc.k12.fl.us
Andrew Smith	HGPS	TRAN SAFETY Sup	813-434-6314	AndrewSmith@SDMC.K12.FL.US
Jody Woods	Chamberlain HS	Assistant Principal	813-975-7677	Jody.Woods@sdsc.k12.fl.us



PROJECT ADVISORY GROUP MEETING #1
West Busch Boulevard (SR 580) Corridor Study
 From North Dale Mabry Highway to North Nebraska Avenue
 Financial Project ID Number: 435908-1-22-01



Tuesday, August 29, 2017 from 10:00 am to 12:00 pm ■ North Tampa Branch Library, 8916 North Boulevard, Tampa, FL 33604

Project Advisory Group (PAG) Sign-In Sheet

NAME (PLEASE PRINT)	AGENCY / ORGANIZATION	TITLE	PHONE	EMAIL
Mark Jennings	CH2M		727 560-8144	mark.jennings@ch2m.com
Cris Schooley	CH2M		407 334 2912	Cris.Schooley@ch2m.com
Chad Polk	CH2M		813.281.7912	chad.polk@ch2m.com
David Hernandez	CH2M		813-281-7912	david.hernandez@ch2m.com
Karen Harrell	QCA		352-257-1651	KarenHarrell@QCAUSA.com
Jessica Francois	QCA		850-356-0699	Jessica.Francois@QCAUSA.com
Brian L. Sawyer	PAOT		813-975-6449	Brian.Sawyer@dot.state.fl.us
TIMOTHEE RATNAYAKE	PAOT		913-975-6057	TIMOTHEE.RATNAYAKE@DOT.STATE.FL.US

Appendix B

Date: August 08, 2017

Subject: **Project Advisory Group (PAG) Meeting**
West Busch Boulevard (SR 580) Corridor Study
from North Dale Mabry Highway to North Nebraska Avenue
Hillsborough County, Florida
FDOT Financial Project ID Number: 435908-1-22-01

Dear Project Stakeholder:

The Florida Department of Transportation (FDOT), District Seven is conducting a Corridor Study on the above referenced study. This letter is an invitation for you to become a member of the Project Advisory Group (PAG) for this project. The PAG meeting is scheduled for Tuesday, August 29, 2017, from 10:00 AM to Noon at the **North Tampa Branch Library, 8916 North Boulevard, Tampa, Florida 33604.**

The West Busch Boulevard Corridor study limits extend from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The study area includes portions of incorporated Hillsborough County and the City of Tampa. The purpose of the Corridor Study is to work with the community, stakeholders and a Project Advisory Group (PAG) to develop a vision and identify needs and issues in the corridor.

The PAG will be an important component of the study. It will serve as a special advisory resource to the FDOT and the consultant team. The PAG participants will help develop a vision for the corridor that will guide proposed short and long-term recommendations. This may include issues related to capacity, traffic operations, safety, access and egress, mobility, freight movements, transit, bicycles, and pedestrian movements.

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220 at least seven (7) working days in advance of the meeting.

If you have any questions about membership in the PAG or require further information concerning this project, please contact Brian Shroyer, Florida Department of Transportation Project Manager at 813-975-6449 or by email at Brian.Shroyer@dot.state.fl.us.

Sincerely,
Brian Shroyer
Multimodal Project Manager
Florida Department of Transportation

Florida Administrative Register Ad:

The Florida Department of Transportation (FDOT), District Seven, is holding a West Busch Boulevard (SR 580) Corridor Study Project Advisory Group (PAG) Meeting for project stakeholders.

DATE AND TIME: August 29, 2017, 10:00 AM to Noon.

PLACE: North Tampa Branch Library, 8916 North Boulevard, Tampa, FL 33604

GENERAL SUBJECT MATTER TO BE CONSIDERED: The Florida Department of Transportation, District Seven, is holding a West Busch Boulevard (SR 580) Corridor Study Project Advisory Group (PAG) Meeting for project stakeholders within the study limits. The limits of the project extend from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The corridor study area includes portions of unincorporated Hillsborough County and the City of Tampa.

The purpose of the Corridor Study is to work with the community, stakeholders and a Project Advisory Group (PAG) to develop a vision and identify needs and issues in the corridor. The PAG will be an important component of the study. It will serve as a special advisory resource to the FDOT and the consultant team. The PAG participants will help develop a vision for the corridor that will guide proposed short and long-term recommendations. This may include issues related to capacity, traffic operations, safety, access and egress, mobility, freight movements, transit, bicycles, and pedestrian movements. A copy of the agenda may be obtained by contacting: Mr. Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us.

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Comuniquese Con Nosotros

Nos importa mucho la opinión del público sobre el proyecto. Si usted tiene preguntas o comentarios, o si simplemente desea más información, por favor comuniquese con nuestra representante, Lilliam Escalera, al (813) 975-6403, Departamento de Transportación de Florida, 11201 N. McKinley Dr., Tampa, FL 33612, Lilliam.Escalera@dot.state.fl.us

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT.

FDOT Public Notices Website:

PROJECT ADVISORY GROUP (PAG) MEETING

District: Seven

Meeting Type: Meeting

Date: August 29, 2017

Time: 10 AM to Noon

Location Name: North Tampa Branch Library

Street Address: 8916 North Boulevard

City: Tampa, FL 33604

Purpose: The Florida Department of Transportation, District Seven, is holding a West Busch Boulevard (SR 580) Corridor Study Project Advisory Group (PAG) Meeting for project stakeholders within the study limits. The limits of the project extend from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The corridor study area includes portions of unincorporated Hillsborough County and the City of Tampa.

The purpose of the Corridor Study is to work with the community, stakeholders and a Project Advisory Group (PAG) to develop a vision and identify needs and issues in the corridor. The PAG will be an important component of the study. It will serve as a special advisory resource to the FDOT and the consultant team. The PAG participants will help develop a vision for the corridor that will guide proposed short and long-term recommendations. This may include issues related to capacity, traffic operations, safety, access and egress, mobility, freight movements, transit, bicycles, and pedestrian movements. A copy of the agenda may be obtained by contacting: Mr. Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us.

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Primary Contact: Brian Shroyer, FDOT Project Manager

Primary Phone: (813) 975-6449

Primary E-mail: Brian.Shroyer@dot.state.fl.us

Additional Contact: Christopher Speese, Public Involvement Coordinator

Additional Phone: (813) 975-6405

Additional E-mail: Christopher.Speese@dot.state.fl.us

Appendix C

West Busch Boulevard (SR 580) Corridor Study

From North Dale Mabry Highway to North Nebraska Avenue

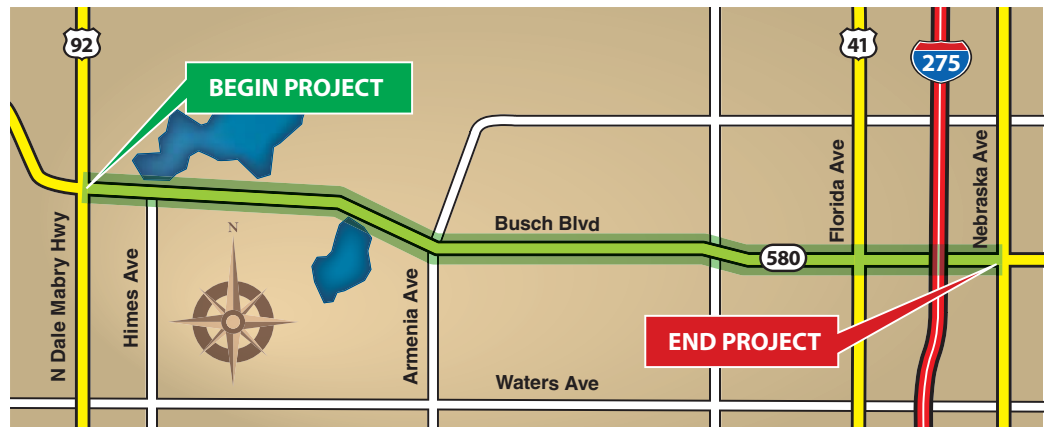
Hillsborough County, FL

Financial Project ID Number: 435908-1-22-01

Corridor Study Begins!

The Florida Department of Transportation (FDOT), District 7 is conducting a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue in Hillsborough County – a distance of approximately 3.3 miles. The West Busch Boulevard (SR 580) corridor is an east-west urban principal arterial connecting and serving local commuters and visitors between North Dale Mabry Highway and Nebraska Avenue. The corridor study area includes portions of unincorporated Hillsborough County and the City of Tampa and is the main route to the Busch Gardens Amusement Park (located approximately 2.15 miles east of the project limits at McKinley Drive).

The corridor supports a variety of land uses and demographics, including schools, churches, large and small businesses with direct access to and from SR 580, transit services, pedestrians, bicyclists and many small communities. The corridor also serves a large commuter population as a connection between Veterans Expressway to the west and I-75 to the east.



The West Busch Boulevard (SR 580) corridor study will use FDOT's Complete Streets policy to showcase how Complete Streets approaches can shape a corridor. The study, which began in March 2017, is expected to continue for approximately 18 months and will include technical work and data analysis, along with various opportunities for the public to be involved in discussing a vision for West Busch Boulevard (SR 580) that makes it accessible for all users.

What is a Corridor Study?

A corridor study is the first step in planning by defining the corridor's needs. A corridor study generally defines the relationships between a roadway and its adjacent land. Corridor studies are also used to: define acceptable levels of access and mobility; determine transportation system needs to support surrounding land uses, and identify operational deficiencies, among other things. This corridor study will include establishing a shared vision that will allow the community to help set project goals and objectives. The vision will guide the project team to develop a set of improvements that leverage local resources, match local conditions and desires, and are consistent with ongoing planning and development efforts.

Project Schedule

The West Busch Boulevard (SR 580) Corridor Study began in March 2017 and is anticipated to be completed in 18 months. Project activities will include:

- Identifying the travel related needs and issues within the West Busch Boulevard project corridor.
- Coordinating with key stakeholders to establish the vision for the corridor and identify solutions to the corridor issues and improve operational efficiency within the West Busch Boulevard corridor.
- Document the vision for all future improvements within the West Busch Boulevard corridor to maximize the “livability” for all users.

ACTIVITY	2017												2018						
	Mar	Apr	May	June	July	Aug	Sept	Oct	Nov	Dec	Jan	Feb	Mar	April	May	June	July	Aug	Sept
Notice to Proceed - Project Begins	★																		
Develop Public Involvement Plan																			
Mailing List																			
Elected Officials/Agency Kick-off Meeting				★															
Project Advisory Group (PAG) Meetings																			
Corridor Visioning Workshop									★										
Alternatives Public Meeting													★						
Final Corridor Alternatives & Strategies Report																			

Project Advisory Group (PAG)

A Project Advisory Group (PAG), composed of local government and agency representatives, are working with FDOT to identify corridor challenges along with potential solutions, assist with documenting the community goals, and develop a vision for the corridor that will guide the study development and future improvements. The PAG will meet at key points throughout the study to collaborate with the project team to ensure the vision and improvement alternatives are well-coordinated with local planning efforts, adjacent projects, and community goals.

How Can You Get Involved?

The project website is your best source for up-to-date information; you can get access at this link:

www.fdotd7studies.com/westbuschblvd/. In addition to the general study information that is already posted on the website, the project documents will be uploaded to this site as they are developed. However, you can already begin providing your input on this project using WikiMapping! This tool allows you to “pin” comments at specific locations along the corridor. You can pick the type of comment you have to help us understand the issues you observe in the corridor. Some of these issue types include: Bus/Transit, Connectivity, Intersection Issue, Pedestrian, Safety, and Traffic/Parking. If you have a comment on an issue that does not have a specific description, just use the “Other” category. The link to the WikiMapping tool is provided on the project website, but for direct access you can click on the following link:

<http://wikimapping.com/wikimap/West-Busch-Boulevard-Corridor-Study.html>. The project team will utilize this information to help inform the process and guide solutions.

Future opportunities for you to get involved include the upcoming Corridor Visioning Workshop that will be conducted in the Fall 2017. At this workshop, we will gather your input on the existing corridor and document your ideas to resolve some of the issues that you have observed. Our goal is to enlist your help to identify improvements that can be implemented in the West Busch Boulevard corridor to resolve the challenges. In advance of this workshop, a User Preference Survey will be available on the website in October; hard copies of this same survey will be available at the Corridor Visioning Workshop. These surveys will be used by the project team to understand the issues and concerns that you have regarding the corridor.

We want your input! We urge anyone interested in shaping the future of the West Busch Boulevard corridor to actively participate by visiting the website, get informed, document issues in the WikiMapping tool, attend meetings, and provide your ideas.

For more information or to comment, please contact:

Brian Shroyer, Project Manager
11201 N. McKinley Drive
Tampa, FL 33612
Office Phone: (813) 975-6449
brian.shroyer@dot.state.fl.us

Kristen Carson, Public Information Officer
11201 N. McKinley Drive
Tampa, FL 33612
Office Phone: (813) 975-6202 or 800-226-7220
kristen.carson@dot.state.fl.us

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT.

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Comuníquese Con Nosotros

Nos importa mucho la opinión del público sobre el proyecto. Si usted tiene preguntas o comentarios, simplemente desea más información, por favor comuníquese con nosotros. Nuestro representante, Señora Lilliam Escalera, 813-975-6445, Departamento de Transportación de Florida, 11201 N. McKinley Dr., Tampa, FL 33612, lilliam.escalera@dot.state.fl.us.



PROJECT ADVISORY GROUP OVERVIEW

1. What is a Project Advisory Group?

A Project Advisory Group (PAG) is established early in a corridor study and is composed of key stakeholders representing local cities, counties, agencies, jurisdictions and organizations within the study area that can help ensure that the explicit needs and concerns of the corridor are addressed. This important group serves as a special advisory resource to the Florida Department of Transportation (FDOT) and the project team; they will gather local input and then disseminate information back to the impacted stakeholders and communities to ensure that any improvements within the corridor continue to serve as vital elements of the community's transportation system. The PAG will also provide policy guidance and technical assistance to FDOT and the project team as the common goals and objectives of the study are defined, issues are identified, and potential solutions are developed.

The West Busch Blvd Corridor Study PAG will meet four (4) times over the course of the study and provides an opportunity for the study team to share project information and receive critical input on a regular basis.

2. What is the Role of the PAG?

The PAG is not an independent decision-making board, but is advisory in nature and will assist the FDOT and the consultant team by providing jurisdictional, policy, technical and administrative guidance throughout the study with the following goals in mind:

- Identify the interests of the stakeholders within the project limits.
- Establish relationships and open the lines of communication.
- Work collectively to bring diverse ideas and approaches together.
- Work as a team to achieve the common goals identified during the study.

To maximize success, PAG members should be honest, open-minded, responsive, cooperative and respectful of all group members.

3. What is the Purpose of PAG Meetings?

The purpose of PAG meetings is to bring together the FDOT, the consultant team, and the PAG members to establish an effective project planning or "partnering" team. The PAG will serve as an advisory board to help the FDOT and the study team gather local input and then disseminate information back to the impacted stakeholders and communities. This flow of information will aid in identifying issues, needs, and potential solutions that can be evaluated and documented the final Corridor Study.

4. What is a Corridor Study?

In general, corridor studies are a planning process used within the engineering community to assess existing conditions, identify transportation-related needs for a corridor and develop a preferred future condition. Corridor studies are used to:

- Define acceptable levels of access and mobility,
- Determine transportation system needs to support surrounding land uses,

- Consolidate and control access points,
- Identify safety concerns on the corridor, and
- Identify operational deficiencies and promote operational efficiency

Potential benefits of the West Busch Blvd Corridor Study may include:

- Maximizing efficiency and life-span of existing infrastructure,
- Improved safety conditions,
- Development of coordinated land uses,
- Access or mobility improvements,
- Improvements to traffic operations,
- Pedestrian, bicycle and transit enhancements or
- Air quality improvements through congestion reduction

Components of the West Busch Blvd Corridor Study

Visioning and Consensus – FDOT will work with project stakeholders to develop a shared vision that guides project goals and objectives. A comprehensive understanding of the SR 580 corridor needs and community endorsement are vital to the effectiveness of the planning, design and implementation processes.

Planning and Conceptual Design – Communities leverage local resources and knowledge to assist in guiding project activities to best meet the needs of their community. Tailoring best practices to match up with local conditions and desires will assist in developing a successful and implementable planning study.

5. Public Involvement for the Corridor Study

Several meetings will be held with community groups during the Corridor Study.

Elected Officials and Agency Kickoff Meeting: This meeting will be held in June 2017. The purpose of the Elected Officials and Agency Kickoff meeting is to provide an opportunity for the FDOT and the study team to introduce the West Busch Blvd Corridor Study and to solicit participation on the PAG.

Corridor Visioning Workshop: This meeting will be held in November 2017 to solicit input from the public on the goals and objectives of the study, and to receive input on the full range of potential improvements to be evaluated.

Alternatives Public Workshop: This meeting will be held in March 2018 to present the public the results of the study to date and provide an opportunity to express concerns, ask questions, state preferences, and make comments that will be included in the project files.

Project Advisory Group (PAG): This group is comprised of agency and community representatives, providing a forum for local experts to share their local knowledge about ongoing planning efforts; past successes and failures that may impact this study; and existing issues and concerns along the project corridor. The PAG will meet at key points throughout the study to collaborate with the project team to ensure that the corridor vision and improvement alternatives are well-coordinated with local planning efforts and community goals.

Informal Meetings: Informal meetings will be held in addition to scheduled meetings, where participation in other meetings with the public, elected officials, special interest groups or public agencies is expected to occur. The purpose of these meetings will be to apprise the attendees of the project status, specific location and design concepts, and to receive input.

6. Government and Agency Coordination

Local, regional, state and federal governments, agencies, businesses and organizations having potential impacts or concerns due to jurisdictional review or expressed interest have been identified and contacted. Coordination will continue with these groups throughout the project process.

7. Contact Information

Brian L. Shroyer
Multimodal Project Manager
Florida Department of Transportation
11201 N. McKinley Drive, MS 7-500
Tampa, FL 33612
813-975-6449
Brian.Shroyer@dot.state.fl.us

Kristen Carson
Public Information Officer
Florida Department of Transportation
11201 N. McKinley Drive, MS 7-110
Tampa, FL 33612
813-975-6060
Kristen.Carson@dot.state.fl.us

8. Project Facts

A. West Busch Blvd Corridor Study and Project Description

The Florida Department of Transportation, District Seven (D7) has identified the need to analyze, evaluate, and document travel-related problems, needs and issues along SR 580/Busch Boulevard from North Dale Mabry Highway to North Nebraska Avenue. The West Busch Blvd Corridor Study will be conducted to identify the cause and characteristics of the transportation issues within the corridor; these may include capacity, traffic operations, safety, access and egress, freight movements, transit, bicycles and pedestrian movements. The study will document both the current and forecasted-future travel conditions and needs and identify workable solutions for those needs.

Concurrent with the study, a Resurfacing, Restoration and Rehabilitation (RRR) design project will proceed under FPID 437530-1-32-01; the limits of the RRR design is SR 580 from East of Armenia Avenue to West of Florida Avenue, a distance of approximately 1.3 miles. The RRR improvements include milling and resurfacing, replacement of sidewalk where deficient and filling in missing gaps where existing right of way is available.

B. Project Location Map



C. West Busch Blvd Corridor Study Purpose and Need

The purpose of this study is to:

1. Determine and document the existing challenges in the corridor, including: existing and projected development, travel-related problems, congestion, access, operational efficiency, safety, pedestrian and bicycle connectivity. This includes soliciting input from the local users and stakeholders.
2. Develop a long-term vision for the corridor that defines the goals and objectives and policy requirements that harmonize with Complete Streets and Vision Zero.
3. Address near-term needs by identifying workable solutions for travel needs, consistent with the long-term vision that is established.

Improvements to West Busch Boulevard (SR 580) are needed to: address congestion in the corridor; improve the level of service at the intersections; reduce the number vehicle/pedestrian/bicycle crashes; and support accessibility to transit. This effort to improve mobility and enhance safety in the corridor will be achieved by developing several alternatives, including near-term improvements such as addition of bike lanes and closing side-walk gaps, where feasible; evaluating turning movement efficiency and access management. Long-term solutions could include addition of continuous sidewalks and bike lanes, if feasible within the context of the corridor.

D. Project Schedule

The West Busch Boulevard (SR 580) study began in March 2017 and is anticipated to be completed in 18 months. The project activities include collecting information, defining a vision for the corridor and developing roadway improvement alternatives. Please see the project schedule below:

SR 580/Busch Boulevard from North Dale Mabry Highway to North Nebraska Avenue Corridor Study																			
PUBLIC INVOLVEMENT PLAN SCHEDULE																			
ACTIVITY	2017											2018							
	Mar	Apr	May	June	July	Aug	Sept	Oct	Nov	Dec	Jan	Feb	Mar	April	May	June	July	Aug	Sept
Notice to Proceed - Project Begins	★																		
Develop Public Involvement Plan																			
Mailing List																			
Elected Officials/Agency Kick-off Meeting					★														
Project Advisory Group (PAG) Meetings																			
Corridor Visioning Workshop																			
Alternatives Public Meeting																			
Final Corridor Alternatives & Strategies Report																			

Learn more about the project here: www.fdotd7studies.com/westbuschblvd/

Project Advisory Group Meeting #1 West Busch Boulevard (SR 580) Corridor Study

from North Dale Mabry Highway to North Nebraska Avenue
Hillsborough County, Florida
FDOT Financial Project ID Number: 435908-1-22-01
North Tampa Branch Library
August 29, 2017 – 10:00 am to 12:00 pm



Agenda for Project Advisory Group Meeting #1

- Introductions
- Existing Roadway and Corridor Characteristics
- Developing the Corridor Vision
- Next Steps
- Questions
- Adjourn

Purpose of the SR 580 Corridor Study

- Define existing conditions
- Identify transportation-related needs
- Define acceptable levels of access and mobility
- Determine transportation system needs to support surrounding land uses
- Consolidate and control access points
- Identify safety concerns
- Identify operational deficiencies and promote operational efficiency

Introduction to PAG #1 Meeting





Introductions

- State your name
- Who do you represent
- What your interest is in the SR 580 corridor

PAG Roles and Responsibilities

- Attend as many meetings as possible
- Clearly and openly articulate your interests
- Listen to varying points of view
- Productively discuss issues with members who may hold divergent views
- Actively generate and evaluate potential improvement options
- Serve as a two-way conduit of information
- Keep others informed of the PAG's efforts

Goals for PAG #1

- Understand roadway characteristics
- Compare EO/Agency Kick-off comments with potential solutions
- Understand existing Context Classification and ground-truth future expectations
- Provide input on User Preference Surveys

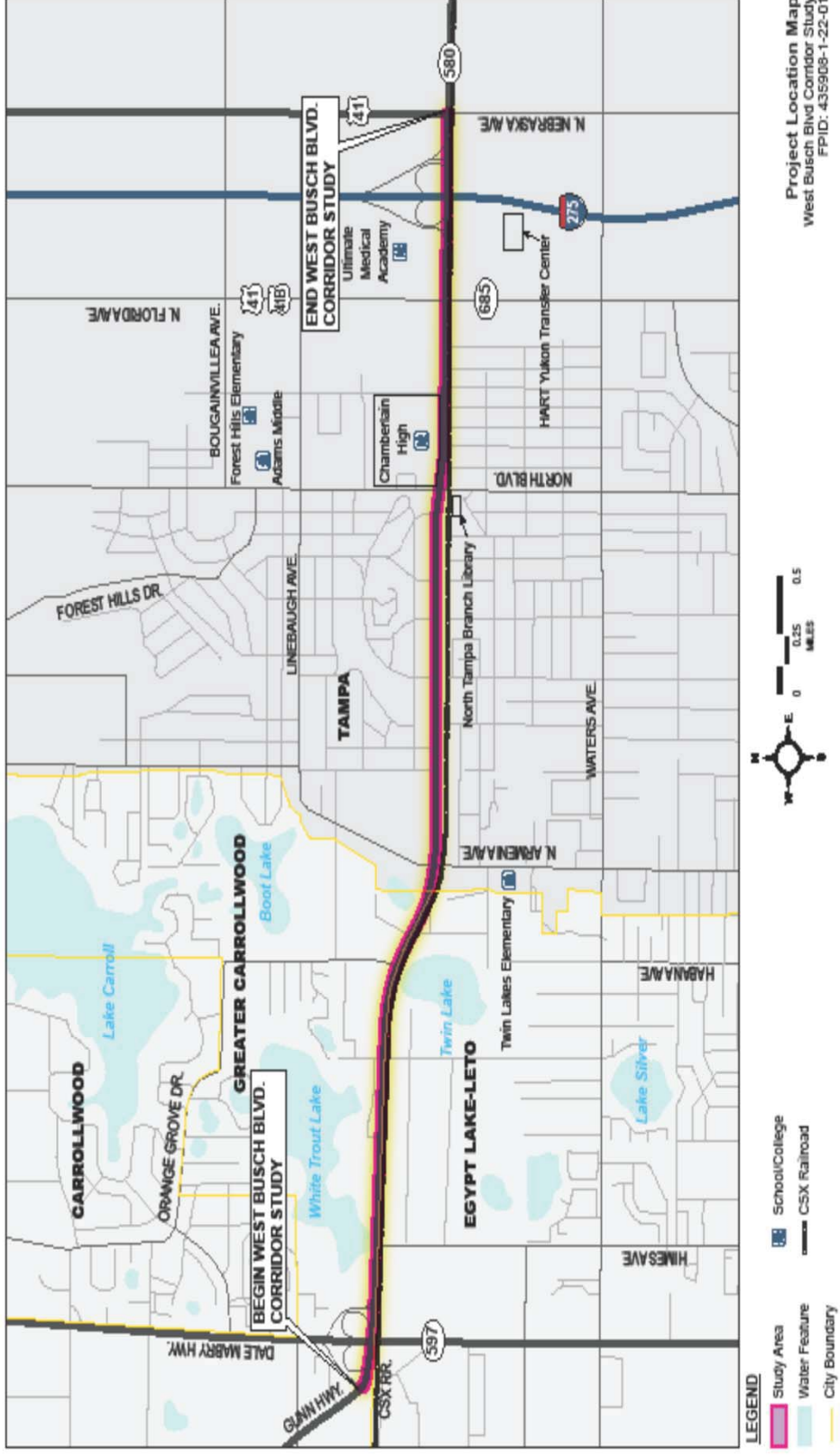
Future PAG meetings

- PAG #2
 - ≈ October 2017
 - ≈ User Preference survey; visioning
- PAG #3
 - ≈ January 2018
 - ≈ Purpose and need; range of solutions
- PAG #4
 - ≈ June 2018
 - ≈ Report on final near-term and long-term decisions

Existing Roadway and Corridor Characteristics



Project Information Location Map



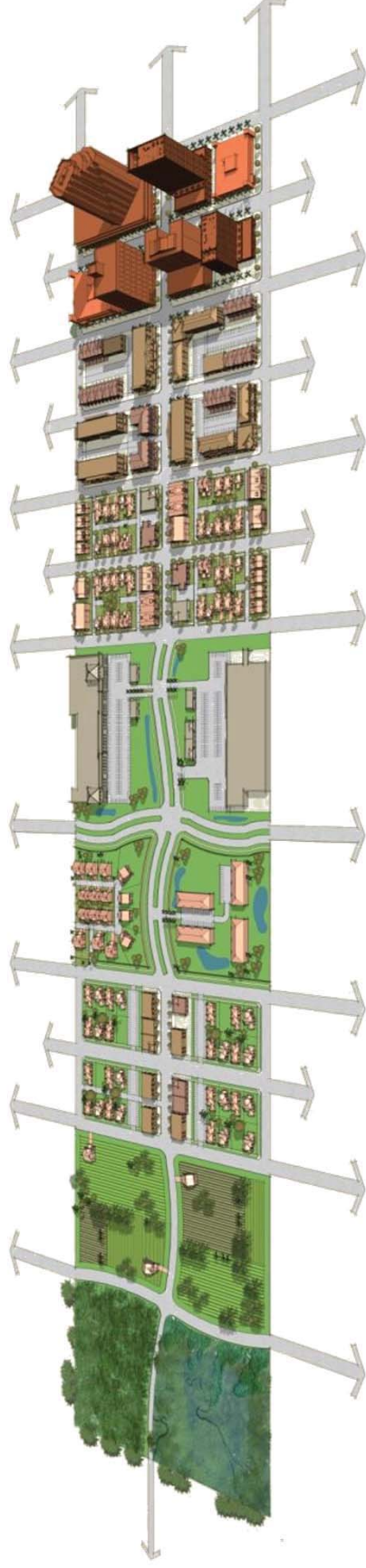
Project Information - FDOT's context classifications

Street Connectivity

- Block Length
- Block Perimeter
- Intersection Density

Development Form and Intensity

- Building Placement
- Presence of Fronting Uses
- Location of Off-Street Parking
- Land Uses
- Building Height



C1-Natural C2-Rural C2T-Rural Town C3R-Suburban Residential C3C-Suburban Commercial C4-Urban General C5-Urban Center C6-Urban Core

Existing context classification

C3R-SUBURBAN RESIDENTIAL



Mostly **residential** uses within large blocks and a disconnected or **sparse roadway network**.

C4-URBAN GENERAL



Mix of uses set within small blocks with a **well-connected roadway network**. The roadway network usually **connects to residential neighborhoods** immediately along the corridor or on the back side of blocks fronting the roadway.

C3C-SUBURBAN COMMERCIAL



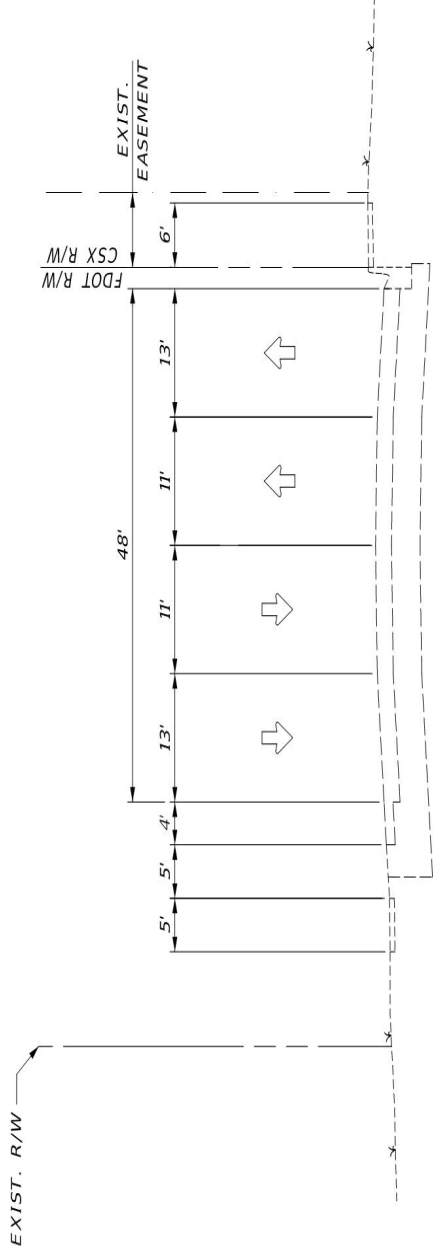
Mostly **non-residential** uses with **large building footprints** and **large parking lots** within large blocks and a disconnected or **sparse roadway network**.

C3R-Suburban Residential – Dale Mabry Hwy to Armenia Ave.



C3R-SUBURBAN RESIDENTIAL

Mostly **residential** uses within large blocks and a disconnected or **sparse roadway network**.

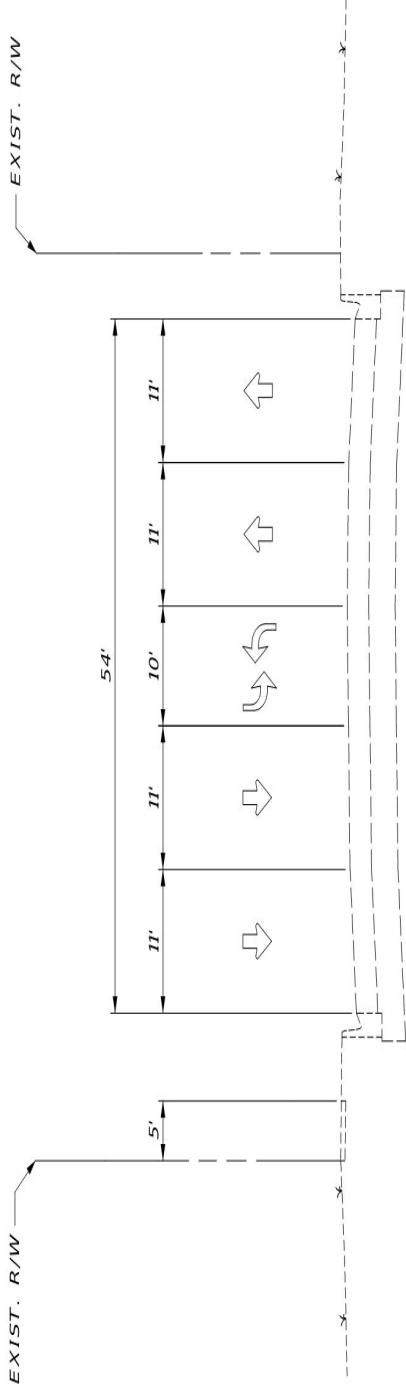


C4-Urban General – Armenia Ave to North Blvd

C4-URBAN GENERAL



Mix of uses set within small blocks with a **well-connected roadway network**. The roadway network usually **connects to residential neighborhoods** immediately along the corridor or on the back side of blocks fronting the roadway.



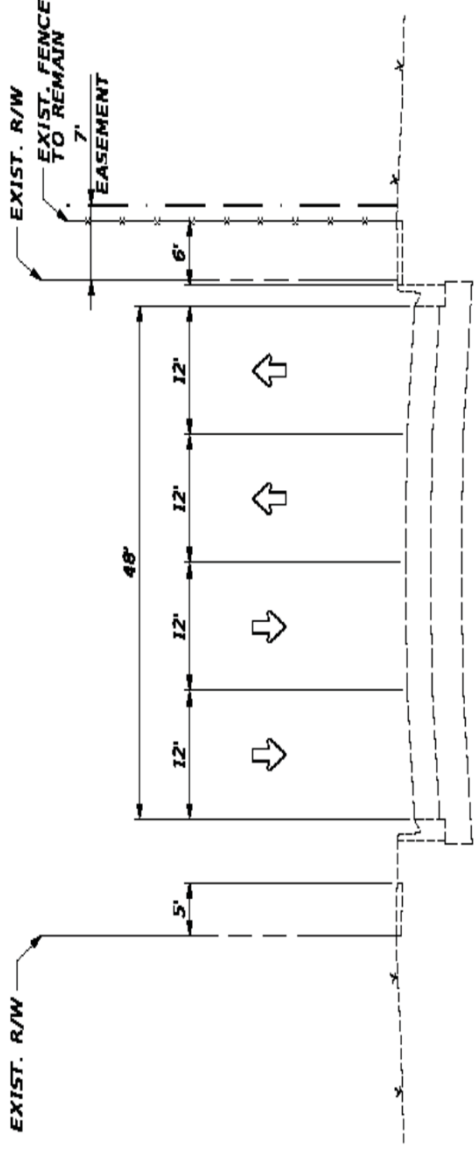
C3C Suburban Commercial – North Blvd to N Nebraska Ave.

C3C-SUBURBAN COMMERCIAL



Mostly **non-residential** uses with **large building footprints** and **large parking lots** within large blocks and a disconnected or **sparse roadway network**.

Section 1: North Blvd to Florida



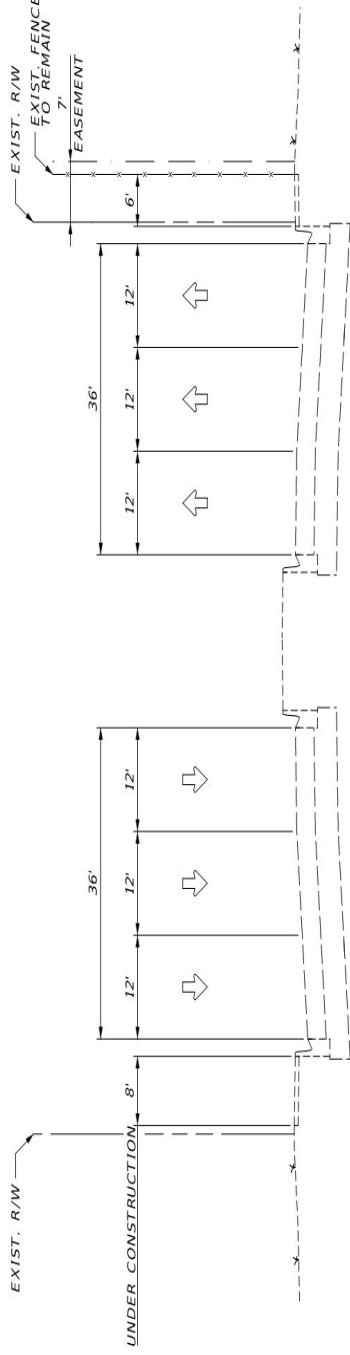
C3C Suburban Commercial – North Blvd to N Nebraska Ave.

C3C-SUBURBAN COMMERCIAL

Section 2: Florida to Nebraska



Mostly **non-residential** uses with **large building footprints** and **large parking lots** within large blocks and a disconnected or **sparse roadway network**.



Why does context classification matter?

- Projects will be evaluated using **future context**
- Future context should be clearly and consistently documented in local policies
- Local form-based codes and zoning can be used to inform FDOT's context classification determination
- On-street parking is a key element in C4, C5 and C6 & where it is not present could be added per local plan, for speed management, or to increase available parking

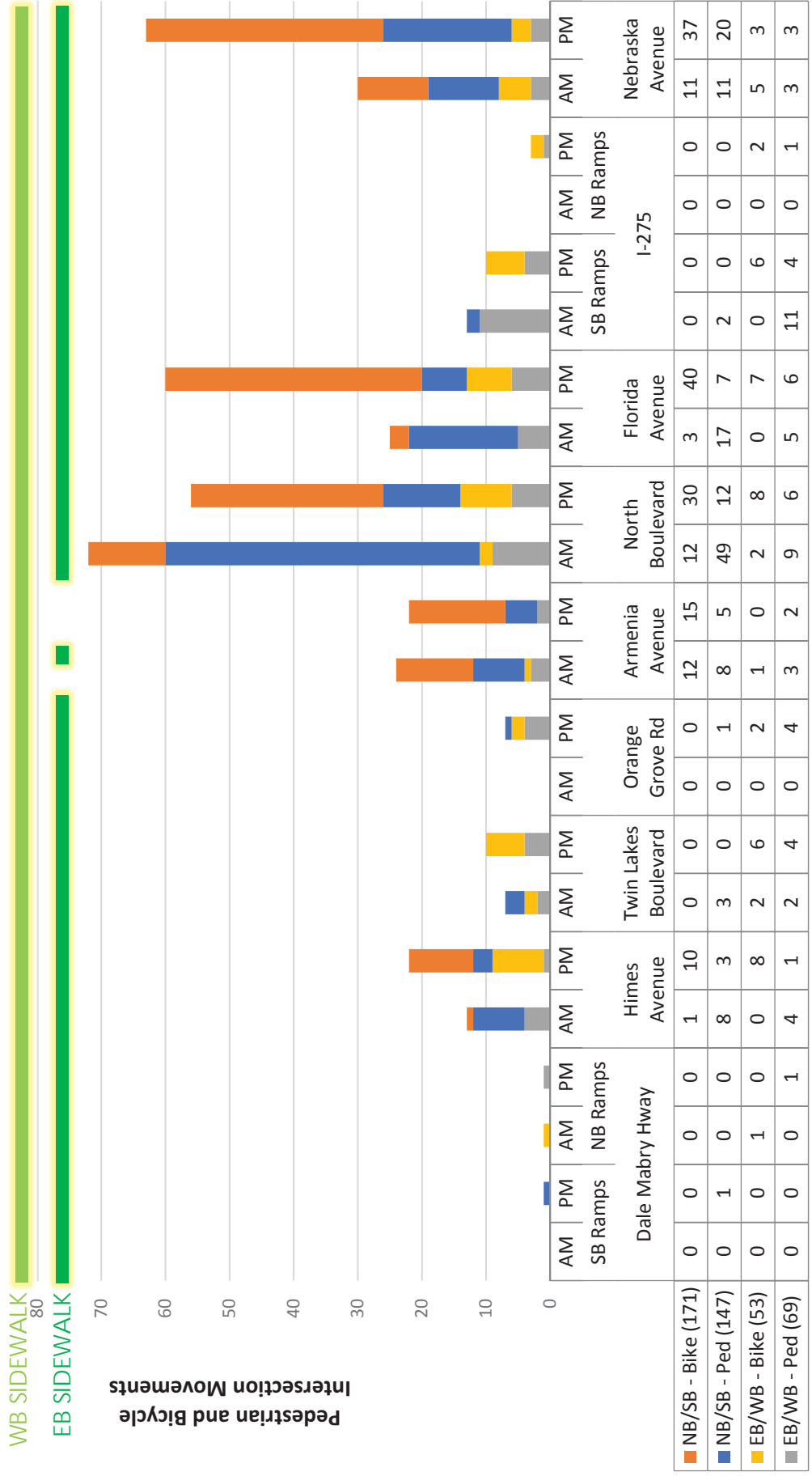
Context Classification	Allowable Design Speed Range for Non-SIS (mph)
C1 – Natural	55-70
C2 – Rural	55-70
C2T – Rural Town	25-45
C3 – Suburban	35-55
C4 – Urban General	30-45
C5 – Urban Center	25-35
C6 – Urban Core	25-30

Take Away from Existing Context Classification information

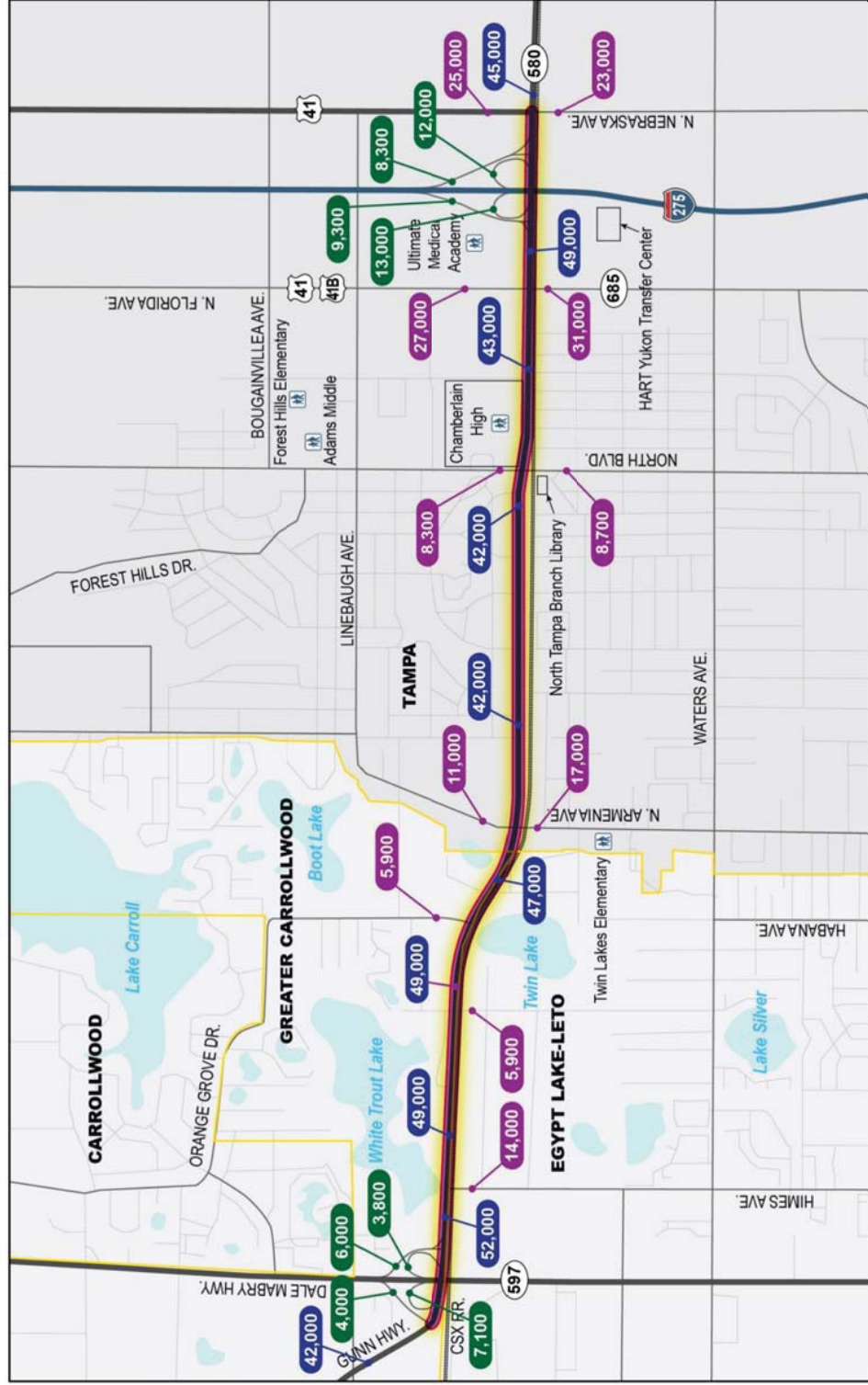
- No bike lanes throughout corridor
- Sidewalk gaps on South side of SR 580
- There is latitude in Context Classifications
- Projects are evaluated on FUTURE context

Pedestrian/Bicycle Counts

This slide illustrates the locations with the heaviest bike/ped traffic (based on peak 2-hour counts [1 day only] – from VHB AADT memo)



Existing Traffic Counts



LEGEND

- Study Area
- Water Feature
- City Boundary
- School/College
- CSX Railroad

Existing Traffic Count Map
West Busch Blvd Corridor Study
FPID: 435908-1-22-01

Scale: 0, 0.25, 0.5 Miles

Compass: N, S, E, W



Traffic Volume Considerations

STANDARDS FOR LOW AND HIGH VOLUME HIGHWAYS IN ANNUAL AVERAGE DAILY VOLUMES

HIGHWAY TYPE	LOW VOLUME AADT	HIGH VOLUME AADT
FREEWAY - URBAN		
4-LANE FACILITY	57,000	69,000
6-LANE FACILITY	86,000	103,000
8-LANE FACILITY	114,000	138,000
FREEWAY - RURAL		
4-LANE FACILITY	46,000	56,000
6-LANE FACILITY	69,000	83,000
8-LANE FACILITY	92,000	111,000
ARTERIALS - URBAN		
2-LANE FACILITY	16,000	20,000
4-LANE FACILITY	37,000	43,000
6-LANE FACILITY	55,000	64,000
8-LANE FACILITY	69,000	80,000
ARTERIALS - RURAL		
2-LANE FACILITY	9,000	14,000
4-LANE FACILITY	38,000	47,000
6-LANE FACILITY	58,000	71,000
COLLECTOR - URBAN		
2-LANE FACILITY	11,000	16,000
4-LANE FACILITY	37,000	45,000
COLLECTOR - RURAL		
2-LANE FACILITY	8,000	13,000
4-LANE FACILITY	30,000	38,000

SR 580 Existing AADT

Minimum = 42,000

Maximum = 52,000

Source: FDOT PPM, Glossary of Terms



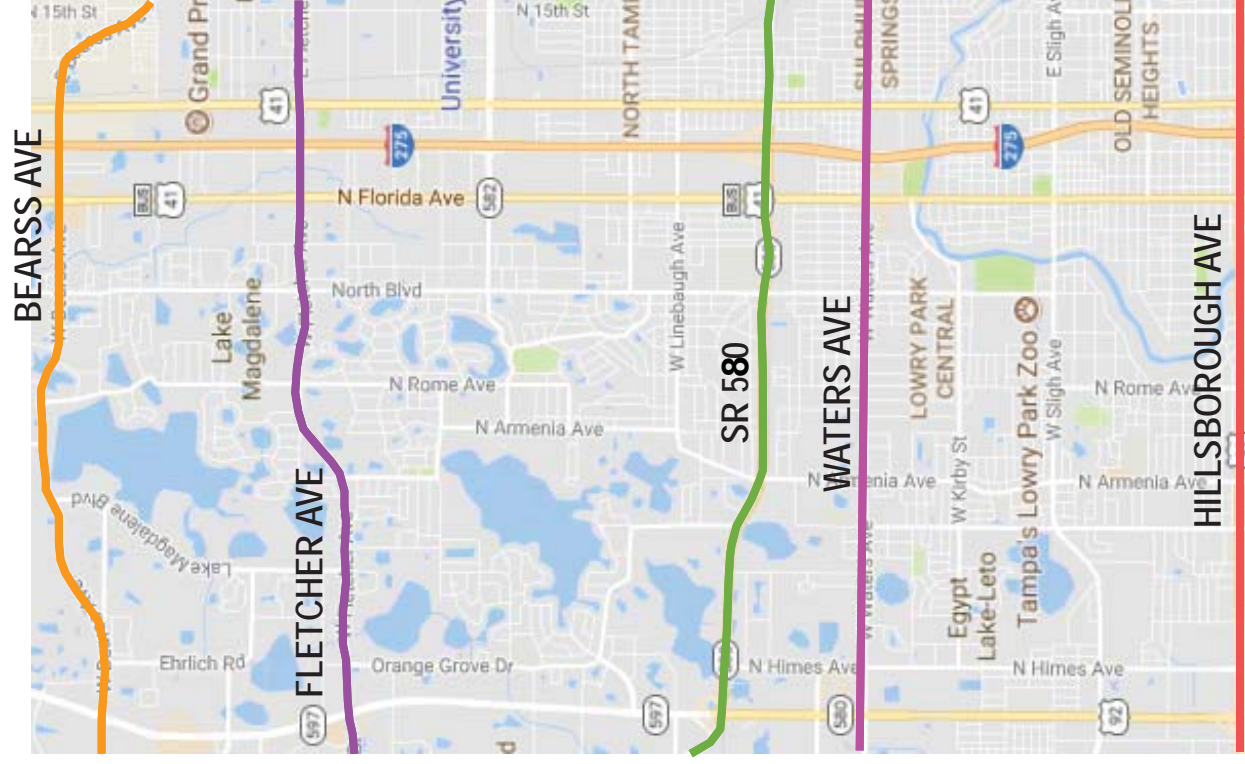
Adjacent East-West Corridor Considerations

Corridor	Lanes	AADT	Notes
Bearss Avenue	4-lane	54,000	3.8 miles north
Fletcher Avenue	4-lane	23,500	Limited western connectivity
SR 580	4-lane	50,000	
Waters Avenue	4-lane	27,000	Limited eastern connectivity
Hillsborough Avenue	6-lane	51,500	2.6 miles south

SR 580 Existing AADT

Minimum = 42,000

Maximum = 52,000



Roundabout Consideration

- Maximum design year total traffic volume entering an intersection is:
 - ≈ 25,000 AADT for single-lane roundabout
 - ≈ 45,000 AADT for two lane roundabout



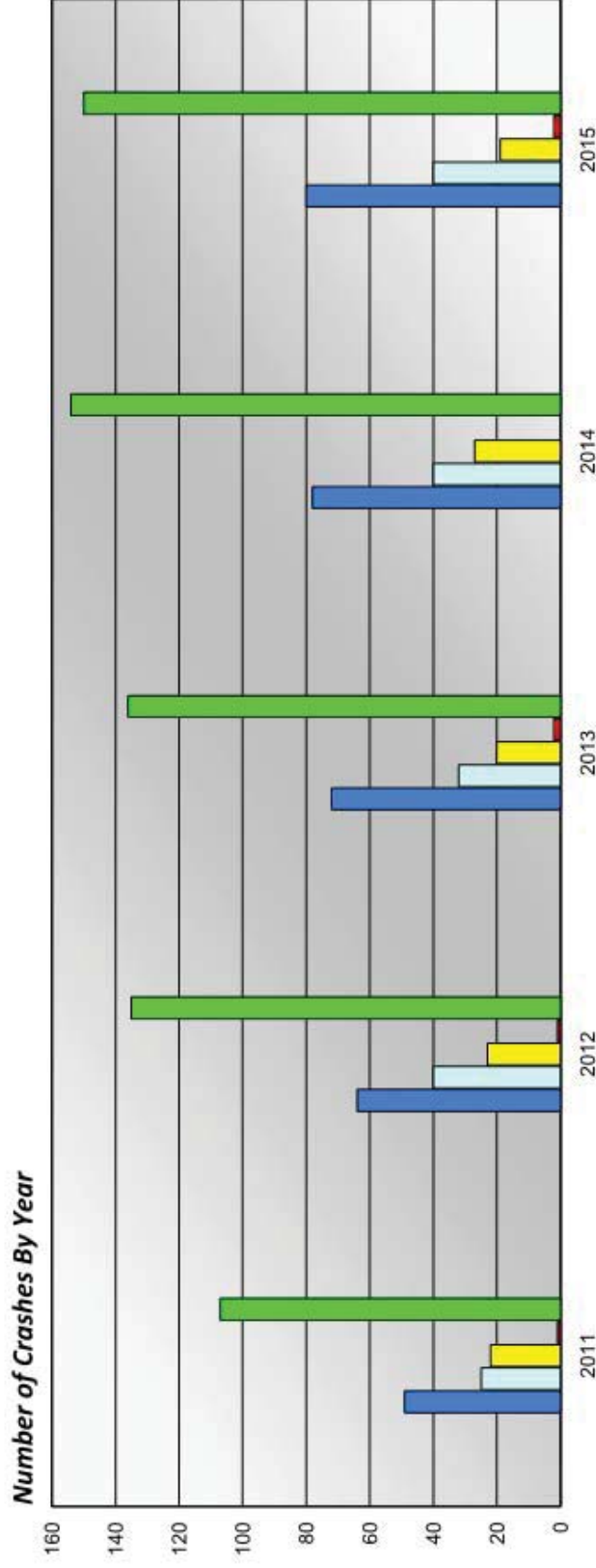
SR 580 Existing AADT
 Minimum = 42,000
 Maximum = 52,000

Intersection	Entering AADT (Existing)
Dale Mabry/SB Ramps	49,100
Dale Mabry/NB Ramps	58,000
Himes	57,500
Twin Lakes	54,900
Orange Grove	50,950
Armenia	58,500
North	51,000
Florida	75,000
I-275/SB Ramps	60,150
I-275/NB Ramps	58,150
Nebraska	60,900

Take Away from existing Counts

- Bike/Ped movement N-S exceeds E-W movement.
- Lane reduction not viable:
 - ≈ The existing traffic is either within or just above the range for 4-lane facility Traffic Volumes.
 - ≈ Adjacent corridors are already at or above capacity and too far away.
- Round-About configuration not viable: within SR 580 corridor exceeds the maximum traffic volume requirements for a round-about and impacts the RR on the south.

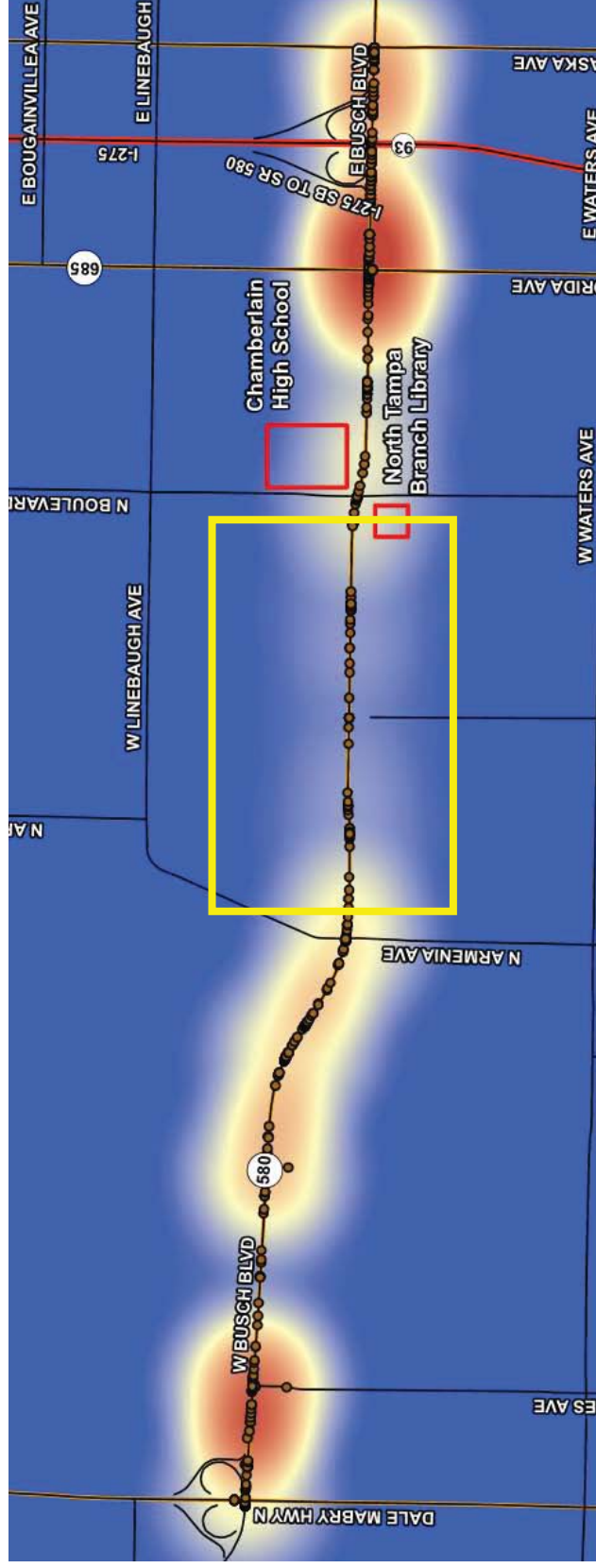
Latest Crash History by Year



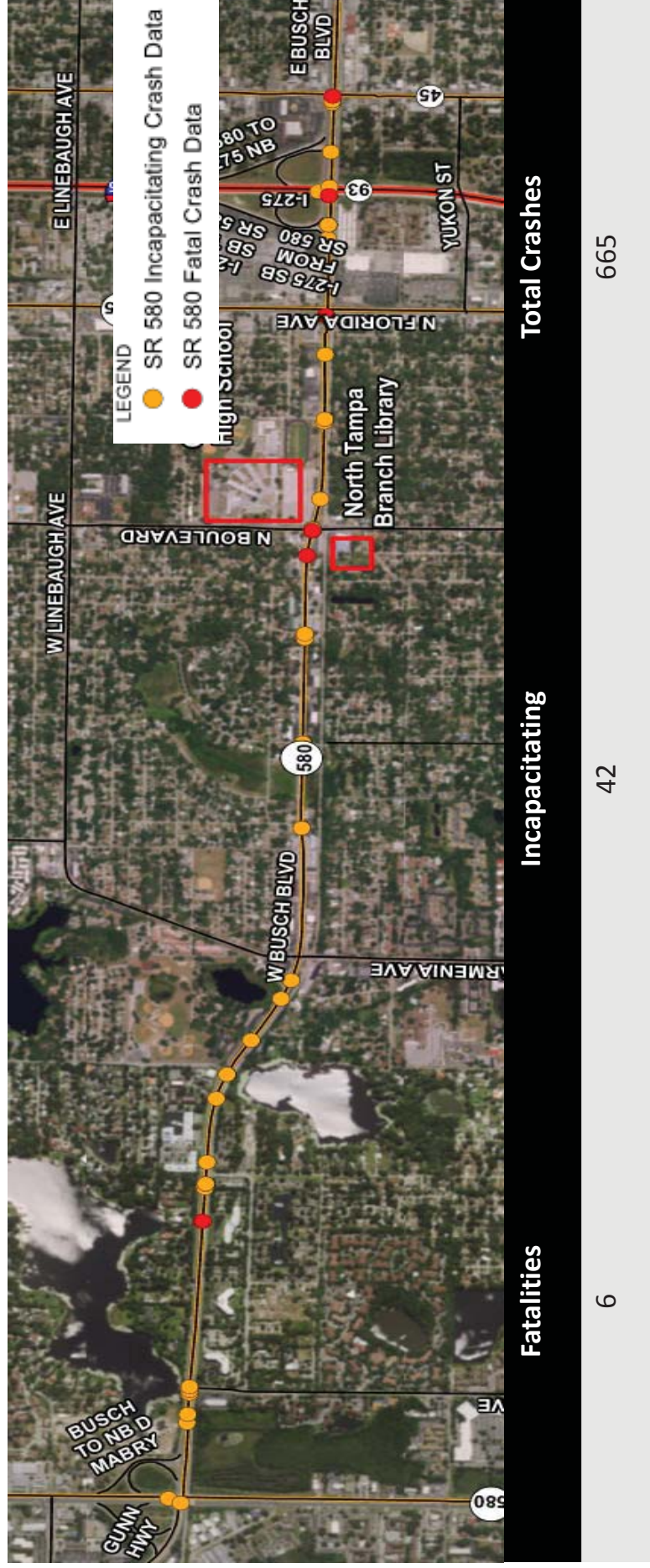
Breakdown of Crashes by Year

	2011	2012	2013	2014	2015
PDO	49	64	72	78	80
Injury Crashes	57	70	62	76	68
Fatal Crashes	1	1	2	0	2
Total Crashes	107	135	136	154	150

Crash-prone areas - heat map



Incapacitating and Fatal Crashes



Take Away from Crash History Data

- Crashes have increased.
- Highest “heat” intensity is at Himes and between North Blvd and Nebraska.
- The 5-lane section has a lower frequency and severity of crashes compared to the 4-lane section.
- Five of the six fatalities occurred between North Blvd and Nebraska

Developing the Corridor Vision



Kick-Off meeting response to: How
would you describe the West Busch
Boulevard corridor?



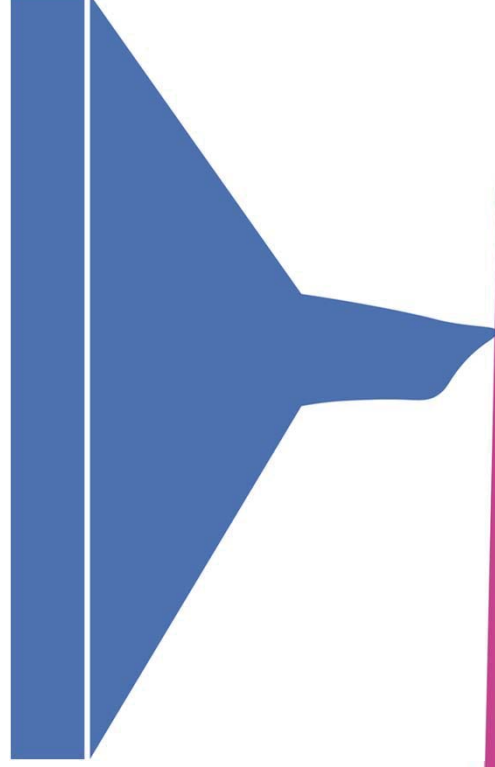
Kick-off meeting response to: Challenges & ideas for the corridor?



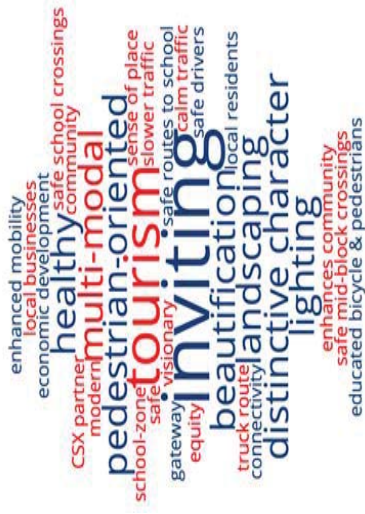
Activity - Goals & objectives to accomplish the vision

enhanced mobility
local businesses
economic development
healthy safe community
school crossings
pedestrian-oriented
sense of place
slower traffic
calm traffic
safe drivers
local residents
distinctive character
lighting
enhances community
safe mid-block crossings
educated bicycle & pedestrians

CSX partner
modern
pedestrian-oriented
school-zone
safe
gateway
visionary
equity
beautification
truck route
connectivity
pedestrian-oriented
sense of place
slower traffic
calm traffic
safe drivers
local residents
distinctive character
lighting
enhances community
safe mid-block crossings
educated bicycle & pedestrians



Activity – Begin process to identify the Vision for West Busch Blvd. Corridor



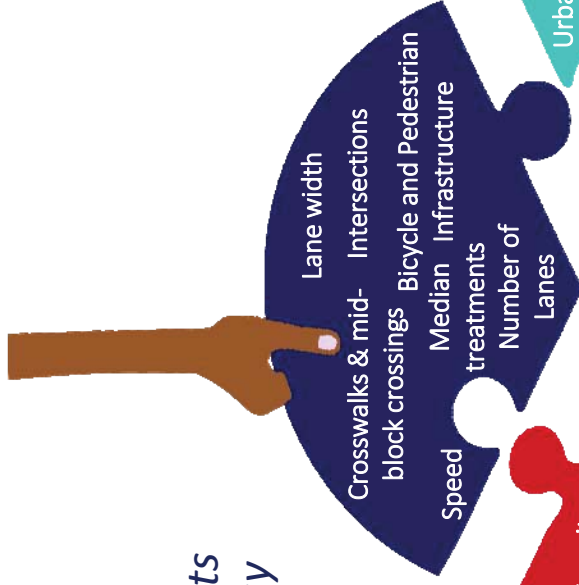
A safe and vibrant corridor that meets the mobility and connectivity needs of all users, regardless of their transportation mode of choice.

The corridor’s distinctive character is a source of pride for local residents and businesses, in addition to serving as an inviting gateway to Busch Gardens for visitors from near and far.

How do we get there?

FDOT

*invests in improvements
within the right-of-way*



Current & Future

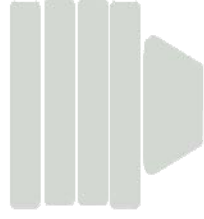
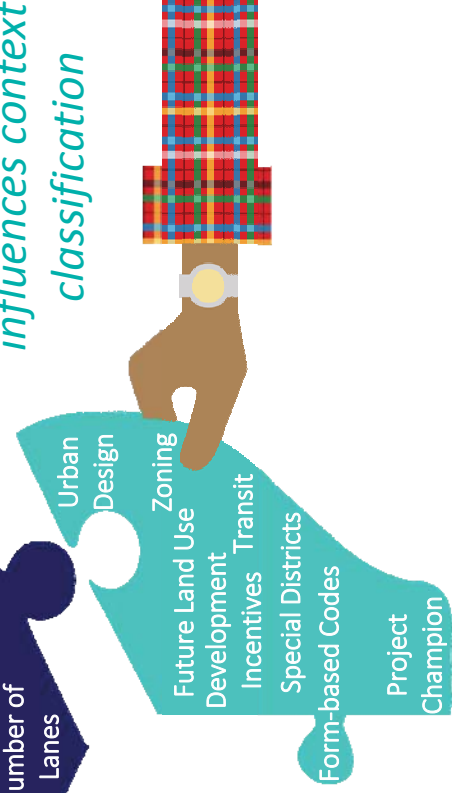
Corridor Users

define mobility needs



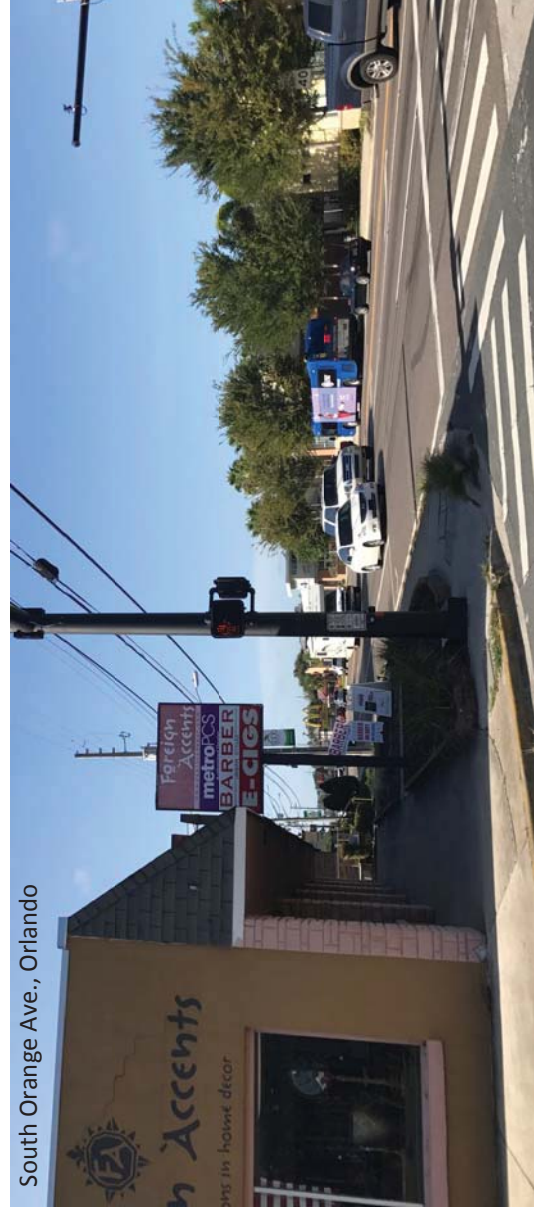
Local

Governments
*influences context
classification*



Vision for the future condition?

- Consensus that context classification will remain the same?
 - ≈ Urban form, setbacks and other land development policies can encourage pedestrian-scale environment

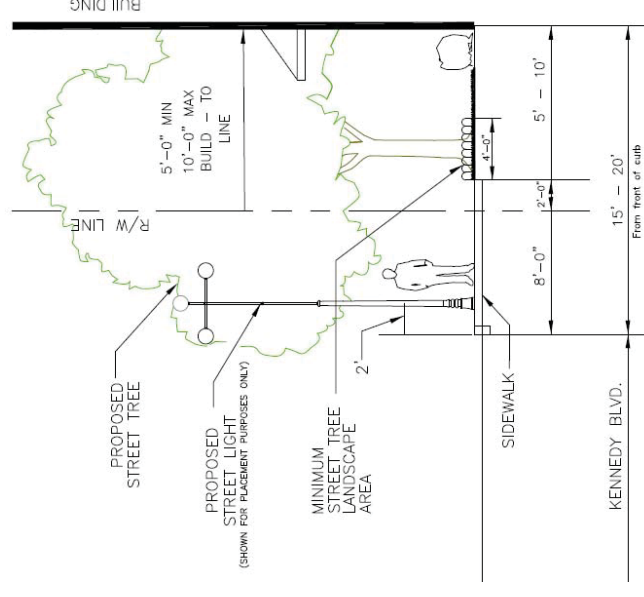


Note: City of Tampa Code of Ordinances Sec. 27-156 Table 4-2 requires 10 ft. front yard (setback) for properties zoned Commercial General,

Coordinated land development & redevelopment policies can complement roadway investments

- Any documented County &/or City form-based codes, overlay districts, right-of-way preservation plans, etc. for the study area to reinforce pedestrian-oriented urban form?
- Are any additional stakeholders needed to help understand this collective future?

SIDEWALK ADJACENT TO ROAD
CROSS SECTION
SCALE: 1" = 5'



Kennedy Blvd. Overlay District Streetscape Guidelines

Activity - Developing the West Busch Blvd User Preference Survey

Include?:

- Bike Lanes
- More/Wider Sidewalks
- On-street Parking
- Mid-block RRFB Crosswalk
- Street Trees
- Lower Speed Limit
- School Zone
- Pedestrian Scale Lighting
- Streetscape/Beautification
- Landscaped Median
- What else?

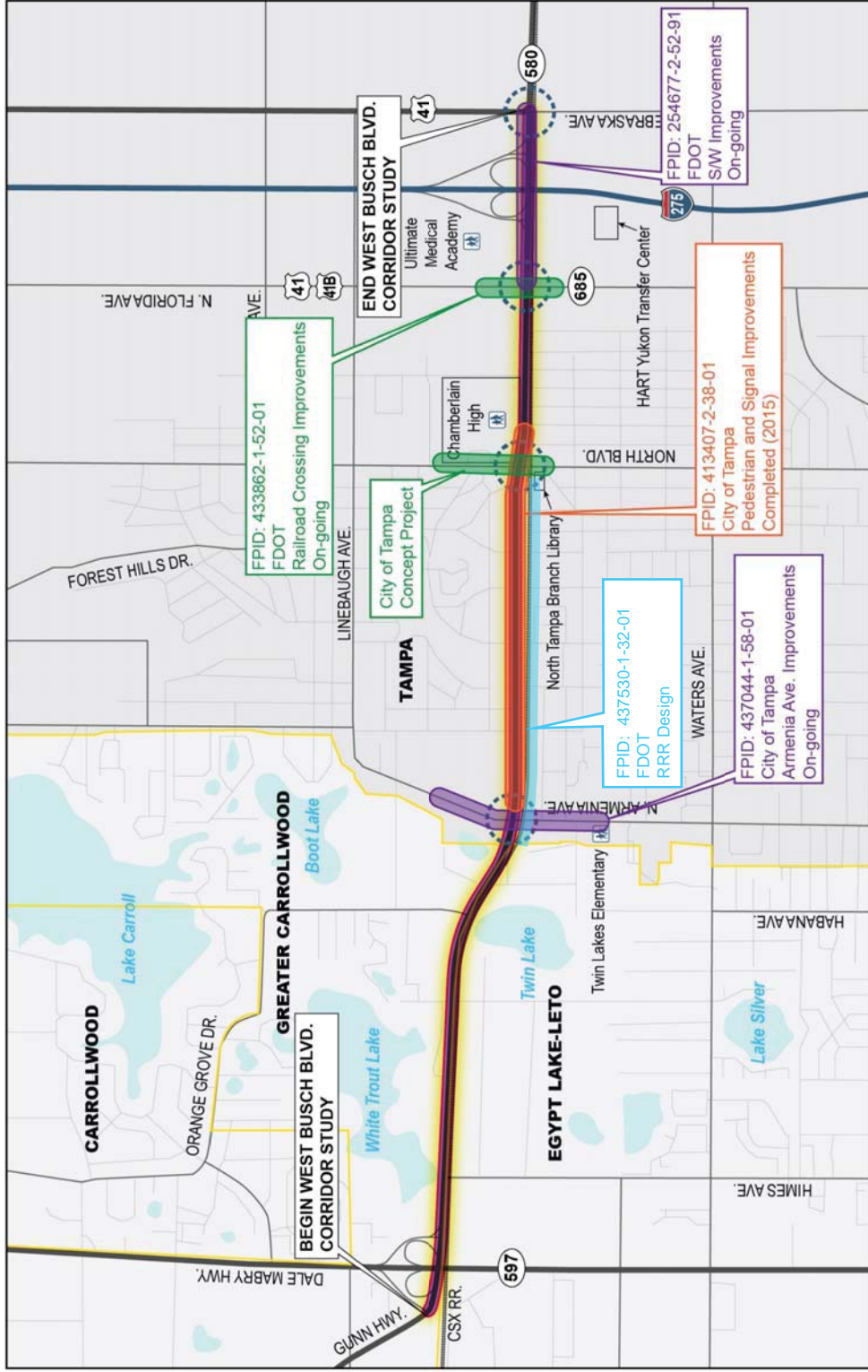
Don't Include:

- Reduced Lanes (Road Diet)
- Roundabouts
- Shared use path

Next Steps



Adjacent Project graphic and list



LEGEND

- Study Area
- Water Feature
- City Boundary
- School/College
- CSX Railroad
- Potential FDOT Lighting Project - Future



Related Projects Map

West Busch Blvd Corridor Study
 FPID: 435908-1-22-01





Next Steps for the Project Team

- Finalize Draft User Preference Survey
- Evaluation and documentation of improvement options
- Further development of the Visioning statement
- Stakeholder meetings and documentation

Next Steps for the PAG

- Discuss the project in your sphere of influence
- Direct interested parties to the website to provide comments
- Direct interested parties to the website to “pin” issues in the corridor using the WikiMapping interactive tool.
- Provide feedback to the project team regarding your conversations with interested parties.

Next Steps - Schedule

SR 580/Busch Boulevard from North Dale Mabry Highway to North Nebraska Avenue Corridor Study

PUBLIC INVOLVEMENT PLAN SCHEDULE

ACTIVITY	2017												2018							
	Mar	Apr	May	June	July	Aug	Sept	Oct	Nov	Dec	Jan	Feb	Mar	April	May	June	July	Aug	Sept	
Notice to Proceed - Project Begins	★																			
Develop Public Involvement Plan																				
Mailing List																				
Elected Officials/Agency Kick-off Meeting				★																
Project Advisory Group (PAG) Meetings																				
Corridor Visioning Workshop																				
Alternatives Public Meeting																				
Final Corridor Alternatives & Strategies Report																				

Questions???



Appendix D



PROJECT ADVISORY GROUP (PAG) MEETING #1

West Busch Boulevard (SR 580) Corridor Study

From North Dale Mabry Highway to North Nebraska Avenue

Financial Project ID Number: 435908-1-22-01



Tuesday, August 29, 2017 • North Tampa Branch Library, 8916 North Boulevard, Tampa, Florida 33604

Please provide your comments below. If more space is needed, please use the back of this sheet or an additional sheet of paper. You may place your comments in the "Comment Box" provided at the meeting, or send to Brian Shroyer, Project Manager at the address on the back of this comment form, by September 8, 2017. Emailed comments are also acceptable to Brian.Shroyer@dot.state.fl.us

*All comments are part of the project record and are available for review by the public and the media.

Any decisions/plans about changes along West Busch Blvd., from N. Dale Mabry Hwy to N. Nebraska Ave. in Hillsborough Cnty, need to consider the highest ease of use for patrons of the H. A. R. T. bus number 391 that runs on Busch Blvd. I live north of Busch Blvd on N. Nebraska Ave. at Boughavilla St. I use the bus transfer point at N. Nebraska Ave and Busch Blvd. and get off the bus at Busch Blvd and North Ave. so I can use the North Tampa library at 8916 North Blvd. Remember, bus patrons, like myself are on foot trying to avoid getting injured by inattentive motorists. I am a retired teacher. I would be delighted to serve as a senior citizen and H.A. R. T. bus patron in any focus group or public advisory council. Please contact me. This project is very important. I can help recruit members of the public for your effort to hear the voices of pedestrians and senior/disabled/Students who do not drive.

CONTACT INFORMATION (PLEASE PRINT)

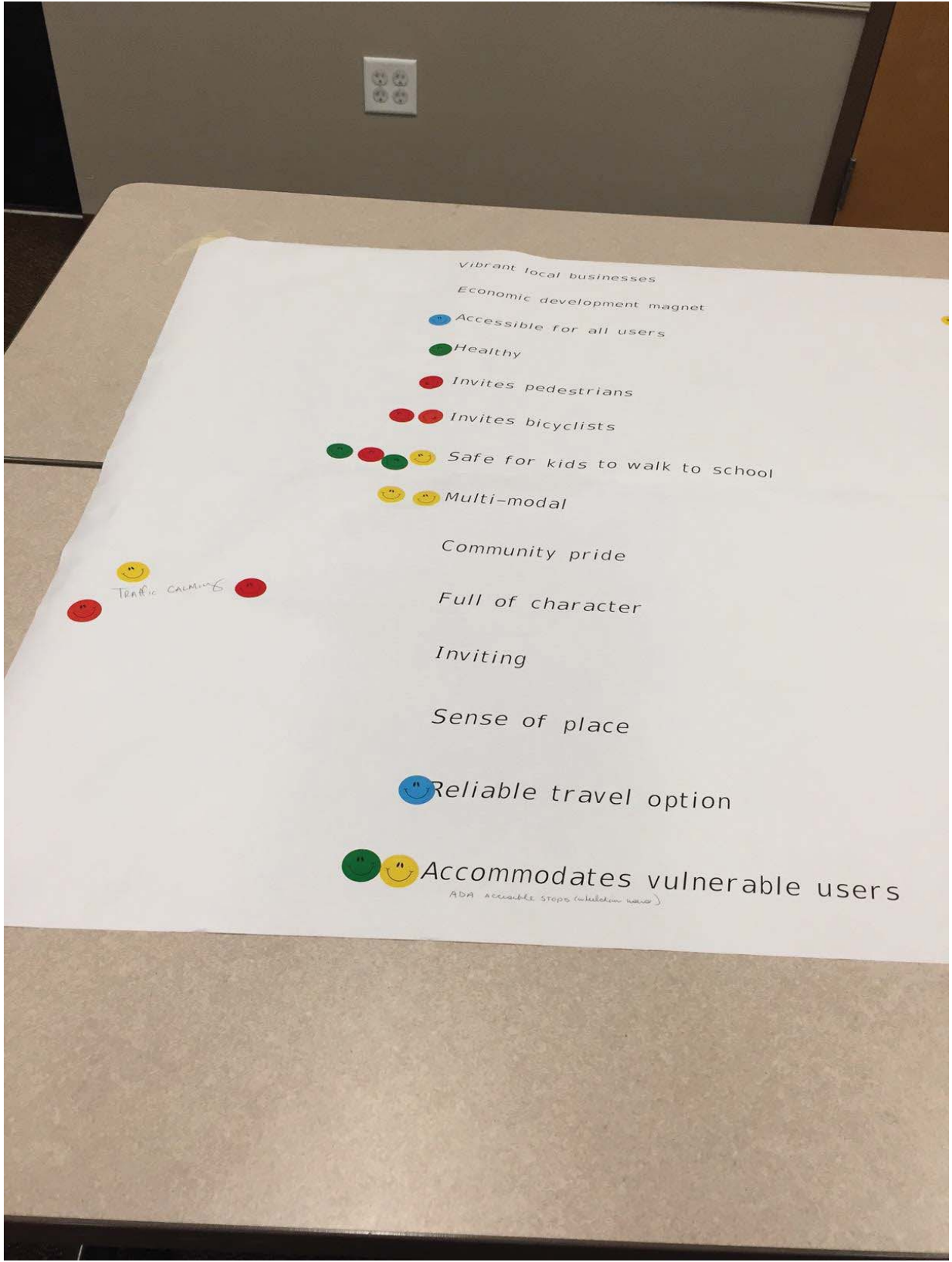
RETURN COMMENTS BY SEPTEMBER 8, 2017 TO:

Name Amy E. Clark
Title patron user of HART buses
Agency/Organization North Tampa Library
Phone Number 774-273-1544
Email quaker.lady@yahoo.com

Brian L. Shroyer
Multimodal Project Manager
Florida Department of Transportation
11201 N. McKinley Drive, MS 7-500
Tampa, FL 33612
Brian.Shroyer@dot.state.fl.us

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status.

Appendix E




vibrant local businesses





Economic development magnet



 Accessible for all users

 Healthy

 Invites pedestrians

  Invites bicyclists

    Safe for kids to walk to school


  Multi-modal

Community pride



Full of character

Inviting


Sense of place

 Reliable travel option

  Accommodates vulnerable users
ADA accessible stops (wheelchair users)

 Traffic Calming 

Accessible for Emergency Services


 Partnership with CSX to share space


 Good commuter route


Safely accommodates truck deliveries

 Landscaped corridor


 Safe conduit for cyclists

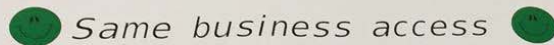
 Options to cross between signalized intersections

 Safe mid-block crossings


 School Zone is enforced so don't speed

Supports High School sports fans

 Lighting helps alert drivers to pedestrians at cross walks
Lighting for Bus stops

 Same business access

Left turn lanes are long enough

 No more two-way left turn lanes

Appendix F

DRAFT User Preference Survey
FPID 435908-1-22-01
SR 580 Corridor Study from Dale Mabry Hwy to Nebraska Ave

1. Please provide your zip code: _____
2. How do you use West Busch Blvd? Check all that apply
 - a. I live on or near the corridor
 - b. I work on or near the corridor
 - c. I travel through Busch Blvd corridor to get onto or off of Dale Mabry Hwy.
 - d. I travel through Busch Blvd corridor to get onto or off of I-275.
 - e. I drive through the corridor to get to work.
3. Do you own a car?
 - a. Yes
 - b. No
4. How do you move through the Busch Blvd corridor?
 - a. Walk
 - b. Ride a bicycle
 - c. Drive a car
 - d. Catch a ride with others
5. What are the biggest challenges you notice when you travel through the West Busch Blvd Corridor?

	Very Important	Somewhat Important	Not Important	No Opinion
Turning at signalized intersection				
Turning at non-signalized intersections				
Turning at Driveways				
Waiting for pedestrians to clear the intersection				
Access to businesses and other uses along the corridor				
Waiting for the train				
Motorist behavior (Speeding, Red Light Running, Aggressive behavior, etc.)				
Drivers using the middle dual-left turn lane to get around traffic				
Traffic Congestion/ Delays				
Pedestrians darting across traffic				

6. What do you think are the most important improvements the Project team should investigate?

	Very Important	Somewhat Important	Not Important	No Opinion
Bike lanes on Busch Blvd				
Sidewalks along both sides				
Quicker commute through corridor				
Safer crossing for pedestrians				
Mid-Block crossing between signalized intersections				
Slowing the traffic down				
Close the median to manage the access				
More lighting in the corridor				
School Zone established at Chamberlain HS				
Landscape along sidewalks				
Landscape in median				



SUPERFLEXIBLE EASELING
TARTELA FLEXIBLE PARA PIZARRA
SUPERFLEXIBLES FEUILLES MONLES SUPROCOLANTES
ELASTIC UNO BLOC DE NOJAS REPOSICIONABLES



30

30 B/P
18.2 cm x 7.62 cm
1.2 g/PT/PP (0.43 oz)

- ZIP CODE - HOME & WORK
- Q2 - ATTEND SCHOOL (ADD)
- ~~Q2~~ Q2 - COMBINE D & E?
- ~~Q2~~ DIRECT QUESTIONS TO NON-MOTORIZED
- HOW? MODE OF TRANSPORT?
- REMOVE Q3 → FOLLOW-UP - How SAFE DO YOU FEEL USING THIS MODE?
 COMFORTABLE, SAFE, CONVENIENT
- # 2 - WHY - LIVE, WORK, SHOP, ETC.
 ASK IF PASSENGER
- # 4 - HOW - WALK, TRANSIT, BIKE, CAR, CARPOOL
~~GET~~ BUS & SCHOOL BUS
 WHAT MAIN MODE THAT APPLY OF TRANSPORT
 CHECK ALL
 HART
- CH. HS - PARENT LEAK, PEACH JAR - COMMUNICATION
- CHALLENGES - 1 TO 5 SCALE - MOST TO LEAST
- ALL MODES IN Q5
- SHOULD WE PROHIBIT LEFT ONTO OLA (QUEUING)
- ADD OPEN-ENDED "COMMENT" FIELD

Project Advisory Group Meeting #2

Date: Tuesday, October 24, 2017
Time: 10:00 a.m. to 12:00 p.m.
Location: North Tampa Branch Library
8916 North Boulevard
Tampa, Florida 33604
Attendees: See Appendix A

Notification of Meeting

Meeting invitation letters were sent by email to elected officials, government officials, local agencies and other interested parties. The invitation letters included the meeting date, time and location, and a summary of the project.

A Florida Administrative Register (FAR) ad was published in Volume 43/199 on October 13, 2017. The Florida Department of Transportation posted meeting information on the FDOT Public Notices website.

Copies of the invitation letter, FAR ad and FDOT Public Notice website ad are included in Appendix B.

Meeting Handouts

A copy of the meeting PowerPoint, User Preference Survey and Newsletter #1 were distributed to the attendees. PAG participants were encouraged to discuss their thoughts and concerns regarding the corridor study.

A copy of the meeting handouts are available in Appendix C.

Attendee Comments

Two written comment forms were received. A copy of the comments received are included in Appendix D.

Summary

The following provides a summary of the items discussed at the meeting:

- Consultant Project Manager, Carol Barker, opened the meeting by welcoming new attendees and thanking attendees for their participation.

- Carol asked everyone, including the project team, to introduce themselves and describe their affiliations.
- Carol provided a project overview and explained the role of PAG.
- Attendees participated in a “What Have You Heard Exercise.” They were given the opportunity to discuss what they or their affiliation has heard about the study outside of the project meetings and distributed information. Topics can be found in Appendix (E).
- Carol guided the PAG through a PowerPoint presentation.
- Carol discussed upcoming PAG meetings and public meetings.
- Carol encouraged the attendees to visit the [FDOT project website](#) and [WikiMapping website](#).
- Carol provided closing remarks. The meeting ended at 12:00 p.m.

Discussion

Project Advisory Group (PAG) questions during discussion included:

Attendee: How many public meetings will there be?

Carol Barker: There will be a Visioning Workshop in November and an Alternatives Public Meeting in the spring of 2018.

Attendee: How are you going to communicate to the public about the meeting?

Carol Barker: We will be mailing out meeting notices and invitations to businesses and residents along the corridor as well as other major stakeholders. We can also email you the pdf of the invite to share.

Attendee: How many addresses will the meeting information be sent to?

Carol Barker: All property owners within 300 feet of the centerline of SR 580 will receive invites. I believe it is 1,000 or more people.

Attendee: Do both the tenants and owners of businesses (buildings) receive a meeting invitation?

Carol Barker: Yes, both will receive an invitation.

Attendee: When are you planning to present to the MPO committees?

Carol Barker: I'd like to get us through the Visioning Workshop in November, then we'll be able to look at dates.

Attendee: What does “visioning” refer to – just the look of the corridor or the actual solutions?

Carol Barker: It’s a nebulous thing. We’ve divided everything into categories such as landscaping, comfortability in moving along the corridor. We’re referring to both functionality and look.

Attendee: Have you been looking at health data, heat, and air quality – those sorts of things – along this corridor?

Carol Barker: No, those are not things we have looked at. A corridor study is different than a PD&E study, in which those things would be looked at.

Attendee: Can you supplement Level of Service (LOS) with Travel Time reliability?

Carol Barker: The FDOT practice is to use LOS.

Attendee: Can engineering solutions be brought to the public as possibilities instead of asking them right out to tell you what they want for this corridor?

Carol Barker: We understand what you’re saying, but we have to try not to use as much technical language when corresponding with the public, so it’s easier for them to tell us what they want.

Attendee: Can you consider more hands-on activities at the public meeting?

Carol Barker: Yes.

Attendee: Are you finding crash trends?

Carol Barker: There are some crashes involving young drivers, but a lot of what we’re seeing is indicative of a heavy volume corridor.

Attendee: Who maintains the signals on the corridor?

Carol Barker: Most of the signals are city maintained.

END OF MEETING SUMMARY

This meeting summary was prepared by Karen Harrell, Public Involvement Coordinator. If you feel clarifications are necessary, or if this differs from your understanding, please notify Karen by telephone at 352-257-1651 or by e-mail at Karen.Harrell@qcausa.com within (5) working days upon receipt of this summary.

Appendix A



PROJECT ADVISORY GROUP MEETING #2
West Busch Boulevard (SR 580) Corridor Study
 From North Dale Mabry Highway to North Nebraska Avenue
 Financial Project ID Number: 435908-1-22-01



Tuesday, October 24, 2017 from 10:00 am to 12:00 pm ■ North Tampa Branch Library, 8916 North Boulevard, Tampa, FL 33604

Project Advisory Group (PAG) Sign-In Sheet

NAME (PLEASE PRINT)	AGENCY / ORGANIZATION	TITLE	PHONE	EMAIL
KAREN MICHALSKI	SERRA CLUB		813-784-4081	Karen.Kaymicha1sk1@gmail.com
Lisa Silva	MPO			silva@placon.com.org
Pedro Parra	TPC		813-625-8020	parrap@placon.com.org
Jennifer Bartlett	PIOT			
Amy E. Clark	Public volunteer	retired teacher	774-273-5442	quaker_lady@yahoo.com
Willis Bowick	Tpa Bay Blk Chamber	President	813- 365-6612 352-4643	KCbowick2@hotmail.com
Wade Reynolds	MPO			en file
Johnny Wong	MPO			n file
AL WALTERS	HILLSboro			ALWA@AFL.com.org
Colvin Thant	OD	Engage	574-7084	Colvin.Thant@Tampabay.net
Huenele Ogilvie	Hillsborough MPO		813-873-3774	Ogilvie.m@placon.com.org



PROJECT ADVISORY GROUP MEETING #2
West Busch Boulevard (SR 580) Corridor Study
 From North Dale Mabry Highway to North Nebraska Avenue
 Financial Project ID Number: 435908-1-22-01

Staff



Tuesday, October 24, 2017 from 10:00 am to 12:00 pm ■ North Tampa Branch Library, 8916 North Boulevard, Tampa, FL 33604

Project Advisory Group (PAG) Sign-In Sheet

NAME (PLEASE PRINT)	AGENCY / ORGANIZATION	TITLE	PHONE	EMAIL
Jessica Francis	SCA		850-350-0107	
Brian L. Smoyer	FDOT	PROJECT MANAGER	813-915-6449	Brian.Smoyer@dot.state.fl.us
Trilite Rutwigale	FDOT	PM	978-6057	TRIYANAE.RUTWIGALE@
Chad Polk	CH2M	Project Engineer	813-281-7912	chad.polk@ch2m.com
Cris Schooley	CH2M	P&BE Engineer	407-354-2912	Cris.Schooley@CH2M.com
Mark Jennings	CH2M	Env. Planner	(813) 286-2177	mark.jennings@ch2m.com
Jody Woods	Chamberlain H.S.	Assistant Municipal	813-975-1677	jody.woods@sdhc.k12.fl.us
phray way	MPG			

Appendix B

Date: October 05, 2017

Subject: **Project Advisory Group (PAG) Meeting #2**
West Busch Boulevard (SR 580) Corridor Study
from North Dale Mabry Highway to North Nebraska Avenue
Hillsborough County, Florida
FDOT Financial Project ID Number: 435908-1-22-01

Dear Project Stakeholder:

The Florida Department of Transportation (FDOT), District Seven is conducting a Corridor Study on the above referenced study. This letter is an invitation for you to participate in the second Project Advisory Group (PAG) meeting for this project. PAG meeting #2 is scheduled for Tuesday, October 24, 2017, from 10:00 AM to Noon at the **North Tampa Branch Library, 8916 North Boulevard, Tampa, Florida 33604.**

The West Busch Boulevard Corridor study limits extend from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The study area includes portions of incorporated Hillsborough County and the City of Tampa. The purpose of the Corridor Study is to work with the community, stakeholders and a Project Advisory Group (PAG) to develop a vision and identify needs and issues in the corridor.

The PAG is an important component of the study. It serves as a special advisory resource to the FDOT and the consultant team. The PAG participants will help develop a vision for the corridor that will guide proposed short and long-term recommendations. This may include issues related to capacity, traffic operations, safety, access and egress, mobility, freight movements, transit, bicycles, and pedestrian movements.

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220 at least seven (7) working days in advance of the meeting.

If you have any questions about membership in the PAG or require further information concerning this project, please contact Brian Shroyer, Florida Department of Transportation Project Manager at 813-975-6449 or by email at Brian.Shroyer@dot.state.fl.us.

Sincerely,
Brian Shroyer
Multimodal Project Manager
Florida Department of Transportation

Florida Administrative Register Ad:

The Florida Department of Transportation, District Seven announces a public meeting to which all persons are invited.

DATE AND TIME: October 24, 2017, 10:00 AM to Noon

PLACE: North Tampa Branch Library, 8916 North Boulevard, Tampa, FL 33604

GENERAL SUBJECT MATTER TO BE CONSIDERED: The Florida Department of Transportation (FDOT), District Seven, is holding the second Project Advisory Group (PAG) meeting for the West Busch Boulevard (SR 580) Corridor Study. The limits of the project extend from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The corridor study area includes portions of unincorporated Hillsborough County and the City of Tampa.

The purpose of the Corridor Study is to work with the community, stakeholders and a Project Advisory Group (PAG) to develop a vision and identify needs and issues in the corridor. The PAG is an important component of the study. It serves as a special advisory resource to the FDOT and the consultant team. The PAG participants will help develop a vision for the corridor that will guide proposed short and long-term recommendations. This may include issues related to capacity, traffic operations, safety, access and egress, mobility, freight movements, transit, bicycles, and pedestrian movements.

A copy of the agenda may be obtained by contacting: Mr. Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability, or family status. Pursuant to the provisions of the Americans with Disabilities Act, any person requiring special accommodations to participate in this workshop/meeting, or persons who require translation services (free of charge) are asked to advise the agency at least 7 days before the workshop/meeting by contacting: Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220

Comuniquese Con Nosotros

Nos importa mucho la opinión del público sobre el proyecto. Si usted tiene preguntas o comentarios, o si simplemente desea más información, por favor comuniquese con nuestra representante, Lilliam Escalera, al (813) 975-6403, Departamento de Transportación de Florida, 11201 N. McKinley Dr., Tampa, FL 33612, Lilliam.Escalera@dot.state.fl.us

If you are hearing or speech impaired, please contact the agency using the Florida Relay Service, 1(800)955-8771 (TDD) or 1(800)955-8770 (Voice).

For more information, you may contact: Mr. Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT.

FDOT Public Notices Website:

PROJECT ADVISORY GROUP (PAG) MEETING

District: Seven

Meeting Type: Meeting

Date: October 24, 2017

Time: 10 AM to Noon

Location Name: North Tampa Branch Library

Street Address: 8916 North Boulevard

City: Tampa, FL 33604

Purpose: The Florida Department of Transportation, District Seven, is holding the second Project Advisory Group (PAG) meeting for the West Busch Boulevard (SR 580) Corridor Study. The limits of the project extend from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The corridor study area includes portions of unincorporated Hillsborough County and the City of Tampa.

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Primary Contact: Brian Shroyer, FDOT Project Manager

Primary Phone: (813) 975-6449

Primary E-mail: Brian.Shroyer@dot.state.fl.us

Additional Contact: Christopher Speese, Public Involvement Coordinator

Additional Phone: (813) 975-6405

Additional E-mail: Christopher.Speese@dot.state.fl.us

Appendix C

Project Advisory Group Meeting #2 West Busch Boulevard (SR 580) Corridor Study

from North Dale Mabry Highway to North Nebraska Avenue

Hillsborough County, Florida

FDOT Financial Project ID Number: 435908-1-22-01

North Tampa Branch Library

August 29, 2017 – 10:00 am to 12:00 pm



Agenda for Project Advisory Group Meeting #2

- Introductions
- Project Overview and Data Updates
- Overview of Visioning Workshop
- Next Steps
- Questions
- Adjourn

Introduction to PAG #2 Meeting





Introductions

- State your name
- Who do you represent
- Feedback on previous meetings
- Other items of interest for the PAG team.

Purpose of the SR 580 Corridor Study

- Define existing conditions
- Identify transportation-related needs
- Define acceptable levels of access and mobility
- Determine transportation system needs to support surrounding land uses
- Consolidate and control access points
- Identify safety concerns
- Identify operational deficiencies and promote operational efficiency

Your Role as a PAG Member

- Gather local input and then disseminate information back to the impacted stakeholders and communities to ensure that any improvements within the corridor continue to serve as vital elements of the community's transportation system
- Provide policy guidance and technical assistance to FDOT and the project team as the common goals and objectives of the study are defined, issues are identified, and potential solutions are developed



What have you been hearing?

- Have you heard any feedback from your agencies or local residents?

Goals for PAG #2

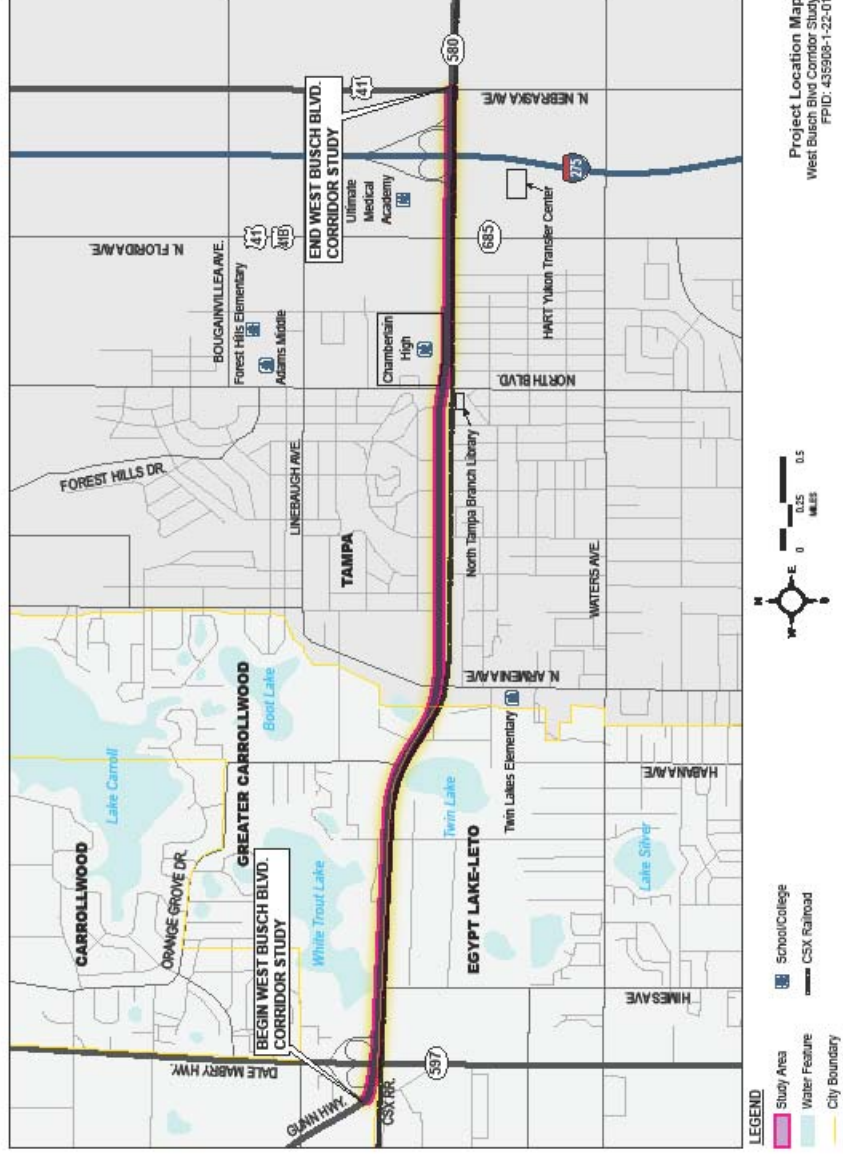
- Project Overview: Existing Characteristics and updated Future Conditions
- Feedback – information will be presented at Visioning Workshop November 16, 2017
- Request for even more feedback from your constituents.

Project Overview and Updated Data

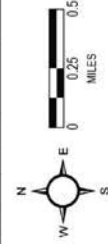
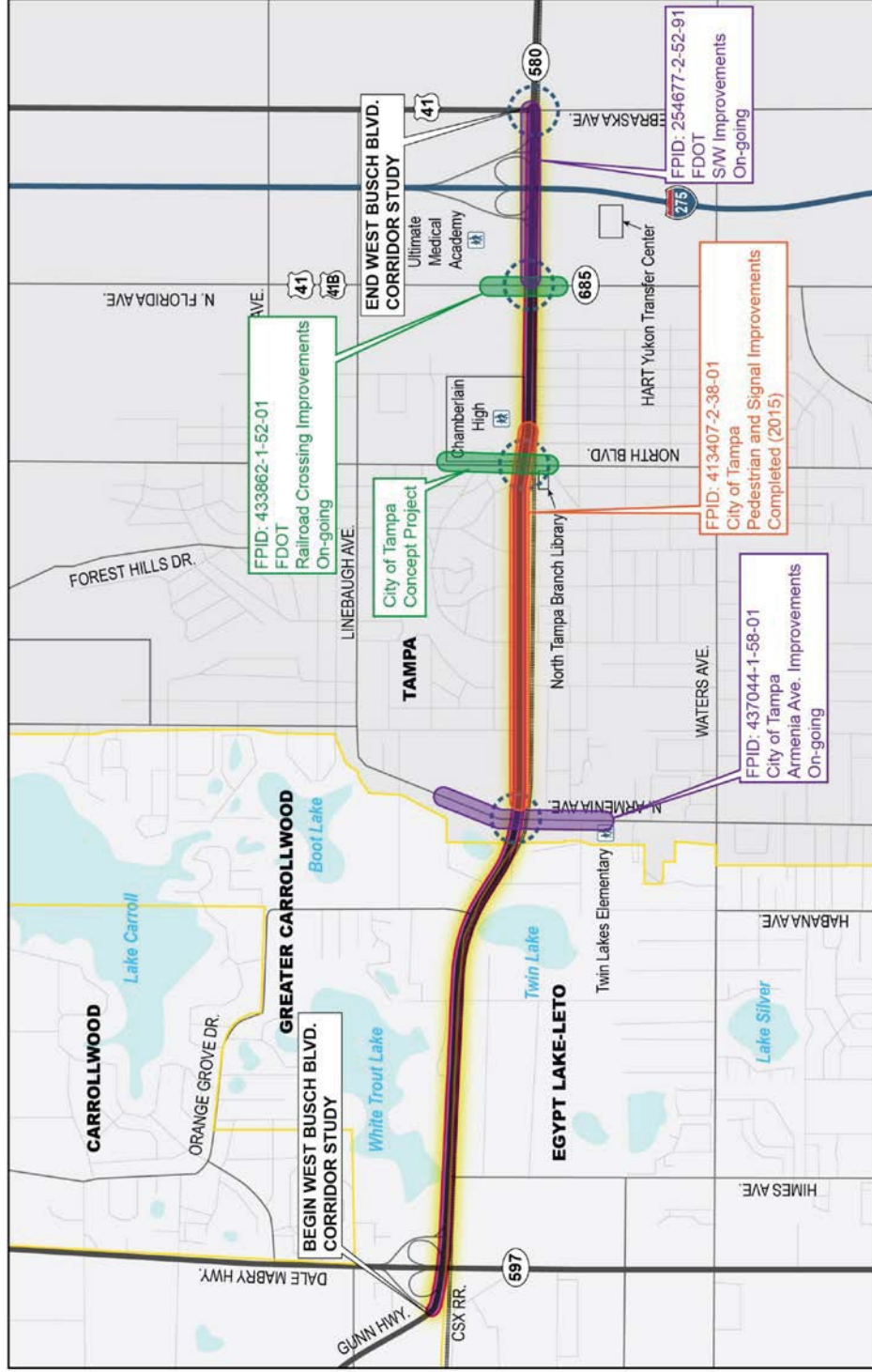


Project Study Area

- 3.3 miles
- Within the city of Tampa and unincorporated Hillsborough County



Adjacent Projects



- LEGEND**
- Study Area
 - Water Feature
 - City Boundary
 - School/College
 - CSX Railroad
 - Potential FDOT Lighting Project - Future

Related Projects Map
 West Busch Blvd Corridor Study
 FPID: 435908-1-22-01



Project Information - FDOT's context classifications

Street Connectivity

- Block Length
- Block Perimeter
- Intersection Density

Development Form and Intensity

- Building Placement
- Presence of Fronting Uses
- Location of Off-Street Parking
- Land Uses
- Building Height



C1-Natural C2-Rural C2T-Rural Town C3R-Suburban Residential C3C-Suburban Commercial E-163 C4-Urban General C5-Urban Center C6-Urban Core

Existing context classification

C3R-SUBURBAN RESIDENTIAL



Mostly **residential** uses within large blocks and a disconnected or **sparse roadway network**.

C4-URBAN GENERAL



Mix of uses set within small blocks with a **well-connected roadway network**. The roadway network usually **connects to residential neighborhoods** immediately along the corridor or on the back side of blocks fronting the roadway.

C3C-SUBURBAN COMMERCIAL



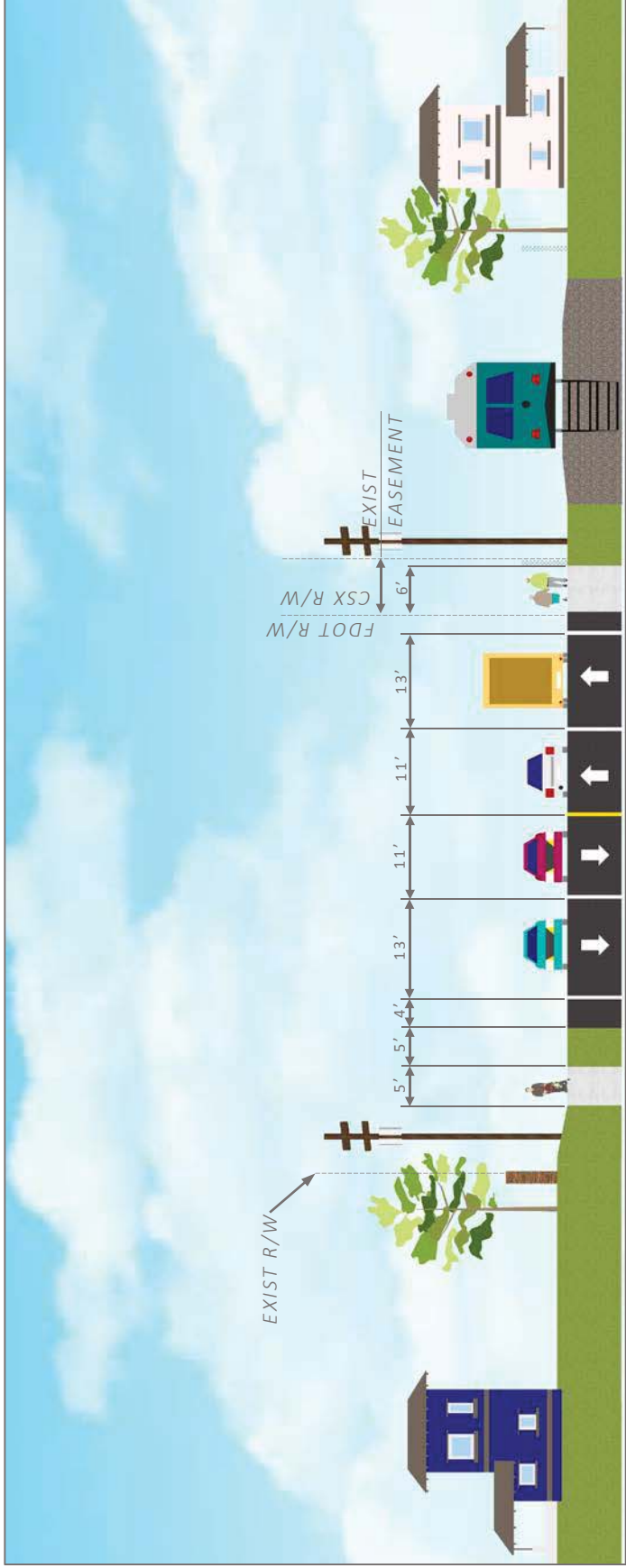
Mostly **non-residential** uses with **large building footprints** and **large parking lots** within large blocks and a disconnected or **sparse roadway network**.

C3R-Suburban Residential – Dale Mabry Hwy to Armenia Ave.

C3R-SUBURBAN RESIDENTIAL

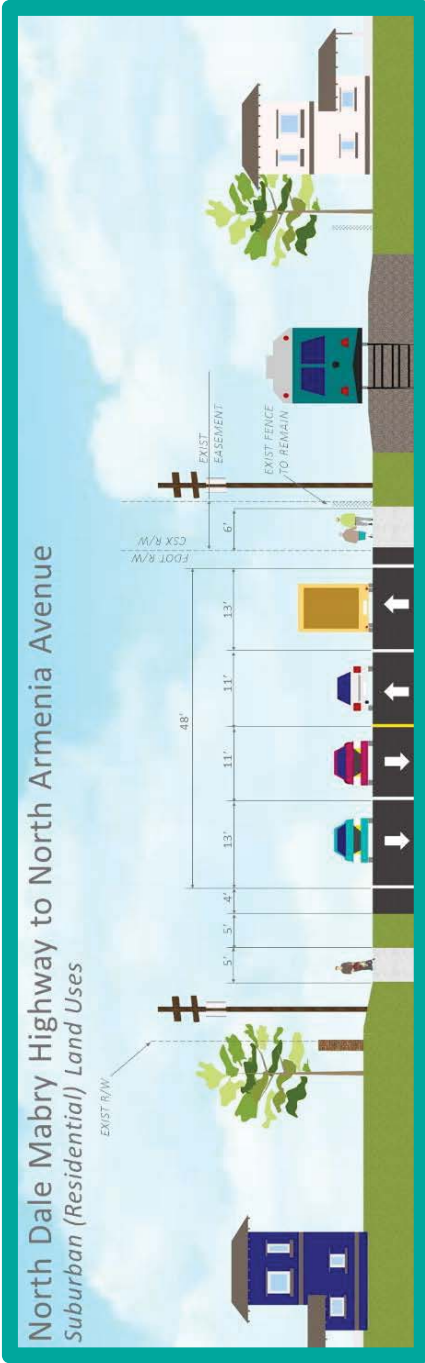


Mostly **residential** uses within large blocks and a disconnected or **sparse roadway network**.



E-165

Existing Conditions Dale Mabry Highway to North Armenia Avenue



North Dale Mabry Highway to North Armenia Avenue
Suburban (Residential) Land Uses

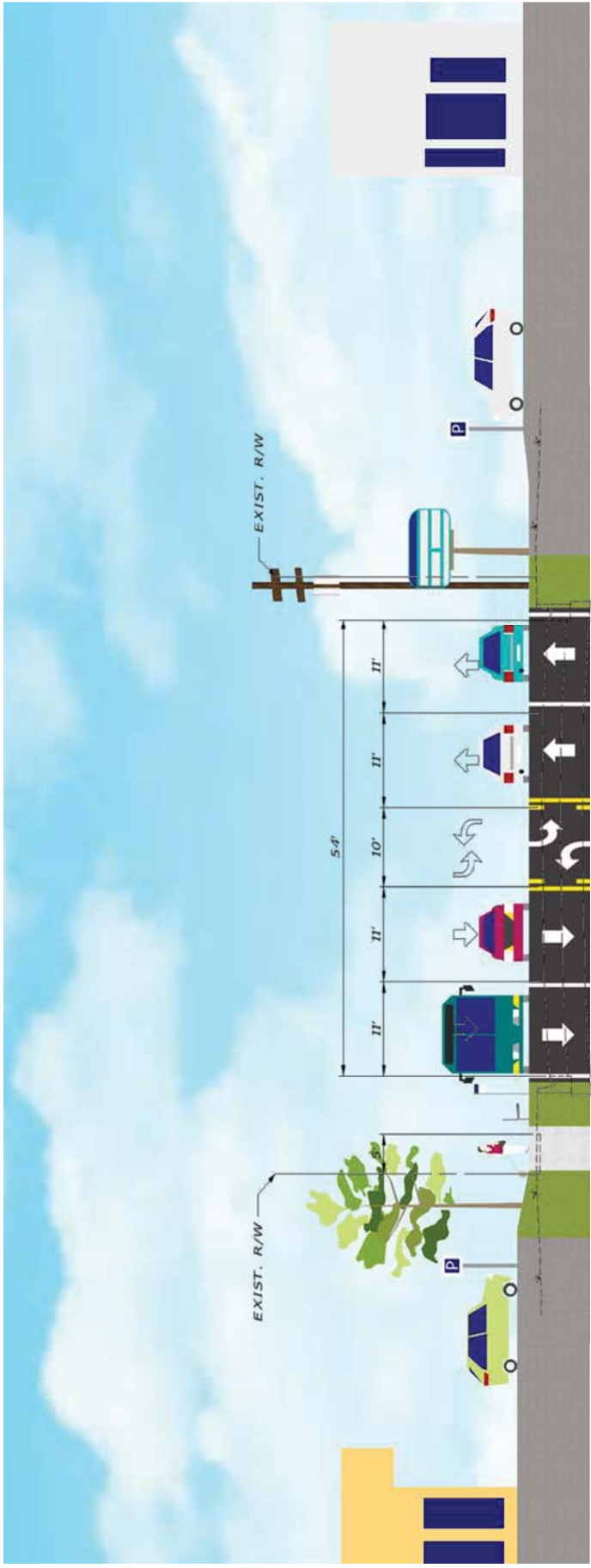


C4-Urban General – Armenia Ave to North Blvd



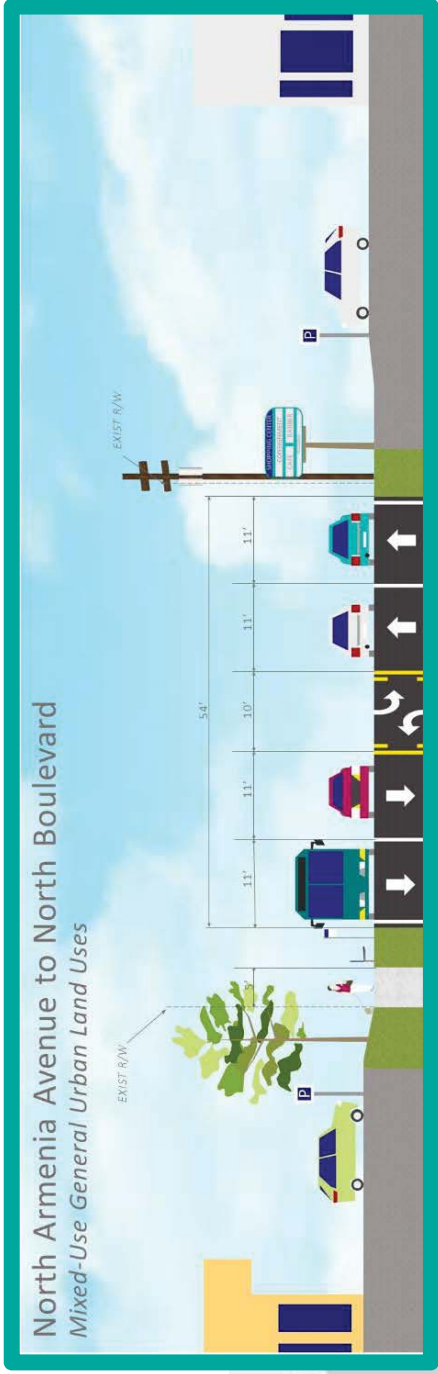
Mix of uses set within small blocks with a **well-connected roadway network**. The roadway network usually **connects to residential neighborhoods** immediately along the corridor or on the back side of blocks fronting the roadway.

C4-URBAN GENERAL



E-167

Existing Conditions North Armenia Avenue to North Boulevard



The map shows a street grid with W Busch Blvd highlighted in yellow. Other streets shown include North Boulevard, Armenia Ave, Florida Ave, Nebraska Ave, Dale Mabry Hwy, Himes Ave, Twin Lakes Blvd, and W Waters Ave. The map also shows various landmarks and green spaces.

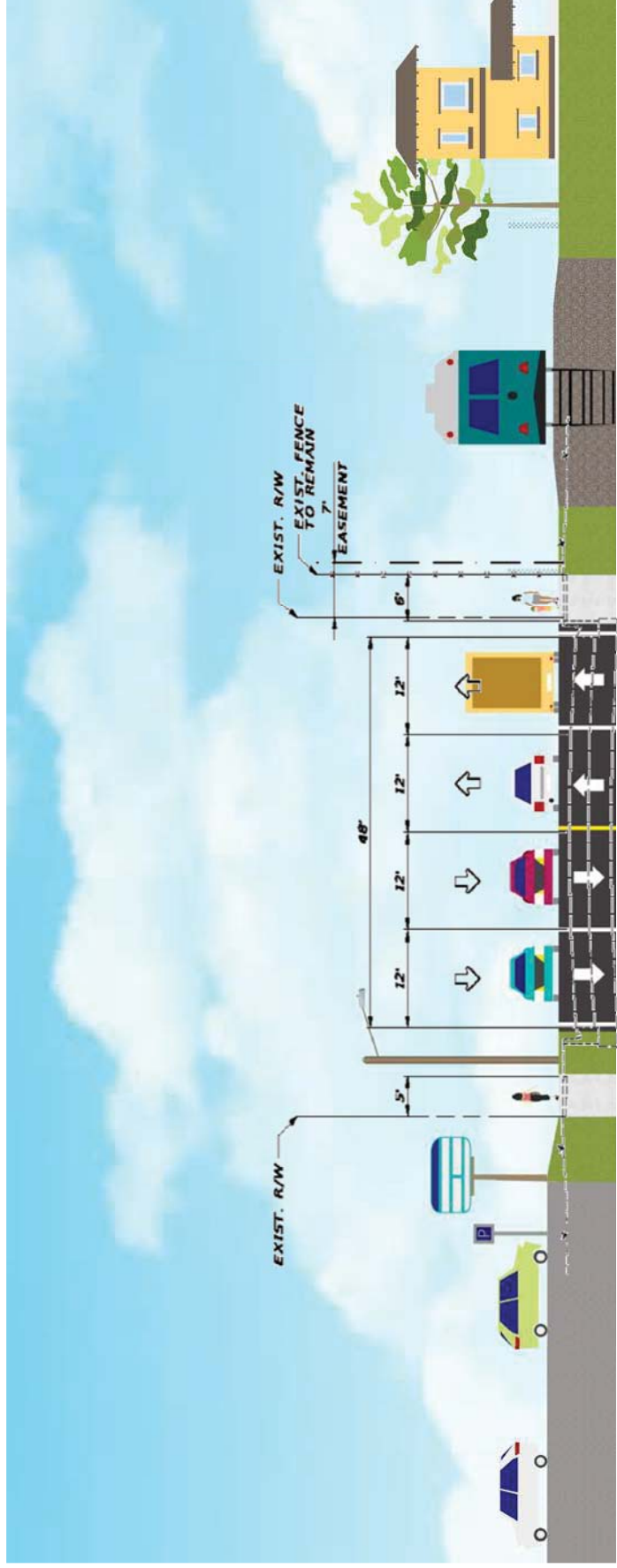
C3C Suburban Commercial – North Blvd to N Nebraska Ave.

C3C-SUBURBAN COMMERCIAL

Section 1: North Blvd to Florida

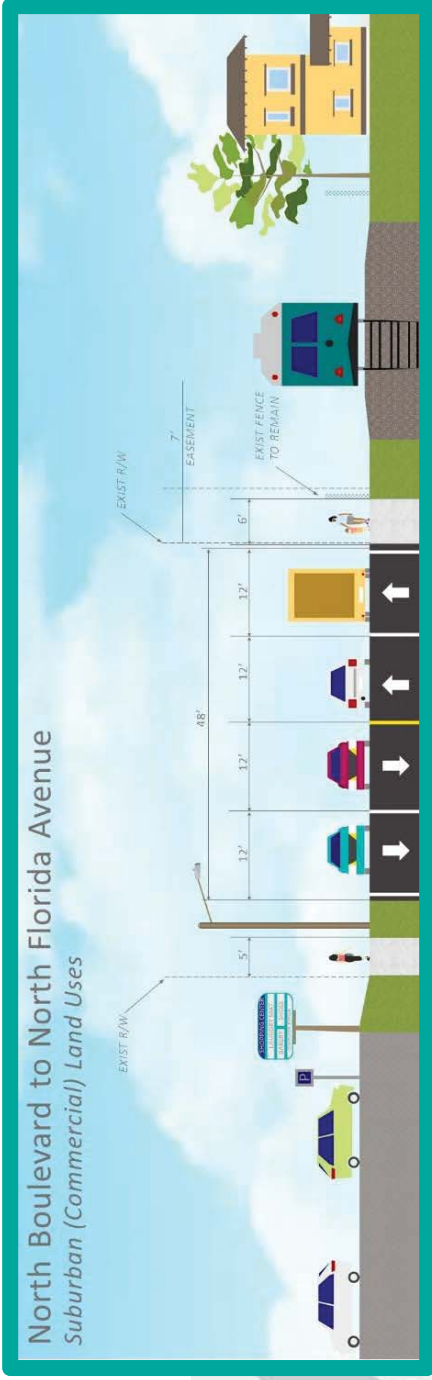


Mostly non-residential uses with large building footprints and large parking lots within large blocks and a disconnected or sparse roadway network.



E-169

Existing Conditions North Boulevard to North Florida Avenue



The map shows a street grid with W Busch Blvd highlighted in yellow. Key streets labeled include:

- DALE MABRY HWY
- TWIN LAKES BLVD
- HIMES AVE
- ARMENIA AVE
- W WATERS AVE
- NORTH BLVD
- FLORIDA AVE
- NEBRASKA AVE



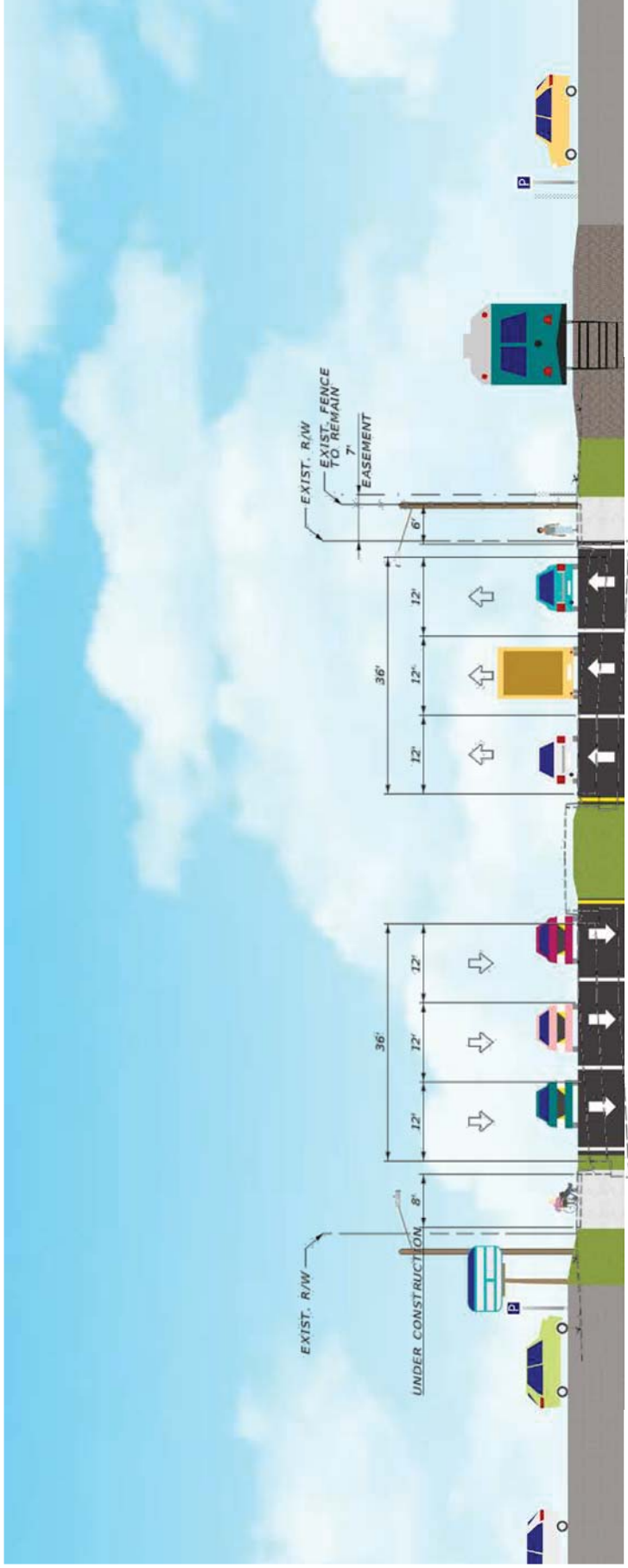
C3C Suburban Commercial – North Blvd to N Nebraska Ave.

C3C-SUBURBAN COMMERCIAL

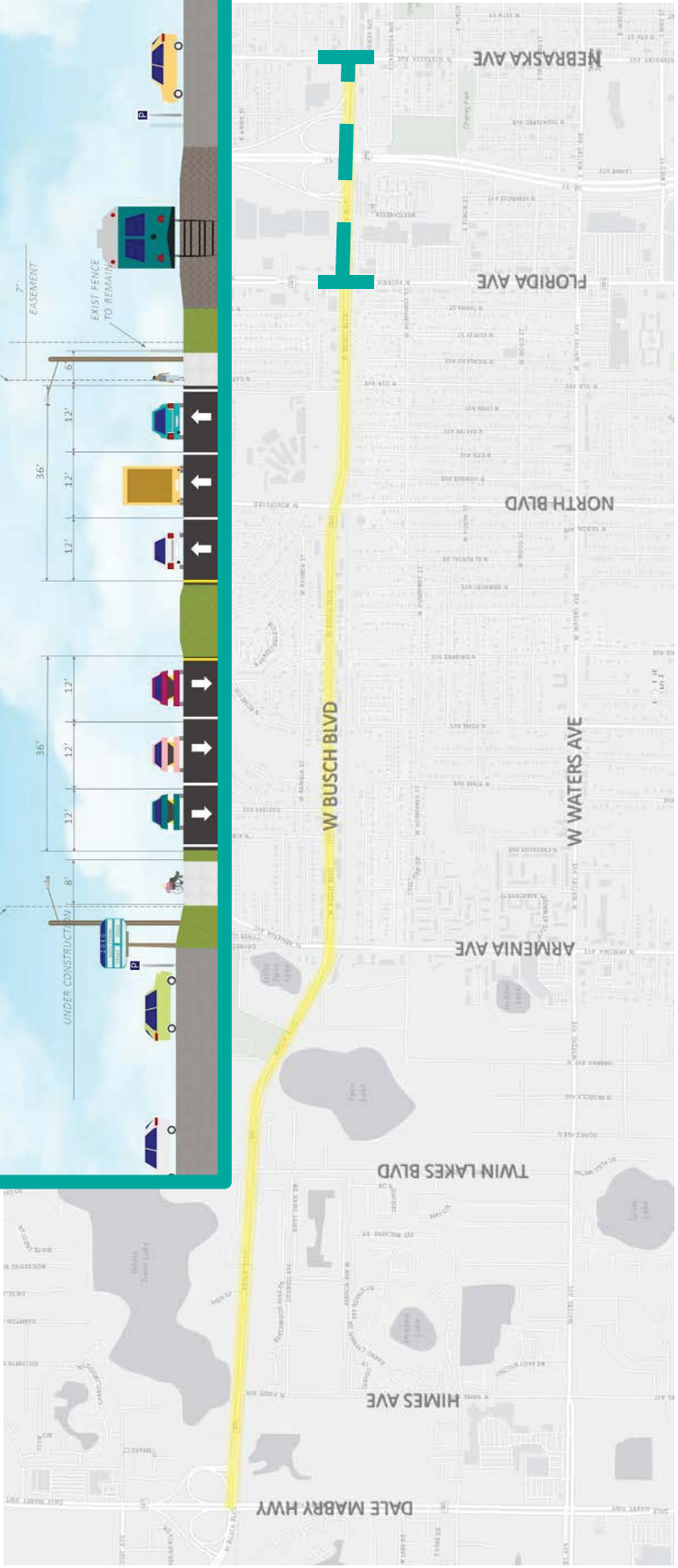
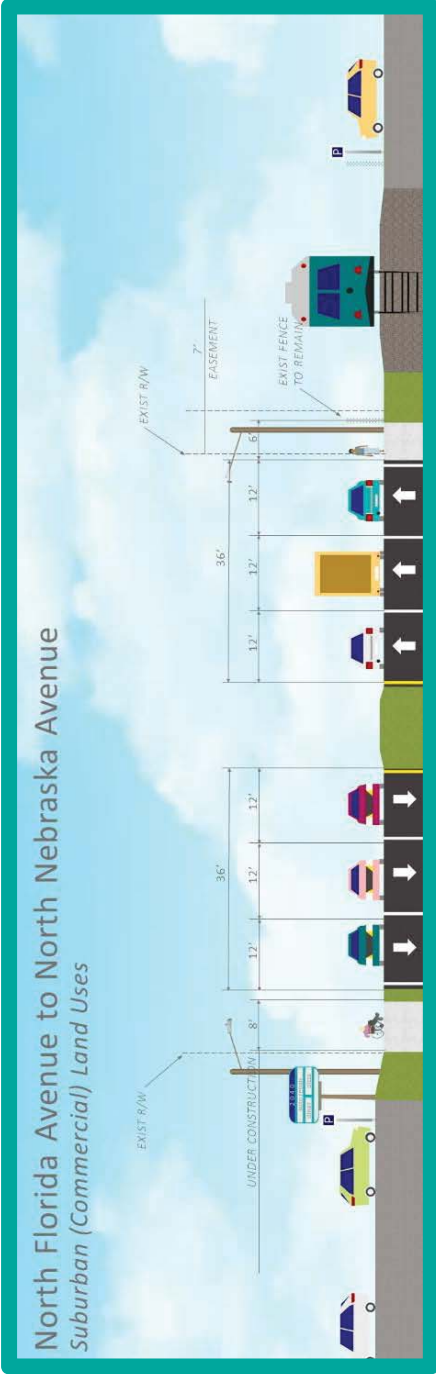
Section 2: Florida to Nebraska



Mostly non-residential uses with large building footprints and large parking lots within large blocks and a disconnected or sparse roadway network.



Existing Conditions North Florida Avenue to North Nebraska Avenue

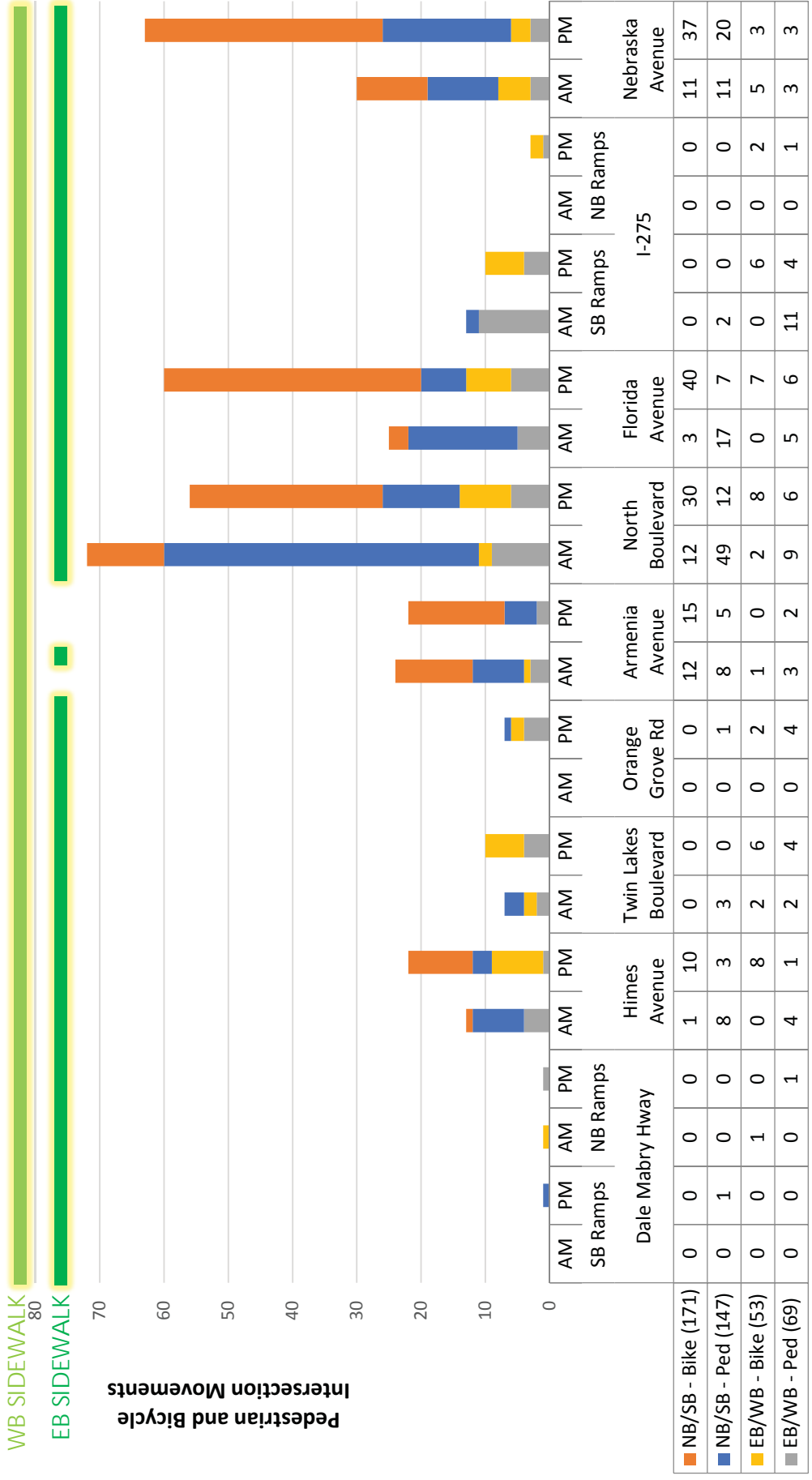


Take Away from Existing Context Classification information

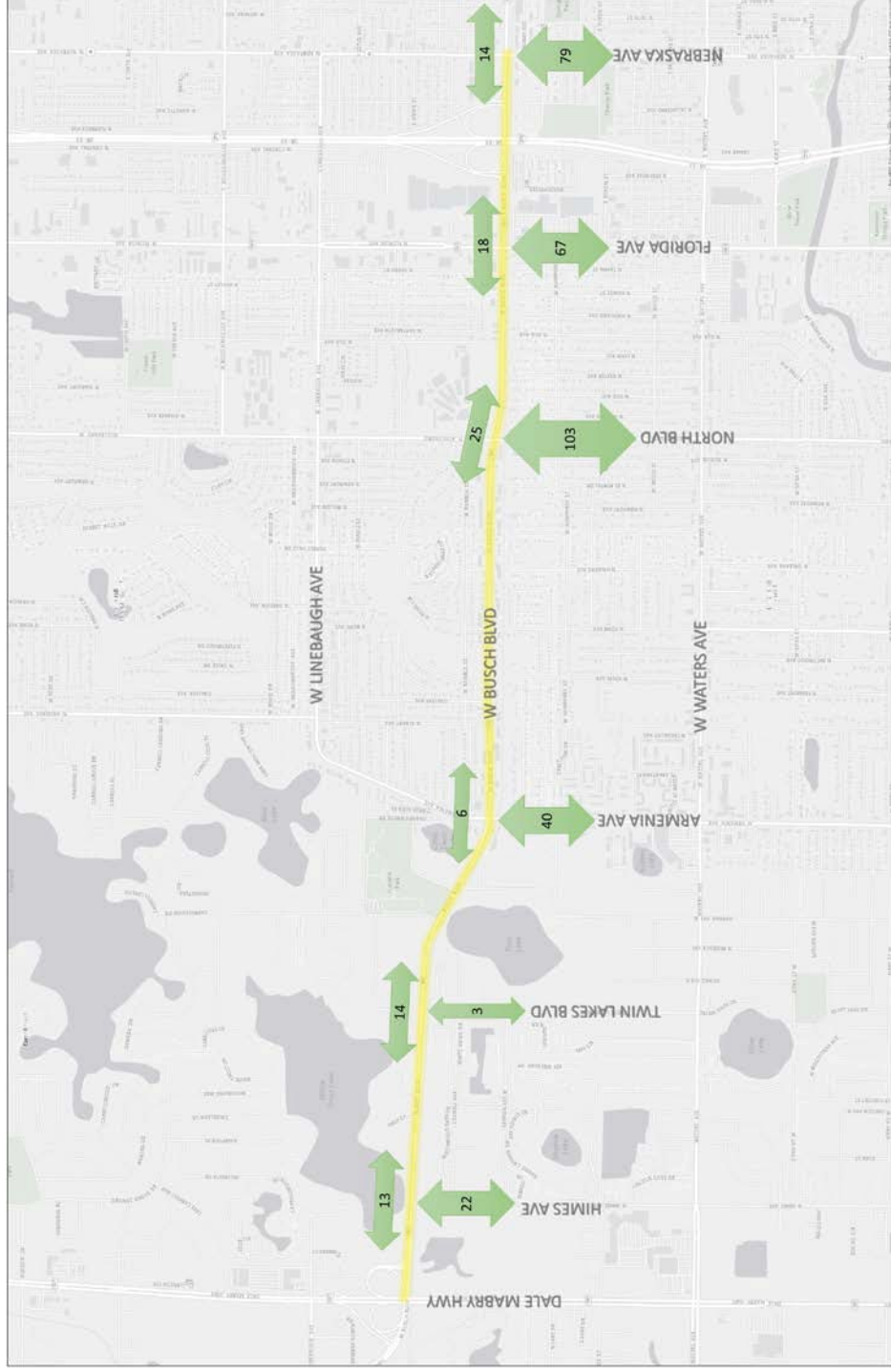
- No bike lanes throughout corridor
- Sidewalk gaps on South side of SR 580
- There is latitude in Context Classifications
- Projects are evaluated on FUTURE context

Pedestrian/Bicycle Counts

This slide illustrates the locations with the heaviest bike/ped traffic (based on peak 2-hour counts [1 day only] – from VHB AADT memo)

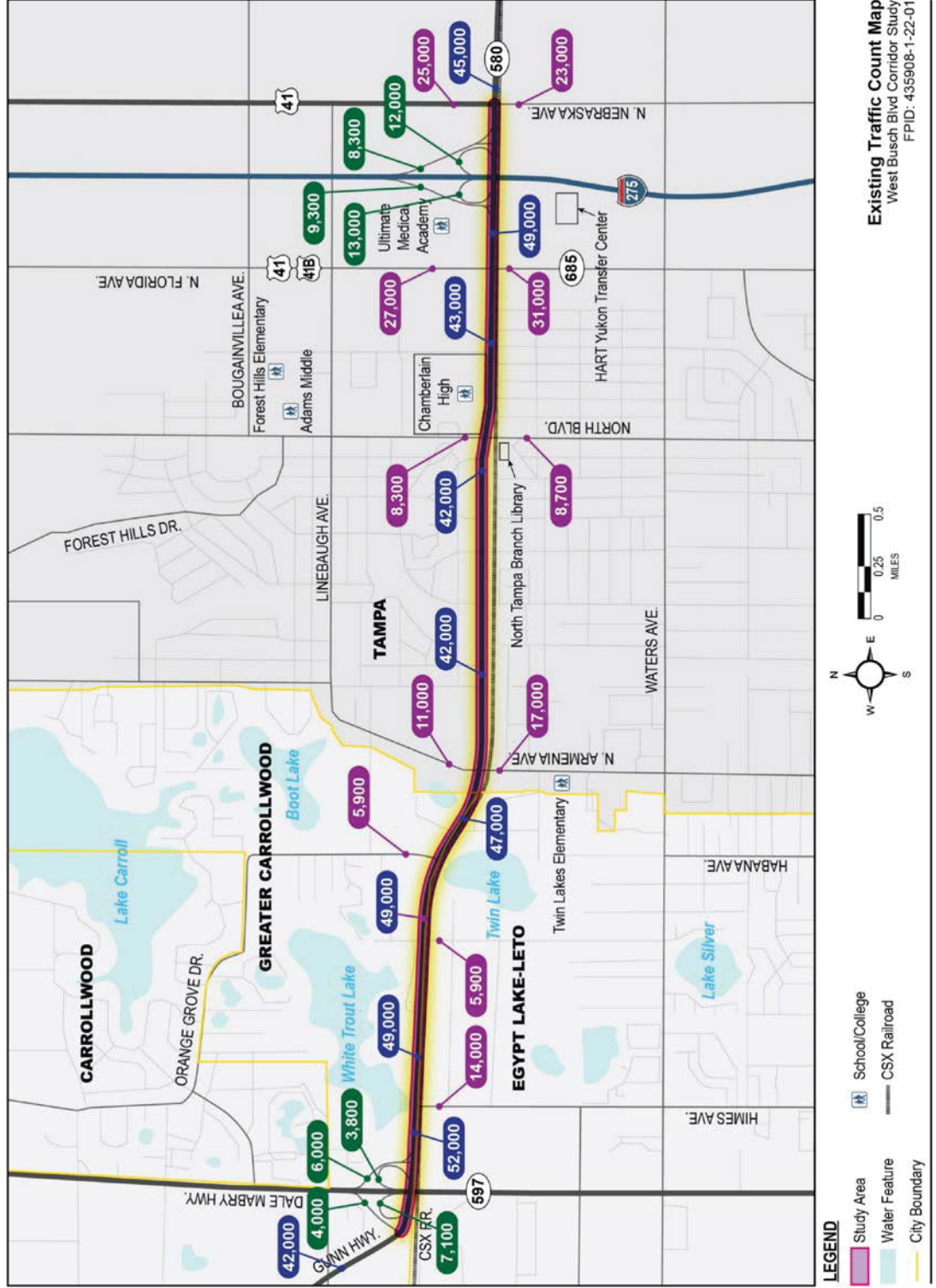


Existing Conditions Bicycle and Pedestrian Counts



Bike/Ped Counts Map (7-9am & 4-6pm Thursday May 4, 2017)

Existing Traffic Counts



Traffic Volume Considerations

STANDARDS FOR LOW AND HIGH VOLUME HIGHWAYS IN ANNUAL AVERAGE DAILY VOLUMES

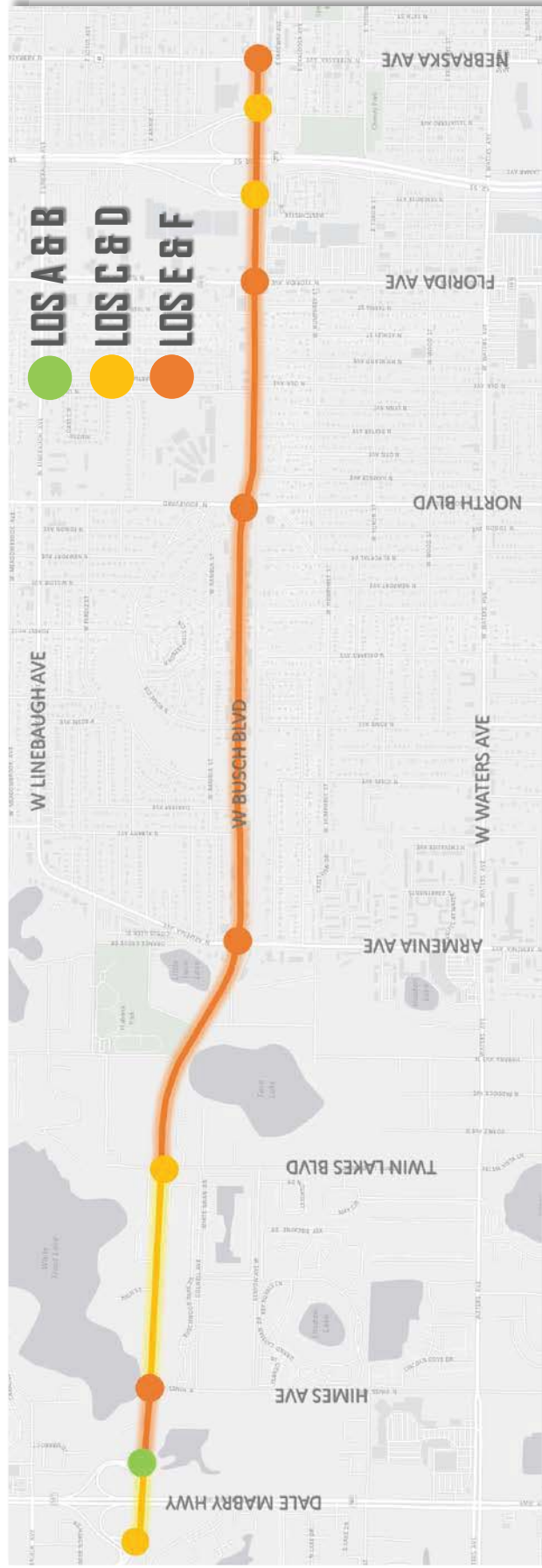
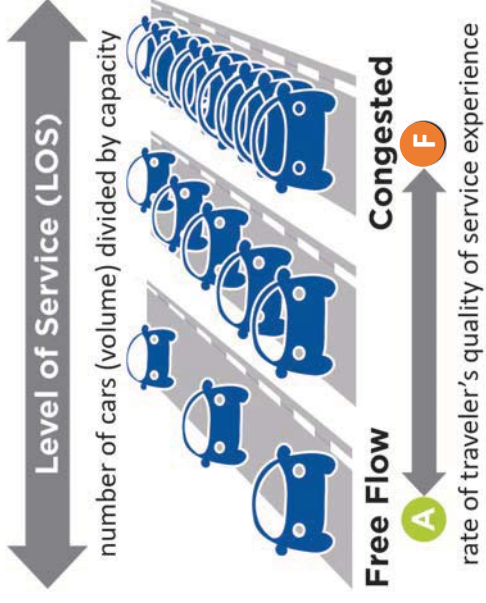
HIGHWAY TYPE	LOW VOLUME AADT	HIGH VOLUME AADT
FREEWAY - URBAN		
4-LANE FACILITY	57,000	69,000
6-LANE FACILITY	86,000	103,000
8-LANE FACILITY	114,000	138,000
FREEWAY - RURAL		
4-LANE FACILITY	46,000	56,000
6-LANE FACILITY	69,000	83,000
8-LANE FACILITY	92,000	111,000
ARTERIALS - URBAN		
2-LANE FACILITY	16,000	20,000
4-LANE FACILITY	37,000	43,000
6-LANE FACILITY	55,000	64,000
8-LANE FACILITY	69,000	80,000
ARTERIALS - RURAL		
2-LANE FACILITY	9,000	14,000
4-LANE FACILITY	38,000	47,000
6-LANE FACILITY	58,000	71,000
COLLECTOR - URBAN		
2-LANE FACILITY	11,000	16,000
4-LANE FACILITY	37,000	45,000
COLLECTOR - RURAL		
2-LANE FACILITY	8,000	13,000
4-LANE FACILITY	30,000	38,000

SR 580 Existing AADT

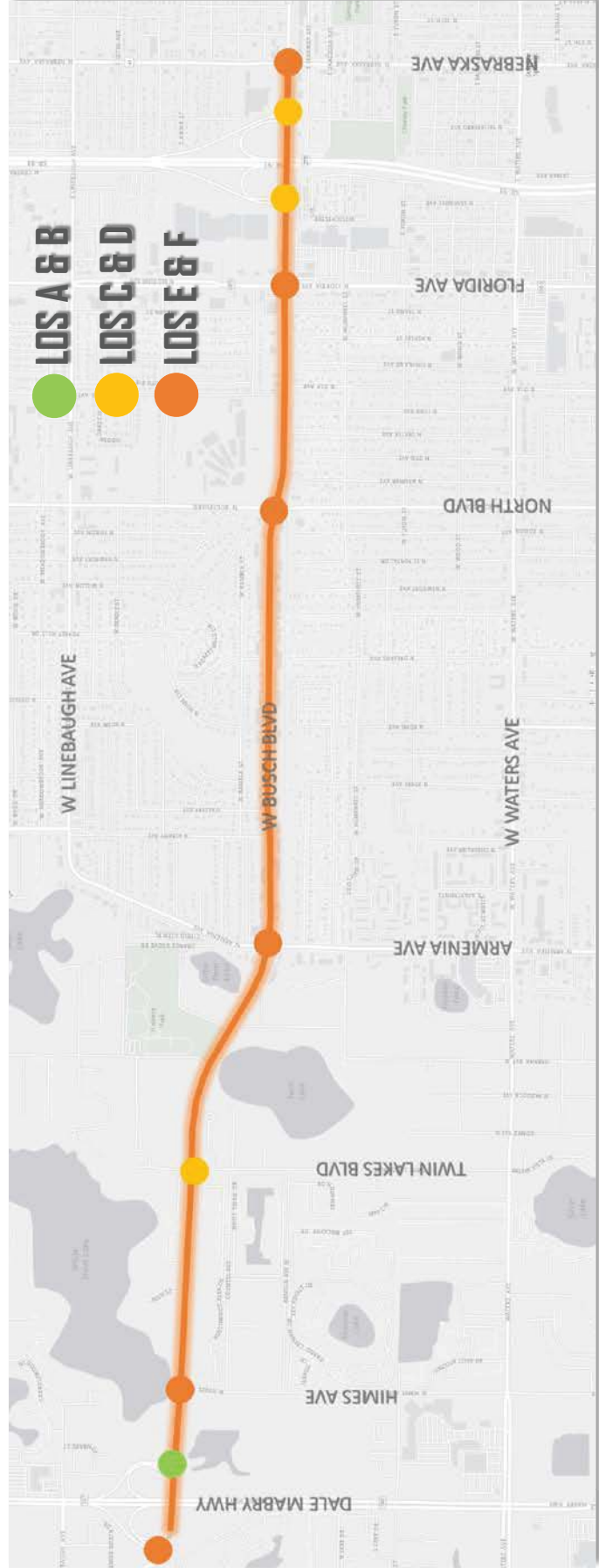
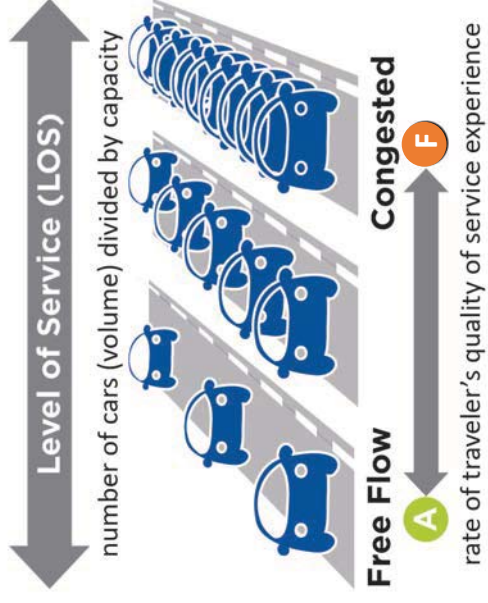
Minimum = 42,000

Maximum = 52,000

Existing Conditions Traffic Volumes (2017)



Existing Conditions Future Traffic Volumes (2040)



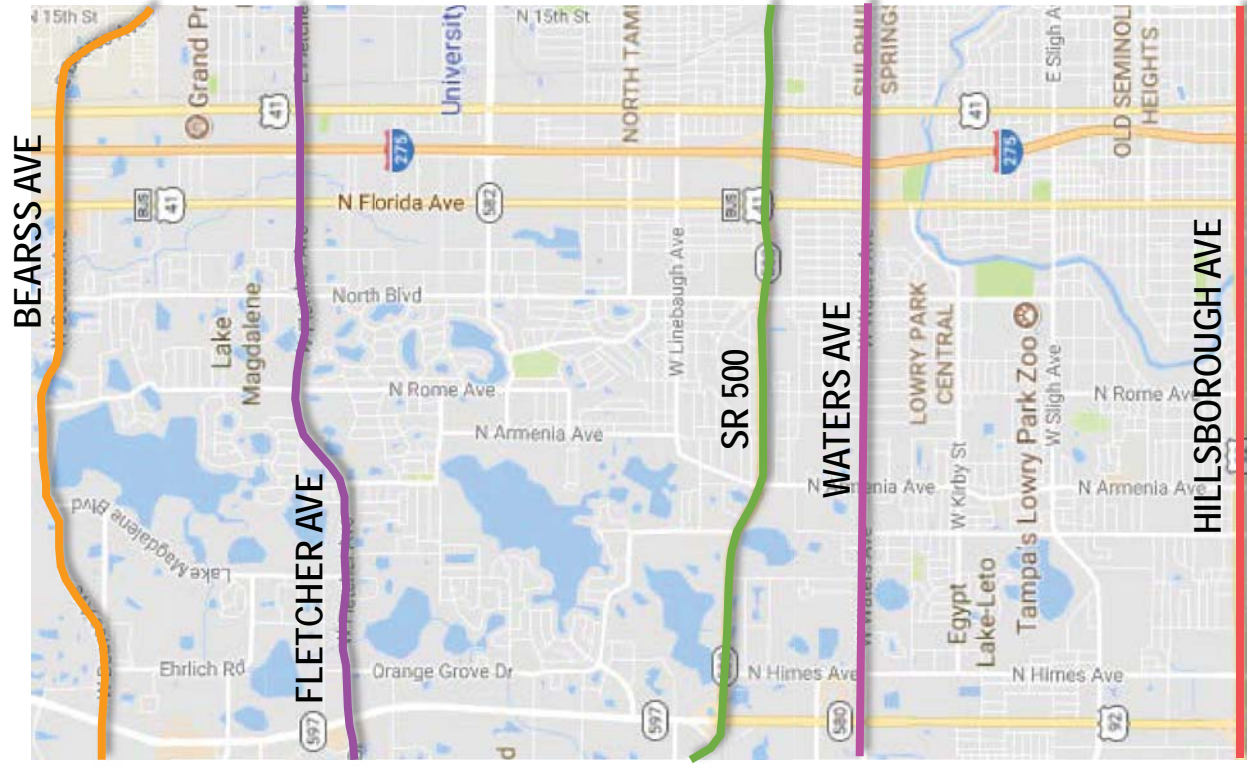
Adjacent East-West Corridor Considerations

Corridor	Lanes	AADT	Notes
Bearss Avenue	4-lane	54,000	3.8 miles north
Fletcher Avenue	4-lane	23,500	Limited western connectivity
SR 580	4-lane	50,000	
Waters Avenue	4-lane	27,000	Limited eastern connectivity
Hillsborough Avenue	6-lane	51,500	2.6 miles south

SR 580 Existing AADT

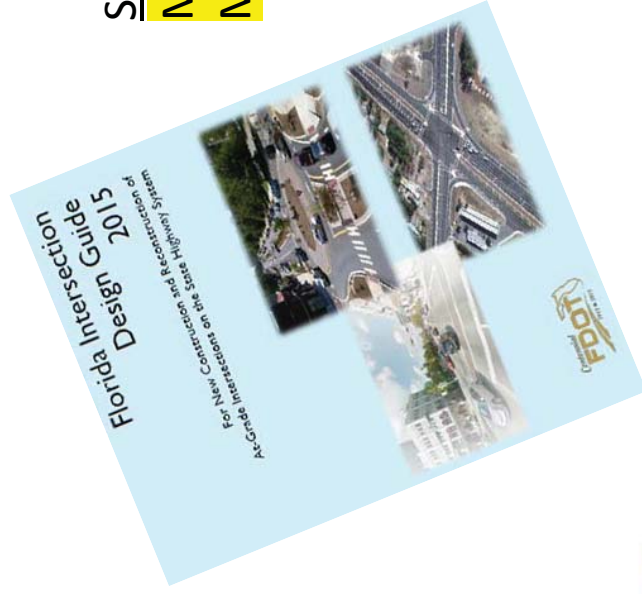
Minimum = 42,000

Maximum = 52,000



Roundabout Consideration

- Maximum design year total traffic volume entering an intersection is:
 - ≈ 25,000 AADT for single-lane roundabout
 - ≈ 45,000 AADT for two lane roundabout



SR 580 Existing AADT

Minimum = 42,000

Maximum = 52,000

Intersection	Entering AADT (Existing)
Dale Mabry/SB Ramps	49,100
Dale Mabry/NB Ramps	58,000
Himes	57,500
Twin Lakes	54,900
Orange Grove	50,950
Armenia	58,500
North	51,000
Florida	75,000
I-275/SB Ramps	60,150
I-275/NB Ramps	58,150
Nebraska	60,900

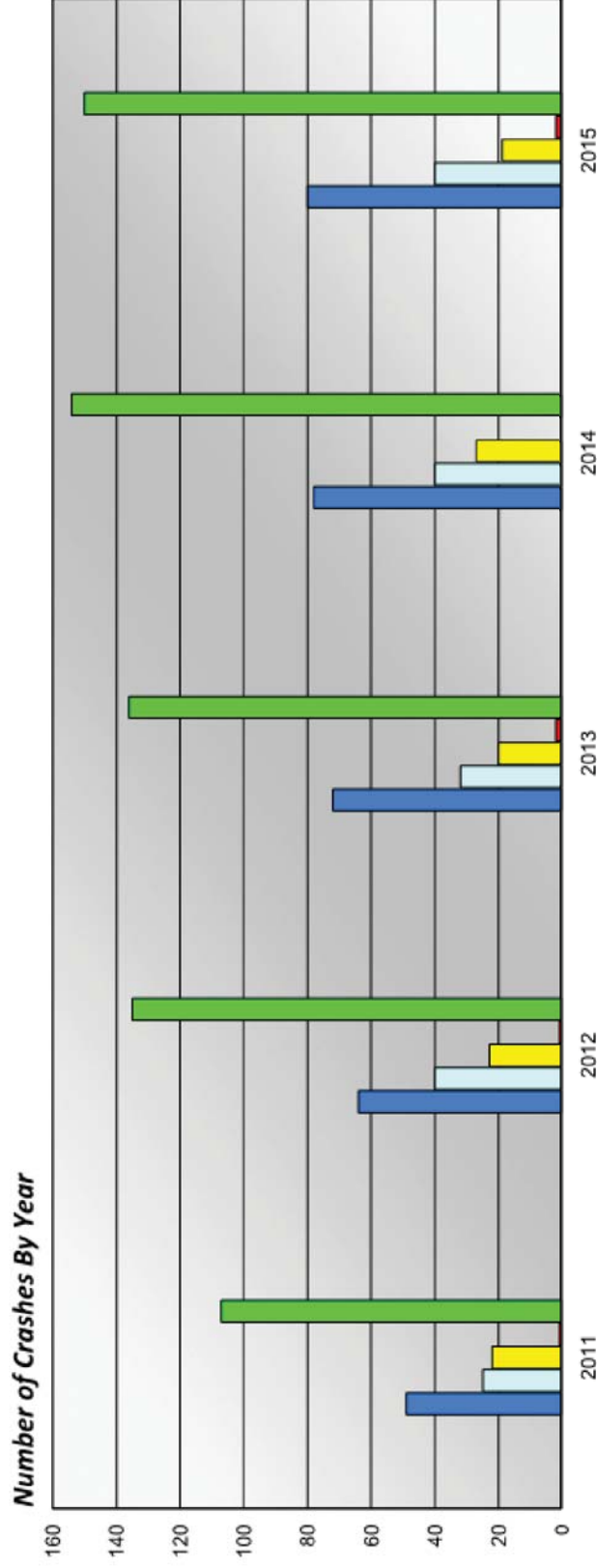
Take Away from existing Counts

- Bike/Ped movement N-S exceeds E-W movement.
- Round-About configuration not viable: within SR 580 corridor exceeds the maximum traffic volume requirements for a round-about and impacts the RR on the south.
- Lane reduction not viable:
 - ≈ The existing traffic is either within or just above the range for 4-lane facility Traffic Volumes.
 - ≈ Adjacent corridors are already at or above capacity and too far away.
- In fact, **6-Lane may be reasonable** for portions of SR 580 – but policy constrained

Note Potential Limitations for Capacity Improvements

- Shows need for 6 lane widening
- Policy constrained (physical constraints) in Tampa Comprehensive Plan – from Dale Mabry Hwy to North Blvd.
 - ≈ *MBY Policy 3.2.2: Constrained roadways, though ineligible for two or more general use thru-lane additions, shall be eligible for transit system enhancements including those described above. Dedicated transit lanes constructed pursuant to this policy shall not be retrofitted for use by automobile or truck traffic so long as the subject roadway remains constrained.*

Latest Crash History by Year



Breakdown of Crashes by Year

	2011	2012	2013	2014	2015
PDO	49	64	72	78	80
Injury Crashes	57	70	62	76	68
Fatal Crashes	1	1	2	0	2
Total Crashes	107	135	136	154	150

Existing Conditions Historical Crashes 2011- 2015



*Source: FDOT Crash Analysis Reporting System (CARS)

Take Away from Crash History Data

- Crashes have increased.
- Highest “heat” intensity is at Himes and between North Blvd and Nebraska.
- The 5-lane section has a lower frequency and severity of crashes compared to the 4-lane section.
- Five of the six fatalities occurred between North Blvd and Nebraska
- Recently checked *FIRES* and found no new fatalities between 1-1-16 and 10-23-17.

Preview of the Public Visioning Workshop



Public Visioning Workshop

- November 16, 2017
 - ≈ 5:30 p.m. – 7:30 p.m.
 - ≈ Open house format
 - ≈ Christian Family Church

FDOT




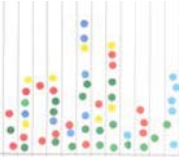


WESTBUSCH BOULEVARD STUDY

You're invited to take part in a Florida Department of Transportation (FDOT) Visioning Workshop for a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue in Hillsborough County – a distance of approximately 3.3 miles. The FDOT is looking for ways to improve West Busch Boulevard (SR 580) for pedestrians, bicyclists, transit users and motorists. At this workshop, we will gather your input on the existing corridor and document your ideas to resolve some of the issues that you have observed.

Thursday, November 16, 2017, 5:30 p.m. to 7:30 p.m.
Christian Family Church, 3457 W. Kenyon Avenue, Tampa, FL 33614

For More Information Contact:
Brian Shroyer, CPM
Project Manager
Florida Department of Transportation
11201 N. McKinley Drive
Tampa, FL 33812
813-975-6449
brian.shroyer@dot.state.fl.us
Visit Our Website
fdot7studies.com/westbuschblvd/

Public Visioning Workshop

- 5 Stations:
 - ≈ Station 1: 10 minute **Project Introduction Video** 
 - ≈ Station 2: **Existing Conditions** displays
 - Project location map
 - Existing typical sections
 - Crash map
 - Bicycle and pedestrian counts
 - Existing and future (no-build) traffic LOS
 - ≈ Station 3: **Concerns and Issues** (sticky-note exercise)
 - ≈ Station 4: **Developing the Corridor Vision** (goals and objectives dot exercise) 
 - ≈ Station 5: **Potential Improvements** (dot exercise) 
 - ≈ Station 6: **Comments** - user preference surveys and comment forms 

Draft User Preference Survey



DRAFT User Preference Survey SR 580 (West Busch Boulevard) Corridor Study from Dale Mabry Hwy. to Nebraska Ave.

Please fill out this survey as best as you can and place in the Survey forms box.

- Home Zip code: _____
- Work/School zip code: _____
- Please circle all that apply:
 - I live on or near Busch Boulevard
 - I work on or near Busch Boulevard
 - I stop/visit businesses on or near Busch Boulevard
 - My school is on or near Busch Boulevard
Which school? _____
 - I travel on Busch Boulevard but rarely stop
 - I don't regularly use Busch Boulevard
 - Other: _____
- Which travel option(s) have you used on Busch Boulevard? Please circle all that apply:
 - Drive
 - Walk
 - Bike
 - HART Bus
 - School Bus
 - Share a ride with others (car, pool, boat, etc.)
 - Other: _____
- When driving on Busch Boulevard, rate your satisfaction on the following (please circle 1 – 5):

Safe	5	4	3	2	1	Unsafe	n/a
Comfortable	5	4	3	2	1	Uncomfortable	n/a
Convenient	5	4	3	2	1	Inconvenient	n/a
- When walking/biking/riding the bus on Busch Boulevard, rate your satisfaction on the following (please circle 1 – 5):

Safe	5	4	3	2	1	Unsafe	n/a
Comfortable	5	4	3	2	1	Uncomfortable	n/a
Convenient	5	4	3	2	1	Inconvenient	n/a

7. How important are the following objectives in your vision for Busch Boulevard?

My Vision is for a Busch Boulevard where...	Very Important	Somewhat Important	Not Important	No Opinion
Trend users have shade and comfortable amenities				
A continuous sidewalk runs the length of the corridor				
Bicycle connectivity is enhanced to adjacent homes and businesses				
Safe and frequent options are provided for pedestrians and bicyclists to cross the corridor				
Aesthetic enhancements provide shade and add beauty to the corridor				
The corridor adds a unique and moving character to the community				
Existing businesses are preserved along the corridor				
The roadway attracts reinvestment into the corridor and surrounding areas				
Travelers experience efficient and reliable travel times				
Facilities are ADA compliant				
Citizens with limited mobility have enough time to cross the street				
Congestion is reduced on the corridor				
Crashes occur less frequently and are not as severe				
Drivers slow down to a safe speed				
Increased lighting improves visibility for drivers and pedestrians				
Children can travel to and from school safely, regardless of mode of travel				
Consolidated driveways reduce conflict points for vehicles and pedestrians				

8. Which of the following potential improvements would you support?

	Strongly Agree	Agree	Neutral	Disagree	Strongly Disagree
Add additional travel lanes					
Create a pedestrian-only phase at key intersections					
Improve traffic signal timing					
Add on-street parking					
Add mid-block crosswalks					
Landscaping, pedestrian lighting, etc.					
Lower speed limit					
Create a wide sidewalk along the entire corridor					
Add on-street bike lanes					
Add buffered bike lanes					
Add dedicated bus lane					
Add school zone					
Fill in sidewalk gaps					
Add bus shelters					
Add a landscaped median					

9. Any other comments and suggestions:

Thank you for taking the time to complete our User Preference Survey. Your valuable input will help inform the identification of challenges/issues and the development of potential solutions.

Next Steps





Next Steps for the Project Team

- Hold the visioning workshop and summarize feedback
- Create vision plan
- Develop preliminary purpose and need
- Develop alternatives and perform a comparative evaluation

Next Steps for the PAG

- Discuss the project in your sphere of influence
- Direct interested parties to the website to provide comments
- Direct interested parties to the website to “pin” issues in the corridor using the WikiMapping interactive tool
- Invite interested parties to the Visioning Workshop
- Provide feedback to the project team regarding your conversations with interested parties

Future PAG meetings

- PAG #3
 - ≈ January 2018
 - ≈ Purpose and need; range of solutions
- PAG #4
 - ≈ June 2018
 - ≈ Report on final near-term and long-term decisions

Next Steps - Schedule

SR 580/Busch Boulevard from North Dale Mabry Highway to North Nebraska Avenue Corridor Study

PUBLIC INVOLVEMENT PLAN SCHEDULE

ACTIVITY	2017												2018							
	Mar	Apr	May	June	July	Aug	Sept	Oct	Nov	Dec	Jan	Feb	Mar	April	May	June	July	Aug	Sept	
Notice to Proceed - Project Begins	★																			
Develop Public Involvement Plan																				
Mailing List																				
Elected Officials/Agency Kick-off Meeting					★															
Project Advisory Group (PAG) Meetings																				
Corridor Visioning Workshop																				
Alternatives Public Meeting																				
Final Corridor Alternatives & Strategies Report																				

Questions???



Please fill out this survey as best as you can and place in the Survey Forms box.

1. Home Zip code: _____

2. Work/School zip code: _____

3. Please **circle** all that apply:

- | | |
|---|--|
| a. I live on or near Busch Boulevard | e. I travel on Busch Boulevard but rarely stop |
| b. I work on or near Busch Boulevard | f. I don't regularly use Busch Boulevard |
| c. I shop/visit businesses on or near Busch Boulevard | g. Other: _____ |
| d. My school is on or near Busch Boulevard | _____ |
| Which school? _____ | |

4. Which travel option(s) have you used on Busch Boulevard? Please **circle** all that apply:

- | | |
|-------------|--|
| a. Drive | e. School Bus |
| b. Walk | f. Share a ride with others (car pool, taxi, etc.) |
| c. Bike | g. Other: _____ |
| d. HART Bus | _____ |

5. When **driving** on Busch Boulevard, rate your satisfaction on the following (**please circle 1 – 5**):

Safe	5	4	3	2	1	Unsafe	n/a
Comfortable	5	4	3	2	1	Uncomfortable	n/a
Convenient	5	4	3	2	1	Inconvenient	n/a

Additional Comments: _____

6. When **walking/biking/riding** the bus on Busch Boulevard, rate your satisfaction on the following (**please circle 1 – 5**):

Safe	5	4	3	2	1	Unsafe	n/a
Comfortable	5	4	3	2	1	Uncomfortable	n/a
Convenient	5	4	3	2	1	Inconvenient	n/a

Additional Comments: _____

7. How important are the following objectives in your vision for Busch Boulevard?

My Vision is for a Busch Boulevard where...	Very Important	Somewhat Important	Not Important	No Opinion
Transit users have shade and comfortable amenities				
A continuous sidewalk runs the length of the corridor				
Bicycle connectivity is enhanced to adjacent homes and businesses				
Safe and frequent options are provided for pedestrians and bicyclists to cross the corridor				
Aesthetic enhancements provide shade and adds beauty to the corridor				
The corridor adds a unique and inviting character to the community				
Existing businesses are preserved along the corridor				
The roadway attracts reinvestment into the corridor and surrounding areas				
Travelers experience efficient and reliable travel times				
Facilities are ADA compliant				
Citizens with limited mobility have enough time to cross the street				
Congestion is reduced on the corridor				
Crashes occur less frequently and are not as severe				
Drivers slow down to a safe speed				
Increased lighting improves visibility for drivers and pedestrians				
Children can travel to and from school safely, regardless of mode of travel				
Consolidated driveways reduce conflict points for vehicles and pedestrians				

Additional Comments:

8. Which of the following potential improvements would you support?

	Strongly Agree	Agree	Neutral	Disagree	Strongly Disagree
Add additional travel lanes					
Create a pedestrian-only phase at key intersections					
Improve traffic signal timing					
Add on-street parking					
Add mid-block crosswalks					
Landscaping, pedestrian lighting, etc.					
Lower speed limit					
Create a wide sidewalk along the entire corridor					
Add on-street bike lanes					
Add buffered bike lanes					
Add dedicated bus lane					
Add school zone					
Fill in sidewalk gaps					
Add bus shelters					
Add a landscaped median					

Additional Comments:

9. Any other comments and suggestions:

Thank you for taking the time to complete our User Preference Survey. Your valuable input will help inform the identification of challenges/issues and the development of potential solutions.

West Busch Boulevard (SR 580) Corridor Study

From North Dale Mabry Highway to North Nebraska Avenue

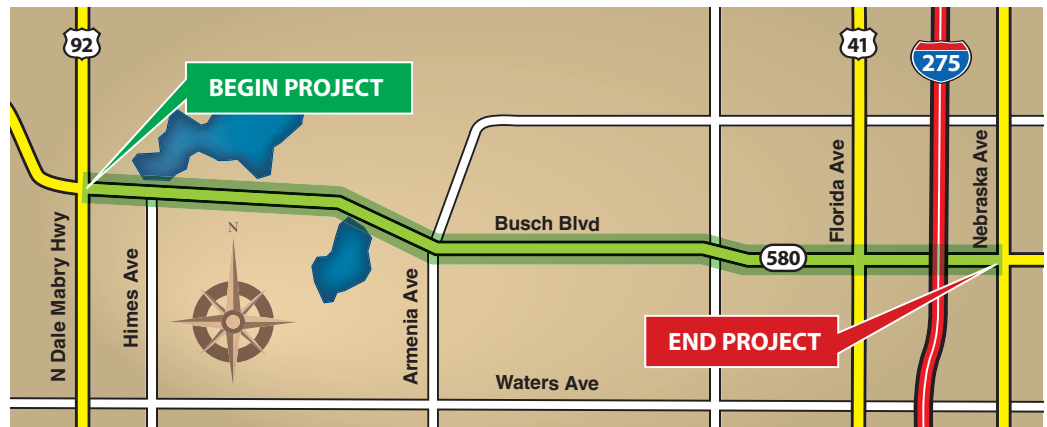
Hillsborough County, FL

Financial Project ID Number: 435908-1-22-01

Corridor Study Begins!

The Florida Department of Transportation (FDOT), District 7 is conducting a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue in Hillsborough County – a distance of approximately 3.3 miles. The West Busch Boulevard (SR 580) corridor is an east-west urban principal arterial connecting and serving local commuters and visitors between North Dale Mabry Highway and Nebraska Avenue. The corridor study area includes portions of unincorporated Hillsborough County and the City of Tampa and is the main route to the Busch Gardens Amusement Park (located approximately 2.15 miles east of the project limits at McKinley Drive).

The corridor supports a variety of land uses and demographics, including schools, churches, large and small businesses with direct access to and from SR 580, transit services, pedestrians, bicyclists and many small communities. The corridor also serves a large commuter population as a connection between Veterans Expressway to the west and I-75 to the east.



The West Busch Boulevard (SR 580) corridor study will use FDOT's Complete Streets policy to showcase how Complete Streets approaches can shape a corridor. The study, which began in March 2017, is expected to continue for approximately 18 months and will include technical work and data analysis, along with various opportunities for the public to be involved in discussing a vision for West Busch Boulevard (SR 580) that makes it accessible for all users.

What is a Corridor Study?

A corridor study is the first step in planning by defining the corridor's needs. A corridor study generally defines the relationships between a roadway and its adjacent land. Corridor studies are also used to: define acceptable levels of access and mobility; determine transportation system needs to support surrounding land uses, and identify operational deficiencies, among other things. This corridor study will include establishing a shared vision that will allow the community to help set project goals and objectives. The vision will guide the project team to develop a set of improvements that leverage local resources, match local conditions and desires, and are consistent with ongoing planning and development efforts.

Project Schedule

The West Busch Boulevard (SR 580) Corridor Study began in March 2017 and is anticipated to be completed in 18 months. Project activities will include:

- Identifying the travel related needs and issues within the West Busch Boulevard project corridor.
- Coordinating with key stakeholders to establish the vision for the corridor and identify solutions to the corridor issues and improve operational efficiency within the West Busch Boulevard corridor.
- Document the vision for all future improvements within the West Busch Boulevard corridor to maximize the “livability” for all users.

ACTIVITY	2017												2018						
	Mar	Apr	May	June	July	Aug	Sept	Oct	Nov	Dec	Jan	Feb	Mar	April	May	June	July	Aug	Sept
Notice to Proceed - Project Begins	★																		
Develop Public Involvement Plan																			
Mailing List																			
Elected Officials/Agency Kick-off Meeting				★															
Project Advisory Group (PAG) Meetings																			
Corridor Visioning Workshop										★									
Alternatives Public Meeting													★						
Final Corridor Alternatives & Strategies Report																			

Project Advisory Group (PAG)

A Project Advisory Group (PAG), composed of local government and agency representatives, are working with FDOT to identify corridor challenges along with potential solutions, assist with documenting the community goals, and develop a vision for the corridor that will guide the study development and future improvements. The PAG will meet at key points throughout the study to collaborate with the project team to ensure the vision and improvement alternatives are well-coordinated with local planning efforts, adjacent projects, and community goals.

How Can You Get Involved?

The project website is your best source for up-to-date information; you can get access at this link:

www.fdotd7studies.com/westbuschblvd/. In addition to the general study information that is already posted on the website, the project documents will be uploaded to this site as they are developed. However, you can already begin providing your input on this project using WikiMapping! This tool allows you to “pin” comments at specific locations along the corridor. You can pick the type of comment you have to help us understand the issues you observe in the corridor. Some of these issue types include: Bus/Transit, Connectivity, Intersection Issue, Pedestrian, Safety, and Traffic/Parking. If you have a comment on an issue that does not have a specific description, just use the “Other” category. The link to the WikiMapping tool is provided on the project website, but for direct access you can click on the following link:

<http://wikimapping.com/wikimap/West-Busch-Boulevard-Corridor-Study.html>. The project team will utilize this information to help inform the process and guide solutions.

Future opportunities for you to get involved include the upcoming Corridor Visioning Workshop that will be conducted in the Fall 2017. At this workshop, we will gather your input on the existing corridor and document your ideas to resolve some of the issues that you have observed. Our goal is to enlist your help to identify improvements that can be implemented in the West Busch Boulevard corridor to resolve the challenges. In advance of this workshop, a User Preference Survey will be available on the website in October; hard copies of this same survey will be available at the Corridor Visioning Workshop. These surveys will be used by the project team to understand the issues and concerns that you have regarding the corridor.

We want your input! We urge anyone interested in shaping the future of the West Busch Boulevard corridor to actively participate by visiting the website, get informed, document issues in the WikiMapping tool, attend meetings, and provide your ideas.

For more information or to comment, please contact:

Brian Shroyer, Project Manager
11201 N. McKinley Drive
Tampa, FL 33612
Office Phone: (813) 975-6449
brian.shroyer@dot.state.fl.us

Kristen Carson, Public Information Officer
11201 N. McKinley Drive
Tampa, FL 33612
Office Phone: (813) 975-6202 or 800-226-7220
kristen.carson@dot.state.fl.us

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT.

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220.

Comuníquese Con Nosotros

Nos importa mucho la opinión del público sobre el proyecto. Si usted tiene preguntas o comentarios, o simplemente desea más información, por favor comuníquese con nosotros. Nuestro representante, Señora Lilliam Escalera, 813-975-6445, Departamento de Transportación de Florida, 11201 N. McKinley Dr., Tampa, FL 33612, lilliam.escalera@dot.state.fl.us.

Appendix D



PROJECT ADVISORY GROUP (PAG) MEETING #2
West Busch Boulevard (SR 580) Corridor Study
 From North Dale Mabry Highway to North Nebraska Avenue
 Financial Project ID Number: 435908-1-22-01



Tuesday, October 24, 2017 • North Tampa Branch Library, 8916 North Boulevard, Tampa, Florida 33604

Please provide your comments below. If more space is needed, please use the back of this sheet or an additional sheet of paper. You may place your comments in the "Comment Box" provided at the meeting, or send to Brian Shroyer, Project Manager at the address on the back of this comment form, by November 6, 2017. Emailed comments are also acceptable to Brian.Shroyer@dot.state.fl.us

*All comments are part of the project record and are available for review by the public and the media.

*We need to have a consistent road design/classification - ~~not~~ switching back and forth to urban/suburban/etc. in the street design creates an inconsistent road design. Think of the overall future condition → URBAN

*Sidewalks are exposed and lack shade. Need for protection and shade/landscaping.

*The entire corridor is in need of upgraded bike/ped connections.

*ATMS and ~~lead~~ pedestrian Interval Signals

*Mid Block HAWKS when signals are spaced too far apart.

CONTACT INFORMATION (PLEASE PRINT)

Name Wade Reynolds
 Title Senior Planner
 Agency / Organization Hillsborough MPO
 Phone Number 813 273-5774
 Email ReynoldsW@plancom.org

RETURN COMMENTS BY November 6, 2017 TO:

Brian L. Shroyer
 Multimodal Project Manager
 Florida Department of Transportation
 11201 N. McKinley Drive, MS 7-500
 Tampa, FL 33612
 Brian.Shroyer@dot.state.fl.us

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PROJECT ADVISORY GROUP (PAG) MEETING #2
West Busch Boulevard (SR 580) Corridor Study
 From North Dale Mabry Highway to North Nebraska Avenue
 Financial Project ID Number: 435908-1-22-01



Tuesday, October 24, 2017 • North Tampa Branch Library, 8916 North Boulevard, Tampa, Florida 33604

Please provide your comments below. If more space is needed, please use the back of this sheet or an additional sheet of paper. You may place your comments in the "Comment Box" provided at the meeting, or send to Brian Shroyer, Project Manager at the address on the back of this comment form, by November 6, 2017. Emailed comments are also acceptable to Brian.Shroyer@dot.state.fl.us

*All comments are part of the project record and are available for review by the public and the media.

- Coming out of the PAG #2 meeting, I have 3 concerns/comments:*
- ① What are the ^{methods you're using to find} members of the public being recruited to attend the Nov. 16th Mtg? Here are ways to reach them; are you utilizing them? (Radio Station PSA's, Newspaper articles with photos, television news clips, [Flyers/posters] for Groceries, business windows, schools churches, buses, libraries, etc.)
 - ② Who is the person representing [HART] bus system you are working with; when will you provide them with [posters] to communicate with bus patrons that they are wanted (and will be rewarded) at the public Mtg?
 - ③ I'm interested in volunteering for station #3 or station #6 to assist in engaging attendees to facilitate their input, which is the task of this public meeting.

I'm interested in your efforts/methods to recruit public members/stakeholders before this meeting, as well as, elicit their input during the meeting process.

CONTACT INFORMATION (PLEASE PRINT)

RETURN COMMENTS BY November 6, 2017 TO:

Name Amy E. Clark
 Title (public volunteer)
 Agency / Organization (HART patron)
 Phone Number 774-273-5442
 Email quaker.lady@yahoo.com

Brian L. Shroyer
 Multimodal Project Manager
 Florida Department of Transportation
 11201 N. McKinley Drive, MS 7-500
 Tampa, FL 33612
 Brian.Shroyer@dot.state.fl.us

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status.

Appendix E

WHAT HAVE YOU HEARD?

- REQUEST ELECTRONIC 8 1/2 x 11 w/
VISIONING WORKSHOP LOGISTICS
- SHARE FLYERS w/ HART, ETC.
- SOCIAL SERVICES - NE CORNER BUSCH/FL
- COMFORT OF VULNERABLE USERS IMPAT
- CONCERNS OVER FREQUENT CHANGE IN
CONTEXT CLASSIFICATIONS

CORRIDOR VISIONING WORKSHOP MEETINGS SUMMARY – November 16, 2017

A corridor visioning workshop was held on Thursday, November 16, 2017 from 5:30 p.m. to 7:30 p.m. at the Christian Family Church (3457 W. Kenyon Avenue, Tampa, Florida 33614). This meeting is the first of two public meetings scheduled to take place during the corridor study. The second meeting will be an alternatives public meeting, anticipated to be scheduled in the spring of 2018.



Public Notification

Meeting invitations were sent by e-mail to 33 elected officials and 115 appointed officials/interested parties. Meeting postcard invitations were sent by mail to 10 Native American Tribes, 1255 property owners and 642 current residents. Meeting flier invitations were distributed along the project corridor to locations where a large number of people gather. The invitations included dates, times and locations for the corridor visioning workshop. Copies of the meeting invitations are included in Appendix A.

Media Notification

A corridor visioning workshop notification was placed in the Florida Administrative Register November 3, 2017 Edition. A press release was distributed by the Florida Department of Transportation (FDOT) to major local media outlets on November 9, 2017. Copies of the Florida Administrative Weekly Register and the press release are included in Appendix B.

Public Meeting Agenda

The meeting was conducted in an interactive open house format utilizing six (6) stations. The public was invited to attend at any time between 5:30 p.m. and 7:30 p.m. Parking was available to accommodate all of the attendees, including the disabled. Signs were placed at the major intersecting roads to direct attendees to the meeting location and from the parking lot to the meeting room.



Meeting attendees were asked to visit the following six (6) stations:

Station 1- PowerPoint

A PowerPoint presentation played on a loop in a separate meeting room. The presentation informed attendees of the purpose of the corridor study and existing conditions throughout the study limits.

Station 2- Existing Conditions Activity

Meeting attendees were given Post-it notes to place on a corridor map, identifying the issues they see in the corridor. A SMART board with a project map and other projects in the area overlay was also available, attendees could identify issues in the corridor on the format of their choosing.

Station 3- Information on Transportation Characteristics of the Corridor

Meeting attendees viewed boards on the following aspects: Existing Typical Sections; Traffic Level of Service Map; Bicycle, Pedestrian and Transit Map; and Crash Locations 2011-2015. Members of the project consultant team were available to answer any questions the attendees had.

Station 4- Corridor Visioning Exercise

Meeting attendees were given 5 star stickers to place on their top priorities for the future of the corridor on a Developing the Corridor board (see Appendix C). The stickers were not placed in a specific priority order (all stickers have equal weight).

Station 5- Potential Improvement Exercise

Meeting attendees were given 5 stickers numbered 1-5 with #1 being the highest priority to place on a Potential Improvements board (see Appendix C).

Station 6- Comments and User Preference Survey

Meeting attendees received a comment sheet and a user preference survey. Attendees were asked to provide feedback by filling out a comment sheet and a user preference survey.

The following display boards were available for public review (see copies of the boards in Appendix D):

- Welcome Board
- Title VI Board / English & Spanish
- Project Sheet 1-3
- Project Location Map
- Traffic Level of Service Map
- Bicycle, Pedestrian and Transit Map
- Existing Typical Sections
- Crash Locations 2011-2015



Meeting Attendance

23 persons signed the sign-in sheet at the corridor visioning workshop. A copy of the sign-in sheets is included in Appendix E.

Attending members of the project team included the Florida Department of Transportation (FDOT) District Seven Office and project consultants. Project representatives were present to discuss the project, receive public input, and answer questions.



Meeting Handouts

A meeting agenda was distributed to the attendees along with newsletter #1 and a user preference survey. Copies of the meeting handouts are included in Appendix F.

Public Comments

2 comment forms were received at the corridor visioning workshop (see attached in Appendix G). No additional comment forms were received by mail and email.



User Preference Survey

Zero user preference surveys were completed by attendees at the Corridor Visioning Workshop. Zero opinion surveys were received by mail and email.

END OF MEETING SUMMARY

This meeting summary was prepared by Karen Harrell, Public Involvement Coordinator. If you feel that clarifications are necessary, or if this differs from your understanding, please notify Karen by telephone at 352-257-1651 or by email at Karen.Harrell@qcausa.com within five (5) working days upon receipt of this summary.

APPENDICES

APPENDIX A – Public Notification

Email Invitation to Elected / Appointed Officials
Meeting Invitation Postcard to Property Owners
Meeting Invitation Flier

APPENDIX B – Media Notification

Florida Administrative Register Ad
Press Release

APPENDIX C – Pictures of Activity Boards

Corridor Visioning Exercise
Potential Improvement Exercise

APPENDIX C – Display Boards / PowerPoint Presentation

APPENDIX D – Sign-in Sheets

APPENDIX E – Handouts

Meeting Agenda
Newsletter #1
User Preference Survey

APPENDIX G – Public Comments

APPENDIX A – Public Notification

Email Invitation to Elected / Appointed Officials

Meeting Invitation Postcard to Property Owners

Meeting Invitation Flier

Date: October 25, 2017

Subject: Visioning Workshop
West Busch Boulevard (SR 580) Corridor Study
from North Dale Mabry Highway to North Nebraska Avenue
Hillsborough County, Florida
FDOT Financial Project ID Number: 435908-1-22-01

Dear Agency Partner:

The Florida Department of Transportation (FDOT), District Seven has scheduled a Visioning Workshop for a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The study area includes portions of incorporated Hillsborough County and the City of Tampa. This notice has been sent to other public officials and agency representatives interested in the project. The Visioning Workshop is scheduled for November 16, 2017, from 5:30 p.m. to 7:30 p.m. at the **Christian Family Church, 3457 W. Kenyon Avenue, Tampa, Florida 33614**.

The FDOT is looking for ways to improve West Busch Boulevard (SR 580) for pedestrians, bicyclists, transit users and motorists. The Visioning Workshop is an opportunity for the project team to report on the findings from the Project Advisory Group Meetings #1 and #2 and for the public to provide input on the vision for the corridor. Attached is a post card invitation that is being sent to property owners, organizations and individuals interested in the project.

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220 at least seven (7) working days in advance of the meeting.

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT.

If you require further information concerning this project, please contact Brian Shroyer, Florida Department of Transportation Project Manager at 813-975-6449 or by email at Brian.Shroyer@dot.state.fl.us.

Sincerely,
Brian Shroyer
Multimodal Project Manager
Florida Department of Transportation



You're invited to take part in a Florida Department of Transportation (FDOT) Visioning Workshop for a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue in Hillsborough County – a distance of approximately 3.3 miles.

The FDOT is looking for ways to improve West Busch Boulevard (SR 580) for pedestrians, bicyclists, transit users and motorists. At this workshop, we will gather your input on the existing corridor and document your ideas to resolve some of the issues that you have observed.

Thursday, November 16, 2017, 5:30 p.m. to 7:30 p.m.
 Christian Family Church, 3457 W. Kenyon Avenue, Tampa, FL 33614

For More Information Contact:

Brian Shroyer, CPM
 Project Manager
 Florida Department of Transportation
 11201 N. McKinley Drive
 Tampa, FL 33612
 813-975-6449
 brian.shroyer@dot.state.fl.us
 Visit Our Website
fdot7studies.com/westbuschblvd/



Brian Shroyer, CPM
Project Manager
Florida Department of Transportation
11201 North McKinley Drive
Tampa, FL 33612

West Busch Boulevard (SR 580) Corridor Study
From North Dale Mabry Highway to North Nebraska Avenue

Visit Our Website

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Comuníquese Con Nosotros Nos importa mucho la opinión del público sobre el proyecto. Si usted tiene preguntas o comentarios, o si simplemente desea más información, por favor comuníquese con nuestra representante, Lilliam Escalera, 813-975-6403, Departamento de Transportación de Florida, 11201 N. McKinley Dr., Tampa, FL 33612, lilliam.escalera@dot.state.fl.us

West Busch Boulevard (SR 580) Corridor Study

From North Dale Mabry Highway to North Nebraska Avenue

Hillsborough County, FL

Financial Project ID Number: 435908-1-22-01

You're Invited to take part in a Florida Department of Transportation (FDOT) Corridor Visioning Workshop for a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue in Hillsborough County – a distance of approximately 3.3 miles.

The FDOT is looking for ways to improve West Busch Boulevard (SR 580) for pedestrians, bicyclists, transit users and motorists. The Visioning Workshop is an opportunity for the project team to report on the findings from Project Advisory Group Meetings and for the public to provide input on the vision for the corridor.



For More Information Contact:

Brian Shroyer, CPM, Project Manager, Florida Department of Transportation
 11201 N. McKinley Drive, Tampa, FL 33612, 813-975-6449, brian.shroyer@dot.state.fl.us

▶ Visit our website: www.fdotd7studies.com/westbuschblvd/

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APPENDIX B – Media Notification

Florida Administrative Register Ad

Press Release

Notice of Meeting/Workshop Hearing

OTHER AGENCIES AND ORGANIZATIONS

OCAusa

The Florida Department of Transportation, District Seven announces a workshop to which all persons are invited.

DATE AND TIME: November 16, 2017, 5:30 PM to 7:30 PM

PLACE: Christian Family Church, 3457 W. Kenyon Avenue, Tampa, FL 33614

GENERAL SUBJECT MATTER TO BE CONSIDERED: The Florida Department of Transportation (FDOT), District Seven, is holding a Visioning Workshop for a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The study area includes portions of unincorporated Hillsborough County and the City of Tampa.

The FDOT is looking for ways to improve West Busch Boulevard (SR 580) for pedestrians, bicyclists, transit users and motorists. The Visioning Workshop is an opportunity for the project team to report on the findings from Project Advisory Group Meetings and for the public to provide input on the vision for the corridor.

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Financial Project Identification Number: 435908-1-22-01

A copy of the agenda may be obtained by contacting: Mr. Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us

Pursuant to the provisions of the Americans with Disabilities Act, any person requiring special accommodations to participate in this workshop/meeting is asked to advise the agency at least 7 days before the workshop/meeting by contacting: Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220

Comuniquese Con Nosotros

Nos importa mucho la opinión del público sobre el proyecto. Si usted tiene preguntas o comentarios, o si simplemente desea más información, por favor comuniquese con nuestra representante, Lilliam Escalera, al (813) 975-6403, Departamento de Transportación de Florida, 11201 N. McKinley Dr., Tampa, FL 33612, Lilliam.Escalera@dot.state.fl.us. If you are hearing or speech impaired, please contact the agency using the Florida Relay Service, 1(800)955-8771 (TDD) or 1(800)955-8770 (Voice).

For more information, you may contact: Mr. Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us

West Busch Boulevard (SR 580) Corridor Study **Visioning Workshop Scheduled**

Tampa, FL – The Florida Department of Transportation (FDOT), District Seven, is holding a Visioning Workshop for a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The study area includes portions of unincorporated Hillsborough County and the City of Tampa.

The Visioning Workshop is scheduled for November 16, 2017, from 5:30 p.m. to 7:30 p.m. at the Christian Family Church, 3457 W. Kenyon Avenue, Tampa, Florida 33614.

The FDOT is looking for ways to improve West Busch Boulevard (SR 580) for pedestrians, bicyclists, transit users and motorists. The Visioning Workshop is an opportunity for the project team to report on the findings from Project Advisory Group Meetings and for the public to provide input on the vision for the corridor. A copy of the agenda may be obtained by contacting: Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us.

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Comuniquese Con Nosotros

Nos importa mucho la opinión del público sobre el proyecto. Si usted tiene preguntas o comentarios, o si simplemente desea más información, por favor comuniquese con nuestra representante, Lilliam Escalera, al (813) 975-6403, Departamento de Transportación de Florida, 11201 N. McKinley Dr., Tampa, FL 33612, Lilliam.Escalera@dot.state.fl.us

For more information on this project, please call the FDOT Project Manager, Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us.

###

APPENDIX C – Pictures of Activity Boards

Corridor Visioning Exercise

Potential Improvement Exercise

Developing the Corridor Vision



Now we want to know what is important to you! Help us shape the future vision of the West Busch Boulevard Corridor. Vote for the goals and objectives presented here, or add in your own!

My Vision is for a West Busch Boulevard where...

ACCESSIBILITY & CONNECTIVITY

- Transit users have shade and comfortable amenities ★
- A continuous sidewalk runs the length of the corridor ★★☆☆
- Bicycle connectivity is enhanced to adjacent homes and businesses ☆
- Safe and frequent options are provided for pedestrians and bicyclists to cross the corridor ★★☆☆☆☆

ECONOMIC DEVELOPMENT

- Aesthetic enhancements provide shade and adds beauty to the corridor ☆
- The corridor adds a unique and inviting character to the community ☆
- Existing businesses are preserved along the corridor ☆★★★★
- The roadway attracts reinvestment into the corridor and surrounding areas ☆☆☆

MOBILITY

- Travelers experience efficient and reliable travel times ☆☆☆
- Facilities are ADA compliant
- Congestion is reduced on the corridor ★☆☆
- Citizens with limited mobility have enough time to cross the street

SAFETY

- Crashes occur less frequently and are not as severe ★★☆☆☆
- Drivers slow down to a safe speed ★★☆☆☆
- Increased lighting improves visibility for drivers and pedestrians ☆
- Children can travel to and from school safely, regardless of mode of travel ☆
- Consolidated driveways reduce conflict points for vehicles and pedestrians ★★☆☆☆☆

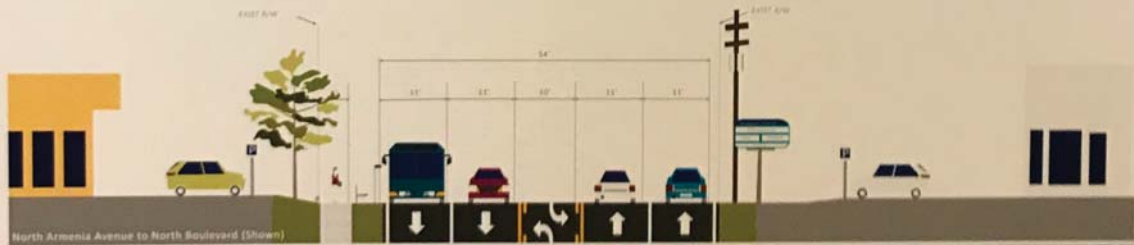
Potential Improvements

The limited available right-of-way along the corridor means that many potential improvements can not be accommodated without impacting adjacent properties. Please help prioritize the list of possible improvements below using the provided stickers to indicate **your top five** desired improvements for the corridor, with “#1” being the most preferred improvement.

- ADD ADDITIONAL TRAVEL LANES** 5 4
- CREATE A PEDESTRIAN-ONLY PHASE AT KEY INTERSECTIONS*** 3 1 2 1 5 3
- IMPROVE TRAFFIC SIGNAL TIMING*** 4 2 3 2 2 3
- ADD ON-STREET PARKING**
- ADD MID-BLOCK CROSSWALKS*** 4
- LANDSCAPING, PEDESTRIAN LIGHTING, ETC.*** 2 4 5
- LOWER SPEED LIMIT*** 2 1 3 4 4 5 5
- CREATE A WIDE SIDEWALK ALONG ENTIRE CORRIDOR** 2 4 1 3 2
- ADD ON-STREET BIKE LANES**
- ADD BUFFERED BIKE LANES** 1
- ADD A DEDICATED BUS LANE**
- ADD SCHOOL ZONE*** 1 3
- FILL IN SIDEWALK GAPS** 3 1 3 1 4
- ADD BUS SHELTERS** 2 5 2
- ADD A LANDSCAPED MEDIAN** 5

*Indicates improvements within the existing right-of-way

(WRITE IN ANYTHING ELSE!)



APPENDIX D – Display Boards / PowerPoint Presentation



Welcome

to the

West Busch Boulevard Study Corridor Visioning Workshop



TITLE VI



The proposed project is being developed in accordance with the Civil Rights Act of 1964, as amended. Under Title VI of the Civil Rights Act, public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status.

To express concern(s), please contact the following:

Christopher Speese

Title VI Coordinator | Florida Department of Transportation, District Seven
11201 N. McKinley Drive | Tampa, Florida 33612
Christopher.Speese@dot.state.fl.us
(813) 975-6405

-- or --

Jacqueline Paramore

Statewide Title VI Coordinator | Equal Opportunity Office
605 Suwannee Street, Room 260 | Mail Station 65
Tallahassee, Florida 32399-0450
Jacqueline.Paramore@dot.state.fl.us
(850) 414-4753



TITULO VI



Este proyecto se está diseñando de acuerdo con la Ley de Derechos Civiles de 1964 y 1968, según enmendada. Bajo el Título VI de la Ley de Derechos Civiles, la participación pública es solicitada sin distinción de raza, color, origen nacional, edad, sexo, incapacidad o estado familiar.

Para expresar alguna preocupación, comuníquese con:

Lilliam Escalera

Florida Department of Transportation, District Seven
11201 N. McKinley Drive | Tampa, Florida 33612
Lilliam.Escalera@dot.state.fl.us
(813) 975-6445

-- or --

Jacqueline Paramore

Statewide Title VI Coordinator | Equal Opportunity Office
605 Suwannee Street, Room 260 | Mail Station 65
Tallahassee, Florida 32399-0450
Jacqueline.Paramore@dot.state.fl.us
(850) 414-4753

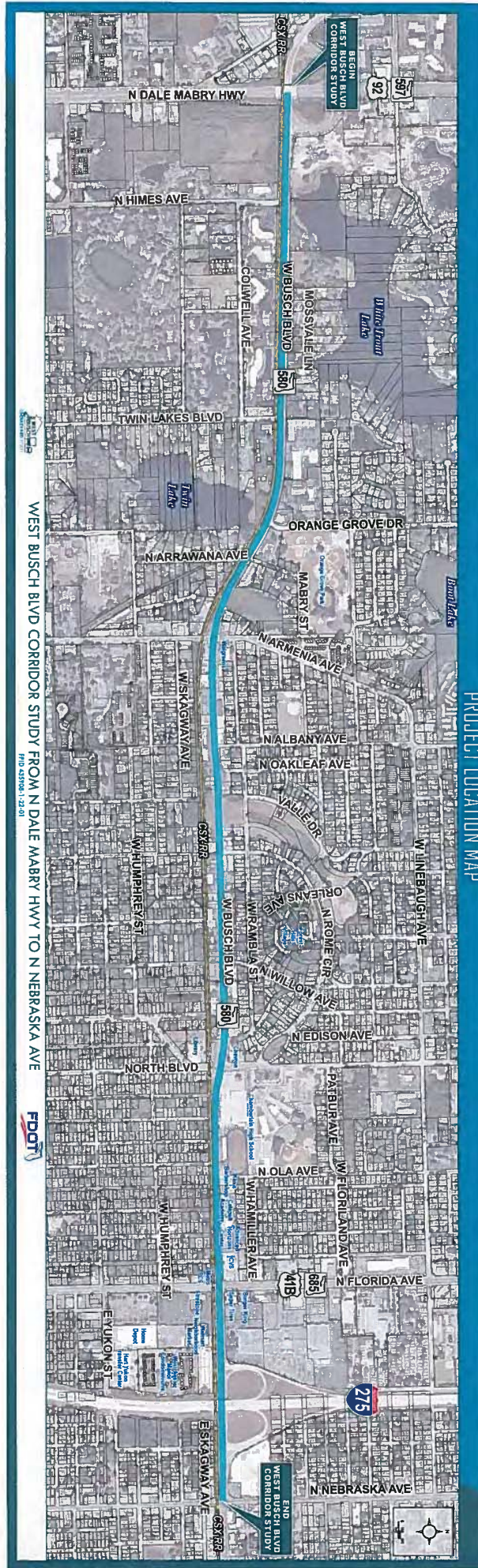
Project Sheet 1 of 3



Project Sheet 2 of 3







WEST BUSCH BLVD CORRIDOR STUDY FROM N DALE MABRY HWY TO N NEBRASKA AVE

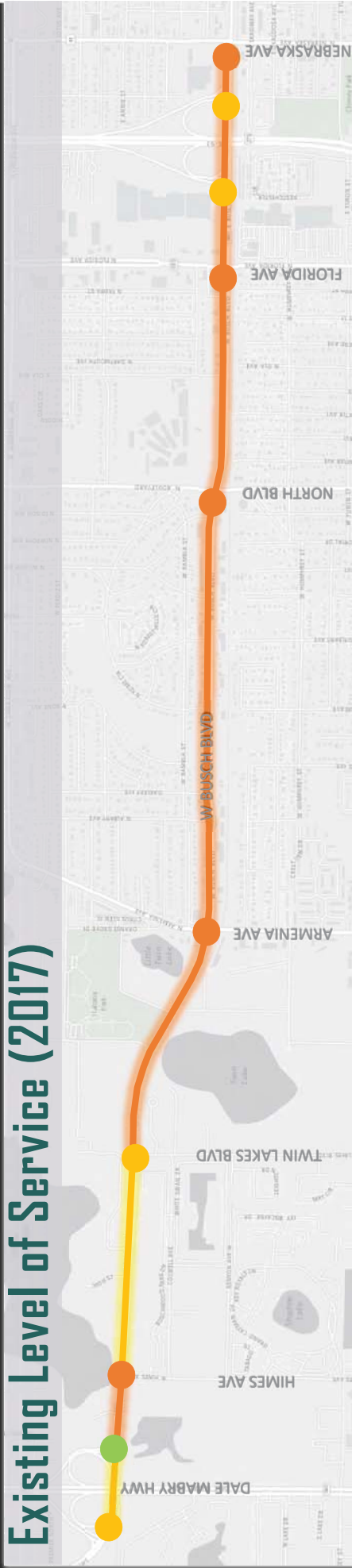


FILE NUMBER: 12-01

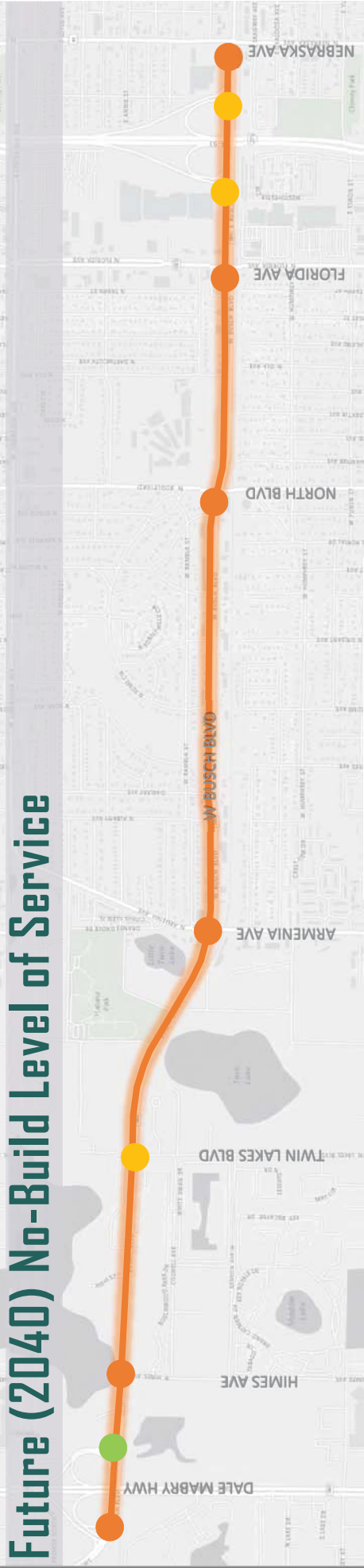


Traffic Level of Service Map

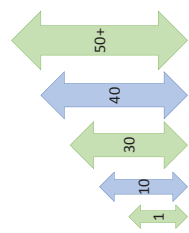
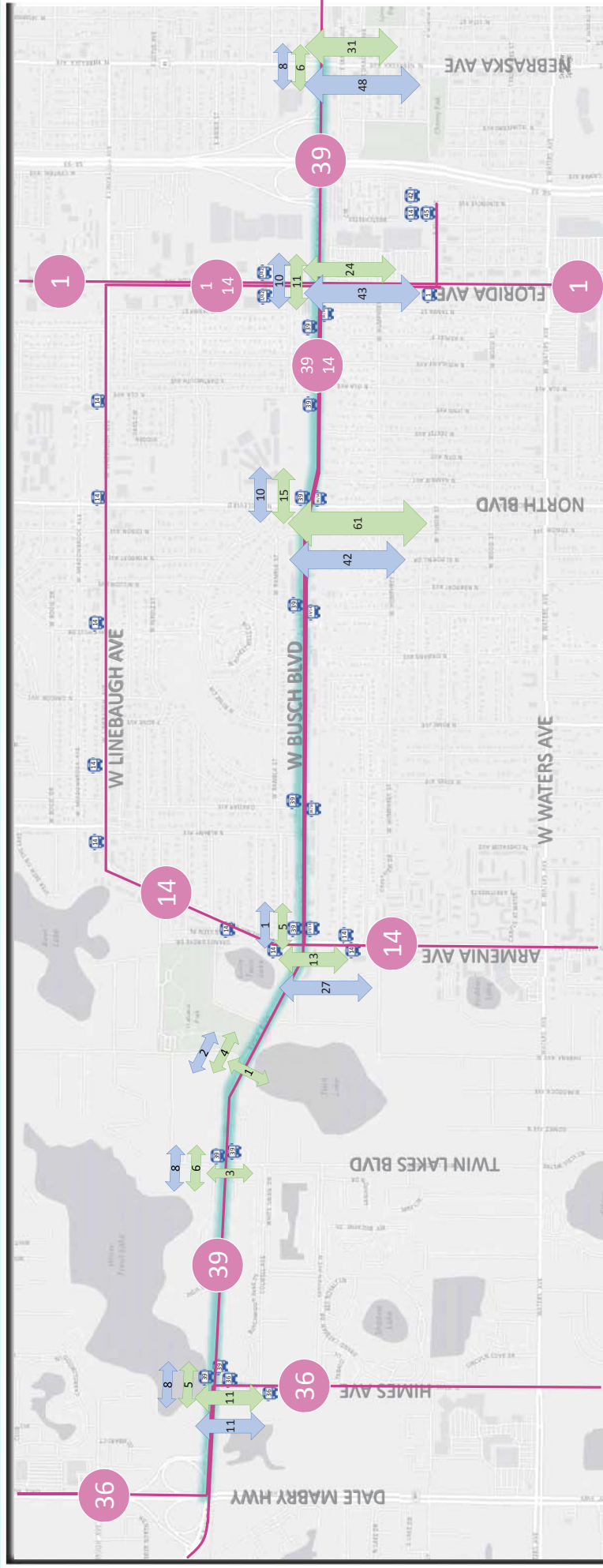
Existing Level of Service (2017)



Future (2040) No-Build Level of Service



Bicycle, Pedestrian, & Transit Map



By Cyclists
(blue)



By Pedestrians
(green)



Transit Stop

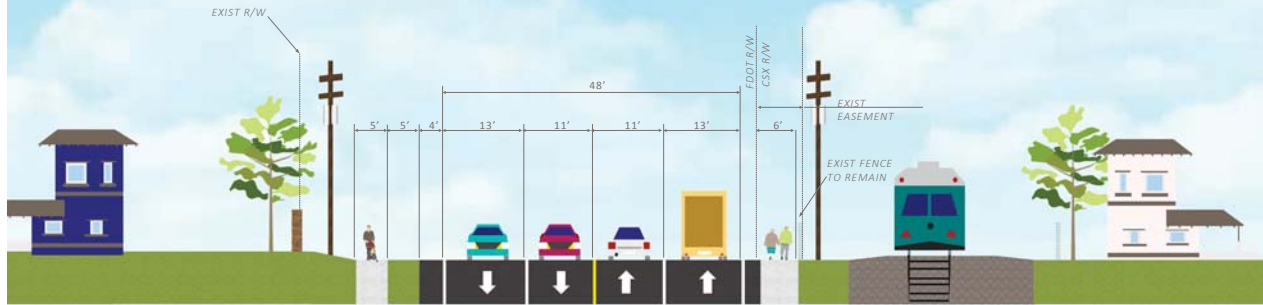


Transit Route

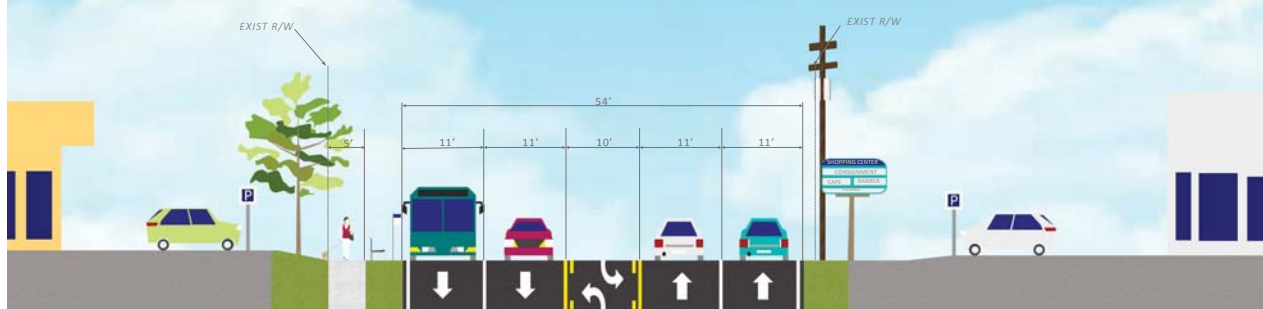
* Based on observations from 7 a.m. – 9 a.m. & 4 p.m. – 6 p.m. on Thursday May 4, 2017

Existing Typical Sections

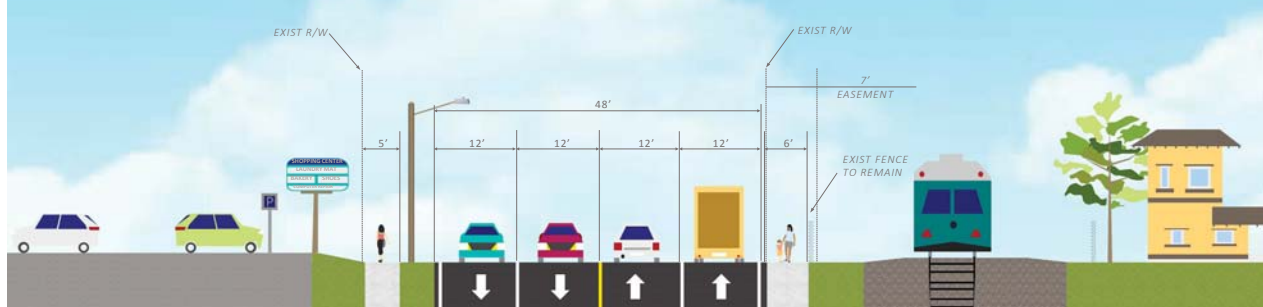
North Dale Mabry Highway to North Armenia Avenue
Suburban (Residential) Land Uses



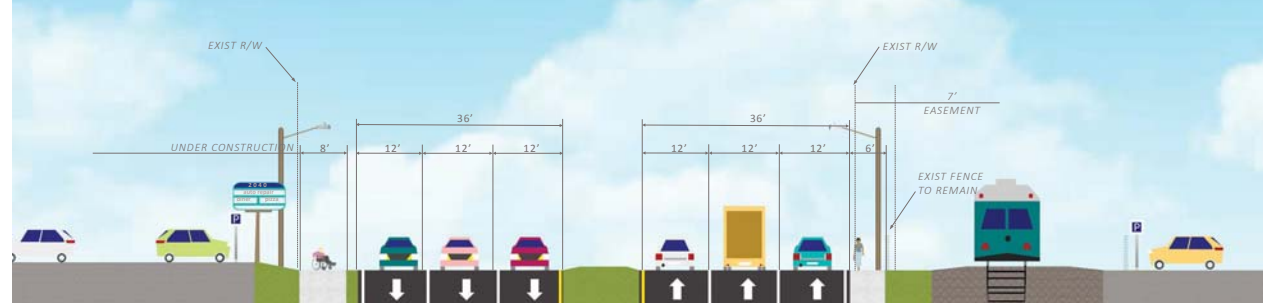
North Armenia Avenue to North Boulevard
Mixed-Use General Urban Land Uses



North Boulevard to North Florida Avenue
Suburban (Commercial) Land Uses

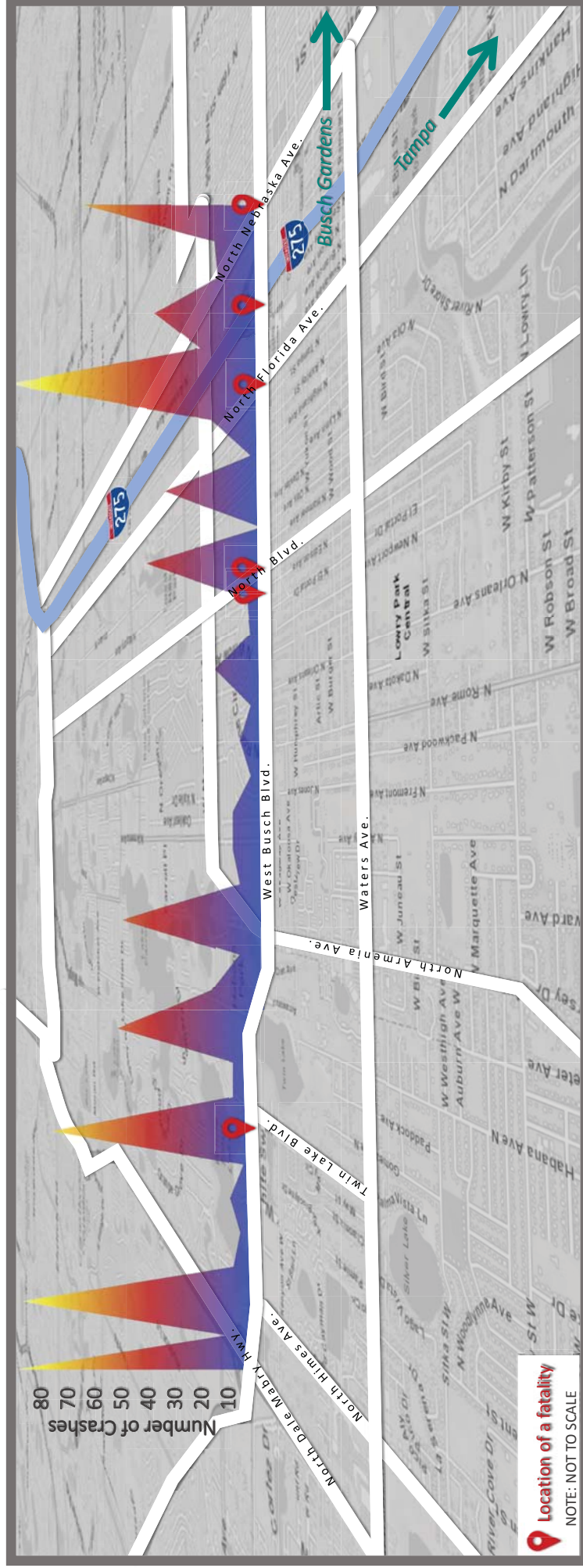


North Florida Avenue to North Nebraska Avenue
Suburban (Commercial) Land Uses





Crash Locations 2011- 2015*



*Source: FDOT Crash Analysis Reporting System (CARS)



Developing the Corridor Vision

You helped us identify the corridor issues at Station 3. Now we want to know what is important to you! Help us shape the future vision of the West Busch Boulevard Corridor. Vote for the goals and objectives presented here, or add in your own!

My Vision is for a West Busch Boulevard where...

ACCESSIBILITY & CONNECTIVITY

Transit users have shade and comfortable amenities

A continuous sidewalk runs the length of the corridor

Bicycle connectivity is enhanced to adjacent homes and businesses

Safe and frequent options are provided for pedestrians and bicyclists to cross the corridor

ECONOMIC DEVELOPMENT

Aesthetic enhancements provide shade and adds beauty to the corridor

The corridor adds a unique and inviting character to the community

Existing businesses are preserved along the corridor

The improved roadway attracts reinvestment into the corridor and surrounding areas

MOBILITY

Travelers experience efficient and reliable travel times

Facilities are ADA compliant

Congestion is reduced on the corridor

Citizens with limited mobility have enough time to cross the street

SAFETY

Crashes occur less frequently and are not as severe

Drivers slow down to a safe speed

Increased lighting improves visibility for drivers and pedestrians

Children can travel to and from school safely, regardless of mode of travel

Consolidated driveway access reduces conflict points for vehicles and pedestrians




Potential Improvements


The limited available right-of-way along the corridor means that many potential improvements can not be accommodated without impacting adjacent properties. Please help prioritize the list of possible improvements below using the provided stickers to indicate **your top five** desired improvements for the corridor, with “#1” being the most preferred improvement.

 **ADD ADDITIONAL TRAVEL LANES**


CREATE A PEDESTRIAN-ONLY PHASE AT KEY INTERSECTIONS 

 **IMPROVE TRAFFIC SIGNAL TIMING**


ADD ON-STREET PARKING 


 **ADD MID-BLOCK CROSSWALKS**


LANDSCAPING, PEDESTRIAN LIGHTING, ETC. 


 **LOWER SPEED LIMIT**

CREATE A WIDE SIDEWALK ALONG ENTIRE CORRIDOR 


 **ADD ON-STREET BIKE LANES**


ADD BUFFERED BIKE LANES 

 **ADD A DEDICATED BUS LANE**

ADD SCHOOL ZONE 

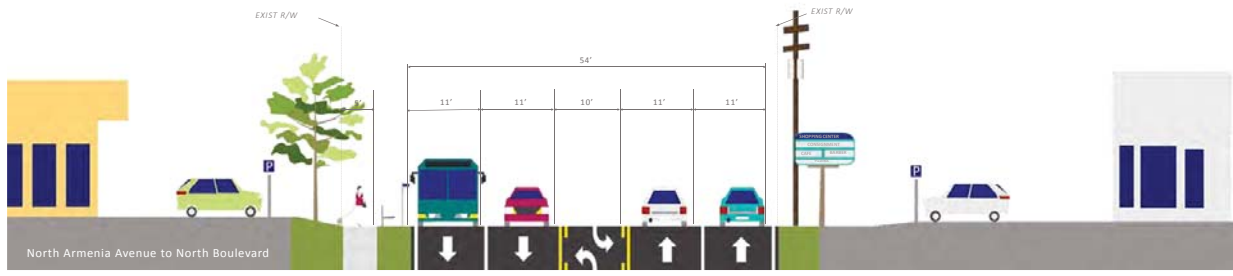
 **FILL IN SIDEWALK GAPS**

ADD BUS SHELTERS 

 **ADD A LANDSCAPED MEDIAN**

*Indicates improvements within the existing right-of-way

(WRITE IN ANYTHING ELSE!)



Welcome to the
West Busch Boulevard (SR 580) Corridor Study
from North Dale Mabry Highway to North Nebraska Avenue

Public Visioning Workshop

Hillsborough County, Florida
FDOT Financial Project ID Number: 435908-1-22-01
Christian Family Church
November 16, 2017 5:30 p.m. – 7:30 p.m.



Presentation

1

Introduction

2

Study Process

3

Develop the Vision

4

Next Steps

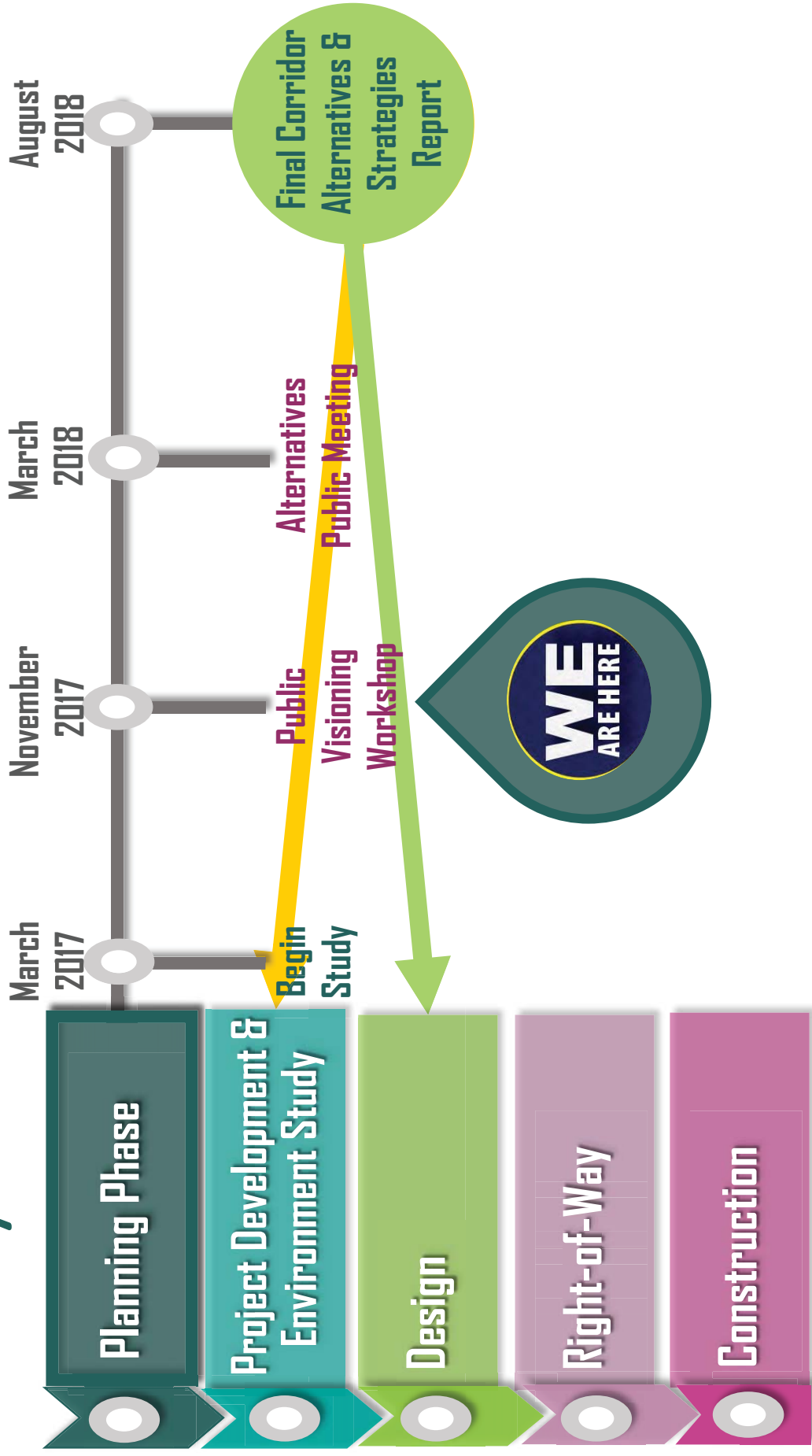


Project Study Area

- 3.3 miles
- Within the city of Tampa and unincorporated Hillsborough County

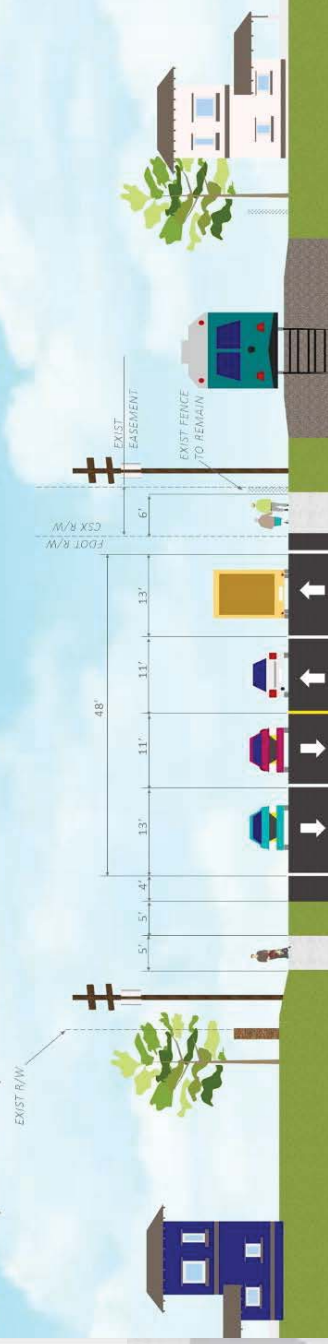


Study Process



Existing Conditions Dale Mabry Highway to North Armenia Avenue

North Dale Mabry Highway to North Armenia Avenue
Suburban (Residential) Land Uses



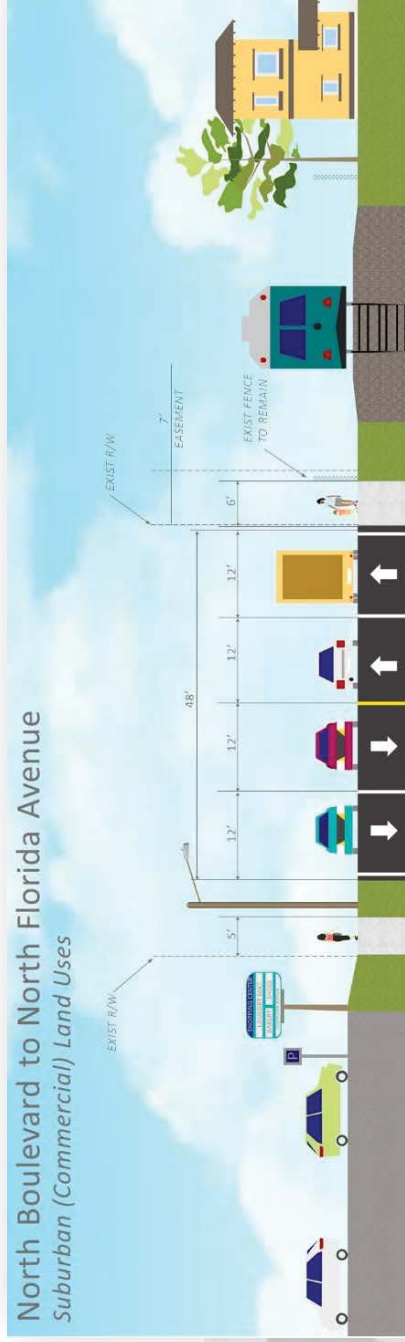
An aerial map showing the project area. A yellow line highlights the route from Dale Mabry Hwy in the north to North Armenia Ave in the south. Key streets shown include Dale Mabry Hwy, W Busch Blvd, W Waters Ave, North Blvd, Florida Ave, and Nebraska Ave. Other streets shown include Himes Ave, Twin Lakes Blvd, and Armenia Ave. The map also shows various residential lots, trees, and water features.

Existing Conditions North Armenia Avenue to North Boulevard

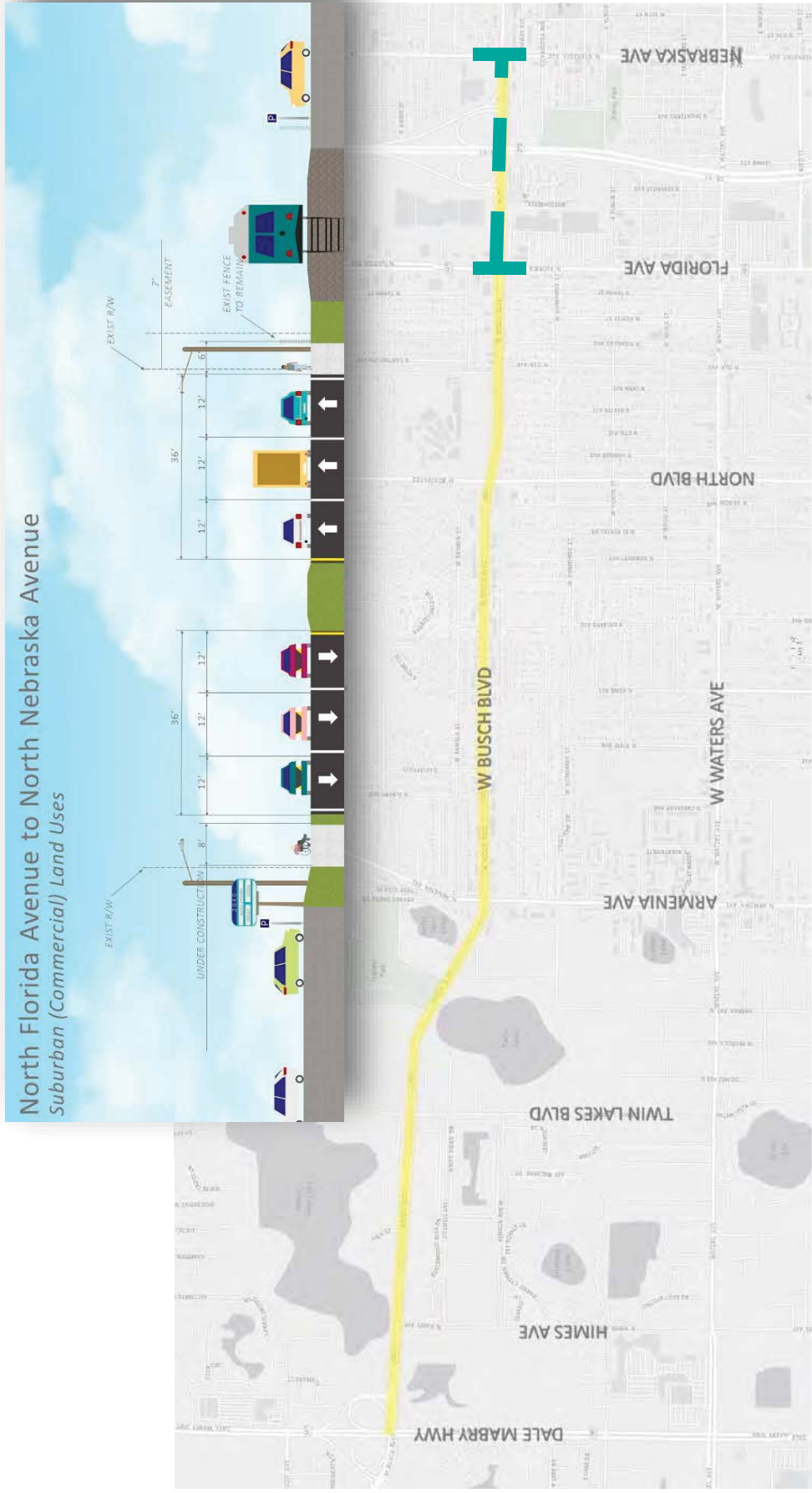


Existing Conditions North Boulevard to North Florida Avenue

North Boulevard to North Florida Avenue
Suburban (Commercial) Land Uses



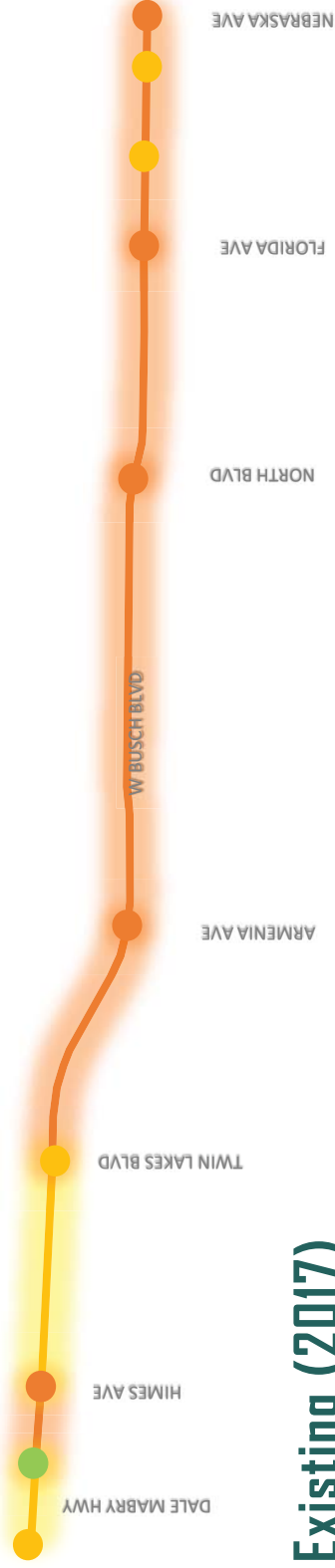
Existing Conditions North Florida Avenue to North Nebraska Avenue



Traffic Level of Service Maps

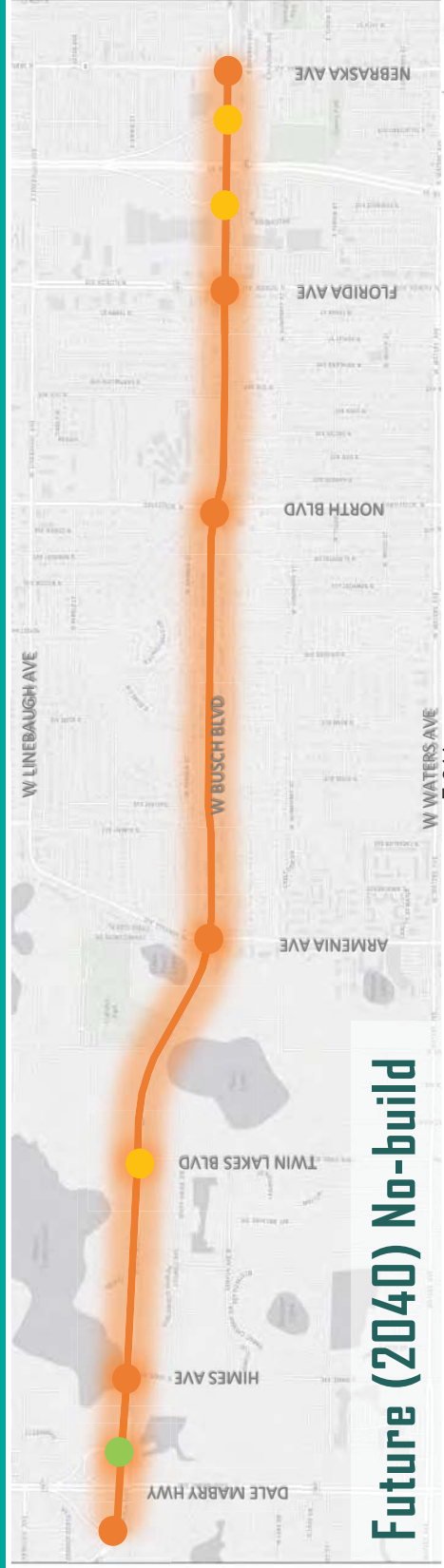


W LINEBAUGH AVE



Existing (2017)

W WATERS AVE

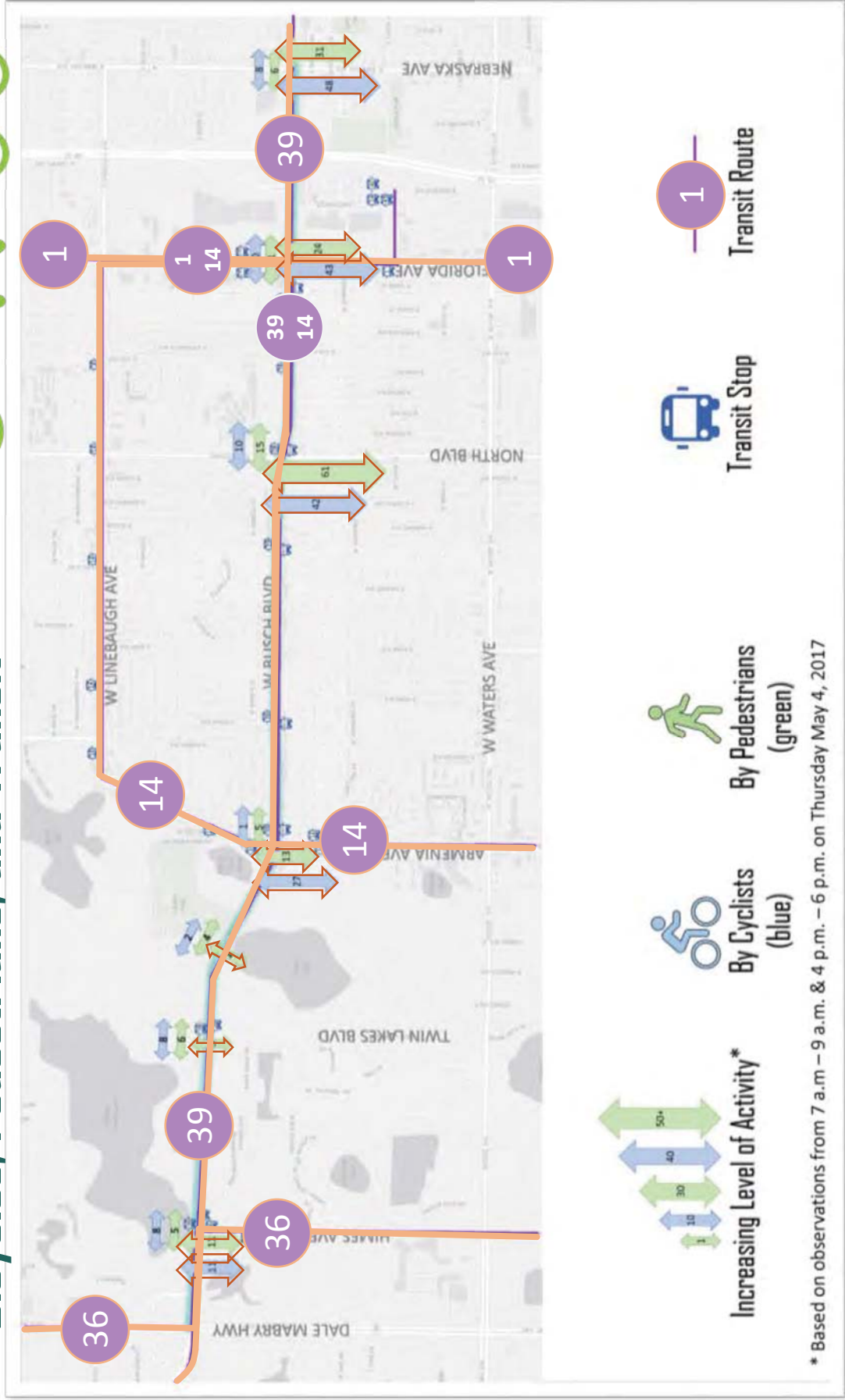


Future (2040) No-build

W WATERS AVE

E-244

Existing Conditions Bicycles, Pedestrians, and Transit



Existing Conditions Crash Locations 2011 through 2015



*Source: FDOT Crash Analysis Reporting System (CARS)

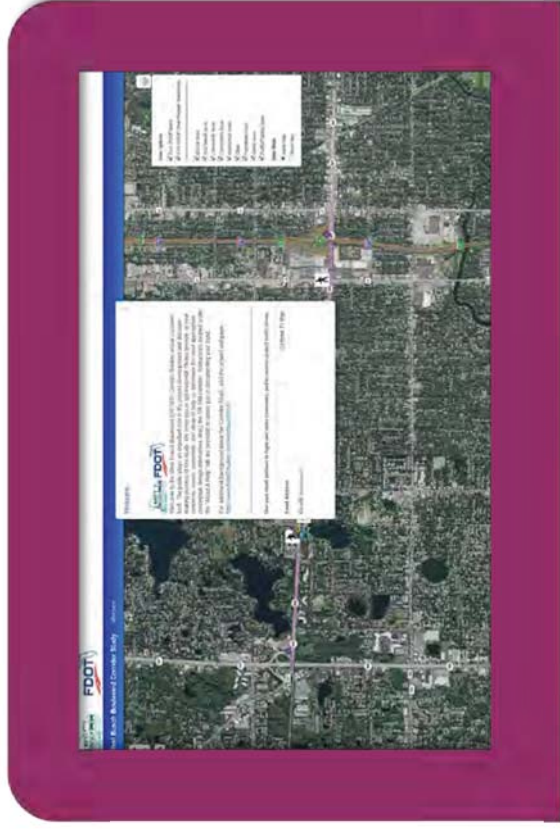
Develop the Vision



Concerns and Issues Along the Corridor A Sticky-Note Exercise



Wiki-Mapping




Use this map to share your concerns


Route I'd ride if...



Route I'd walk if...




Need bike parking 


Problem 

Facilities

 Sidewalk


 Greenway - Paved

 Greenway - Unpaved


 Bike Lane - Both sides

 Bike Lane - One side

 Sidewalk

 Wide outside shoulder

 Dirt Trails

 Bus routes

Visit: <http://wikimapping.com/wikimap/West-Busch-Boulevard-Corridor-Study.html>



Developing the Corridor Vision

You helped us identify the corridor issues at Station 3. Now we want to know what is important to you! Help us shape the future vision of the West Busch Boulevard Corridor. Vote for the goals and objectives presented here, or add in your own!

My Vision is for a West Busch Boulevard where...

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Transit users have shade and comfortable amenities

A continuous sidewalk runs the length of the corridor

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Aesthetic enhancements provide shade and adds beauty to the corridor

The corridor adds a unique and inviting character to the community

Existing businesses are preserved along the corridor

The roadway attracts reinvestment into the corridor and surrounding areas

MOBILITY

Travelers experience efficient and reliable travel times

Facilities are ADA compliant

Congestion is reduced on the corridor

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SAFETY

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Increased lighting improves visibility for drivers and pedestrians

Children can travel to and from school safely, regardless of mode of travel

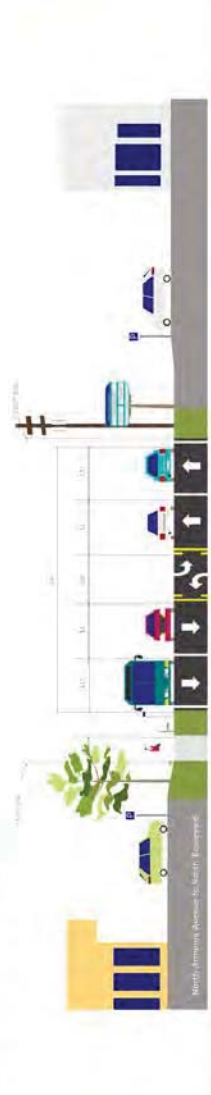
Consolidated driveways reduce conflict points for vehicles and pedestrians



Potential Improvements

The limited available right-of-way along the corridor means that many potential improvements can not be accommodated without impacting adjacent properties. Please help prioritize the list of possible improvements below using the provided stickers to indicate your top five desired improvements for the corridor, with "#1" being the most preferred improvement.

- ADD ADDITIONAL TRAVEL LANES
 - CREATE A PEDESTRIAN-ONLY PHASE AT KEY INTERSECTIONS
 - IMPROVE TRAFFIC SIGNAL TIMING
 - ADD ON-STREET PARKING
 - ADD MID-BLOCK CROSSWALKS
 - LANDSCAPING, PEDESTRIAN LIGHTING, ETC.
 - LOWER SPEED LIMIT
 - CREATE A WIDE SIDEWALK ALONG ENTIRE CORRIDOR
 - ADD ON-STREET BIKE LANES
 - ADD BUFFERED BIKE LANES
 - ADD A DEDICATED BUS LANE
 - ADD SCHOOL ZONE
 - FILL IN SIDEWALK GAPS
 - ADD BUS SHELTERS
 - ADD A LANDSCAPED MEDIAN
- (WRITE IN ANYTHING ELSE!)



User Preference Survey



DRAFT User Preference Survey
SR 580 (West Busch Boulevard) Corridor Study
 from Dale Mabry Hwy. to Nebraska Ave.

Please fill out this survey as best as you can and place in the Survey forms box.

- Home Zip code: _____
- Work/School zip code: _____
- Please circle all that apply:
 - I live on or near Busch Boulevard
 - I work on or near Busch Boulevard
 - I shop/visit businesses on or near Busch Boulevard
 - My school is on or near Busch Boulevard
 Which school? _____
 - I travel on Busch Boulevard but rarely stop
 - I don't regularly use Busch Boulevard
 - Other: _____

4. Which travel option(s) have you used on Busch Boulevard? Please circle all that apply:

- Drive
- Walk
- Bike
- HART Bus
- School Bus
- Share a ride with others (car, pool, boat, etc.)
- Other: _____

5. When driving on Busch Boulevard, rate your satisfaction on the following (please circle 1 – 5):

Safe	5	4	3	2	1	Unsafe	n/a
Comfortable	5	4	3	2	1	Uncomfortable	n/a
Convenient	5	4	3	2	1	Inconvenient	n/a

6. When walking/biking/riding the bus on Busch Boulevard, rate your satisfaction on the following (please circle 1 – 5):

Safe	5	4	3	2	1	Unsafe	n/a
Comfortable	5	4	3	2	1	Uncomfortable	n/a
Convenient	5	4	3	2	1	Inconvenient	n/a

7. How important are the following objectives in your vision for Busch Boulevard?

My Vision is for a Busch Boulevard where...	Very Important	Somewhat Important	Not Important	No Opinion
Transit users have shade and comfortable amenities.				
A continuous sidewalk runs the length of the corridor				
Bicycle connectivity is enhanced to adjacent homes and businesses.				
Safe and frequent options are provided for pedestrians and bicyclists to cross the corridor				
Aesthetic enhancements provide shade and add beauty to the corridor				
The corridor adds a unique and inviting character to the community				
Existing businesses are preserved along the corridor				
The roadway attracts reinvestment into the corridor and surrounding areas				
Travelers experience efficient and reliable travel times				
Facilities are ADA compliant				
Citizens with limited mobility have enough time to cross the street				
Congestion is reduced on the corridor				
Crashes occur less frequently and are not as severe				
Drivers slow down to a safe speed				
Increased lighting improves visibility for drivers and pedestrians				
Children can travel to and from school safely, regardless of mode of travel				
Consolidated driveways reduce conflict points for vehicles and pedestrians				

8. Which of the following potential improvements would you support?

	Strongly Agree	Agree	Neutral	Disagree	Strongly Disagree
Add additional travel lanes					
Create a pedestrian-only phase at key intersections					
Improve traffic signal timing					
Add on-street lighting					
Add mid-block crosswalks					
Landscaping, pedestrian lighting, etc.					
Lower speed limit					
Create a wide sidewalk along the entire corridor					
Add on-street bike lanes					
Add buffered bike lanes					
Add dedicated bus lane					
Add school zone					
Fill in sidewalk gaps					
Add bus shelters					
Add a landscaped median					

9. Any other comments and suggestions:

Thank you for taking the time to complete our User Preference Survey. Your valuable input will help inform the identification of challenges/issues and the development of potential solutions.

Next Steps



Next Steps - Schedule

- Define a corridor vision, goals and objectives
- Identify alternative near-term and long-term solutions to meet the identified goals and objectives
- Alternatives Public Meeting Spring 2018

SR 580/Busch Boulevard from North Dale Mabry Highway to North Nebraska Avenue Corridor Study

PUBLIC INVOLVEMENT PLAN SCHEDULE

ACTIVITY	2017												2018							
	Mar	Apr	May	June	July	Aug	Sept	Oct	Nov	Dec	Jan	Feb	Mar	April	May	June	July	Aug	Sept	
Notice to Proceed - Project Begins	★																			
Develop Public Involvement Plan																				
Mailing List																				
Elected Officials/Agency Kick-off Meeting				★																
Project Advisory Group (PAG) Meetings																				
Corridor Visioning Workshop																				
Alternatives Public Meeting																				
Final Corridor Alternatives & Strategies Report																				

Next Steps – Stay Connected

- Complete a **comment form**
- Submit a comment on our **website**
- **Email** your comments
- **Mail** written comments
- Take the **survey**

Brian Shroyer
*Project Manager for
Florida Department of Transportation*
11201 N. McKinley Drive
Tampa, FL 33612
Phone: (813) 975-6444
Email: Brian.Shroyer@dot.state.fl.us

Kristen Carson
*Public Involvement Officer
Florida Department of Transportation*
11201 N. McKinley Drive
Tampa, FL 33612
Phone: (813) 975-6202 or (800) 226-7220
Email: Kristen.Carson@dot.state.fl.us

www.fdotd7studies.com/westbuschblvd/

E-255



Thank you for attending!



Next Show Starts in:



APPENDIX E – Sign-in Sheets



CORRIDOR VISIONING WORKSHOP
West Busch Boulevard (SR 580) Corridor Study
 From North Dale Mabry Highway to North Nebraska Avenue
 Financial Project ID Number: 435908-1-22-01



Thursday, November 16, 2017 from 5:30 pm to 7:30 pm

Christian Family Church, 3457 W. Kenyon Avenue, Tampa, Florida 33614.

Sign-In Sheet

NAME (PLEASE PRINT)	AGENCY / ORGANIZATION	TITLE	PHONE	EMAIL
Jessica Francois	QUA		850 380 0079	Jessica.Francois@ch2m.com
Joe Belt	BEST ENGINEERING			
Bill Besseleur			813-263-5854	BESSELEUR@GARTL.COM
RAY CHIARAMONTE	TBARTA			
Karen Harrill	QCA		352-257-1651	Karen.Harrill@QCAUSA.com
Jennifer Bartlett				Jbartlett@twinkl.com
Carol Barker	CH2M		407-650-2173	carol.barker@ch2m.com
David Hernandez	CH2M			David.Hernandez@Ch2m.com
Chad Polk	CH2M		813-281-7912	chad.polk@ch2m.com
Jenn Rhodes	CH2M			jenn.rhodes@ch2m.com
Mark Jennings	CH2M			Mark.Jennings@ch2m.com
Cris Schaeck	CH2M			cris.schaeck@ch2m.com



CORRIDOR VISIONING WORKSHOP
West Busch Boulevard (SR 580) Corridor Study
 From North Dale Mabry Highway to North Nebraska Avenue
 Financial Project ID Number: 435908-1-22-01



Thursday, November 16, 2017 from 5:30 pm to 7:30 pm

Christian Family Church, 3457 W. Kenyon Avenue, Tampa, Florida 33614.

Sign-In Sheet

NAME (PLEASE PRINT)	AGENCY / ORGANIZATION	TITLE	PHONE	EMAIL
CELIN GRIMES		LANDOWNER	813/744-3169	TERRACE CARDESO VERTIZON, NEFT,
EDWIN MALETTE LECAE				
Faye Parsons Ken Kupperman	9344 Forest Hills Dr Tampa 33612			
Anna Alberts-Garcia	Home owner.	2109 W. Busch Blvd.		anke.garcia@gmail.com
Richard Garcia	" " "			
Tina Castle Petro Parra	The Planning Commission			
Sean Martin BRIAN L. SHOOKER	Well Built Bikes FDOT			Seannmartin@gmail.com info@wb

APPENDIX F – Handouts

Meeting Agenda

Newsletter #1

User Preference Survey



PUBLIC VISIONING WORKSHOP

AGENDA

West Busch Boulevard (SR 580) Corridor Study

from North Dale Mabry Highway to North Nebraska Avenue

Hillsborough County, Florida

FDOT Financial Project ID Number: 435908-1-22-01

Christian Family Church

November 16, 2017 – 5:30 pm to 7:00 pm

This Visioning Workshop is organized as an interactive open house. The opportunities for you to provide your input are outlined below. Project team staff are available to answer your questions, just look for the staff with nametags.

1. Power Point presentation to introduce the Corridor Study
2. Existing Conditions activity – provide input on issues in the corridor
3. Information on transportation characteristics of the corridor
4. Visioning Exercise – help define the project goals and objectives
5. Potential Improvement Exercise – identify important improvement opportunities
6. Provide feedback to the study team – comments, user preference survey

THANK YOU FOR YOUR PARTICIPATION TONIGHT

West Busch Boulevard (SR 580) Corridor Study

From North Dale Mabry Highway to North Nebraska Avenue

Hillsborough County, FL

Financial Project ID Number: 435908-1-22-01

Corridor Study Begins!

The Florida Department of Transportation (FDOT), District 7 is conducting a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue in Hillsborough County – a distance of approximately 3.3 miles. The West Busch Boulevard (SR 580) corridor is an east-west urban principal arterial connecting and serving local commuters and visitors between North Dale Mabry Highway and Nebraska Avenue. The corridor study area includes portions of unincorporated Hillsborough County and the City of Tampa and is the main route to the Busch Gardens Amusement Park (located approximately 2.15 miles east of the project limits at McKinley Drive).

The corridor supports a variety of land uses and demographics, including schools, churches, large and small businesses with direct access to and from SR 580, transit services, pedestrians, bicyclists and many small communities. The corridor also serves a large commuter population as a connection between Veterans Expressway to the west and I-75 to the east.



The West Busch Boulevard (SR 580) corridor study will use FDOT's Complete Streets policy to showcase how Complete Streets approaches can shape a corridor. The study, which began in March 2017, is expected to continue for approximately 18 months and will include technical work and data analysis, along with various opportunities for the public to be involved in discussing a vision for West Busch Boulevard (SR 580) that makes it accessible for all users.

What is a Corridor Study?

A corridor study is the first step in planning by defining the corridor's needs. A corridor study generally defines the relationships between a roadway and its adjacent land. Corridor studies are also used to: define acceptable levels of access and mobility; determine transportation system needs to support surrounding land uses, and identify operational deficiencies, among other things. This corridor study will include establishing a shared vision that will allow the community to help set project goals and objectives. The vision will guide the project team to develop a set of improvements that leverage local resources, match local conditions and desires, and are consistent with ongoing planning and development efforts.

Project Schedule

The West Busch Boulevard (SR 580) Corridor Study began in March 2017 and is anticipated to be completed in 18 months. Project activities will include:

- Identifying the travel related needs and issues within the West Busch Boulevard project corridor.
- Coordinating with key stakeholders to establish the vision for the corridor and identify solutions to the corridor issues and improve operational efficiency within the West Busch Boulevard corridor.
- Document the vision for all future improvements within the West Busch Boulevard corridor to maximize the “livability” for all users.

ACTIVITY	2017												2018						
	Mar	Apr	May	June	July	Aug	Sept	Oct	Nov	Dec	Jan	Feb	Mar	April	May	June	July	Aug	Sept
Notice to Proceed - Project Begins	★																		
Develop Public Involvement Plan																			
Mailing List																			
Elected Officials/Agency Kick-off Meeting				★															
Project Advisory Group (PAG) Meetings																			
Corridor Visioning Workshop									★										
Alternatives Public Meeting													★						
Final Corridor Alternatives & Strategies Report																			

Project Advisory Group (PAG)

A Project Advisory Group (PAG), composed of local government and agency representatives, are working with FDOT to identify corridor challenges along with potential solutions, assist with documenting the community goals, and develop a vision for the corridor that will guide the study development and future improvements. The PAG will meet at key points throughout the study to collaborate with the project team to ensure the vision and improvement alternatives are well-coordinated with local planning efforts, adjacent projects, and community goals.

How Can You Get Involved?

The project website is your best source for up-to-date information; you can get access at this link:

www.fdotd7studies.com/westbuschblvd/. In addition to the general study information that is already posted on the website, the project documents will be uploaded to this site as they are developed. However, you can already begin providing your input on this project using WikiMapping! This tool allows you to “pin” comments at specific locations along the corridor. You can pick the type of comment you have to help us understand the issues you observe in the corridor. Some of these issue types include: Bus/Transit, Connectivity, Intersection Issue, Pedestrian, Safety, and Traffic/Parking. If you have a comment on an issue that does not have a specific description, just use the “Other” category. The link to the WikiMapping tool is provided on the project website, but for direct access you can click on the following link:

<http://wikimapping.com/wikimap/West-Busch-Boulevard-Corridor-Study.html>. The project team will utilize this information to help inform the process and guide solutions.

Future opportunities for you to get involved include the upcoming Corridor Visioning Workshop that will be conducted in the Fall 2017. At this workshop, we will gather your input on the existing corridor and document your ideas to resolve some of the issues that you have observed. Our goal is to enlist your help to identify improvements that can be implemented in the West Busch Boulevard corridor to resolve the challenges. In advance of this workshop, a User Preference Survey will be available on the website in October; hard copies of this same survey will be available at the Corridor Visioning Workshop. These surveys will be used by the project team to understand the issues and concerns that you have regarding the corridor.

We want your input! We urge anyone interested in shaping the future of the West Busch Boulevard corridor to actively participate by visiting the website, get informed, document issues in the WikiMapping tool, attend meetings, and provide your ideas.

For more information or to comment, please contact:

Brian Shroyer, Project Manager
11201 N. McKinley Drive
Tampa, FL 33612
Office Phone: (813) 975-6449
brian.shroyer@dot.state.fl.us

Kristen Carson, Public Information Officer
11201 N. McKinley Drive
Tampa, FL 33612
Office Phone: (813) 975-6202 or 800-226-7220
kristen.carson@dot.state.fl.us

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT.

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220.

Comuniquese Con Nosotros

Nos importa mucho la opinión del público sobre el proyecto. Si usted tiene preguntas o comentarios, o simplemente desea más información, por favor comuníquese con nosotros. Nuestro representante, Señora Lilliam Escalera, 813-975-6445, Departamento de Transportación de Florida, 11201 N. McKinley Dr., Tampa, FL 33612, lilliam.escalera@dot.state.fl.us.

Please fill out this survey as best as you can and place in the Survey Forms box.

1. Home Zip code: _____

2. Work/School zip code: _____

3. Please **circle** all that apply:

- a. I live on or near Busch Boulevard
- b. I work on or near Busch Boulevard
- c. I shop/visit businesses on or near Busch Boulevard
- d. My school is on or near Busch Boulevard
Which school? _____
- e. I travel on Busch Boulevard but rarely stop
- f. I don't regularly use Busch Boulevard
- g. Other: _____

4. Which travel option(s) have you used on Busch Boulevard? Please **circle** all that apply:

- a. Drive
- b. Walk
- c. Bike
- d. HART Bus
- e. School Bus
- f. Share a ride with others (car pool, taxi, etc.)
- g. Other: _____

5. When **driving** on Busch Boulevard, rate your satisfaction on the following (**please circle 1 – 5**):

Safe	5	4	3	2	1	Unsafe	n/a
Comfortable	5	4	3	2	1	Uncomfortable	n/a
Convenient	5	4	3	2	1	Inconvenient	n/a

Additional Comments: _____

6. When **walking/biking/riding** the bus on Busch Boulevard, rate your satisfaction on the following (**please circle 1 – 5**):

Safe	5	4	3	2	1	Unsafe	n/a
Comfortable	5	4	3	2	1	Uncomfortable	n/a
Convenient	5	4	3	2	1	Inconvenient	n/a

Additional Comments: _____

7. How important are the following objectives in your vision for Busch Boulevard?

My Vision is for a Busch Boulevard where...	Very Important	Somewhat Important	Not Important	No Opinion
Transit users have shade and comfortable amenities				
A continuous sidewalk runs the length of the corridor				
Bicycle connectivity is enhanced to adjacent homes and businesses				
Safe and frequent options are provided for pedestrians and bicyclists to cross the corridor				
Aesthetic enhancements provide shade and adds beauty to the corridor				
The corridor adds a unique and inviting character to the community				
Existing businesses are preserved along the corridor				
The roadway attracts reinvestment into the corridor and surrounding areas				
Travelers experience efficient and reliable travel times				
Facilities are ADA compliant				
Citizens with limited mobility have enough time to cross the street				
Congestion is reduced on the corridor				
Crashes occur less frequently and are not as severe				
Drivers slow down to a safe speed				
Increased lighting improves visibility for drivers and pedestrians				
Children can travel to and from school safely, regardless of mode of travel				
Consolidated driveways reduce conflict points for vehicles and pedestrians				

Additional Comments:

8. Which of the following potential improvements would you support?

	Strongly Agree	Agree	Neutral	Disagree	Strongly Disagree
Add additional travel lanes					
Create a pedestrian-only phase at key intersections					
Improve traffic signal timing					
Add on-street parking					
Add mid-block crosswalks					
Landscaping, pedestrian lighting, etc.					
Lower speed limit					
Create a wide sidewalk along the entire corridor					
Add on-street bike lanes					
Add buffered bike lanes					
Add dedicated bus lane					
Add school zone					
Fill in sidewalk gaps					
Add bus shelters					
Add a landscaped median					

Additional Comments:

9. Any other comments and suggestions:

Thank you for taking the time to complete our User Preference Survey. Your valuable input will help inform the identification of challenges/issues and the development of potential solutions.

APPENDIX G – Public Comments



CORRIDOR VISIONING WORKSHOP
West Busch Boulevard (SR 580) Corridor Study
 From North Dale Mabry Highway to North Nebraska Avenue
 Financial Project ID Number: 435908-1-22-01



Thursday, November 16, 2017 Christian Family Church, 3457 W Kenyon Ave, Tampa, Florida 33614

Please provide your comments below. If more space is needed, please use the back of this sheet or an additional sheet of paper. You may place your comments in the "Comment Box" provided at the meeting, or send to Brian Shroyer, Project Manager at the address on the back of this comment form, by November 27, 2017. Emailed comments are also acceptable to Brian.Shroyer@dot.state.fl.us

*All comments are part of the project record and are available for review by the public and the media.

LEAVE PLENTY OF ROOM FOR TRUCK DELIVERY
 TO BUSINESSES ALONG BUSCH BLVD. SAFETY
 BEING MAIN CONCERN.

CONTACT INFORMATION (PLEASE PRINT)

Name COLIN GRIMES
 Title LANDOWNER
 Agency / Organization TERRACE CARPETS
 Phone Number 813/711-3169
 Email TERRACECARPETS@VERIZON.NET

PLEASE RETURN COMMENTS BY NOV 27, 2017 TO:

Brian L. Shroyer
 Multimodal Project Manager
 Florida Department of Transportation
 11201 N. McKinley Drive, MS 7-500
 Tampa, FL 33612
 Brian.Shroyer@dot.state.fl.us

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status.

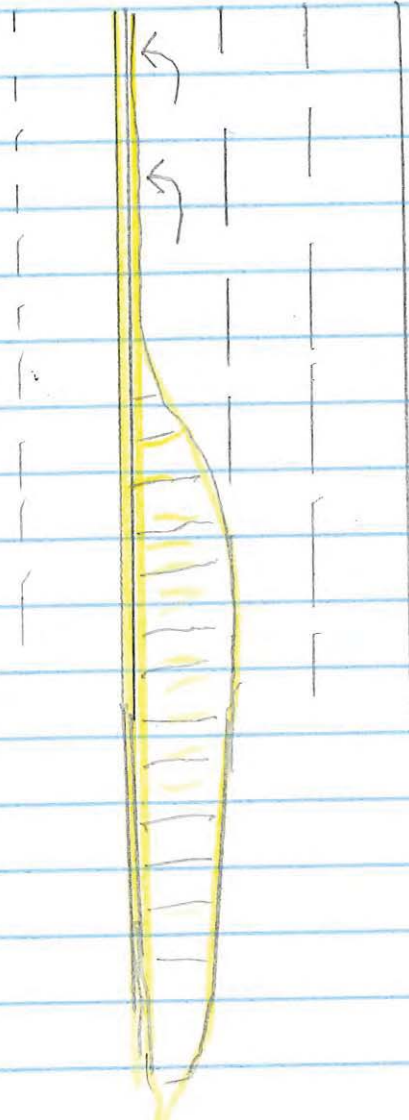
If you continue to issue Licenses to people who come to this country from countries where they do not drive -- you can't solve Road problems

- 1) people don't use their blinkers
- 2) people don't use their mirrors
- 3) people don't know our cultures
- 4) slow drives using fast Lane
- 5) temp-licenses should be issued until they come back and take another test



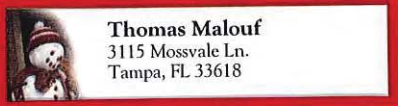
Thomas Malouf
3115 Mossvale Ln.
Tampa, FL 33618

Roadway



Yellow dead zone should be eliminated to make room for more cars turning

- A) Busch Blvd & Homes
- B) Florida Ave & Busch Blvd

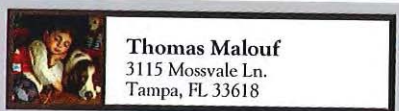


Sidewalks

- 1) Complete sidewalk south-side Busch Blvd.
between N. Blvd + Armenia
- 2) make sidewalk south side of Busch Blvd
a BIKE path --
- 3) Bikes should not be on road
(Not enough room)

Rail Road Right-A-WAY

The R/R company should be responsible
for cleaning their property at least
twice @ year



USER PREFERENCE SURVEY



BOULEVARD STUDY

Please fill out this survey as best as you can and place in the Survey Forms box.

1. Home Zip code: 33618

2. Work/School zip code: 33618

3. Please circle all that apply:

- a. I live on or near Busch Boulevard
- b. I work on or near Busch Boulevard
- c. I shop/visit businesses on or near Busch Boulevard
- d. My school is on or near Busch Boulevard
Which school? _____
- e. I travel on Busch Boulevard but rarely stop
- f. I don't regularly use Busch Boulevard
- g. Other: _____

4. Which travel option(s) have you used on Busch Boulevard? Please circle all that apply:

- a. Drive
- b. Walk
- c. Bike
- d. HART Bus
- e. School Bus
- f. Share a ride with others (car pool, taxi, etc.)
- g. Other: _____

5. When driving on Busch Boulevard, rate your satisfaction on the following (please circle 1 - 5):

Safe	5	4	3	2	1	<u>Unsafe</u>	n/a
Comfortable	5	4	3	2	<u>1</u>	Uncomfortable	n/a
Convenient	5	4	<u>3</u>	2	1	Inconvenient	n/a

Additional Comments: Too Much traffic. Too Fast. No improvements in years. Very UNSAFE.

6. When walking/biking/riding the bus on Busch Boulevard, rate your satisfaction on the following (please circle 1 - 5):

Safe	5	4	3	<u>2</u>	1	Unsafe	n/a
Comfortable	5	4	3	<u>2</u>	1	Uncomfortable	n/a
Convenient	5	4	<u>3</u>	2	1	Inconvenient	n/a

Additional Comments: Does Not Apply fo Bus. Walking Very dangerous

USER PREFERENCE SURVEY



BOULEVARD STUDY

7. How important are the following objectives in your vision for Busch Boulevard?

My Vision is for a Busch Boulevard where...	Very Important	Somewhat Important	Not Important	No Opinion
Transit users have shade and comfortable amenities	✓			
A continuous sidewalk runs the length of the corridor	✓			
Bicycle connectivity is enhanced to adjacent homes and businesses	✓			
Safe and frequent options are provided for pedestrians and bicyclists to cross the corridor	✓			
Aesthetic enhancements provide shade and adds beauty to the corridor	✓			
The corridor adds a unique and inviting character to the community	✓			
Existing businesses are preserved along the corridor			✓	
The roadway attracts reinvestment into the corridor and surrounding areas		✓		
Travelers experience efficient and reliable travel times	✓			
Facilities are ADA compliant		✓		
Citizens with limited mobility have enough time to cross the street	✓			
Congestion is reduced on the corridor	✓	✓		
Crashes occur less frequently and are not as severe	✓			
Drivers slow down to a safe speed	✓	✓		
Increased lighting improves visibility for drivers and pedestrians	✓			
Children can travel to and from school safely, regardless of mode of travel	✓			
Consolidated driveways reduce conflict points for vehicles and pedestrians	✓			

Additional Comments:

Cycle lights and Reduce speeds

USER PREFERENCE SURVEY



8. Which of the following potential improvements would you support?

	Strongly Agree	Agree	Neutral	Disagree	Strongly Disagree
Add additional travel lanes			✓		
Create a pedestrian-only phase at key intersections	✓				
Improve traffic signal timing	✓				
Add on-street parking			✓		
Add mid-block crosswalks	✓				
Landscaping, pedestrian lighting, etc.	✓				
Lower speed limit	✓				
Create a wide sidewalk along the entire corridor		✓			
Add on-street bike lanes		✓			
Add buffered bike lanes		✓			
Add dedicated bus lane		✓			
Add school zone	✓				
Fill in sidewalk gaps	✓				
Add bus shelters		✓			
Add a landscaped median	✓				

Additional Comments:

A new East-West High Capacity Road is needed in North Hillsborough. Elevated and Widen Bearrs & Rich from Xpressway to 275

9. Any other comments and suggestions:

New overpass at Dale Mabry & Waters and Dale Mabry & Busch

Thank you for taking the time to complete our User Preference Survey. Your valuable input will help inform the identification of challenges/issues and the development of potential solutions.

Now over 2000 of DoI
EPA projects = 1700
to the number of
B. 21
in 1983

USER PREFERENCE SURVEY



Please fill out this survey as best as you can and place in the Survey Forms box.

1. Home Zip code: 33618

2. Work/School zip code: 33606

3. Please circle all that apply:

- | | |
|--|--|
| <input checked="" type="checkbox"/> a. I live on or near Busch Boulevard | <input checked="" type="checkbox"/> e. I travel on Busch Boulevard but rarely stop |
| b. I work on or near Busch Boulevard | f. I don't regularly use Busch Boulevard |
| c. I shop/visit businesses on or near Busch Boulevard | g. Other: _____ |
| d. My school is on or near Busch Boulevard | _____ |
| Which school? _____ | |

4. Which travel option(s) have you used on Busch Boulevard? Please circle all that apply:

- | | |
|--|--|
| <input checked="" type="checkbox"/> a. Drive | e. School Bus |
| b. Walk | f. Share a ride with others (car pool, taxi, etc.) |
| c. Bike | g. Other: _____ |
| d. HART Bus | _____ |

5. When driving on Busch Boulevard, rate your satisfaction on the following (please circle 1 – 5):

Safe	5	4	<input checked="" type="radio"/> 3	2	1	Unsafe	n/a
Comfortable	5	4	<input checked="" type="radio"/> 3	2	1	Uncomfortable	n/a
Convenient	5	4	<input checked="" type="radio"/> 3	2	1	Inconvenient	n/a

Additional Comments: _____

6. When walking/biking/riding the bus on Busch Boulevard, rate your satisfaction on the following (please circle 1 – 5):

Safe	5	4	3	2	1	Unsafe	n/a
Comfortable	5	4	3	2	1	Uncomfortable	n/a
Convenient	5	4	3	2	1	Inconvenient	n/a

Additional Comments: don't bike there

USER PREFERENCE SURVEY



7. How important are the following objectives in your vision for Busch Boulevard?

My Vision is for a Busch Boulevard where...	Very Important	Somewhat Important	Not Important	No Opinion
Transit users have shade and comfortable amenities	✓			
A continuous sidewalk runs the length of the corridor	✓			
Bicycle connectivity is enhanced to adjacent homes and businesses	✓			
Safe and frequent options are provided for pedestrians and bicyclists to cross the corridor	✓			
Aesthetic enhancements provide shade and adds beauty to the corridor	✓			
The corridor adds a unique and inviting character to the community		✓		
Existing businesses are preserved along the corridor		✓		
The roadway attracts reinvestment into the corridor and surrounding areas	✓			
Travelers experience efficient and reliable travel times	✓			
Facilities are ADA compliant				
Citizens with limited mobility have enough time to cross the street	✓			
Congestion is reduced on the corridor		✓		
Crashes occur less frequently and are not as severe	✓			
Drivers slow down to a safe speed	✓			
Increased lighting improves visibility for drivers and pedestrians		✓		
Children can travel to and from school safely, regardless of mode of travel	✓			
Consolidated driveways reduce conflict points for vehicles and pedestrians	✓			

Additional Comments:

USER PREFERENCE SURVEY



8. Which of the following potential improvements would you support?

	Strongly Agree	Agree	Neutral	Disagree	Strongly Disagree
Add additional travel lanes				✓	
Create a pedestrian-only phase at key intersections		✓			
Improve traffic signal timing			✓		
Add on-street parking				✓	
Add mid-block crosswalks		✓			
Landscaping, pedestrian lighting, etc.	✓				
Lower speed limit		✓			
Create a wide sidewalk along the entire corridor		✓			
Add on-street bike lanes				✓	
Add buffered bike lanes		✓			
Add dedicated bus lane		✓			
Add school zone	✓				
Fill in sidewalk gaps	✓				
Add bus shelters	✓				
Add a landscaped median			✓		

Additional Comments:

9. Any other comments and suggestions:

Thank you for taking the time to complete our User Preference Survey. Your valuable input will help inform the identification of challenges/issues and the development of potential solutions.

USER PREFERENCE SURVEY



Please fill out this survey as best as you can and place in the Survey Forms box.

1. Home Zip code: 33618

2. Work/School zip code: 33604

3. Please circle all that apply:

- a. I live on or near Busch Boulevard
- b. I work on or near Busch Boulevard
- c. I shop/visit businesses on or near Busch Boulevard
- d. My school is on or near Busch Boulevard
Which school? _____
- e. I travel on Busch Boulevard but rarely stop
- f. I don't regularly use Busch Boulevard
- g. Other: I Live on Busch Blvd

4. Which travel option(s) have you used on Busch Boulevard? Please circle all that apply:

- a. Drive
- b. Walk
- c. Bike
- d. HART Bus
- e. School Bus
- f. Share a ride with others (car pool, taxi, etc.)
- g. Other: _____

5. When driving on Busch Boulevard, rate your satisfaction on the following (please circle 1 – 5):

Safe	5	4	3	2	1	Unsafe	n/a
Comfortable	5	4	3	2	1	Uncomfortable	n/a
Convenient	5	4	3	2	1	Inconvenient	n/a

Additional Comments: Make a Turn Lane!!

6. When walking/biking/riding the bus on Busch Boulevard, rate your satisfaction on the following (please circle 1 – 5):

Safe	5	4	3	2	1	Unsafe	n/a
Comfortable	5	4	3	2	1	Uncomfortable	n/a
Convenient	5	4	3	2	1	Inconvenient	n/a

Additional Comments: _____

USER PREFERENCE SURVEY



7. How important are the following objectives in your vision for Busch Boulevard?

My Vision is for a Busch Boulevard where...	Very Important	Somewhat Important	Not Important	No Opinion
Transit users have shade and comfortable amenities				✓
A continuous sidewalk runs the length of the corridor	✓			
Bicycle connectivity is enhanced to adjacent homes and businesses				✓
Safe and frequent options are provided for pedestrians and bicyclists to cross the corridor	✓			
Aesthetic enhancements provide shade and adds beauty to the corridor		✓		
The corridor adds a unique and inviting character to the community		✓		
Existing businesses are preserved along the corridor		✓		
The roadway attracts reinvestment into the corridor and surrounding areas			✓	
Travelers experience efficient and reliable travel times	✓			
Facilities are ADA compliant				✓
Citizens with limited mobility have enough time to cross the street				✓
Congestion is reduced on the corridor	✓			
Crashes occur less frequently and are not as severe	✓			
Drivers slow down to a safe speed	✓			
Increased lighting improves visibility for drivers and pedestrians		✓		
Children can travel to and from school safely, regardless of mode of travel	✓			
Consolidated driveways reduce conflict points for vehicles and pedestrians	✓			

Additional Comments:

WE need a turn lane

USER PREFERENCE SURVEY



8. Which of the following potential improvements would you support?

	Strongly Agree	Agree	Neutral	Disagree	Strongly Disagree
Add additional travel lanes (turn lane)	✓				
Create a pedestrian-only phase at key intersections	✓				
Improve traffic signal timing		✓			
Add on-street parking					✓
Add mid-block crosswalks					✓
Landscaping, pedestrian lighting, etc.					✓
Lower speed limit	✓	Agree			
Create a wide sidewalk along the entire corridor					✓
Add on-street bike lanes					✓
Add buffered bike lanes			✓		
Add dedicated bus lane					✓
Add school zone			✓		
Fill in sidewalk gaps			✓		
Add bus shelters					✓
Add a landscaped median					✓

Additional Comments:

create a turn lane for those of us that live on Busch Blvd.

9. Any other comments and suggestions:

turn lane & reduce speeds so those of us that live on busch don't get rear ended or can't even get out of our driveways

Thank you for taking the time to complete our User Preference Survey. Your valuable input will help inform the identification of challenges/issues and the development of potential solutions.

USER PREFERENCE SURVEY



Please fill out this survey as best as you can and place in the Survey Forms box.

1. Home Zip code: 33618

2. Work/School zip code: 33602

3. Please circle all that apply:

- a. I live on or near Busch Boulevard
- b. I work on or near Busch Boulevard
- c. I stop/visit businesses on or near Busch Boulevard
- d. My school is on or near Busch Boulevard
Which school? _____
- e. I travel on Busch Boulevard but rarely stop
- f. I don't regularly use Busch Boulevard
- g. Other: _____

4. Which travel option(s) have you used on Busch Boulevard? Please circle all that apply:

- a. Drive
- b. Walk
- c. Bike
- d. HART Bus
- e. School Bus
- f. Share a ride with others (car pool, taxi, etc.)
- g. Other: _____

5. When driving on Busch Boulevard, rate your satisfaction on the following (please circle 1 - 5):

Safe	5	4	3	2	<input checked="" type="radio"/> 1	Unsafe	n/a
Comfortable	5	4	3	2	<input checked="" type="radio"/> 1	Uncomfortable	n/a
Convenient	5	4	<input checked="" type="radio"/> 3	2	1	Inconvenient	n/a

Additional Comments: Impossible to get out of driveway at certain times

6. When walking/biking/riding the bus on Busch Boulevard, rate your satisfaction on the following (please circle 1 - 5):

Safe	5	4	3	2	<input checked="" type="radio"/> 1	Unsafe	n/a
Comfortable	5	4	3	2	<input checked="" type="radio"/> 1	Uncomfortable	n/a
Convenient	5	4	3	2	<input checked="" type="radio"/> 1	Inconvenient	n/a

Additional Comments: _____

USER PREFERENCE SURVEY



30 BOULEVARD STUDY

7. How important are the following objectives in your vision for Busch Boulevard?

My Vision is for a Busch Boulevard where...	Very Important	Somewhat Important	Not Important	No Opinion
Transit users have shade and comfortable amenities			X	
A continuous sidewalk runs the length of the corridor	X			
Bicycle connectivity is enhanced to adjacent homes and businesses		X		
Safe and frequent options are provided for pedestrians and bicyclists to cross the corridor	X			
Aesthetic enhancements provide shade and adds beauty to the corridor			X	
The corridor adds a unique and inviting character to the community				X
Existing businesses are preserved along the corridor			X	
The roadway attracts reinvestment into the corridor and surrounding areas			X	
Travelers experience efficient and reliable travel times	X			
Facilities are ADA compliant			X	
Citizens with limited mobility have enough time to cross the street	X			
Congestion is reduced on the corridor	X			
Crashes occur less frequently and are not as severe	X			
Drivers slow down to a safe speed	X			
Increased lighting improves visibility for drivers and pedestrians	X			
Children can travel to and from school safely, regardless of mode of travel	X			
Consolidated driveways reduce conflict points for vehicles and pedestrians	X			

Additional Comments:

USER PREFERENCE SURVEY



COULEVARD STUDY

8. Which of the following potential improvements would you support?

	Strongly Agree	Agree	Neutral	Disagree	Strongly Disagree
Add additional travel lanes		X			
Create a pedestrian-only phase at key intersections		X			
Improve traffic signal timing	X				
Add on-street parking				X	
Add mid-block crosswalks			X		
Landscaping, pedestrian lighting, etc.		X			
Lower speed limit		X			
Create a wide sidewalk along the entire corridor		X			
Add on-street bike lanes		X			
Add buffered bike lanes		X			
Add dedicated bus lane				X	
Add school zone			X		
Fill in sidewalk gaps		X			
Add bus shelters			X		
Add a landscaped median			X		

Additional Comments:

9. Any other comments and suggestions:

Too congested, unsafe to turn into or out of driveway

Thank you for taking the time to complete our User Preference Survey. Your valuable input will help inform the identification of challenges/issues and the development of potential solutions.



USER PREFERENCE SURVEY



Please fill out this survey as best as you can and place in the Survey Forms box.

1. Home Zip code: 33625 - Citrus Park

2. Work/School zip code: 33612 - FDOT D7

3. Please circle all that apply:

- a. I live on or near Busch Boulevard
- b. I work on or near Busch Boulevard
- c. I shop/visit businesses on or near Busch Boulevard
- d. My school is on or near Busch Boulevard
Which school? _____
- e. I travel on Busch Boulevard but rarely stop
- f. I don't regularly use Busch Boulevard
- g. Other: _____

4. Which travel option(s) have you used on Busch Boulevard? Please circle all that apply:

- a. Drive
- b. Walk
- c. Bike
- d. HART Bus (Bus is inconvenient - takes too long to destination)
- e. School Bus
- f. Share a ride with others (car pool, taxi, etc.)
- g. Other: _____

5. When driving on Busch Boulevard, rate your satisfaction on the following (please circle 1 – 5):

Safe	5	4	3	2	1	<u>Unsafe</u>	n/a
Comfortable	5	4	3	2	1	<u>Uncomfortable</u>	n/a
Convenient	5	4	3	2	1	<u>Inconvenient</u>	n/a

Additional Comments: Feels unsafe with double line and no median or turn lane from Dale Mabry to Armenia, cars travelling in left thru lane of opposite direction often drift (cell phone use) into oncoming traffic. Students/parents perform unsafe left turns from SB Ola onto EB Busch next to Chamberlain HS - drivers creeping through stopped WB traffic lanes try to turn left in front of oncoming traffic in EB lanes of Busch. Left EB Busch travel lane turning left/NB to Ola.

6. When walking/biking/riding the bus on Busch Boulevard, rate your satisfaction on the following (please circle 1 – 5):

Safe	5	4	3	2	1	<u>Unsafe</u>	n/a
Comfortable	5	4	3	2	1	<u>Uncomfortable</u>	n/a
Convenient	5	4	3	2	1	<u>Inconvenient</u>	n/a

Additional Comments: Pedestrians from temp employment office cross Busch midblock to get to bus stop or convenience store on north side. Students in particular practice unsafe crossing habits when running late for school.

USER PREFERENCE SURVEY



7. How important are the following objectives in your vision for Busch Boulevard?

My Vision is for a Busch Boulevard where...	Very Important	Somewhat Important	Not Important	No Opinion
Transit users have shade and comfortable amenities		X		
A continuous sidewalk runs the length of the corridor		X		
Bicycle connectivity is enhanced to adjacent homes and businesses		X		
Safe and frequent options are provided for pedestrians and bicyclists to cross the corridor - emphasis on student safety		X		
Aesthetic enhancements provide shade and adds beauty to the corridor			X	
The corridor adds a unique and inviting character to the community			X	
Existing businesses are preserved along the corridor			X (ROW needed)	
The roadway attracts reinvestment into the corridor and surrounding areas			X	
Travelers experience efficient and reliable travel times	**X**			
Facilities are ADA compliant		X		
Citizens with limited mobility have enough time to cross the street		X		
Congestion is reduced on the corridor	**X**			
Crashes occur less frequently and are not as severe	X			
Drivers slow down to a safe speed		X		
Increased lighting improves visibility for drivers and pedestrians		X		
Children can travel to and from school safely, regardless of mode of travel		X		
Consolidated driveways reduce conflict points for vehicles and pedestrians		X		

Additional Comments:

Chamberlain students have been observed running/dodging through traffic to cross Busch heading south, then deliberately racing to cross the CSX ROW ahead of oncoming train.

USER PREFERENCE SURVEY



8. Which of the following potential improvements would you support?

	Strongly Agree	Agree	Neutral	Disagree	Strongly Disagree
Add additional travel lanes	XXX				
Create a pedestrian-only phase at key intersections		X			
Improve traffic signal timing	XXX				
Add on-street parking					X
Add mid-block crosswalks					X
Landscaping, pedestrian lighting, etc.				X	
Lower speed limit					X
Create a wide sidewalk along the entire corridor	X				
Add on-street bike lanes			X		
Add buffered bike lanes			X		
Add dedicated bus lane (consider bus bays if possible)	X				
Add school zone			X		
Fill in sidewalk gaps		X			
Add bus shelters		X			
Add a landscaped median (consider pedestrian refuge)		X			

Additional Comments:

The traffic signal timing along this corridor negatively impacts congestion. Improvements to the signal timing will allow traffic to flow in a continuous manner. Synchronizing the signals to turn green or red in a sequenced pattern would smooth out traffic flow.

9. Any other comments and suggestions:

The signal at Busch and Armenia is among the worst in the corridor. However, inbound and outbound Chamberlain traffic attempting to make left turns onto Ola from Busch or left turns from Ola onto Busch clog traffic flow also. A center median preventing left turns would improve safety at this intersection. Students from Chamberlain HS have been observed dodging traffic mid block crossing or at the North Blvd crosswalk without waiting for the pedestrian crossing signal. They have also been observed deliberately racing to cross the CSX tracks before an oncoming train reaches the intersection at North Blvd - this risk taking behavior poses a major safety hazard to the students.

Thank you for taking the time to complete our User Preference Survey. Your valuable input will help inform the identification of challenges/issues and the development of potential solutions.



Local Agency Coordination- City of Tampa

PREPARED BY: Cris Schooley (CH2M)
PROJECT: SR 580/Busch Blvd. from N. Dale Mabry Hwy. to N. Nebraska Ave. Hillsborough County
FPID 435908-1-22-01 (PD&E) and 435908-2-32-01 (Corridor Design)
MEETING DATE: April 12, 2018
MEETING TIME: 2:30 PM to 4:00 PM
LOCATION: 306 E. Jackson Street 6th Floor North Wing
ATTENDEES: See Sign-in

1. Introductions

Jenn Rhodes/CH2M began the meeting. Cris Schooley/CH2M, RRR PM Indike Ratnayake/FDOT, and Calvin Thornton/Tampa introduced themselves.

2. Presentation

Jenn and Cris presented the attached slides. Jenn, Cris, and Indike answered questions and responded to comments.

3. Discussion Items

- a. School access is a critical concern for the City. **Agree, multiple recommendations propose improving safe access to the schools along North Boulevard that are accessed by crossing Busch Blvd.**
- b. City is not necessarily opposed to showing on-street parking as an option between Armenia Ave and North Boulevard, but parking enforcement would be a concern. **We can discuss the possibility of on-street parking in the report and note that City controlled development changes would be needed to implement.**
- c. The future transit-rail corridor could potentially lead to Transit Oriented Development (TOD) along Busch Blvd. **We will review the documentation and note TOD potential as appropriate.**
- d. Area lighting, as opposed to just street lighting, may be better for Crime Prevention Through Environmental Design. That way there are less shadows between the sidewalk and adjacent buildings. Police prefer this as well. The City typically uses TECO to design, install, and maintain their lighting. TECO does not maintain lighting they don't design. City might prefer to lead the lighting project as a LAP.
- e. An additional signalized intersection at Rome is preferable to a pedestrian crossing with RFBs or a mid-block crossing.
- f. Signals were connected with copper communication lines within city limits. This may or may not still be the case. **Upcoming project is proposing fiber optic interconnection cables. Discussed that it is common for the City and County to trade signals so that logical corridors are interconnected.**

- g. A city overlay district might not be seen as feasible here in the short-term, but is being used in other parts of the city to improve the pedestrian environment in the right of way and in the adjacent development. In the long-term, can see the benefit of an overlay district to achieve the vision. Calvin stated he is interested in the Planning Commission staff's input on this idea. Ultimately, this recommendation would have to be coordinated with the City of Tampa planning staff at the City and Planning Commission.
- h. The CSX railroad creates a neighborhood barrier issue. Discussed children jumping the fence to cross the corridor. ***This is true but options are limited. City could explore a pedestrian underpass at Jones Ave.***
- i. MPO has a walk/bike improvement map with more detail. Specifically, MPO Bike/Walk Plan #2 on MPO website. ***MPO has indicated they will send us an updated bike plan.***
- j. Calvin stated that he is good with the FDOT showing all of the recommended improvements discussed during the meeting at the Public Alternatives Meeting and will take the input gathered back to the city. Final determination of which improvements the city will partner on implementing depend on the overall feedback and interest of the community.

4. Action Items

Task	Description	Responsible Person	Status	Comments

5. Next Scheduled Meeting: Project Advisory Group meeting #4 May 24, 2018. Time TBA.



LOCAL AGENCY COORDINATION MEETING

DRAFT AGENDA

West Busch Boulevard (SR 580) Corridor Study

from North Dale Mabry Highway to North Nebraska Avenue
Hillsborough County, Florida

FDOT Financial Project ID Number: 435908-1-22-01

601 E Kennedy Blvd, 18th floor, Tampa, FL, 33602

April 12, 2018

1. Introductions
2. Project Overview
3. Typical Section Alternatives
4. Inter-agency recommendations (Interim Design)
5. Other Recommendations

West Busch Boulevard (SR 580) Corridor Study

from North Dale Mabry Highway to North Nebraska Avenue

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FDOT Financial Project ID Number: 435908-1-22-01

Local Agency Coordination Meeting



Purpose of this meeting

1. Discuss draft typical section alternatives
2. Gauge agency interest in recommendations for further study which would require inter-agency coordination (JPA, maintenance agreements, etc.)



Agenda

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2. Project Overview
3. Typical Section Alternatives
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Introductions

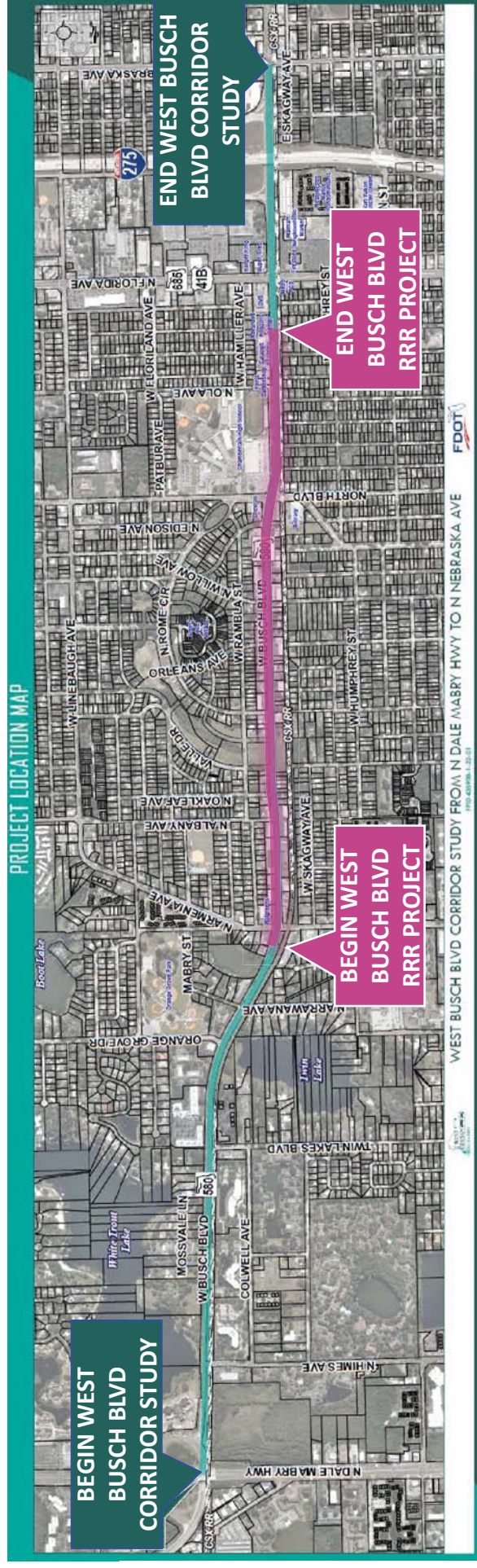


Project Overview



Project Study Area

- 3.3 miles
- Within the city of Tampa and unincorporated Hillsborough County
- Concurrent RRR project



Study Purpose

Achieve the Corridor Vision:

*“The Busch Boulevard corridor offers **safe, comfortable and convenient access through and across the corridor for all users and all travel modes.**”*

PD&E Study Changes
(implement typical section changes)

Interim Design Project Changes
(implement spot improvements)

Resurfacing, Restoration and Rehabilitation Project (3R) Changes

Busch Boulevard Corridor Study
(identify the vision, recommendations & implementation plan)

**We
Are
Here**

Updated Study Schedule

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Long Term Recommendations: Typical Section Alternatives

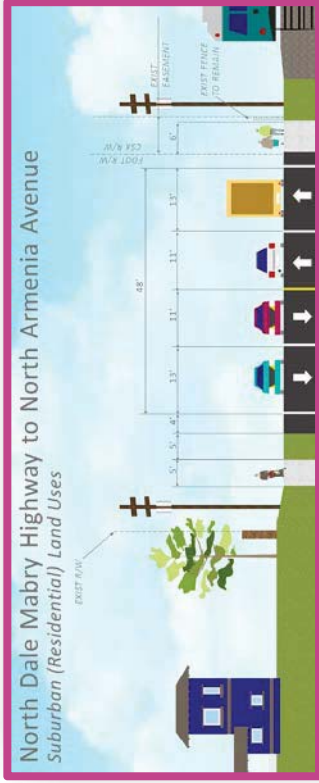


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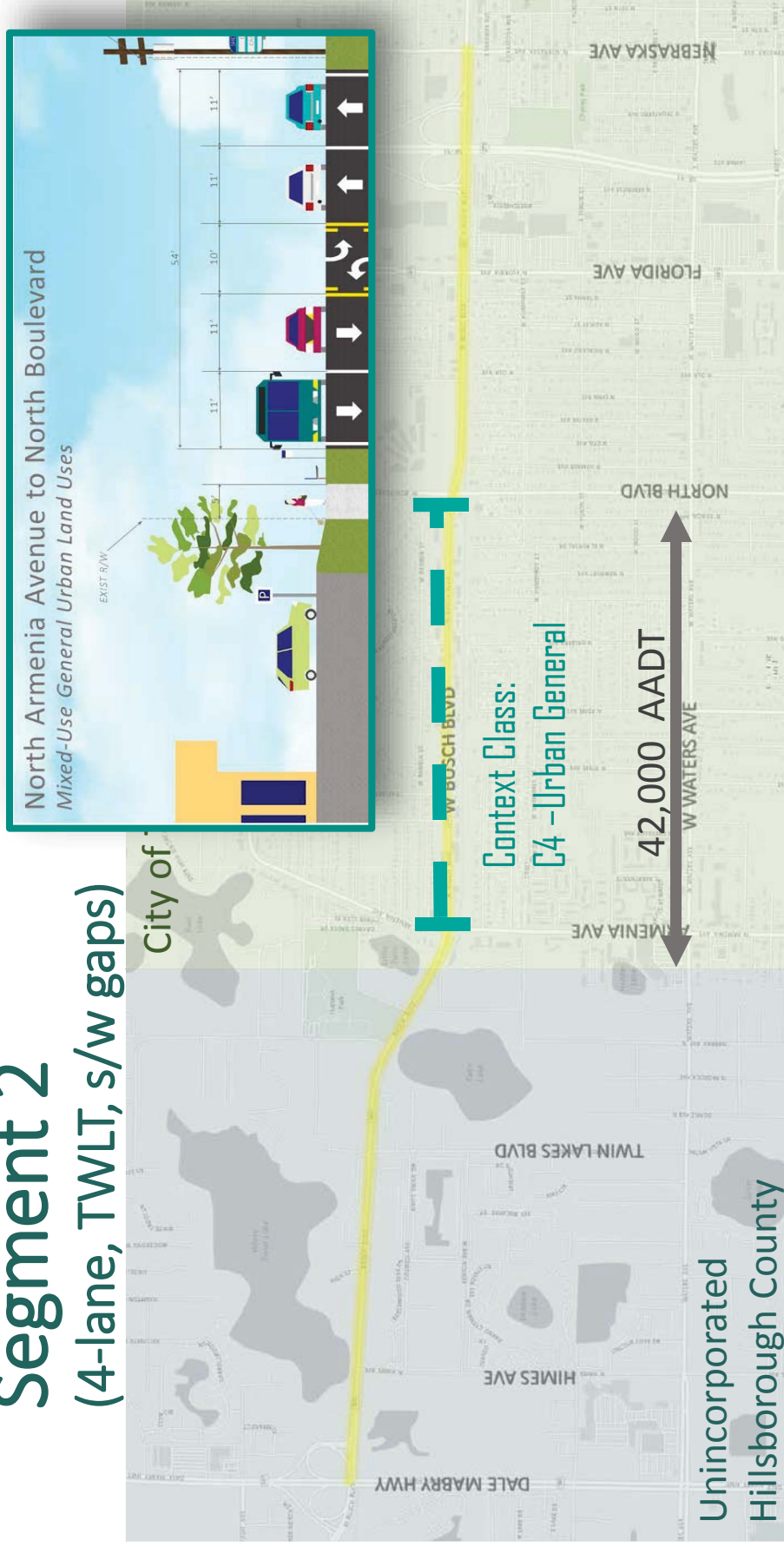
Existing Conditions (no build) Segment 1 (4-lane undivided w/sidewalk)



Existing Conditions (no build)

Segment 2

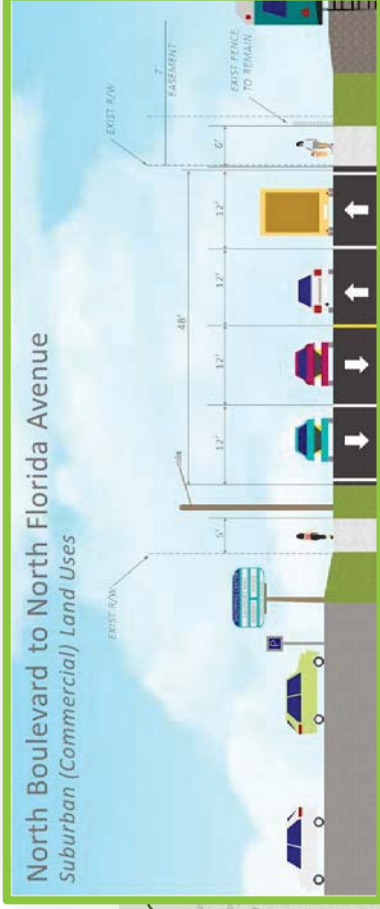
(4-lane, TWLI, s/w gaps)



Existing Conditions (no build)

Segment 3

(4-lane w/sidewalk)

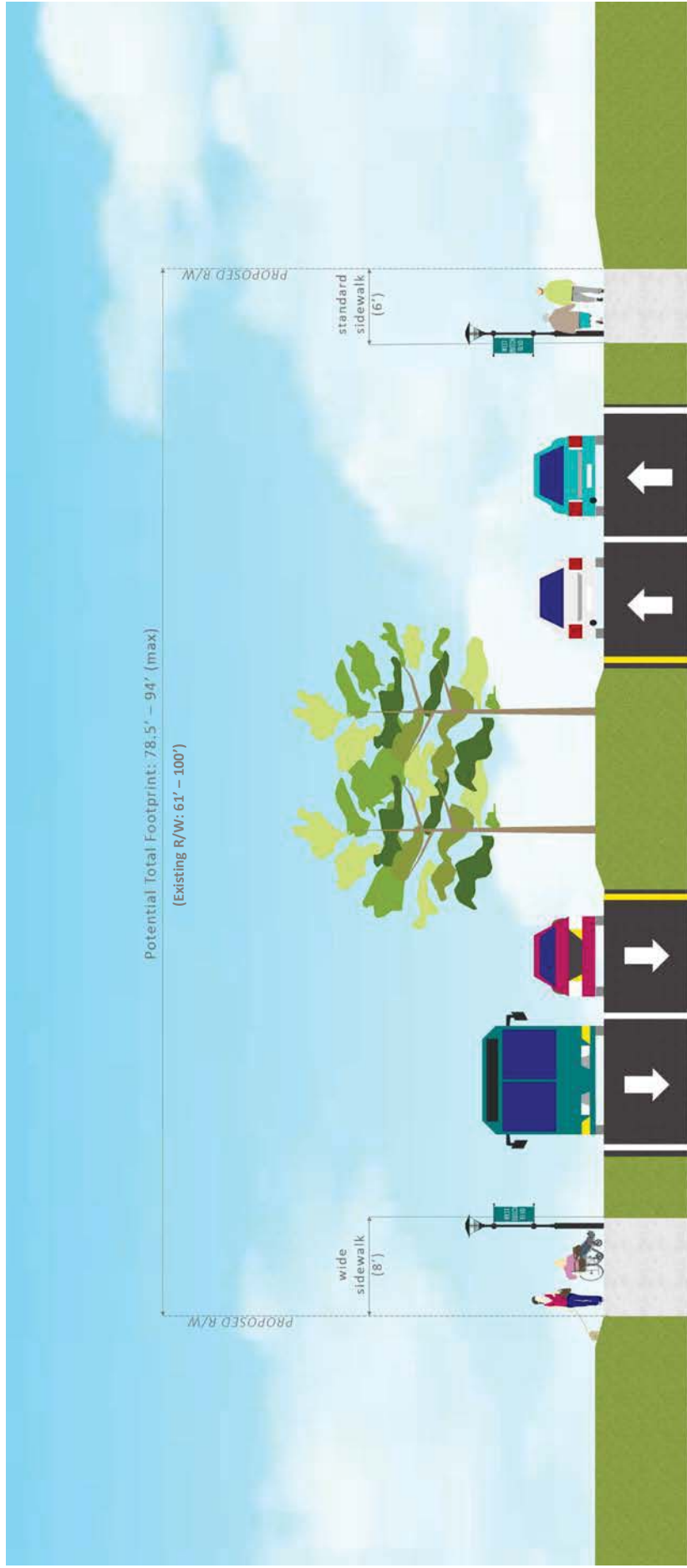


Existing Conditions (no build)

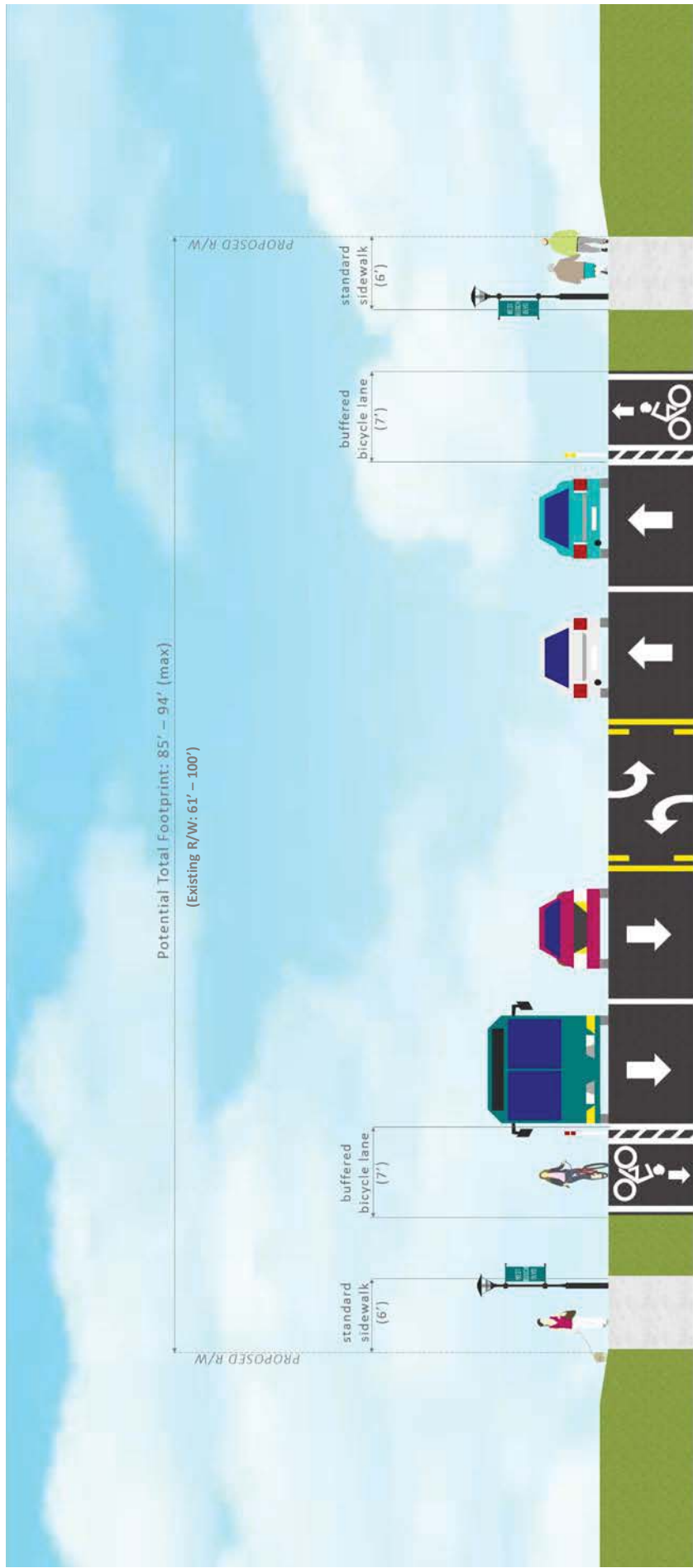
Segment 4 (6-lane, median)



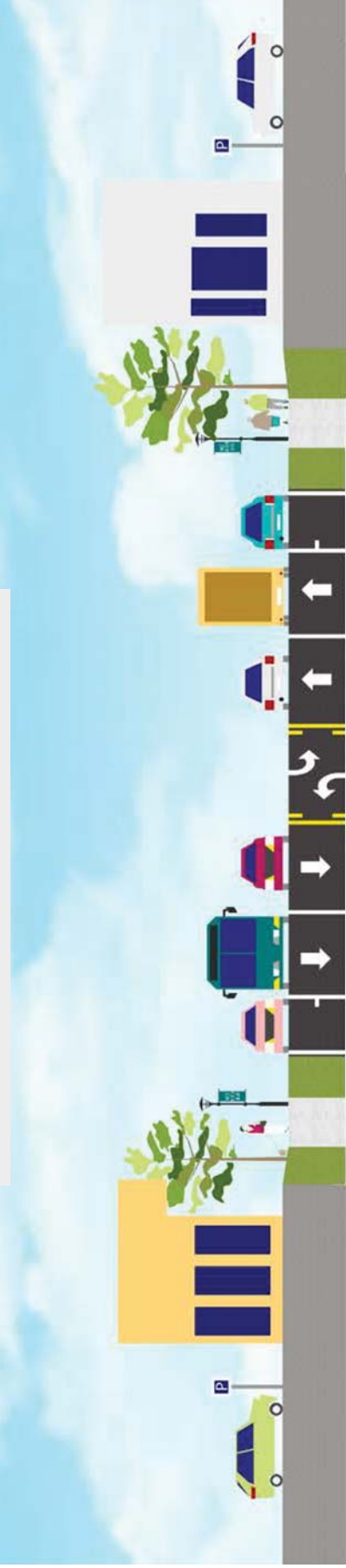
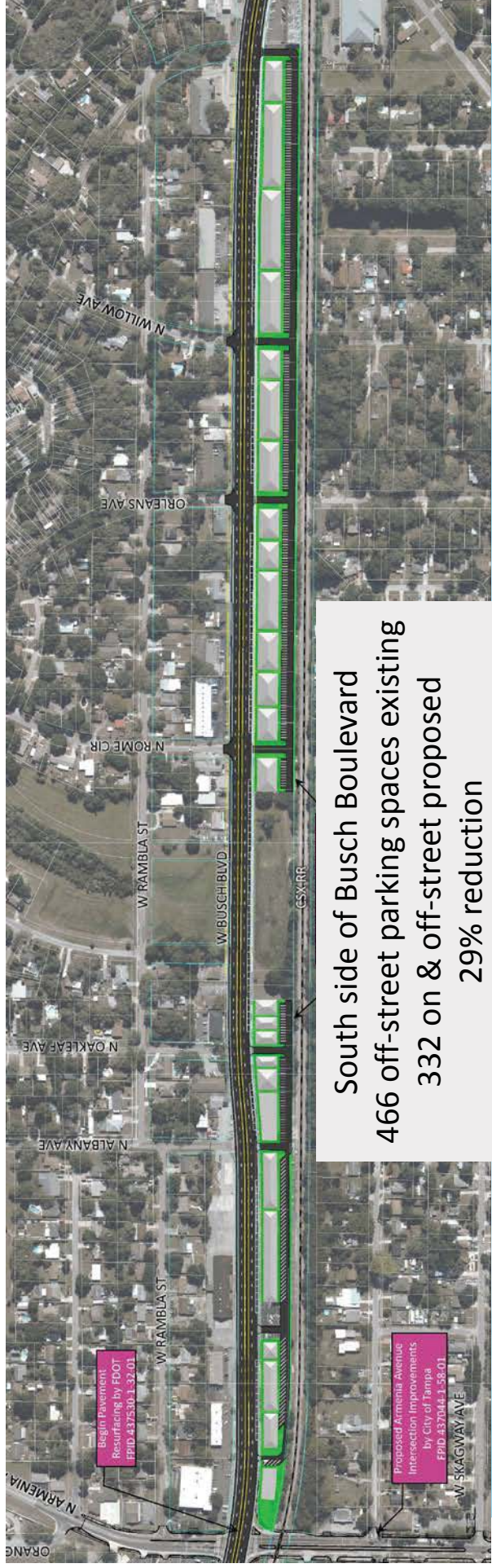
Alternative 1: 4 Lanes with Median and Wide Sidewalk on the North Side



Alternative 2: 4 Lanes with Center Two-Way Left Turn Lane and Buffered Bike Lanes

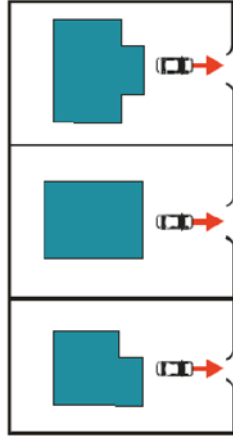


Potential Parallel Parking Option for Segment 2 (North Armenia Ave. to North Blvd.)

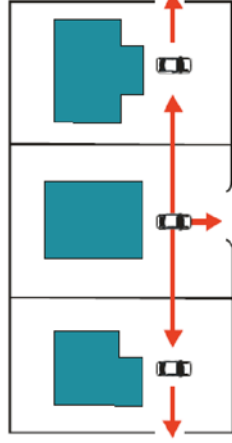


Width of parallel parking = buffered bike lane

City of Tampa code does not currently address cross/joint access and discourages commercial access from non-arterial streets (Sec. 27-283.12.(j)(1)).



Avoid



Promote

- Cross Access
- Joint Access
- Complete on-site circulation

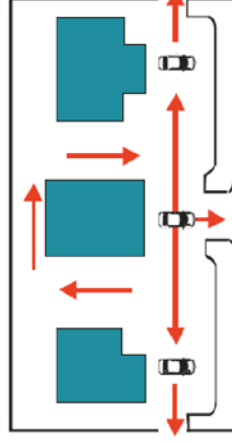


Image: Driveway Information Guide (FDOT 2008) p. 85

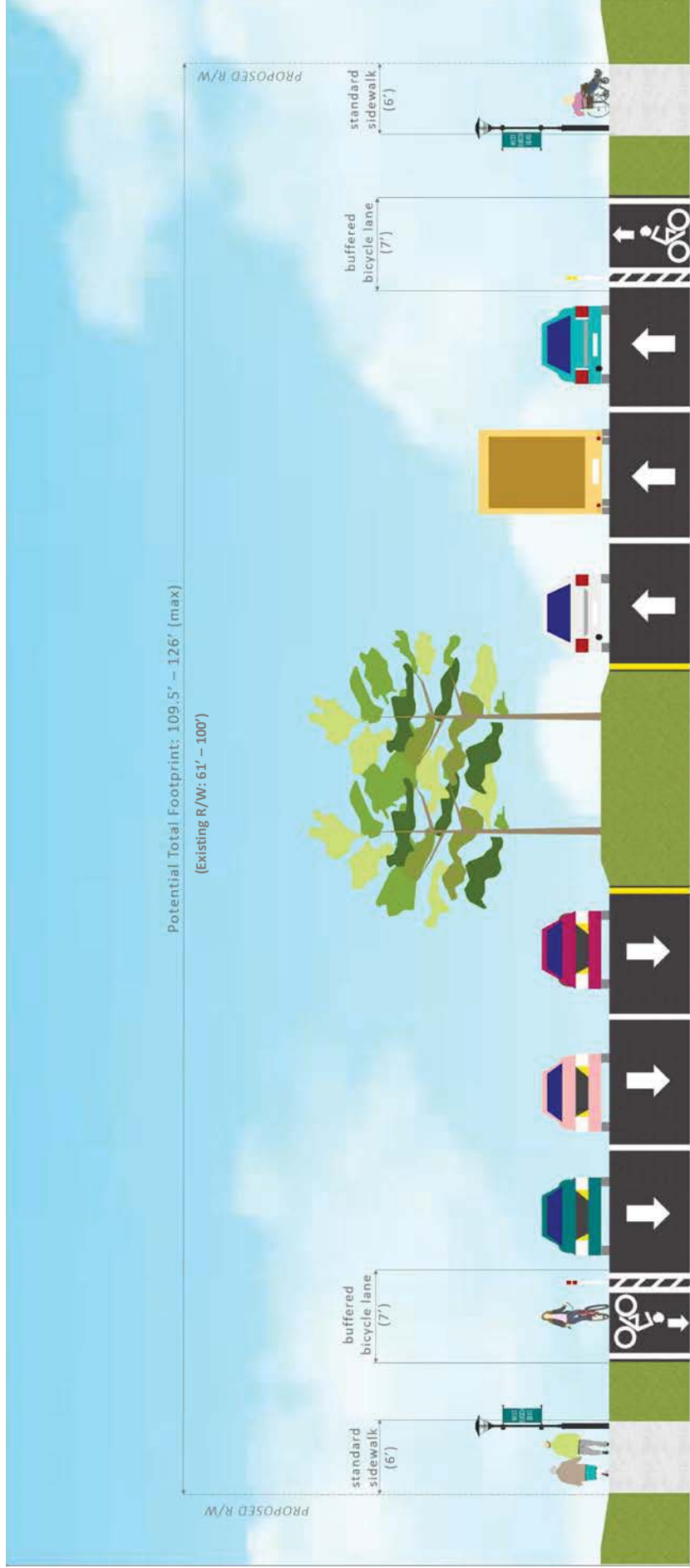
To address the public interest in these matters, land development regulations include conditions or circumstances where nonconforming access features may be brought into conformance. Such conditions may include:

- when new driveway permits are requested;
- an increase in land use intensity;
- substantial enlargements or improvements;
- significant change in trip generation; and
- as changes to roadway design allow. (p.1-8)

Overlay requirements may address any issues of concern, such as joint access, parking lot cross access, reverse frontage, driveway spacing, and limitations on new driveways. (p. 1-9)

Source: "Land Development and Subdivision Regulations that Support Access Management", Center for Urban Transportation Research, University of South Florida, January 1994. Williams, Kristine M. <https://www.cutr.usf.edu/oldpubs/pcm/files/Model%20Land%20Development%20and%20Subdivision%20Regulations%20that%20Support%20Access%20Management.pdf>

Alternative 3: 6 Lanes with Median and Buffered Bike Lanes

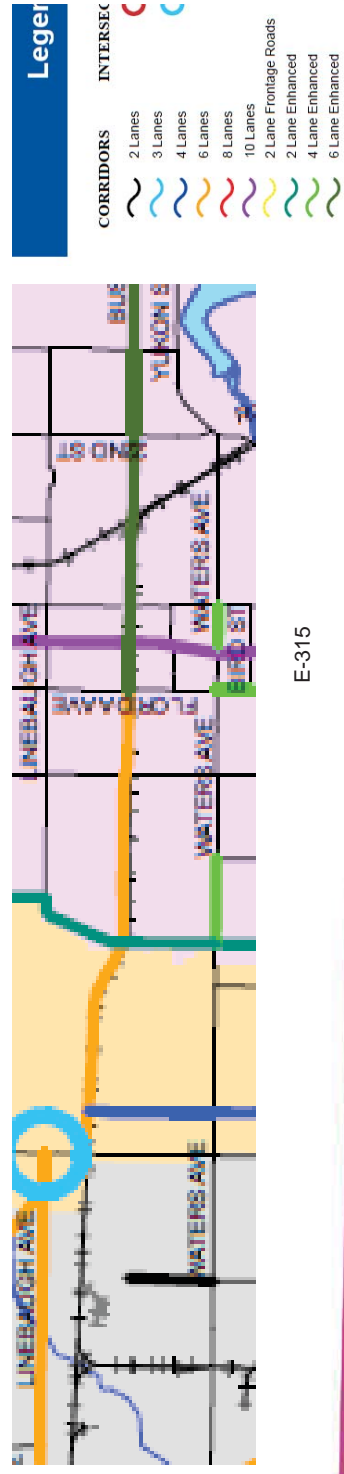


Planning Consistency – 6 Lane Alternative

- 2040 LRTP
 - ≈ Need for 6/7 lanes on “Beyond 2040” Map (pg. 98) from Armenia Blvd. to North Blvd.



- Tampa Comprehensive Plan (effective date February 20, 2016)
 - Constrained Roadways (physical constraints)
 - ≈ Busch Blvd. from Dale Mabry Blvd. to North Blvd. constrained to 4 lanes
 - ≈ “Page 151; MBY (Mobility) Policy 3.2.2: Constrained roadways... shall be eligible for... Dedicated transit lanes...”
- Hillsborough County Corridor Preservation Plan (Map 25) shows preserved for 6 lanes/ 6 lanes enhanced through study limit



Inter-agency Recommendations (Interim Design)



Interim Design Project Recommendations

- Safety improvements at Dale Mabry off-ramp
- Turn lane extensions as feasible
- Complete sidewalk gaps
- Add signal and/or mid-block crossing at Rome Circle or Willow Ave.
- Leading pedestrian interval or ped-only phase at North Blvd.
- Add refuge islands or tighten curb radius at North Florida Ave. and North Nebraska Ave.
- Add YOUR SPEED radar sign(s)

Potential additional improvements with local agreements include...

Corridorwide Recommendations



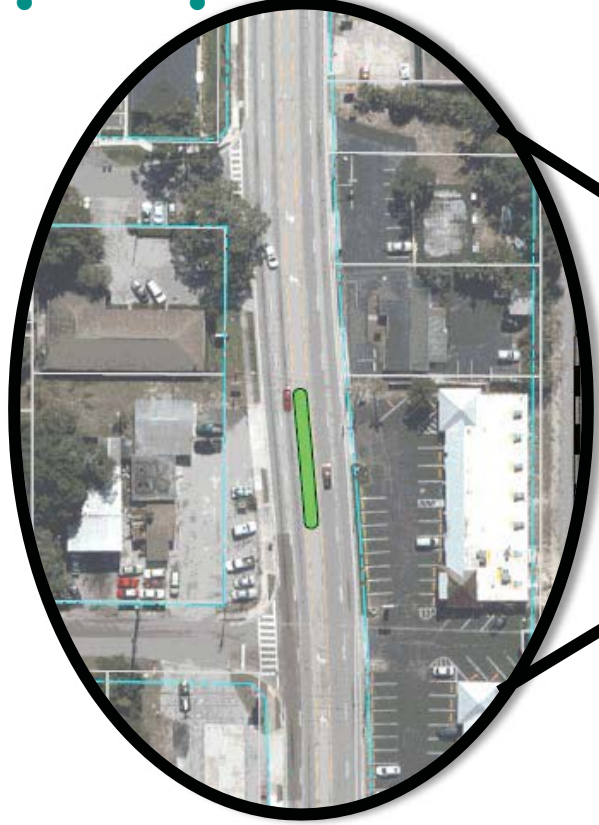
- Add pedestrian lighting
- Interconnect signals to increase platooning
 - ≈ Traffic signal speed signs
 - ≈ Explore potential of dynamic signal timing and/or setting signals to a 35 mph target speed
 - ≈ City and county signals are not interconnected



*Traffic Signal Speed Sign
MUTCD, Section 2H.03 Traffic Signal Speed
Sign (11-1)*

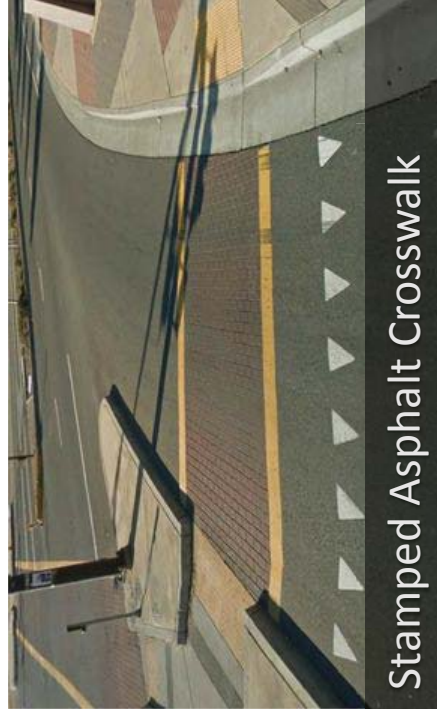
Florida Friendly/ Pedestrian Deterrent Landscaping in Opportunity Areas

- Opportunity areas defined where:
 - **At least one existing driveway per property accessible via left turn**
 - **At least 50-ft. of turn lane** provided in advance of the driveway



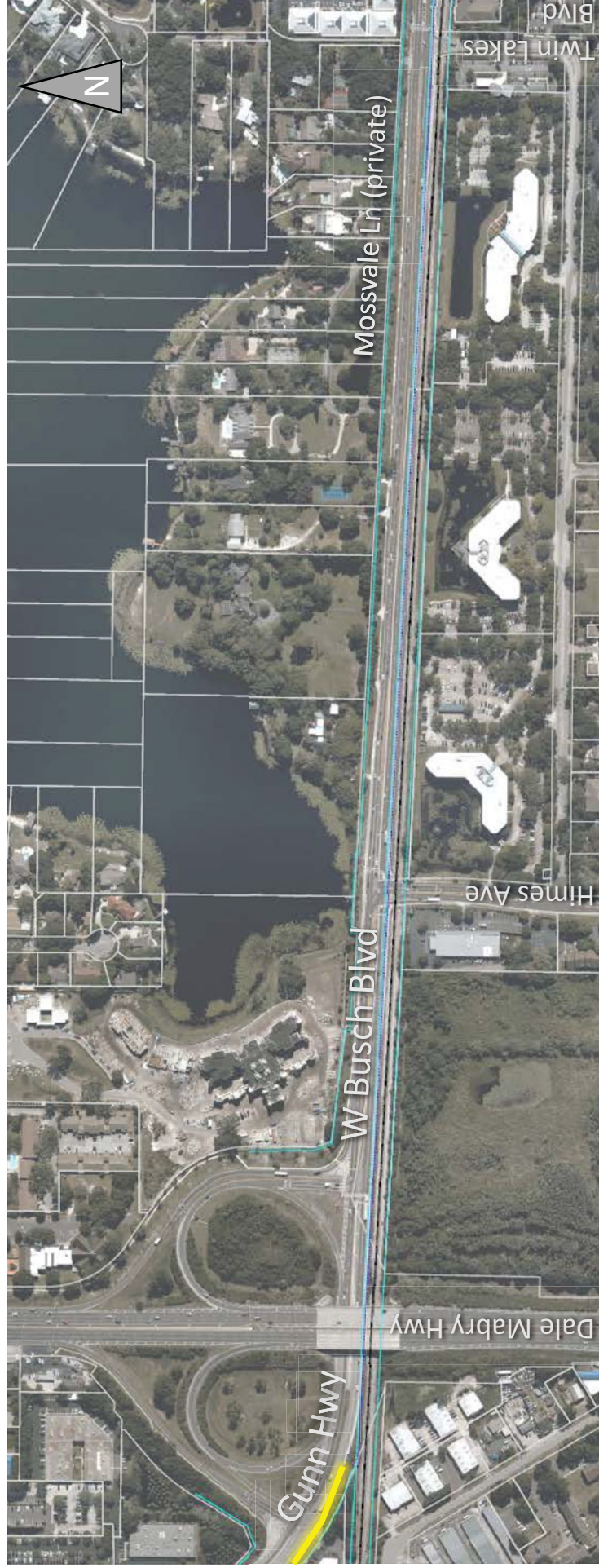
Example: 13th Street, Gainesville, FL

High-emphasis pedestrian crossings / hardscape intersections



Gunn Highway Sidewalk Gap

- County jurisdiction



Mossvale Lane Extension + Intersection

- County land-use jurisdiction

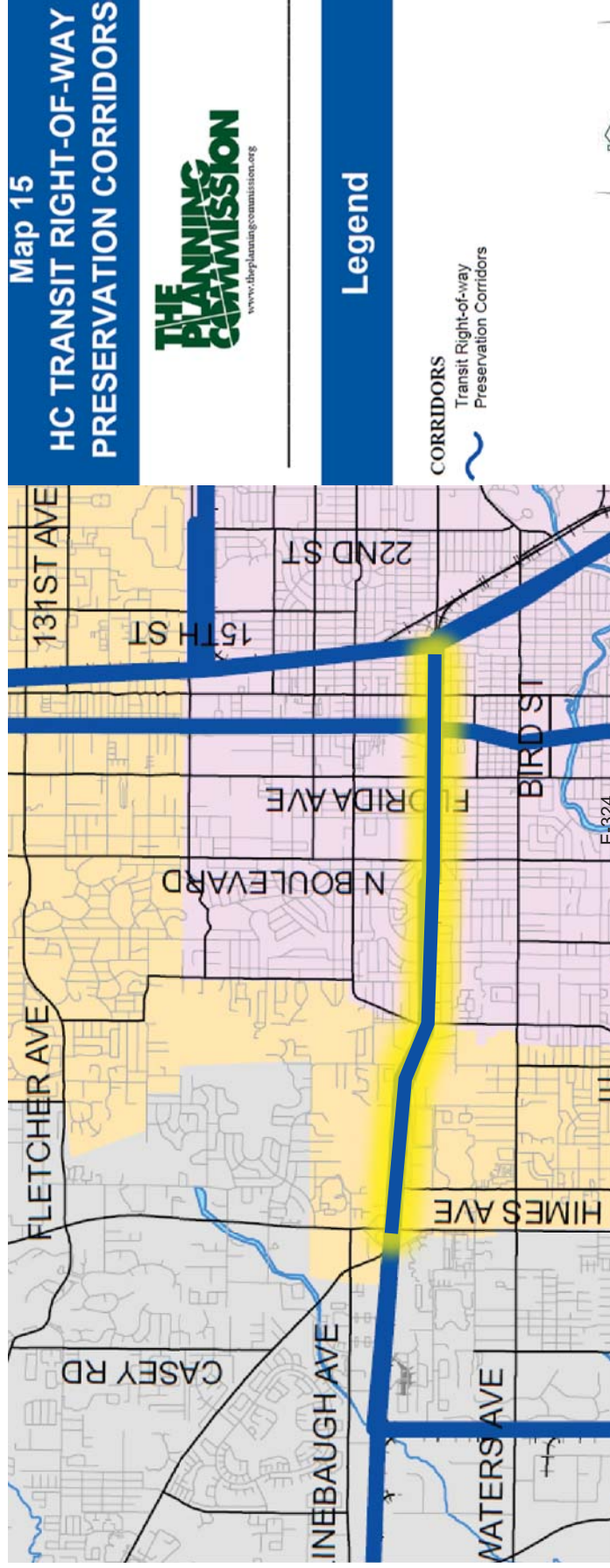


Other Recommendations



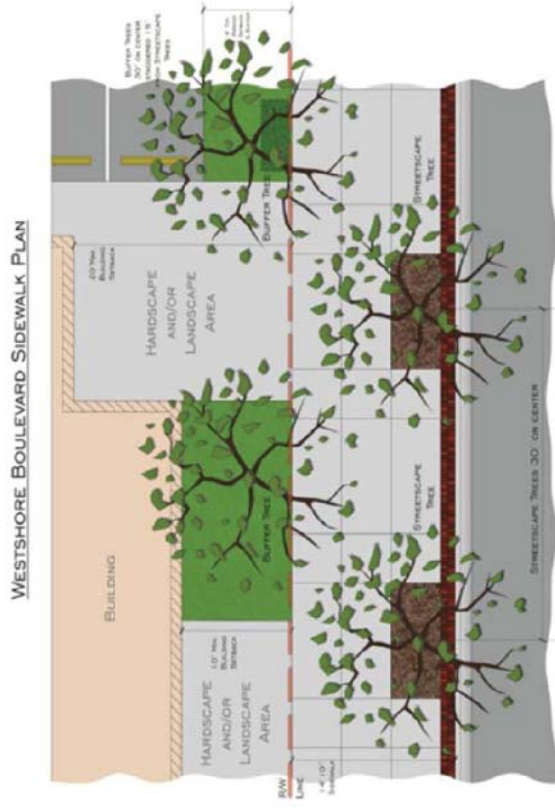
Recommendations related to future transit/rail plans

- The CSX line runs parallel to the corridor along the south for most of the study limits
- **Are there any plans related to transit or the rail corridor that should be documented in this report?**



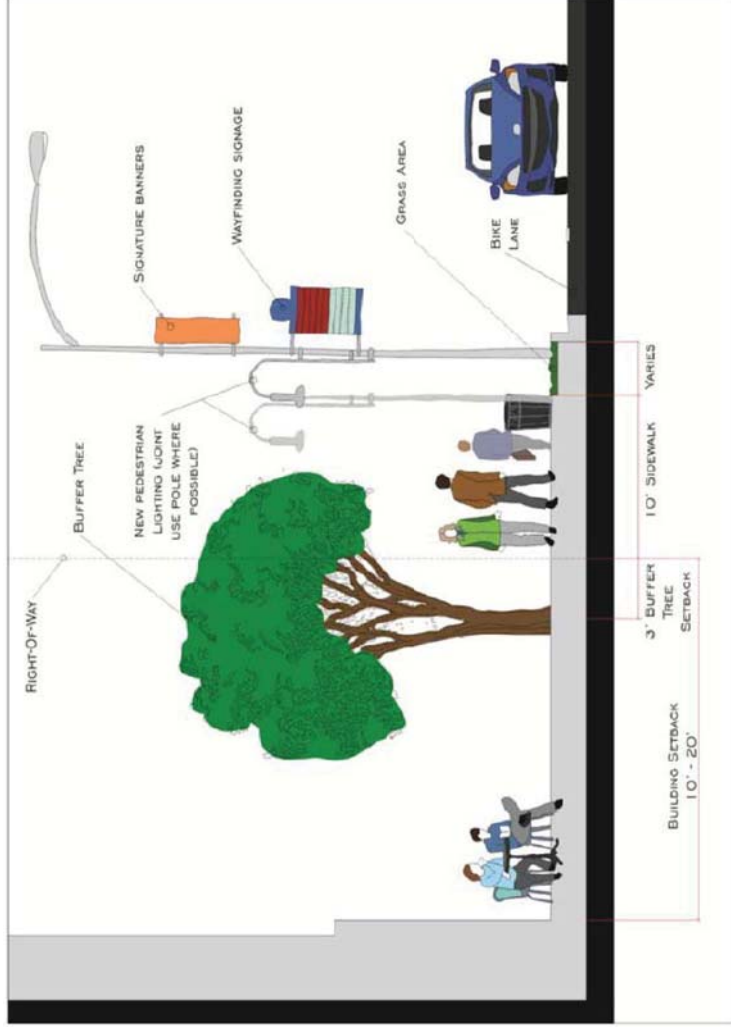
Speed Management along Corridor

- Recommending further study of lowering the speed limit
- Recommend school safety study to explore school zone designation
- Speed reduction can be bolstered by:
 - ≈ Urban form changes
 - ≈ Increasing police enforcement
 - ≈ Parallel parking (requires speed limit of 35 mph or less)
 - ≈ Landscaping
- Development/redevelopment requirement to provide landscaping along sidewalk can calm traffic and improve pedestrian environment



Adopt an overlay district to support corridor vision

- Coordinate landscaping opportunities with development code
- Set-back for R/W preservation based on recommended alternative(s)
- Reduced/revised parking requirements
- Form-based code to promote pedestrian environment



City of Tampa Code Sec. 27-238 Westshore Overlay District Table 238.c

Bicycle Master Plan/ Bicycle Network

- Overall regional bicycle connectivity benefits are limited
- Bicycle routes defined at a regional level would help support the potential future investment of bicycle lanes on Busch Boulevard
- Parallel route to Busch Blvd. may be preferable
- MPO/County should lead

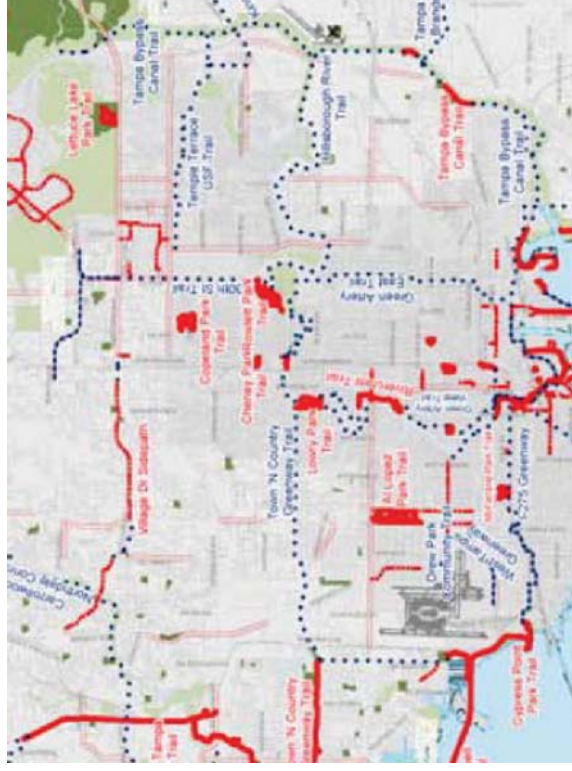


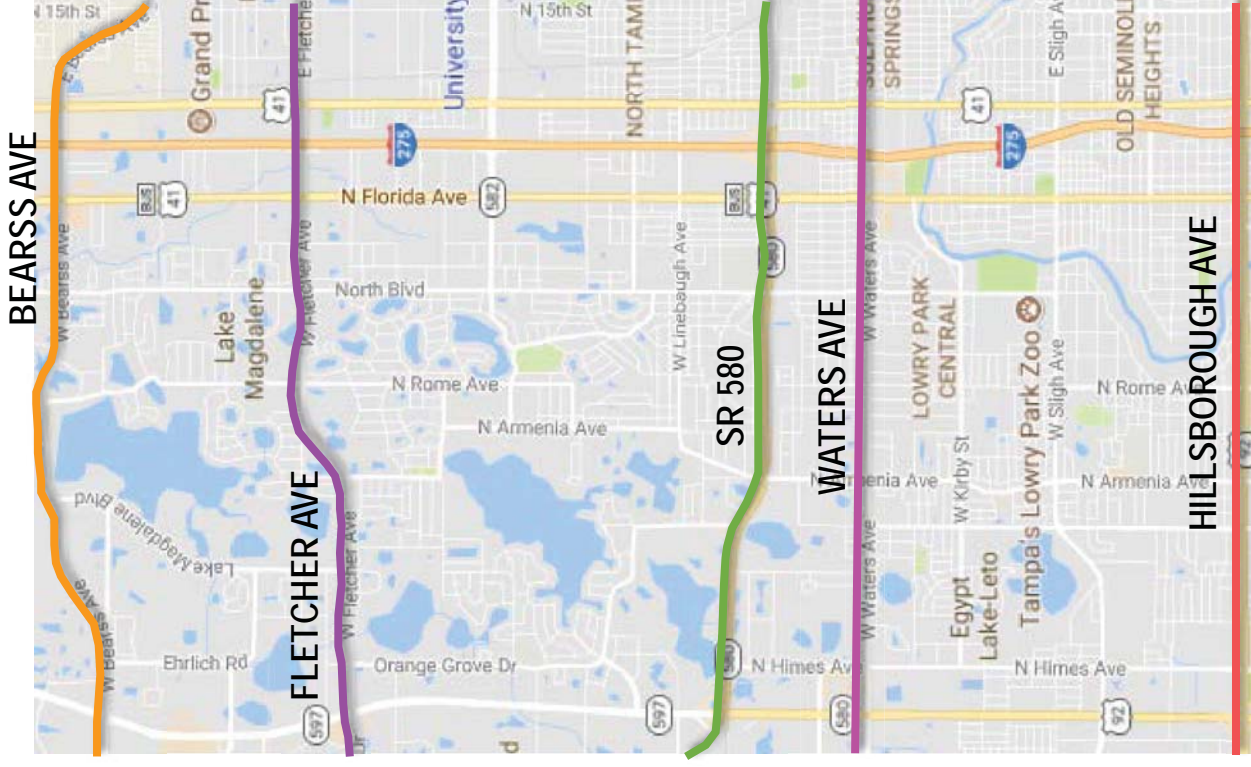
Figure 3-23
Trails/Sidepaths Planned and Potential Corridors Map



E-327

Regional origin/destination planning study

- To study the perceived "through traffic" along the corridor between Dale Mabry Hwy and I-275
- Limited east-west corridors and existing congestion suggest the opportunity to address regional travel flows at a more regional level than within the limits of this corridor study
- MPO or FDOT should lead



Corridor	Lanes	AADT	Notes
Bearss Avenue	4-lane	54,000	3.8 miles north
Fletcher Avenue	4-lane	23,500	Limited western connectivity
SR 580	4-lane	50,000	
Waters Avenue	4-lane	27,000	Limited eastern connectivity
Hillsborough Avenue	6-lane	51,500	2.6 miles south



Local Agency Coordination- Hillsborough County

PREPARED BY: Cris Schooley (CH2M)
 PROJECT: SR 580/Busch Blvd. from N. Dale Mabry Hwy. to N. Nebraska Ave. Hillsborough County
 FPID 435908-1-22-01 (PD&E) and 435908-2-32-01 (Corridor Design)
 MEETING DATE: April 12, 2018
 MEETING TIME: 1:20 PM to 2:10 PM
 LOCATION: 22FL-MtgRm-2202E-Lobby-30cap (County Center - 601 East Kennedy Blvd.)
 ATTENDEES: See Sign-in

1. Introductions

Jenn Rhodes/CH2M began the meeting. Cris Schooley/CH2M, RRR PM Indike Ratnayake/FDOT, and the other attendees introduced themselves.

2. Presentation

Jenn and Cris presented the attached slides. Jenn, Cris, and Indike answered questions and responded to comments.

3. Discussion Items

- a. Why is the wide sidewalk on the north side and not the south? ***The north side has more destinations and higher use because of the railroad adjacent to the south for most of the project length.***
- b. The county would be supportive of corridor lighting for roadway, intersection, and pedestrians if it addresses a needed reduction in crashes. ***Crashes will be documented along with potential benefits of the proposed improvements.***
- c. The county would be supportive of corridor landscaping if it addresses a needed reduction in crashes. ***Crashes will be documented along with potential benefits of the proposed improvements.***
- d. The county would be interested in a joint project agreement/local agency program to complete the Gunn Highway sidewalks (in County jurisdiction) as part of the upcoming FDOT design project.
- e. The Mossvale lane extension proposal, though a good idea, may be too late. Plats for the subdivision have already been approved.

4. Action Items

Task	Description	Responsible Person	Status	Comments

5. **Next Scheduled Meeting:** Project Advisory Group meeting #4 May 24, 2018. Time TBA.



LOCAL AGENCY COORDINATION MEETING

SIGN-IN

West Busch Boulevard (SR 580) Corridor Study
from North Dale Mabry Highway to North Nebraska Avenue
Hillsborough County, Florida
FDOT Financial Project ID Number: 435908-1-22-01
601 E Kennedy Blvd, 18th floor, Tampa, FL, 33602
April 12, 2018

Table with 2 columns: Name/Agency, Email. Contains sign-in entries for Cris Schooley, Indike Ratnayake, Jenn Rhodes, Lindsey Michele, James Hudock, Adam Gormly, and Leland Dicus.



LOCAL AGENCY COORDINATION MEETING

DRAFT AGENDA

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Purpose of this meeting

1. Discuss draft typical section alternatives
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Introductions



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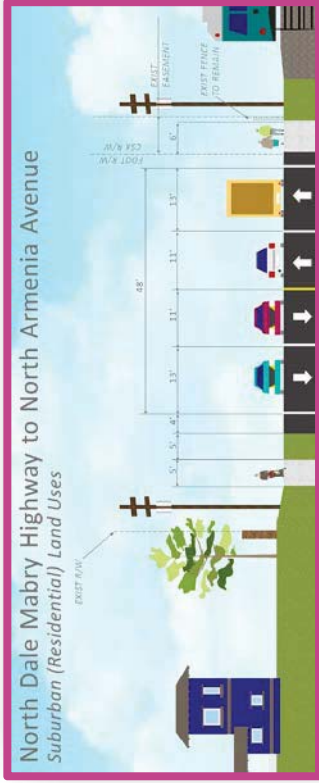


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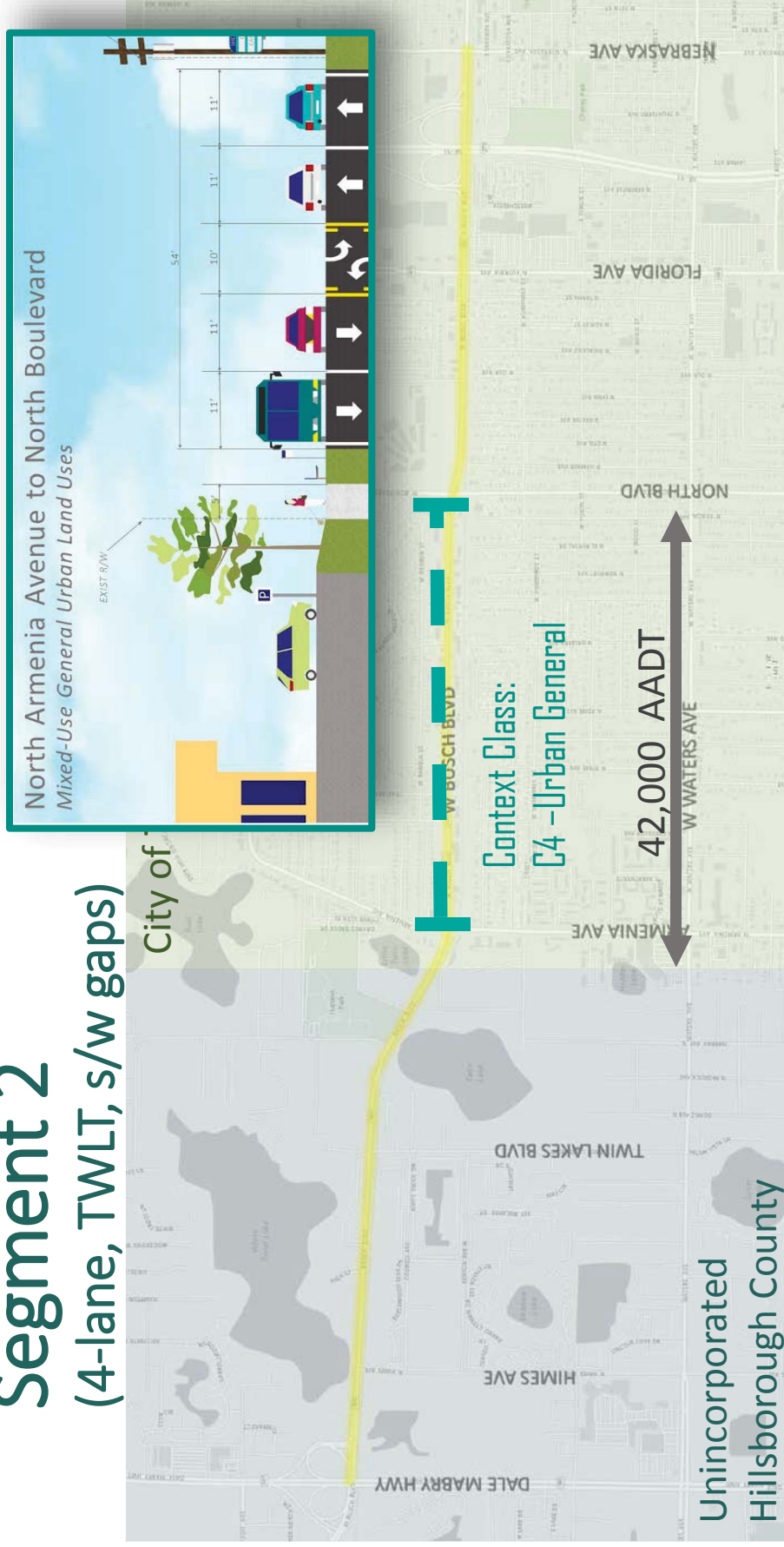
Existing Conditions (no build) Segment 1 (4-lane undivided w/sidewalk)



Existing Conditions (no build)

Segment 2

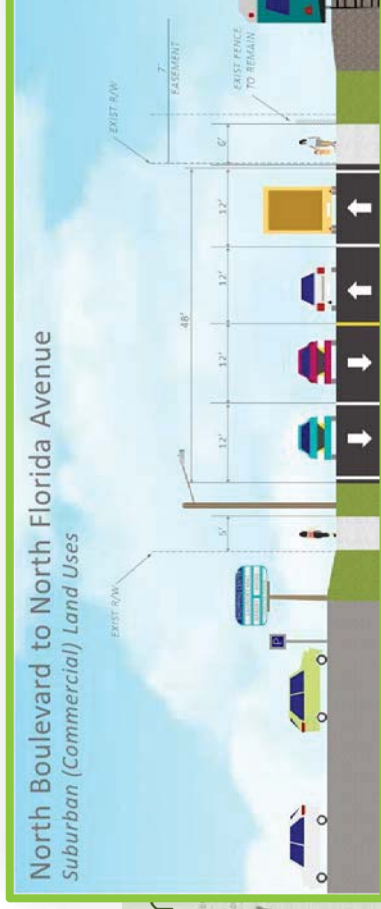
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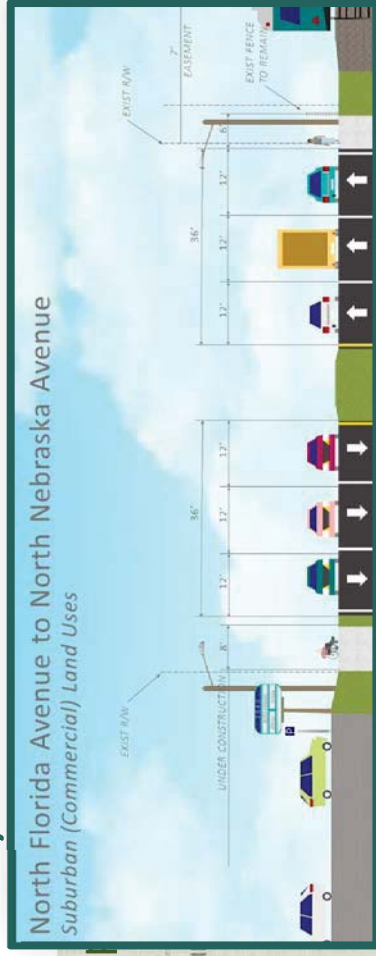
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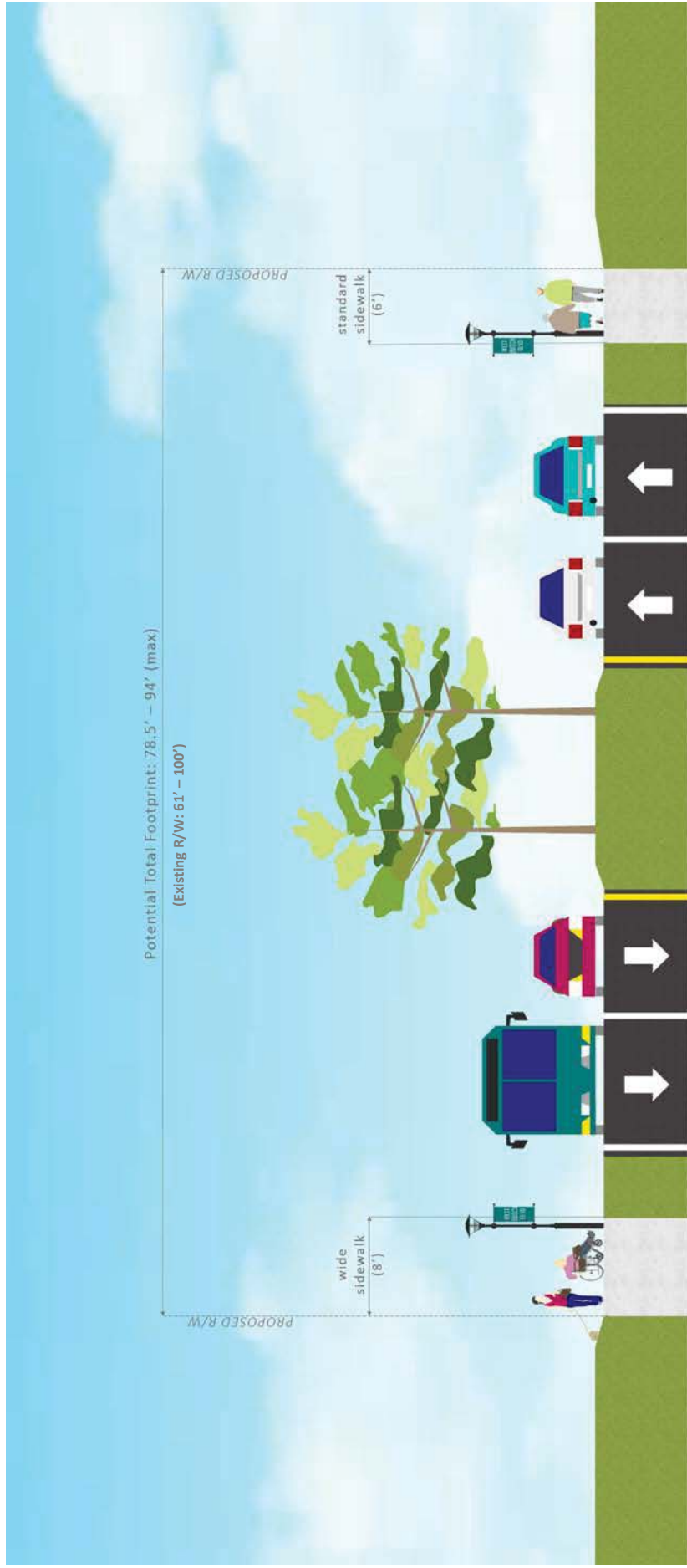


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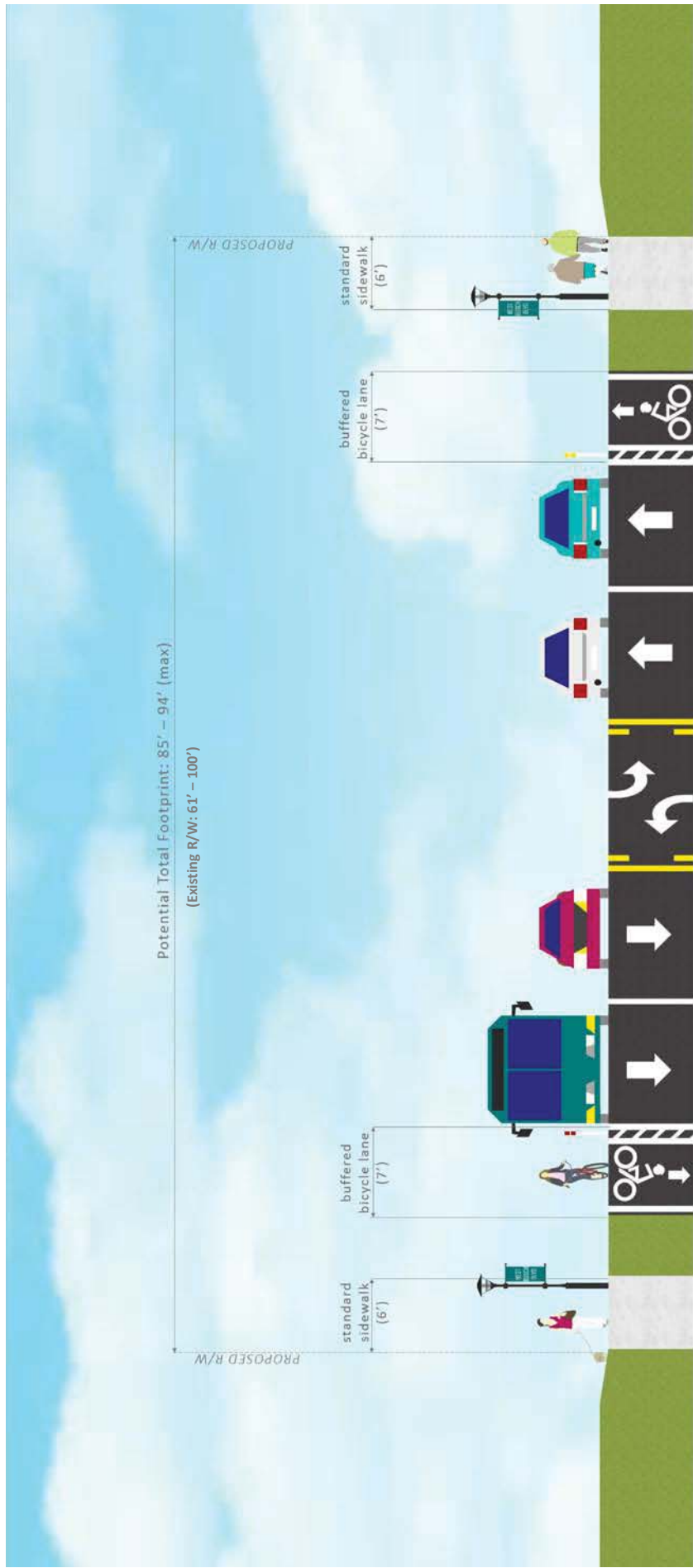
Segment 4 (6-lane, median)



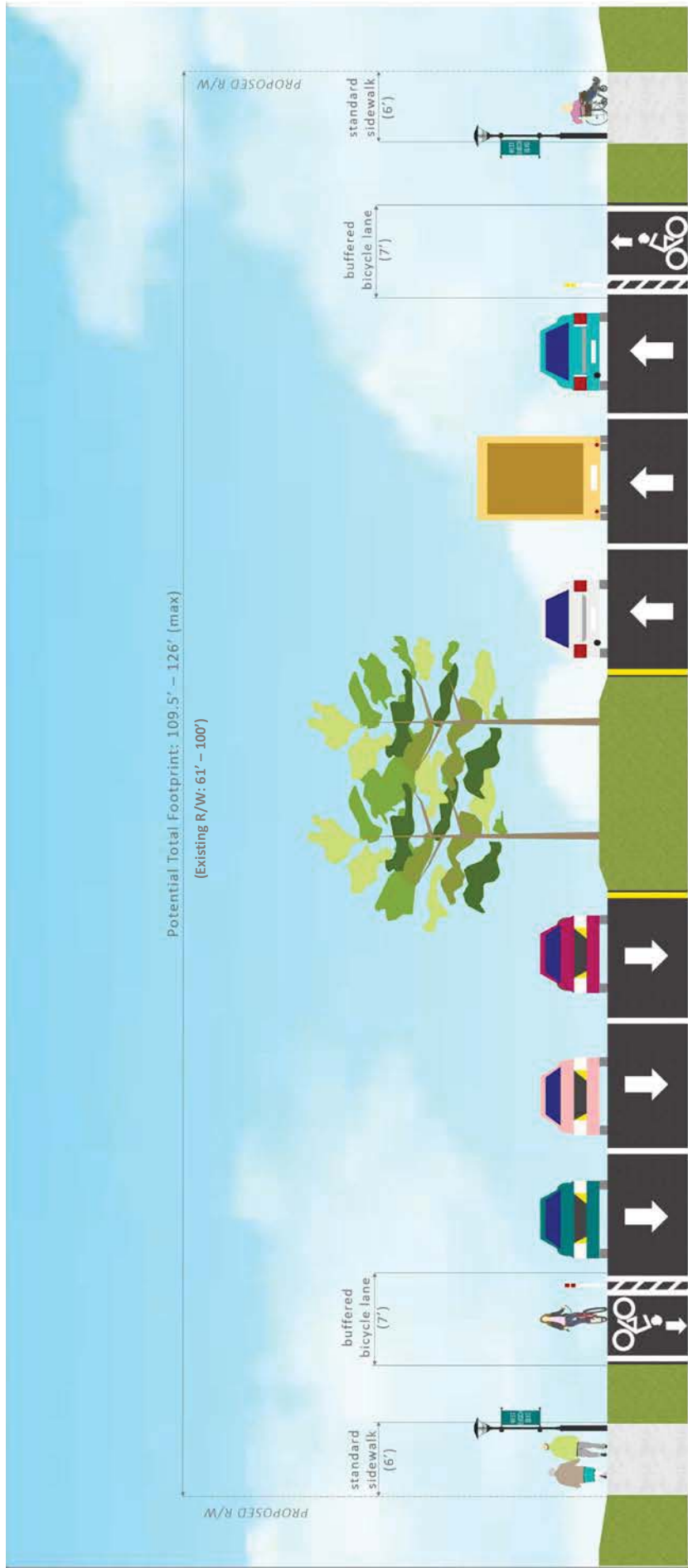
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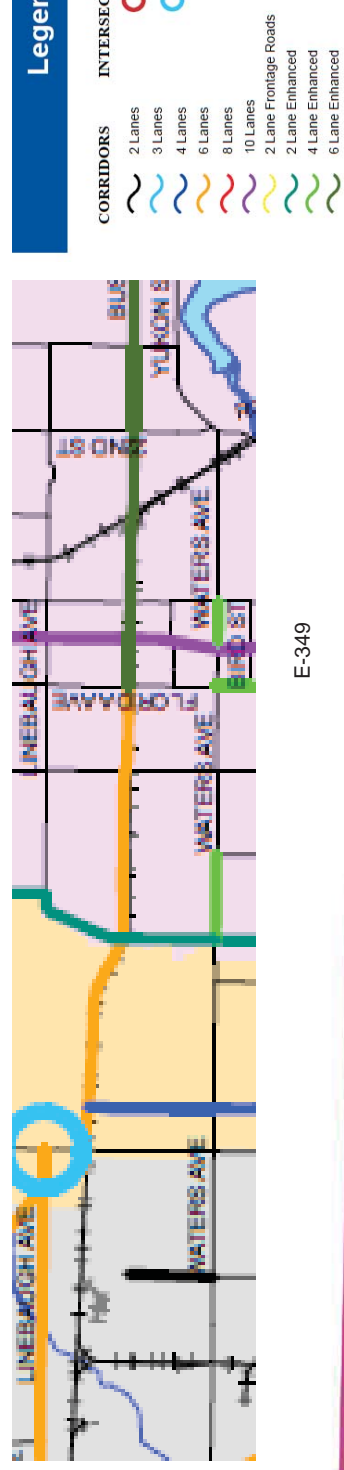


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*Traffic Signal Speed Sign
MUTCD, Section 2H.03 Traffic Signal Speed
Sign (11-1)*

Florida Friendly/ Pedestrian Deterrent Landscaping in Opportunity Areas

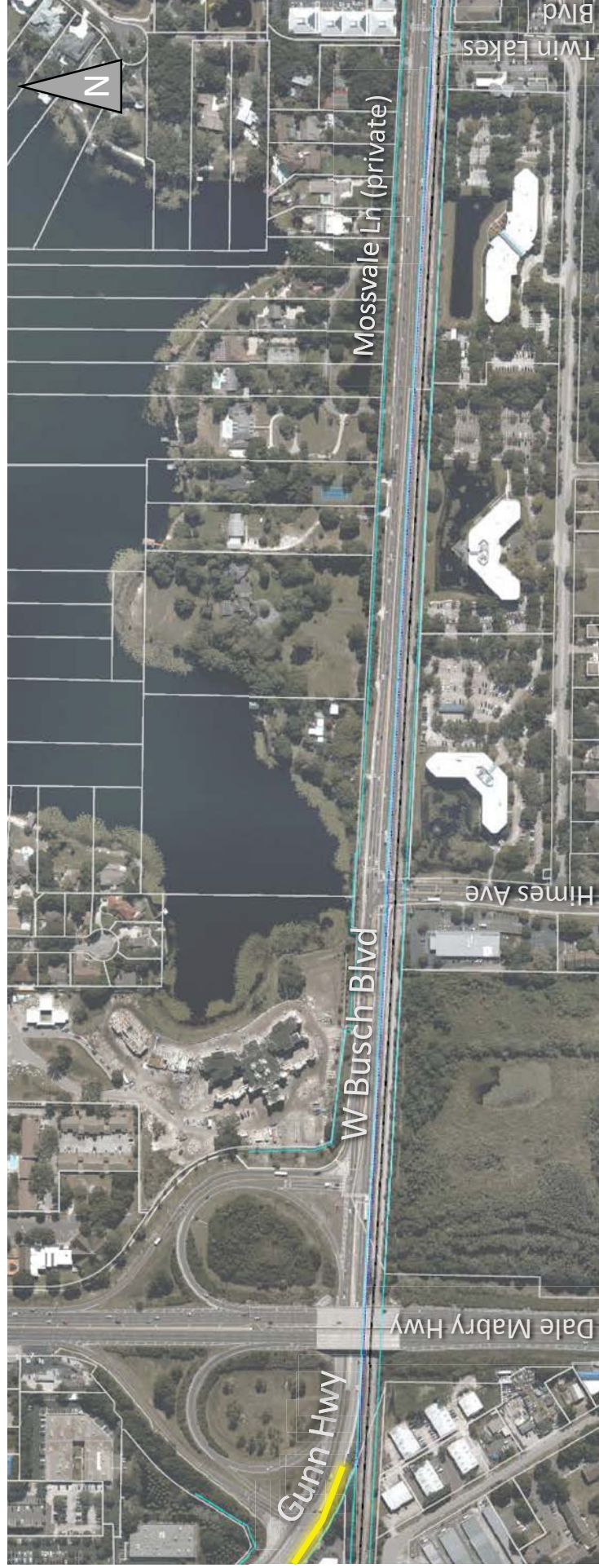
- Opportunity areas defined where:
 - **At least one existing driveway per property accessible via left turn**
 - **At least 50-ft. of turn lane** provided in advance of the driveway



Example: 13th Street, Gainesville, FL

Gunn Highway Sidewalk Gap

- County jurisdiction



Mossvale Lane Extension + Intersection

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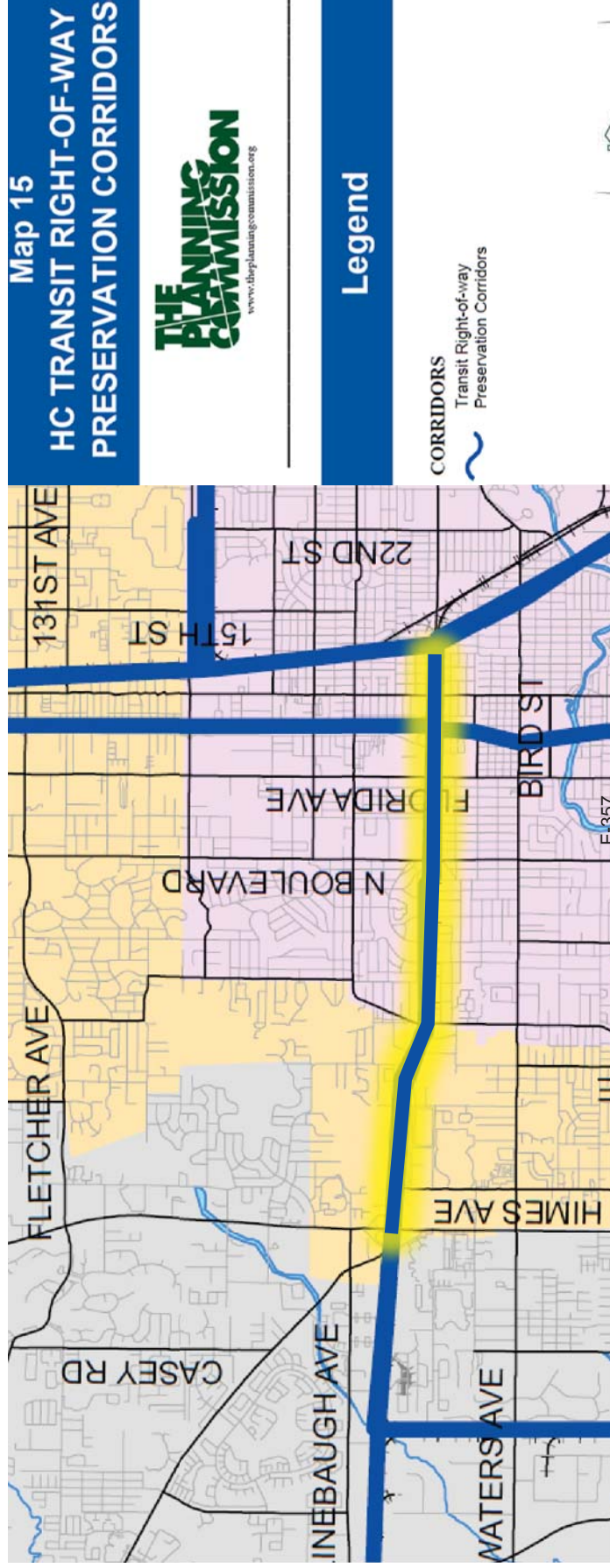


Other Recommendations



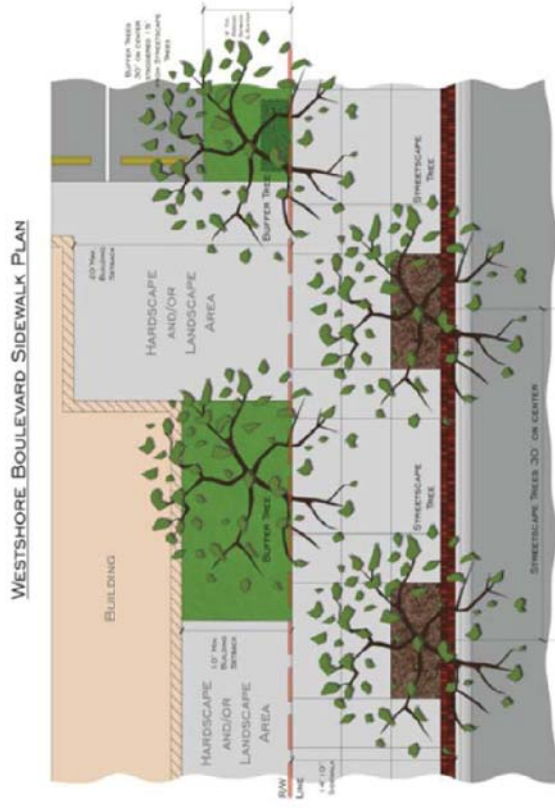
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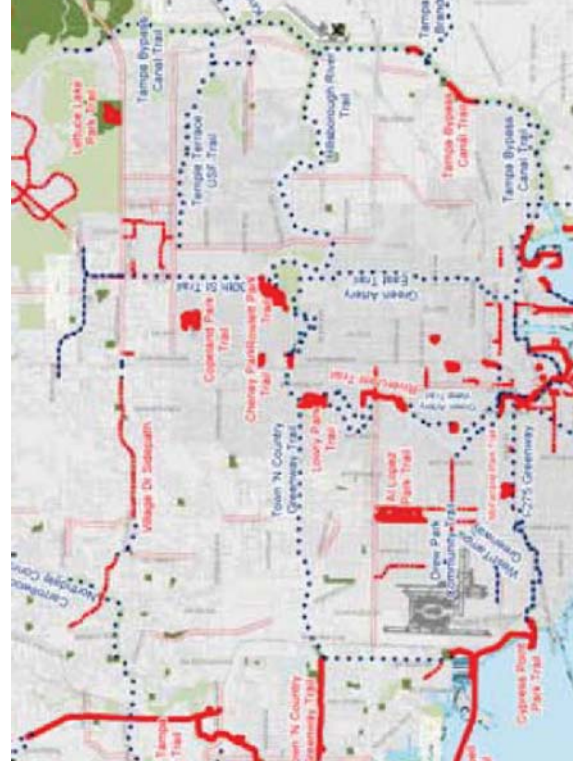


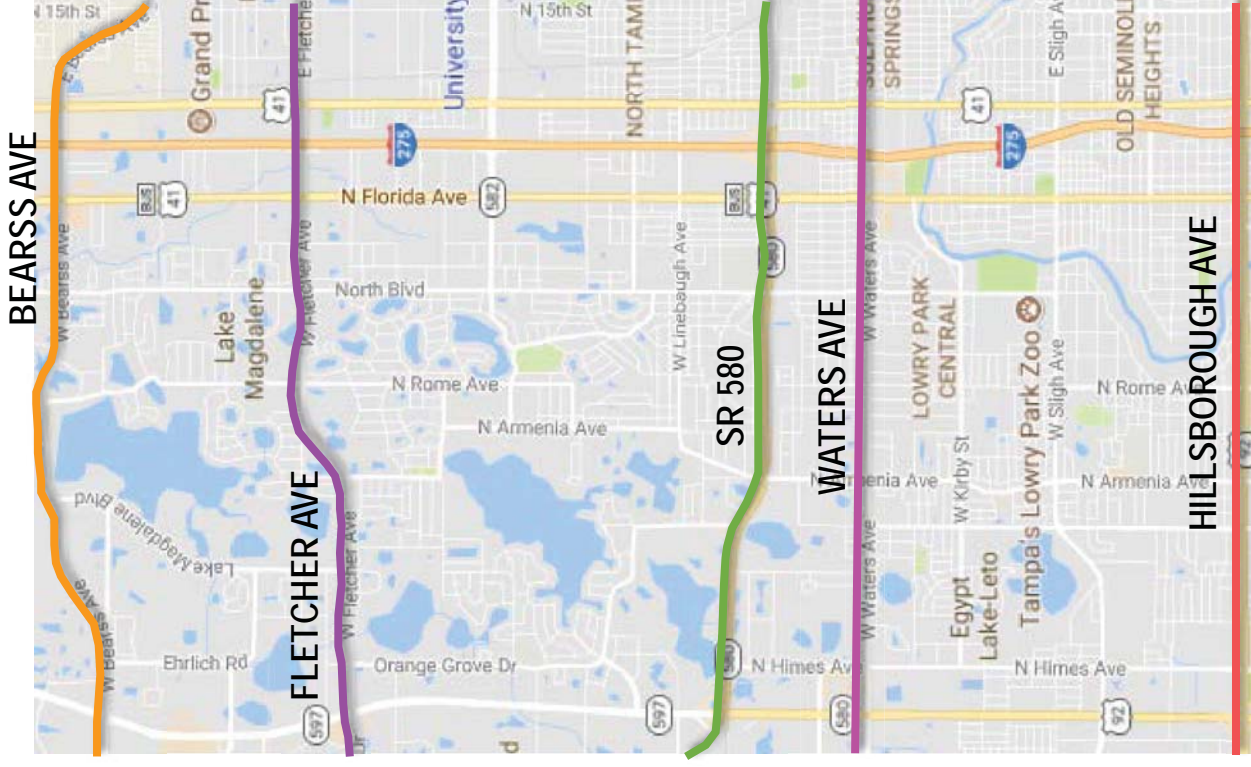
Figure 3-23
Trails/Sidepaths Planned and Potential Corridors Map



E-359

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Corridor	Lanes	AADT	Notes
Bearss Avenue	4-lane	54,000	3.8 miles north
Fletcher Avenue	4-lane	23,500	Limited western connectivity
SR 580	4-lane	50,000	
Waters Avenue	4-lane	27,000	Limited eastern connectivity
Hillsborough Avenue	6-lane	51,500	2.6 miles south



Local Agency Coordination- Hillsborough Planning Commission and MPO

PREPARED BY: Cris Schooley (CH2M)
PROJECT: SR 580/Busch Blvd. from N. Dale Mabry Hwy. to N. Nebraska Ave. Hillsborough County
FPID 435908-1-22-01 (PD&E) and 435908-2-32-01 (Corridor Design)
MEETING DATE: April 12, 2018
MEETING TIME: 11:00 AM to 12:00 PM
LOCATION: Plan Hillsborough Room (County Center - 601 East Kennedy Blvd., 18th Floor)
ATTENDEES: See Sign-in

1. Introductions

Jenn Rhodes/CH2M began the meeting, introduced Cris Schooley/CH2M and RRR PM Indike Ratnayake/FDOT. Other attendees introduced themselves. Attendees represented planning staff for the city of Tampa, planning staff for Hillsborough County, and MPO staff.

2. Presentation

Jenn and Cris presented the attached slides. Jenn, Cris, and Indike answered questions and responded to comments.

3. Discussion Items

- a. The MPO committee schedule is available. Multiple committees would like to have a presentation on the study recommendations.
- b. What is a RRR? **Resurfacing, Restoration, and Rehabilitation project. Mostly maintenance items with limited other improvements.**
- c. Will the access changes (median) be shown to the public? **Yes, the alternatives will be drawn out in plan view for the Alternatives Public Meeting. Any access changes would require more study and a public hearing.**
- d. Is the heat island effect from adding more pavement being evaluated? **Heat is not being directly evaluated, but indirectly the benefits of reducing crossing distance and adding landscaping will also help reduce heat. We will make a note of this in the report.**
- e. Do the alternatives include relocating utilities underground? **Utility relocations have not been evaluated at this time. While relocating overhead utilities underground increases the potential for tree canopy, the poles for street lighting would remain. The future typical sections will show utility/light poles at the alternatives public meeting.**
- f. The CVS at Florida Ave. and Busch Blvd has a significant number of HART boardings; consider adding a shelter and/or bus pull-out. **HART coordination is ongoing.**
- g. Are you considering new pedestrian crossings? **Yes, an additional signalized or unsignalized crossing between Armenia Ave. and North Boulevard is a potential spot improvement proceeding to design.**

h. Is there a recommendation for the left turns at Ola Ave. being considered? **Yes, as part of the design improvement recommendations, a small raised median with a “No-Left Turn” at Ola Ave. is included.**

i. What does the Tampa Comprehensive Plan roadway constraint mean? **The MBY Table: Constrained Roadways on page 159 of the Imagine 2040: Tampa Comprehensive Plan lists Busch Blvd from Dale Mabry to Boulevard as 4 Lane Undivided with physical constraints. However, MBY Policy 3.3.2 states:**

“Constrained roadways, though ineligible for two or more general use thru-lane additions, shall be eligible for transit system enhancements including those described above [Bus turn-outs and transit station areas, Queue-jump lanes, Dedicated transit thru-lanes/rail facilities]. Dedicated transit lanes constructed pursuant to this policy shall not be retrofitted for use by automobile or truck traffic so long as the subject roadway remains constrained.”

This means that the construction of transit lanes is consistent with the Plan so long as they are Bus Only.

j. Fletcher Avenue is an example of what the City is likely to support:



Image of Fletcher Ave (above) not shown in meeting.

- k. The MPO’s school safety study is complete with specific recommendations that may apply to the Busch Blvd. Interim Design Project. Lisa to send latest.
- l. Linebaugh and Busch intersection lighting is very good. Suggest replicating through the corridor. MPO supports lighting the corridor.
- m. Suggest noting *why* we are doing the improvements in the report and to the public. Especially benefits to disabled and aging population.
- n. The I-275 interchange with Busch Blvd innovation way gateway project is working through pedestrian improvements with FDOT which include a high-emphasis pedestrian crossing/hardscape intersection. These recommendations should be noted in the Report. These are in addition to what this study should recommend at North Boulevard.
- o. There are transit plans effecting the CSX ROW and more updated bike plans. MPO will provide.
- p. Some confusion regarding the long-term potential for a change in design speed after PD&E and the near-term speed management recommendations to be included as part of

the design project. ***Will clarify in report and public materials: The Interim Design (Spot improvements) will implement Speed Management best practices. The Ultimate typical section (PD&E Study) will evaluate a reduction in design speed/speed limit; not yet approved. Benefits of a lower design speed will be presented at the public alternatives meeting.***

4. Action Items

Task	Description	Responsible Person	Status	Comments
<i>PM</i>	<i>Schedule MPO committee presentations</i>	<i>Brian Shroyer/FDOT</i>		
	<i>Send Study team the latest MPO School safety study for Chamberlain HS</i>	<i>Lisa Silva/MPO</i>		
	<i>Send Study team the latest Rail transit and Bike plans</i>	<i>MPO</i>		
	<i>Incorporate comments into Corridor Alternatives and Strategies Report</i>	<i>Cris Schooley/CH2M</i>		

5. Next Scheduled Meeting: Project Advisory Group meeting #4 May 24, 2018. Time TBA.



LOCAL AGENCY COORDINATION MEETING

SIGN-IN

West Busch Boulevard (SR 580) Corridor Study
 from North Dale Mabry Highway to North Nebraska Avenue
 Hillsborough County, Florida

FDOT Financial Project ID Number: 435908-1-22-01

601 E Kennedy Blvd, 18th floor, Tampa, FL, 33602

April 12, 2018

Name/Agency	Email
Beth Alden	aldenb@plancom.org
Cris Schooley/CH2M	Cris.Schooley@ch2m.com
David Hey	heyd@plancom.org
Gena Torres	torresg@plancom.org
Indike Ratnayake/FDOT	Liyanage.Ratnayake@dot.state.fl.us
Jenn Rhodes/CH2M	Carol.Barker@CH2M.com
Johnny Wong	wongj@plancom.org
Krista Kelly	kellyk@plancom.org
Lisa Silva	silval@plancom.org
Melissa Zornitta	zornittam@plancom.org
Michele Ogilvie	ogilviem@plancom.org
Pedro Parra	parrap@plancom.org
Rich Clarendon	clarendonr@plancom.org
Sandy Mortellaro	sandy@plancom.org
Tony Garcia	garciat@plancom.org
Wade Reynolds	reynoldsw@plancom.org
Wanda West	WestW@plancom.org
<i>BUOWHITSAHA</i>	<i>BUOWHITSAHA@PLANCOM.ORG</i>

Comp Planner ✓

Comp Planner ✓

Comp Planner ✓



LOCAL AGENCY COORDINATION MEETING

DRAFT AGENDA

West Busch Boulevard (SR 580) Corridor Study

from North Dale Mabry Highway to North Nebraska Avenue
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April 12, 2018

1. Introductions
2. Project Overview
3. Typical Section Alternatives
4. Inter-agency recommendations (Interim Design)
5. Other Recommendations

April 27th or in June

if you can come in April

Hillsborough MPO Upcoming Board and Committee Agenda Items - Current Status

May Committees (June MPO)															
Item	Presenter	Staff Responsible	Presenter Confirmed?	Meetings Malicious OA Review	9-May 2-May 25-Apr	CAC	BPAC	TAC	ITS	Policy	LRC	TDCEB	STWG	MPO	Notes
ACTION ITEMS															
TR Adoption (Public Hearing at MPO meeting)	Sarah McKinley	Sarah McKinley			Yes			Yes						Yes	
PPP Update	Johnny Wong	Brandon Berry			Yes			Yes						Yes	
MPO School Safety Study and Top 10 Report	Watt Weaver	Lisa Silva			Yes			Yes						Yes	
BPAC request to MPO Board	Patrick Thorpe	Wade Reynolds	Yes	Y	Yes			Yes						Yes	
TBRPC Introductory Agreement	Allison Yeh	Allison Yeh			Yes			Yes						Yes	
STATUS REPORTS															
Tampa Bay Next Quarterly Update	J. Patrick	Allison Yeh	Yes		Yes			Yes						Yes	
Innovation District Subarea (42nd/46th Street) Study	Tony LaColla	Lisa Silva	Yes		Yes			Yes						Yes	RRG to study extending 46th to BBD, per JP's scope
City of Tampa Parklets	Michael Case	Beth Alden			Yes			Yes						Yes	And/or full board in August
MPO Regional Planning Best Practices Study	Brian Shroyer	Allison Yeh			Yes			Yes						Yes	Schedule after walking tour
W. Busch Blvd Corridor Study	Jim Hudock	Gena Torres	No		Yes			Yes						Yes	
Hillsborough County Design Standards					Yes			Yes						Yes	
Tampa Sidewalk Requirements		Rich Clarendon			Yes			Yes						Yes	requested by Vance Arnett - check w/ Jan Washington
Planning Commission Alexander Street Marketing Corridor Study	Jay Collins	Gena Torres			Yes			Yes						Yes	Jay thinks it should be ready and a good topic
Toll Lane Before/After Studies		Rich Clarendon			Yes			Yes						Yes	confirm w/ David Cook @ Turnpike
Channelside Drive Design Workshop		Lisa Silva			Yes			Yes						No	
OLD/NEW BUSINESS															
ADDENDUM ITEMS															
SPECIAL PRESENTATIONS															

West Busch Boulevard (SR 580) Corridor Study

from North Dale Mabry Highway to North Nebraska Avenue

Hillsborough County, Florida

FDOT Financial Project ID Number: 435908-1-22-01

Local Agency Coordination Meeting



Purpose of this meeting

1. Discuss draft typical section alternatives
2. Gauge agency interest in recommendations for further study which would require inter-agency coordination (JPA, maintenance agreements, etc.)



Agenda

1. Introductions
2. Project Overview
3. Typical Section Alternatives
4. Inter-agency Recommendations (Interim Design)
5. Other Recommendations



Introductions



Project Overview



Project Study Area

- 3.3 miles
- Within the city of Tampa and unincorporated Hillsborough County
- Concurrent RRR project



Study Purpose

Achieve the Corridor Vision:

*“The Busch Boulevard corridor offers **safe, comfortable and convenient access through and across the corridor for all users and all travel modes.**”*

PD&E Study Changes
(implement typical section changes)

Interim Design Project Changes
(implement spot improvements)

Resurfacing, Restoration and Rehabilitation Project (3R) Changes

Busch Boulevard Corridor Study
(identify the vision, recommendations & implementation plan)

**We
Are
Here**

Updated Study Schedule

- Began in March 2017
- Held Agency Project Kick-off Meeting, 3 Project Advisory Group Meetings, & Public Visioning Workshop
- **Meet with local agencies to review draft alternatives and recommendations**
- PAG #4 – May 24, 2018
- Alternatives Public Meeting – June 2018
- Final Corridor Alternatives and Strategies Report – August 2018

Long Term Recommendations: Typical Section Alternatives

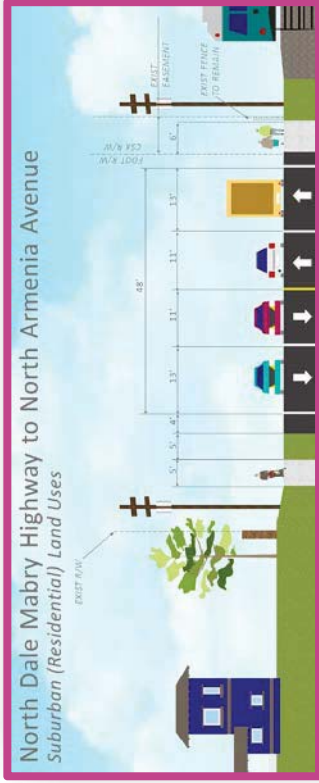


Currently Developing Range of Alternatives

Alternative	Description
No-Build	Existing condition
4-Lane	Construct 4-lane divided roadway without bike lanes, but with raised median, border width, and wide (8') sidewalk to accommodate additional pedestrian demand
5-Lane	Construct a 5-lane divided roadway with flush unrestricted median, bike/parking lanes, border width, 6' sidewalks
6-Lane	Construct a 6-lane divided roadway with raised median, bike lanes, border width, 6' sidewalks

- Many variations are possible, but these alternatives provide a good range to receive feedback and further refine the concepts for the corridor vision
- To be presented at the Public Alternatives Meeting
- PD&E Alternative(s) could be all, some, or a variation of these alternatives

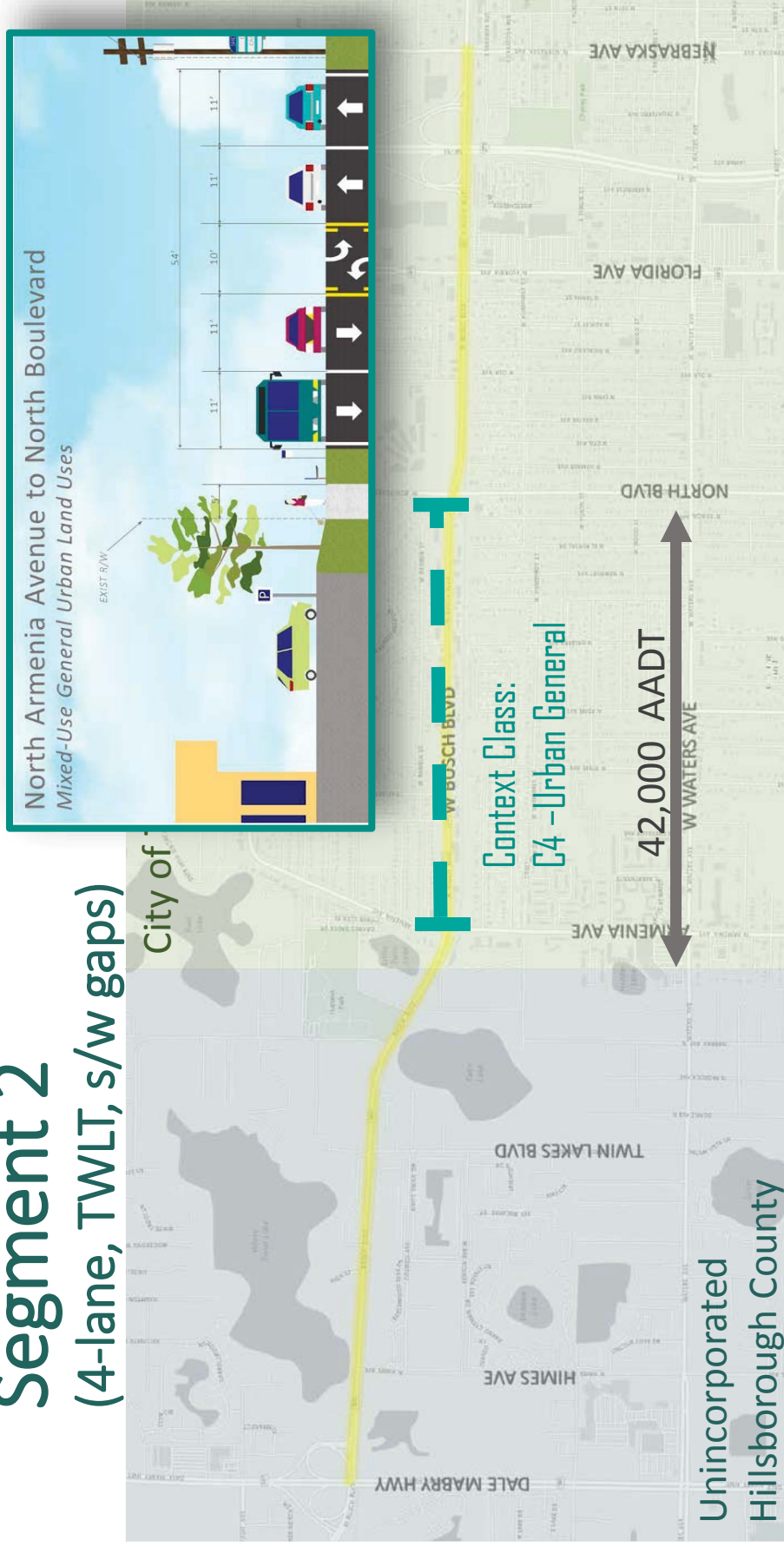
Existing Conditions (no build) Segment 1 (4-lane undivided w/sidewalk)



Existing Conditions (no build)

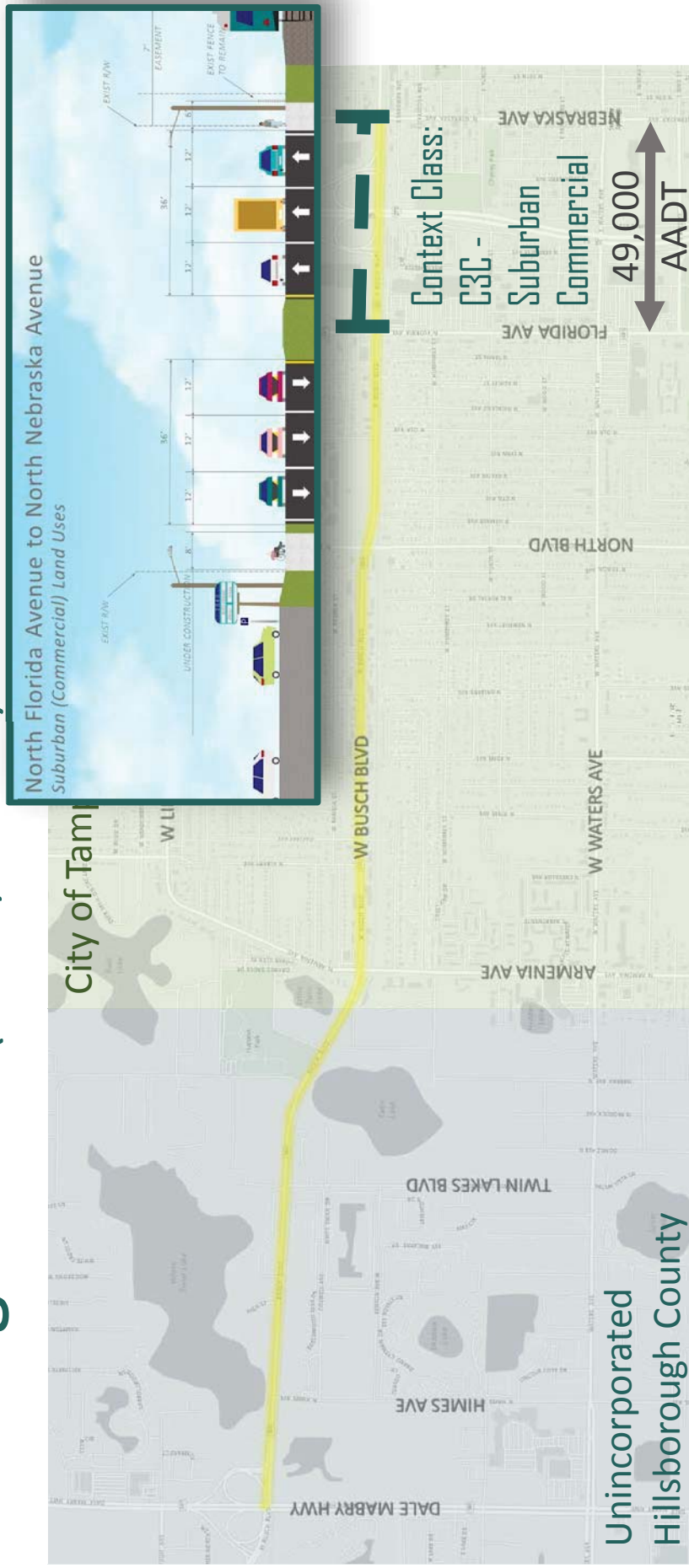
Segment 2

(4-lane, TWLI, s/w gaps)

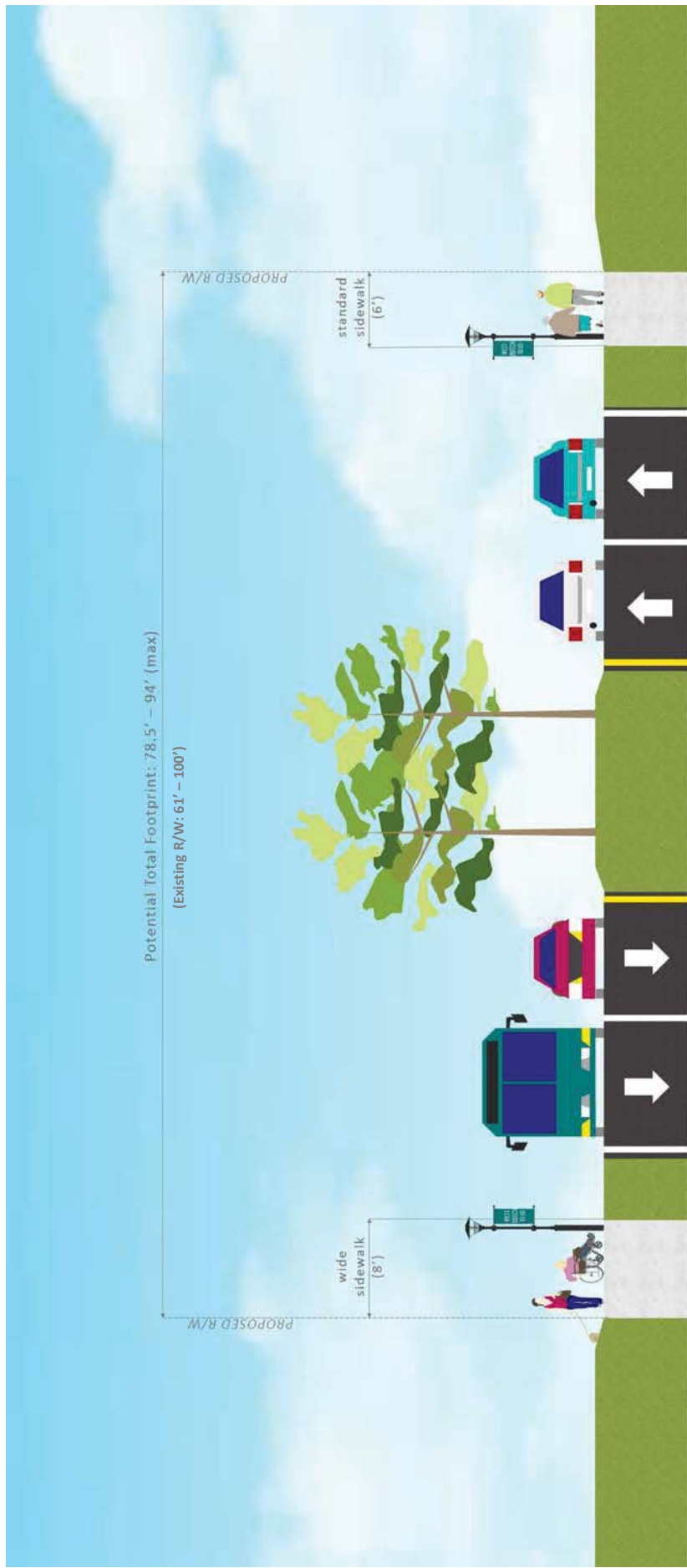


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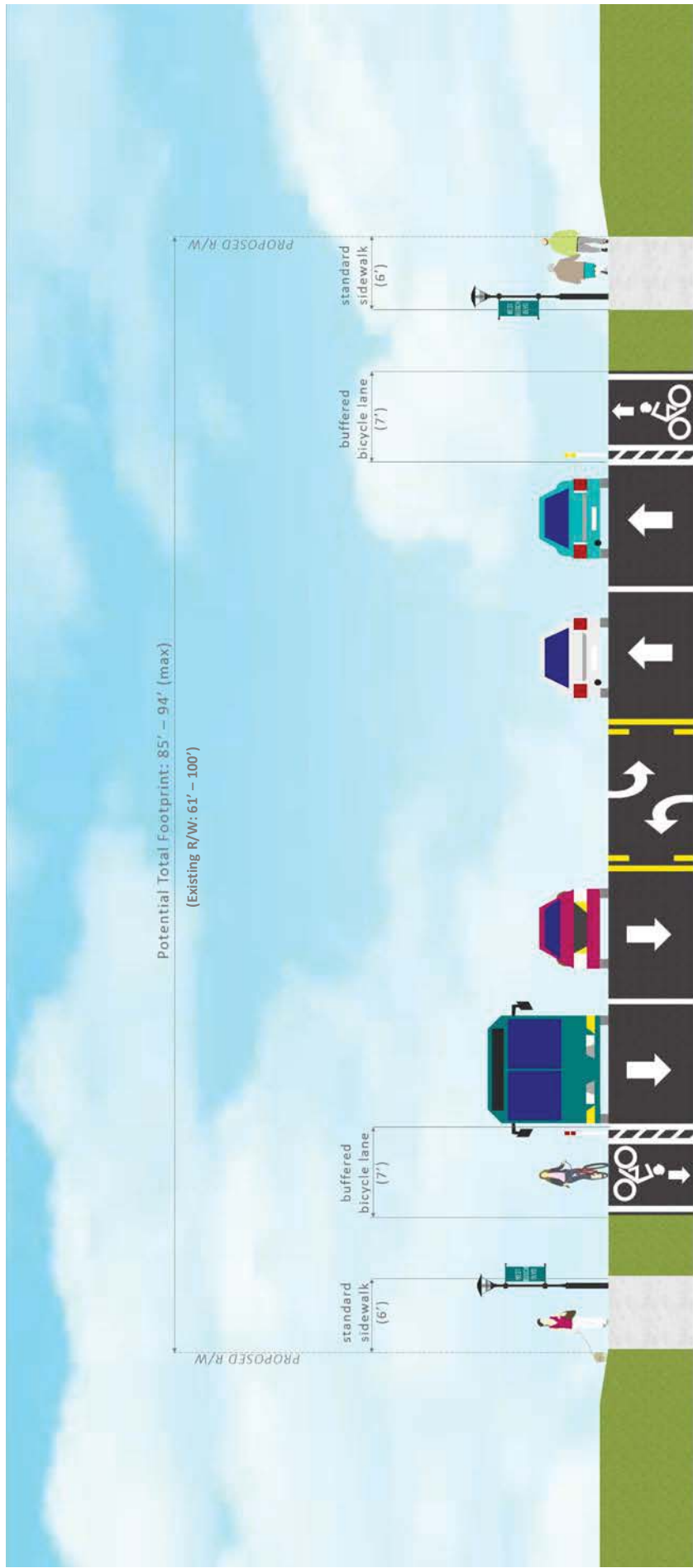
Segment 4 (6-lane, median)



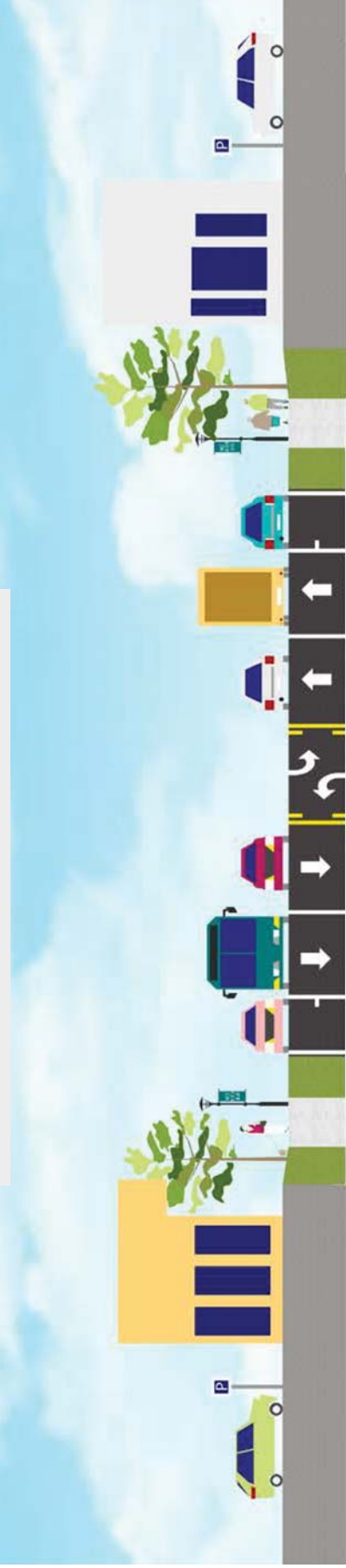
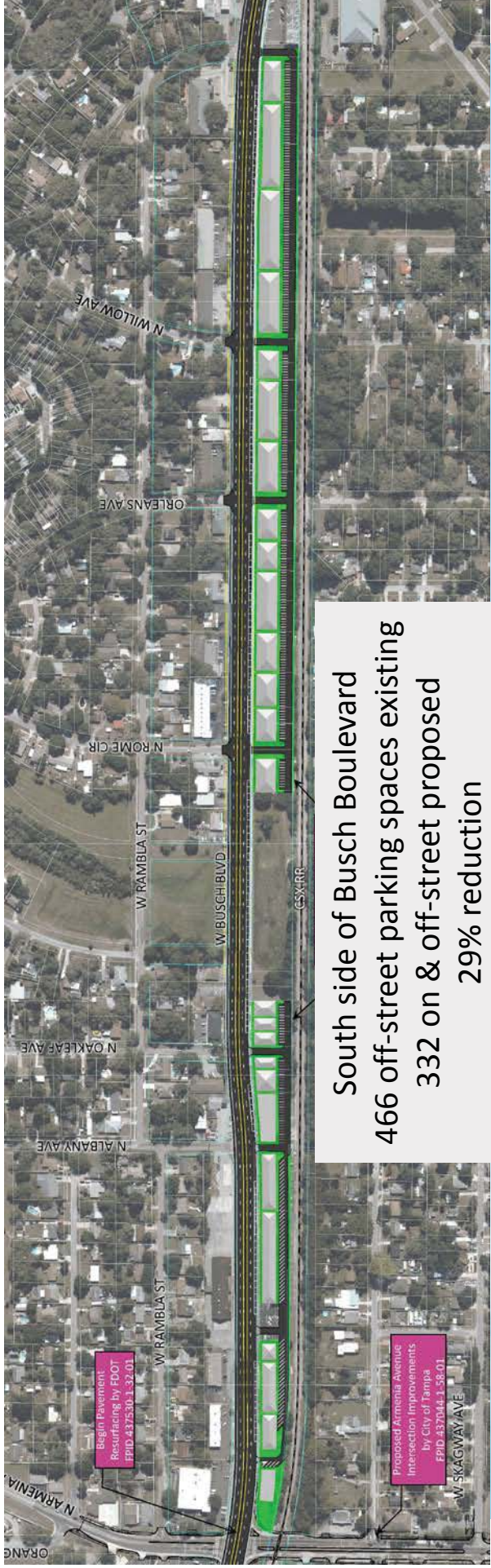
Alternative 1: 4 Lanes with Median and Wide Sidewalk on the North Side



Alternative 2: 4 Lanes with Center Two-Way Left Turn Lane and Buffered Bike Lanes

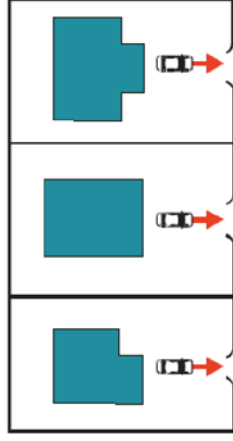


Potential Parallel Parking Option for Segment 2 (North Armenia Ave. to North Blvd.)

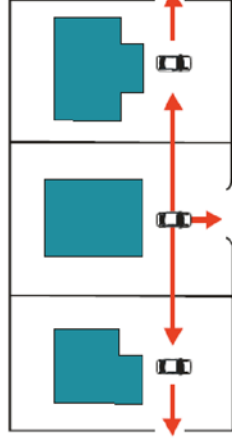


Width of parallel parking = buffered bike lane

City of Tampa code does not currently address cross/joint access and discourages commercial access from non-arterial streets (Sec. 27-283.12.(j)(1)).



Avoid



Promote

- Cross Access
- Joint Access
- Complete on-site circulation

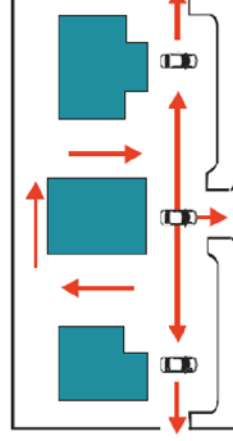


Image: Driveway Information Guide (FDOT 2008) p. 85

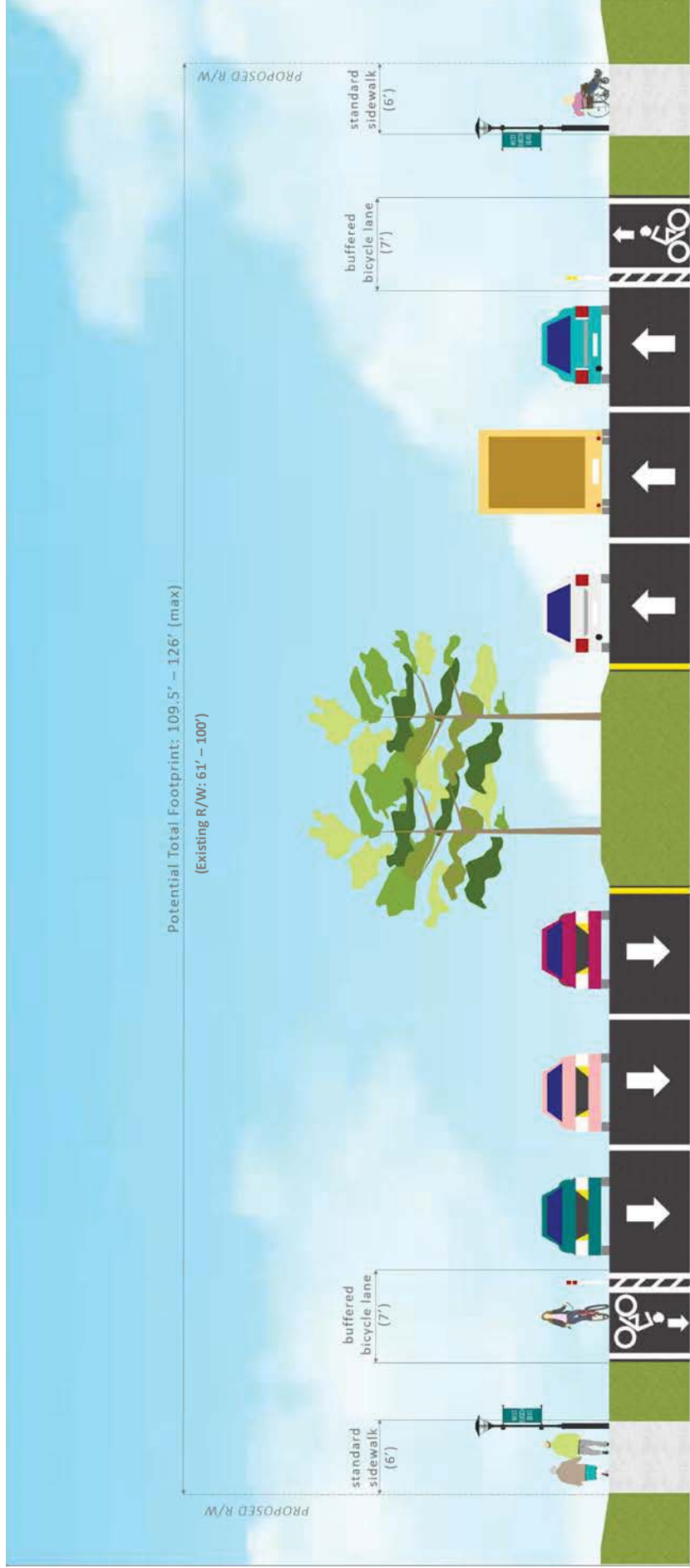
To address the public interest in these matters, land development regulations include conditions or circumstances where nonconforming access features may be brought into conformance. Such conditions may include:

- when new driveway permits are requested;
- an increase in land use intensity;
- substantial enlargements or improvements;
- significant change in trip generation; and
- as changes to roadway design allow. (p.1-8)

Overlay requirements may address any issues of concern, such as joint access, parking lot cross access, reverse frontage, driveway spacing, and limitations on new driveways. (p. 1-9)

Source: "Land Development and Subdivision Regulations that Support Access Management", Center for Urban Transportation Research, University of South Florida, January 1994. Williams, Kristine M. <https://www.cutr.usf.edu/oldpubs/pcm/files/Model%20Land%20Development%20and%20Subdivision%20Regulations%20that%20Support%20Access%20Management.pdf>

Alternative 3: 6 Lanes with Median and Buffered Bike Lanes

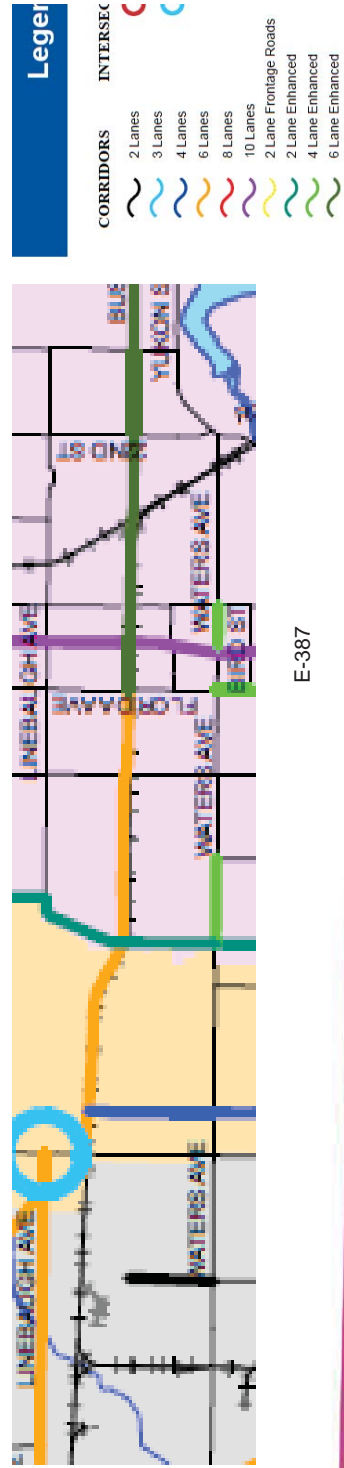


Planning Consistency – 6 Lane Alternative

- 2040 LRTP
 - ≈ Need for 6/7 lanes on “Beyond 2040” Map (pg. 98) from Armenia Blvd. to North Blvd.



- Tampa Comprehensive Plan (effective date February 20, 2016)
 - Constrained Roadways (physical constraints)
 - ≈ Busch Blvd. from Dale Mabry Blvd. to North Blvd. constrained to 4 lanes
 - ≈ “Page 151; MBY (Mobility) Policy 3.2.2: Constrained roadways... shall be eligible for... Dedicated transit lanes...”
- Hillsborough County Corridor Preservation Plan (Map 25) shows preserved for 6 lanes/ 6 lanes enhanced through study limit



Inter-agency Recommendations (Interim Design)



Interim Design Project Recommendations

- Safety improvements at Dale Mabry off-ramp
- Turn lane extensions as feasible
- Complete sidewalk gaps
- Add signal and/or mid-block crossing at Rome Circle or Willow Ave.
- Leading pedestrian interval or ped-only phase at North Blvd.
- Add refuge islands or tighten curb radius at North Florida Ave. and North Nebraska Ave.
- Add YOUR SPEED radar sign(s)

Potential additional improvements with local agreements include...

Corridorwide Recommendations



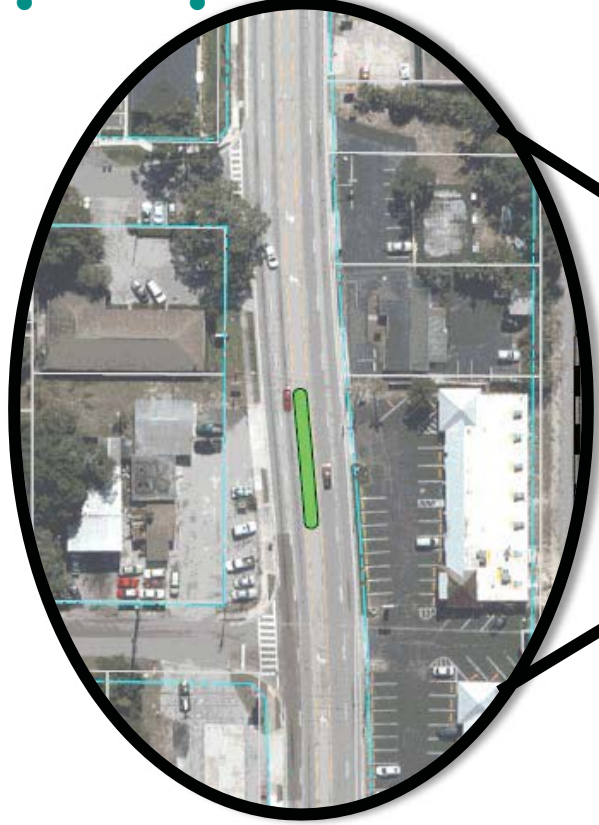
- Add pedestrian lighting
- Interconnect signals to increase platooning
 - ≈ Traffic signal speed signs
 - ≈ Explore potential of dynamic signal timing and/or setting signals to a 35 mph target speed
 - ≈ City and county signals are not interconnected



*Traffic Signal Speed Sign
MUTCD, Section 2H.03 Traffic Signal Speed
Sign (11-1)*

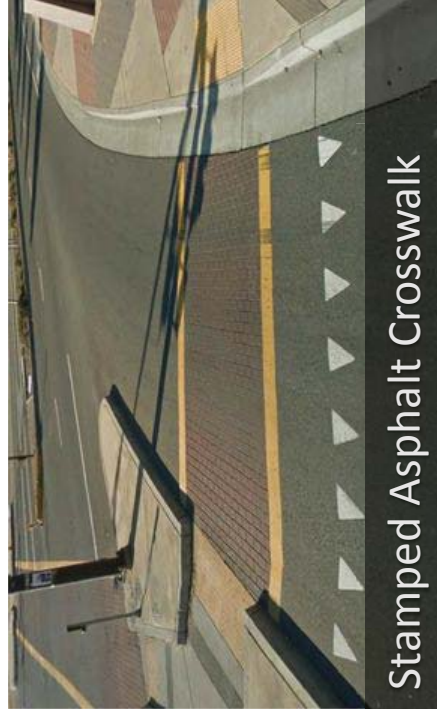
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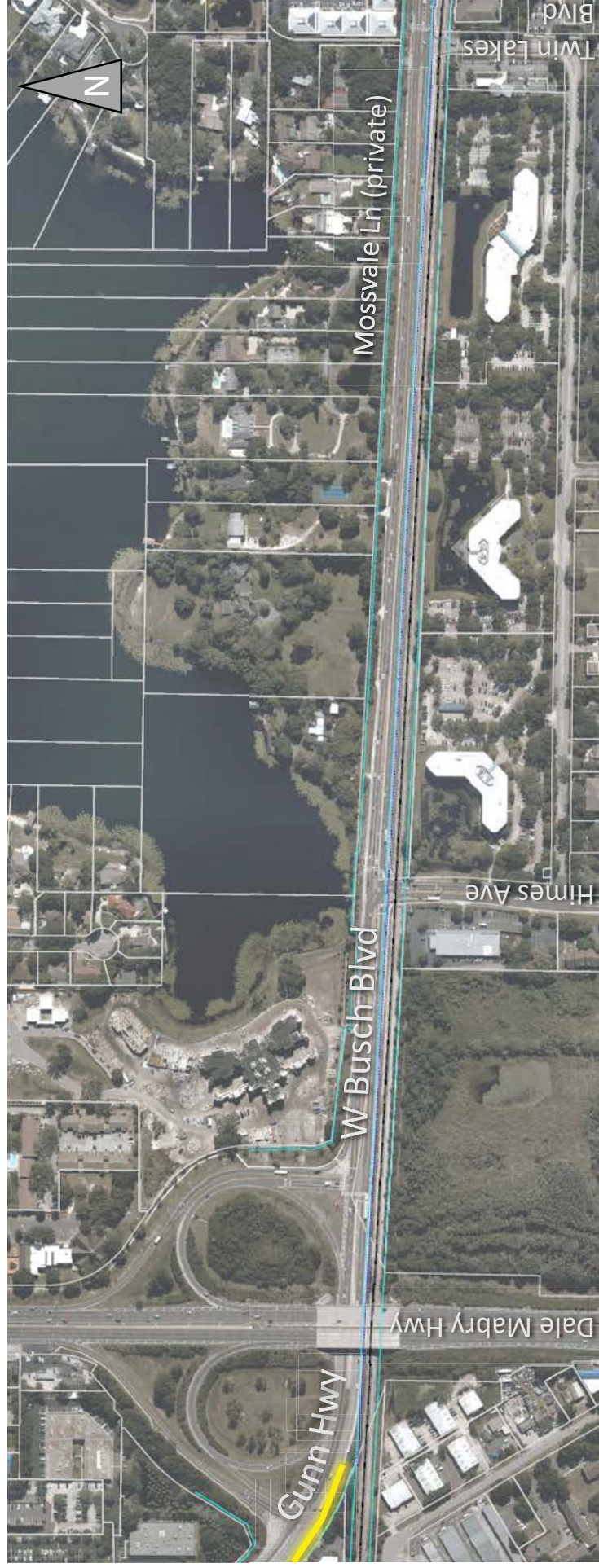
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High-emphasis pedestrian crossings / hardscape intersections



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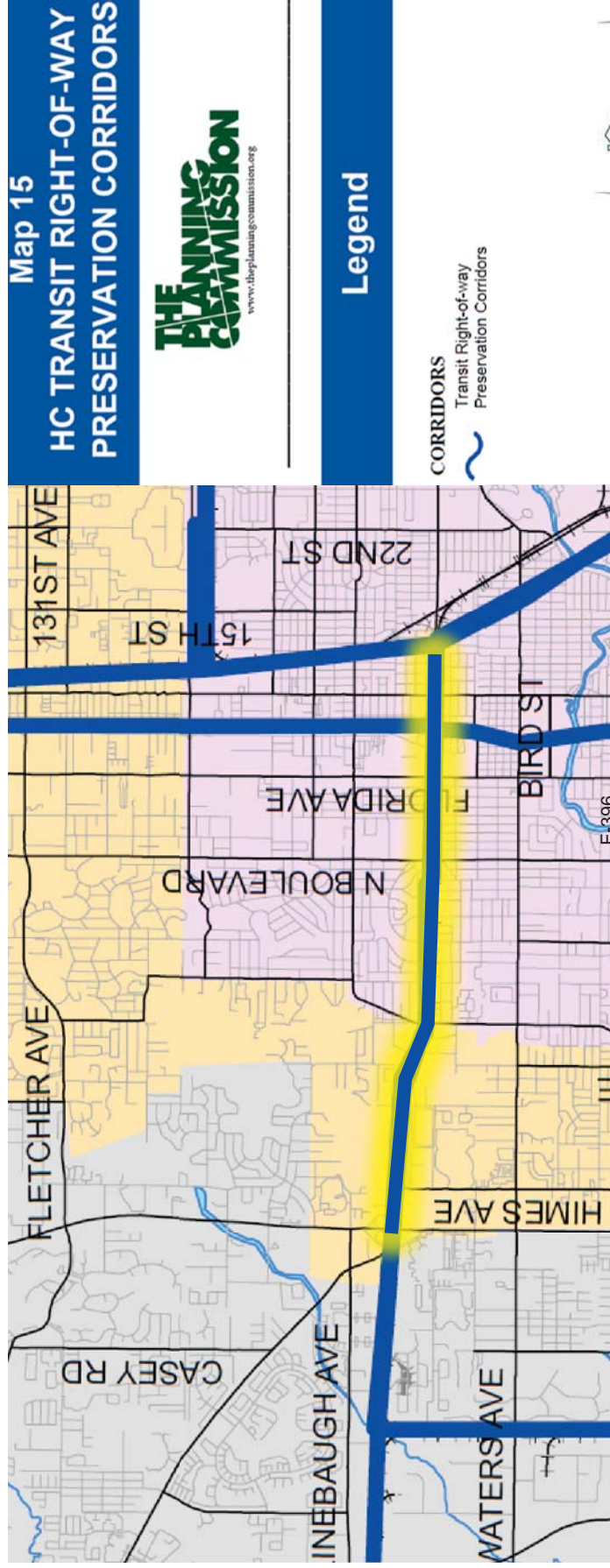


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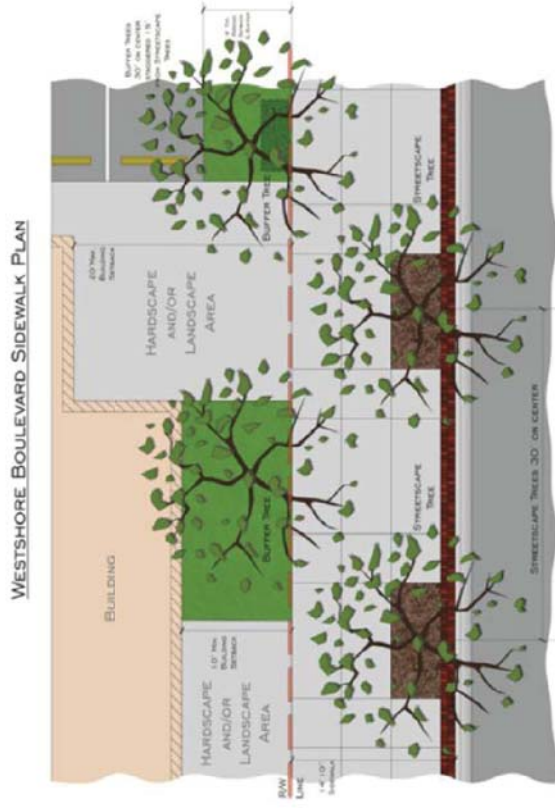
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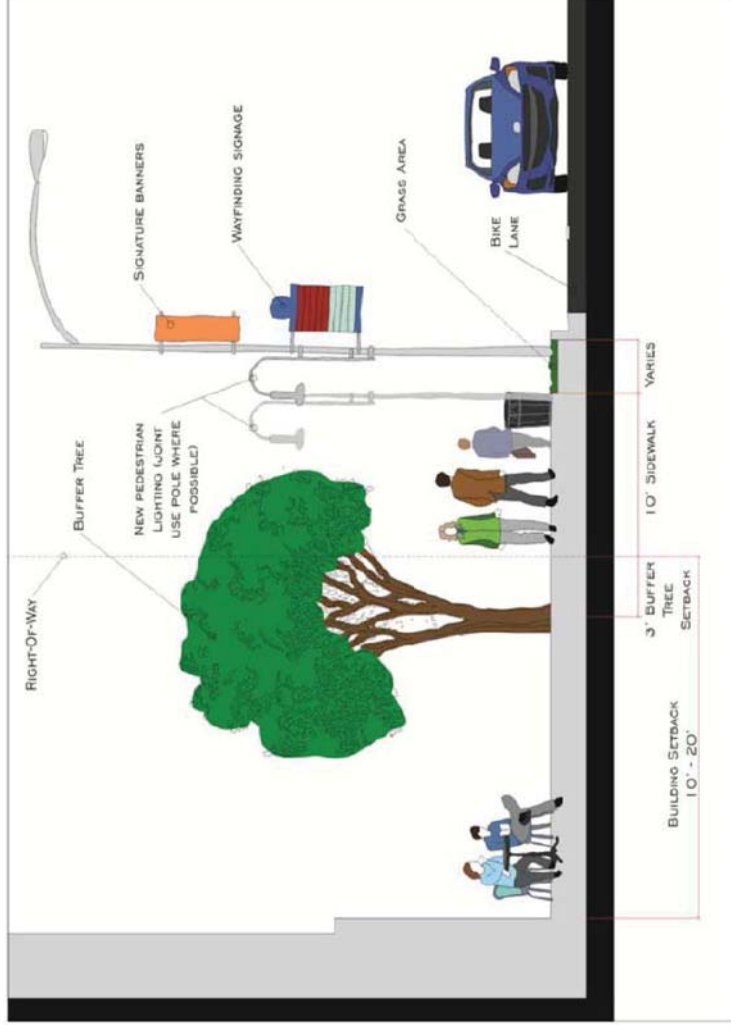
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 - ≈ Landscaping
- Development/redevelopment requirement to provide landscaping along sidewalk can calm traffic and improve pedestrian environment



Adopt an overlay district to support corridor vision

- Coordinate landscaping opportunities with development code
- Set-back for R/W preservation based on recommended alternative(s)
- Reduced/revised parking requirements
- Form-based code to promote pedestrian environment



City of Tampa Code Sec. 27-238 Westshore Overlay District Table 238.c

Bicycle Master Plan/ Bicycle Network

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- Bicycle routes defined at a regional level would help support the potential future investment of bicycle lanes on Busch Boulevard
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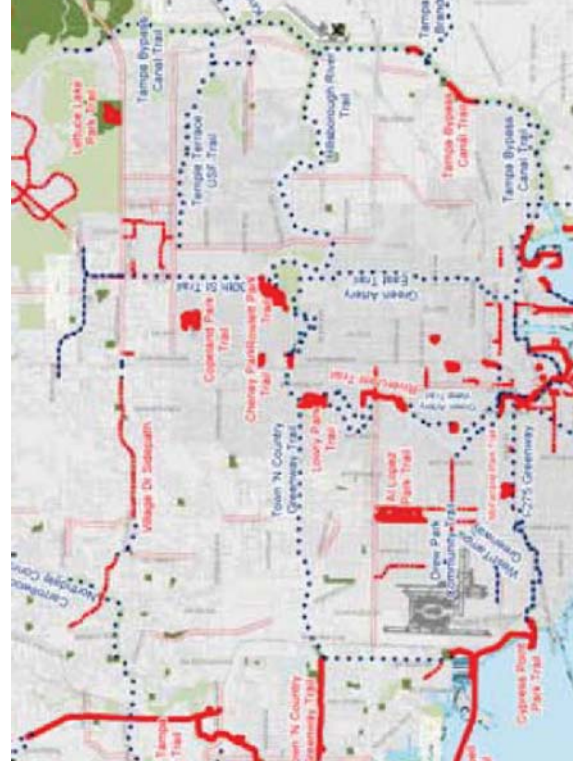
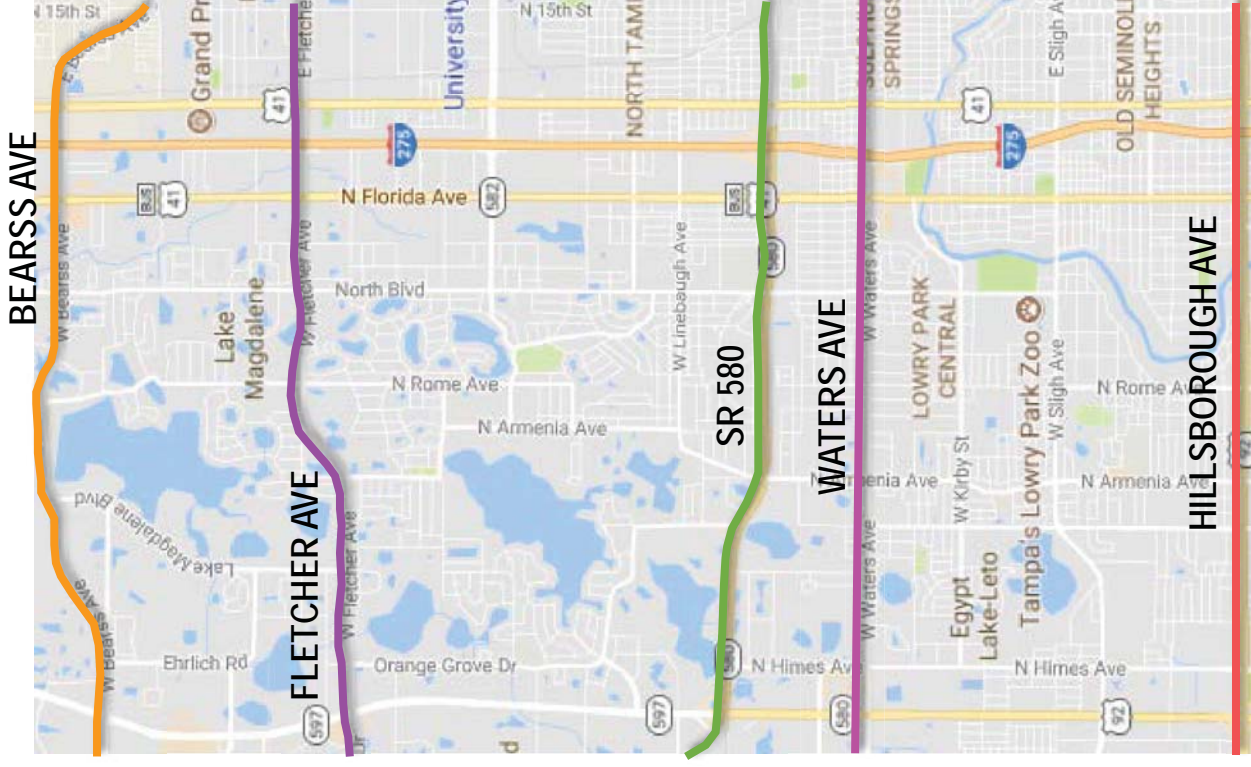


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A long-exposure photograph of a high-speed train at night, creating a sense of rapid motion. The train's lights and structure are blurred into streaks of white, blue, and orange, radiating from a central point in the distance. The background is dark, with some distant lights visible.

CSX - Existing Regional Freight Rail Corridors

Previous Studies

Transportation Management Area
Leadership Group Meeting
November 6, 2015

Context: TMA Priorities

Approved by TMA
Leadership Group
June 5, 2015

"TOP 5" REQUESTS							
TBX* Phase 1	I-275 @ SR 60	Interchange Modification		CST		PE Funded 2019 ROW Funded 2016-19	
	I-275 From SR 60 to N. Boulevard	TBX* with Express Bus		PE & CST		PD&E Underway	
	I-275 From 4th Street to Gandy Blvd.	TBX* with Express Bus Connection to Downtown St. Pete		PE & CST, Gateway Transit Center PD&E		PD&E Underway	
	I-4 From 22nd St to CR 579	TBX* with Express Bus		PE & CST		PD&E Underway	
	I-275 @ I-4	Interchange Modification		CST		Re-eval Underway	
	I-275 From Osborne Ave to Bearss Ave	TBX* with Express Bus		PE & CST, Fletcher Transit Ctr PD&E		PD&E Underway	
	I-4 from east of I-75 to Polk Parkway	TBX* with Express Bus		PD&E			
	I-75 From US 301 to Fowler Ave	TBX* with Express Bus		PE & CST		PD&E Complete	
	I-75 From SR 674 to US 301	TBX* with Express Bus		PE & CST		PD&E Complete	
	Westshore Multimodal Center & Connections to Downtown & Airport	Intermodal Center		PE, ROW, CST		Site Selection Underway	
	People Mover Connection to Airport						
	Modern Street Car Extension		PE for extension to Marion Transit Ctr		PD&E funded for extension to MTC		
US 41 CSX Rail Corridor	Commuter Transit		Corridor Study/PDE		CSX-FDOT MOU for joint use pending		
	Standard Fare Pavment Media Across Counties		CST		Partially funded		
Regional Farebox System	North Gap: John Chesnut Park to Bighthouse Networks Field; South Gap: Belleair Rd to San Martin Bridge		Design & CST		Partially funded		
Duke Energy Trail							

Abbreviated History of Rail Initiatives in the Tampa Bay Area

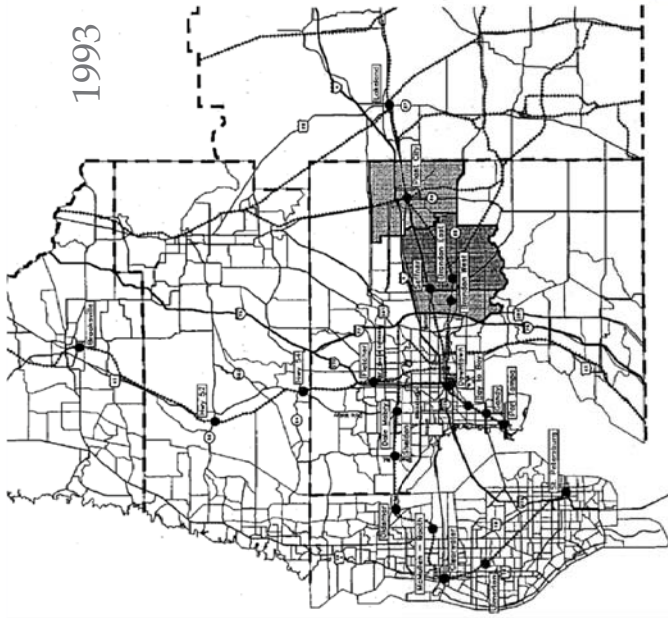
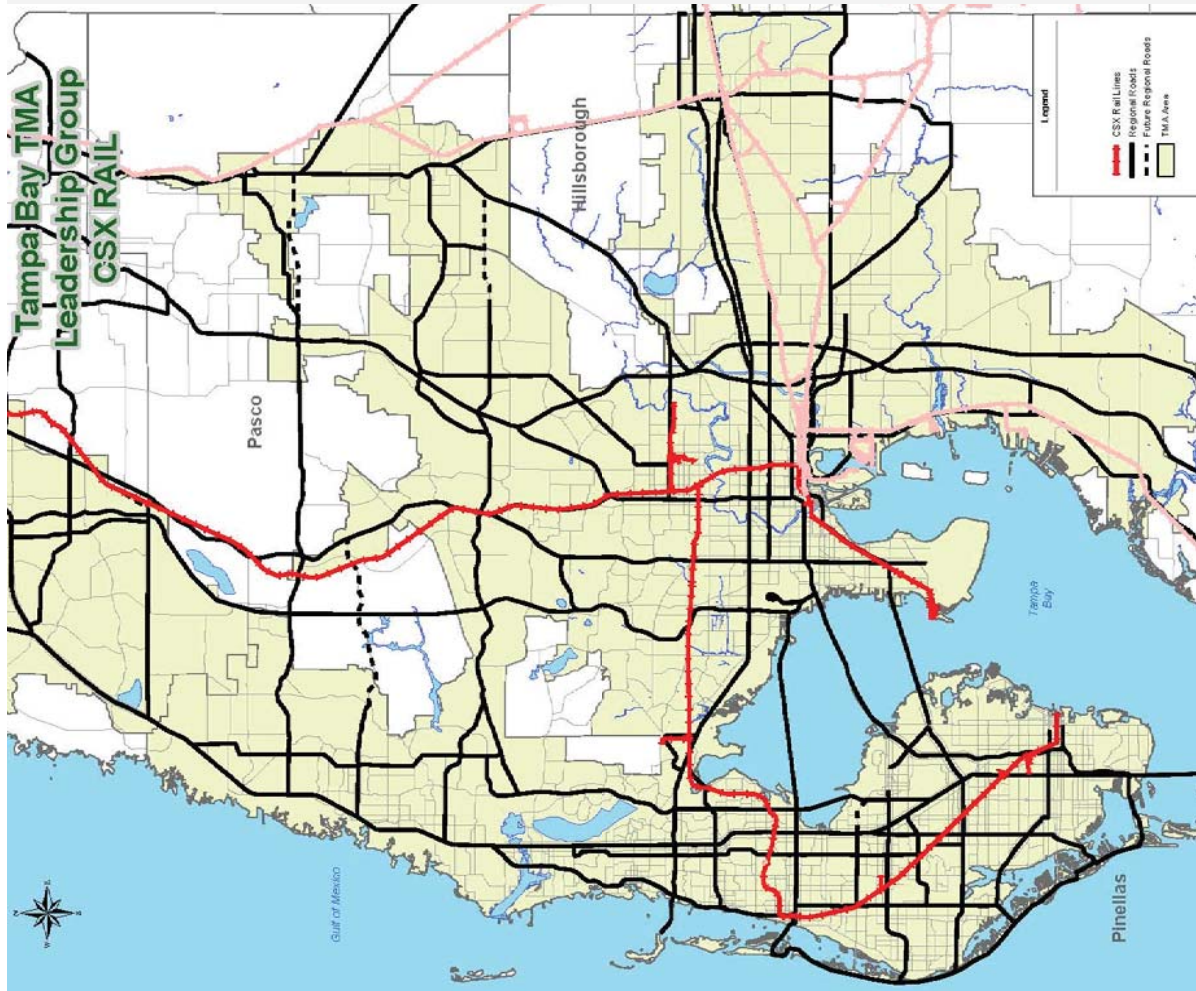
- Tampa Bay Commuter Rail Authority (1990-1996)
 - Commuter Rail Development Plan (1992)
 - Commuter Rail Feasibility Study (1993)
- HART Mobility Major Investment Study (1998) “The Mobility Study”
- HART Tampa Rail Project – Alternatives Analysis & Environmental Impact Statement (2002 Record of Decision)
- Pinellas Mobility Initiative
- West Central Florida MPO Chairs Coordinating Cmte (1991-present)
 - CCC 2025 Regional LRTP (2004)
 - CCC 2035 Regional LRTP (2009)
- FDOT-D7 Strategic Regional Transit Needs Assessment (2006)
- TBARTA Master Plan (2009)
- Moving Hillsborough Forward (2010)
- Green Light Pinellas (2014)

TAMPA BAY COMMUTER RAIL AUTHORITY

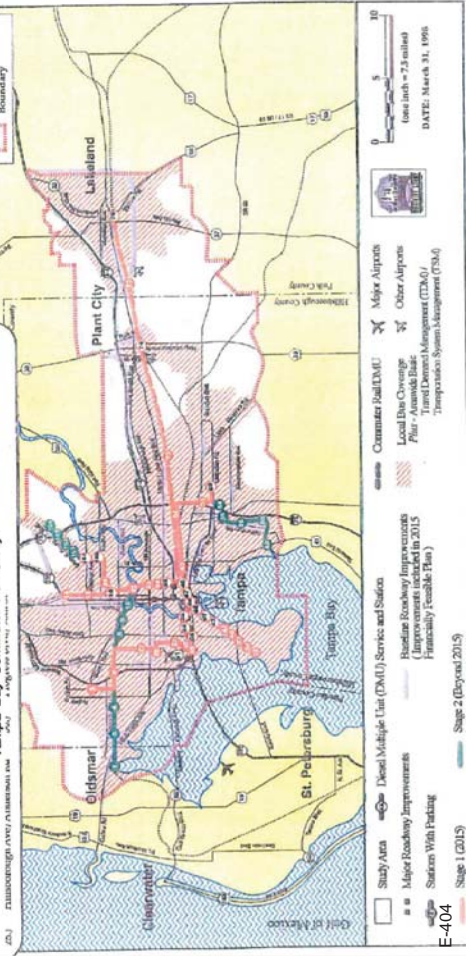


West Central Florida Metropolitan Planning Organizations
Chairs Coordinating Committee
Providing Transportation Planning for Citrus, Hernando, Hillsborough, Manatee, Pasco,
Pinellas, Polk and Sarasota Counties





SERVICE AREAS - TAMPA TO LAKELAND "S" LINE
Tampa Bay Commuter Rail Study

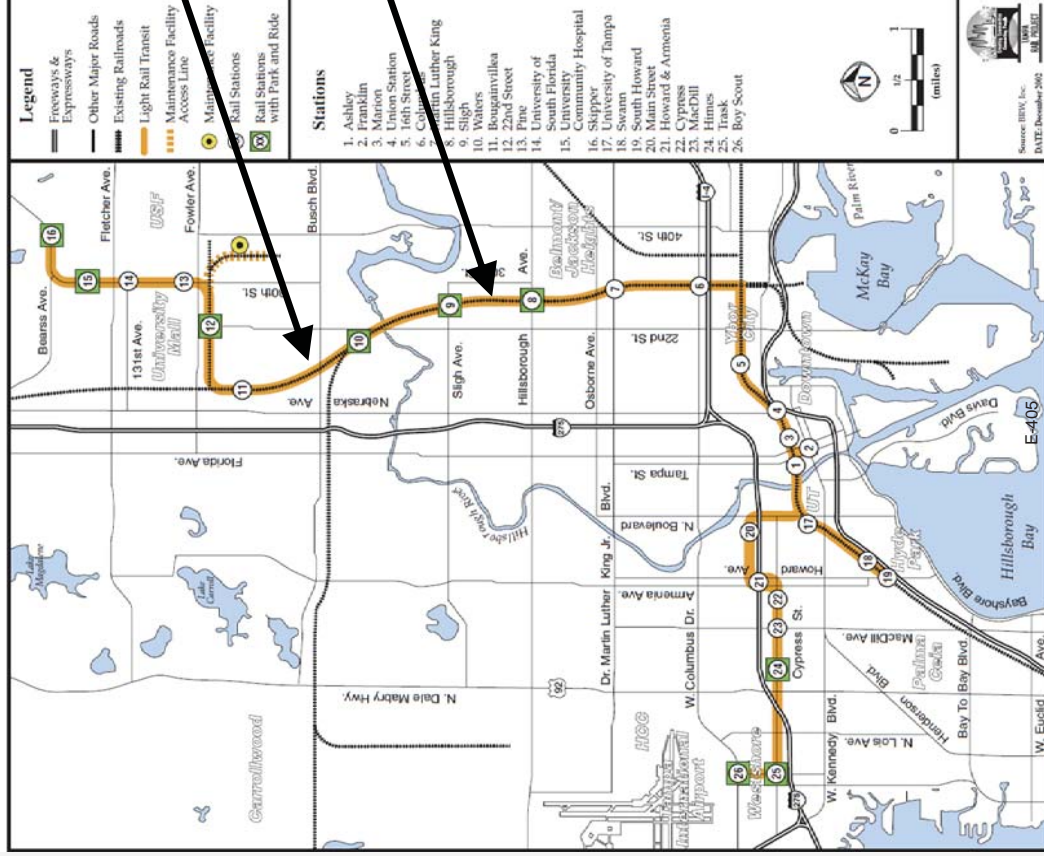


1998

Figure 3-15
Long Term Vision & Locally Preferred Strategy

Figure I-6

Locally Preferred Alternative



HART
Tampa Rail Project
Final Environmental
Impact Statement:
2002 Record of
Decision

TBARTA Master Plan 2009

Long-Term Regional Network (2050)

Shows What Our Future Can Be

What the Long-Term Regional Network Includes:



Short-Distance Rail - Probably light rail, to connect regional anchors. (135 miles)



Long-Distance Rail - Commuter service on rail operating to serve peak commuting hours. (115 miles)



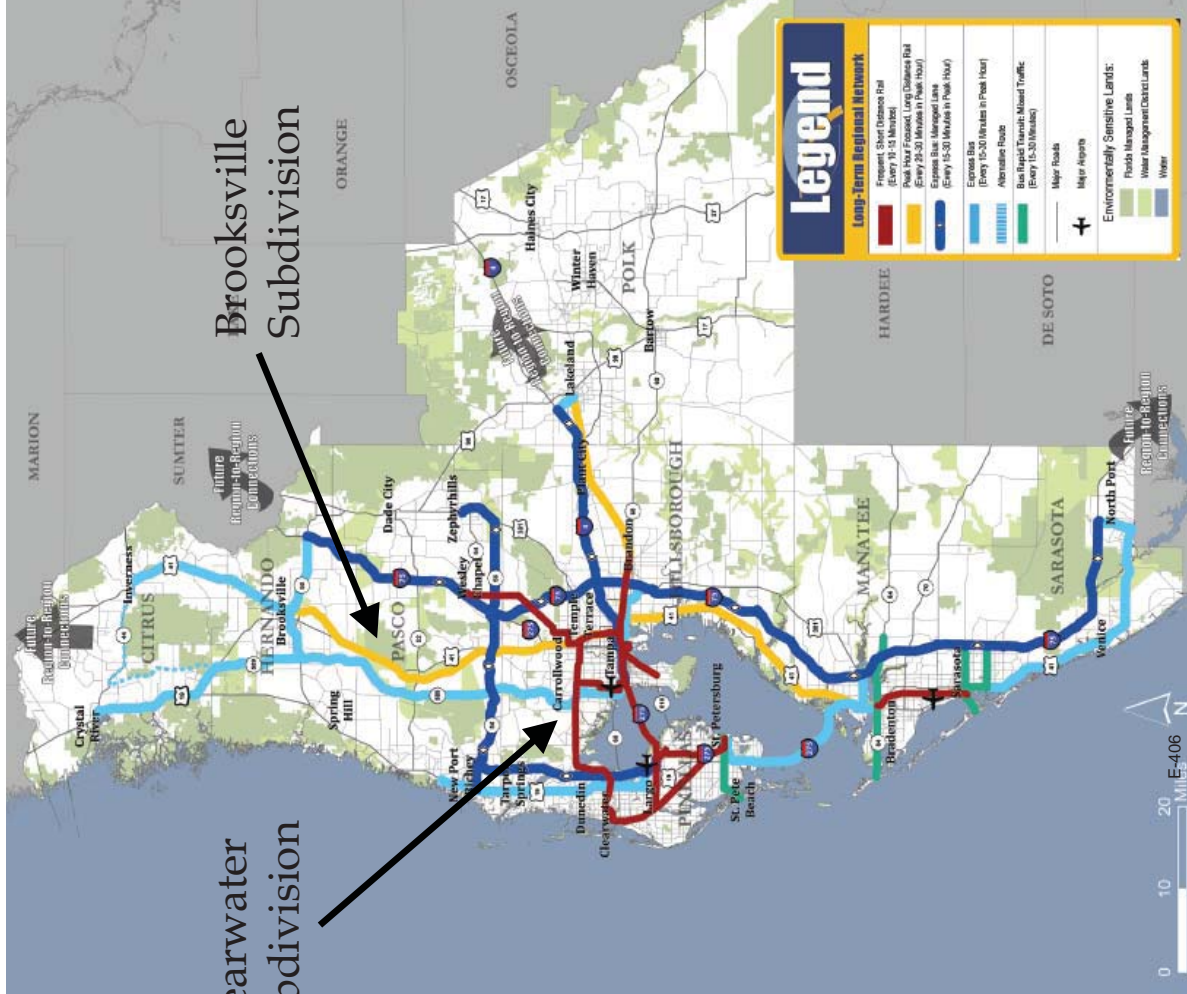
BRT in Mixed Traffic - Buses that make limited stops, operating in lanes shared with cars. (42 miles)



Managed Lanes - Special lanes on highways for buses and carpool vehicles, with tolls for other vehicles. (220 miles)



Express Bus - Long-distance service (very few stops), but operating either in Managed Lanes or in mixed-flow traffic on highways and/or major roads. (217 miles)



Clearwater
Subdivision

Brooksville
Subdivision

Master Plan Development, Phase 2

- ❖ Focused on technology (ex, LRT, BRT, CRT, etc.)
- ❖ Reviewed all the regional corridors with respect to potential ridership, physical constraints, and public comment
- ❖ Suggested the best-fit technology for each corridor
- ❖ Note: not all corridors advanced into Phase 3
- ❖ The Clearwater and Brooksville Subdivision corridors were evaluated for four travel markets:
 - a) Connecting Pasco to urbanized areas of Hillsborough & Pinellas
 - b) Connecting Hillsborough & Pinellas to each other
 - c) North-south within Pinellas
 - d) Longer-distance trips from Citrus south to Hillsborough



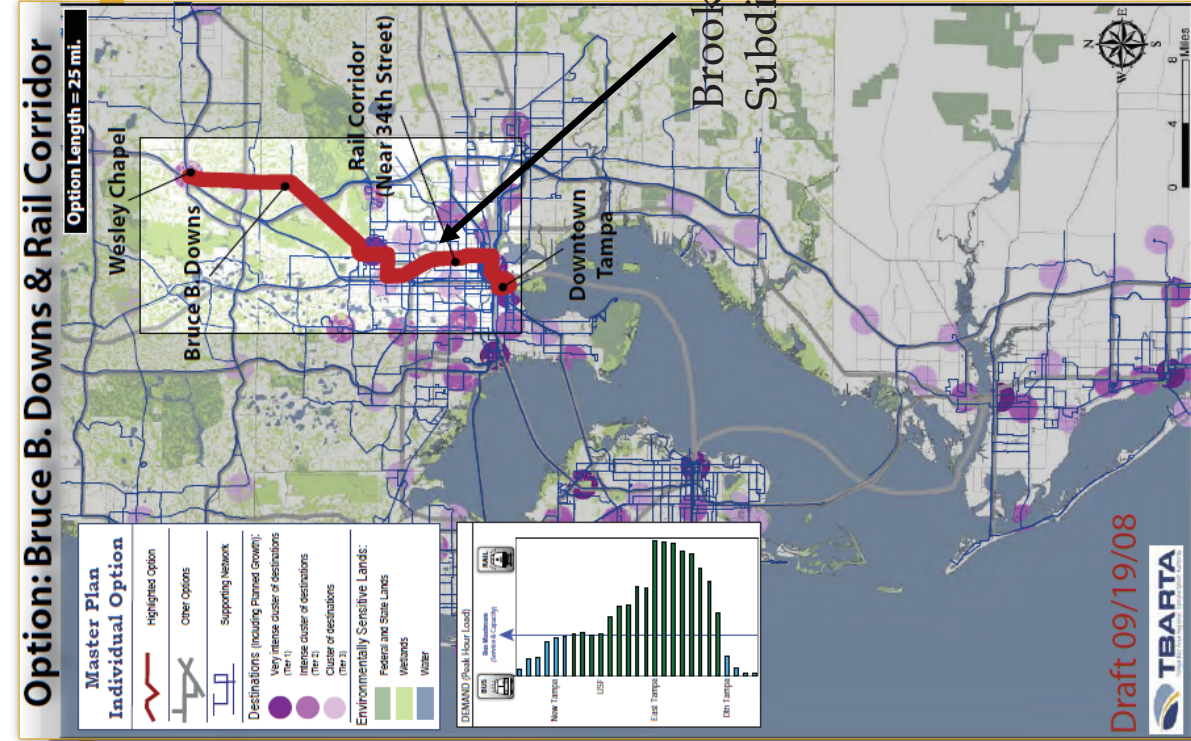
IMPROVEMENT TYPES

Does the Option Serve Regional Trips?	Does the Improvement Match the Potential Demand?	Level of Engineering or Financial Constraints?
YES	YES	HIGH

Light Rail		
<ul style="list-style-type: none"> - Rail on entire length - Frequent service: Every 10 min. - Stations spaced between 1/2 and 1 miles apart 	<p>YES</p> <ul style="list-style-type: none"> - Typical Trip Length is 20 mi. - Crosses 2 Counties - Connects to 9 Intermodal Facilities - Connects to 9 Destination Clusters 	<p>HIGH</p> <ul style="list-style-type: none"> - Grade-separated crossings may be needed at several major roads; to be acquired - Unless FRA compliant, requires separation from freight

Commuter Rail		
<ul style="list-style-type: none"> - Commuter rail on entire length - Commuter service: Every 30 min. - Stations spaced between 5 and 10 miles apart - Park-and-Ride facilities 	<p>YES</p> <ul style="list-style-type: none"> - Typical Trip Length is 20 mi. - Crosses 2 Counties - Connects to 9 Intermodal Facilities - Connects to 9 Destination Clusters 	<p>MEDIUM</p> <ul style="list-style-type: none"> - Unless FRA compliant, requires separation from freight

Bus Rapid Transit with Traffic Priority		
<ul style="list-style-type: none"> - BRT on entire length (Frequent Service) 	<p>YES</p> <ul style="list-style-type: none"> - Typical Trip Length is 20 mi. - Crosses 2 Counties - Connects to 9 Intermodal Facilities - Connects to 9 Destination Clusters 	<p>LOW</p> <ul style="list-style-type: none"> - Need intersection improvements for traffic priority treatments



IMPROVEMENT TYPES

Does the Option Serve Regional Trips?	Does the Improvement Match the Potential Demand?	Level of Engineering or Financial Constraints?
YES	YES	HIGH

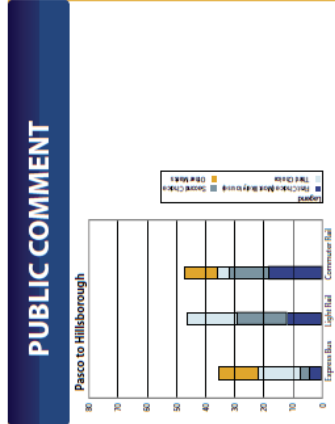
Light Rail		
<ul style="list-style-type: none"> - Rail on entire length - Frequent service: Every 10 min. - Stations spaced between 1/2 and 1 miles apart 	<p>YES</p> <ul style="list-style-type: none"> - Typical Trip Length is 20 mi. - Crosses 2 Counties - Connects to 9 Intermodal Facilities - Connects to 9 Destination Clusters 	<p>HIGH</p> <ul style="list-style-type: none"> - Grade-separated crossings may be needed at several major roads; to be acquired - Unless FRA compliant, requires separation from freight

Commuter Rail		
<ul style="list-style-type: none"> - Commuter rail on entire length - Commuter service: Every 30 min. - Stations spaced between 5 and 10 miles apart - Park-and-Ride facilities 	<p>YES</p> <ul style="list-style-type: none"> - Typical Trip Length is 20 mi. - Crosses 2 Counties - Connects to 9 Intermodal Facilities - Connects to 9 Destination Clusters 	<p>MEDIUM</p> <ul style="list-style-type: none"> - Unless FRA compliant, requires separation from freight

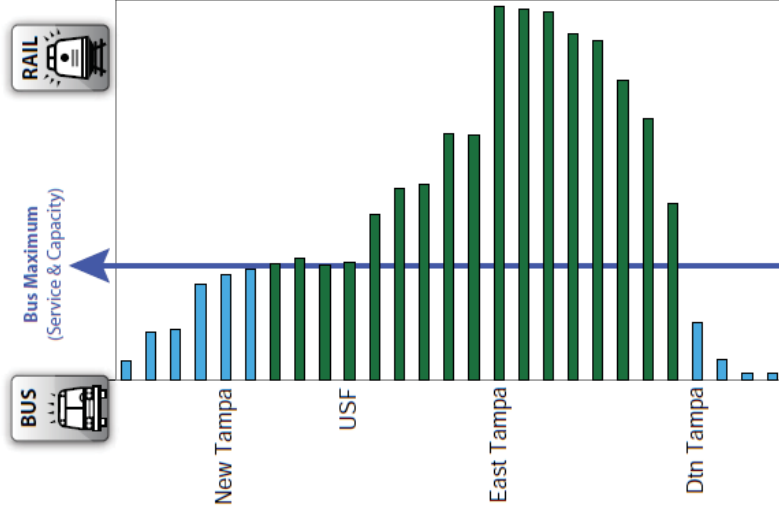
Bus Rapid Transit with Traffic Priority		
<ul style="list-style-type: none"> - BRT on entire length (Frequent Service) 	<p>YES</p> <ul style="list-style-type: none"> - Typical Trip Length is 20 mi. - Crosses 2 Counties - Connects to 9 Intermodal Facilities - Connects to 9 Destination Clusters 	<p>LOW</p> <ul style="list-style-type: none"> - Need intersection improvements for traffic priority treatments

DISCUSSION POINTS

- 1) Requires negotiations with CSX.
- 2) Coordinate with Bruce B. Downs widening project (need additional ROW on Bruce B. Downs).
- 3) Unless FRA compliant, requires separation from freight.
- 4) Consistency with Tampa Rail Project.



DEMAND (Peak Hour Load)



RECOMMENDATION

Light Rail

Note: In Phase 3, the term "Light Rail" was generalized to "Short Distance Rail" so as not to exclude DMU/EMU technology.



DISCUSSION POINTS

- 1) Requires negotiations with CSX.
- 2) Coordinate with Bruce B. Downs widening project (need additional ROW on Bruce B. Downs).
- 3) Unless FRA compliant, requires separation from freight.
- 4) Consistency with Tampa Rail Project.

IMPROVEMENT TYPES

Does the Option Serve Regional Trips?	Does the Improvement Match the Potential Demand?	Level of Engineering or Financial Constraints?
---------------------------------------	--	--

Light Rail

- Rail on entire length
- Frequent service: Every 10 min.
- Stations spaced between 1/2 and 1 miles apart

- Typical Trip Length is 20-25 mi.
- Crosses 2 Counties
- Connects to 7-9 Intermodal Facilities
- Connects to 14-16 Destination Clusters

- Demand exceeds capacity south of Race Track Rd. (See demand insert to left)

- ROW is limited along I-275 north of downtown Tampa and may have to be acquired
- ROW is not available on Veterans Expressway

Commuter Rail

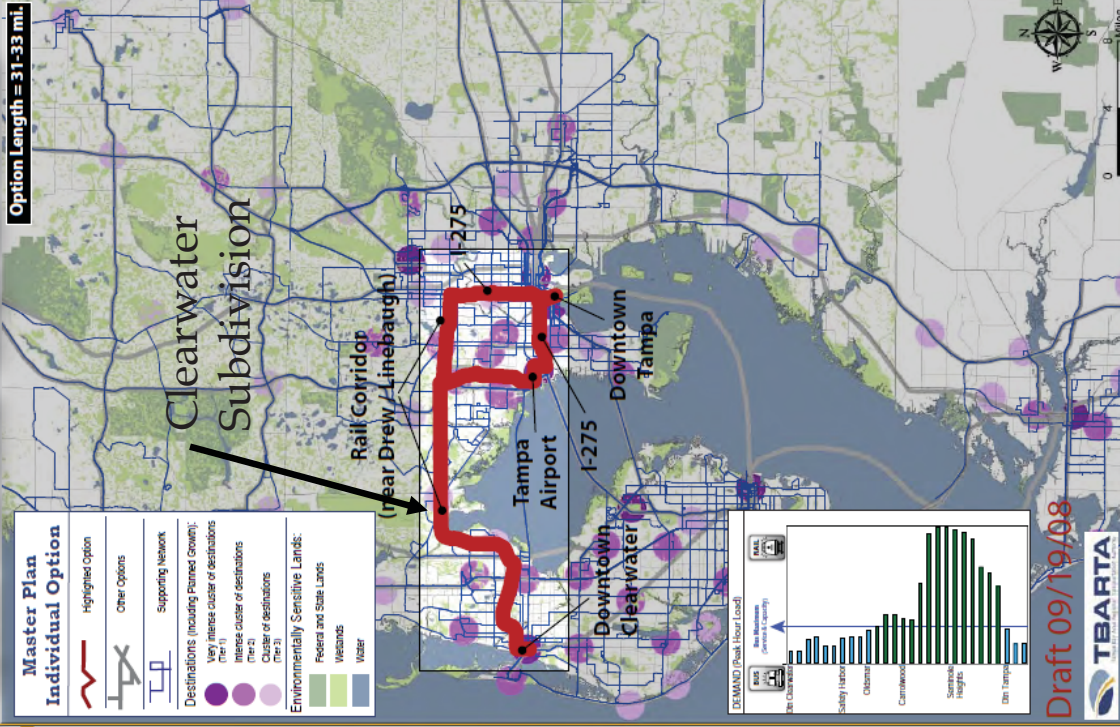
- Commuter rail on entire length
- Commuter service: Every 30 min.
- Stations spaced between 5 and 10 miles apart
- Park-and-Ride facilities

- Typical Trip Length is 20-25 mi.
- Crosses 2 Counties
- Connects to 7-9 Intermodal Facilities
- Connects to 14-16 Destination Clusters

- Demand exceeds capacity south of Busch Blvd and east of Westshore.
- Station spacing and frequency needed may exceed commuter rail characteristics.

- Freight rail traffic is low on Busch/Linebaugh line and Anderson line; shared use may be feasible.
- New interchange structures and grade separate crossings of major roads.
- Note corridor through TIA cannot accommodate typical commuter rail vehicles

Option: Clearwater - Tampa Rail Corridor



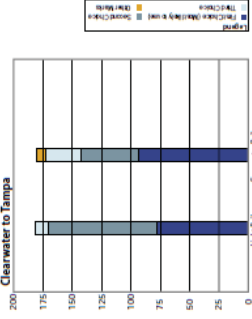
RECOMMENDATION

Light Rail



PUBLIC COMMENT

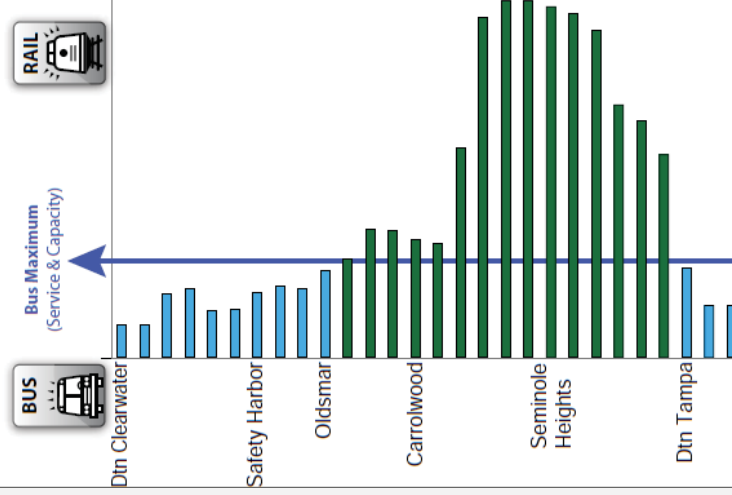
Clearwater to Tampa



DISCUSSION POINTS

- 1) Overlapping service w/ commuter rail on US 41 and express bus on Veterans, to be resolved in network development.
- 2) Demand is lower west of Race Track Rd., may want to consider combined commuter rail alt.
- 3) Coordinate with Tampa Airport regarding corridor through property.

DEMAND (Peak Hour Load)



RECOMMENDATION

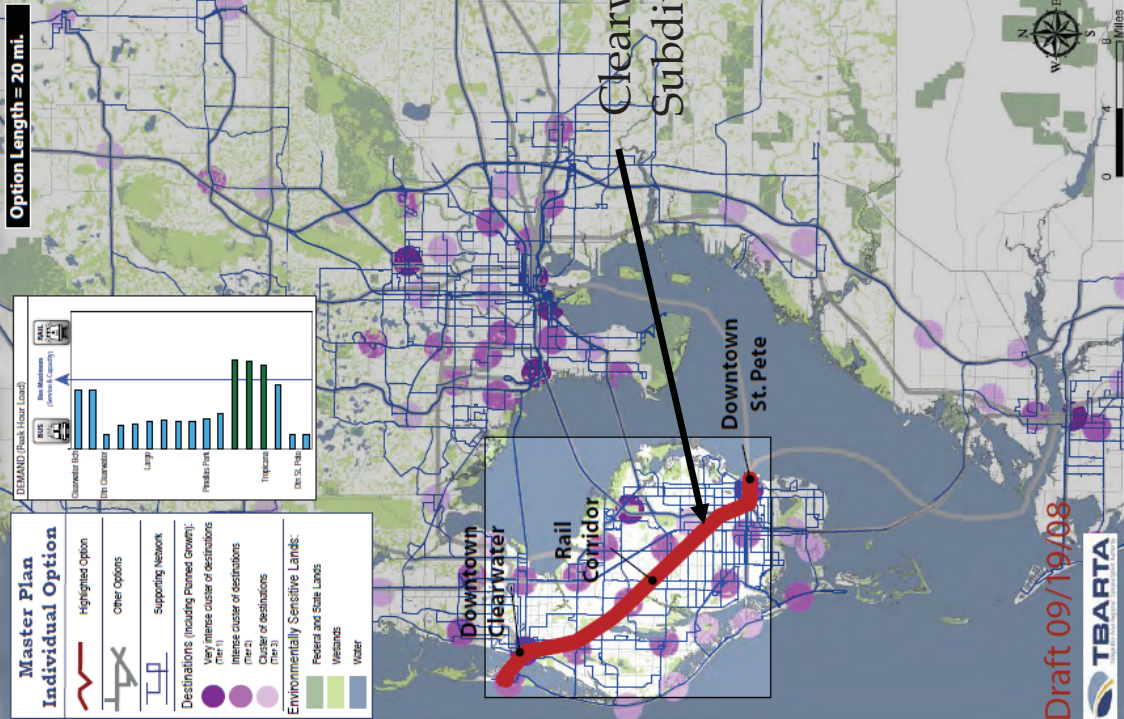
Light Rail



DISCUSSION POINTS

- 1) Overlapping service w/ commuter rail on US 41 and express bus on Veterans, to be resolved in network development.
- 2) Demand is lower west of Race Track Rd., may want to consider combined commuter rail alt.
- 3) Coordinate with Tampa Airport regarding corridor through property.

Option: Clearwater - St. Pete Rail Corridor



IMPROVEMENT TYPES	Does the Option Serve Regional Trips?	Does the Improvement Match the Potential Demand?	Level of Engineering or Financial Constraints?
Light Rail <ul style="list-style-type: none"> - Light rail on entire length - Frequent service: every 10 min. - Frequent stops: stations spaced between 0.5 and 1.0 miles apart 	YES <ul style="list-style-type: none"> - Typical Trip Length is 15 mi. - Crosses 1 County - Connects to 2 Intermodal Facilities - Connects to 8 Destination Clusters 	NO <ul style="list-style-type: none"> - Light rail capacity is greater than demand (See demand insert to left) 	HIGH <ul style="list-style-type: none"> - Existing freight rail corridor ROW is limited S of 52nd Ave N & may have to be acquired - New crossing of Clearwater Harbor needed & grade-sep. crossings of many major roads needed
Commuter Rail or Busway <ul style="list-style-type: none"> - Commuter rail or bus on entire length - Commuter service: every 30 min. - Less frequent stops: stations spaced between 5 and 10 miles apart 	YES <ul style="list-style-type: none"> - Typical Trip Length is 15 mi. - Crosses 1 County - Connects to 2 Intermodal Facilities - Connects to 8 Destination Clusters 	YES <ul style="list-style-type: none"> - Low frequency commuter rail meets demand (Deisel Multiple Unit) (See demand insert to left) 	MEDIUM <ul style="list-style-type: none"> - Freight rail traffic is low and shared use of the track may be feasible - If busway is developed, additional ROW may be needed south of 52nd Ave N - Feasibility of Clearwater Harbor crossing

Clearwater Subdivision

RECOMMENDATION

Commuter Rail or Busway

E-412

PUBLIC COMMENT

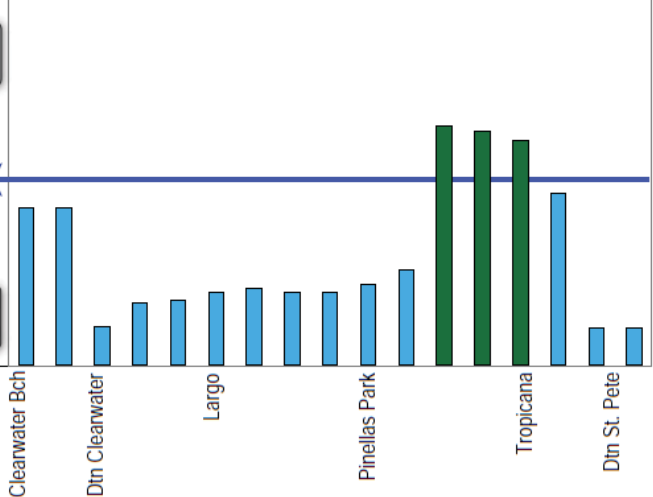
Clearwater to St. Pete

Mode	Express Bus	Light Rail	Commuter Rail
Clearwater to St. Pete	~10	~40	~50

DISCUSSION POINTS

- 1) Requires negotiations with CSX.
- 2) Consistency with Bluff to Beach transit project.

DEMAND (Peak Hour Load)



RECOMMENDATION

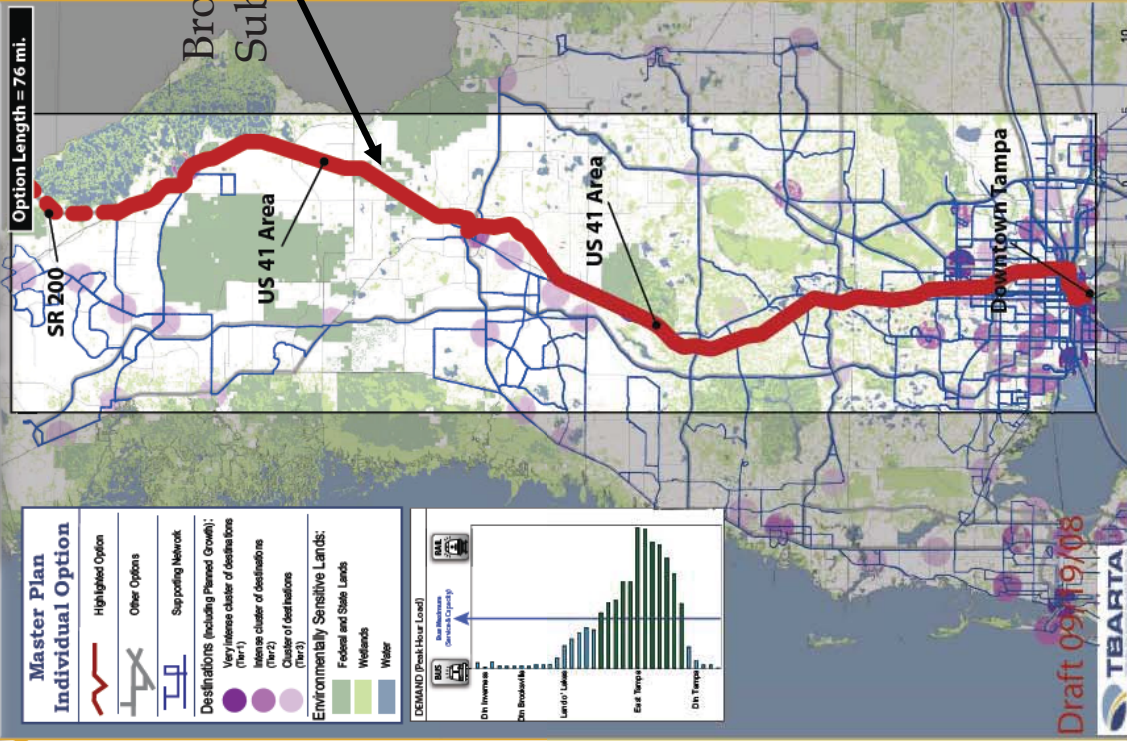
Commuter Rail or Busway



DISCUSSION POINTS

- 1) Requires negotiations with CSX.
- 2) Consistency with Bluff to Beach transit project.

Option: US 41 Area



IMPROVEMENT TYPES	Does the Option Serve Regional Trips?	Does the Improvement Match the Potential Demand?	Level of Engineering or Financial Constraints?
-------------------	---------------------------------------	--	--

Light Rail

- Rail on entire length
- Frequent service: Every 10 min.
- Stations spaced between 1/2 and 1 miles apart

YES

- Typical Trip Length is 30 mi.
- Crosses 3 Counties
- Connects to 12 Intermodal Facilities
- Connects to 13 Destination Clusters

NO

- Rail capacity is greater than demand north of SR 54. (See demand insert to left)

HIGH

- Existing freight rail corridor converted to ped/bike trail north of Brooksville.
- ROW is limited along US 41 N of Brooksville
- Unless FRA compliant, requires separation from freight

Brooksville Subdivision

Commuter Rail to Brooksville plus Bus Rapid Transit with Traffic Priority north of Brooksville

- Express Bus on entire length
- Commuter service: Every 30 min.
- Stations spaced between 5 and 10 miles apart
- Park-and-Ride facilities
- Bus rapid transit on US 41 north of Brooksville

YES

- Typical Trip Length is 30 mi.
- Crosses 3 Counties
- Connects to 12 Intermodal Facilities
- Connects to 13 Destination Clusters

PARTIALLY

- Commuter rail capacity is greater than demand north of SR 54. (See demand insert to left)

LOW

- Freight rail traffic is very low from Brooksville to Busch Blvd. & sharing of the corridor may be possible.
- Requires intersection improvements for priority treatments north of Brooksville

Light Rail plus Bus Rapid Transit with Traffic Priority

- Light rail south of SR 54. (Frequent Service)
- Bus rapid transit with traffic priority on US 41 north of SR 54. (Commuter Service)

YES

- Typical Trip Length is 30 mi.
- Crosses 3 Counties
- Connects to 12 Intermodal Facilities
- Connects to 13 Destination Clusters

YES

- Varying improvement types satisfy potential demand on entire length.

HIGH

- Grade-separated crossings of several major roads may be needed
- Unless FRA compliant requires separation from freight

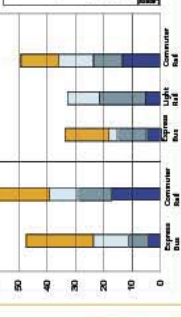
RECOMMENDATION

Commuter Rail to Brooksville plus BRT with Traffic Priority North of Brooksville



PUBLIC COMMENT

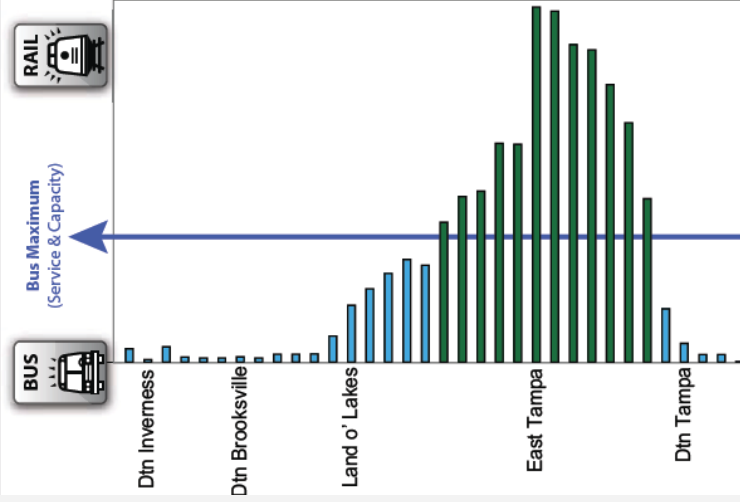
Passes to Hillsborough



DISCUSSION POINTS

- 1) Less frequent commuter service to meet demand in north Pasco and Hernando Counties.
- 2) Requires negotiations with CSX to either run on CSX tracks or in CSX right-of-way.

DEMAND (Peak Hour Load)



RECOMMENDATION

Commuter Rail to Brooksville plus BRT with Traffic Priority North of Brooksville



DISCUSSION POINTS

- 1) Less frequent commuter service to meet demand in north Pasco and Hernando Counties.
- 2) Requires negotiations with CSX to either run on CSX tracks or in CSX right-of-way.

Lessons Learned from 2010 Referendum



- Yes, you need something for everyone. But everyone doesn't want the same thing. Don't propose a rail segment to every suburb!
- The amount of the tax increase does matter to one in five opponents. That may be a majority-minority tipping point.
- Create a package for the major constituencies based on survey research. The majority of people who voted didn't attend meetings and had only marginal awareness that there was a Plan.
- There is a fraction of residents that won't support any proposal.

PlanHillsborough.org
/2035 Plan Post-Referendum Analysis

Focus group participants who were in favor of expanding mass transit were generally supportive of a more incremental approach.

Common perception that Tampa area is too sprawling, car-centric, and dispersed for mass transit to work.

Did view transit as a basic public service, even though they were largely unfamiliar with HART services.

... So, expand bus service... but also expand public info about bus service!

No clear, accurate understanding of rail modes, how they function, how to get to them– i.e., “If I did take a train or bus, how do I get where I really want to go?”

“Create familiarity” with rail with a demonstration line—but address high cost concerns.

Hillsborough Post-Referendum Study Cost-Reduction Strategies

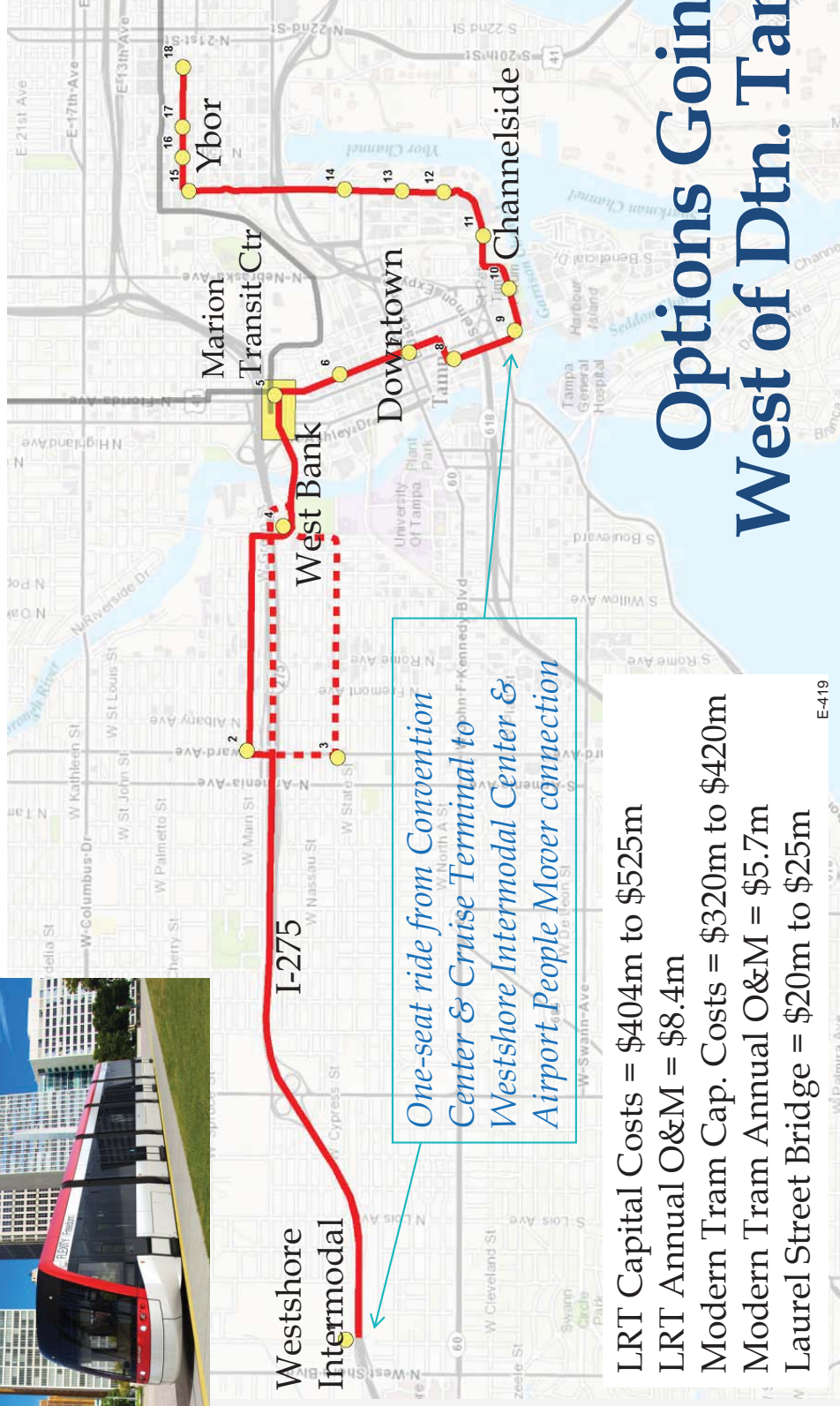
DMU rail pilot

- **Reuse existing freight rail corridor**
 - Requires agreement with CSX
 - Could be operational sooner than light rail
 - Connects two of Hillsborough's biggest employment centers but doesn't go to the airport!





DMU vehicles don't make sharp enough turns to run on the TECOLINE Streetcar track. BUT- Modern trams can run on TECOLINE track, and can navigate into I-275 median for quick ride to Westshore Intermodal. North & West lines can connect at MTC.



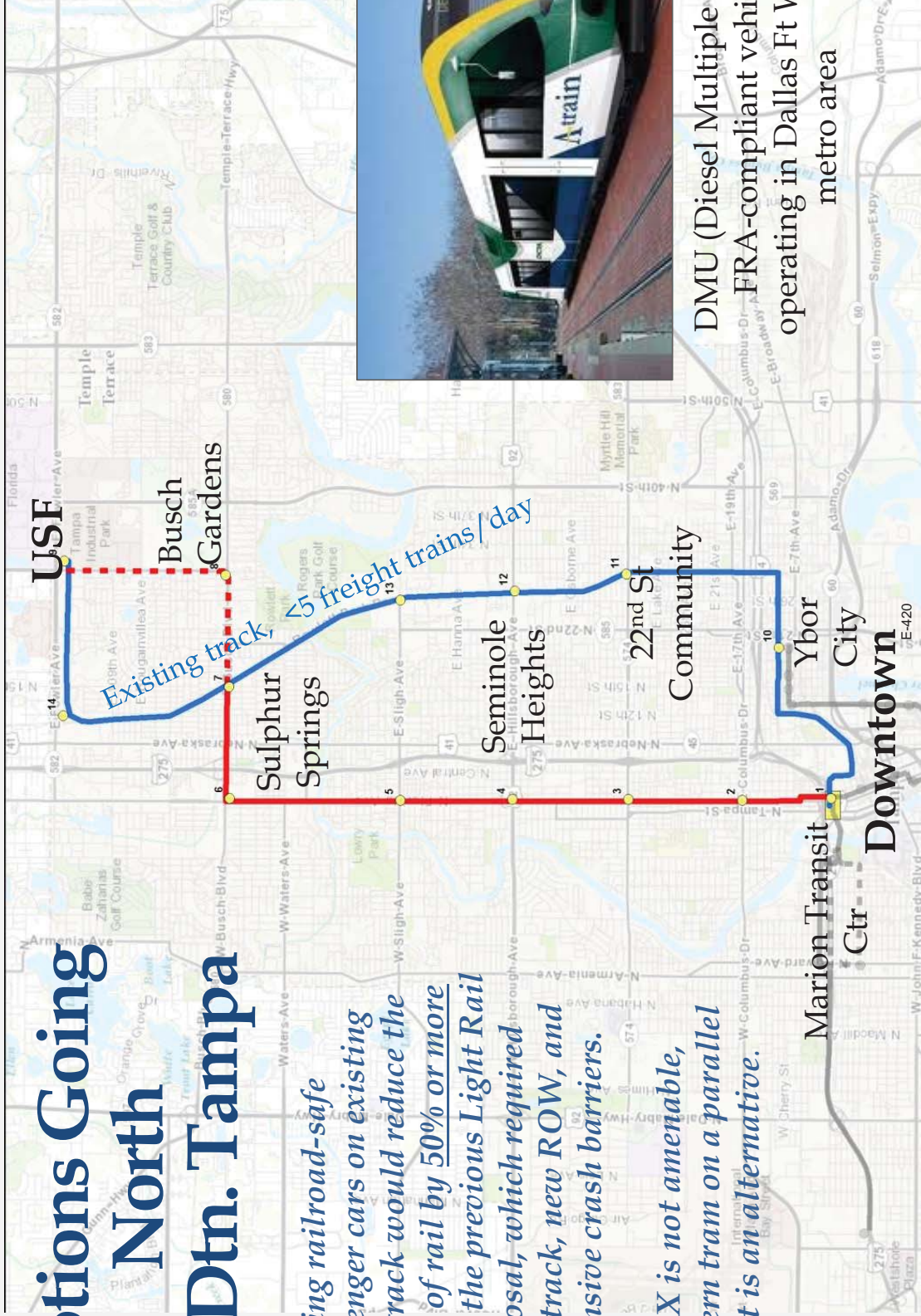
- LRT Capital Costs = \$404m to \$525m
- LRT Annual O&M = \$8.4m
- Modern Tram Cap. Costs = \$320m to \$420m
- Modern Tram Annual O&M = \$5.7m
- Laurel Street Bridge = \$20m to \$25m

Options Going West of Dtn. Tampa

Options Going North of Dtn. Tampa

Putting railroad-safe passenger cars on existing rail track would reduce the price of rail by 50% or more from the previous Light Rail proposal, which required new track, new ROW, and expensive crash barriers.

If CSX is not amenable, modern tram on a parallel street is an alternative.



DMU (Diesel Multiple Unit) FRA-compliant vehicle operating in Dallas Ft Worth metro area

Potential Regional Connections

Transit Assets and Opportunities Study

Building on Current Assets

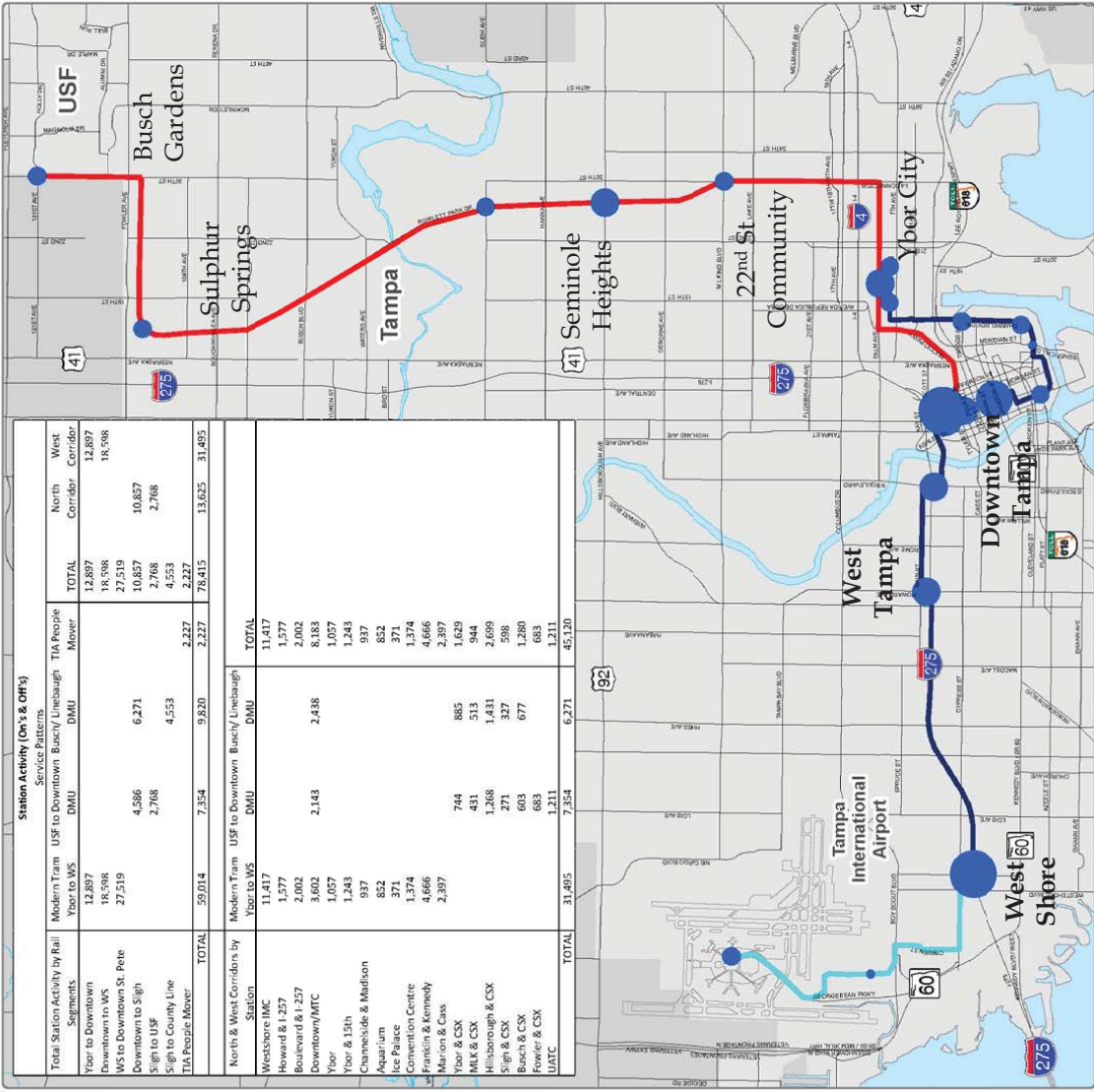
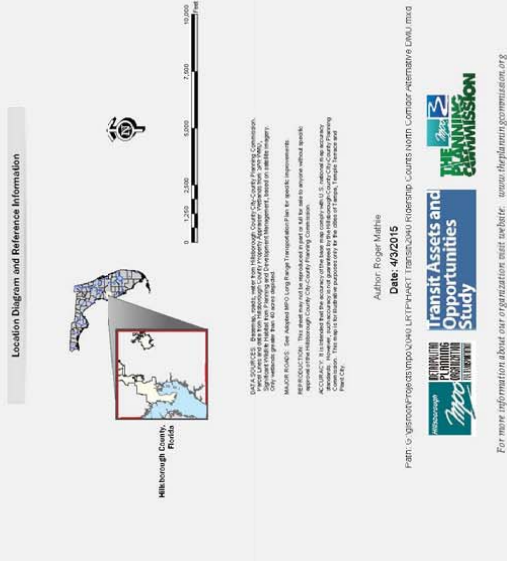
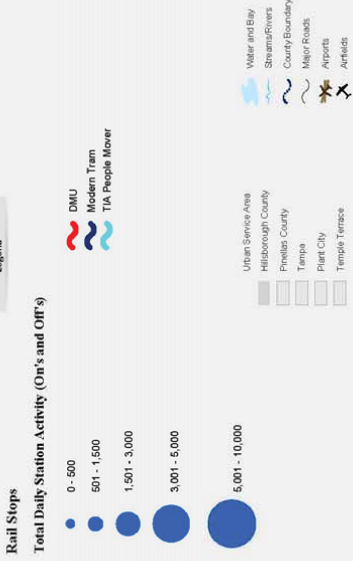
- Local System Utilizes Existing Streetcar Corridor, Existing CSX Rail Corridors, Airport People-Mover Extension, I-275 Right-of-Way, and The Marion Street Transitway

Future Connections Utilize

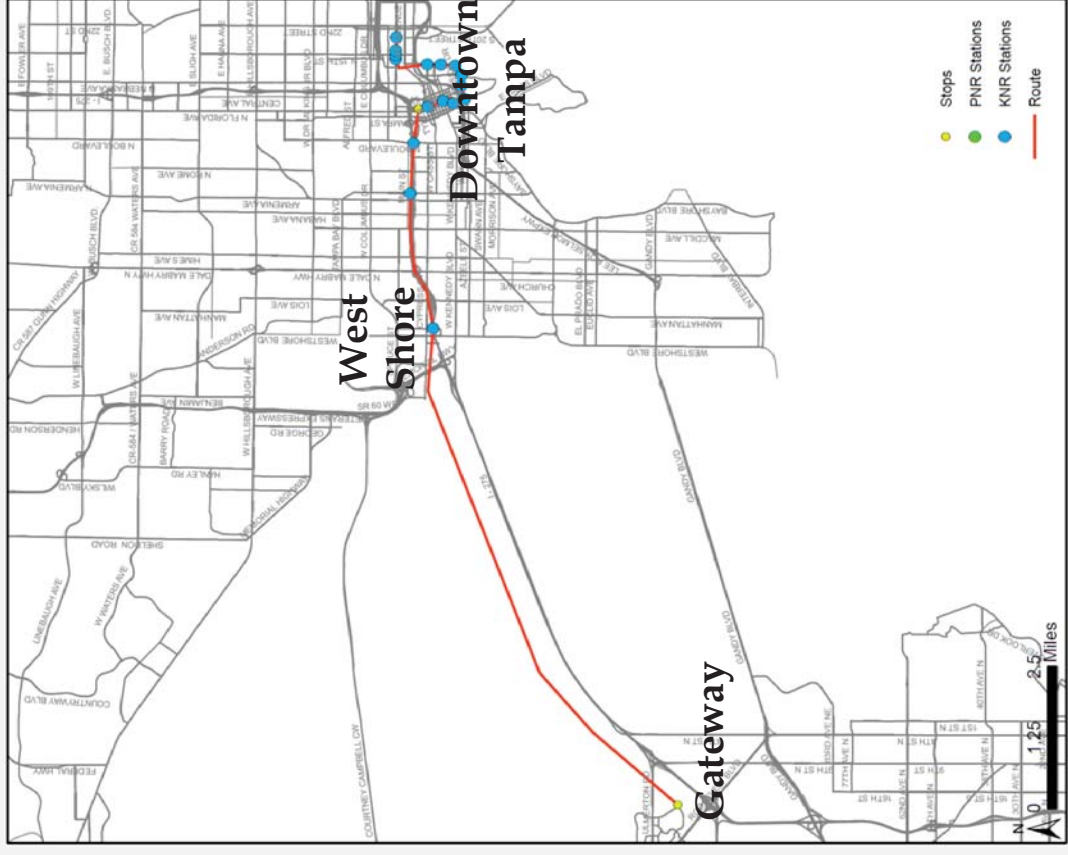
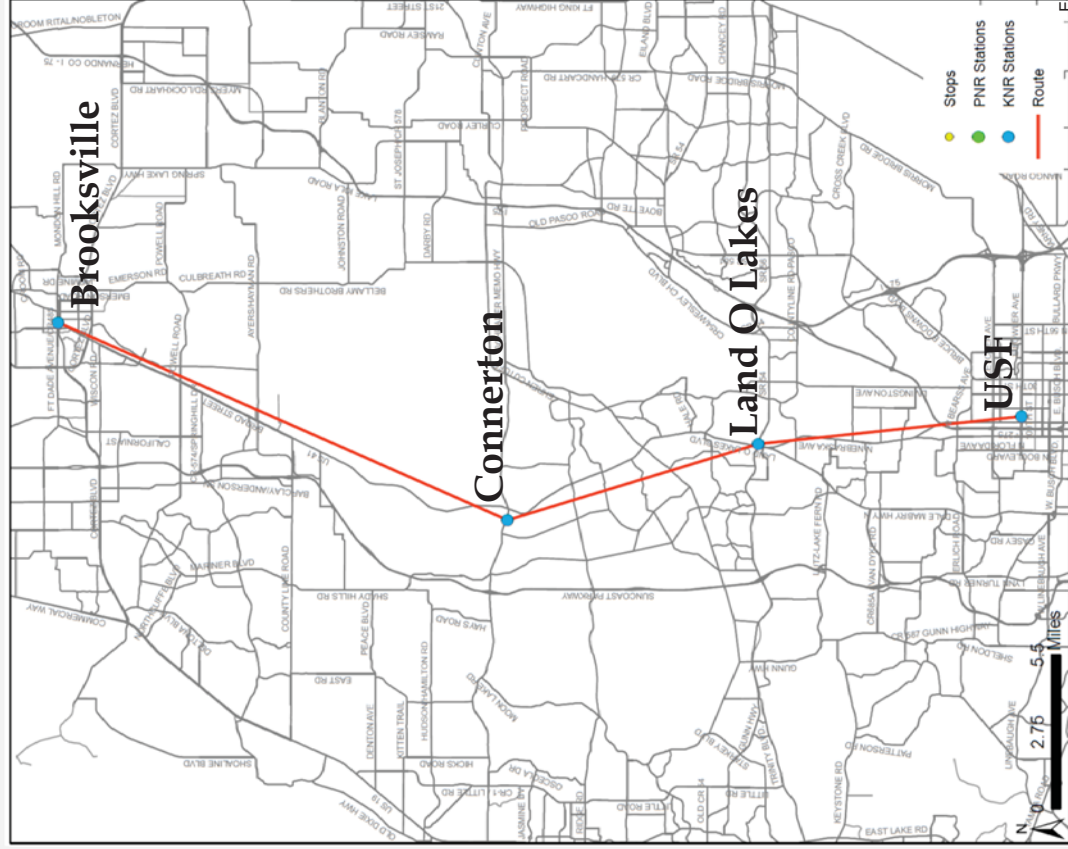
- Howard Frankland Bridge, I-4 Right-of-Way, and Existing CSX Rail Corridors



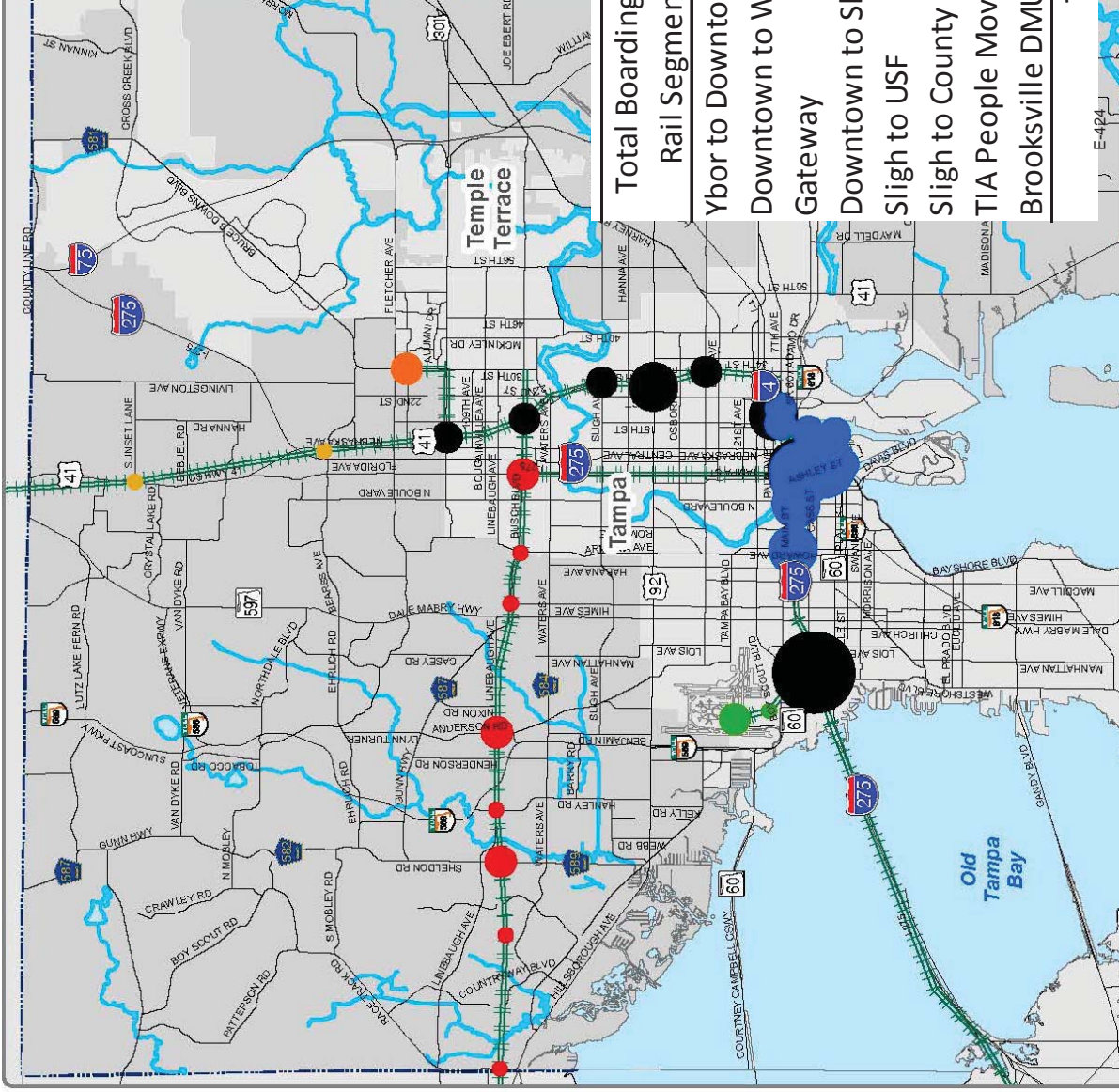
Transit Assets & Opportunities Study Supplement
2040 RIDERSHIP FORECAST
MODERN TRAM & DMU ALTERNATIVE



Tampa Bay Regional Planning Model: 2040 Plan Needs Assessment Ridership Forecasting

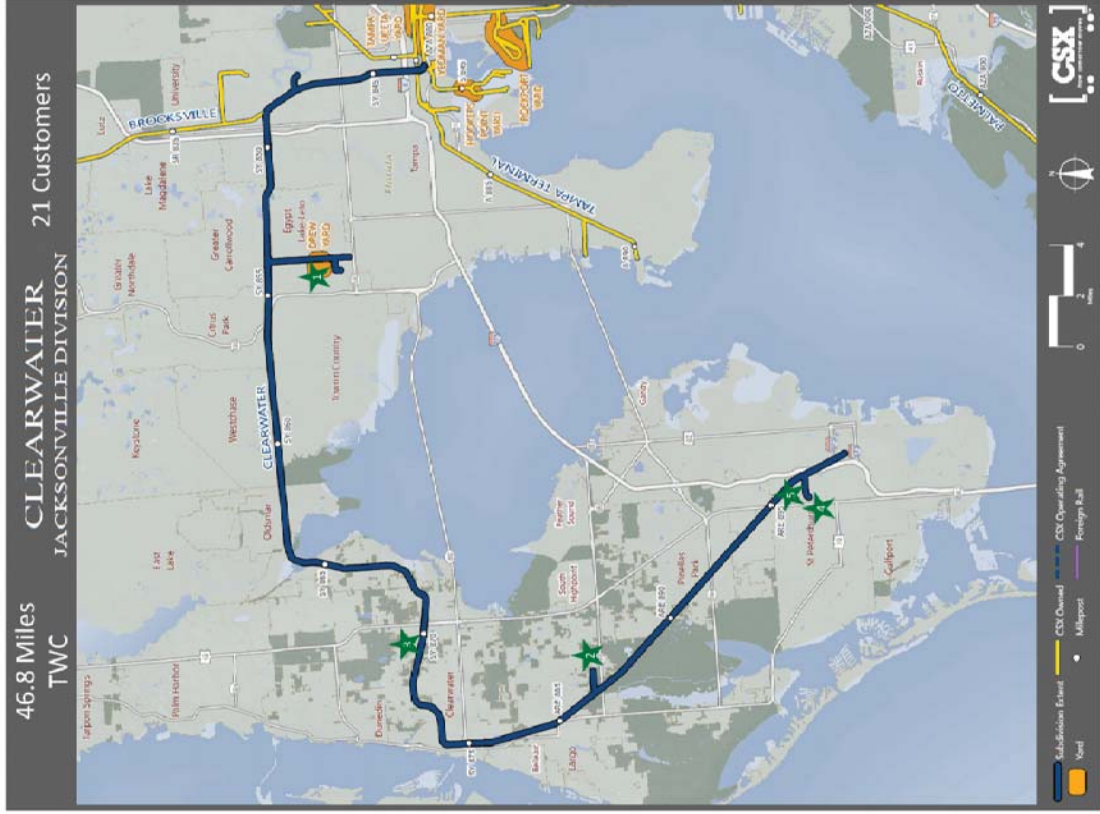


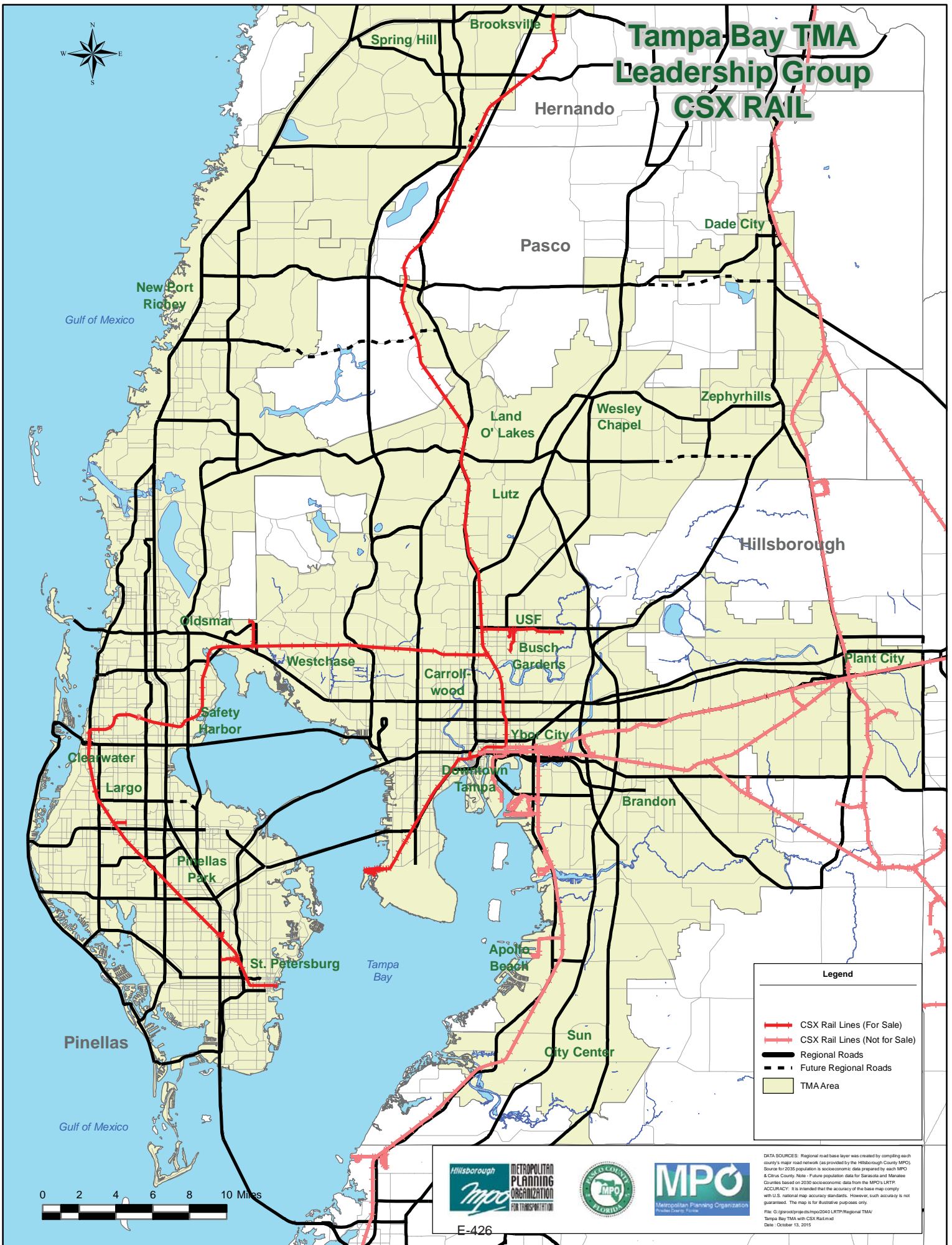
Tampa Bay Regional Planning Model: 2040 Plan Needs Assessment Ridership Forecasting



Total Boardings by Rail Segments	Brooksville DMU		Modern Streetcar		USF to Downtown		Busch/Linebaugh DMU	
	DMU	940	Streetcar	12,909	Downtown	4,893	Downtown	5,381
Ybor to Downtown				18,575				
Downtown to WS Gateway				6,572				
Downtown to Sligh						2,435		
Sligh to USF								4,066
Sligh to County Line								
TIA People Mover								
Brooksville DMU	940							
TOTAL	940	38,056	7,328	9,447				

Clearwater Sub





Tampa Bay TMA Leadership Group CSX RAIL

Legend

- CSX Rail Lines (For Sale)
- - - CSX Rail Lines (Not for Sale)
- Regional Roads
- - -** Future Regional Roads
- TMA Area



DATA SOURCES: Regional road base layer was created by compiling each county's major road network (as provided by the Hillsborough County MPO, Pinellas County MPO, and Manatee County MPO). Source for 2015 population is socioeconomic data prepared by each MPO & Census County Note - Future population data for Sarasota and Manatee Counties based on 2010 socioeconomic data from the MPO's LRTP. ACCURACY: It is intended that the accuracy of the base map comply with U.S. national map accuracy standards. However, such accuracy is not guaranteed. The map is for illustrative purposes only.

File: G:\gisroot\projects\mopo2010_LRTP\Regional TMA Tampa Bay TMA with CSX Rail.mxd
Date: October 13, 2015



Hillsborough Area Regional Transit Authority

1201 E. 7th Avenue • Tampa, Florida 33605
(813) 223-6831 • fax (813) 223-7976 • www.goHART.org

April 12, 2010

Mr. Steven A. Potter
Assistant Vice President – Network Planning
CSX Transportation - J315
500 Water Street
Jacksonville, FL 32259

Dear Mr. Potter:

Hillsborough Area Regional Transit (HART) serves the citizens of Hillsborough County, Florida by providing them with safe and affordable public transportation. We are planning for tomorrow with projects that will make transit travel in Hillsborough County easier and more convenient. HART is conducting an Alternatives Analysis for Light Rail Transit to address transportation needs in a study area that includes major destinations and activity centers in northern Hillsborough County. The Alternatives Analysis is a planning study to identify and evaluate alternative transit modes and potential alignments.

Among the alternatives we are seeking to evaluate are two that are proximate to segments of CSX Transportation's (CSXT) rights-of-way. Each of these alignments would originate near the intersection of Bruce B. Downs Boulevard and Skipper Road in northeast Hillsborough County. Both alignments follow routings into downtown Tampa onto Franklin Street. One of the alignments would follow CSXT's right-of-way from the Hillsborough Spur near the Brooksville Subdivision, through short segments of the Clearwater and Tampa Terminal Subdivisions by Tampa Union Station, and continue along the Port Tampa Spur to Franklin Street. The other route would follow CSXT's Clearwater Subdivision along Busch Boulevard between North Nebraska Avenue and North Florida Avenue.

We would like to explore possibilities with you and your staff to determine if these routings are feasible, and what CSX Transportation would require of HART to enable us to develop this project further. To that end, we propose a meeting at a mutually convenient date and time in Jacksonville, to provide you with a detailed overview of our proposal.

We understand that your cooperation with us on the development of this project would require some of the following considerations:

- HART and CSXT would agree to terms and conditions as set forth in a Memorandum of Understanding defining the scope and the tasks of a proposed project
- That HART would be required to compensate CSXT for time and materials expended by CSXT personnel.

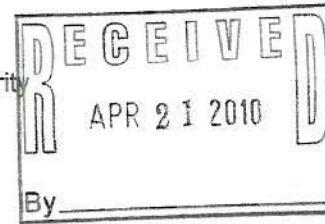
Celebrating 30 Years



500 Water Street - J315
Jacksonville, FL 32202
(904) 359-3205
Fax (904) 359-1373

April 20, 2010

Mr. David Armijo
Chief Executive Officer of the Hillsborough Area Regional Transit Authority
1201 E. 7th Avenue
Tampa, Florida 33605



Dear Mr. Armijo:

I have received your letter of April 12, 2010 and appreciate your efforts to evaluate alternative alignments for Hillsborough Area Regional Transit Light Rail. I also appreciate your early contact to my team. This will allow us to work cooperatively with your organization in offering our technical feedback and review.

We recognize the public benefit of passenger/commuter rail and where possible, we partner with state and local governments to help develop commuter systems. In fact, passenger/commuter systems operate on approximately 20% of our network. Where our freight operations allow such use, or can be changed to allow such use, we work very hard to accommodate communities.

Occupancy of CSX's freight right-of-way by passenger/commuter rail operations involves a number of challenges, including safety, operating capacity, compensation, and liability. CSX uses the following four principles in considering any passenger operations proposed on CSX's network as the basis of our assessment:

- Safety
Any planned system cannot jeopardize the safe operation of freight or passenger rail service in the corridor.
- Capacity
Passenger systems cannot consume capacity needed to serve existing customers or future growth.
- Compensation
Just as private land owners are fairly compensated for land acquisition needed for road construction, freight railroads must be compensated for the right-of-way and capacity consumed by passenger operations. Furthermore, freight railroads will not subsidize passenger operations through waived access payments, maintenance costs, dispatching, etc.
- Liability
Freight railroads cannot assume liability for risks that would not exist but for the presence of passenger operations.

In general, freight rail and light rail are incompatible. If CSX-owned corridors must be considered for light rail operations, the following minimum requirements must be factored into the planning analysis:

- Without a crash barrier with intrusion alarms, light rail tracks must be laterally separated by a minimum of 50 ft from the centerline of the nearest freight track
- When a crash barrier with intrusion alarms is present, the face of that barrier must have a minimum lateral separation of 25 ft from the center of the nearest freight track to the face of the crash barrier with an intrusion alarm and the LRT must be at least 8 ft beyond the crash barrier.
- All light rail – freight rail crossings must be grade separated with the light rail over the freight rail providing at least 25 ft vertical clearance above top-of-rail of the freight track.
- All LRT stations on CSX property must be equipped with pedestrian overpasses or underpasses.

These standards are needed to ensure the safe operation of both freight and passenger rail service.

CSX's freight rail network has long offered a competitive advantage to Tampa Bay and this state. That advantage will be magnified in the coming years as traffic grows and demands for safety and environmental benefits in transportation increase. Please contact Bob O'Malley (407-803-3969, Bob_O'Malley@csx.com) to schedule a convenient meeting time in Jacksonville, where we can discuss HART's alignment proposal. Again, we appreciate the commitment of your organization to providing safe and affordable travel, and we are pleased that our core freight operations bring so much to the economic vitality of this area. We look forward to our upcoming meeting.

Sincerely,



Steven A. Potter
AVP Network Planning and Joint Facilities

cc:
Bob O'Malley, CSX



Hillsborough Area Regional Transit Authority

1201 E. 7th Avenue • Tampa, Florida 33605
(813) 223-6831 • fax (813) 223-7976 • www.goHART.org

May 18, 2010

Bob O'Malley
Resident Vice President
State Government and Community Affairs
CSX Transportation
283 Cranes Roost Boulevard, Suite 111
Altamonte Springs, FL 38701-3437

Dear Mr. O'Malley,

We at Hillsborough Transit Authority (HART) greatly appreciate the opportunity to have met with Cheryl Boehm, Jake Hunter, and you on May 12, 2010. It is important for your organization to understand our ongoing Alternative Analysis (AA) and how our alternatives relate to your service and/or right-of-way. At the meeting, we provided a brief overview of the alternatives currently under consideration and left materials prepared from earlier studies. If you have any questions or need additional information, please contact us and we will gladly provide it.

I personally appreciated the opportunity for a candid discussion and to learn about CSX Transportation's requirements when assessing the public benefit of passenger rail systems to operate on your network.

I look forward to continued discussions specifically to the development of a Memorandum of Understanding. I understand this is the next step in our partnering process. We await your response.

Sincerely,



David Armijo
Chief Executive Officer

Cc: Cheryl Boehm, CSX Director Passenger Operations
Jake Hunter, CSX Operations Manager
Mary Shavalier, HART Chief of Strategic Planning & Program Development
Philip Hale, HART Chief of Maintenance

Celebrating 30 Years



500 Water Street - J315
Jacksonville, FL 32202
(904) 359-3205
Fax-(904) 359-1373

May 27, 2010

Mr. David Armijo
Chief Executive Officer of the Hillsborough Area Regional Transit Authority
1201 E. 7th Avenue
Tampa, Florida 33605

Dear Mr. Armijo:

Thank you for taking the time to meet with representatives of CSXT on May 12, 2010 to share information about HART's Alternative Analysis. I commend you for your efforts to provide mass transit solutions to the citizens of Hillsborough County. I regret that I was not able to attend the presentation but I appreciate your early contact with my team. By sharing information early in the process, we can help guide your efforts to avoid alternatives that are not feasible or are too costly.

Based on the materials you provided, it is my understanding that various modes of transportation, including light rail, are being considered in the planning study. Among these options under consideration, two alignments require use of CSXT's rights-of-way. As I stated in my previous letter to you on April 20, 2010, in general freight rail and light rail transit are incompatible. If CSXT-owned rail corridors are considered, CSXT's minimum standards must be factored into the planning analysis, as follows:

- Without a crash barrier with intrusion alarms, light rail tracks must be laterally separated by a minimum of 50 feet from the centerline of the nearest freight track
- When a crash barrier with intrusion alarms is present, the face of that barrier must have a minimum lateral separation of 25 feet from the center of the nearest freight track to the face of the crash barrier with an intrusion alarm and the LRT must be at least 8 ft beyond the crash barrier.
- All light rail – freight rail crossings must be grade separated with the light rail over the freight rail providing at least 25 feet vertical clearance above top-of-rail of the freight track.
- All LRT stations on CSXT property must be equipped with pedestrian overpasses or underpasses.

These standards are needed to ensure the safe operation of both freight and passenger rail service.

CSXT recognizes the public benefit that your project will provide the citizens of Hillsborough County and the importance of selecting the right alternative. As such, it is imperative to inform you early in the process that our internal preliminary review of HART's routing proposal over CSXT's corridor indicates that there is insufficient clearance within the majority of CSXT's right-

of-way to satisfy the minimum standards to accommodate safe operation of both freight and light rail operations. To ensure safe clearance, acquisition of private land adjacent to the CSXT corridor will be necessary, which will add significant cost to this particular alternative. Our recommendation would be for HART to pursue the publicly-owned I-275 alignment instead of CSXT's freight rail corridor. Much like the proposed Tampa-Orlando High Speed Rail system will be built on public right-of-way; the development of light rail transit on the I-275 corridor will be less expensive and will be completely under the control of public governance.

While I-275 is the most viable corridor, we recognize you may still need to study the CSX corridor for the purposes of the Alternatives Analysis. If this is the case, we recommend proceeding under the direction of the Florida Rail Enterprise. As you know, the Florida Legislature recently created the enterprise to coordinate publicly funded passenger rail operations in the state. The involvement of the enterprise also will provide a regional framework and comprehensive approach for the development of high speed rail and light rail transit throughout Tampa Bay. If you would like CSXT to participate in meetings with the Florida Rail Enterprise, we stand ready to do so.

Sincerely,



Steven A. Potter
AVP Network Planning and Joint Facilities

cc:
Bob O'Malley, CSX



Hillsborough Area Regional Transit Authority

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(813) 223-6831 • fax (813) 223-7976 • www.goHART.org

June 15, 2010

Mr. Steven A. Potter
Assistant Vice President – Network Planning
CSX Transportation - J315
500 Water Street
Jacksonville, FL 32202

Subject: Hillsborough Area Regional Transit Alternatives Analysis Study

Dear Mr. Potter:

Thank you for your May 27, 2010 letter regarding our Alternatives Analysis study. I am sorry that we did not have the opportunity to meet in person, but we found your team to be receptive and attentive to our proposal and challenges as we work towards enhancing public transit in Hillsborough County.

I appreciate you taking the time to delineate the specific requirements that must be met in order to share CSX's corridors with your freight train operations. Each requirement is clearly understood. Our planning to date accounts for a 25 foot vertical separation of all rail crossings for transit and pedestrian movements. Likewise, we are evaluating your lateral separation requirements of 50 feet without a crash barrier and 25 feet with a crash barrier. We agree that intrusion alarms would serve to protect the users and passengers of both operations. We share CSX's strong emphasis on safety for employees, passengers, and neighbors for our respective operations.

That there is insufficient right of way to preserve CSX's current operation and accommodate LRT is not a surprise. To that end, HART does not rule out the possibility of obtaining sufficient adjacent property to meet this requirement. As part of this Alternatives Analysis, HART will evaluate potential alternatives to better understand the issues and impacts.

If an acceptable solution cannot be achieved through adjacent property acquisition, HART is interested in exploring the possibility of acquiring both the Clearwater and Brooksville subdivisions from CSX. HART would contract freight operations to a mutually acceptable third party freight operator to preserve existing and potential new traffic for CSX's operation while simultaneously enabling HART to develop its mass transit system. This solution could preserve CSX's line haul traffic while mitigating its potential liability exposure and minimizing its operating expense.

HART will commit to keeping the Florida Rail Enterprise aware of our planning efforts and endeavors.


Celebrating **30** *Years*

Mr. Steven A. Potter
CSX Transportation
Page ~2~

As we work toward completing our Alternatives Analysis study, we are continuing to evaluate the remaining alternatives, including the Interstate 275 Alternative. You indicated in your letter that CSX would be willing to work with HART as we continue our analysis. We are very interested in continuing to work with CSX for the mutual benefit of both organizations. To that end, we invite you to provide a draft Memo of Understanding for our review in the hope that we may commence a thorough examination of each of the remaining viable alternatives.

Our team looks forward to working with yours on what we will strive to make a beneficial partnership for both of our organizations. I will call you later this week to arrange a date and time for a meeting.

Sincerely,



David J. Arnijo
Chief Executive Officer
(813) 223-6831 x1701

cc: Bob O'Malley, CSX Transportation
Fred Wise, Florida Department of Transportation
Kevin Thibault, Florida Rail Enterprise

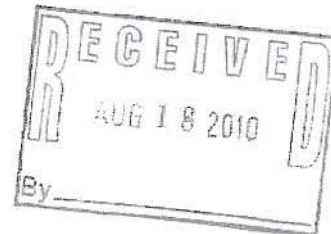


Bob O'Malley
Resident VP Florida
State Government and Community Affairs

283 Cranes Roost Boulevard, Suite 111
Altamonte Springs, FL 32701
Tel. 407-215-0457
Cell. 407-803-3969

August 13, 2010

Mr. David Armijo
Chief Executive Officer
Hillsborough Area Regional Transit Authority
1201 E. 7th Avenue
Tampa, Florida 33605



Dear Mr. Armijo:

Thank you for the update you and your team provided earlier this week (August 10, 2010) to CSXT regarding HART's Alternatives Analysis. The conference call was very productive. It was helpful to hear the progress of your efforts to develop a passenger rail system in Hillsborough County. I hope our input during the call, combined with the technical feedback we have provided in past meetings and letters, helps you select a feasible alignment.

CSXT knows the importance of transportation. Our freight rail network provides a competitive advantage to the Port of Tampa and businesses in the region. We make significant investments in our rail infrastructure to maintain that competitive advantage and to connect our customers in Tampa Bay to markets throughout the United States and the world. Rail provides a safe, efficient and environmentally-friendly means for our customers to move freight. We recognize the similar benefits rail provides for commuters, so we understand your interest in passenger rail transportation solutions.

We appreciate the opportunity to be involved in the Alternatives Analysis. By sharing our expertise early in the planning process, we can help you avoid alternatives that are incompatible with the freight needs of the Tampa Bay region. As we discussed on yesterday's call and in previous meetings, the following minimum safety standards must be factored into the analysis.

- Without a crash barrier with intrusion alarms, light rail tracks must be laterally separated by a minimum of 50 feet from the centerline of the nearest freight track
- When a crash barrier with intrusion alarms is present, the face of that barrier must have a minimum lateral separation of 25 feet from the center of the nearest freight track to the face of the crash barrier with an intrusion alarm and the LRT must be at least 8 ft beyond the crash barrier.

- At-grade crossings are not permitted. All light rail-freight rail crossings must be grade separated with the light rail over the freight rail providing at least 25 feet vertical clearance above top-of-rail of the freight track.
- All LRT stations on CSXT property must be equipped with pedestrian overpasses or underpasses.
- Temporal separation of light rail transit and freight rail is not an option. At no time may light rail vehicles operate on freight lines or within the lateral separation requirements listed above.

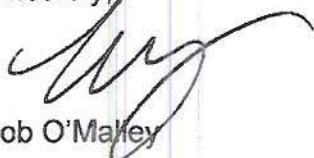
These standards are needed to ensure the safe operation of both freight and passenger rail service.

On the August 10th call, two new issues were discussed: (1) use of our A Line and (2) acquisition of the Brooksville and Clearwater subdivisions. To reiterate our position, the A Line is not an option for light rail transit. This important section of our main line serves the Port of Tampa and is vital to Tampa Bay's economy. Light rail transit cannot safely operate on this corridor and would have severe adverse impacts on freight movement in the region and the companies that ship and/or receive that freight.

As for acquisition, although we are not marketing these rail lines for sale, we recognize the appeal of these corridors and we are open to reviewing any proposals. As we stated on the call however, any discussion of acquisition must be done on a regional level. As you know, the Brooksville subdivision extends into Pasco and Hernando Counties, and the Clearwater subdivision extends into Pinellas County. We will not sell a Hillsborough-only portion of either subdivision.

We commend you for your leadership in providing transportation solutions to the citizens of Hillsborough County. As you move forward, please let me know if you would like a CSXT representative to attend future Board meetings or public hearings to answer questions. We stand ready to help you select the right alignments needed for a safe and effective passenger rail system.

Sincerely,



Bob O'Malley

cc: Steve Potter, CSX Transportation
Kevin Thibault, Florida Rail Enterprise
Don Skelton, FDOT District 7



Hillsborough Area Regional Transit Authority

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September 10, 2010

Bob O'Malley
Resident Vice President
State Government and Community Affairs
CSX Transportation
283 Cranes Roost Boulevard, Suite 111
Altamonte Springs, Florida 38701-3437

SUBJECT: Update on HART's Alternatives Analysis Study

Dear Mr. O'Malley:

Thank you for your letter dated August 13, 2010 regarding a potential purchase of the CSXT Brooksville and Clearwater subdivisions and the A Line.

You indicated that these subdivisions must be purchased whole and not subdivided. To this end, I requested Ramond Chiaramonte, Executive Director of the Hillsborough County Metropolitan Planning Organization to convene a meeting with Hillsborough County stakeholders. This meeting was held on August 31 and included FDOT District 7 Secretary Don Skelton, TBARTA Executive Director Bob Clifford, Mr. Chiaramonte, Lucia Garsys Hillsborough County Strategic Management Initiatives Officer, Ben Money, City of Tampa Chief of Planning and Project Management, Mary Shavalier, HART Chief of Planning & Program Development and myself.

Acknowledging that most of the Brooksville and Clearwater subdivisions are outside of Hillsborough County (see map on Page 2) the group determined that further discussion regarding potential purchase of these subdivisions would be handled by FDOT and TBARTA.

HART is still interested in discussing use of the A Line since this corridor including the extension south to MacDill AFB is within the service plans of HART. For the purpose of the Alternatives Analysis study, it will not be possible to include a purchase option, but we would like to continue discussions for a right-of-way lease option.

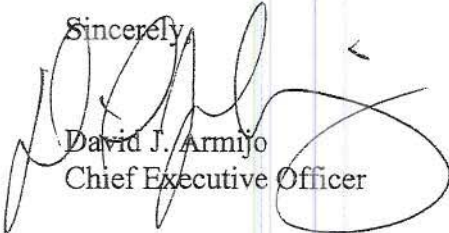
On a separate note, HART's Alternatives Analysis Study continues to move forward. In November, the HART Board of Directors will choose the Locally Preferred Alternative, which is essentially the final routing and type of service mode (light rail, bus rapid transit, regular bus improvements) that we will then take to the Federal Transit Administration for our capital funding request. The Board's choice will be based on the final modeling and community outreach stages of the Alternatives Analysis which are underway.

Celebrating 30 Years

Letter to Bob O'Malley
Update on HART's Alternatives Analysis Study
September 10, 2010
Page 2

I appreciate your involvement in the Alternatives Analysis process and contributing your expertise early in the planning process, and I look forward to our ongoing discussions.

Sincerely,

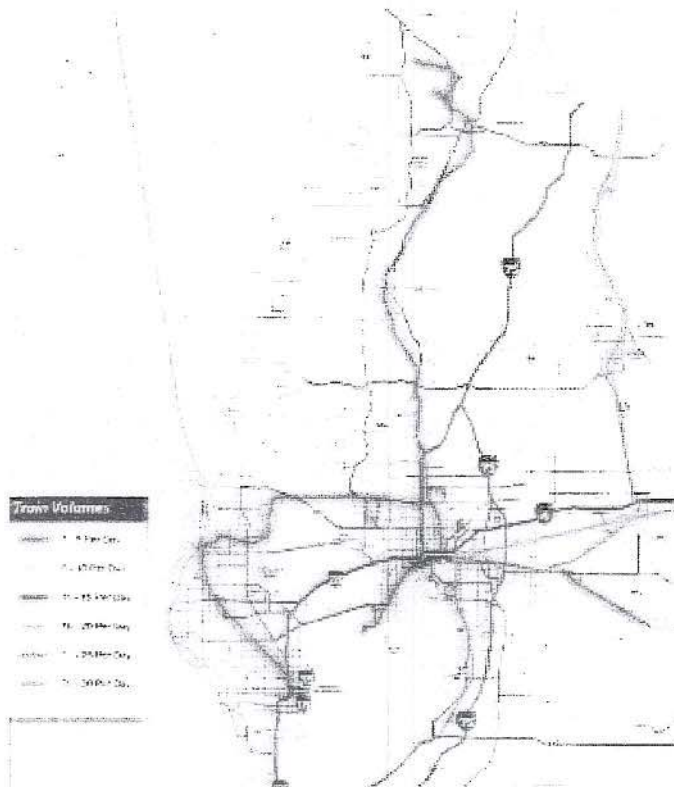


David J. Armijo
Chief Executive Officer

cc: Steve Potter, CSXT Assistant Vice President Network Planning & Joint Facilities
Don Skelton, Florida Department of Transportation District 7 Secretary
Bob Clifford, TBARTA Executive Director
Ramond Chiamonte, Executive Director, Hillsborough County MPO
Lucia Garsys, Hillsborough County Strategic Management Initiatives Officer
Ben Money, City of Tampa Chief of Planning and Project Management
Mary Shavaliier HART Chief of Planning & Program Development

**CSXT Rail
Subdivisions**

Brooksville
Clearwater
Subdivisions and
A Line shown in
red





Hillsborough Area Regional Transit Authority

1201 E. 7th Avenue • Tampa, Florida 33605
(813) 223-6831 • fax (813) 223-7976 • www.goHART.org

DATE: SEPTEMBER 13, 2010
TO: HART BOARD OF DIRECTORS
FROM: DAVID J. ARMIJO, CHIEF ADMINISTRATIVE OFFICER
SUBJECT: COMMUNICATIONS WITH CSX TRANSPORTATION

As HART continues its communication with CSX as part of the Alternatives Analysis Study, these efforts have proved to be very important as CSX has contributed information and expertise that is vital to the planning process.

In response to a prior communication from CSX regarding the potential to purchase the CSX Brooksville and Clearwater subdivisions and the A Line several Hillsborough County stakeholders met on August 31 to discuss this possibility.

The situation is thus: CSX has indicated that these subdivisions must be purchased whole and not subdivided. Most of the Brooksville and Clearwater subdivisions are outside of Hillsborough County, therefore the group determined that further discussion regarding potential purchase of these subdivisions would be handled by FDOT and TBARTA. The A Line is the only corridor that falls within HART's service plans.

The attached letter to CSX summarizes the August 31 meeting and communicates HART's continued interest in discussing use of the A Line, yet for purposes of the Alternatives Analysis study, it will not be possible to include a purchase option, but we would like to continue discussions for a right-of-way lease option.

Attachments:

1. Letter to B. O'Malley of CSX dated September 10, 2010
2. Letter from B. O'Malley of CSX dated August 13, 2010



City of Tampa
Community Aesthetic Feature Request
Concept Report

Fowler Ave at I-275 and
Busch Boulevard at I-275
Phase I only
Hillsborough County

Prepared by
Atkins North America on behalf of IP
4030 W. Boy Scout Boulevard, Suite 700,
Tampa, FL 33607

August 2017

Revised Concept Submittal

Landscape Architect of Record
Harry Belton, RLA
RLA No. 900
Date: August 31, 2017

Table of Contents

Background	3
Community Aesthetic Feature Category.....	3
Conceptual Drawings	3
Site Plans	3
Design Speed.....	4
Governing Standards, Criteria and Building Code.....	4
Submittal Approval Letters (Exhibit 23-A)	4
Design Narrative	4-5

Conceptual Drawings

IP District Limits Map	6
Existing Site Photos and Aerial Map Fowler Avenue	7
Existing Site Photos and Aerial Map Busch Boulevard	8
Fowler Avenue Proposed Phasing Sketches	9
Busch Boulevard Proposed Phasing Sketches.....	10
Site Plans with Dimensions	11-12
Under Bridge Lighting Details	13-15
Pedestrian Lighting Details	16
Crosswalk Details	17

Appendices

Appendix A: Submittal Approval letters (Exhibit 23-A)	18-19
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Background

City of Tampa has collaborated with both the IP and the Hillsborough Metropolitan Planning Organization (MPO) to pursue a Community Aesthetic Features (CAF) Permit to create a gateway at the interchanges of I-275 at Busch Blvd and I-275 at Fowler Avenue. These interchanges mark the western boundary of the IP District; its geographical boundaries are Busch Boulevard to the south, Bearss Avenue to the north, and interstates 75 and 275 to the east and west, including parts of the City of Tampa, Temple Terrace and unincorporated Hillsborough County.

An initial design for each interchange was developed during four steering committee meetings that took place between November 2015 and May 2016. Comments were recorded and analyzed to effectively scale back the design to meet maintenance needs and the desired construction budget. The updated designs were presented to the steering committee during early 2017 and then to the public on April 12, 2017. The public meeting was held at the same time and location as the Fowler Avenue Streetscape meeting held by Sam Schwartz Engineering. Comments received in early 2017 directed the final designs, which were presented to the County Commission, the MPO Committee, the MPO Policy Committee, and the IP advisory Committee in May 2017. The designs were most recently presented to the IP Executive Board meeting on June 2, 2017.

This concept CAF presents the Phase I Concept, which is funded for construction within 1 year of Final CAF approval. Phase II & III is presented in a separate CAF application, which is anticipated for construction within 5-7 years.

1. Location 1 – Fowler Ave. I-275

Interchange medians on all four quadrants of the interchange and under the bridge.

- a. Under Bridge Light Emitting Diode (LED) Lighting
- b. Pedestrian Lighting
- c. Enhanced Crosswalks

2. Location 2 – Busch Blvd. and I-275

Interchange medians on off ramp loops and under the bridge.

- a. Under Bridge LED Lighting
- b. Pedestrian Lighting
- c. Enhanced Crosswalks

Community Aesthetic Feature Category

Local ID Markers, stand alone and affixed

Conceptual Drawings

See page 6-10

Site Plans and Details

See pages 11-17

Design Speed

1. Location 1 – Fowler Ave. at I-275 Interchange
 - a. Posted Speed Fowler Ave.: **40 mph**
 - b. Design Speed Fowler Ave.: **45 mph**
2. Location 2 – Busch Blvd. at I-275 Interchange
 - a. Posted Speed Busch Blvd.: **40 mph**
 - b. Design Speed Busch Blvd.: **45 mph**

Governing Design Standards, Criteria, and Building Code

Florida Department of Transportation (FDOT), 2017 Design Standards and revised index drawings as appended herein and 2017 Standard Specifications for Road and Bridge Construction as amended by contract documents.

FL Building Code, 5th Edition (2014), Section 16 Wind Load (150 MPH), ASCE 7-10, Load Case = 0.6W+D

The ID Markers shall meet all applicable local state and federal building codes and design criteria.

The ID Markers shall not obstruct signs, sight distance, sight triangles or view zones.

The ID Markers shall have no adverse environmental impacts.

Submittal Approval Letters (Exhibit 23-A)

Please see Appendix A

Design Narrative

The overall design is divided into three phases to accommodate the budget. This CAF application only covers the first phase. The City of Tampa has reviewed this concept CAF and has agreed to maintain design elements.

Phase I consists of under bridge lighting, pedestrian lighting, and enhanced crosswalks. All plan view graphics are oriented with North up, unless otherwise noted.

Under bridge lighting consists solely of LED downlights by Philips Kinetic (or similar) that apply a solid color wash on the existing columns. Lighting fixtures will be attached to piers; no lighting will be attached to superstructure or beams. Lights will show one color at a time, but will be programmed (via a data line and control box) to fade slowly into a new color every 2 hours. In Phase II, column art consisting of simple patterns or images made with high contrasting colors will coordinate and change under the wash of colored light. The column art will “carry the aesthetic burden” since the columns are not architecturally significant. Colors frequently associated with traffic control devices (primary red, yellow, green, disabled parking blue, bike lane green, MOT orange, etc) are to be avoided in large quantities and no lights will flash in quick succession. In addition, elements themselves will not look like traffic control devices. Only white light will be used to illuminate street signs because colored lighting will change the color of these signs. However, while white light is preferred, the use of white light by itself will not provide the desired aesthetic visual impact to the under bridge space. Colors to be used to illuminate the columns may include purple, blue, pink, or orange and will coordinate with future column art in Phase II. Colors will not be changed to commemorate the hundreds of holidays or “causes” throughout the year. Nighttime lighting

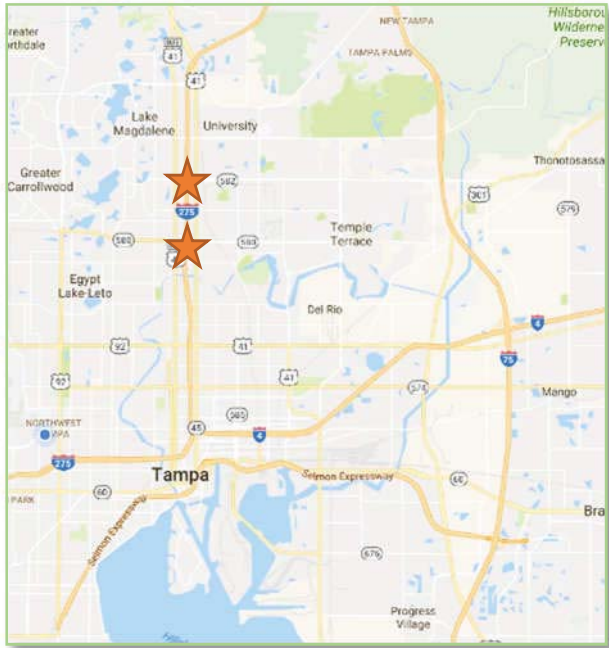
provided by the proposed aesthetic treatments will not be distracting to drivers or in any way make it more difficult for drivers to safely navigate interchange vehicular movements.

Pedestrian lighting consists of 9 feet tall square columns that utilize shielding to illuminate an exclamation point on the top half, subtly hinting at the IP logo. They consist of grey posts with green light or green posts with white light and are similar in shape to the Rincon Pedestrian Lighting by Forms+Surfaces. Final selection of the pedestrian lighting should meet the requirements indicated by Roadway Design Bulletin 17-09. More detail is provided on page 16.

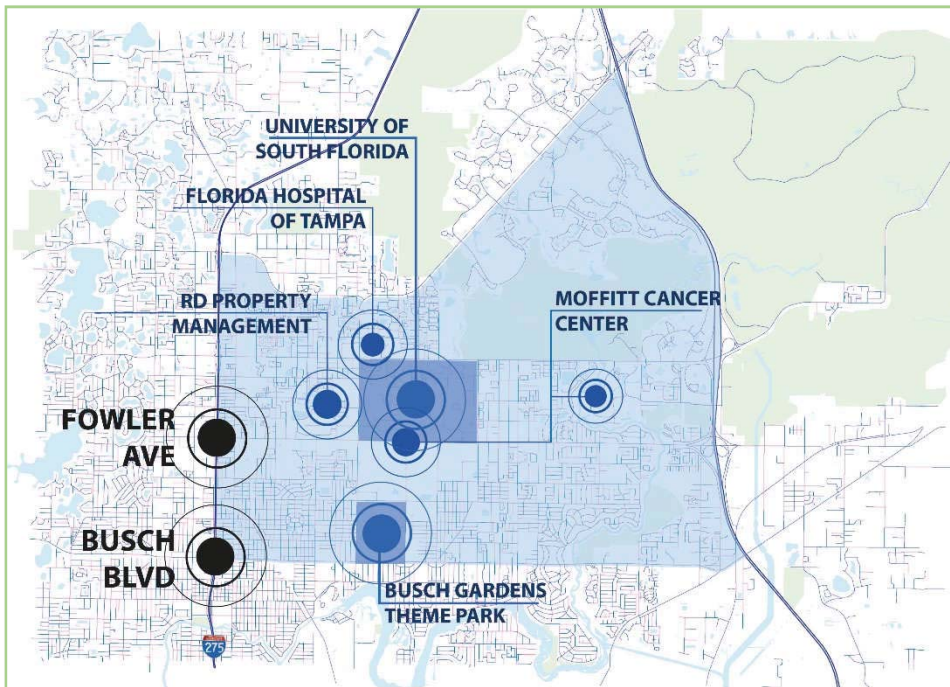
Enhanced crosswalks are comprised of 10 crosswalks (each 10 feet wide and between 18' to 56' long) for a total of 340 linear feet at Fowler and 6 crosswalks (each 10 feet wide and between 20' to 96' long) for a total of 300 linear feet. Enhanced crosswalks are to be ladder style with solid white lines and a dark green pattern in the spaces between the rungs. More detail is provided on page 17.

Concrete power washing or sandblasting of the existing structures (including columns, sloped walls, abutments) and weed removal within the cracks of the concrete sections of the bridge is necessary for a polished look for LED lighting at each interchange. Cleaning will also prep surfaces for aesthetic enhancements in Phase II.

IP District Limits Map in relation to Tampa's downtown



IP District Limits Map



Fowler Ave. Existing Aerial Map



Fowler Ave. Existing Site Photo looking West



Busch Blvd. Existing Aerial Map



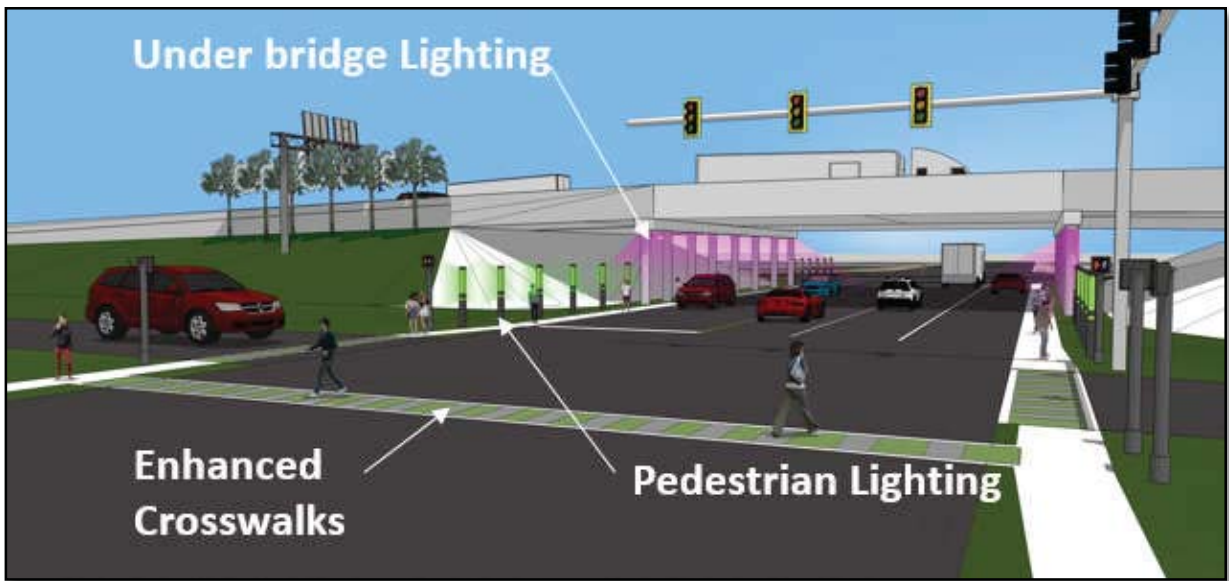
Busch Blvd. Existing Photos looking West, East, and West



Fowler Ave. Proposed Phasing Sketches



Existing View looking West

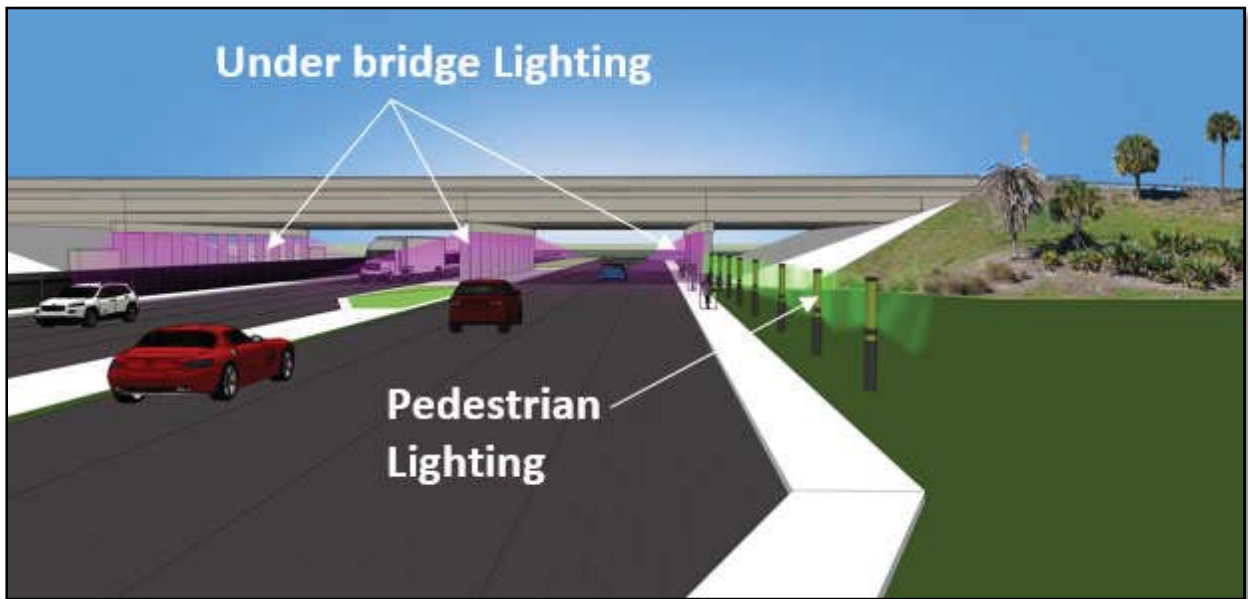


Phase One View

Busch Blvd. Proposed Phasing Sketches

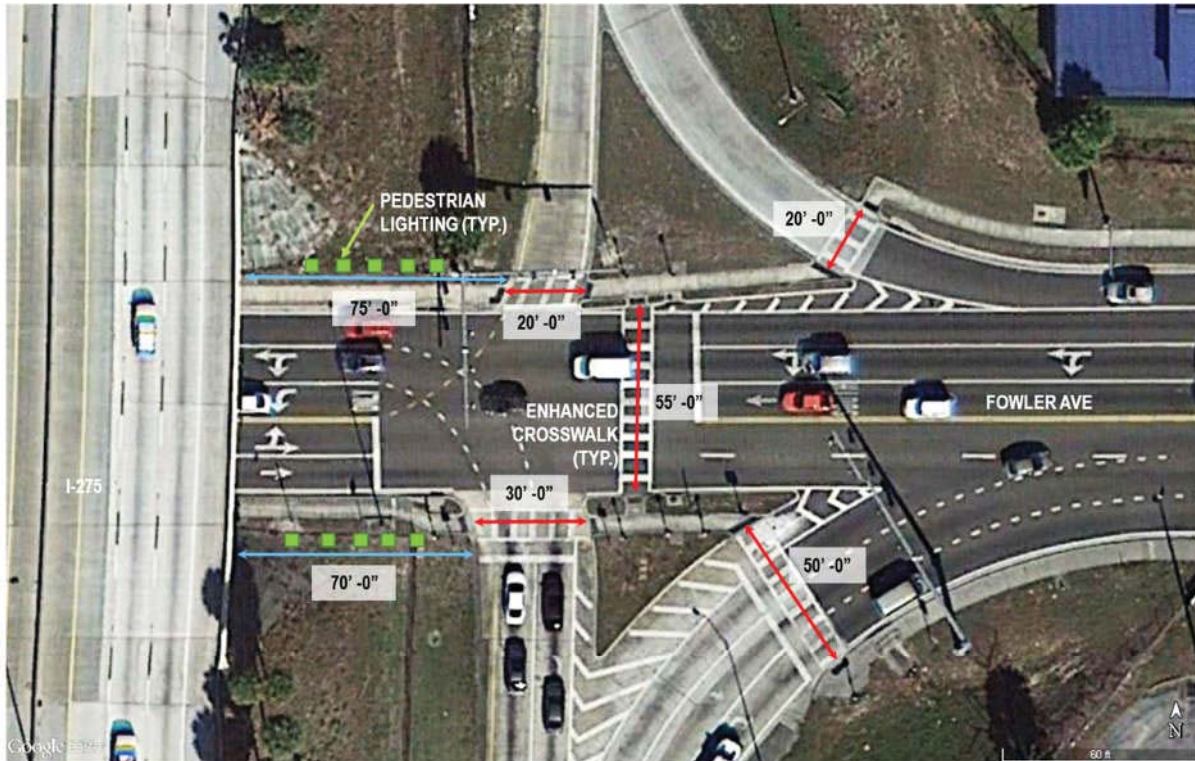


Existing View looking West

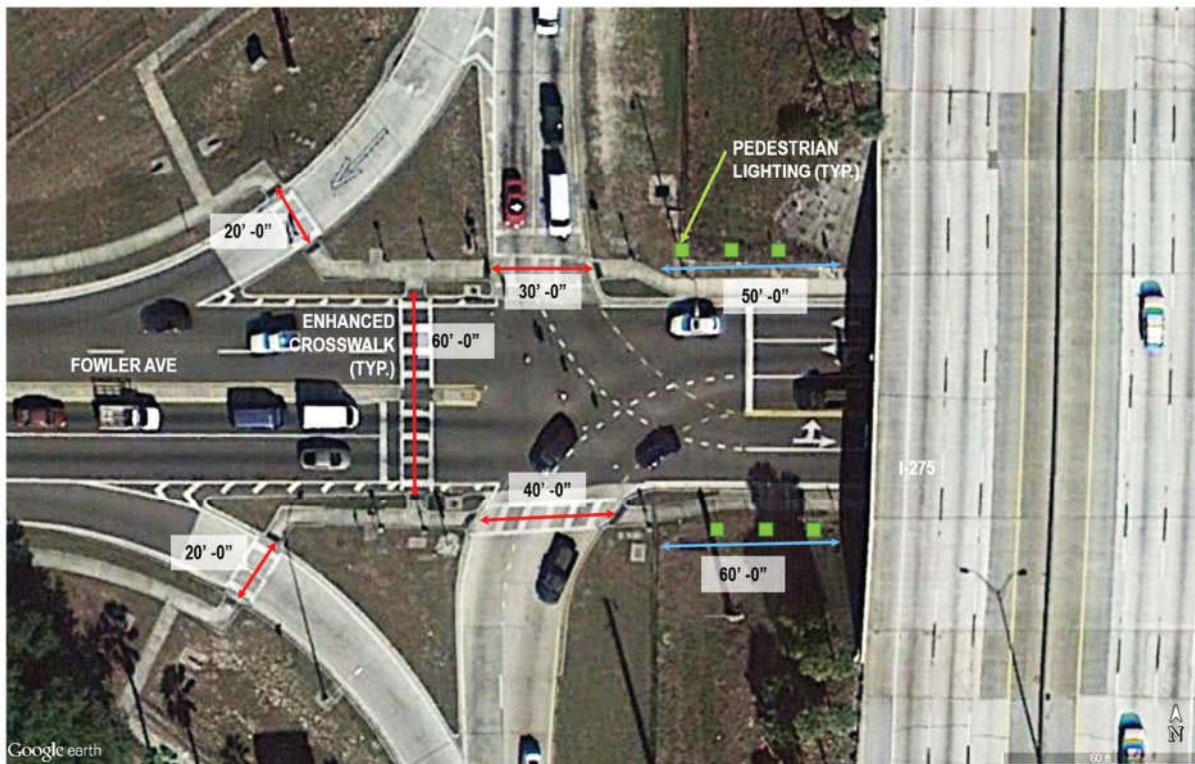


Phase One View

Site plans with dimensions – Fowler Ave.



ENHANCED CROSSWALKS AND PEDESTRIAN LIGHTING AT FOWLER AVE. (EAST)



ENHANCED CROSSWALKS AND PEDESTRIAN LIGHTING AT FOWLER AVE. (WEST)

Site plans with dimensions – Busch Blvd.

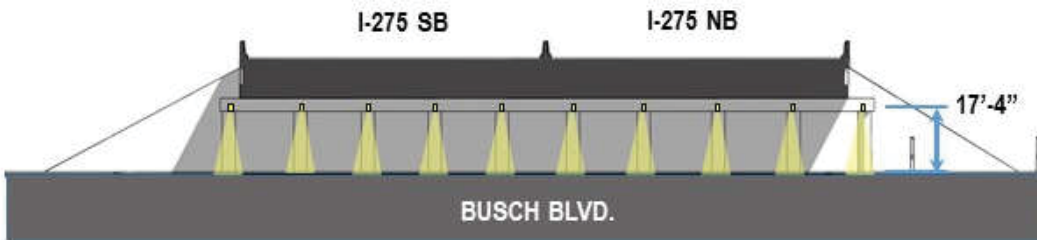


ENHANCED CROSSWALKS AND PEDESTRIAN LIGHTING AT BUSCH BLVD. (EAST)

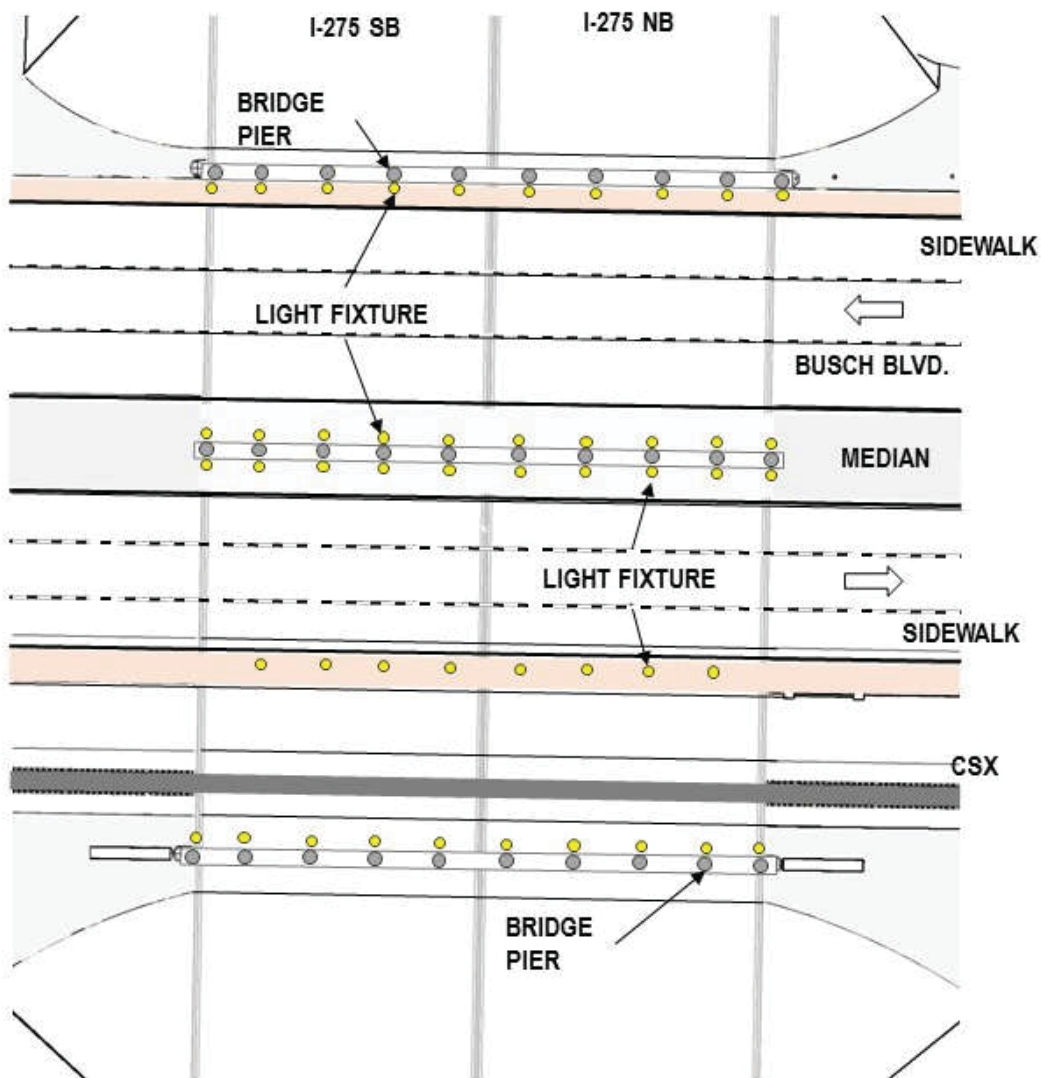


ENHANCED CROSSWALKS AND PEDESTRIAN LIGHTING AT BUSCH BLVD. (WEST)

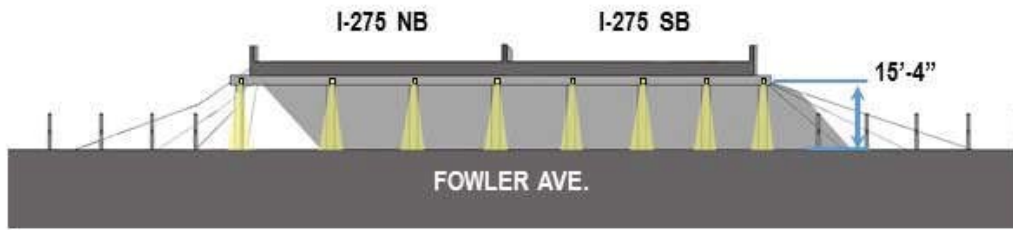
Under Bridge Lighting Details



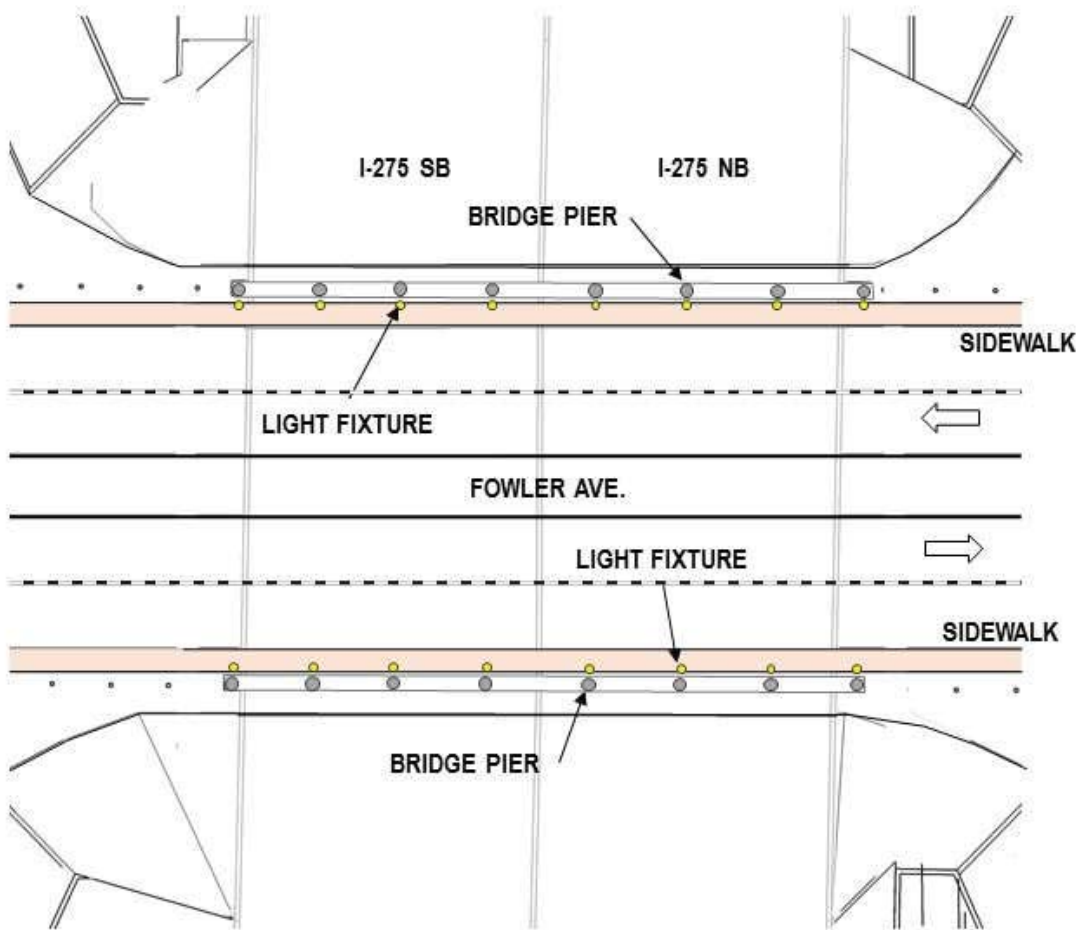
SECTION OF I-275 AT BUSCH BLVD. UNDER BRIDGE LIGHTING



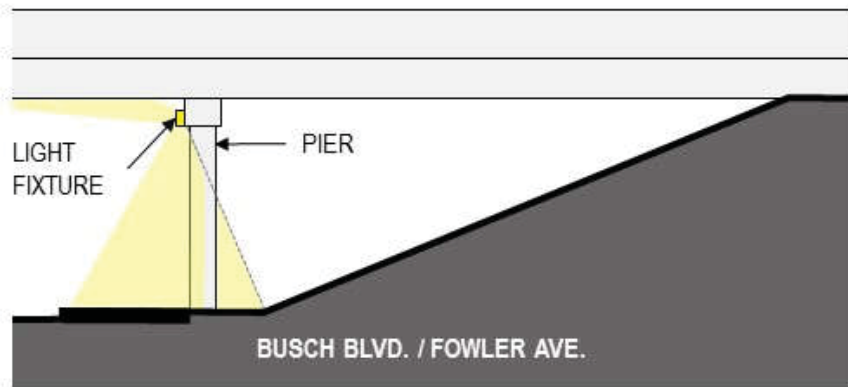
PLAN VIEW OF UNDER BRIDGE LIGHTING I-275 AT BUSCH BLVD.



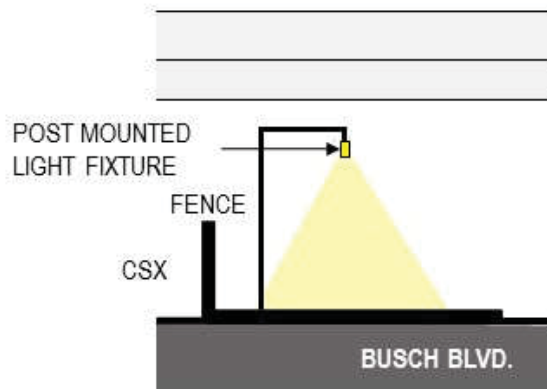
SECTION OF I-275 AT FOWLER AVE. UNDER BRIDGE LIGHTING



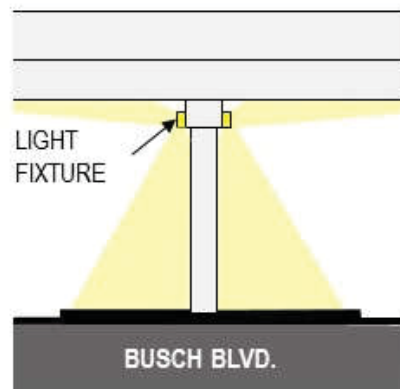
PLAN VIEW OF UNDER BRIDGE LIGHTING I-275 AT FOWLER BLVD.



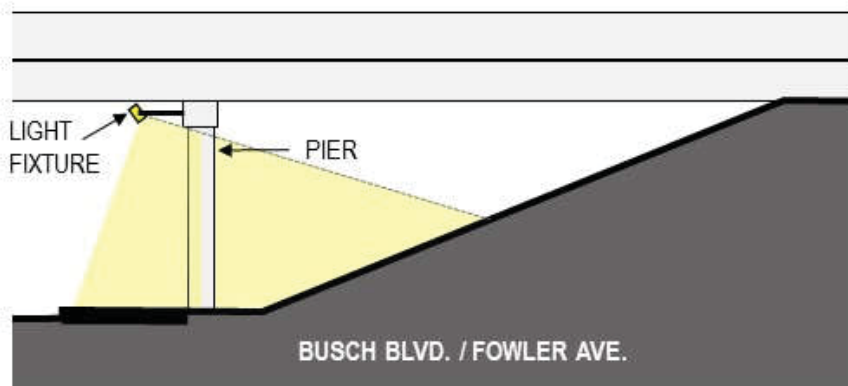
TYPICAL LIGHTING AT PIERS ALONG SIDEWALK, LIGHT WASHING BRIDGE STRUCTURE



LIGHTING ALONG SIDEWALK



LIGHTING AT PIERS ALONG MEDIAN



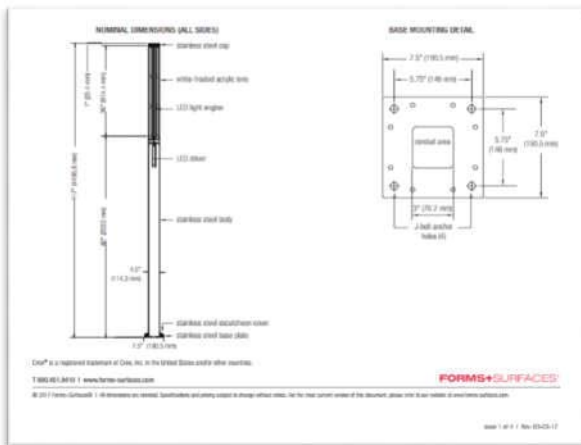
ALTERNATE LIGHTING MOUNT AT PIERS ALONG SIDEWALK

No lighting will be permitted to be attached to the superstructure and beams.
Lighting can be attached to the Piers, Bents, and Columns.

Pedestrian Lighting Detail

The IP pedestrian lighting post is based on the commercially available Forms+Surfaces brand 117" tall square lighting post with shields added to all four sides to make the ! (exclamation mark) design. Posts are hollow stainless steel and could be square or round to achieve the desired exclamation mark effect. Post and light color should be consistent for both interchanges: two potential options are shown in the image below.

A more detailed review utilizing a survey should be undertaken to determine the total number of lights and their exact placement. Installation of the posts will need to be coordinated with utilities and FDOT early in the design phase and must not block any street signs or traffic control devices. They must also not be placed in the FDOT required sight triangles or otherwise impair driver's vision. It is also preferred that they are placed as far away from the sidewalks as possible with as few posts as possible while still achieving the desired illumination and visual effect. Colors of both posts and light color should be coordinated with other design elements (under bridge lighting, painted mast arms, mosaic, column art, etc).



- Option 1: Slate textured posts with colored light
- Option 2: Lime textured posts with white light



Crosswalk Detail

Crosswalks at Fowler Ave. are 10' wide. They have a high emphasis ladder style with white rungs spaced 14' apart and inlay patterns in dark green to maintain contrast for visibility. Patterned inlays are allowed but cannot go over or visually obstruct or distort any portions of the white ladder. White ladder lines are continuous, skid resistant, and if possible, longitudinal markings (ladder rungs) are to be aligned with the edges of lanes to avoid premature wear. The materials will be the Paveway STS (<http://www.pavewaysystems.com/>) because they are more durable than thermoplastics. Non FDOT standard crosswalks which meet the Manual on Uniform Traffic Control Devices (MUTCD) will need a Design Memo for approval.



Crosswalks at Busch Blvd. are 10' wide. They have a high emphasis ladder style with white rungs spaced 14' apart and inlay patterns in dark green to maintain contrast for visibility and to avoid confusion with the green bicycle lane color. Patterned inlays are allowed but cannot go over or visually obstruct or distort any portions of the white ladder. White ladder lines are continuous, skid resistant, and if possible, longitudinal markings (ladder rungs) are to be aligned with the edges of lanes to avoid premature wear. The materials will be the Paveway STS (<http://www.pavewaysystems.com/>) because they are more durable than thermoplastics. Non FDOT standard crosswalks which meet the Manual on Uniform Traffic Control Devices (MUTCD) will need a Design Memo for approval.



Submittal/Approval Letter

To: Richard Moss, P.E. Date: August 31, 2017
District Design Engineer

Financial Project ID: N/A New Construction RRR

Federal Aid Number: N/A

Project Name: IP Community Design Feature (Location 1: Busch Blvd)

State Road Number: SR 93 Co./Sec./Sub.: 10 320 000

Begin Project MP: 4.993 End Project MP: 5.278

Full Federal Oversight: Yes No

Request for: Design Exception , Design Variation

Community Aesthetic Feature: Conceptual , Final

(For Design Exception or Variations Requiring Central Office Approval)

Re-submittal: Yes No Original Ref# _____ - _____ - _____

Requested for the following element(s):

- | | | | |
|---|---|---|--|
| <input type="checkbox"/> Design Speed | <input type="checkbox"/> Lane Widths | <input type="checkbox"/> Shoulder Widths | <input type="checkbox"/> Bridge Widths |
| <input type="checkbox"/> Structural Capacity | <input type="checkbox"/> Vertical Clearance | <input type="checkbox"/> Grades | <input type="checkbox"/> Cross Slope |
| <input type="checkbox"/> Superelevation | <input type="checkbox"/> Horizontal Alignment | <input type="checkbox"/> Vertical Alignment | <input type="checkbox"/> Stopping Sight Distance |
| <input type="checkbox"/> Horizontal Clearance | <input type="checkbox"/> Other _____ | | |

DESCRIPTION:

Location 1 of 2 (Busch Blvd and I-275) Public Art to include both stand alone and affixed features acting as gateway features for the Innovation District of Tampa, Florida. Elements are to include under bridge LED lighting attached to bridge columns and stand alone LED lighting elements 9'-9" tall along the back of sidewalks. Also included are decorative crosswalks.

DOCUMENTATION:

See attached documentation for complete description of all elements proposed.

Recommended by:

_____ Date _____
 Responsible Professional Engineer or Landscape Architect (Landscape-Only Projects)

Approvals:

_____ Date _____	_____ Date _____
<small>District or Turnpike Design Engineer</small>	<small>District Structures Design Engineer</small>
_____ Date _____	_____ Date _____
<small>State Roadway Design Engineer</small>	<small>State Structures Design Engineer</small>
_____ Date _____	_____ Date _____
<small>Chief Engineer</small>	<small>FHWA Division Administrator</small>

Submittal/Approval Letter

To: Richard Moss, P.E. Date: August 31, 2017
 District Design Engineer

Financial Project ID: N/A New Construction RRR

Federal Aid Number: N/A

Project Name: IP Community Design Feature (Location 2: Fowler Ave)

State Road Number: SR 93 Co./Sec./Sub.: 10 320 000

Begin Project MP: 6.278 End Project MP: 6.719

Full Federal Oversight: Yes No

Request for: Design Exception , Design Variation

Community Aesthetic Feature: Conceptual , Final

(For Design Exception or Variations Requiring Central Office Approval)

Re-submittal: Yes No Original Ref# _____ - _____ - _____

Requested for the following element(s):

- | | | | |
|--|---|---|--|
| <input type="checkbox"/> Design Speed | <input type="checkbox"/> Lane Widths | <input type="checkbox"/> Shoulder Widths | <input type="checkbox"/> Bridge Widths |
| <input type="checkbox"/> Structural Capacity | <input type="checkbox"/> Vertical Clearance | <input type="checkbox"/> Grades | <input type="checkbox"/> Cross Slope |
| <input type="checkbox"/> Superelevation | <input type="checkbox"/> Horizontal Alignment | <input type="checkbox"/> Vertical Alignment | <input type="checkbox"/> Stopping Sight Distance |
| <input type="checkbox"/> Horizontal Clearance <input type="checkbox"/> Other _____ | | | |

DESCRIPTION:

Location 2 of 2 (Fowler Avenue and I-275) Public Art to include both stand alone and affixed features acting as gateway features for the Innovation District of Tampa, Florida. Elements are to include under bridge LED lighting attached to bridge columns and stand alone LED lighting elements 9'-9" tall along the back of sidewalks. Also included are decorative crosswalks.

DOCUMENTATION:

See attached documentation for complete description of all elements proposed.

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_____ Date _____
 Responsible Professional Engineer or Landscape Architect (Landscape-Only Projects)

Approvals:

_____ Date _____	_____ Date _____
District or Turnpike Design Engineer	District Structures Design Engineer
_____ Date _____	_____ Date _____
State Roadway Design Engineer	State Structures Design Engineer
_____ Date _____	_____ Date _____
Chief Engineer	FHWA Division Administrator



HILLSBOROUGH COUNTY MPO

SCHOOL SAFETY STUDY

FINAL DRAFT APRIL 2018

School Transportation Safety Study

FINAL DRAFT April 2018

Prepared For:



Hillsborough County Metropolitan Planning Organization
601 East Kennedy Boulevard, 18th Floor
Tampa, FL 33601

Task Authorization: TOA-05

Prepared By:



Tindale Oliver
1000 North Ashley Drive, Suite 400
Tampa, FL 33602



Element Engineering Group
1713 East 9th Avenue
Tampa, FL 33605

Study Area: Hillsborough MPO School Safety Study

City: City of Tampa & Unincorporated Hillsborough County

County: Hillsborough County

This item has been digitally signed and sealed by:

Printed copies of this document are not considered signed and sealed and the signature must be verified on any electronic copies.

ELEMENT ENGINEERING GROUP

1713 E. 9th AVENUE | TAMPA, FL 33605

PHONE: 813.386.2101

Certificate of Authorization #: 26921

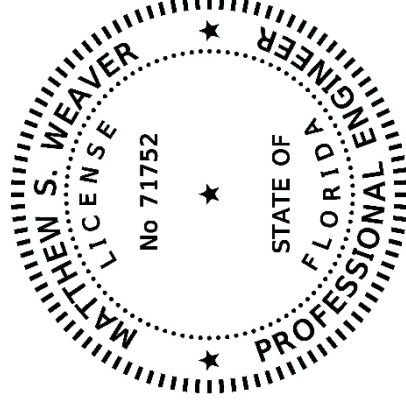


Table of Contents

School Area Multimodal Safety and Mobility Reviews	1
George D. Chamberlain High School	8
Dr. John A. Coleman Middle School	17
Girls Preparatory Academy at Ferrell Middle Magnet School.....	28
George S. Middleton High School	39
Nathan B. Young Middle Magnet School.....	46
Muller Elementary Magnet School.....	53
C. Leon King High School	61
A. P. Leto High School	66
Pierce Middle School.....	73
H. B. Plant High School.....	78
Sulphur Springs K-8 Community School.....	85
Van Buren Middle School (Carter G. Woodson K-8 School)	93
Appendices	

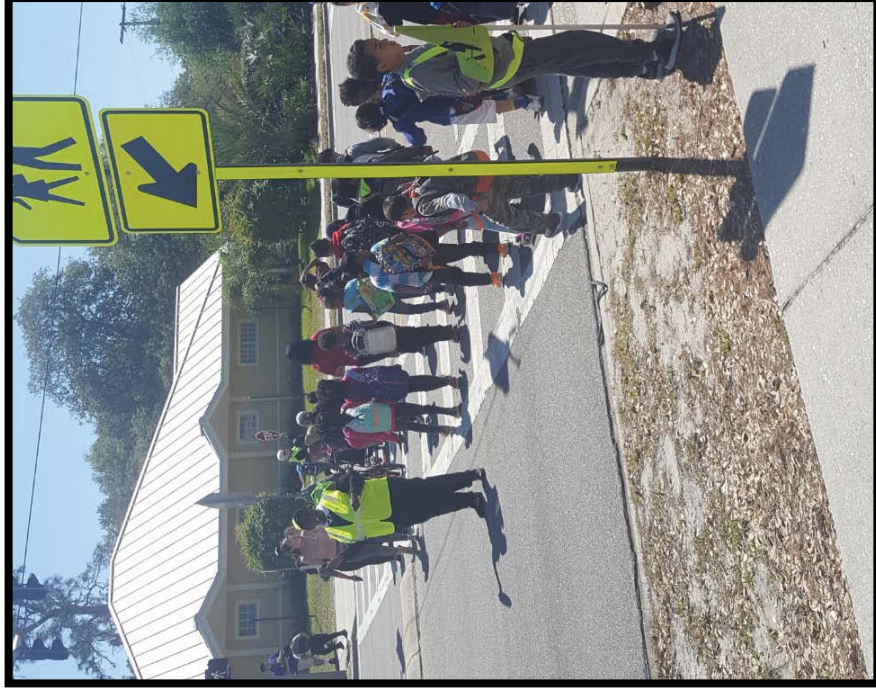
Appendix A: Identification and Prioritization of School Areas for Multimodal Safety Reviews Methodology

Appendix B: School Administration Letters

Appendix C: Data-Driven Lighting Prioritization for the City of Tampa

Appendix D: Summary Tables by Maintaining Agency

Appendix E: Planning-Level Cost Estimate Supporting Documentation



Introduction

The Hillsborough County Metropolitan Planning Organization (MPO) has a longstanding commitment to improving safety and mobility for all users and modes of transportation. The MPO, along with the MPO's School Transportation Working Group (STWG), has made improving safety and mobility for students one of its top priorities. In an effort to identify opportunities to enhance the safety and comfort of getting to and from school, the MPO has initiated a School Safety Study to prioritize public school areas based on a data driven method, conduct safety and mobility reviews at selected school locations, and develop a list of actionable safety and mobility improvements.

Identifying and Prioritizing School Areas

Before conducting safety and mobility reviews and developing recommendations, a data driven methodology for prioritizing and identifying school areas for reviews needed to be developed. Using a data driven method ensures that the school areas are selected based on data inputs rather than a complaint driven system. Data inputs such as pedestrian and bicycle crash history, number of students living in proximity to the school, and other safety, socioeconomic, and school related data inputs were used to identify school areas for further review. A detailed description of the methodology developed to prioritize and identify school areas for further review can be found in Appendix A of this document.

In general, the school areas were prioritized through a process that defined a school area boundary using a combination of the school attendance boundaries and a two-mile walking distance, attributed data to those school areas, and then evaluated and weighted the

school areas based on the agreed upon methodology. Based on the results of the evaluation methodology, 10 school areas were selected for multimodal safety and mobility reviews. Those school areas are listed below and are displayed in Figure 1.

School Areas for Review:

- George D. Chamberlain High School
- Dr. John A. Coleman Middle School
- Combined School Area due to Proximity
 - Girls Preparatory Academy at Ferrell Middle Magnet School
 - George S. Middleton High School
 - Nathan B. Young Middle Magnet School
- Muller Elementary Magnet School
- C. Leon King High School
- A. P. Leto High School
- Pierce Middle School
- H. B. Plant High School
- Sulphur Springs K-8 Community School
- Van Buren Middle School (Carter G. Woodson K-8 School)

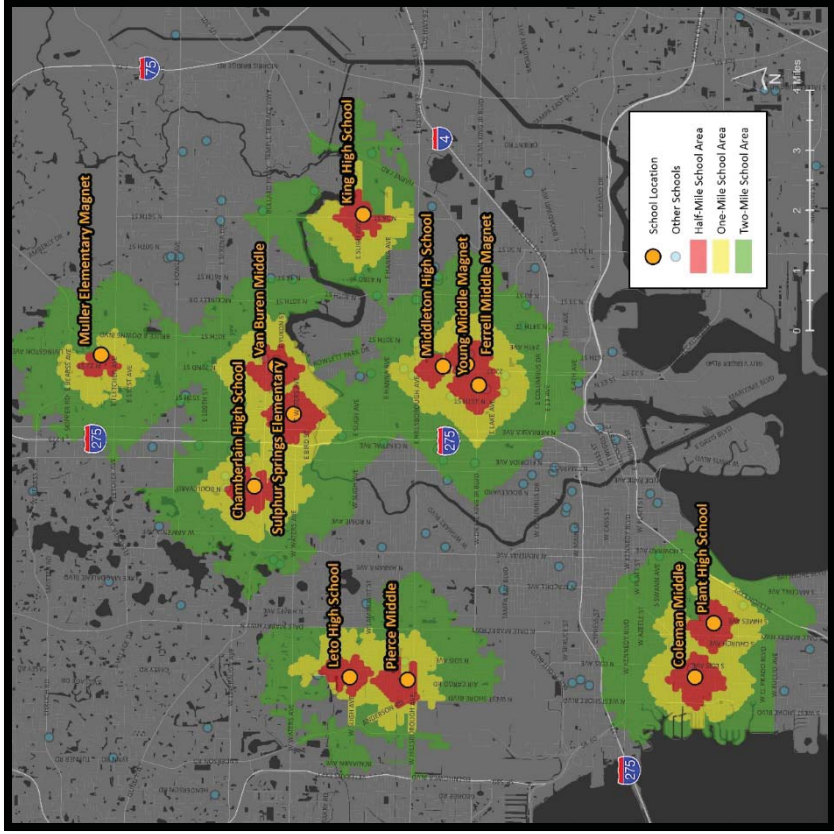


Figure 1: School Areas Selected for Review

School Area Multimodal Safety and Mobility Reviews

The school area multimodal safety and mobility reviews were conducted to help identify potential safety and mobility enhancements aimed at improving the pedestrian and bicycle environment around the selected school areas. The reviews provided

an opportunity to go beyond a data evaluation, to assess existing physical and behavioral elements that pose as potential safety issues and/or inhibit or discourage mobility. The reviews included an evaluation of roadway and roadside features, design elements, and environmental features such as visibility and lighting.

In addition to determining if the needs of all roadway users are being adequately and safely met, the reviews provided an opportunity to observe the behavior and interaction between various users of the transportation system. The ultimate goal of the multimodal reviews is to consider all users and identify opportunities to improve safety and mobility for those users.

The following sections provide an overview of the multimodal safety and mobility review process, the structure of the review findings, an overview of systemic/best practice improvements, and detailed site-specific review findings.

Review Process

While not an official Roadway Safety Audit (RSA), the school area multimodal safety and mobility reviews were completed using a method based on the RSA process established by the Federal Highway Administration (FHWA) Safety Office. The multimodal reviews were conducted as a qualitative assessment and summary of potential road safety issues that resulted in a list of opportunities for improvements in engineering, enforcement, and potential educational opportunities to improve safety and mobility for users.

Review Findings Structure

The multimodal review findings are structured to provide the location, an overview of the observations, suggestions for

HILLSBOROUGH MPO SCHOOL SAFETY STUDY

consideration, and list the responsible agency for each suggestion for consideration. The observations and corresponding suggestions are assigned to one of three levels of effort categories – low, medium, or high:

- Low effort level suggestions consist of basic improvements such as signage and pavement markings that can generally be completed with in-house maintenance staff.
- Medium effort level suggestions are typically more involved and while they may be able to be completed using in-house staff, they typically require some level of engineering design and may be applicable for a design-build pushbutton contract, which can implement improvements in shorter amounts of time than standard design-bid-build projects.
- High effort level suggestions are the most involved and may require right-of-way, public involvement, and typically require a work program/capital improvement program project to complete.

The observation and suggestions are also assigned to one of three time frame categories – short-term, mid-term, or long-term:

- Short-Term suggestions are those that could be completed in as little as couple of weeks up to two years.
- Mid-Term suggestions are those that can typically be completed from two years to five years depending on the complexity of the improvement and funding.
- Long-Term suggestions are those that can take more than five years to implement due to right of way limitations or overall project cost.

The observations and corresponding suggestions are also grouped into two categories – systemic or general area-wide suggestions or site specific suggestions:

- Systemic suggestions relate to recurring safety and mobility issues observed throughout a study area and/or are related to recognized “best-practice” enhancements that should be considered throughout the study area.
- Site specific suggestions relate to an issue or issues that are at a single specified location within the study area.

Systemic Enhancements

- Systemic Recommendations
 - Roadway Lighting
 - Sidewalk Connections
 - Crosswalk Markings
 - Driveway Design
 - Leading Pedestrian Intervals
 - Signage
 - School Zone Signage and Markings

Pedestrian and bicycle crashes, particularly those involving students traveling to and from school, often occur at random and do not exhibit clear patterns and clustering that are often seen in other crash types. As such, a systemic approach, along with site specific improvements, is often followed to help mitigate and address safety and mobility issues related to pedestrians and bicyclists. The following is a list of systemic or “best practice” measures that are commonly used to mitigate pedestrian and bicycle safety and mobility issues.

HILLSBOROUGH MPO SCHOOL SAFETY STUDY

Roadway and Intersection Lighting

Roadway lighting is a critical component of roadway safety and should be designed to provide adequate illumination for all roadway users. Many factors affect roadway lighting, such as location, orientation, intensity, color, ambient light, etc., and its effectiveness in improving safety. There has been an increased effort to improve roadway lighting with an emphasis on improving lighting at signalized intersections and marked crosswalks. The Florida Department of Transportation (FDOT) has adopted new standards for intersection lighting design (Figure 2) and illuminance levels for urban roadways with an elevated pedestrian crash risk.

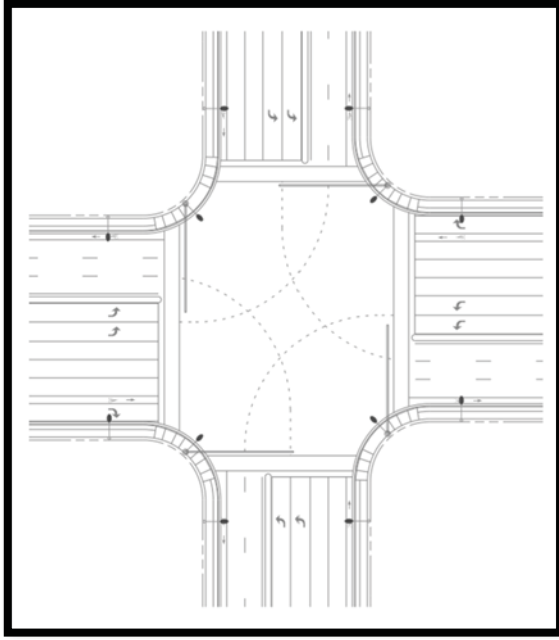


Figure 2: Typical Urban Signalized Intersection Lighting Design (FDM Figure 231.3.4)

The Florida Design Manual (FDM) Chapter 231 addresses the process and criteria for the development of lighting designs on the state

highway system. Opportunities to enhance roadway lighting should be explored, while focused at intersections and marked crossings with high pedestrian volumes.

To assist the City of Tampa in prioritizing any available funding, a list of lighting priorities has been assembled in Appendix C, specifically sorted by the determined ranking of each school where roadway lighting (either evaluation, upgrades or new lighting) was recommended. This ranking was based on the information included in Appendix A which explains the methodology and prioritization of the schools that were analyzed for this study. The City of Tampa has a number of ongoing projects, some of which cover areas included in the recommendations. This list can be utilized to supplement ongoing efforts and can be utilized as a basis for future projects or potentially to develop funding requests.

Sidewalk Connections

Sidewalks are the backbone of the non-motorized transportation network. They provide pedestrians, and often bicyclists, with a dedicated facility that is separated from vehicular traffic. Sidewalks, on both sides, should be considered (based on context) on all new or major roadway reconstruction projects and an effort should be made to retrofit existing roadways that currently do not have sidewalks, particularly along arterial and collector roadways. Sidewalks are also useful in encouraging pedestrians and bicyclists to cross at preferred crossing points by making access to those points more convenient and obvious. Installing sidewalks along roadways with either no sidewalk or significant sidewalk gaps should be considered, especially along arterial and collector roadways and those roadways where there is a high likelihood of student activity.

Connections to Adjacent Properties

Connections to adjacent roadways should be made a priority to avoid lengthy walking routes for students accessing school sites. Balancing the access points for the school and the length of the walking route is a key topic and should be explored for each school site to avoid deterring walking and bicycling.

Crosswalk Markings

Similar to sidewalks, crosswalks are a vital part of the non-motorized transportation network. They define a designated crossing area for pedestrians and bicyclists and alert drivers to the likelihood of pedestrian activity. There are many different types of acceptable crosswalk markings/treatments, but the special emphasis crosswalk marking (Figure 3) is often considered the preferred treatment and is also known as a ladder crosswalk.

The longitudinal markings along with the parallel edge line markings of the ladder crosswalk provide more surface area to be seen by drivers and are more visible from further distances. Consider providing ladder crosswalk markings at all signalized intersections and at other locations with a high frequency of pedestrian crossings, including across side streets along primary walking corridors.

Driveway Design

Driveways provide a physical connection between roadways and the properties along them. The design and location of driveways are based on multiple considerations, but ultimately driveways need to provide safe entry and exit from a site, minimize impacts on traffic, and provide a clear and safe environment for all roadway users. While driveways provide a critical connection between the roadway and properties along the roadway, driveways also create inconsistencies in the walking/biking environment and increase the number of potential conflict points between pedestrians/bicyclists and motor vehicles. Furthermore, the physical design of a driveway can have a significant impact on the safety and level of comfort for pedestrians/bicyclists along a roadway. Understanding the role that driveway design, location, and frequency have on pedestrian and bicycle safety and mobility is essential when trying to encourage an environment that supports and promotes walking and biking.

Leading Pedestrian Interval

Leading pedestrian intervals (LPI) give pedestrians the “Walk” signal (typically 3-7 seconds) before drivers are allowed to proceed through the intersection. The Manual on Uniform Traffic Control Devices (MUTCD) provides guidance on the implementation of LPIs and states, “If a leading pedestrian interval is used, it should be at least 3

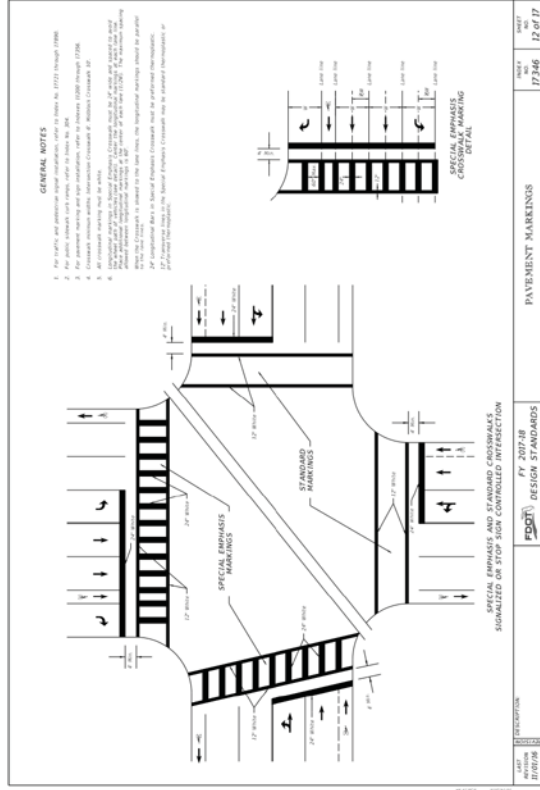


Figure 3: Common Crosswalk Markings

seconds in duration and should be timed to allow pedestrians to cross at least one lane of traffic or, in the case of large corner radius, to travel far enough for pedestrians to establish their position ahead of turning traffic before the turning traffic is released.” LPIs improve pedestrian visibility and increase the likelihood that a driver will yield to pedestrians in the crosswalk. Consider implementing LPIs at larger intersections in the area of schools with multiple potential conflicts and at intersections with higher pedestrian volumes.

Signage

Signage can be used to warn roadway users of potential threats and can be used as visual reminders of how drivers are required to act. Signs such as the Turning Vehicles Yield to Pedestrians or Manual of Uniform Traffic Control Devices (MUTCD) R10-15 sign (Figure 4) remind right-turning drivers of their responsibility to yield to pedestrians. However, the placement of signage should be done with care as the overuse of signs can desensitize drivers and subsequently result in noncompliance.



Figure 4: Turning Vehicles Yield to Pedestrians (MUTCD R10-15) Sign

School Zone Markings and Signage

School zone markings and signage are used to warn drivers and other roadway users of an increased presence of students and can also serve as visual reminders on how drivers are required to act in specific circumstances. Signs such as the S1-1, W16-9P signs (Figure 5) and S5-1 (Figure 6) from the MUTCD remind drivers that they are entering a school zone.



Figure 5: School Crossing Ahead (MUTCD S1-1 and W16-9P) Sign Assembly



Figure 6: School Speed Limit When Flashing (MUTCD S5-1) Sign

School Area Review Findings

This section focuses on the school area specific findings. It is important to note that the enhancements identified in the school area multimodal safety and mobility reviews represent potential opportunities and are not necessarily recommendations; rather, they

HILLSBOROUGH MPO SCHOOL SAFETY STUDY

are suggestions for further consideration. It should also be understood that, in many instances, the identified enhancements, while they've been reviewed with the appropriate implementing agencies, will require additional evaluation, analysis, and/or engineering design to determine the full feasibility of each potential enhancement. The following sections, broken down by school site, provide the detailed versions of the multimodal safety and mobility reviews. Appendix D provides a summary of the observations and suggestions for consideration broken down by each maintaining agency.

Planning-Level Estimates

Appendix E includes planning-level estimates developed to be a starting point to support efforts to obtain funding, program project or for possible grant applications. The estimates cover the potential construction cost estimate, including a percentage for maintenance of traffic (MOT), mobilization (MOB) and project unknowns. Each estimate includes a by pay item summary of the effort potentially required to construct the described enhancement, but does not include any design fees due to the variety of levels of design due to the differences in design cost based on the project delivery method. If a consultant design firm is required to complete the design, a percentage of the construction estimate could be calculated to determine a starting point, between 25 and 40 percent based on the overall project cost, with the higher design fee applicable as the overall project cost goes down.

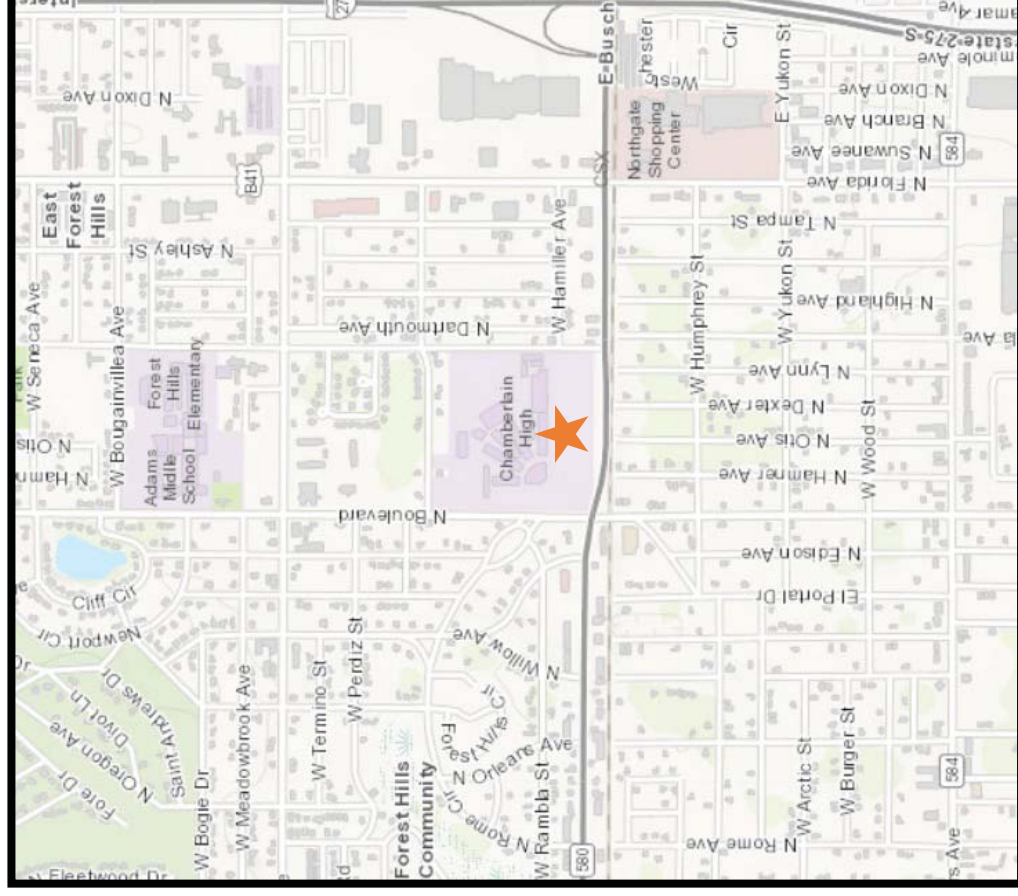
George D. Chamberlain High School

School Location



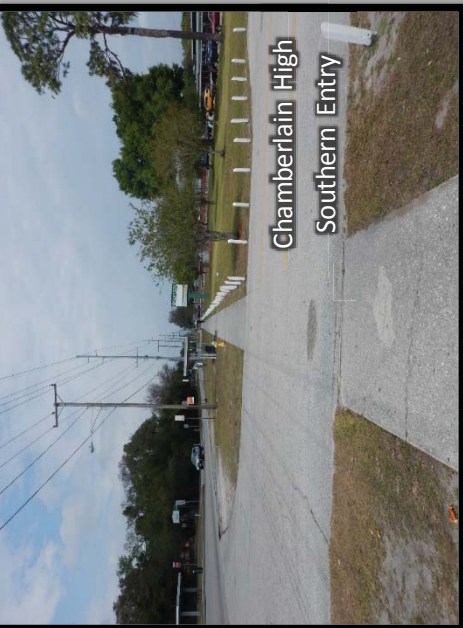
George D. Chamberlain High School is located within the City of Tampa, just north of Busch Boulevard (SR 580) on North Boulevard.

Meeting with School Administration

The review team met with Assistant Principal for Administration Jody Woods on March 19. Ms. Woods discussed the general traffic patterns around the campus involving students, and explained how students begin arriving to the campus as early as 6:30 am. One of the school administration's primary concerns is lighting at the front entrance and along North Boulevard due to the arrival time and darkness. A floodlight on the front of the school covers a portion of the drop-off loop at the school entrance driveway but doesn't cover any of North Boulevard, which has a high volume of pedestrian, bicycle and vehicular traffic during dark arrival times. Existing high-pressure sodium street lights provide a small amount of light along North Boulevard. Ms. Woods also discussed the administration's desire to provide the right crossing facilities to encourage the students to cross North Boulevard at controlled locations due to the tendency of the students to cross at random to the convenience store and other destinations on the west side of North Boulevard. Ms. Woods also stated that many students utilize HART routes and are walking to the transit stops both on North Boulevard and on Busch Boulevard (SR 580).

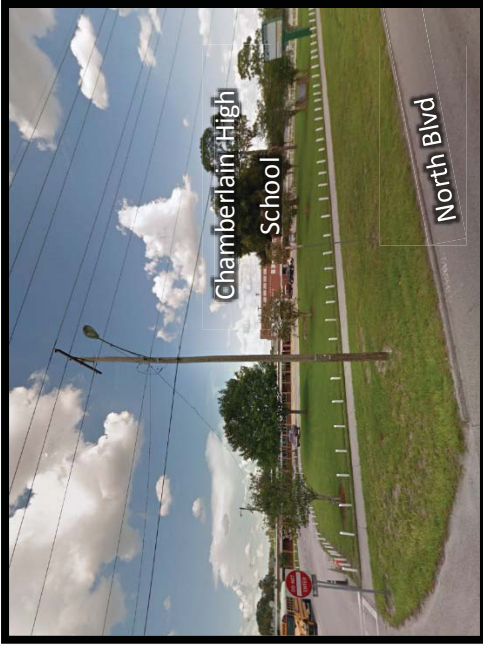


George D. Chamberlain High School

ID	Location Description:	Observation Overview:	Suggestions for Consideration:
A1	North Boulevard from Sligh Avenue to Country Club Drive	Faded and unmarked crosswalks	Enhance or add pavement markings
		<p>Observation Detail: Side street crosswalks along North Boulevard from Sligh Avenue to Country Club Drive were unmarked or faded.</p>	
		<p>Suggestion Detail: Refurbish existing crosswalks and add new crosswalks for all side street crossings on North Boulevard, including high-volume driveways.</p>	
		<p>Responsible Agency: City of Tampa</p> <p>Improvement Type: Enhanced pavement markings</p> <p>Time Frame: Short Term</p> <p>Level of Effort: Low</p> <p>EEE: Engineering</p> <p>Cost Estimate: \$69,670</p> <p>Feasibility Review Comment:</p>	

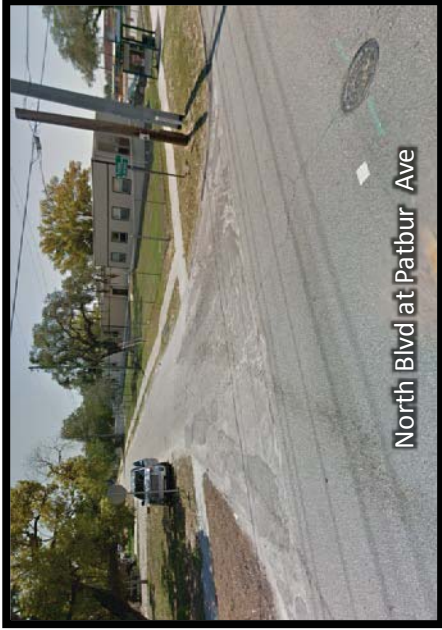
George D. Chamberlain High School

ID	Location Description:	Observation Overview:	Suggestions for Consideration:
A2	North Boulevard from Waters Avenue to Country Club Drive	Condition of existing high-pressure sodium (HPS) street lights	Upgrade to light emitting diode (LED) and add additional luminaires as necessary
<p data-bbox="399 995 423 1215">Observation Detail:</p> <p data-bbox="448 199 505 1215">Due to the school hours, students are walking, biking, and driving to school during dark hours, especially during daylight saving months.</p>			
<p data-bbox="630 1005 654 1215">Suggestion Detail:</p> <p data-bbox="678 199 768 1215">Consider upgrading all the existing high-pressure sodium luminaires to light emitting diode luminaires to enhance lighting on North Boulevard focusing on the area in front of the school and near key crosswalks and transit stops along the corridor.</p>			
Responsible Agency:		Improvement Type:	
City of Tampa		Enhanced street lighting	
Time Frame:		Level of Effort:	
Mid Term		Medium	
EEE:		Cost Estimate:	
Engineering		\$880,602	
Feasibility Review Comment:			




George D. Chamberlain High School

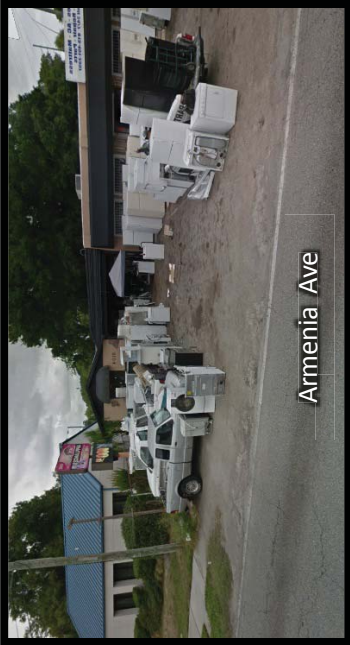
ID	Location Description:	Observation Overview:	Suggestions for Consideration:
A3	North Boulevard from Sligh Avenue to Country Club Drive	Potential ADA enhancements	Enhance curb ramps and detectable warnings
Observation Detail:			
Several crossings along North Boulevard were identified that could be enhanced to include current ADA features such as detectable warnings, although they may have met compliance requirements upon installation. The picture on the left is taken at the intersection of Patbur Avenue and North Boulevard, just north of the Chamberlain High School campus, however, similar conditions exist at multiple intersections throughout North Boulevard corridor and present various walkability challenges.			
Suggestion Detail:			
Consider ADA and curb ramp upgrades by modifying the existing curb ramps and adding detectable warnings along this key pedestrian and bicycling corridor for George D. Chamberlain High School, Adams Middle School and Forest Hills Elementary School. Pushbuttons and ramps at the North Boulevard and Linebaugh Avenue signal should be included if this suggestion is pursued.			
Responsible Agency:		Improvement Type:	
City of Tampa		Pedestrian Features	
Time Frame:		Level of Effort:	
Mid Term		Medium	
EEE:		Cost Estimate:	
Engineering		\$173,600	
Feasibility Review Comment:			
Potential right of way challenges at some locations, but clear areas could be incorporated as part of ongoing routine sidewalk maintenance or included in any ongoing capital improvement projects in the area.			



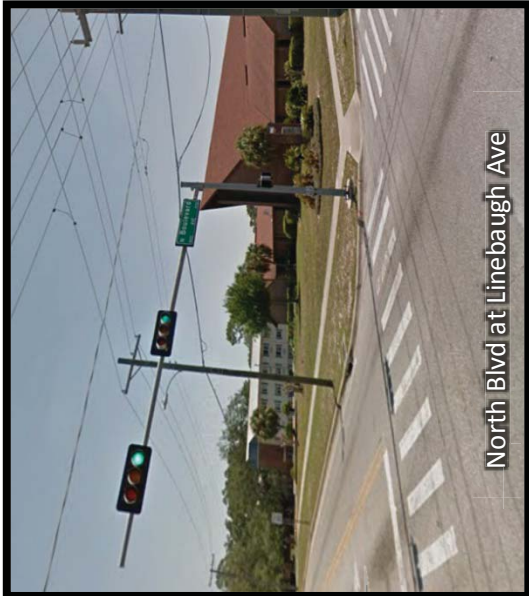
George D. Chamberlain High School

ID	Location Description:	Observation Overview:	Suggestions for Consideration:
A4	North Boulevard from Busch Blvd. (SR 580) to Country Club Drive	Midblock crossing opportunities	Consider additional enhanced midblock crossing locations
			
<p>Observation Detail:</p> <p>Several standard crossing locations for pedestrians were observed, though many were not formally defined with signing, ADA curb ramps, or other enhancements.</p>			
<p>Suggestion Detail:</p> <p>Consider completing a corridor pedestrian and bicycling study to determine the most appropriate location for potential enhanced crossing locations that will provide connectivity for George D. Chamberlain High School, Adams Middle School, Forest Hills Elementary School and the residents and transit users in the area, focusing on the area in front of George D. Chamberlain High School.</p>			
<p>Responsible Agency:</p> <p>City of Tampa</p> <p>Improvement Type:</p> <p>Study & Potentially Pedestrian Features</p> <p>Level of Effort:</p> <p>Medium</p> <p>Cost Estimate:</p> <p>\$35,215</p> <p>Feasibility Review Comment:</p>			

George D. Chamberlain High School

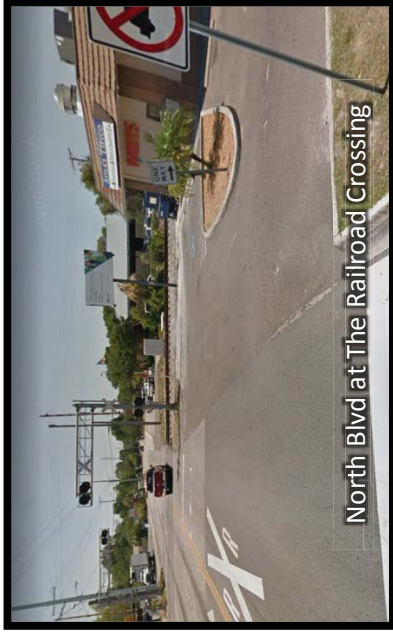
ID		Location Description:	Observation Overview:	Suggestions for Consideration:
A5	The northwest quadrant of the intersection of Bird Street & Armenia		Stored products block pedestrian pathways	Code enforcement contact to request cleared path
				
Observation Detail:				
Items for sale encroach on the pedestrian walking paths along the right of way line and natural sidewalk area.				
Suggestion Detail:				
Consider involving code enforcement to request clear walking paths and avoid the potential for pedestrians being forced to walk onto the shoulder or roadway.				
Responsible Agency:				
City of Tampa			Improvement Type:	
			Code Enforcement	
Time Frame:				
Short Term			Level of Effort:	
			Low	
EEE:				
Enforcement			Cost Estimate:	
			N/A	
Feasibility Review Comment:				

George D. Chamberlain High School

ID	Location Description:	Observation Overview:	Suggestions for Consideration:
A6	Busch Blvd. (SR 580) at North Blvd. North Blvd. at Linebaugh Avenue	Signal phasing opportunities	Modify signal phasing to avoid permissive left-turn movements that conflict with pedestrians.
		<p>Observation Detail:</p> <p>A review of the crash reports showed a number of pedestrian-involved crashes during permissive left-turn phases. The two referenced intersections have permissive left-turn movements for the north and southbound left turns at Busch Blvd (SR 580) and all movements at Linebaugh Avenue.</p>	
		<p>Suggestion Detail:</p> <p>Consider modifying the signal phasing at Busch Blvd (SR 580) to restrict left-turns to protected only during school arrival and departure hours. Flashing yellow arrows have already been implemented for these movements. Consider adding flashing yellow arrow four-section signal heads at the Linebaugh Avenue intersection, although this suggestion would require significant modifications and potentially a signal rebuild.</p>	
		<p>Responsible Agency:</p> <p>City of Tampa & Florida Department of Transportation</p>	<p>Improvement Type:</p> <p>Signal Timing & Equipment Modifications</p>
		<p>Time Frame:</p> <p>Mid Term</p>	<p>Level of Effort:</p> <p>Medium</p>
		<p>EEE:</p> <p>Engineering</p>	<p>Cost Estimate:</p> <p>\$479,910</p>
		<p>Feasibility Review Comment:</p> <p>Potential constructability challenges. Existing structures at Linebaugh Avenue and North Boulevard may not meet structural loading requirements for any additional loading.</p>	

George D. Chamberlain High School

ID	Location Description:	Observation Overview:	Suggestions for Consideration:
A7	Busch Blvd. (SR 580) at Florida Ave. & Busch Blvd. (SR 580) at North Blvd.	Sidewalk connectivity and railroad gates	Enhance sidewalk connectivity and pedestrian safety near railroad crossings.
		Observation Detail:	
		Several railroad sidewalk crossings along North Boulevard were observed that lacked connectivity and pedestrian railroad gates to block the sidewalk when a train is present. The west side of North Boulevard has a sidewalk gap in the area of the railroad tracks.	
		Suggestion Detail:	
		Consider filling the gap and enhancing the sidewalk connectivity in this area and including pedestrian railroad crossing gates.	
		Responsible Agency:	Improvement Type:
		City of Tampa & CSX	Pedestrian features & sidewalk
		Time Frame:	Level of Effort:
		Long Term	High
		EEE:	Cost Estimate:
		Engineering	\$17,140
		Feasibility Review Comment:	
		Potential right of way and constructability challenges, but could be incorporated as part of ongoing routine sidewalk maintenance. The Florida Avenue location is being completed in an upcoming railroad crossing improvement project to be completed by the Florida Department of Transportation.	



George D. Chamberlain High School

ID	Location Description:	Observation Overview:	Suggestions for Consideration:
A8	Busch Blvd. (SR 580) at North Blvd.	Potential need for reduced speed school zone	Evaluate location for implementation of a reduced speed school zone.
Observation Detail:			
A school crossing is signed at the signalized intersection of North Boulevard on Busch Boulevard (SR 580) but no reduction in speed is present during school arrival and departure hours, only advisory speed signing for a school entrance.			
Suggestion Detail:			
Evaluate this location per the guidelines in the Florida Department of Transportation's Manual on Speed Zoning for Highways, Roads, and Streets in Florida to determine if a reduced speed school zone is warranted along Busch Boulevard (SR 580), due to the presence of students walking a long sidewalk immediately adjacent to the curb and roadway.			
Responsible Agency:		Florida Department of Transportation	Improvement Type: Engineering Study & Signing if Warranted
Time Frame:		Mid Term	Level of Effort: Medium
EEE:		Engineering	Cost Estimate: \$26,790
Feasibility Review Comment:			
<div data-bbox="639 1234 1109 1892" data-label="Image"> <p>Busch Blvd Existing Signage</p> </div>			

Section 2

PAG Meetings

Section 2.1

PAG Meeting 1

Project Advisory Group Meeting #1

Date: Tuesday, August 29, 2017
Time: 10:00 a.m. to 12:00 p.m.
Location: North Tampa Branch Library
8916 North Boulevard
Tampa, Florida 33604
Attendees: See Appendix A

Notification of Meeting

Meeting invitation letters were sent by email to elected officials, government officials, local agencies and other interested parties. The invitation letters included the meeting date, time and location, and a summary of the project.

A Florida Administrative Register (FAR) ad was published in Volume 43/161 on August 18, 2017. The Florida Department of Transportation posted meeting information on the FDOT Public Notices website.

Copies of the invitation letter, FAR ad and FDOT Public Notice website ad are included in Appendix B.

Meeting Handouts

A copy of the meeting PowerPoint, Project Advisory Group (PAG) handout and Newsletter #1 were distributed to the attendees. PAG participants were encouraged to discuss their thoughts and concerns regarding the corridor study.

A copy of the meeting handouts is available in Appendix C.

Attendee Comments

One written comment form was received. A copy of the comment received is included in Appendix D.

Summary

The following provides a summary of the items discussed at the meeting:

- Brian Shroyer of the Florida Department of Transportation (FDOT) opened the meeting by thanking attendees for their participation.

- Consultant Project Manager, Carol Barker, introduced the project team members and asked everyone to introduce themselves and describe their affiliations.
- Carol provided a project overview and explained the role of PAG (See Appendix C).
- Carol guided the PAG through a PowerPoint presentation (See Appendix C).
- Attendees participated in a Color Dot Survey Exercise. They were given green, yellow, blue and red dot stickers and were asked to rank what is most important to least important to them for the SR 580 Corridor. Responses can be found in Appendix E.
- A Draft User Preference Survey was distributed to attendees. Carol went through each potential survey question and asked the attendees for suggestions. Participant's responses can be found in Appendix F. The consultant will revise the User Preference Survey and show the updated version at PAG Meeting #2.
- Carol discussed upcoming PAG meetings and public meetings.
- Carol encouraged the attendees to visit the [FDOT project website](#) and [WikkiMapping website](#).
- At the conclusion of the meeting, attendees were provided copies of the newsletters to distribute to other interested parties.
- Carol provided closing remarks. The meeting ended at 12:00 p.m.
- After the PAG meeting, the consultant team distributed newsletters to businesses along the project corridor.

Discussion

Project Advisory Group (PAG) questions during discussion included:

Attendee: Have you looked into the causes of crashes along the corridor?

Carol Barker: Carol advised "Yes" and discussed the following PowerPoint slides: Crash History Data, Crash Prone Areas Heat Map and Latest Crash History by Year.

Attendee: I was advised a School Zone Safety Study has not been done. Can you please, let us know if you hear different?

Carol Barker: I am not aware of a current School Zone Safety Study but there was a Mid-Block crossing study. I will let you know at the next PAG meeting if I hear any information

Attendee: The MPO provides translation for specific Latino groups such as Puerto Rican and Cuban. Will the User Preference Survey be in Spanish also?

Carol Barker: The User Preference Survey can be translated into Spanish if the need arises.

Attendee: Maybe project information can be pushed out through the Parent Link at local schools?

Carol Barker: That sounds like a great idea. I will discuss with the PAG school representatives.

Attendee: Has there been any discussions with CSX?

Carol Barker: I will be speaking with Brian Shroyer/FDOT, about setting up an individual meeting with Bob O'Malley (CSX), in Bob's Orlando Office.

Attendee: Will you be meeting with the MPO?

Carol Barker: Yes, we will be meeting with the MPO.

END OF MEETING SUMMARY

This meeting summary was prepared by Karen Harrell, Public Involvement Coordinator. If you feel clarifications are necessary, or if this differs from your understanding, please notify Karen by telephone at 352-257-1651 or by e-mail at Karen.Harrell@qcausa.com within (5) working days upon receipt of this summary.

Appendix A



PROJECT ADVISORY GROUP MEETING #1
West Busch Boulevard (SR 580) Corridor Study
 From North Dale Mabry Highway to North Nebraska Avenue
 Financial Project ID Number: 435908-1-22-01



Tuesday, August 29, 2017 from 10:00 am to 12:00 pm ■ North Tampa Branch Library, 8916 North Boulevard, Tampa, FL 33604

Project Advisory Group (PAG) Sign-In Sheet

NAME (PLEASE PRINT)	AGENCY / ORGANIZATION	TITLE	PHONE	EMAIL
Mark Jennings	CH2M		722 560-8144	mark.jennings@ch2m.com
Cris Schooley	CH2M		407 334 2912	Cris.Schooley@ch2m.com
Chad Polk	CH2M		813.281.7912	chad.polk@ch2m.com
David Hernandez	CH2M		813-281-7912	david.hernandez@ch2m.com
Karen Harrell	QCA		352-257-1651	KarenHarrell@QCAUSA.com
Jessica Francois	QCA		850-356-0679	Jessica.francois@QCAUSA.com
Brian L. Simpson	FAOT		813-975-6449	Brian.Simpson@DOT.STATE.FL.US
TIMOTHEE RATNAYAKE	FAOT		913-975-6057	TIMOTHEE.RATNAYAKE@DOT.STATE.FL.US

Appendix B

Date: August 08, 2017

Subject: **Project Advisory Group (PAG) Meeting**
West Busch Boulevard (SR 580) Corridor Study
from North Dale Mabry Highway to North Nebraska Avenue
Hillsborough County, Florida
FDOT Financial Project ID Number: 435908-1-22-01

Dear Project Stakeholder:

The Florida Department of Transportation (FDOT), District Seven is conducting a Corridor Study on the above referenced study. This letter is an invitation for you to become a member of the Project Advisory Group (PAG) for this project. The PAG meeting is scheduled for Tuesday, August 29, 2017, from 10:00 AM to Noon at the **North Tampa Branch Library, 8916 North Boulevard, Tampa, Florida 33604**.

The West Busch Boulevard Corridor study limits extend from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The study area includes portions of incorporated Hillsborough County and the City of Tampa. The purpose of the Corridor Study is to work with the community, stakeholders and a Project Advisory Group (PAG) to develop a vision and identify needs and issues in the corridor.

The PAG will be an important component of the study. It will serve as a special advisory resource to the FDOT and the consultant team. The PAG participants will help develop a vision for the corridor that will guide proposed short and long-term recommendations. This may include issues related to capacity, traffic operations, safety, access and egress, mobility, freight movements, transit, bicycles, and pedestrian movements.

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220 at least seven (7) working days in advance of the meeting.

If you have any questions about membership in the PAG or require further information concerning this project, please contact Brian Shroyer, Florida Department of Transportation Project Manager at 813-975-6449 or by email at Brian.Shroyer@dot.state.fl.us.

Sincerely,
Brian Shroyer
Multimodal Project Manager
Florida Department of Transportation

Florida Administrative Register Ad:

The Florida Department of Transportation (FDOT), District Seven, is holding a West Busch Boulevard (SR 580) Corridor Study Project Advisory Group (PAG) Meeting for project stakeholders.

DATE AND TIME: August 29, 2017, 10:00 AM to Noon.

PLACE: North Tampa Branch Library, 8916 North Boulevard, Tampa, FL 33604

GENERAL SUBJECT MATTER TO BE CONSIDERED: The Florida Department of Transportation, District Seven, is holding a West Busch Boulevard (SR 580) Corridor Study Project Advisory Group (PAG) Meeting for project stakeholders within the study limits. The limits of the project extend from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The corridor study area includes portions of unincorporated Hillsborough County and the City of Tampa.

The purpose of the Corridor Study is to work with the community, stakeholders and a Project Advisory Group (PAG) to develop a vision and identify needs and issues in the corridor. The PAG will be an important component of the study. It will serve as a special advisory resource to the FDOT and the consultant team. The PAG participants will help develop a vision for the corridor that will guide proposed short and long-term recommendations. This may include issues related to capacity, traffic operations, safety, access and egress, mobility, freight movements, transit, bicycles, and pedestrian movements. A copy of the agenda may be obtained by contacting: Mr. Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us.

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220 at least seven (7) working days in advance of the meeting.

Comuniquese Con Nosotros

Nos importa mucho la opinión del público sobre el proyecto. Si usted tiene preguntas o comentarios, o si simplemente desea más información, por favor comuniquese con nuestra representante, Lilliam Escalera, al (813) 975-6403, Departamento de Transportación de Florida, 11201 N. McKinley Dr., Tampa, FL 33612, Lilliam.Escalera@dot.state.fl.us

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT.

FDOT Public Notices Website:

PROJECT ADVISORY GROUP (PAG) MEETING

District: Seven

Meeting Type: Meeting

Date: August 29, 2017

Time: 10 AM to Noon

Location Name: North Tampa Branch Library

Street Address: 8916 North Boulevard

City: Tampa, FL 33604

Purpose: The Florida Department of Transportation, District Seven, is holding a West Busch Boulevard (SR 580) Corridor Study Project Advisory Group (PAG) Meeting for project stakeholders within the study limits. The limits of the project extend from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The corridor study area includes portions of unincorporated Hillsborough County and the City of Tampa.

The purpose of the Corridor Study is to work with the community, stakeholders and a Project Advisory Group (PAG) to develop a vision and identify needs and issues in the corridor. The PAG will be an important component of the study. It will serve as a special advisory resource to the FDOT and the consultant team. The PAG participants will help develop a vision for the corridor that will guide proposed short and long-term recommendations. This may include issues related to capacity, traffic operations, safety, access and egress, mobility, freight movements, transit, bicycles, and pedestrian movements. A copy of the agenda may be obtained by contacting: Mr. Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us.

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Primary Contact: Brian Shroyer, FDOT Project Manager

Primary Phone: (813) 975-6449

Primary E-mail: Brian.Shroyer@dot.state.fl.us

Additional Contact: Christopher Speese, Public Involvement Coordinator

Additional Phone: (813) 975-6405

Additional E-mail: Christopher.Speese@dot.state.fl.us

Appendix C

West Busch Boulevard (SR 580) Corridor Study

From North Dale Mabry Highway to North Nebraska Avenue

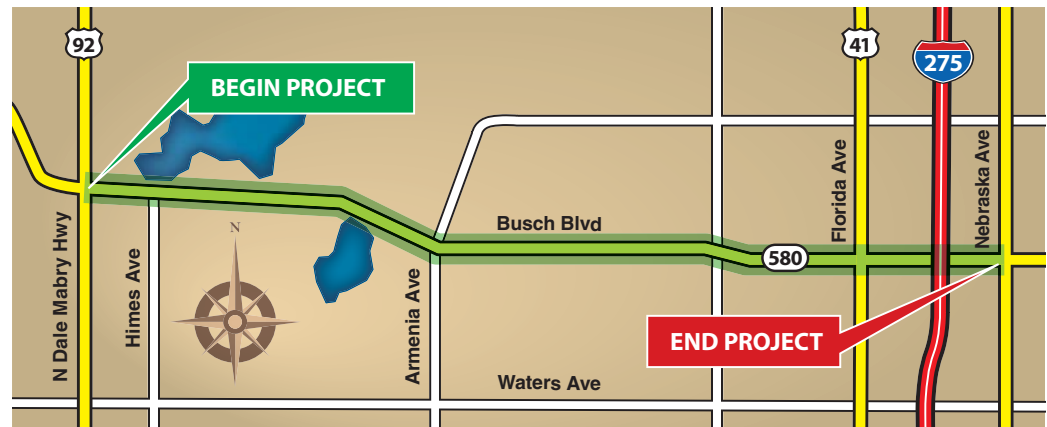
Hillsborough County, FL

Financial Project ID Number: 435908-1-22-01

Corridor Study Begins!

The Florida Department of Transportation (FDOT), District 7 is conducting a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue in Hillsborough County – a distance of approximately 3.3 miles. The West Busch Boulevard (SR 580) corridor is an east-west urban principal arterial connecting and serving local commuters and visitors between North Dale Mabry Highway and Nebraska Avenue. The corridor study area includes portions of unincorporated Hillsborough County and the City of Tampa and is the main route to the Busch Gardens Amusement Park (located approximately 2.15 miles east of the project limits at McKinley Drive).

The corridor supports a variety of land uses and demographics, including schools, churches, large and small businesses with direct access to and from SR 580, transit services, pedestrians, bicyclists and many small communities. The corridor also serves a large commuter population as a connection between Veterans Expressway to the west and I-75 to the east.



The West Busch Boulevard (SR 580) corridor study will use FDOT's Complete Streets policy to showcase how Complete Streets approaches can shape a corridor. The study, which began in March 2017, is expected to continue for approximately 18 months and will include technical work and data analysis, along with various opportunities for the public to be involved in discussing a vision for West Busch Boulevard (SR 580) that makes it accessible for all users.

What is a Corridor Study?

A corridor study is the first step in planning by defining the corridor's needs. A corridor study generally defines the relationships between a roadway and its adjacent land. Corridor studies are also used to: define acceptable levels of access and mobility; determine transportation system needs to support surrounding land uses, and identify operational deficiencies, among other things. This corridor study will include establishing a shared vision that will allow the community to help set project goals and objectives. The vision will guide the project team to develop a set of improvements that leverage local resources, match local conditions and desires, and are consistent with ongoing planning and development efforts.

Project Schedule

The West Busch Boulevard (SR 580) Corridor Study began in March 2017 and is anticipated to be completed in 18 months. Project activities will include:

- Identifying the travel related needs and issues within the West Busch Boulevard project corridor.
- Coordinating with key stakeholders to establish the vision for the corridor and identify solutions to the corridor issues and improve operational efficiency within the West Busch Boulevard corridor.
- Document the vision for all future improvements within the West Busch Boulevard corridor to maximize the “livability” for all users.

ACTIVITY	2017												2018							
	Mar	Apr	May	June	July	Aug	Sept	Oct	Nov	Dec	Jan	Feb	Mar	April	May	June	July	Aug	Sept	
Notice to Proceed - Project Begins	★																			
Develop Public Involvement Plan																				
Mailing List																				
Elected Officials/Agency Kick-off Meeting				★																
Project Advisory Group (PAG) Meetings																				
Corridor Visioning Workshop									★											
Alternatives Public Meeting													★							
Final Corridor Alternatives & Strategies Report																				

Project Advisory Group (PAG)

A Project Advisory Group (PAG), composed of local government and agency representatives, are working with FDOT to identify corridor challenges along with potential solutions, assist with documenting the community goals, and develop a vision for the corridor that will guide the study development and future improvements. The PAG will meet at key points throughout the study to collaborate with the project team to ensure the vision and improvement alternatives are well-coordinated with local planning efforts, adjacent projects, and community goals.

How Can You Get Involved?

The project website is your best source for up-to-date information; you can get access at this link:

www.fdotd7studies.com/westbuschblvd/. In addition to the general study information that is already posted on the website, the project documents will be uploaded to this site as they are developed. However, you can already begin providing your input on this project using WikiMapping! This tool allows you to “pin” comments at specific locations along the corridor. You can pick the type of comment you have to help us understand the issues you observe in the corridor. Some of these issue types include: Bus/Transit, Connectivity, Intersection Issue, Pedestrian, Safety, and Traffic/Parking. If you have a comment on an issue that does not have a specific description, just use the “Other” category. The link to the WikiMapping tool is provided on the project website, but for direct access you can click on the following link:

<http://wikimapping.com/wikimap/West-Busch-Boulevard-Corridor-Study.html>. The project team will utilize this information to help inform the process and guide solutions.

Future opportunities for you to get involved include the upcoming Corridor Visioning Workshop that will be conducted in the Fall 2017. At this workshop, we will gather your input on the existing corridor and document your ideas to resolve some of the issues that you have observed. Our goal is to enlist your help to identify improvements that can be implemented in the West Busch Boulevard corridor to resolve the challenges. In advance of this workshop, a User Preference Survey will be available on the website in October; hard copies of this same survey will be available at the Corridor Visioning Workshop. These surveys will be used by the project team to understand the issues and concerns that you have regarding the corridor.

We want your input! We urge anyone interested in shaping the future of the West Busch Boulevard corridor to actively participate by visiting the website, get informed, document issues in the WikiMapping tool, attend meetings, and provide your ideas.

For more information or to comment, please contact:

Brian Shroyer, Project Manager
11201 N. McKinley Drive
Tampa, FL 33612
Office Phone: (813) 975-6449
brian.shroyer@dot.state.fl.us

Kristen Carson, Public Information Officer
11201 N. McKinley Drive
Tampa, FL 33612
Office Phone: (813) 975-6202 or 800-226-7220
kristen.carson@dot.state.fl.us

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT.

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Comuníquese Con Nosotros

Nos importa mucho la opinión del público sobre el proyecto. Si usted tiene preguntas o comentarios, o simplemente desea más información, por favor comuníquese con nosotros. Nuestro representante, Señora Lilliam Escalera, 813-975-6445, Departamento de Transportación de Florida, 11201 N. McKinley Dr., Tampa, FL 33612, lilliam.escalera@dot.state.fl.us.



PROJECT ADVISORY GROUP OVERVIEW

1. What is a Project Advisory Group?

A Project Advisory Group (PAG) is established early in a corridor study and is composed of key stakeholders representing local cities, counties, agencies, jurisdictions and organizations within the study area that can help ensure that the explicit needs and concerns of the corridor are addressed. This important group serves as a special advisory resource to the Florida Department of Transportation (FDOT) and the project team; they will gather local input and then disseminate information back to the impacted stakeholders and communities to ensure that any improvements within the corridor continue to serve as vital elements of the community's transportation system. The PAG will also provide policy guidance and technical assistance to FDOT and the project team as the common goals and objectives of the study are defined, issues are identified, and potential solutions are developed.

The West Busch Blvd Corridor Study PAG will meet four (4) times over the course of the study and provides an opportunity for the study team to share project information and receive critical input on a regular basis.

2. What is the Role of the PAG?

The PAG is not an independent decision-making board, but is advisory in nature and will assist the FDOT and the consultant team by providing jurisdictional, policy, technical and administrative guidance throughout the study with the following goals in mind:

- Identify the interests of the stakeholders within the project limits.
- Establish relationships and open the lines of communication.
- Work collectively to bring diverse ideas and approaches together.
- Work as a team to achieve the common goals identified during the study.

To maximize success, PAG members should be honest, open-minded, responsive, cooperative and respectful of all group members.

3. What is the Purpose of PAG Meetings?

The purpose of PAG meetings is to bring together the FDOT, the consultant team, and the PAG members to establish an effective project planning or "partnering" team. The PAG will serve as an advisory board to help the FDOT and the study team gather local input and then disseminate information back to the impacted stakeholders and communities. This flow of information will aid in identifying issues, needs, and potential solutions that can be evaluated and documented the final Corridor Study.

4. What is a Corridor Study?

In general, corridor studies are a planning process used within the engineering community to assess existing conditions, identify transportation-related needs for a corridor and develop a preferred future condition. Corridor studies are used to:

- Define acceptable levels of access and mobility,
- Determine transportation system needs to support surrounding land uses,

- Consolidate and control access points,
- Identify safety concerns on the corridor, and
- Identify operational deficiencies and promote operational efficiency

Potential benefits of the West Busch Blvd Corridor Study may include:

- Maximizing efficiency and life-span of existing infrastructure,
- Improved safety conditions,
- Development of coordinated land uses,
- Access or mobility improvements,
- Improvements to traffic operations,
- Pedestrian, bicycle and transit enhancements or
- Air quality improvements through congestion reduction

Components of the West Busch Blvd Corridor Study

Visioning and Consensus – FDOT will work with project stakeholders to develop a shared vision that guides project goals and objectives. A comprehensive understanding of the SR 580 corridor needs and community endorsement are vital to the effectiveness of the planning, design and implementation processes.

Planning and Conceptual Design – Communities leverage local resources and knowledge to assist in guiding project activities to best meet the needs of their community. Tailoring best practices to match up with local conditions and desires will assist in developing a successful and implementable planning study.

5. Public Involvement for the Corridor Study

Several meetings will be held with community groups during the Corridor Study.

Elected Officials and Agency Kickoff Meeting: This meeting will be held in June 2017. The purpose of the Elected Officials and Agency Kickoff meeting is to provide an opportunity for the FDOT and the study team to introduce the West Busch Blvd Corridor Study and to solicit participation on the PAG.

Corridor Visioning Workshop: This meeting will be held in November 2017 to solicit input from the public on the goals and objectives of the study, and to receive input on the full range of potential improvements to be evaluated.

Alternatives Public Workshop: This meeting will be held in March 2018 to present the public the results of the study to date and provide an opportunity to express concerns, ask questions, state preferences, and make comments that will be included in the project files.

Project Advisory Group (PAG): This group is comprised of agency and community representatives, providing a forum for local experts to share their local knowledge about ongoing planning efforts; past successes and failures that may impact this study; and existing issues and concerns along the project corridor. The PAG will meet at key points throughout the study to collaborate with the project team to ensure that the corridor vision and improvement alternatives are well-coordinated with local planning efforts and community goals.

Informal Meetings: Informal meetings will be held in addition to scheduled meetings, where participation in other meetings with the public, elected officials, special interest groups or public agencies is expected to occur. The purpose of these meetings will be to apprise the attendees of the project status, specific location and design concepts, and to receive input.

6. Government and Agency Coordination

Local, regional, state and federal governments, agencies, businesses and organizations having potential impacts or concerns due to jurisdictional review or expressed interest have been identified and contacted. Coordination will continue with these groups throughout the project process.

7. Contact Information

Brian L. Shroyer
Multimodal Project Manager
Florida Department of Transportation
11201 N. McKinley Drive, MS 7-500
Tampa, FL 33612
813-975-6449
Brian.Shroyer@dot.state.fl.us

Kristen Carson
Public Information Officer
Florida Department of Transportation
11201 N. McKinley Drive, MS 7-110
Tampa, FL 33612
813-975-6060
Kristen.Carson@dot.state.fl.us

8. Project Facts

A. West Busch Blvd Corridor Study and Project Description

The Florida Department of Transportation, District Seven (D7) has identified the need to analyze, evaluate, and document travel-related problems, needs and issues along SR 580/Busch Boulevard from North Dale Mabry Highway to North Nebraska Avenue. The West Busch Blvd Corridor Study will be conducted to identify the cause and characteristics of the transportation issues within the corridor; these may include capacity, traffic operations, safety, access and egress, freight movements, transit, bicycles and pedestrian movements. The study will document both the current and forecasted-future travel conditions and needs and identify workable solutions for those needs.

Concurrent with the study, a Resurfacing, Restoration and Rehabilitation (RRR) design project will proceed under FPID 437530-1-32-01; the limits of the RRR design is SR 580 from East of Armenia Avenue to West of Florida Avenue, a distance of approximately 1.3 miles. The RRR improvements include milling and resurfacing, replacement of sidewalk where deficient and filling in missing gaps where existing right of way is available.

B. Project Location Map



C. West Busch Blvd Corridor Study Purpose and Need

The purpose of this study is to:

1. Determine and document the existing challenges in the corridor, including: existing and projected development, travel-related problems, congestion, access, operational efficiency, safety, pedestrian and bicycle connectivity. This includes soliciting input from the local users and stakeholders.
2. Develop a long-term vision for the corridor that defines the goals and objectives and policy requirements that harmonize with Complete Streets and Vision Zero.
3. Address near-term needs by identifying workable solutions for travel needs, consistent with the long-term vision that is established.

Improvements to West Busch Boulevard (SR 580) are needed to: address congestion in the corridor; improve the level of service at the intersections; reduce the number vehicle/pedestrian/bicycle crashes; and support accessibility to transit. This effort to improve mobility and enhance safety in the corridor will be achieved by developing several alternatives, including near-term improvements such as addition of bike lanes and closing side-walk gaps, where feasible; evaluating turning movement efficiency and access management. Long-term solutions could include addition of continuous sidewalks and bike lanes, if feasible within the context of the corridor.

D. Project Schedule

The West Busch Boulevard (SR 580) study began in March 2017 and is anticipated to be completed in 18 months. The project activities include collecting information, defining a vision for the corridor and developing roadway improvement alternatives. Please see the project schedule below:

SR 580/Busch Boulevard from North Dale Mabry Highway to North Nebraska Avenue Corridor Study																			
PUBLIC INVOLVEMENT PLAN SCHEDULE																			
ACTIVITY	2017											2018							
	Mar	Apr	May	June	July	Aug	Sept	Oct	Nov	Dec	Jan	Feb	Mar	April	May	June	July	Aug	Sept
Notice to Proceed - Project Begins	★																		
Develop Public Involvement Plan																			
Mailing List																			
Elected Officials/Agency Kick-off Meeting					★														
Project Advisory Group (PAG) Meetings																			
Corridor Visioning Workshop																			
Alternatives Public Meeting																			
Final Corridor Alternatives & Strategies Report																			

Learn more about the project here: www.fdotd7studies.com/westbuschblvd/

Project Advisory Group Meeting #1 West Busch Boulevard (SR 580) Corridor Study

from North Dale Mabry Highway to North Nebraska Avenue
Hillsborough County, Florida
FDOT Financial Project ID Number: 435908-1-22-01
North Tampa Branch Library
August 29, 2017 – 10:00 am to 12:00 pm



Agenda for Project Advisory Group Meeting #1

- Introductions
- Existing Roadway and Corridor Characteristics
- Developing the Corridor Vision
- Next Steps
- Questions
- Adjourn

Purpose of the SR 580 Corridor Study

- Define existing conditions
- Identify transportation-related needs
- Define acceptable levels of access and mobility
- Determine transportation system needs to support surrounding land uses
- Consolidate and control access points
- Identify safety concerns
- Identify operational deficiencies and promote operational efficiency

Introduction to PAG #1 Meeting





Introductions

- State your name
- Who do you represent
- What your interest is in the SR 580 corridor

PAG Roles and Responsibilities

- Attend as many meetings as possible
- Clearly and openly articulate your interests
- Listen to varying points of view
- Productively discuss issues with members who may hold divergent views
- Actively generate and evaluate potential improvement options
- Serve as a two-way conduit of information
- Keep others informed of the PAG's efforts

Goals for PAG #1

- Understand roadway characteristics
- Compare EO/Agency Kick-off comments with potential solutions
- Understand existing Context Classification and ground-truth future expectations
- Provide input on User Preference Surveys

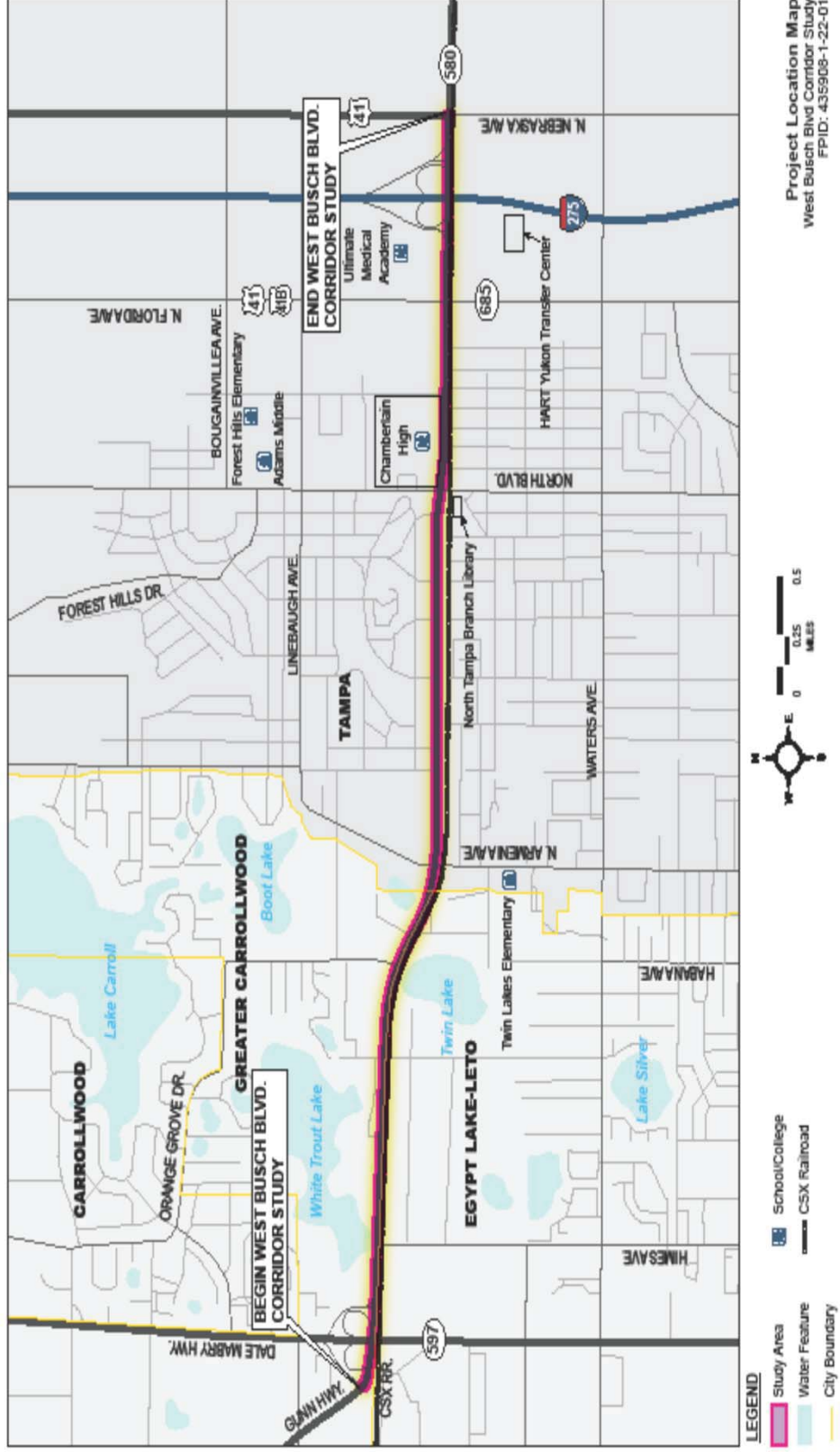
Future PAG meetings

- PAG #2
 - ≈ October 2017
 - ≈ User Preference survey; visioning
- PAG #3
 - ≈ January 2018
 - ≈ Purpose and need; range of solutions
- PAG #4
 - ≈ June 2018
 - ≈ Report on final near-term and long-term decisions

Existing Roadway and Corridor Characteristics



Project Information Location Map



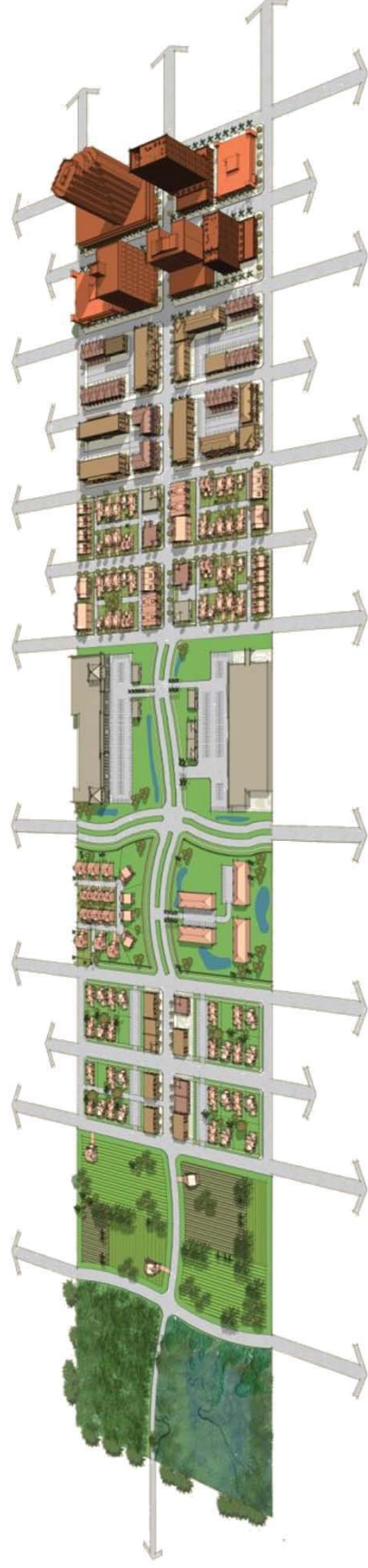
Project Information - FDOT's context classifications

Street Connectivity

- Block Length
- Block Perimeter
- Intersection Density

Development Form and Intensity

- Building Placement
- Presence of Fronting Uses
- Location of Off-Street Parking
- Land Uses
- Building Height



C1-Natural C2-Rural C2T-Rural Town C3R-Suburban Residential C3C-Suburban Commercial C4-Urban General C5-Urban Center C6-Urban Core

Existing context classification

C3R-SUBURBAN RESIDENTIAL



Mostly **residential** uses within large blocks and a disconnected or **sparse roadway network**.

C4-URBAN GENERAL



Mix of uses set within small blocks with a **well-connected roadway network**. The roadway network usually **connects to residential neighborhoods** immediately along the corridor or on the back side of blocks fronting the roadway.

C3C-SUBURBAN COMMERCIAL



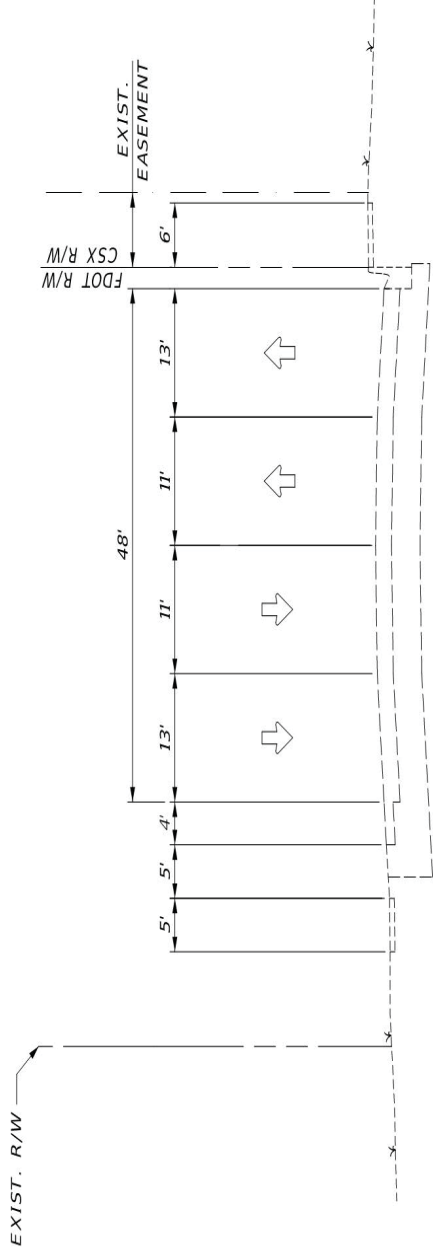
Mostly **non-residential** uses with **large building footprints** and **large parking lots** within large blocks and a disconnected or **sparse roadway network**.

C3R-Suburban Residential – Dale Mabry Hwy to Armenia Ave.



C3R-SUBURBAN RESIDENTIAL

Mostly **residential** uses within large blocks and a disconnected or **sparse roadway network**.

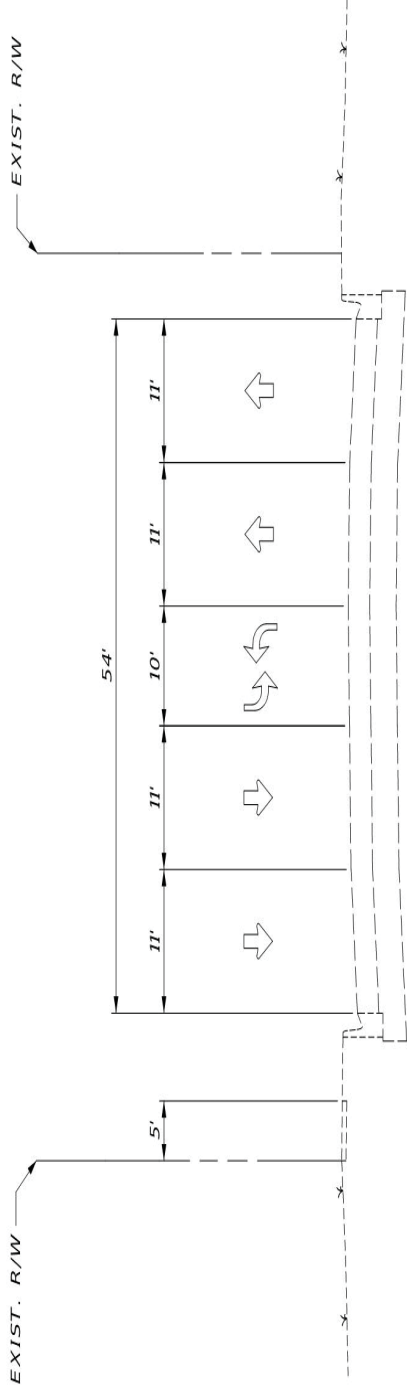


C4-Urban General – Armenia Ave to North Blvd

C4-URBAN GENERAL



Mix of uses set within small blocks with a **well-connected roadway network**. The roadway network usually **connects to residential neighborhoods** immediately along the corridor or on the back side of blocks fronting the roadway.



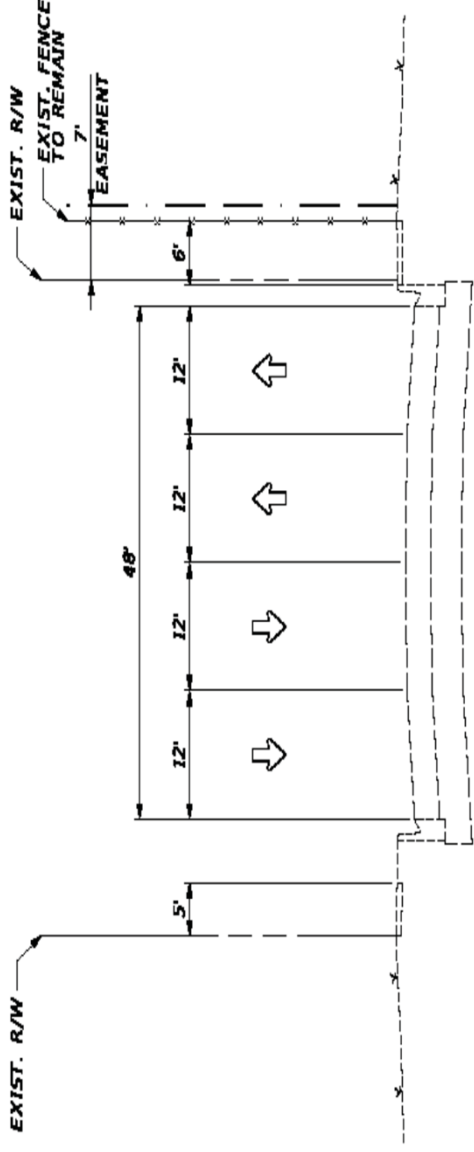
C3C Suburban Commercial – North Blvd to N Nebraska Ave.

C3C-SUBURBAN COMMERCIAL



Mostly **non-residential** uses with **large building footprints** and **large parking lots** within large blocks and a disconnected or **sparse roadway network**.

Section 1: North Blvd to Florida



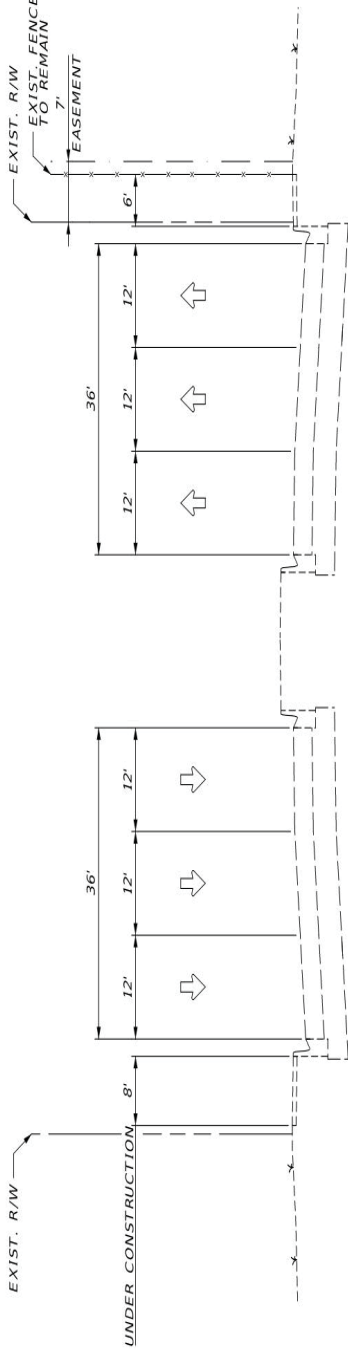
C3C Suburban Commercial – North Blvd to N Nebraska Ave.

C3C-SUBURBAN COMMERCIAL

Section 2: Florida to Nebraska



Mostly **non-residential** uses with **large building footprints** and **large parking lots** within large blocks and a disconnected or **sparse roadway network**.



Why does context classification matter?

- Projects will be evaluated using **future context**
- Future context should be clearly and consistently documented in local policies
- Local form-based codes and zoning can be used to inform FDOT's context classification determination
- On-street parking is a key element in C4, C5 and C6 & where it is not present could be added per local plan, for speed management, or to increase available parking

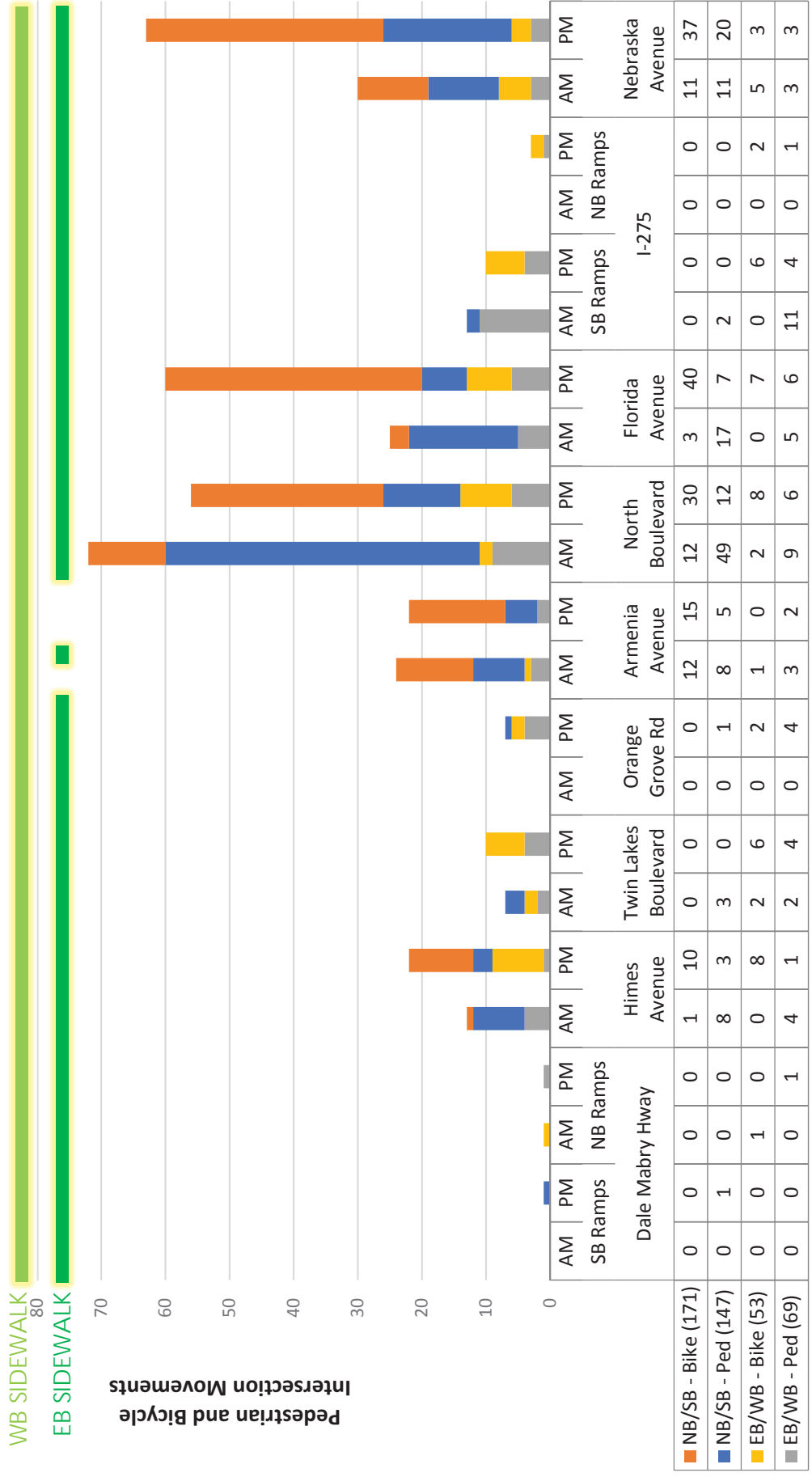
Context Classification	Allowable Design Speed Range for Non-SIS (mph)
C1 – Natural	55-70
C2 – Rural	55-70
C2T – Rural Town	25-45
C3 – Suburban	35-55
C4 – Urban General	30-45
C5 – Urban Center	25-35
C6 – Urban Core	25-30

Take Away from Existing Context Classification information

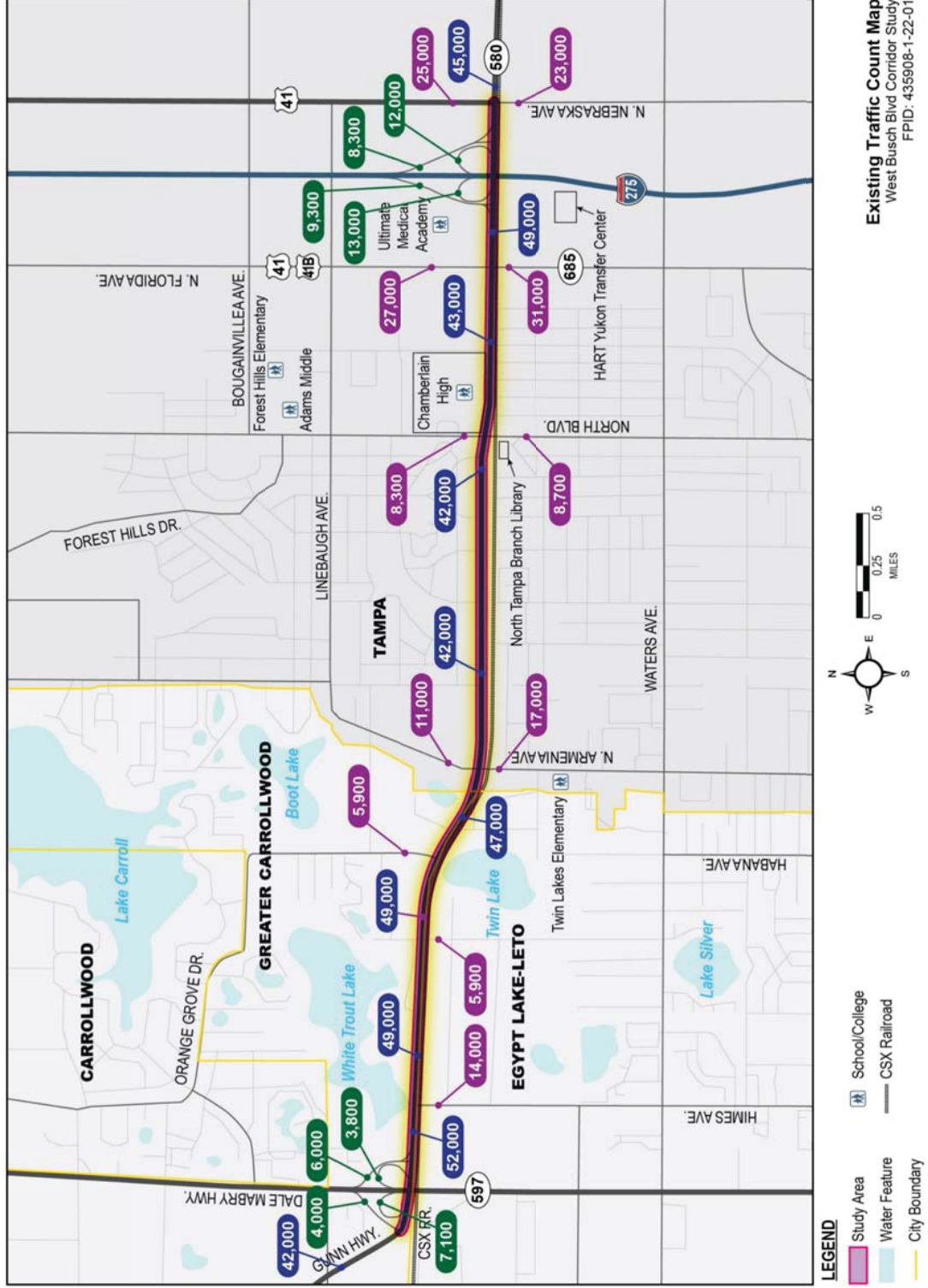
- No bike lanes throughout corridor
- Sidewalk gaps on South side of SR 580
- There is latitude in Context Classifications
- Projects are evaluated on FUTURE context

Pedestrian/Bicycle Counts

This slide illustrates the locations with the heaviest bike/ped traffic (based on peak 2-hour counts [1 day only] – from VHB AADT memo)



Existing Traffic Counts



Traffic Volume Considerations

STANDARDS FOR LOW AND HIGH VOLUME HIGHWAYS IN ANNUAL AVERAGE DAILY VOLUMES

HIGHWAY TYPE	LOW VOLUME AADT	HIGH VOLUME AADT
FREEWAY - URBAN		
4-LANE FACILITY	57,000	69,000
6-LANE FACILITY	86,000	103,000
8-LANE FACILITY	114,000	138,000
FREEWAY - RURAL		
4-LANE FACILITY	46,000	56,000
6-LANE FACILITY	69,000	83,000
8-LANE FACILITY	92,000	111,000
ARTERIALS - URBAN		
2-LANE FACILITY	16,000	20,000
4-LANE FACILITY	37,000	43,000
6-LANE FACILITY	55,000	64,000
8-LANE FACILITY	69,000	80,000
ARTERIALS - RURAL		
2-LANE FACILITY	9,000	14,000
4-LANE FACILITY	38,000	47,000
6-LANE FACILITY	58,000	71,000
COLLECTOR - URBAN		
2-LANE FACILITY	11,000	16,000
4-LANE FACILITY	37,000	45,000
COLLECTOR - RURAL		
2-LANE FACILITY	8,000	13,000
4-LANE FACILITY	30,000	38,000

SR 580 Existing AADT

Minimum = 42,000

Maximum = 52,000

Source: FDOT PPM, Glossary of Terms



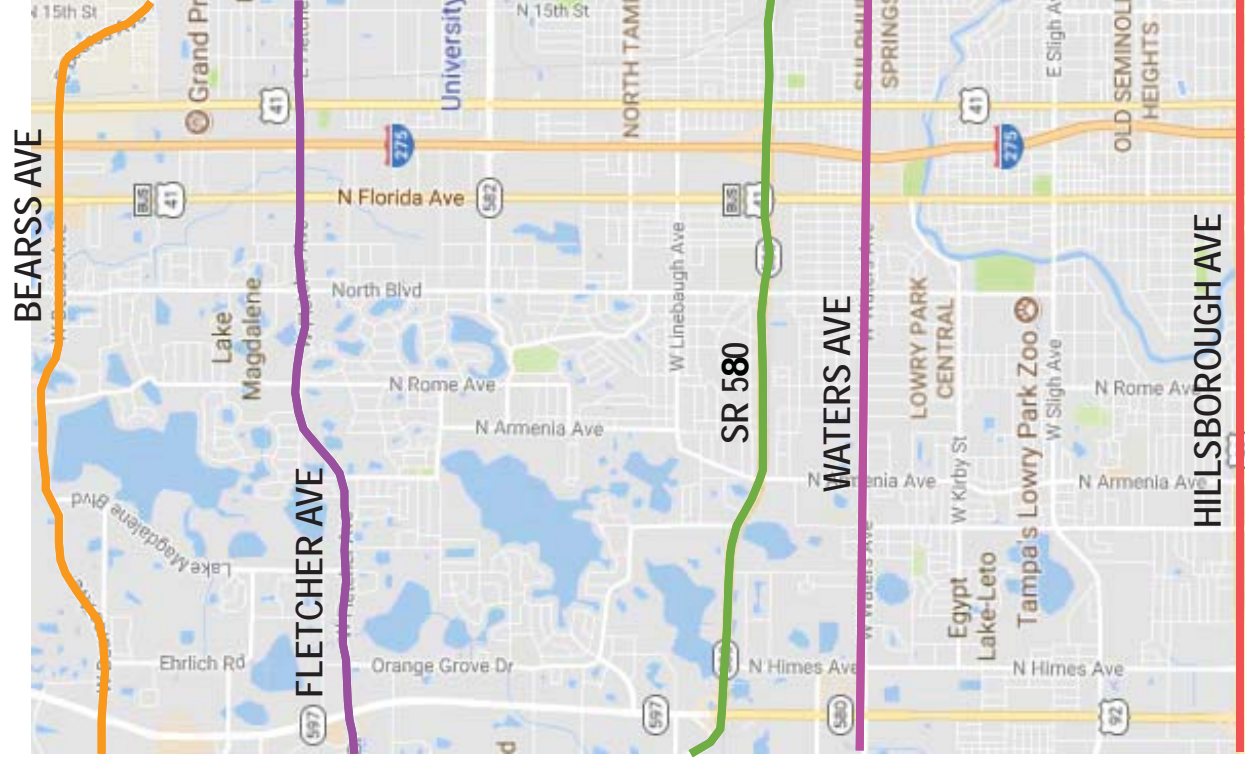
Adjacent East-West Corridor Considerations

Corridor	Lanes	AADT	Notes
Bearss Avenue	4-lane	54,000	3.8 miles north
Fletcher Avenue	4-lane	23,500	Limited western connectivity
SR 580	4-lane	50,000	
Waters Avenue	4-lane	27,000	Limited eastern connectivity
Hillsborough Avenue	6-lane	51,500	2.6 miles south

SR 580 Existing AADT

Minimum = 42,000

Maximum = 52,000



Roundabout Consideration

- Maximum design year total traffic volume entering an intersection is:
 - ≈ 25,000 AADT for single-lane roundabout
 - ≈ 45,000 AADT for two lane roundabout



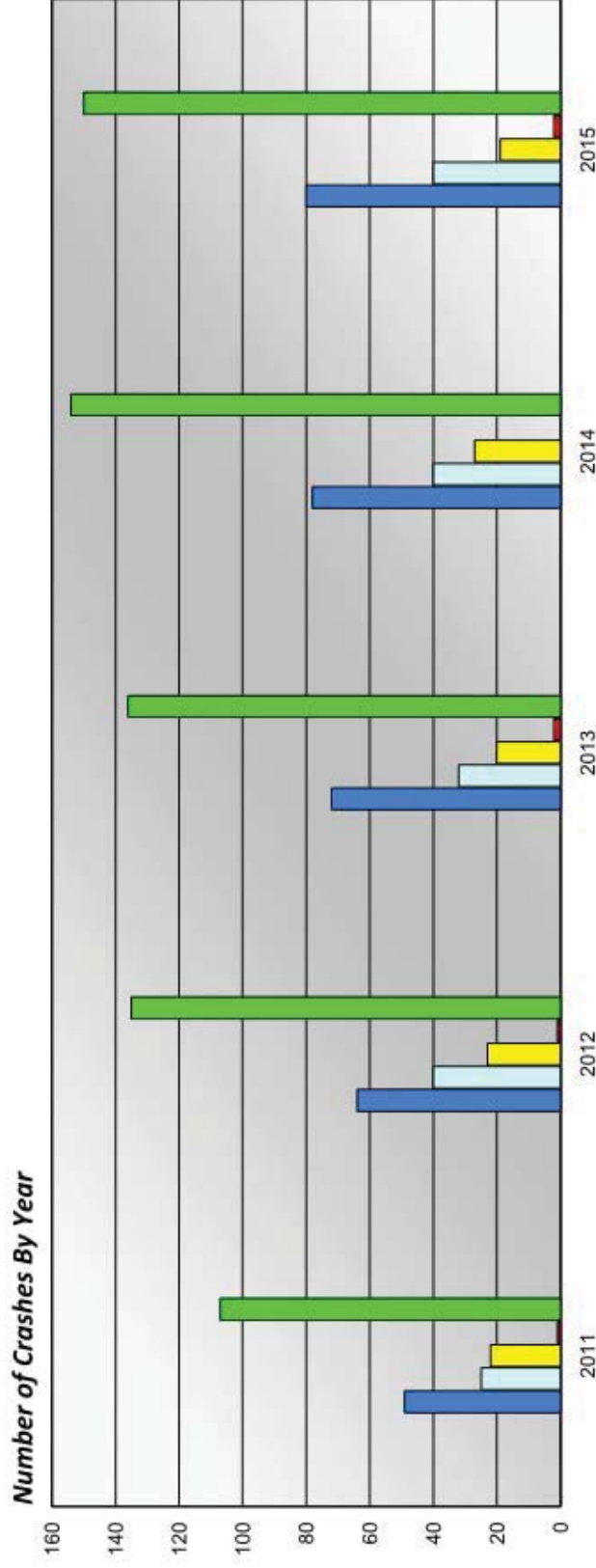
SR 580 Existing AADT
 Minimum = 42,000
 Maximum = 52,000

Intersection	Entering AADT (Existing)
Dale Mabry/SB Ramps	49,100
Dale Mabry/NB Ramps	58,000
Himes	57,500
Twin Lakes	54,900
Orange Grove	50,950
Armenia	58,500
North	51,000
Florida	75,000
I-275/SB Ramps	60,150
I-275/NB Ramps	58,150
Nebraska	60,900

Take Away from existing Counts

- Bike/Ped movement N-S exceeds E-W movement.
- Lane reduction not viable:
 - ≈ The existing traffic is either within or just above the range for 4-lane facility Traffic Volumes.
 - ≈ Adjacent corridors are already at or above capacity and too far away.
- Round-About configuration not viable: within SR 580 corridor exceeds the maximum traffic volume requirements for a round-about and impacts the RR on the south.

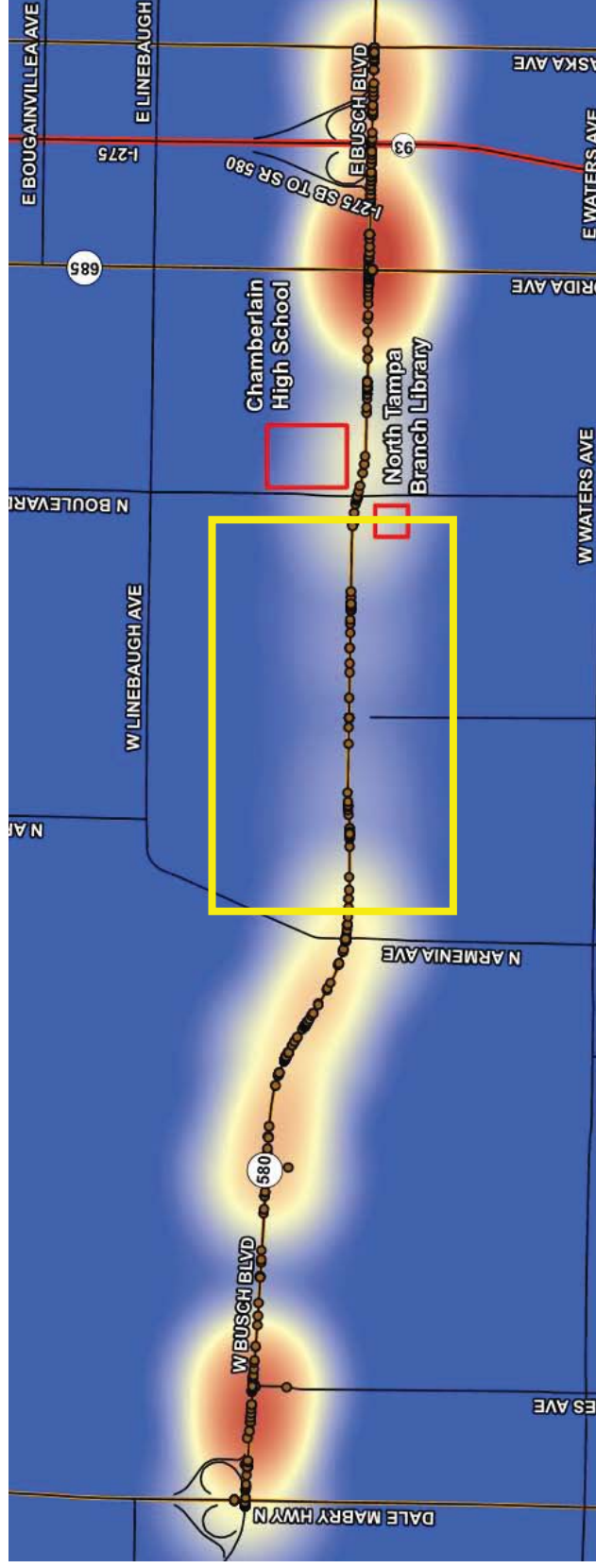
Latest Crash History by Year



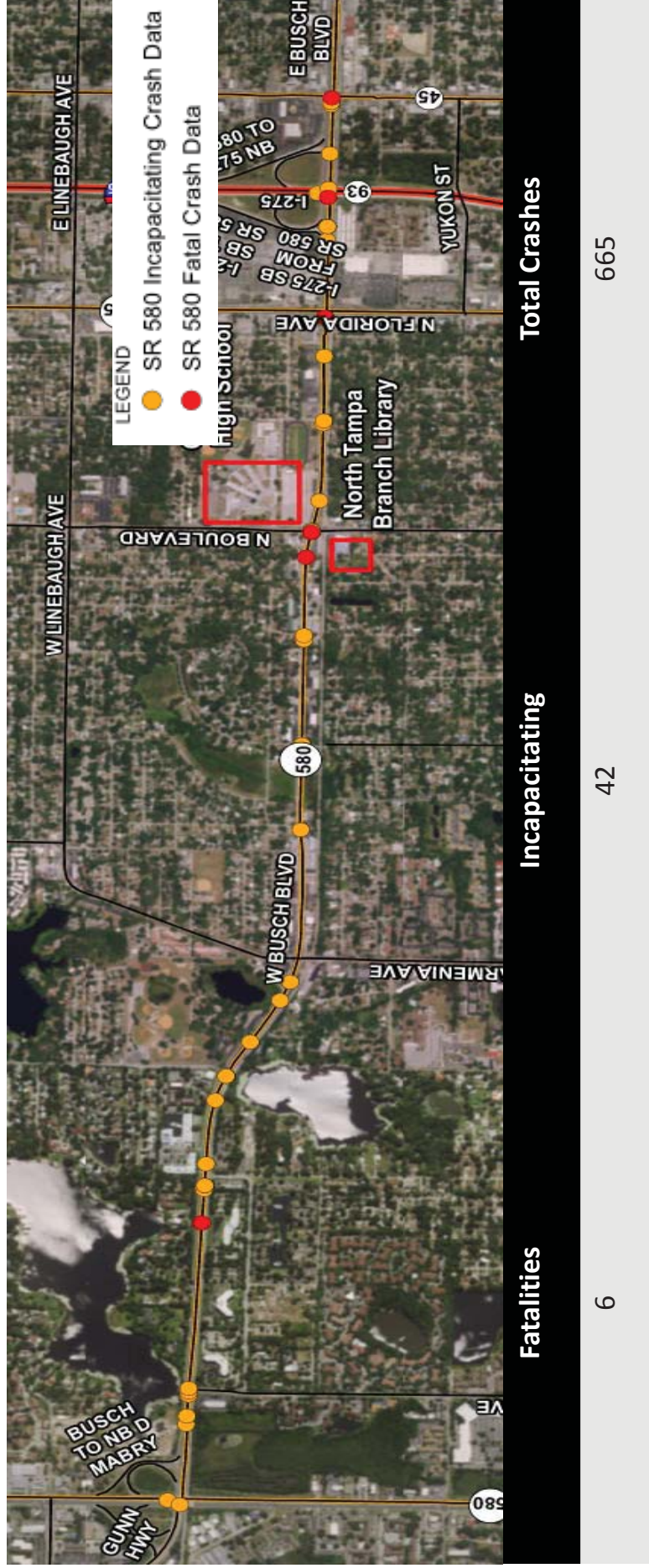
Breakdown of Crashes by Year

	2011	2012	2013	2014	2015
PDO	49	64	72	78	80
Injury Crashes	57	70	62	76	68
Fatal Crashes	1	1	2	0	2
Total Crashes	107	135	136	154	150

Crash-prone areas - heat map



Incapacitating and Fatal Crashes



Take Away from Crash History Data

- Crashes have increased.
- Highest “heat” intensity is at Himes and between North Blvd and Nebraska.
- The 5-lane section has a lower frequency and severity of crashes compared to the 4-lane section.
- Five of the six fatalities occurred between North Blvd and Nebraska

Developing the Corridor Vision



Kick-Off meeting response to: How
would you describe the West Busch
Boulevard corridor?

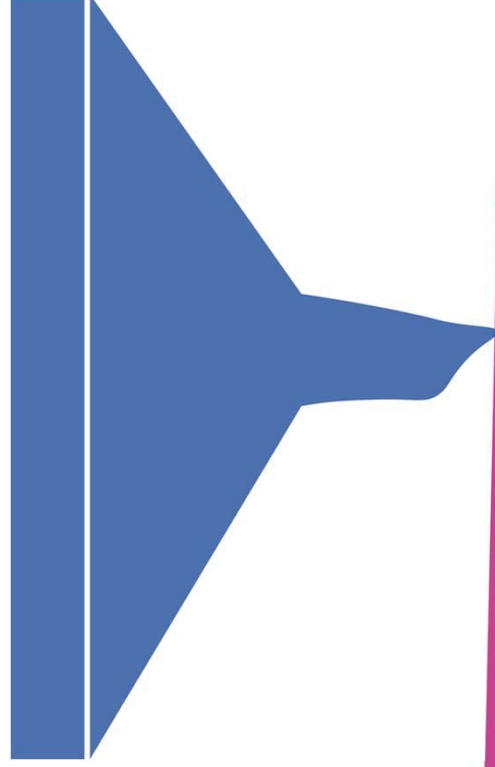


Kick-off meeting response to: Challenges & ideas for the corridor?

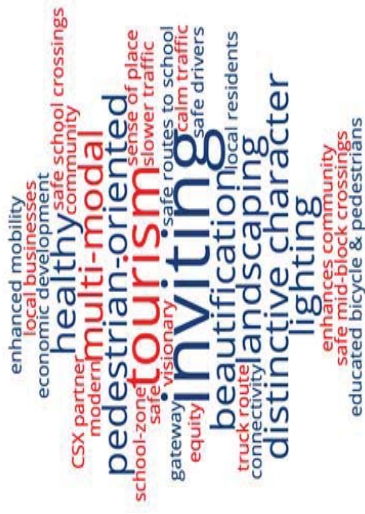


Activity - Goals & objectives to accomplish the vision

enhanced mobility
local businesses
economic development
healthy community
safe school crossings
pedestrian-oriented
multi-modal
sense of place
slower traffic
school-zone safe
tourism
visionary
safe routes to school
calm traffic
safe drivers
gateway equity
beautification
local residents
truck route connectivity
landscaping
distinctive character
lighting
enhances community
safe mid-block crossings
educated bicycle & pedestrians



Activity – Begin process to identify the Vision for West Busch Blvd. Corridor



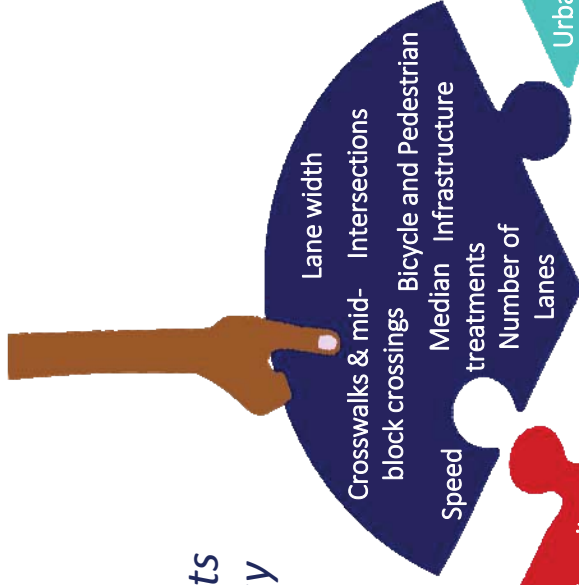
A safe and vibrant corridor that meets the mobility and connectivity needs of all users, regardless of their transportation mode of choice.

The corridor’s distinctive character is a source of pride for local residents and businesses, in addition to serving as an inviting gateway to Busch Gardens for visitors from near and far.

How do we get there?

FDOT

*invests in improvements
within the right-of-way*



Current & Future

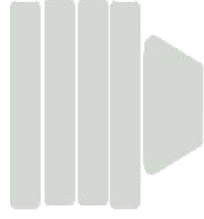
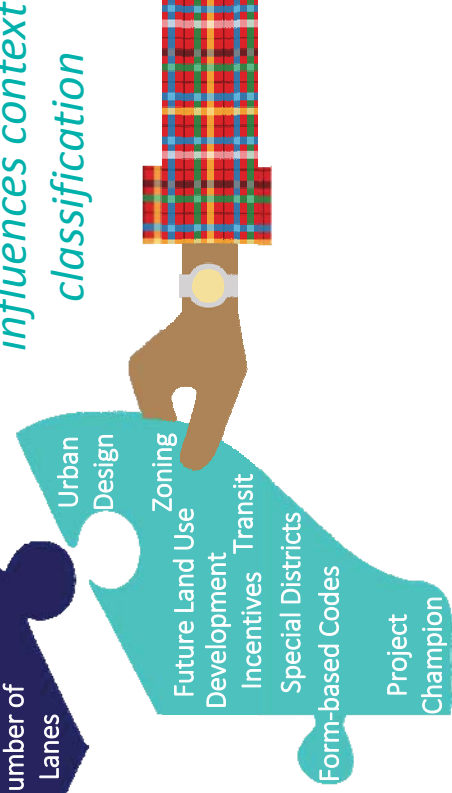
Corridor Users

define mobility needs



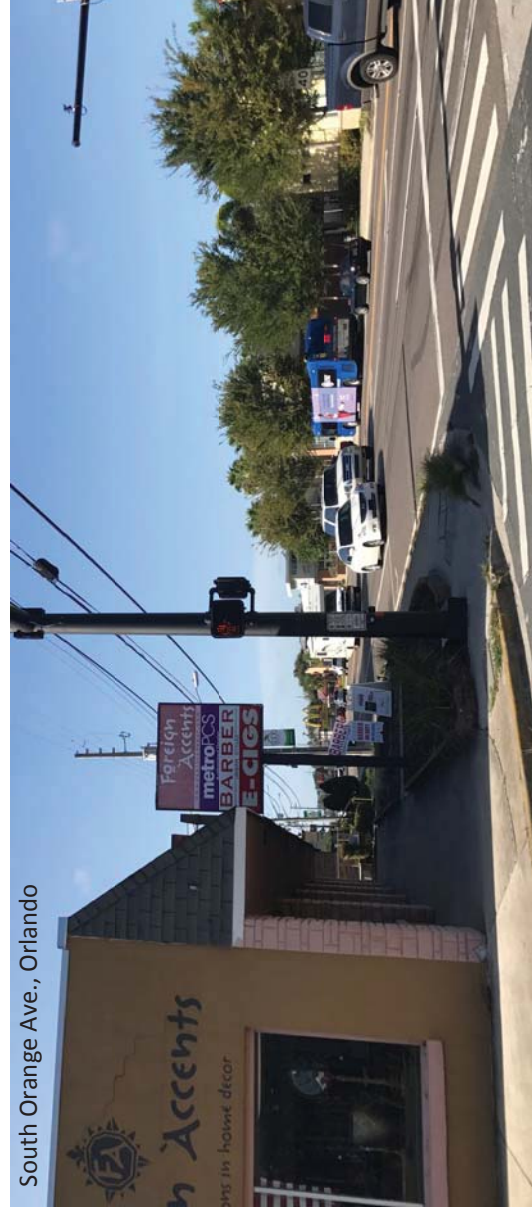
Local

Government
*influences context
classification*



Vision for the future condition?

- Consensus that context classification will remain the same?
 - ≈ Urban form, setbacks and other land development policies can encourage pedestrian-scale environment

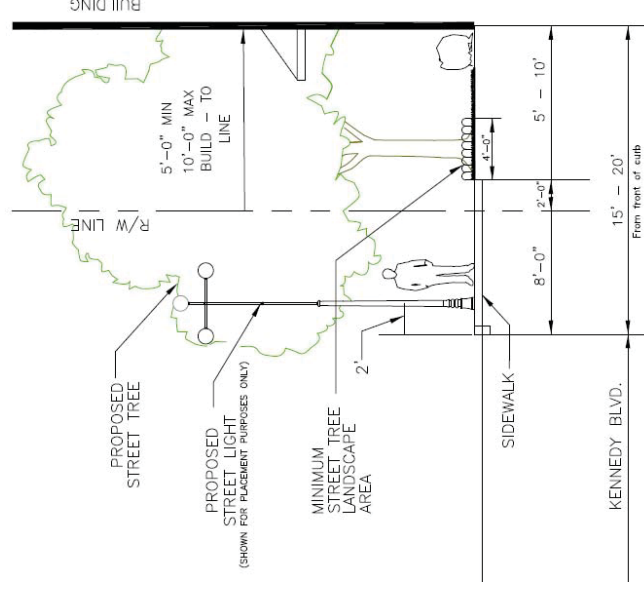


Note: City of Tampa Code of Ordinances Sec. 27-156 Table 4-2 requires 10 ft. front yard (setback) for properties zoned Commercial General,

Coordinated land development & redevelopment policies can complement roadway investments

- Any documented County &/or City form-based codes, overlay districts, right-of-way preservation plans, etc. for the study area to reinforce pedestrian-oriented urban form?
- Are any additional stakeholders needed to help understand this collective future?

SIDEWALK ADJACENT TO ROAD
CROSS SECTION
SCALE: 1" = 5'



Kennedy Blvd. Overlay District Streetscape Guidelines

Activity - Developing the West Busch Blvd User Preference Survey

Include?:

- Bike Lanes
- More/Wider Sidewalks
- On-street Parking
- Mid-block RRFB Crosswalk
- Street Trees
- Lower Speed Limit
- School Zone
- Pedestrian Scale Lighting
- Streetscape/Beautification
- Landscaped Median
- What else?

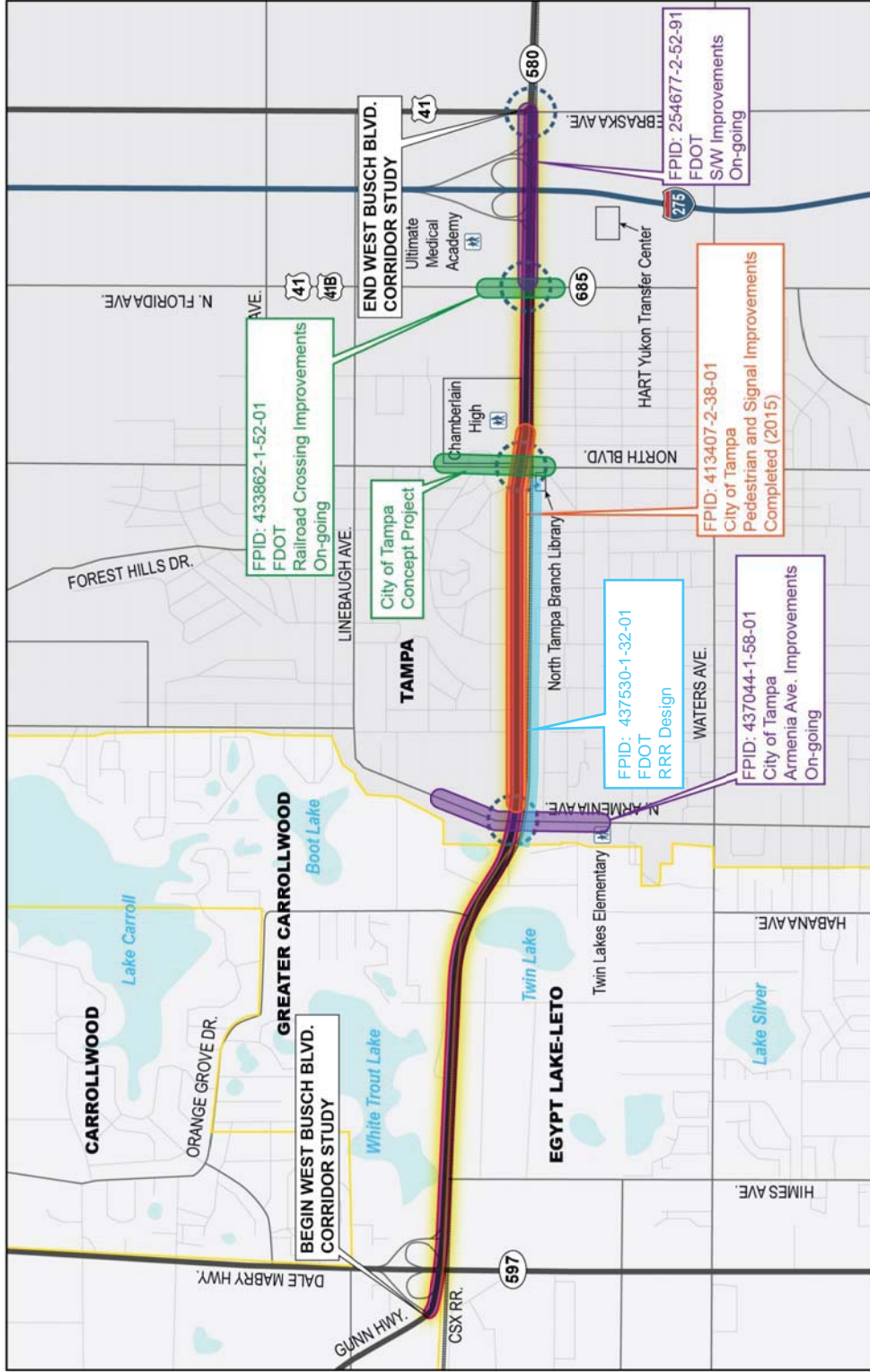
Don't Include:

- Reduced Lanes (Road Diet)
- Roundabouts
- Shared use path

Next Steps



Adjacent Project graphic and list



LEGEND

- Study Area
- Water Feature
- City Boundary
- School/College
- CSX Railroad
- Potential FDOT Lighting Project - Future



Related Projects Map
 West Busch Blvd Corridor Study
 FPID: 435908-1-22-01





Next Steps for the Project Team

- Finalize Draft User Preference Survey
- Evaluation and documentation of improvement options
- Further development of the Visioning statement
- Stakeholder meetings and documentation

Next Steps for the PAG

- Discuss the project in your sphere of influence
- Direct interested parties to the website to provide comments
- Direct interested parties to the website to “pin” issues in the corridor using the WikiMapping interactive tool.
- Provide feedback to the project team regarding your conversations with interested parties.

Next Steps - Schedule

SR 580/Busch Boulevard from North Dale Mabry Highway to North Nebraska Avenue Corridor Study

PUBLIC INVOLVEMENT PLAN SCHEDULE

ACTIVITY	2017												2018							
	Mar	Apr	May	June	July	Aug	Sept	Oct	Nov	Dec	Jan	Feb	Mar	April	May	June	July	Aug	Sept	
Notice to Proceed - Project Begins	★																			
Develop Public Involvement Plan																				
Mailing List																				
Elected Officials/Agency Kick-off Meeting					★															
Project Advisory Group (PAG) Meetings																				
Corridor Visioning Workshop																				
Alternatives Public Meeting																				
Final Corridor Alternatives & Strategies Report																				

Questions???



Appendix D



PROJECT ADVISORY GROUP (PAG) MEETING #1

West Busch Boulevard (SR 580) Corridor Study

From North Dale Mabry Highway to North Nebraska Avenue

Financial Project ID Number: 435908-1-22-01



Tuesday, August 29, 2017 • North Tampa Branch Library, 8916 North Boulevard, Tampa, Florida 33604

Please provide your comments below. If more space is needed, please use the back of this sheet or an additional sheet of paper. You may place your comments in the "Comment Box" provided at the meeting, or send to Brian Shroyer, Project Manager at the address on the back of this comment form, by September 8, 2017. Emailed comments are also acceptable to Brian.Shroyer@dot.state.fl.us

*All comments are part of the project record and are available for review by the public and the media.

Any decisions/plans about changes along West Busch Blvd., from N. Dale Mabry Hwy to N. Nebraska Ave. in Hillsborough Cnty, need to consider the highest ease of use for patrons of the H. A. R. T. bus number 391 that runs on Busch Blvd. I live north of Busch Blvd on N. Nebraska Ave. at Boughavilla St. I use the bus transfer point at N. Nebraska Ave and Busch Blvd. and get off the bus at Busch Blvd and North Ave. so I can use the North Tampa library at 8916 North Blvd. Remember, bus patrons, like myself are on foot trying to avoid getting injured by inattentive motorists. I am a retired teacher. I would be delighted to serve as a senior citizen and H.A. R. T. bus patron in any focus group or public advisory council. Please contact me. This project is very important. I can help recruit members of the public for your effort to hear the voices of pedestrians and senior/disabled students who do not drive.

CONTACT INFORMATION (PLEASE PRINT)

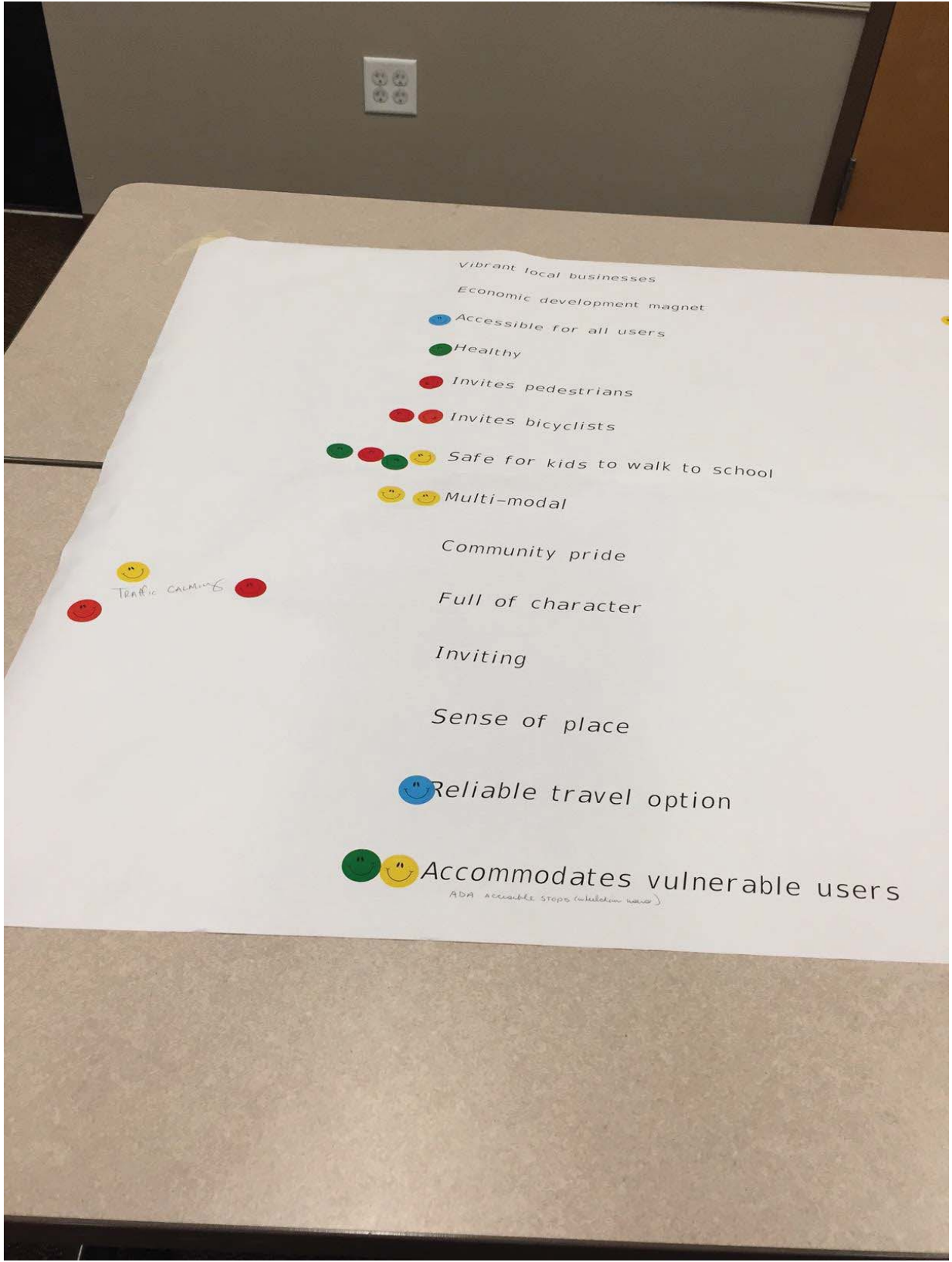
RETURN COMMENTS BY SEPTEMBER 8, 2017 TO:

Name Amy E. Clark
Title patron user of HART buses
Agency/Organization North Tampa Library
Phone Number 774-273-1544
Email quaker.lady@yahoo.com

Brian L. Shroyer
Multimodal Project Manager
Florida Department of Transportation
11201 N. McKinley Drive, MS 7-500
Tampa, FL 33612
Brian.Shroyer@dot.state.fl.us


Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status.


Appendix E




vibrant local businesses





Economic development magnet

 Accessible for all users

 Healthy

 Invites pedestrians

  Invites bicyclists

    Safe for kids to walk to school


  Multi-modal

Community pride



Full of character

Inviting


Sense of place

 Reliable travel option

  Accommodates vulnerable users
ADA accessible stops (wheelchair users)


Traffic Calming 

Accessible for Emergency Services


 Partnership with CSX to share space

 Good commuter route


Safely accommodates truck deliveries

 Landscaped corridor

 Safe conduit for cyclists



 Options to cross between signalized intersections

 Safe mid-block crossings



 School Zone is enforced so don't speed

Supports High School sports fans

 Lighting helps alert drivers to pedestrians at cross walks
Lighting for Bus stops

 Same business access 

Left turn lanes are long enough

  No more two-way left turn lanes

Appendix F

DRAFT User Preference Survey
FPID 435908-1-22-01
SR 580 Corridor Study from Dale Mabry Hwy to Nebraska Ave

1. Please provide your zip code: _____
2. How do you use West Busch Blvd? Check all that apply
 - a. I live on or near the corridor
 - b. I work on or near the corridor
 - c. I travel through Busch Blvd corridor to get onto or off of Dale Mabry Hwy.
 - d. I travel through Busch Blvd corridor to get onto or off of I-275.
 - e. I drive through the corridor to get to work.
3. Do you own a car?
 - a. Yes
 - b. No
4. How do you move through the Busch Blvd corridor?
 - a. Walk
 - b. Ride a bicycle
 - c. Drive a car
 - d. Catch a ride with others
5. What are the biggest challenges you notice when you travel through the West Busch Blvd Corridor?

	Very Important	Somewhat Important	Not Important	No Opinion
Turning at signalized intersection				
Turning at non-signalized intersections				
Turning at Driveways				
Waiting for pedestrians to clear the intersection				
Access to businesses and other uses along the corridor				
Waiting for the train				
Motorist behavior (Speeding, Red Light Running, Aggressive behavior, etc.)				
Drivers using the middle dual-left turn lane to get around traffic				
Traffic Congestion/ Delays				
Pedestrians darting across traffic				

6. What do you think are the most important improvements the Project team should investigate?

	Very Important	Somewhat Important	Not Important	No Opinion
Bike lanes on Busch Blvd				
Sidewalks along both sides				
Quicker commute through corridor				
Safer crossing for pedestrians				
Mid-Block crossing between signalized intersections				
Slowing the traffic down				
Close the median to manage the access				
More lighting in the corridor				
School Zone established at Chamberlain HS				
Landscape along sidewalks				
Landscape in median				



SUPERFLEXIBLE EASELING
TARTELA FLEXIBLE PARA Pizarra
SUPERFLEXIBLE EASELING
SUPERFLEXIBLE EASELING



30

30 B/P
18.2 cm x 7.62 cm
1.2 g/PT (0.43 oz)

- ZIP CODE - HOME & WORK
- Q2 - ATTEND SCHOOL (ADD)
- ~~Q2~~ Q2 - COMBINE D & E?
- ~~Q2~~ DIRECT QUESTIONS TO NON-MOTORIZED
- HOW? MODE OF TRANSPORT?
- REMOVE Q3 → FOLLOW-UP - How SAFE DO YOU FEEL USING THIS MODE? ← COMFORTABLE, SAFE, CONVENIENT
- # 2 - WHY - LIVE, WORK, SHOP, ETC. ← ASK IF PASSENGER
- # 4 - HOW - WALK, TRANSIT, BIKE, CAR, CARPOOL
 WHAT MAIN MODE THAT APPLY OF TRANSPORT ← CHECK ALL MAIN MODES THAT APPLY
 HART BUS & SCHOOL BUS
- CH. HS - PARENT LEAK, PEACH JAR - COMMUNICATION
- CHALLENGES - 1 TO 5 SCALE - MOST TO LEAST
- ALL MODES IN Q5
- SHOULD WE PROHIBIT LEFT ONTO OLA (QUEUING)
- ADD OPEN-ENDED "COMMENT" FIELD

Section 2.2

PAG Meeting 2

Project Advisory Group Meeting #2

Date: Tuesday, October 24, 2017
Time: 10:00 a.m. to 12:00 p.m.
Location: North Tampa Branch Library
8916 North Boulevard
Tampa, Florida 33604
Attendees: See Appendix A

Notification of Meeting

Meeting invitation letters were sent by email to elected officials, government officials, local agencies and other interested parties. The invitation letters included the meeting date, time and location, and a summary of the project.

A Florida Administrative Register (FAR) ad was published in Volume 43/199 on October 13, 2017. The Florida Department of Transportation posted meeting information on the FDOT Public Notices website.

Copies of the invitation letter, FAR ad and FDOT Public Notice website ad are included in Appendix B.

Meeting Handouts

A copy of the meeting PowerPoint, User Preference Survey and Newsletter #1 were distributed to the attendees. PAG participants were encouraged to discuss their thoughts and concerns regarding the corridor study.

A copy of the meeting handouts is available in Appendix C.

Attendee Comments

Two written comment forms were received. A copy of the comments received are included in Appendix D.

Summary

The following provides a summary of the items discussed at the meeting:

- Consultant Project Manager, Carol Barker, opened the meeting by welcoming new attendees and thanking attendees for their participation.

- Carol asked everyone, including the project team, to introduce themselves and describe their affiliations.
- Carol provided a project overview and explained the role of PAG.
- Attendees participated in a “What Have You Heard Exercise.” They were given the opportunity to discuss what they or their affiliation has heard about the study outside of the project meetings and distributed information. Topics can be found in Appendix (E).
- Carol guided the PAG through a PowerPoint presentation.
- Carol discussed upcoming PAG meetings and public meetings.
- Carol encouraged the attendees to visit the [FDOT project website](#) and [WikkiMapping website](#).
- Carol provided closing remarks. The meeting ended at 12:00 p.m.

Discussion

Project Advisory Group (PAG) questions during discussion included:

Attendee: How many public meetings will there be?

Carol Barker: There will be a Visioning Workshop in November and an Alternatives Public Meeting in the spring of 2018.

Attendee: How are you going to communicate to the public about the meeting?

Carol Barker: We will be mailing out meeting notices and invitations to businesses and residents along the corridor as well as other major stakeholders. We can also email you the pdf of the invite to share.

Attendee: How many addresses will the meeting information be sent to?

Carol Barker: I don't have that number off the top of my head but I believe it's 1,000 or more.

Attendee: Do both the tenants and owners of businesses (buildings) receive a meeting invitation?

Carol Barker: Yes, both will receive an invitation.

Attendee: When are you planning to present to the MPO committees?

Carol Barker: I'd like to get us through the Visioning Workshop in November, then we'll be able to look at dates.

Attendee: What does “visioning” refer to – just the look of the corridor or the actual solutions?

Carol Barker: It's a nebulous thing. We've divided everything into categories such as landscaping, comfortability in moving along the corridor. We're referring to both functionality and look.

Attendee: Have you been looking at health data, heat, and air quality – those sorts of things – along this corridor?

Carol Barker: No, those are not things we have looked at. A corridor study is different than a PD&E study, in which those things would be looked at.

Attendee: Can you supplement Level of Service (LOS) with Trouble Time ability?

Carol Barker: The FDOT practice is to use LOS.

Attendee: Can engineering solutions be brought to the public as possibilities instead of asking them right out to tell you what they want for this corridor?

Carol Barker: We understand what you're saying, but we have to try not to use as much technical language when corresponding with the public, so it's easier for them to tell us what they want.

Attendee: Can you consider more hands on activities at the public meeting?

Carol Barker: Yes.

Attendee: Are you finding crash trends?

Carol Barker: There are many crashes involving young drivers, but a lot of what we're seeing is indicative of a heavy volume corridor.

Attendee: Who assigns the signal?

Carol Barker: Most of the signals are city maintained.

END OF MEETING SUMMARY

This meeting summary was prepared by Karen Harrell, Public Involvement Coordinator. If you feel clarifications are necessary, or if this differs from your understanding, please notify Karen by telephone at 352-257-1651 or by e-mail at Karen.Harrell@qcausa.com within (5) working days upon receipt of this summary.

Appendix A



PROJECT ADVISORY GROUP MEETING #2
West Busch Boulevard (SR 580) Corridor Study
 From North Dale Mabry Highway to North Nebraska Avenue
 Financial Project ID Number: 435908-1-22-01



Tuesday, October 24, 2017 from 10:00 am to 12:00 pm ■ North Tampa Branch Library, 8916 North Boulevard, Tampa, FL 33604

Project Advisory Group (PAG) Sign-In Sheet

NAME (PLEASE PRINT)	AGENCY / ORGANIZATION	TITLE	PHONE	EMAIL
KAREN MICHALSKI	SERRA CLUB		813-784-4081	Karen.Kaymicha1sk1@gmail.com
Lisa Silva	MPO			silval@planning.com.org
Pedro Parra	TPC		813-625-8020	parrap@planning.com.org
Jennifer Bartlett	PIOT			
Amy E. Clark	Public volunteer	retired teacher	774-273-5442	quaker_lady@yahoo.com
Willis Bowick	Tpa Bay Blk Chamber	President	813- 365-6612 332-4643	KCbowick2@hotmail.com
Wade Reynolds	MPO			en file
Johnny Wong	MPO			n file
AL WALTERS	HILLS APC			ALW@ALWALTERS.COM
Colvin Thusha	OD	Engage	574-7084	Colvin.Thusha@Tampabay.net
Hunele Ogilvie	Hillsborough MPO		813-873-3774	Ogilvie.m@planning.com.org



PROJECT ADVISORY GROUP MEETING #2
West Busch Boulevard (SR 580) Corridor Study
 From North Dale Mabry Highway to North Nebraska Avenue
 Financial Project ID Number: 435908-1-22-01

Staff



Tuesday, October 24, 2017 from 10:00 am to 12:00 pm ■ North Tampa Branch Library, 8916 North Boulevard, Tampa, FL 33604

Project Advisory Group (PAG) Sign-In Sheet

NAME (PLEASE PRINT)	AGENCY / ORGANIZATION	TITLE	PHONE	EMAIL
Jessica Francis	SCA		850-350-0107	
Brandon L. Smoyer	FDOT	PROJECT MANAGER	813-915-6449	Brandon.Smoyer@dot.state.fl.us
Trilite Rutwigale	FDOT	PM	978-6057	LTYMAN@RINWAYEE@W
Chad Polk	CH2M	Project Engineer	813-281-7912	chad.polk@ch2m.com
Cris Schooley	CH2M	PM/IE Engineer	407-354-2912	Cris.Schooley@CH2M.com
Mark Jennings	CH2M	Env. Planner	(813) 286-2777	mark.jennings@ch2m.com
Jody Woods	Chamberlain H.S.	Assistant Principal	813-975-1677	jody.woods@sdhc.k12.fl.us
Theray Wong	MPG			

Appendix B

Date: October 05, 2017

Subject: **Project Advisory Group (PAG) Meeting #2**
West Busch Boulevard (SR 580) Corridor Study
from North Dale Mabry Highway to North Nebraska Avenue
Hillsborough County, Florida
FDOT Financial Project ID Number: 435908-1-22-01

Dear Project Stakeholder:

The Florida Department of Transportation (FDOT), District Seven is conducting a Corridor Study on the above referenced study. This letter is an invitation for you to participate in the second Project Advisory Group (PAG) meeting for this project. PAG meeting #2 is scheduled for Tuesday, October 24, 2017, from 10:00 AM to Noon at the **North Tampa Branch Library, 8916 North Boulevard, Tampa, Florida 33604.**

The West Busch Boulevard Corridor study limits extend from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The study area includes portions of incorporated Hillsborough County and the City of Tampa. The purpose of the Corridor Study is to work with the community, stakeholders and a Project Advisory Group (PAG) to develop a vision and identify needs and issues in the corridor.

The PAG is an important component of the study. It serves as a special advisory resource to the FDOT and the consultant team. The PAG participants will help develop a vision for the corridor that will guide proposed short and long-term recommendations. This may include issues related to capacity, traffic operations, safety, access and egress, mobility, freight movements, transit, bicycles, and pedestrian movements.

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220 at least seven (7) working days in advance of the meeting.

If you have any questions about membership in the PAG or require further information concerning this project, please contact Brian Shroyer, Florida Department of Transportation Project Manager at 813-975-6449 or by email at Brian.Shroyer@dot.state.fl.us.

Sincerely,
Brian Shroyer
Multimodal Project Manager
Florida Department of Transportation

Florida Administrative Register Ad:

The Florida Department of Transportation, District Seven announces a public meeting to which all persons are invited.

DATE AND TIME: October 24, 2017, 10:00 AM to Noon

PLACE: North Tampa Branch Library, 8916 North Boulevard, Tampa, FL 33604

GENERAL SUBJECT MATTER TO BE CONSIDERED: The Florida Department of Transportation (FDOT), District Seven, is holding the second Project Advisory Group (PAG) meeting for the West Busch Boulevard (SR 580) Corridor Study. The limits of the project extend from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The corridor study area includes portions of unincorporated Hillsborough County and the City of Tampa.

The purpose of the Corridor Study is to work with the community, stakeholders and a Project Advisory Group (PAG) to develop a vision and identify needs and issues in the corridor. The PAG is an important component of the study. It serves as a special advisory resource to the FDOT and the consultant team. The PAG participants will help develop a vision for the corridor that will guide proposed short and long-term recommendations. This may include issues related to capacity, traffic operations, safety, access and egress, mobility, freight movements, transit, bicycles, and pedestrian movements.

A copy of the agenda may be obtained by contacting: Mr. Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability, or family status. Pursuant to the provisions of the Americans with Disabilities Act, any person requiring special accommodations to participate in this workshop/meeting, or persons who require translation services (free of charge) are asked to advise the agency at least 7 days before the workshop/meeting by contacting: Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220

Comuniquese Con Nosotros

Nos importa mucho la opinión del público sobre el proyecto. Si usted tiene preguntas o comentarios, o si simplemente desea más información, por favor comuniquese con nuestra representante, Lilliam Escalera, al (813) 975-6403, Departamento de Transportación de Florida, 11201 N. McKinley Dr., Tampa, FL 33612, Lilliam.Escalera@dot.state.fl.us

If you are hearing or speech impaired, please contact the agency using the Florida Relay Service, 1(800)955-8771 (TDD) or 1(800)955-8770 (Voice).

For more information, you may contact: Mr. Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT.

FDOT Public Notices Website:

PROJECT ADVISORY GROUP (PAG) MEETING

District: Seven

Meeting Type: Meeting

Date: October 24, 2017

Time: 10 AM to Noon

Location Name: North Tampa Branch Library

Street Address: 8916 North Boulevard

City: Tampa, FL 33604

Purpose: The Florida Department of Transportation, District Seven, is holding the second Project Advisory Group (PAG) meeting for the West Busch Boulevard (SR 580) Corridor Study. The limits of the project extend from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The corridor study area includes portions of unincorporated Hillsborough County and the City of Tampa.

The purpose of the Corridor Study is to work with the community, stakeholders and a Project Advisory Group (PAG) to develop a vision and identify needs and issues in the corridor. The PAG is an important component of the study. It serves as a special advisory resource to the FDOT and the consultant team. The PAG participants will help develop a vision for the corridor that will guide proposed short and long-term recommendations. This may include issues related to capacity, traffic operations, safety, access and egress, mobility, freight movements, transit, bicycles, and pedestrian movements. A copy of the agenda may be obtained by contacting: Mr. Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us.

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Primary Contact: Brian Shroyer, FDOT Project Manager

Primary Phone: (813) 975-6449

Primary E-mail: Brian.Shroyer@dot.state.fl.us

Additional Contact: Christopher Speese, Public Involvement Coordinator

Additional Phone: (813) 975-6405

Additional E-mail: Christopher.Speese@dot.state.fl.us

Appendix C

Project Advisory Group Meeting #2 West Busch Boulevard (SR 580) Corridor Study

from North Dale Mabry Highway to North Nebraska Avenue

Hillsborough County, Florida

FDOT Financial Project ID Number: 435908-1-22-01

North Tampa Branch Library

August 29, 2017 – 10:00 am to 12:00 pm



Agenda for Project Advisory Group Meeting #2

- Introductions
- Project Overview and Data Updates
- Overview of Visioning Workshop
- Next Steps
- Questions
- Adjourn

Introduction to PAG #2 Meeting





Introductions

- State your name
- Who do you represent
- Feedback on previous meetings
- Other items of interest for the PAG team.

Purpose of the SR 580 Corridor Study

- Define existing conditions
- Identify transportation-related needs
- Define acceptable levels of access and mobility
- Determine transportation system needs to support surrounding land uses
- Consolidate and control access points
- Identify safety concerns
- Identify operational deficiencies and promote operational efficiency

Your Role as a PAG Member

- Gather local input and then disseminate information back to the impacted stakeholders and communities to ensure that any improvements within the corridor continue to serve as vital elements of the community's transportation system
- Provide policy guidance and technical assistance to FDOT and the project team as the common goals and objectives of the study are defined, issues are identified, and potential solutions are developed



What have you been hearing?

- Have you heard any feedback from your agencies or local residents?

Goals for PAG #2

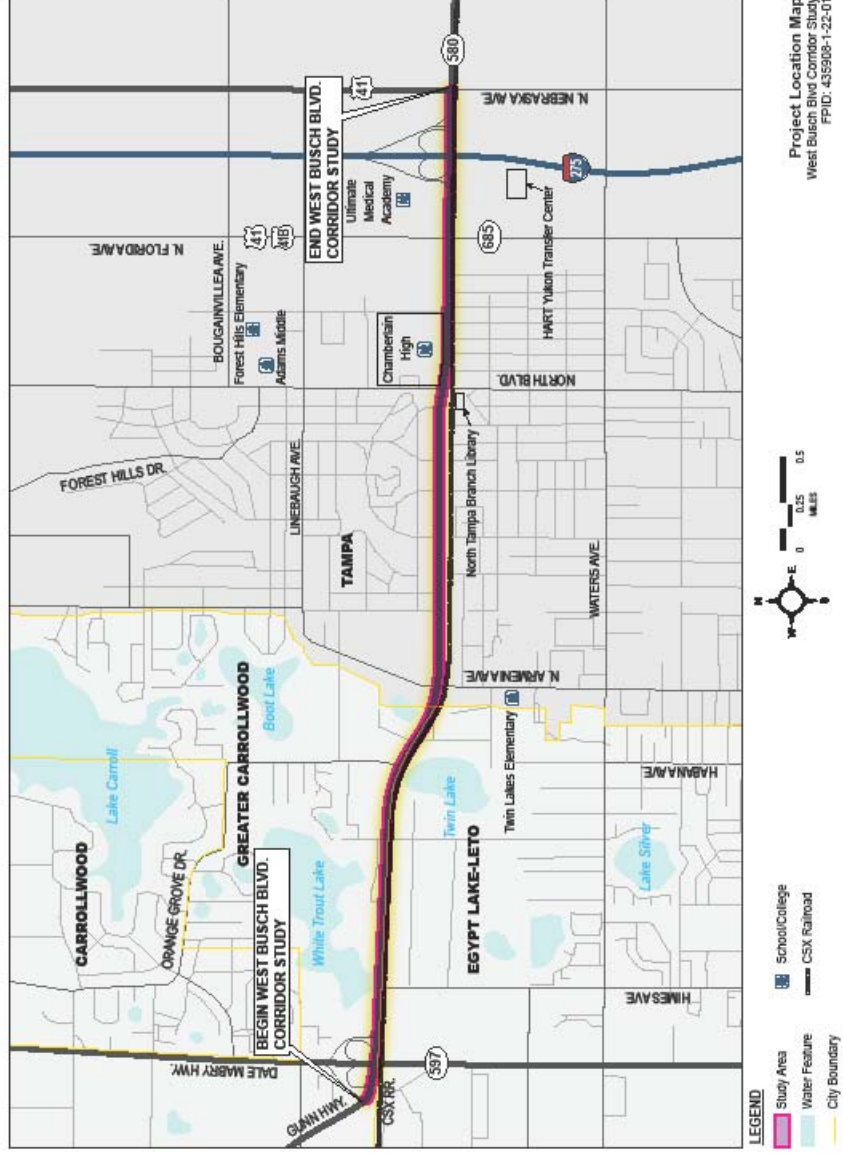
- Project Overview: Existing Characteristics and updated Future Conditions
- Feedback – information will be presented at Visioning Workshop November 16, 2017
- Request for even more feedback from your constituents.

Project Overview and Updated Data

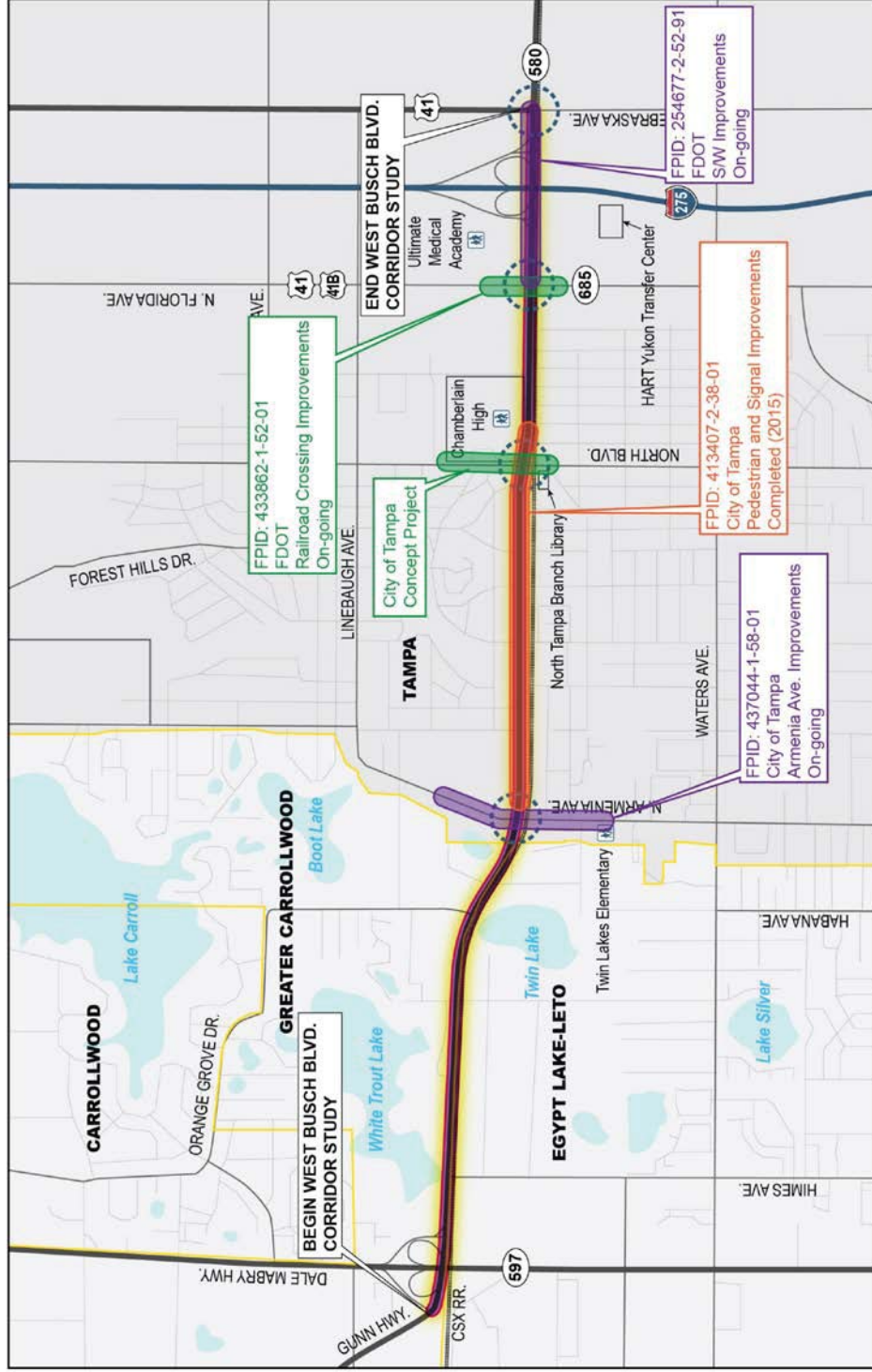


Project Study Area

- 3.3 miles
- Within the city of Tampa and unincorporated Hillsborough County



Adjacent Projects



LEGEND

- Study Area
- Water Feature
- City Boundary
- School/College
- CSX Railroad
- Potential FDOT Lighting Project - Future

Scale: 0, 0.25, 0.5 MILES

Compass Rose: N, S, E, W

Related Projects Map
 West Busch Blvd Corridor Study
 FPID: 435908-1-22-01



Project Information - FDOT's context classifications

Street Connectivity

- Block Length
- Block Perimeter
- Intersection Density

Development Form and Intensity

- Building Placement
- Presence of Fronting Uses
- Location of Off-Street Parking
- Land Uses
- Building Height



C1-Natural C2-Rural C2T-Rural Town C3R-Suburban Residential C3C-Suburban Commercial C4-Urban General C5-Urban Center C6-Urban Core

Existing context classification

C3R-SUBURBAN RESIDENTIAL



Mostly **residential** uses within large blocks and a disconnected or **sparse roadway network**.

C4-URBAN GENERAL



Mix of uses set within small blocks with a **well-connected roadway network**. The roadway network usually **connects to residential neighborhoods** immediately along the corridor or on the back side of blocks fronting the roadway.

C3C-SUBURBAN COMMERCIAL



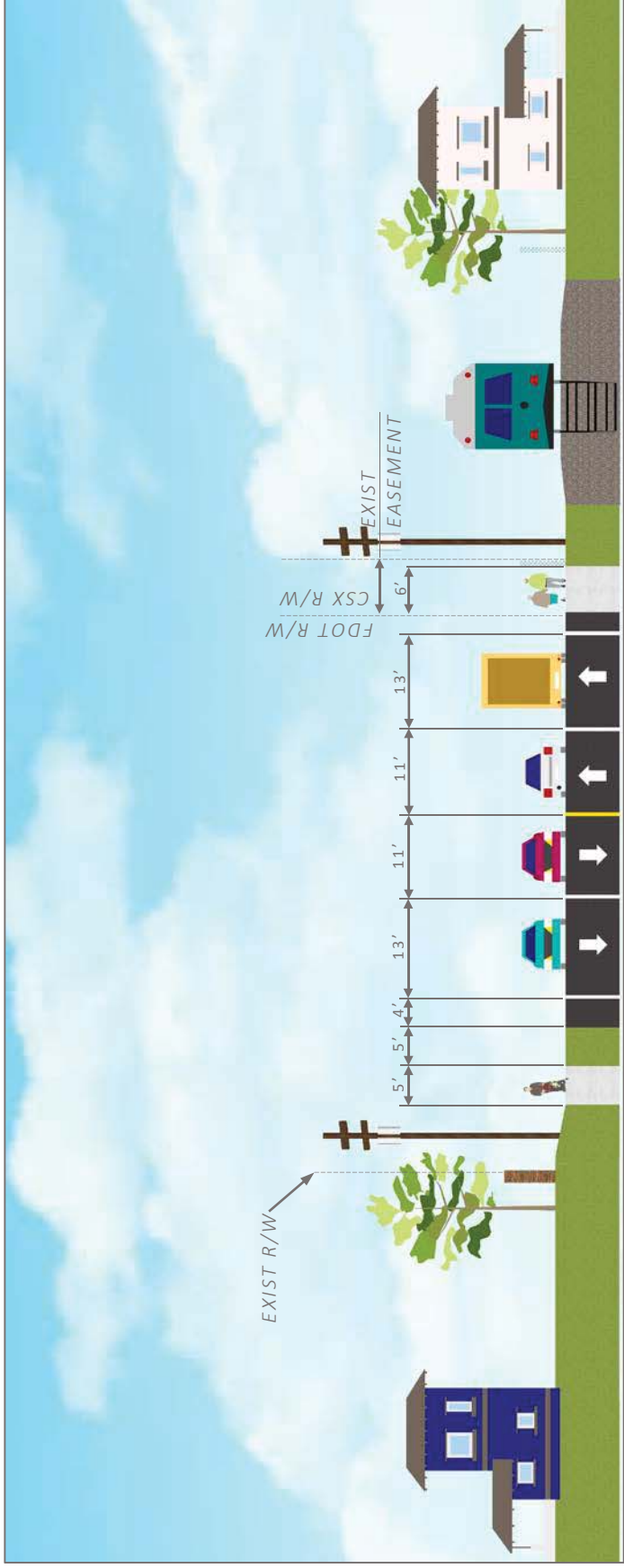
Mostly **non-residential** uses with **large building footprints** and **large parking lots** within large blocks and a disconnected or **sparse roadway network**.

C3R-Suburban Residential – Dale Mabry Hwy to Armenia Ave.



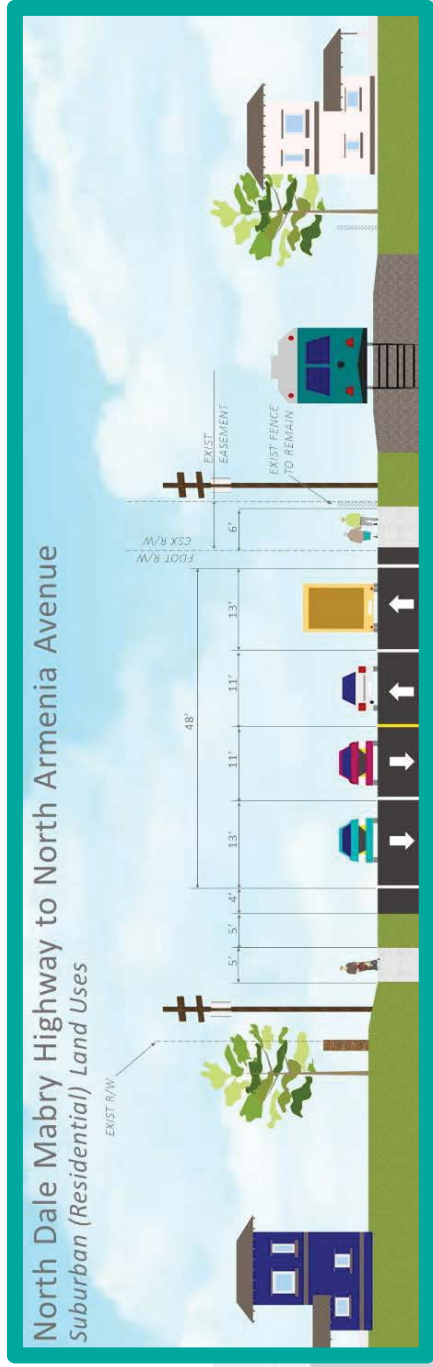
Mostly **residential** uses within large blocks and a disconnected or **sparse roadway network**.

C3R-SUBURBAN RESIDENTIAL



E-675

Existing Conditions Dale Mabry Highway to North Armenia Avenue



The map displays the project area from Dale Mabry Hwy to North Armenia Ave. Key streets shown include:

- DALE MABRY HWY
- TWIN LAKES BLVD
- HIMES AVE
- ARMENIA AVE
- W BUSCH BLVD
- NORTH BLVD
- FLORIDA AVE
- NEBRASKA AVE
- W WATERS AVE

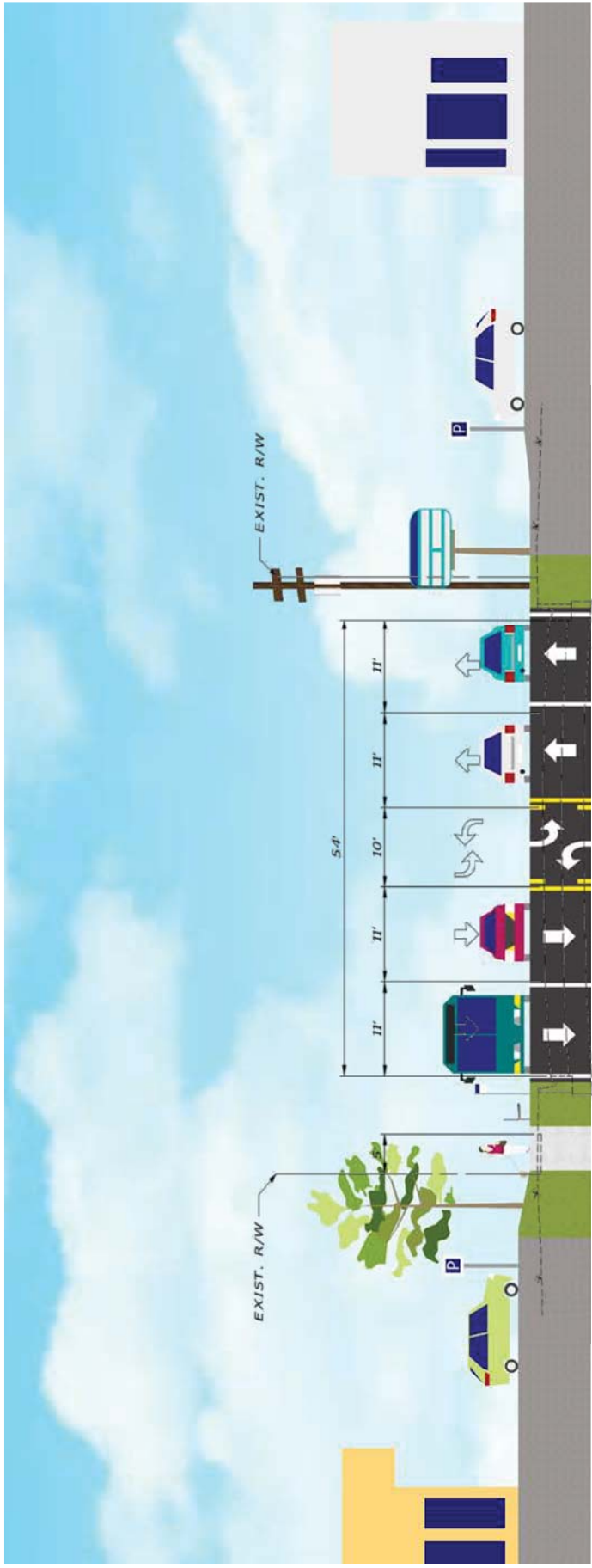
Green dashed lines indicate the project boundaries along Dale Mabry Hwy and North Armenia Ave.

C4-Urban General – Armenia Ave to North Blvd



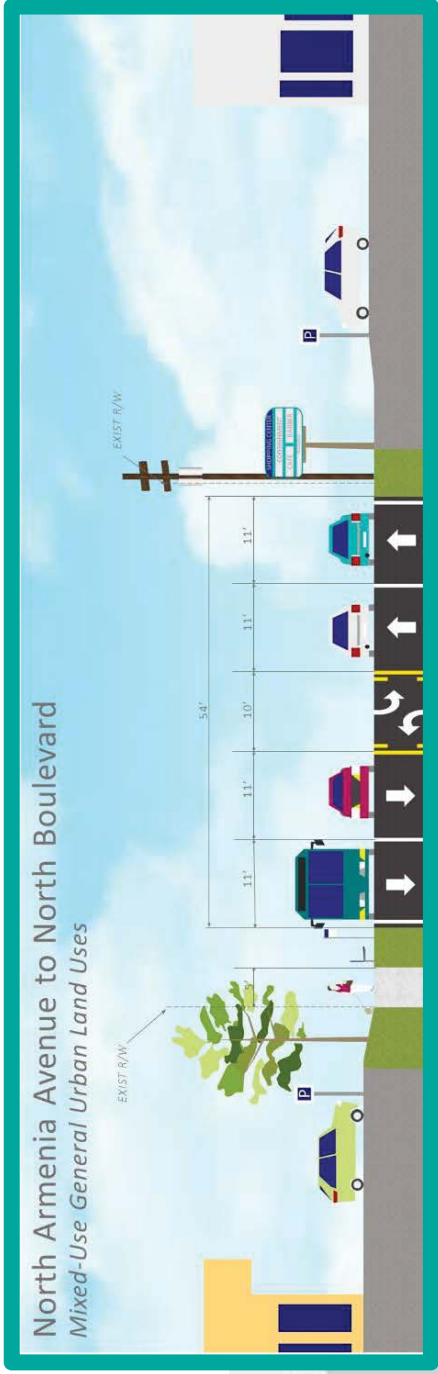
Mix of uses set within small blocks with a **well-connected roadway network**. The roadway network usually **connects to residential neighborhoods** immediately along the corridor or on the back side of blocks fronting the roadway.

C4-URBAN GENERAL



E-577

Existing Conditions North Armenia Avenue to North Boulevard



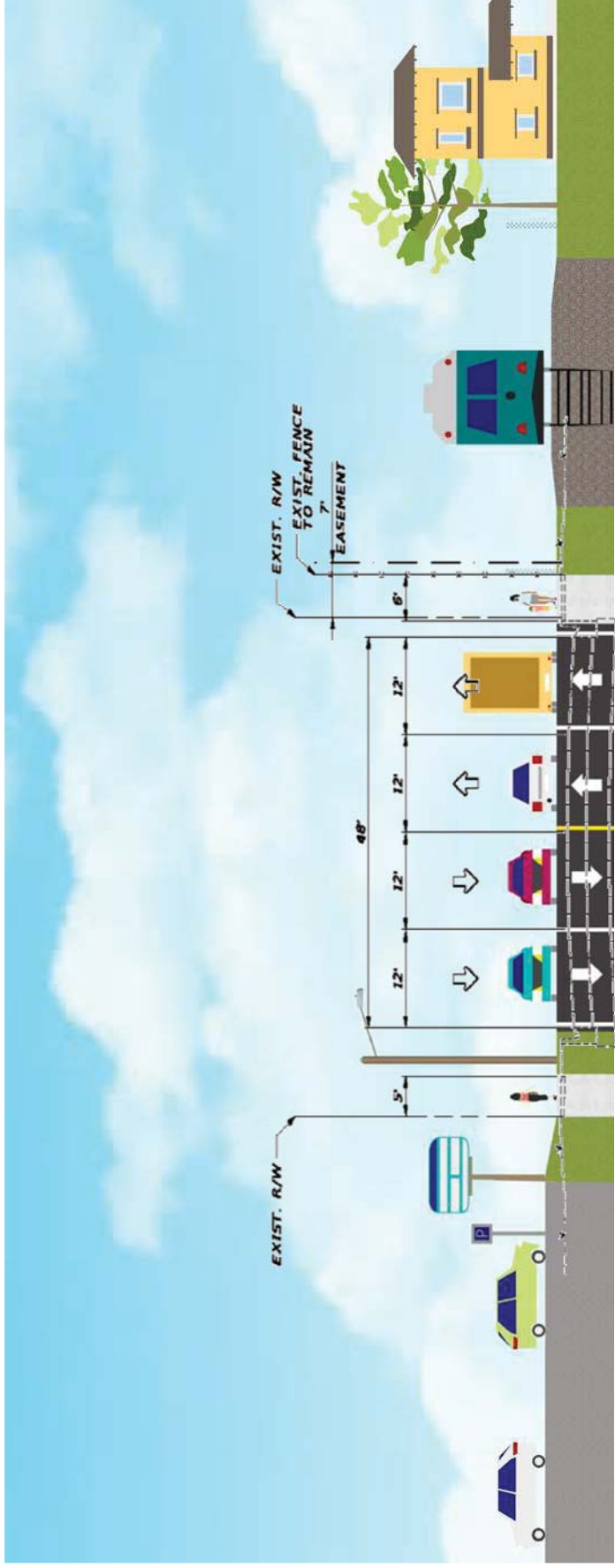
C3C Suburban Commercial – North Blvd to N Nebraska Ave.

C3C-SUBURBAN COMMERCIAL

Section 1: North Blvd to Florida

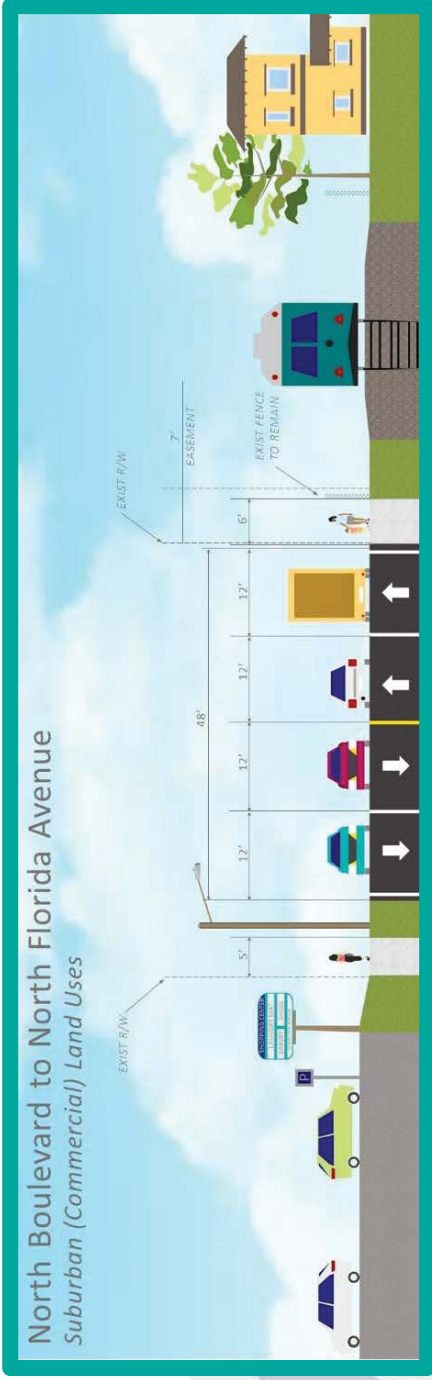


Mostly **non-residential** uses with **large building footprints** and **large parking lots** within large blocks and a **disconnected or sparse roadway network**.



E-579

Existing Conditions North Boulevard to North Florida Avenue



The map displays the study area along W Busch Blvd, highlighted in yellow. Key streets shown include:

- DALE MABRY HWY
- HIMES AVE
- TWIN LAKES BLVD
- ARMENIA AVE
- W WATERS AVE
- NORTH BLVD
- FLORIDA AVE
- NEBRASKA AVE

A large 'X' is overlaid on the map, indicating the study area between North Boulevard and North Florida Avenue.

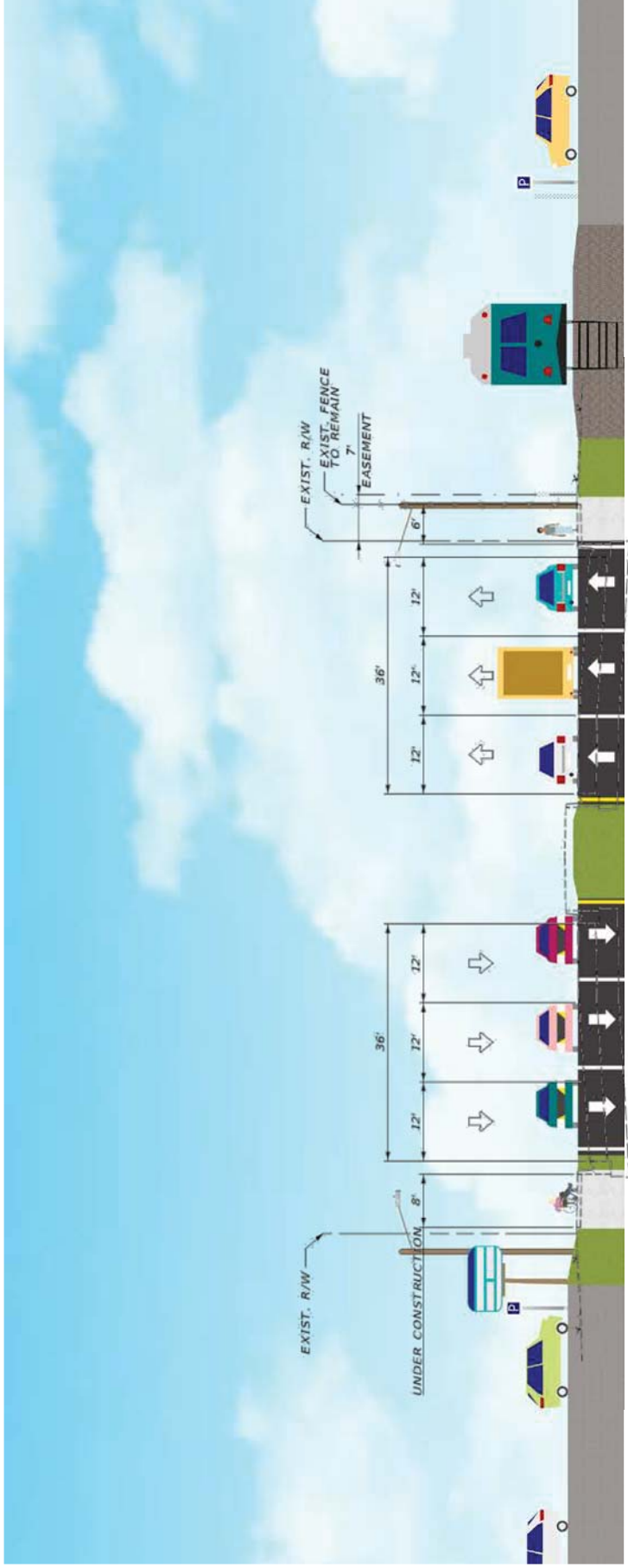
C3C Suburban Commercial – North Blvd to N Nebraska Ave.



Mostly non-residential uses with large building footprints and large parking lots within large blocks and a disconnected or sparse roadway network.

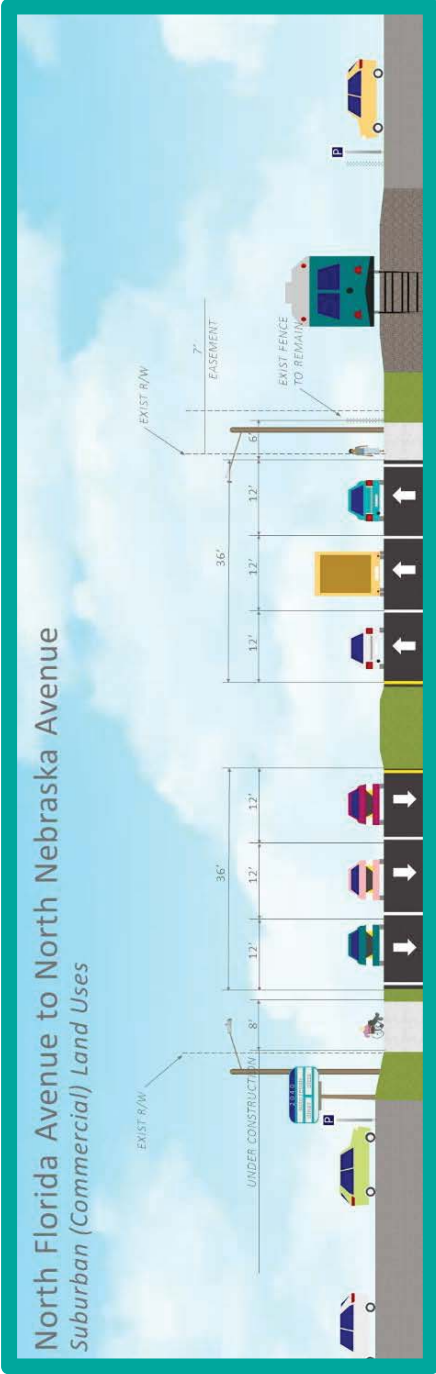
C3C-SUBURBAN COMMERCIAL

Section 2: Florida to Nebraska

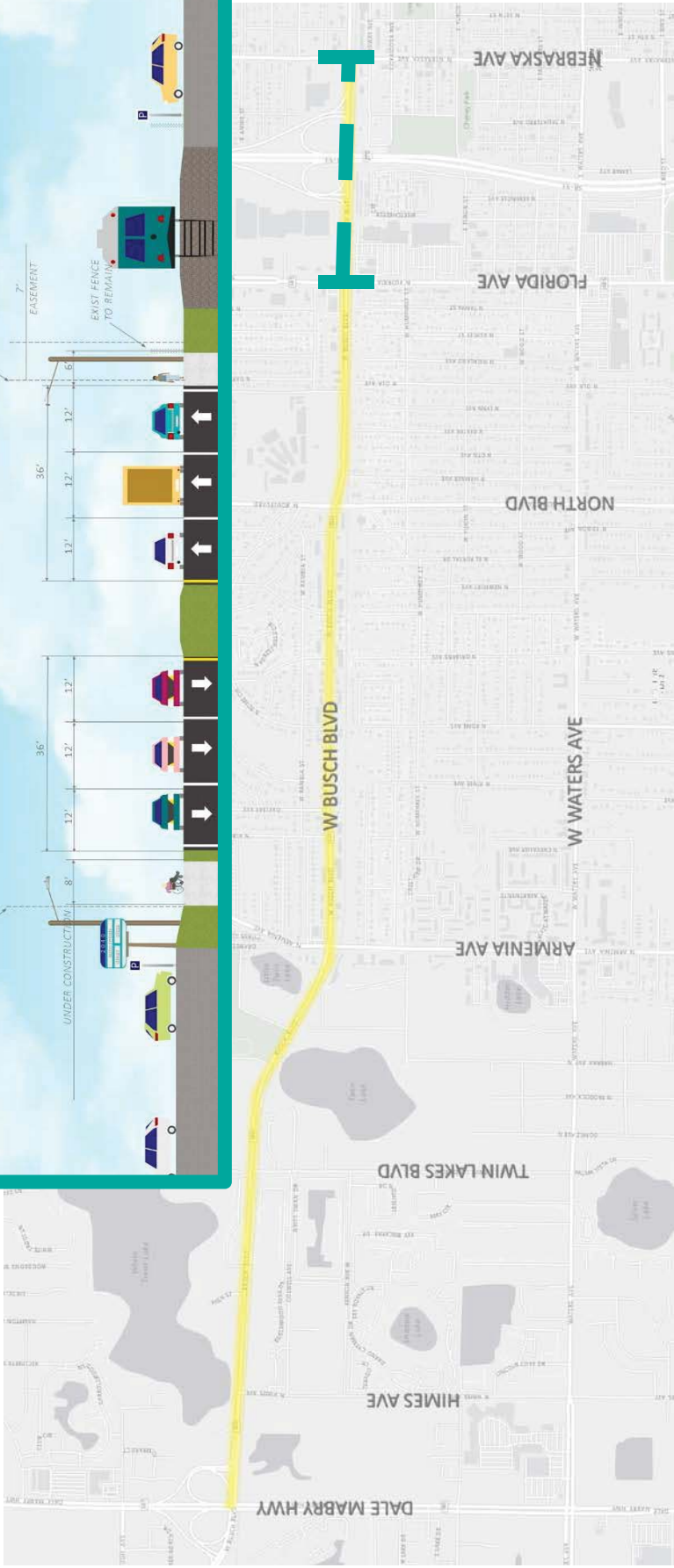


E-581

Existing Conditions North Florida Avenue to North Nebraska Avenue



North Florida Avenue to North Nebraska Avenue
Suburban (Commercial) Land Uses

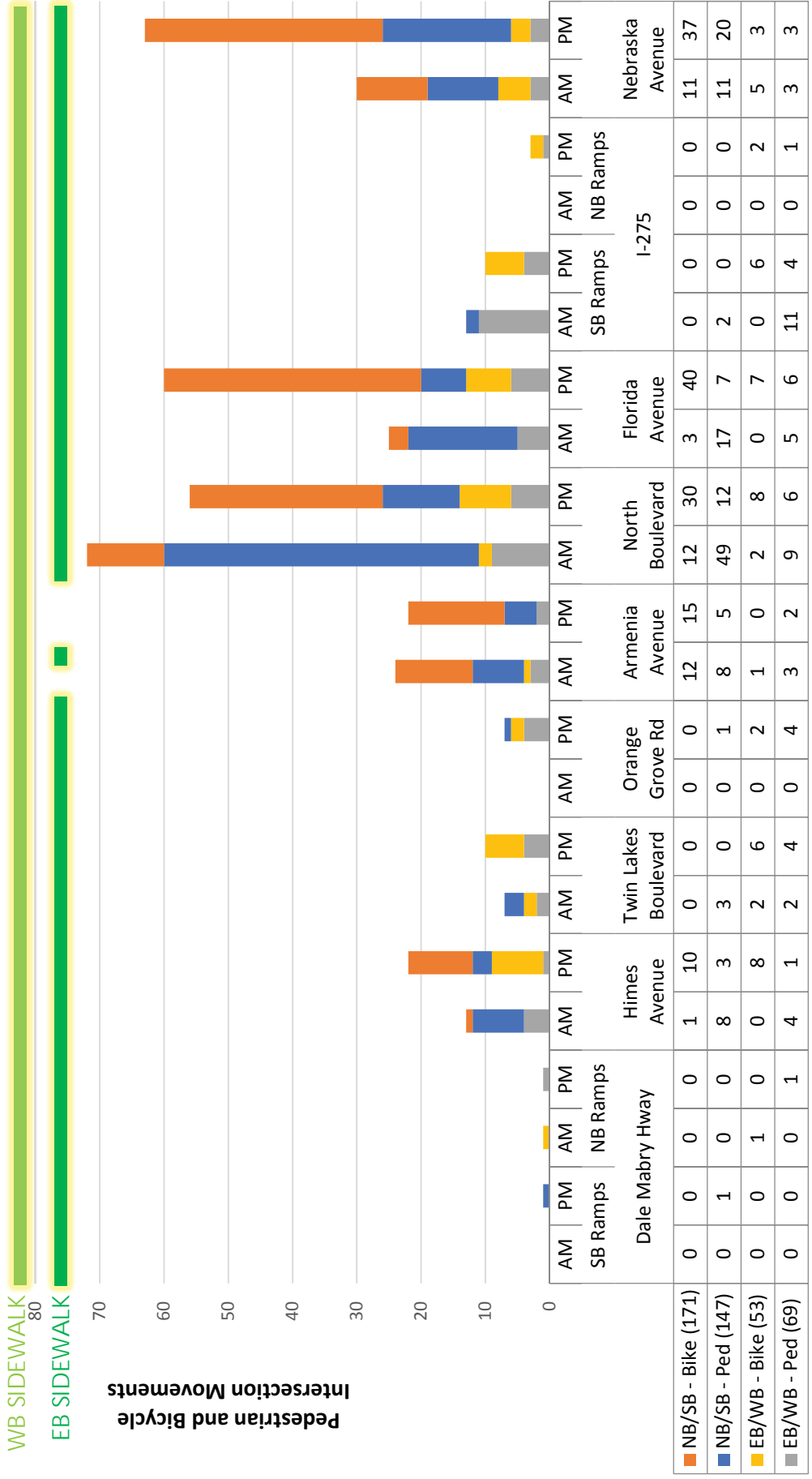


Take Away from Existing Context Classification information

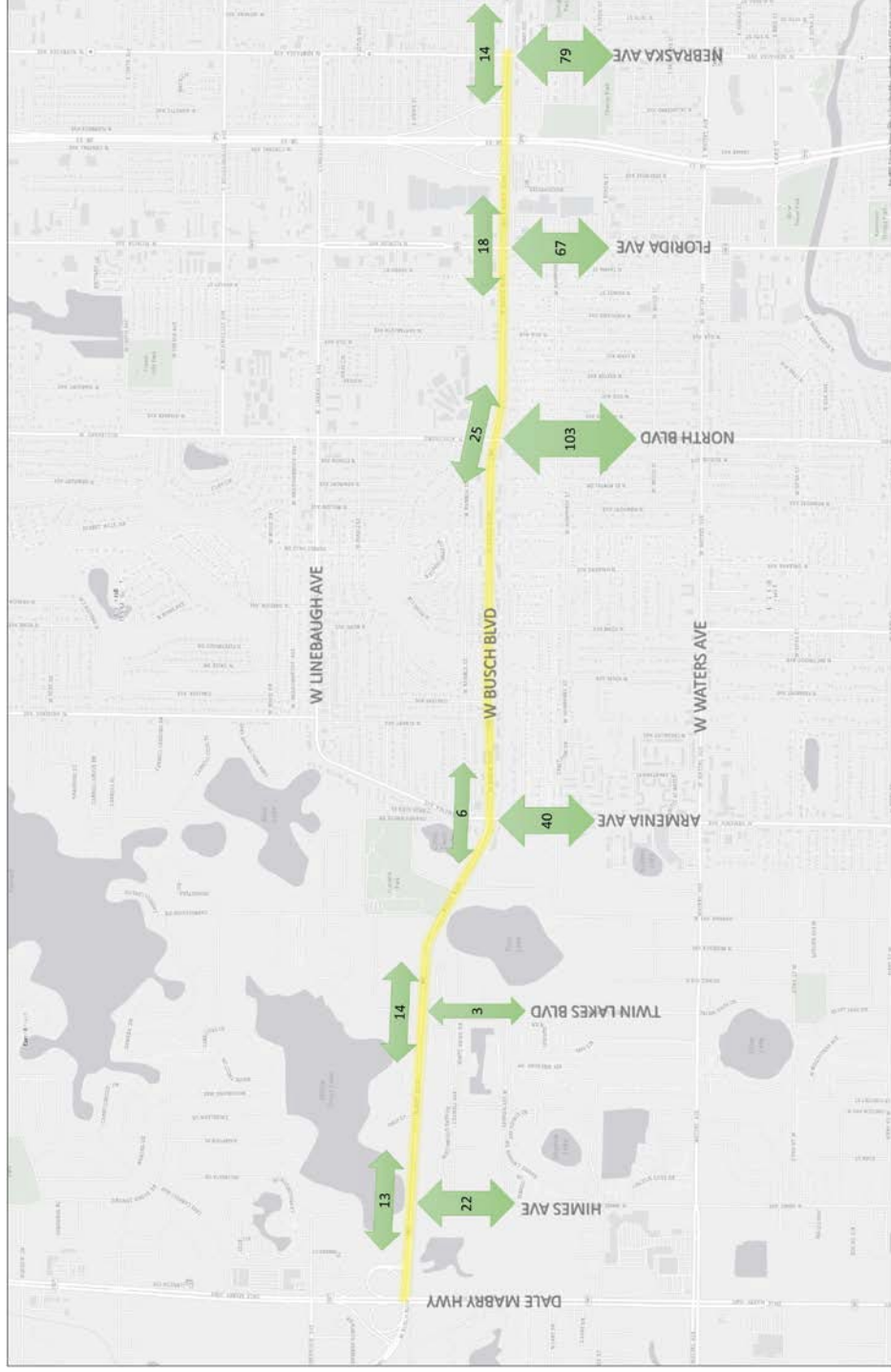
- No bike lanes throughout corridor
- Sidewalk gaps on South side of SR 580
- There is latitude in Context Classifications
- Projects are evaluated on FUTURE context

Pedestrian/Bicycle Counts

This slide illustrates the locations with the heaviest bike/ped traffic (based on peak 2-hour counts [1 day only] – from VHB AADT memo)

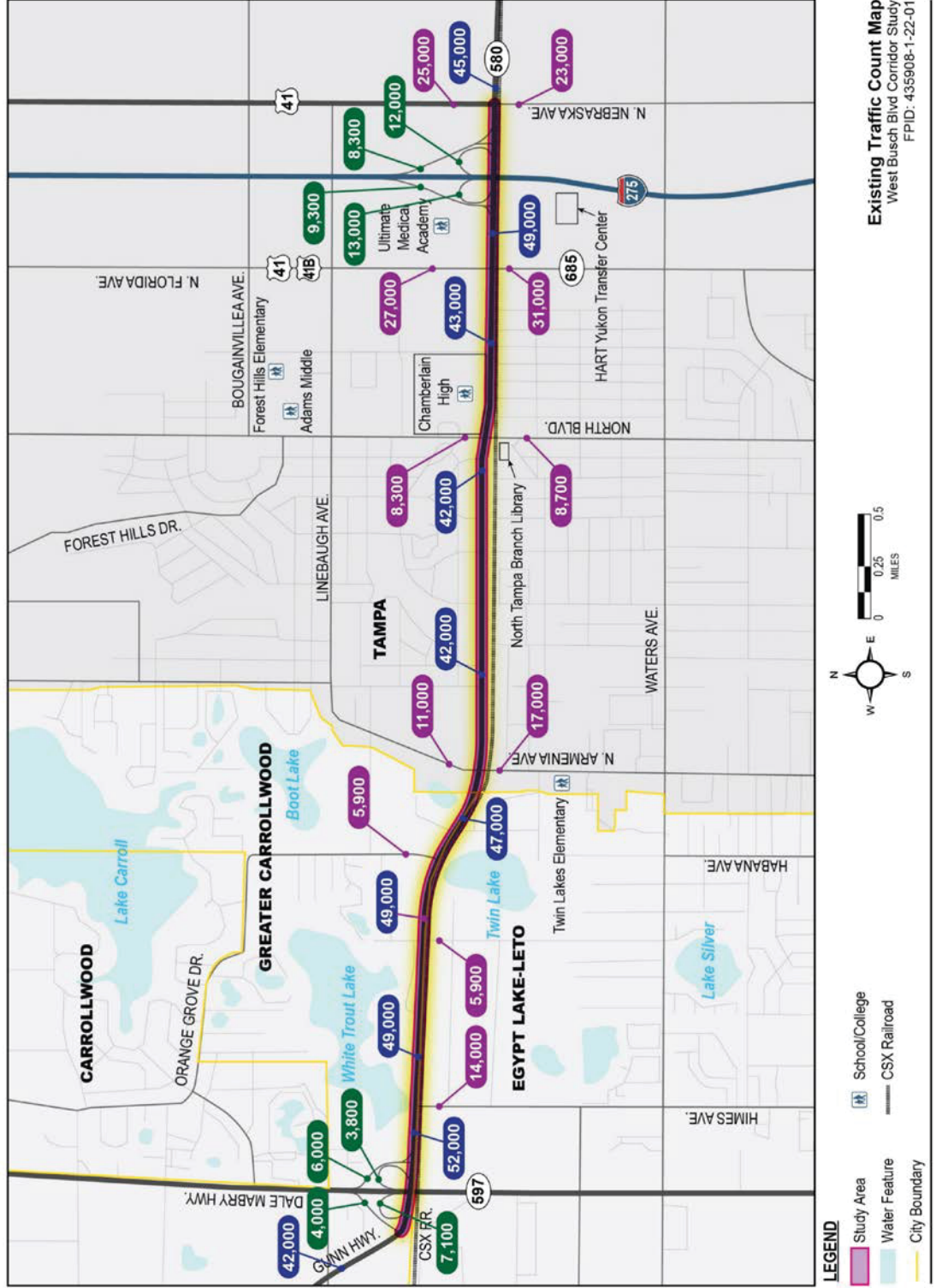


Existing Conditions Bicycle and Pedestrian Counts



Bike/Ped Counts Map (7-9am & 4-6pm Thursday May 4, 2017)

Existing Traffic Counts



Traffic Volume Considerations

STANDARDS FOR LOW AND HIGH VOLUME HIGHWAYS IN ANNUAL AVERAGE DAILY VOLUMES

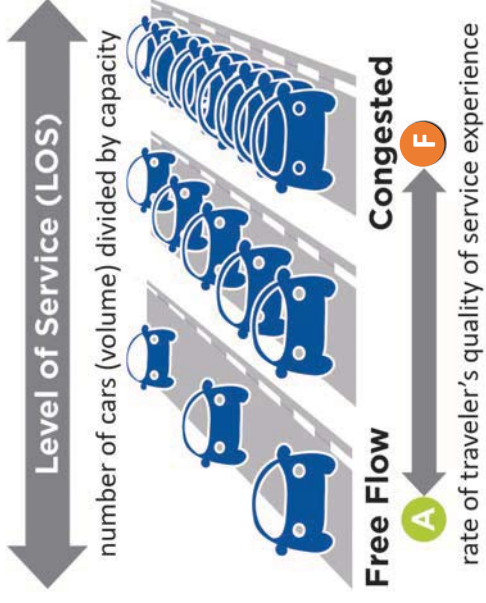
HIGHWAY TYPE	LOW VOLUME AADT	HIGH VOLUME AADT
FREEWAY - URBAN		
4-LANE FACILITY	57,000	69,000
6-LANE FACILITY	86,000	103,000
8-LANE FACILITY	114,000	138,000
FREEWAY - RURAL		
4-LANE FACILITY	46,000	56,000
6-LANE FACILITY	69,000	83,000
8-LANE FACILITY	92,000	111,000
ARTERIALS - URBAN		
2-LANE FACILITY	16,000	20,000
4-LANE FACILITY	37,000	43,000
6-LANE FACILITY	55,000	64,000
8-LANE FACILITY	69,000	80,000
ARTERIALS - RURAL		
2-LANE FACILITY	9,000	14,000
4-LANE FACILITY	38,000	47,000
6-LANE FACILITY	58,000	71,000
COLLECTOR - URBAN		
2-LANE FACILITY	11,000	16,000
4-LANE FACILITY	37,000	45,000
COLLECTOR - RURAL		
2-LANE FACILITY	8,000	13,000
4-LANE FACILITY	30,000	38,000

SR 580 Existing AADT

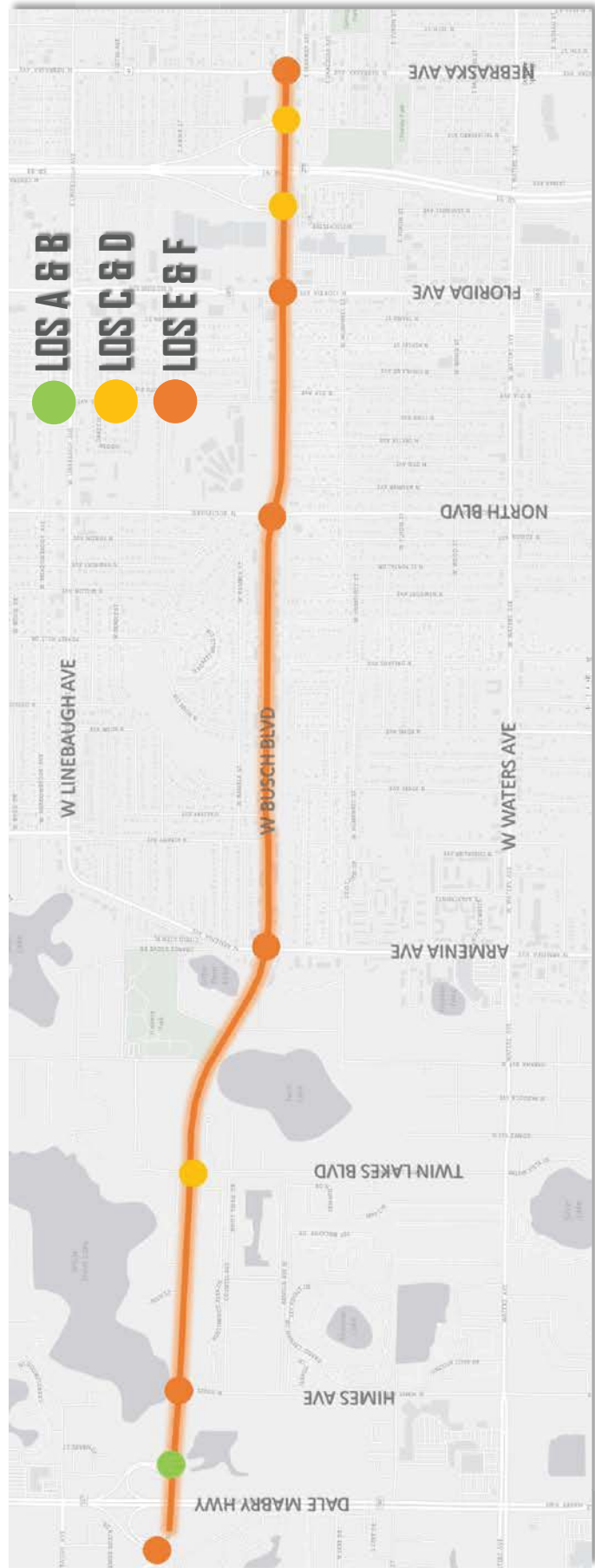
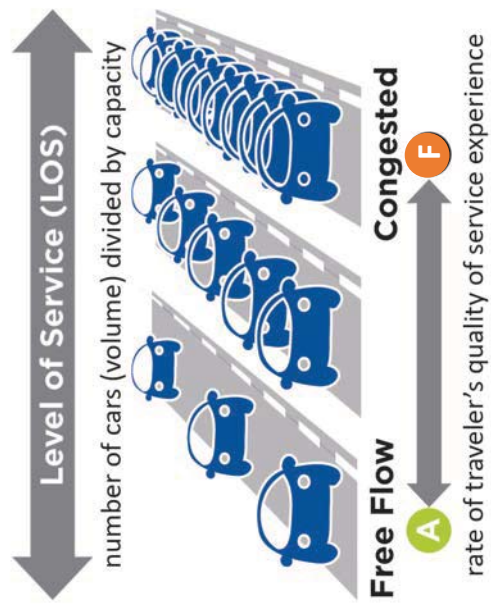
Minimum = 42,000

Maximum = 52,000

Existing Conditions Traffic Volumes (2017)



Existing Conditions Future Traffic Volumes (2040)



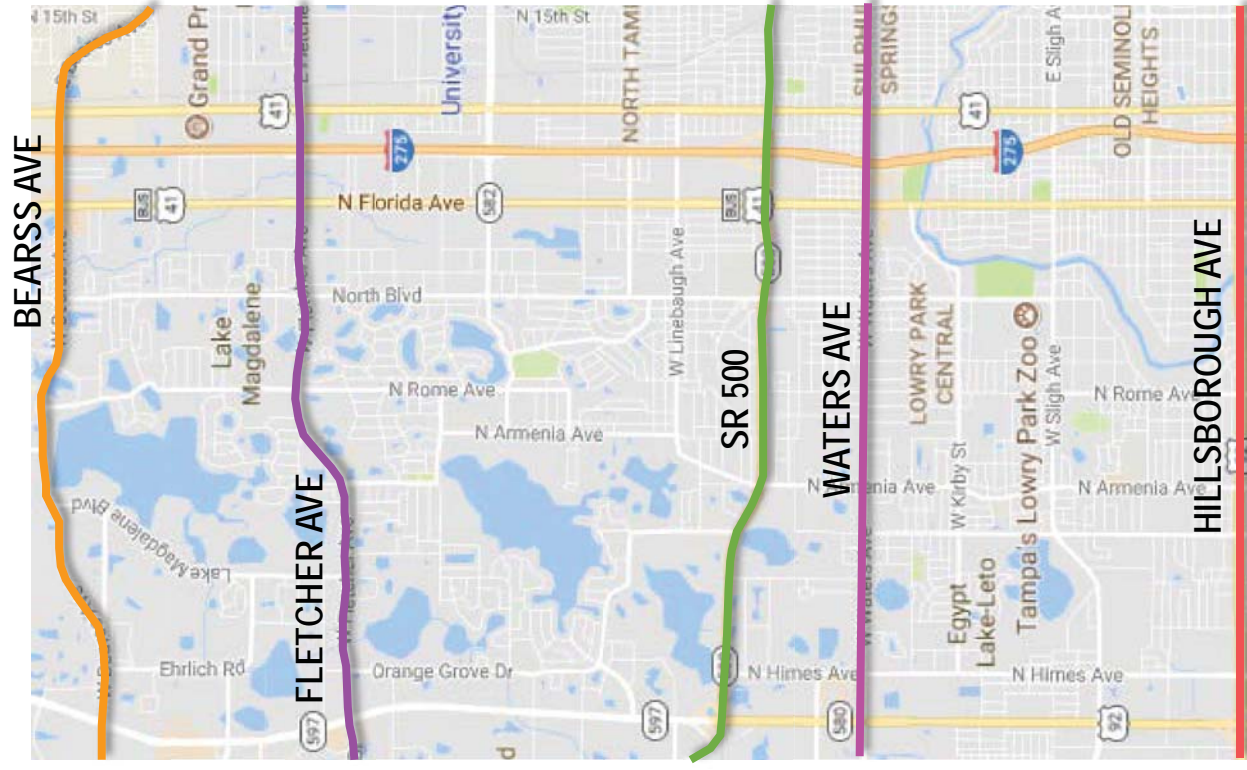
Adjacent East-West Corridor Considerations

Corridor	Lanes	AADT	Notes
Bearss Avenue	4-lane	54,000	3.8 miles north
Fletcher Avenue	4-lane	23,500	Limited western connectivity
SR 580	4-lane	50,000	
Waters Avenue	4-lane	27,000	Limited eastern connectivity
Hillsborough Avenue	6-lane	51,500	2.6 miles south

SR 580 Existing AADT

Minimum = 42,000

Maximum = 52,000



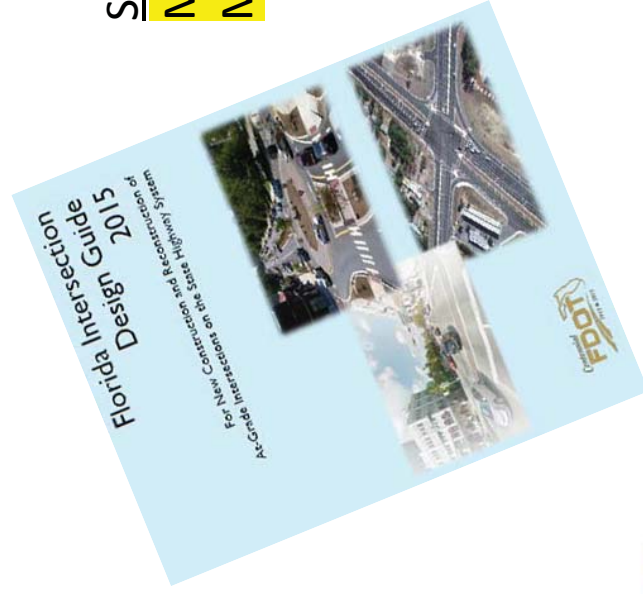
Roundabout Consideration

- Maximum design year total traffic volume entering an intersection is:
 - ≈ 25,000 AADT for single-lane roundabout
 - ≈ 45,000 AADT for two lane roundabout

SR 580 Existing AADT

Minimum = 42,000

Maximum = 52,000



Intersection	Entering AADT (Existing)
Dale Mabry/SB Ramps	49,100
Dale Mabry/NB Ramps	58,000
Himes	57,500
Twin Lakes	54,900
Orange Grove	50,950
Armenia	58,500
North	51,000
Florida	75,000
I-275/SB Ramps	60,150
I-275/NB Ramps	58,150
Nebraska	60,900

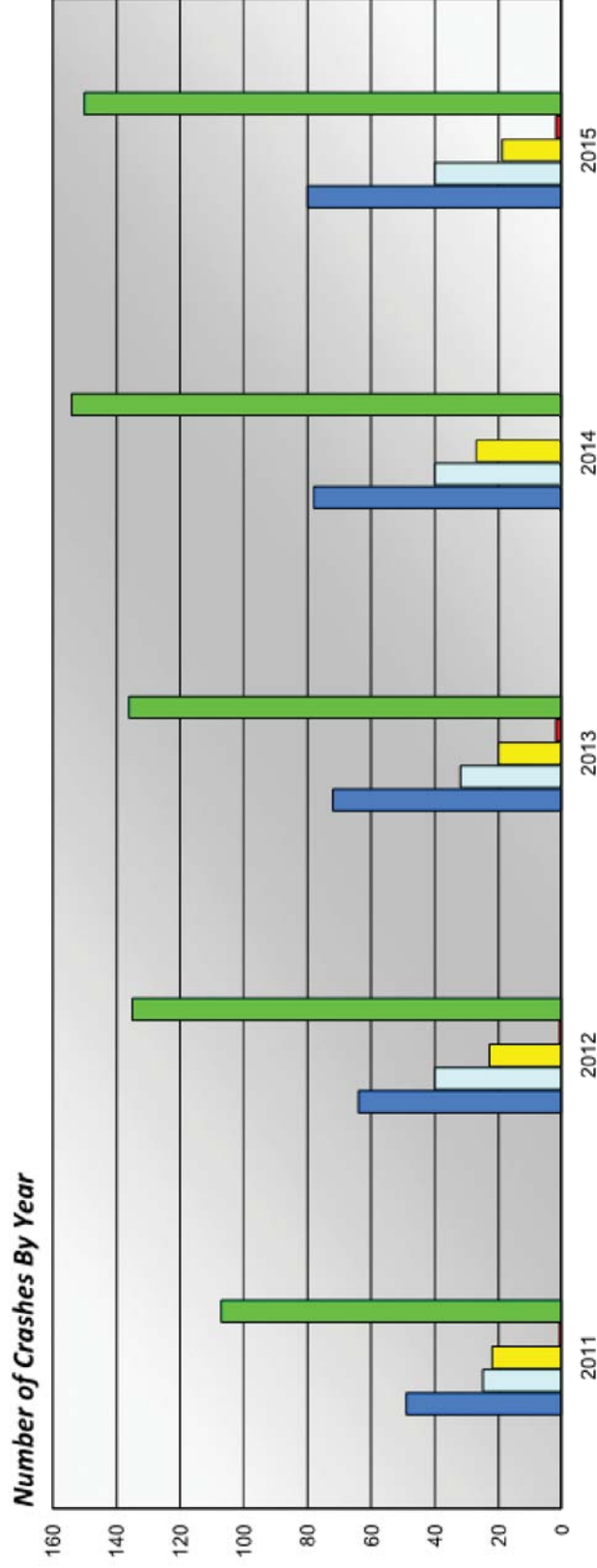
Take Away from existing Counts

- Bike/Ped movement N-S exceeds E-W movement.
- Round-About configuration not viable: within SR 580 corridor exceeds the maximum traffic volume requirements for a round-about and impacts the RR on the south.
- Lane reduction not viable:
 - ≈ The existing traffic is either within or just above the range for 4-lane facility Traffic Volumes.
 - ≈ Adjacent corridors are already at or above capacity and too far away.
- In fact, **6-Lane may be reasonable** for portions of SR 580 – but policy constrained

Note Potential Limitations for Capacity Improvements

- Shows need for 6 lane widening
- Policy constrained (physical constraints) in Tampa Comprehensive Plan – from Dale Mabry Hwy to North Blvd.
 - ≈ *MBY Policy 3.2.2: Constrained roadways, though ineligible for two or more general use thru-lane additions, shall be eligible for transit system enhancements including those described above. Dedicated transit lanes constructed pursuant to this policy shall not be retrofitted for use by automobile or truck traffic so long as the subject roadway remains constrained.*

Latest Crash History by Year



Breakdown of Crashes by Year

	2011	2012	2013	2014	2015
PDO	49	64	72	78	80
Injury Crashes	57	70	62	76	68
Fatal Crashes	1	1	2	0	2
Total Crashes	107	135	136	154	150

Existing Conditions Historical Crashes 2011- 2015



*Source: FDOT Crash Analysis Reporting System (CARS)

Take Away from Crash History Data

- Crashes have increased.
- Highest “heat” intensity is at Himes and between North Blvd and Nebraska.
- The 5-lane section has a lower frequency and severity of crashes compared to the 4-lane section.
- Five of the six fatalities occurred between North Blvd and Nebraska
- Recently checked *FIRES* and found no new fatalities between 1-1-16 and 10-23-17.

Preview of the Public Visioning Workshop



Public Visioning Workshop

- November 16, 2017
 - ≈ 5:30 p.m. – 7:30 p.m.
 - ≈ Open house format
 - ≈ Christian Family Church

FDOT




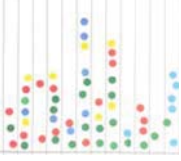


WESTBUSCH BOULEVARD STUDY

You're invited to take part in a Florida Department of Transportation (FDOT) Visioning Workshop for a corridor planning study along Westbusch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue in Hillsborough County – a distance of approximately 3.3 miles. The FDOT is looking for ways to improve Westbusch Boulevard (SR 580) for pedestrians, bicyclists, transit users and motorists. At this workshop, we will gather your input on the existing corridor and document your ideas to resolve some of the issues that you have observed.

Thursday, November 16, 2017, 5:30 p.m. to 7:30 p.m.
Christian Family Church, 3457 W. Kenyon Avenue, Tampa, FL 33614

For More Information Contact:
Brian Shroyer, CPM
Project Manager
Florida Department of Transportation
11201 N. McKinley Drive
Tampa, FL 33812
813-975-6449
brian.shroyer@dot.state.fl.us
Visit Our Website
fdot7studies.com/westbuschblvd/

Public Visioning Workshop

- 5 Stations:
 - ≈ Station 1: 10 minute **Project Introduction Video** 
 - ≈ Station 2: **Existing Conditions** displays
 - Project location map
 - Existing typical sections
 - Crash map
 - Bicycle and pedestrian counts
 - Existing and future (no-build) traffic LOS
 - ≈ Station 3: **Concerns and Issues** (sticky-note exercise) 
 - ≈ Station 4: **Developing the Corridor Vision** (goals and objectives dot exercise) 
 - ≈ Station 5: **Potential Improvements** (dot exercise) 
 - ≈ Station 6: **Comments** - user preference surveys and comment forms 

Draft User Preference Survey



DRAFT User Preference Survey SR 580 (West Busch Boulevard) Corridor Study from Dale Mabry Hwy. to Nebraska Ave.

Please fill out this survey as best as you can and place in the Survey forms box.

- Home Zip code: _____
- Work/School zip code: _____
- Please circle all that apply:
 - I live on or near Busch Boulevard
 - I work on or near Busch Boulevard
 - I stop/visit businesses on or near Busch Boulevard
 - My school is on or near Busch Boulevard
Which school? _____
 - I travel on Busch Boulevard but rarely stop
 - I don't regularly use Busch Boulevard
 - Other: _____
- Which travel option(s) have you used on Busch Boulevard? Please circle all that apply:
 - Drive
 - Walk
 - Bike
 - HART Bus
 - School Bus
 - Share a ride with others (car, pool, taxi, etc.)
 - Other: _____
- When driving on Busch Boulevard, rate your satisfaction on the following (please circle 1 – 5):

Safe	5	4	3	2	1	Unsafe	n/a
Comfortable	5	4	3	2	1	Uncomfortable	n/a
Convenient	5	4	3	2	1	Inconvenient	n/a
- When walking/biking/riding the bus on Busch Boulevard, rate your satisfaction on the following (please circle 1 – 5):

Safe	5	4	3	2	1	Unsafe	n/a
Comfortable	5	4	3	2	1	Uncomfortable	n/a
Convenient	5	4	3	2	1	Inconvenient	n/a

7. How important are the following objectives in your vision for Busch Boulevard?

My Vision is for a Busch Boulevard where...	Very Important	Somewhat Important	Not Important	No Opinion
Trend users have shade and comfortable amenities				
A continuous sidewalk runs the length of the corridor				
Bicycle connectivity is enhanced to adjacent homes and businesses				
Safe and frequent options are provided for pedestrians and bicyclists to cross the corridor				
Aesthetic enhancements provide shade and add beauty to the corridor				
The corridor adds a unique and moving character to the community				
Existing businesses are preserved along the corridor				
The roadway attracts reinvestment into the corridor and surrounding areas				
Travelers experience efficient and reliable travel times				
Facilities are ADA compliant				
Citizens with limited mobility have enough time to cross the street				
Congestion is reduced on the corridor				
Crashes occur less frequently and are not as severe				
Drivers slow down to a safe speed				
Increased lighting improves visibility for drivers and pedestrians				
Children can travel to and from school safely, regardless of mode of travel				
Consolidated driveways reduce conflict points for vehicles and pedestrians				

8. Which of the following potential improvements would you support?

	Strongly Agree	Agree	Neutral	Disagree	Strongly Disagree
Add additional travel lanes					
Create a pedestrian-only phase at key intersections					
Improve traffic signal timing					
Add on-street parking					
Add mid-block crosswalks					
Landscaping, pedestrian lighting, etc.					
Lower speed limit					
Create a wide sidewalk along the entire corridor					
Add on-street bike lanes					
Add buffered bike lanes					
Add dedicated bus lane					
Add school zone					
Fill in sidewalk gaps					
Add bus shelters					
Add a landscaped median					

9. Any other comments and suggestions:

Thank you for taking the time to complete our User Preference Survey. Your valuable input will help inform the identification of challenges/issues and the development of potential solutions.

Next Steps



Next Steps for the Project Team

- Hold the visioning workshop and summarize feedback
- Create vision plan
- Develop preliminary purpose and need
- Develop alternatives and perform a comparative evaluation

Next Steps for the PAG

- Discuss the project in your sphere of influence
- Direct interested parties to the website to provide comments
- Direct interested parties to the website to “pin” issues in the corridor using the WikiMapping interactive tool
- Invite interested parties to the Visioning Workshop
- Provide feedback to the project team regarding your conversations with interested parties

Future PAG meetings

- PAG #3
 - ≈ January 2018
 - ≈ Purpose and need; range of solutions
- PAG #4
 - ≈ June 2018
 - ≈ Report on final near-term and long-term decisions

Next Steps - Schedule

SR 580/Busch Boulevard from North Dale Mabry Highway to North Nebraska Avenue Corridor Study

PUBLIC INVOLVEMENT PLAN SCHEDULE

ACTIVITY	2017												2018							
	Mar	Apr	May	June	July	Aug	Sept	Oct	Nov	Dec	Jan	Feb	Mar	April	May	June	July	Aug	Sept	
Notice to Proceed - Project Begins	★																			
Develop Public Involvement Plan																				
Mailing List																				
Elected Officials/Agency Kick-off Meeting					★															
Project Advisory Group (PAG) Meetings																				
Corridor Visioning Workshop																				
Alternatives Public Meeting																				
Final Corridor Alternatives & Strategies Report																				

Questions???



Please fill out this survey as best as you can and place in the Survey Forms box.

1. Home Zip code: _____

2. Work/School zip code: _____

3. Please **circle** all that apply:

- a. I live on or near Busch Boulevard
- b. I work on or near Busch Boulevard
- c. I shop/visit businesses on or near Busch Boulevard
- d. My school is on or near Busch Boulevard
Which school? _____
- e. I travel on Busch Boulevard but rarely stop
- f. I don't regularly use Busch Boulevard
- g. Other: _____

4. Which travel option(s) have you used on Busch Boulevard? Please **circle** all that apply:

- a. Drive
- b. Walk
- c. Bike
- d. HART Bus
- e. School Bus
- f. Share a ride with others (car pool, taxi, etc.)
- g. Other: _____

5. When **driving** on Busch Boulevard, rate your satisfaction on the following (**please circle 1 – 5**):

Safe	5	4	3	2	1	Unsafe	n/a
Comfortable	5	4	3	2	1	Uncomfortable	n/a
Convenient	5	4	3	2	1	Inconvenient	n/a

Additional Comments: _____

6. When **walking/biking/riding** the bus on Busch Boulevard, rate your satisfaction on the following (**please circle 1 – 5**):

Safe	5	4	3	2	1	Unsafe	n/a
Comfortable	5	4	3	2	1	Uncomfortable	n/a
Convenient	5	4	3	2	1	Inconvenient	n/a

Additional Comments: _____

7. How important are the following objectives in your vision for Busch Boulevard?

My Vision is for a Busch Boulevard where...	Very Important	Somewhat Important	Not Important	No Opinion
Transit users have shade and comfortable amenities				
A continuous sidewalk runs the length of the corridor				
Bicycle connectivity is enhanced to adjacent homes and businesses				
Safe and frequent options are provided for pedestrians and bicyclists to cross the corridor				
Aesthetic enhancements provide shade and adds beauty to the corridor				
The corridor adds a unique and inviting character to the community				
Existing businesses are preserved along the corridor				
The roadway attracts reinvestment into the corridor and surrounding areas				
Travelers experience efficient and reliable travel times				
Facilities are ADA compliant				
Citizens with limited mobility have enough time to cross the street				
Congestion is reduced on the corridor				
Crashes occur less frequently and are not as severe				
Drivers slow down to a safe speed				
Increased lighting improves visibility for drivers and pedestrians				
Children can travel to and from school safely, regardless of mode of travel				
Consolidated driveways reduce conflict points for vehicles and pedestrians				

Additional Comments:

8. Which of the following potential improvements would you support?

	Strongly Agree	Agree	Neutral	Disagree	Strongly Disagree
Add additional travel lanes					
Create a pedestrian-only phase at key intersections					
Improve traffic signal timing					
Add on-street parking					
Add mid-block crosswalks					
Landscaping, pedestrian lighting, etc.					
Lower speed limit					
Create a wide sidewalk along the entire corridor					
Add on-street bike lanes					
Add buffered bike lanes					
Add dedicated bus lane					
Add school zone					
Fill in sidewalk gaps					
Add bus shelters					
Add a landscaped median					

Additional Comments:

9. Any other comments and suggestions:

Thank you for taking the time to complete our User Preference Survey. Your valuable input will help inform the identification of challenges/issues and the development of potential solutions.

West Busch Boulevard (SR 580) Corridor Study

From North Dale Mabry Highway to North Nebraska Avenue

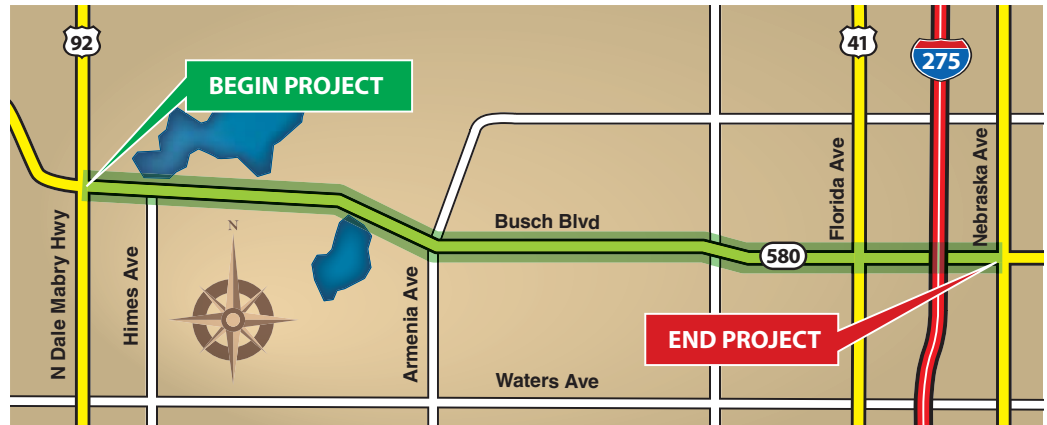
Hillsborough County, FL

Financial Project ID Number: 435908-1-22-01

Corridor Study Begins!

The Florida Department of Transportation (FDOT), District 7 is conducting a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue in Hillsborough County – a distance of approximately 3.3 miles. The West Busch Boulevard (SR 580) corridor is an east-west urban principal arterial connecting and serving local commuters and visitors between North Dale Mabry Highway and Nebraska Avenue. The corridor study area includes portions of unincorporated Hillsborough County and the City of Tampa and is the main route to the Busch Gardens Amusement Park (located approximately 2.15 miles east of the project limits at McKinley Drive).

The corridor supports a variety of land uses and demographics, including schools, churches, large and small businesses with direct access to and from SR 580, transit services, pedestrians, bicyclists and many small communities. The corridor also serves a large commuter population as a connection between Veterans Expressway to the west and I-75 to the east.



The West Busch Boulevard (SR 580) corridor study will use FDOT’s Complete Streets policy to showcase how Complete Streets approaches can shape a corridor. The study, which began in March 2017, is expected to continue for approximately 18 months and will include technical work and data analysis, along with various opportunities for the public to be involved in discussing a vision for West Busch Boulevard (SR 580) that makes it accessible for all users.

What is a Corridor Study?

A corridor study is the first step in planning by defining the corridor’s needs. A corridor study generally defines the relationships between a roadway and its adjacent land. Corridor studies are also used to: define acceptable levels of access and mobility; determine transportation system needs to support surrounding land uses, and identify operational deficiencies, among other things. This corridor study will include establishing a shared vision that will allow the community to help set project goals and objectives. The vision will guide the project team to develop a set of improvements that leverage local resources, match local conditions and desires, and are consistent with ongoing planning and development efforts.

Project Schedule

The West Busch Boulevard (SR 580) Corridor Study began in March 2017 and is anticipated to be completed in 18 months. Project activities will include:

- Identifying the travel related needs and issues within the West Busch Boulevard project corridor.
- Coordinating with key stakeholders to establish the vision for the corridor and identify solutions to the corridor issues and improve operational efficiency within the West Busch Boulevard corridor.
- Document the vision for all future improvements within the West Busch Boulevard corridor to maximize the “livability” for all users.

ACTIVITY	2017												2018						
	Mar	Apr	May	June	July	Aug	Sept	Oct	Nov	Dec	Jan	Feb	Mar	April	May	June	July	Aug	Sept
Notice to Proceed - Project Begins	★																		
Develop Public Involvement Plan																			
Mailing List																			
Elected Officials/Agency Kick-off Meeting				★															
Project Advisory Group (PAG) Meetings																			
Corridor Visioning Workshop										★									
Alternatives Public Meeting													★						
Final Corridor Alternatives & Strategies Report																			

Project Advisory Group (PAG)

A Project Advisory Group (PAG), composed of local government and agency representatives, are working with FDOT to identify corridor challenges along with potential solutions, assist with documenting the community goals, and develop a vision for the corridor that will guide the study development and future improvements. The PAG will meet at key points throughout the study to collaborate with the project team to ensure the vision and improvement alternatives are well-coordinated with local planning efforts, adjacent projects, and community goals.

How Can You Get Involved?

The project website is your best source for up-to-date information; you can get access at this link:

www.fdotd7studies.com/westbuschblvd/. In addition to the general study information that is already posted on the website, the project documents will be uploaded to this site as they are developed. However, you can already begin providing your input on this project using WikiMapping! This tool allows you to “pin” comments at specific locations along the corridor. You can pick the type of comment you have to help us understand the issues you observe in the corridor. Some of these issue types include: Bus/Transit, Connectivity, Intersection Issue, Pedestrian, Safety, and Traffic/Parking. If you have a comment on an issue that does not have a specific description, just use the “Other” category. The link to the WikiMapping tool is provided on the project website, but for direct access you can click on the following link:

<http://wikimapping.com/wikimap/West-Busch-Boulevard-Corridor-Study.html>. The project team will utilize this information to help inform the process and guide solutions.

Future opportunities for you to get involved include the upcoming Corridor Visioning Workshop that will be conducted in the Fall 2017. At this workshop, we will gather your input on the existing corridor and document your ideas to resolve some of the issues that you have observed. Our goal is to enlist your help to identify improvements that can be implemented in the West Busch Boulevard corridor to resolve the challenges. In advance of this workshop, a User Preference Survey will be available on the website in October; hard copies of this same survey will be available at the Corridor Visioning Workshop. These surveys will be used by the project team to understand the issues and concerns that you have regarding the corridor.

We want your input! We urge anyone interested in shaping the future of the West Busch Boulevard corridor to actively participate by visiting the website, get informed, document issues in the WikiMapping tool, attend meetings, and provide your ideas.

For more information or to comment, please contact:

Brian Shroyer, Project Manager
11201 N. McKinley Drive
Tampa, FL 33612
Office Phone: (813) 975-6449
brian.shroyer@dot.state.fl.us

Kristen Carson, Public Information Officer
11201 N. McKinley Drive
Tampa, FL 33612
Office Phone: (813) 975-6202 or 800-226-7220
kristen.carson@dot.state.fl.us

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT.

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220.

Comuníquese Con Nosotros

Nos importa mucho la opinión del público sobre el proyecto. Si usted tiene preguntas o comentarios, o simplemente desea más información, por favor comuníquese con nosotros. Nuestro representante, Señora Lilliam Escalera, 813-975-6445, Departamento de Transportación de Florida, 11201 N. McKinley Dr., Tampa, FL 33612, lilliam.escalera@dot.state.fl.us.

Appendix D



PROJECT ADVISORY GROUP (PAG) MEETING #2
West Busch Boulevard (SR 580) Corridor Study
 From North Dale Mabry Highway to North Nebraska Avenue
 Financial Project ID Number: 435908-1-22-01



Tuesday, October 24, 2017 • North Tampa Branch Library, 8916 North Boulevard, Tampa, Florida 33604

Please provide your comments below. If more space is needed, please use the back of this sheet or an additional sheet of paper. You may place your comments in the "Comment Box" provided at the meeting, or send to Brian Shroyer, Project Manager at the address on the back of this comment form, by November 6, 2017. Emailed comments are also acceptable to Brian.Shroyer@dot.state.fl.us

*All comments are part of the project record and are available for review by the public and the media.

- *We need to have a consistent road design/classification - ~~not~~ switching back and forth to urban/suburban/etc. in the street design creates an inconsistent road design. Think of the overall future condition → URBAN
- *Sidewalks are exposed and lack shade. Need for protection and shade/landscaping.
- *The entire corridor is in need of upgraded bike/ped connections.
- *ATMS and ~~lead~~ pedestrian Interval Signals
- *Mid Block HAWKS when signals are spaced too far apart.

CONTACT INFORMATION (PLEASE PRINT)

Name Wade Reynolds
 Title Senior Planner
 Agency / Organization Hillsborough MPO
 Phone Number 813 273-5774
 Email ReynoldsW@plancom.org

RETURN COMMENTS BY November 6, 2017 TO:

Brian L. Shroyer
 Multimodal Project Manager
 Florida Department of Transportation
 11201 N. McKinley Drive, MS 7-500
 Tampa, FL 33612
 Brian.Shroyer@dot.state.fl.us

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status.



PROJECT ADVISORY GROUP (PAG) MEETING #2
West Busch Boulevard (SR 580) Corridor Study
 From North Dale Mabry Highway to North Nebraska Avenue
 Financial Project ID Number: 435908-1-22-01



Tuesday, October 24, 2017 • North Tampa Branch Library, 8916 North Boulevard, Tampa, Florida 33604

Please provide your comments below. If more space is needed, please use the back of this sheet or an additional sheet of paper. You may place your comments in the "Comment Box" provided at the meeting, or send to Brian Shroyer, Project Manager at the address on the back of this comment form, by November 6, 2017. Emailed comments are also acceptable to Brian.Shroyer@dot.state.fl.us

*All comments are part of the project record and are available for review by the public and the media.

- Coming out of the PAG #2 meeting, I have 3 concerns/comments:*
- ① What are the ^{methods you're using to find} members of the public being recruited to attend the Nov. 16th Mtg? Here are ways to reach them; are you utilizing them? (Radio Station PSA's, Newspaper articles with photos, television news clips, [Flyers/posters] for Groceries, business windows, schools churches, buses, libraries, etc.
 - ② Who is the person representing [HART] bus system you are working with; when will you provide them with [posters] to communicate with bus patrons that they are wanted (and will be rewarded) at the public Mtg?
 - ③ I'm interested in volunteering for station #3 or station #6 to assist in engaging attendees to facilitate their input, which is the task of this public meeting.

I'm interested in your efforts/methods to recruit public members/stakeholders before this meeting, as well as, elicit their input during the meeting process.

CONTACT INFORMATION (PLEASE PRINT)

RETURN COMMENTS BY November 6, 2017 TO:

Name Amy E. Clark
 Title (public volunteer)
 Agency / Organization (HART patron)
 Phone Number 774-273-5442
 Email quaker.lady@yahoo.com

Brian L. Shroyer
 Multimodal Project Manager
 Florida Department of Transportation
 11201 N. McKinley Drive, MS 7-500
 Tampa, FL 33612
 Brian.Shroyer@dot.state.fl.us

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status.

Appendix E

WHAT HAVE YOU HEARD?

- REQUEST ELECTRONIC 8 1/2 x 11 w/
VISIONING WORKSHOP LOGISTICS
- SHARE FLYERS w/ HART, ETC.
- SOCIAL SERVICES - NE CORNER BUSCH/FL
- COMFORT OF VULNERABLE USERS IMPAT
- CONCERNS OVER FREQUENT CHANGE IN
CONTEXT CLASSIFICATIONS

Section 2.3

PAG Meeting 3

Project Advisory Group Meeting #3

Date: Tuesday, January 23, 2018
Time: 10:00 a.m. to 12:00 p.m.
Location: North Tampa Branch Library
8916 North Boulevard
Tampa, Florida 33604
Attendees: See Appendix A

Notification of Meeting

Meeting invitation letters were sent by email to elected officials, government officials, local agencies and other interested parties. The invitation letters included the meeting date, time and location, and a summary of the project.

A Florida Administrative Register (FAR) ad was published in Volume 44/09 on January 12, 2018. The Florida Department of Transportation posted meeting information on the FDOT Public Notices website.

Copies of the invitation letter, FAR ad and FDOT Public Notice website ad are included in Appendix B.

Meeting Handouts

A copy of the meeting agenda, PowerPoint and Newsletter #2 were distributed to the attendees. PAG participants were encouraged to discuss their thoughts and concerns regarding the corridor study.

A copy of the meeting handouts is available in Appendix C.

Attendee Comments

Two written comment forms were received. A copy of the comments received are included in Appendix D.

Summary

The following provides a summary of the items discussed at the meeting:

- Consultant Project Manager, Carol Barker, opened the meeting by welcoming new attendees and thanking attendees for their participation.

- Carol asked everyone, including the project team, to introduce themselves and describe their affiliations.
- Carol guided the PAG through a PowerPoint presentation.
- Carol gave an overview of the Visioning Workshop and information received from the public.
- Carol showed the PAG members the project video that was shown at the Visioning Workshop.
- Attendees participated in a “Corridor Vision Statement Exercise” They were given the opportunity to review and revise the draft vision statement. The draft vision statement and the final vision statement can be found in Appendix (E).
- Attendees participated in a “Potential Spot Improvements Exercise”. They reviewed the roll plots and potential spot improvements. They added notes to the roll plots. The roll plots can be found in Appendix (F).
- Carol advised the project team is working on a range of alternatives.
- Carol discussed the upcoming Alternatives Public Meeting.
- Carol encouraged the attendees to visit the [FDOT project website](#) and [WikkiMapping website](#).
- Carol provided closing remarks. The meeting ended at 12:00 p.m.

Discussion

Project Advisory Group (PAG) questions during discussion included:

Attendee: Why is lighting listed under aesthetic improvements instead of under safety concerns?

Carol Barker: The feedback we received from the public at the Visioning Workshop during a “Concerns and Issues” sticky note exercise was summarized into general categories. Agree that lighting is also a safety concern.

Attendee: I am surprised school safety wasn’t listed as a concern or issue at the Visioning Workshop. This is a big issue, how is it going to be addressed?

Carol Barker: This is information we received at the Visioning Workshop from the public. The team is reviewing PAG concerns also.

Attendee: Center two way left turn lanes are used as a thru lane and cause head on collisions, what addresses this?

Brian Shroyer: A landscaped median was one of the potential improvements presented at the Visioning Workshop. Median treatments are being considered in the development of the corridor study alternatives. Changes to the median cannot be included in the 3R as it requires a public hearing.

Attendee: Are the spot improvements in the 5 year plan?

Brian Shroyer: I am working on it. I am pushing the safety goals to leverage funding.

Attendee: When are you going to merge PAG and community input?

Carol Barker: All recommendations consider PAG and community input equally. In advance of the alternatives public meeting, the MPO will be able to review the alternatives.

Attendee: What type of public outreach has been done?

Karen Harrell: Business owners and tenants were mailed meeting invitations for the Visioning Workshop. Invitations were posted on the project website and on the FDOT meeting information website. Elected officials, agency partners, interested parties and PAG members were sent meeting invitations via e-mail. Fliers and newsletters have been hand distributed along the corridor where large numbers of people gather. Schools received fliers and newsletters to place in their school front offices. Florida Administrative Register (FAR) ads were placed. Meeting reminder phone calls have been made to PAG members and other interested parties to remind them of upcoming meetings.

Attendee: I can't believe closing suicide lanes and safety didn't rank higher on the board.

Brian Shroyer: The PAG and the public may not have the same views on issues. We are reviewing information received from the public and the PAG. Access management changes will happen. We will address concerns with the existing center two way left turn lanes. As of now, FDOT is not supportive of a designated school zone because of the proximity of the traffic signals. We need the push from the PAG to help get this done.

Attendee: Pedestrian improvements is one of the key features that needs to be improved, specifically sidewalks.

Brian Shroyer: We know there are pedestrian safety concerns. We are discussing and addressing those issues. We are working on the alternatives and determining options for filling sidewalk gaps.

Attendee: You need to do outreach to high school students, specifically Chamberlain High School. You really need to talk to the students and get their input.

END OF MEETING SUMMARY

This meeting summary was prepared by Karen Harrell, Public Involvement Coordinator. If you feel clarifications are necessary, or if this differs from your understanding, please notify Karen by telephone at 352-257-1651 or by e-mail at Karen.Harrell@qcausa.com within (5) working days upon receipt of this summary.

Appendix A



PROJECT ADVISORY GROUP MEETING #3
West Busch Boulevard (SR 580) Corridor Study
 From North Dale Mabry Highway to North Nebraska Avenue
 Financial Project ID Number: 435908-1-22-01



Tuesday, January 23, 2018 from 10:00 am to 12:00 pm ■ North Tampa Branch Library, 8916 North Boulevard, Tampa, FL 33604

Project Advisory Group (PAG) Sign-In Sheet

NAME (PLEASE PRINT)	AGENCY / ORGANIZATION	TITLE	PHONE	EMAIL
Kym Graves	QIA	Comm Manager		kymbady.gm@verizon.com
Karen Harrell	QCA	Comm Mgr.	352-257-1651	Karen.Harrell@QCAVSA.com
Lester Ottinger	HCPs	FCI	813-982-5500	Lester.Ottinger@sdhc.k12.fl.us
Fatzin Zafarian	FDA	GEC PM	813-975-6155	Fatzin.Zafarian@dot.state.fl.us
DAVID SPANER	FDOT	PROJECT MANAGER	813-975-6449	DAVID.SPANER@DOT.STATE.FL.US
Jenn Rhodes	CH2M	Planner	407-497-9319	Jenn.Rhodes@ch2m.com
Carol Barker	CH2M	Consultant Project Manager	407-650-2173	Carol.barker@ch2m.com
Chuck Seigel			813-505-6066	Seigel@verizon.net
RUD WAREHEGA	HILLS MPO		813-282-5840	RUD@PACT.COM.ORG
David Hernandez	CDLM	Routing Designer		David.Hernandez@ch2m.com
Wade Reynolds	MPO	Planner	or 414	or 414
Salvo Cann	TRC	Planner		psav@plancm.com
Johnny Wong	MPO	Senior Planner	or 414	or 414
Lisa SINDA	MPO	Principal Planner	"	"
Colleen Roof	Busch Gardens	Community Relations	813-987-5300	colleen.roof@buschgardens.com
LINDA Walker	MART	Planner II	813-384-6554	LWalker@ghmnet.org
LISA Montelione	CDC of Tampa	Employee Relations	813-695-5030	Lisa.Montelione@cdcotampa.org

Appendix B

Date: January 05, 2018

Subject: **Project Advisory Group (PAG) Meeting #3**
West Busch Boulevard (SR 580) Corridor Study
from North Dale Mabry Highway to North Nebraska Avenue
Hillsborough County, Florida
FDOT Financial Project ID Number: 435908-1-22-01

Dear Project Stakeholder:

The Florida Department of Transportation (FDOT), District Seven is conducting a Corridor Study on the above referenced study. This letter is an invitation for you to participate in the third Project Advisory Group (PAG) meeting for this project. PAG meeting #3 is scheduled for Tuesday, January 23, 2018, from 10:00 AM to Noon at the **North Tampa Branch Library, 8916 North Boulevard, Tampa, Florida 33604.**

The West Busch Boulevard Corridor study limits extend from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The study area includes portions of incorporated Hillsborough County and the City of Tampa. The purpose of the Corridor Study is to work with the community, stakeholders and a Project Advisory Group (PAG) to develop a vision and identify needs and issues in the corridor.

The PAG is an important component of the study. It serves as a special advisory resource to the FDOT and the consultant team. The PAG participants will help develop a vision for the corridor that will guide proposed short and long-term recommendations. This may include issues related to capacity, traffic operations, safety, access and egress, mobility, freight movements, transit, bicycles, and pedestrian movements.

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220 at least seven (7) working days in advance of the meeting.

If you have any questions about membership in the PAG or require further information concerning this project, please contact Brian Shroyer, Florida Department of Transportation Project Manager at 813-975-6449 or by email at Brian.Shroyer@dot.state.fl.us.

Sincerely,
Brian Shroyer
Multimodal Project Manager
Florida Department of Transportation

FDOT Public Notices Website:

WEST BUSCH BOULEVARD (SR 580) CORRIDOR STUDY PROJECT ADVISORY GROUP (PAG) MEETING #3

District: Seven

Meeting Type: Meeting

Date: Tuesday, January 23, 2018

Time: 10 AM to Noon

Location Name: North Tampa Branch Library

Street Address: 8916 North Boulevard

City: Tampa, FL 33604

Purpose: The Florida Department of Transportation, District Seven, is holding the third Project Advisory Group (PAG) meeting for the West Busch Boulevard (SR 580) Corridor Study. The limits of the project extend from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The corridor study area includes portions of unincorporated Hillsborough County and the City of Tampa.

The purpose of the Corridor Study is to work with the community, stakeholders and a Project Advisory Group (PAG) to develop a vision and identify needs and issues in the corridor. The PAG is an important component of the study. It serves as a special advisory resource to the FDOT and the consultant team. The PAG participants will help develop a vision for the corridor that will guide proposed short and long-term recommendations. This may include issues related to capacity, traffic operations, safety, access and egress, mobility, freight movements, transit, bicycles, and pedestrian movements. A copy of the agenda may be obtained by contacting: Mr. Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us.

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220 at least seven (7) working days in advance of the meeting.

Comuniquese Con Nosotros

Nos importa mucho la opinión del público sobre el proyecto. Si usted tiene preguntas o comentarios, o si simplemente desea más información, por favor comuniquese con nuestra representante, Lilliam Escalera, al (813) 975-6403, Departamento de Transportación de Florida, 11201 N. McKinley Dr., Tampa, FL 33612, Lilliam.Escalera@dot.state.fl.us

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT.

Primary Contact: Brian Shroyer, FDOT Project Manager

Primary Phone: (813) 975-6449

Primary E-mail: Brian.Shroyer@dot.state.fl.us

Additional Contact: Christopher Speese, Public Involvement Coordinator

Additional Phone: (813) 975-6405

Additional E-mail: Christopher.Speese@dot.state.fl.us

Florida Administrative Register Ad:

The Florida Department of Transportation, District Seven announces a public meeting to which all persons are invited.

DATE AND TIME: January 23, 2018, 10:00 AM to Noon

PLACE: North Tampa Branch Library, 8916 North Boulevard, Tampa, FL 33604

GENERAL SUBJECT MATTER TO BE CONSIDERED: The Florida Department of Transportation (FDOT), District Seven, is holding the third Project Advisory Group (PAG) meeting for the West Busch Boulevard (SR 580) Corridor Study. The limits of the project extend from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The corridor study area includes portions of unincorporated Hillsborough County and the City of Tampa.

The purpose of the Corridor Study is to work with the community, stakeholders and a Project Advisory Group (PAG) to develop a vision and identify needs and issues in the corridor. The PAG is an important component of the study. It serves as a special advisory resource to the FDOT and the consultant team. The PAG participants will help develop a vision for the corridor that will guide proposed short and long-term recommendations. This may include issues related to capacity, traffic operations, safety, access and egress, mobility, freight movements, transit, bicycles, and pedestrian movements.

A copy of the agenda may be obtained by contacting: Mr. Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability, or family status. Pursuant to the provisions of the Americans with Disabilities Act, any person requiring special accommodations to participate in this workshop/meeting, or persons who require translation services (free of charge) are asked to advise the agency at least 7 days before the workshop/meeting by contacting: Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220 or via email at Christopher.Speese@dot.state.fl.us

Comuniquese Con Nosotros

Nos importa mucho la opinión del público sobre el proyecto. Si usted tiene preguntas o comentarios, o si simplemente desea más información, por favor comuniquese con nuestra representante, Lilliam Escalera, al (813) 975-6403, Departamento de Transportación de Florida, 11201 N. McKinley Dr., Tampa, FL 33612, Lilliam.Escalera@dot.state.fl.us

If you are hearing or speech impaired, please contact the agency using the Florida Relay Service, 1(800)955-8771 (TDD) or 1(800)955-8770 (Voice).

For more information, you may contact: Mr. Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT.

Appendix C



PROJECT ADVISORY GROUP MEETING #3

AGENDA

West Busch Boulevard (SR 580) Corridor Study
from North Dale Mabry Highway to North Nebraska Avenue
Hillsborough County, Florida
FDOT Financial Project ID Number: 435908-1-22-01
North Tampa Branch Library
January 23, 2018 – 10:00 am to 12:00 pm

1. Introductions
2. Visioning Workshop overview
3. Corridor Vision Statement Discussion
4. Preliminary Spot Improvement options
5. Next Steps
6. Closing Remarks and Adjourn

West Busch Boulevard (SR 580) Corridor Study

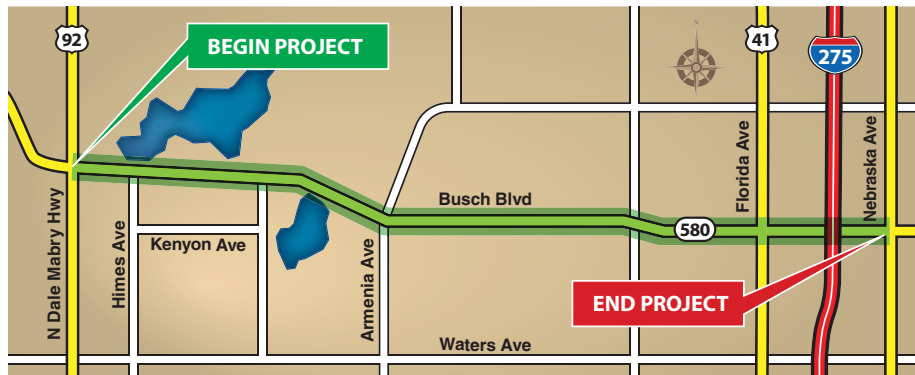
From North Dale Mabry Highway to North Nebraska Avenue

Hillsborough County, FL

Financial Project ID Number: 435908-1-22-01

Corridor Study Update

The Florida Department of Transportation (FDOT), District 7 is currently conducting a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue in Hillsborough County – a distance of approximately 3.3 miles.



The study began in March 2017 and will conclude in late fall of 2018. The outcome of the study will be a Corridor Alternatives and Strategies Report which will document the vision for short-term and long-term improvements within the corridor to meet the mobility and connectivity needs of all users, regardless of their transportation mode of choice.

To date, the study team has:

- Evaluated existing conditions, including existing and future traffic levels, crash history, existing bicycle, pedestrian and transit activity and infrastructure, and the context of the surrounding land uses;
- Developed potential goals and objectives for the future of West Busch Boulevard related to accessibility and connectivity, economic development, mobility, and safety;
- Began developing a range of potential improvement alternatives that address the anticipated traffic needs, multi-modal considerations (bicycle, pedestrian, transit), address safety concerns, incorporate suggestions based on comments received so far (from agencies, elected officials, Wiki-Mapping interactive tool, and public meetings). These improvements will be separated into two categories: those that can be implemented within the existing right-of-way (near term) and those that may require additional right-of-way and require further study;
- Held three Project Advisory Group (PAG) meetings, where local government and agency representatives have worked with FDOT to identify corridor challenges along with potential solutions, assisted in documenting community goals, and helped develop a vision for the corridor that will guide the study development and future improvements;

About the Corridor

The West Busch Boulevard (SR 580) corridor is an east-west urban principal arterial connecting and serving local commuters and visitors between North Dale Mabry Highway and Nebraska Avenue. The corridor study area includes portions of unincorporated Hillsborough County and the City of Tampa and is the main route to the Busch Gardens Amusement Park (located approximately 2.15 miles east of the project limits at McKinley Drive).

The corridor supports a variety of land uses and demographics, including schools, churches, large and small businesses with direct access to and from SR 580, transit services, pedestrians, bicyclists and many small communities. The corridor also serves a large commuter population as a connection between Veterans Expressway to the west and I-275 to the east.

What is a Corridor Study?

A corridor study is the first step in planning by defining the corridor's needs. A corridor study generally defines the relationships between a roadway and its adjacent land. Corridor studies are also used to: define acceptable levels of access and mobility; determine transportation system needs to support surrounding land uses, and identify operational deficiencies, among other things. This corridor study will include establishing a shared vision that will allow the community to help set project goals and objectives. The vision will guide the project team to develop a set of improvements that leverage local resources, match local conditions and desires, and are consistent with ongoing planning and development efforts.

- Conducted a Public Visioning Workshop, where members of the community were invited to provide input on the current conditions and concerns along the corridor, prioritize goals and objectives of the study, and identify potential improvements that are most important to them; and
- Solicited input online through the project website, Wikimapping tool, and a Community Preference Survey.

▶ Learn more about the project here: www.fdot7studies.com/westbuschblvd/

Next Steps

Based on the technical work, data analysis, and input received to date from the Project Advisory Group (PAG) and the public, the goals and objectives and desired potential improvements will be further defined in early 2018 into options for future typical sections and alignment alternatives.

These draft recommendations will then be presented to the public for additional input at the Alternatives Public Meeting, anticipated to be held in Spring 2018. One additional PAG meeting is also scheduled before the conclusion of the study.

The Final Corridor Alternatives and Strategies Report will be published in fall of 2018. In addition to documenting the corridor vision, the report will document improvements that can be advanced to the design phase; these are improvements within the existing right-of-way. The report will also document long-term improvements that must be further refined through a more detailed Project Development and Environment (PD&E) Study.

ACTIVITY	2017												2018						
	Mar	Apr	May	June	July	Aug	Sept	Oct	Nov	Dec	Jan	Feb	Mar	April	May	June	July	Aug	Sept
Notice to Proceed - Project Begins	★																		
Develop Public Involvement Plan																			
Mailing List																			
Elected Officials/Agency Kick-off Meeting																			
Project Advisory Group (PAG) Meetings																			
Corridor Visioning Workshop																			
Alternatives Public Meeting																			
Final Corridor Alternatives & Strategies Report																			

How Can You Stay Involved?

We want your input! We urge anyone interested in shaping the future of the West Busch Boulevard corridor to actively participate by visiting the website, get informed, document issues in the WikiMapping tool, attend meetings, and provide your ideas.

The project website is your best source for up-to-date information; you can get access at this link:

www.fdot7studies.com/westbuschblvd/. The website not only houses general study information, but also all project documents that have been developed to date, including presentations and meeting materials presented at the PAG meetings and the Public Visioning Workshop. The website will continue to be updated throughout the study with the latest documents and information.

You also have the continued opportunity to provide input using WikiMapping! This tool allows you to “pin” comments at specific locations along the corridor. The link to the WikiMapping tool is provided on the project website, but for direct access you can click on the following link: <http://wikimapping.com/wikimap/West-Busch-Boulevard-Corridor-Study.html>. The project team will continue to utilize this information to help inform the process and guide solutions.

Future opportunities for you to get involved include the upcoming Alternatives Public Meeting, anticipated to be held in Spring 2018. Please keep a look out on the project website for additional information regarding the Alternatives Public Meeting!

For more information or to comment, please contact:

Brian Shroyer, Project Manager
11201 N. McKinley Drive
Tampa, FL 33612
Office Phone: (813) 975-6449
brian.shroyer@dot.state.fl.us

Kristen Carson, Public Information Officer
11201 N. McKinley Drive
Tampa, FL 33612
Office Phone: (813) 975-6202 or 800-226-7220
kristen.carson@dot.state.fl.us

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT.

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Project Advisory Group Meeting #3 West Busch Boulevard (SR 580) Corridor Study

from North Dale Mabry Highway to North Nebraska Avenue

Hillsborough County, Florida

FDOT Financial Project ID Number: 435908-1-22-01

North Tampa Branch Library

January 23, 2018 – 10:00 am to 12:00 pm



Review of the Public Visioning Workshop



Public Visioning Workshop

- November 16, 2017
 - ≈ 5:30 p.m. – 7:30 p.m.
 - ≈ Open house format
 - ≈ Christian Family Church
- Invitation Statistics
 - ≈ 33 Elected Officials
 - ≈ 115 Appointed Officials
 - ≈ 10 Native American Tribes
 - ≈ 1255 Property Owners
 - ≈ 642 Current Residents

FDOT

WEST BUSCH BOULEVARD STUDY

You're invited to take part in a Florida Department of Transportation (FDOT) Visioning Workshop for a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue in Hillsborough County – a distance of approximately 3.3 miles. The FDOT is looking for ways to improve West Busch Boulevard (SR 580) for pedestrians, bicyclists, transit users and motorists. At this workshop, we will gather your input on the existing corridor and document your ideas to resolve some of the issues that you have observed.

Thursday, November 16, 2017, 5:30 p.m. to 7:30 p.m.
Christian Family Church, 3457 W. Kenyon Avenue, Tampa, FL 33614




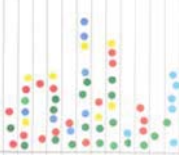


For More Information Contact:
Brian Shroyer, CPM
Project Manager
Florida Department of Transportation
11201 N. McKinley Drive
Tampa, FL 33812
813-975-6449
brian.shroyer@dot.state.fl.us
Visit Our Website
fdot7studies.com/westbuschblvd/

BEGIN PROJECT

END PROJECT

Map labels: N Dale Mabry Hwy, Himes Ave, Kenyon Ave, Armenta Ave, Busch Blvd, Volusia Ave, Florida Ave, Nebraska Ave, SR 275, SR 41, SR 580.

Public Visioning Workshop

- 5 Stations:
 - ≈ Station 1: 10 minute **Project Introduction Video** 
 - ≈ Station 2: **Existing Conditions** displays
 - Project location map
 - Existing typical sections
 - Crash map
 - Bicycle and pedestrian counts
 - Existing and future (no-build) traffic LOS
 - ≈ Station 3: **Concerns and Issues** (sticky-note exercise) 
 - ≈ Station 4: **Developing the Corridor Vision** (goals and objectives dot exercise) 
 - ≈ Station 5: **Potential Improvements** (dot exercise) 
 - ≈ Station 6: **Comments** - user preference surveys and comment forms 

E-635



Visioning Workshop Video

- Watch presentation video



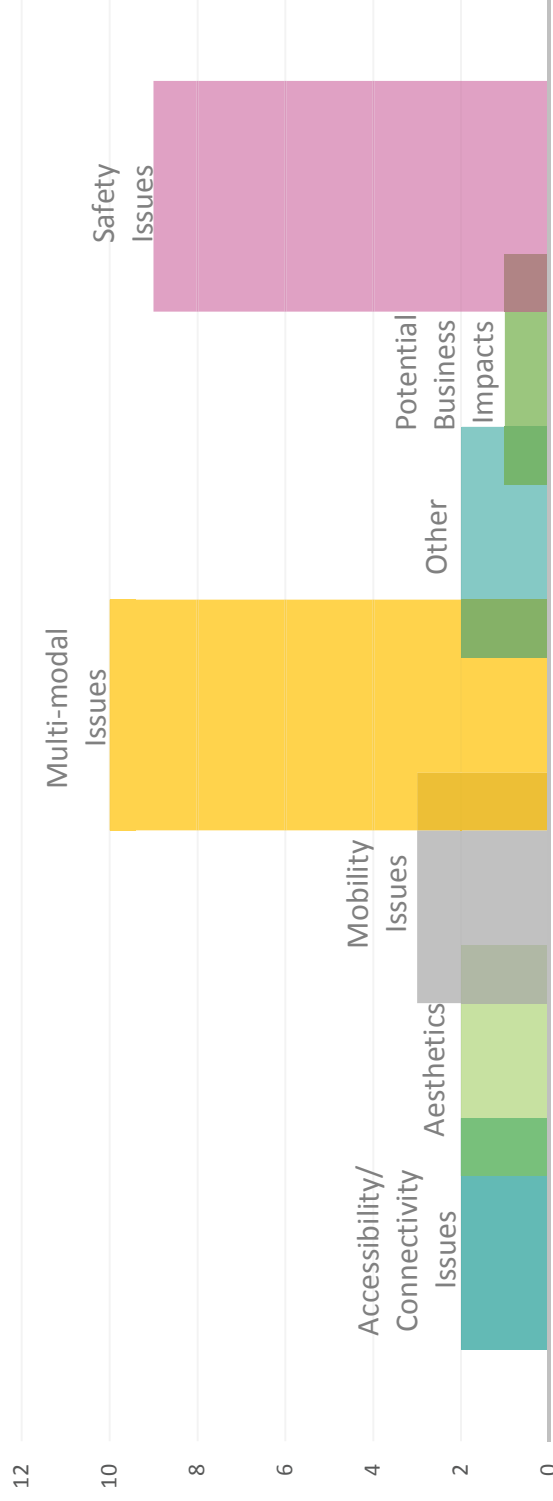
Feedback at the Workshop

- 2 Comment forms
- No responses to preference survey
- High participation at the interactive board stations

Feedback at the Workshop: Concerns and Issues Sticky-note exercise

- Hardcopy plots and a SmartBoard to record feedback
- 29 comments provided
- Study team added this feedback to the Wiki-mapping tool

Sticky-note exercise feedback categories



1

E-638

Feedback at the Workshop: Concerns and Issues Sticky-note exercise

- Safety concerns
 - ≈ Safety issues at the N. Dale Mabry ramp
 - ≈ Vehicles travel above speed limit
 - ≈ Two-way left turn lanes are used as a travel lane
- Multi-modal concerns
 - ≈ Safety concerns for pedestrians and bicyclists due to high # of driveways
 - ≈ School bus and HART drop-off and pick-up locations
 - ≈ Sidewalk gaps
 - ≈ Crosswalks are not well marked and do not feel safe to cross
 - ≈ Concerns about the amenities provided at the bus stops (specifically eastbound)
- Traffic concerns
 - ≈ Signal timing
 - ≈ Congestion at intersections
 - ≈ Difficulty of getting into/out of driveway near Mossvale Lane/ White Trout Lake proposed development
- Aesthetic improvements (lighting, raised medians and landscaping)

Feedback at the Workshop: Developing the Vision

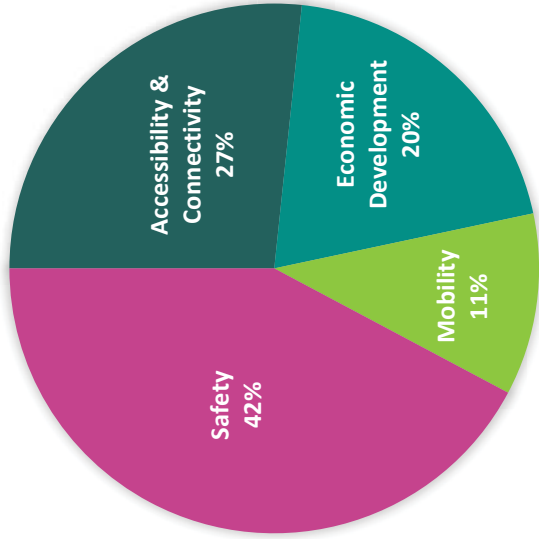
Developing the Corridor Vision

Now we want to know what is important to you! Help us shape the future vision of the West Busch Boulevard Corridor. Vote for the goals and objectives presented here, or add in your own!

My Vision is for a West Busch Boulevard where...

Category	Goal/Objective	Rating
ACCESSIBILITY & CONNECTIVITY	Transit users have shade and comfortable amenities	★
	A continuous sidewalk runs the length of the corridor	★★★
	Bicycle connectivity is enhanced to adjacent homes and businesses	★
	Safe and frequent options are provided for pedestrians and bicyclists to cross the corridor	★★★★
ECONOMIC DEVELOPMENT	Aesthetic enhancements provide shade and adds beauty to the corridor	★
	The corridor adds a unique and inviting character to the community	★★
	Existing businesses are preserved along the corridor	★★★★
	The roadway attracts reinvestment into the corridor and surrounding areas	★★
MOBILITY	Travelers experience efficient and reliable travel times	★★
	Facilities are ADA compliant	★
	Congestion is reduced on the corridor	★★
	Citizens with limited mobility have enough time to cross the street	★
SAFETY	Crashes occur less frequently and are not as severe	★★★
	Drivers slow down to a safe speed	★★★★
	Increased lighting improves visibility for drivers and pedestrians	★★
	Children can travel to and from school safely, regardless of mode of travel	★★★
	Consolidated driveways reduce conflict points for vehicles and pedestrians	★★★★

RESPONSES BY GOAL



PAG Exercise: Feedback helped shape the draft corridor vision statement – now we will refine it.

Busch Boulevard is a corridor that offers safe and comfortable access both through and across the corridor for all users, regardless of their transportation mode of choice.

Feedback at the Workshop:

Potential Improvements

The limited available right-of-way along the corridor means that many potential improvements can not be accommodated without impacting adjacent properties. Please help prioritize the list of possible improvements below using the provided stickers to indicate your top five desired improvements for the corridor, with "1" being the most preferred improvement.

- 1 ADD ADDITIONAL TRAVEL LANES 5 4
- CREATE A PEDESTRIAN-ONLY PHASE AT KEY INTERSECTIONS 3 1 2 1 5 3
- IMPROVE TRAFFIC SIGNAL TIMING 4 2 3 2 2 3
- ADD ON-STREET PARKING
- ADD MID-BLOCK CROSSWALKS 4
- LANDSCAPING, PEDESTRIAN LIGHTING, ETC. 2 4 5
- LOWER SPEED LIMIT 2 7 3 4 4 3 5 5
- CREATE A WIDE SIDEWALK ALONG ENTIRE CORRIDOR 2 4 1 3 2
- ADD ON-STREET BIKE LANES
- ADD BUFFERED BIKE LANES 1
- ADD A DEDICATED BUS LANE
- ADD SCHOOL ZONE 1 1 3
- FILL IN SIDEWALK GAPS 3 1 3 1 4
- ADD BUS SHELTERS 2 5 2
- ADD A LANDSCAPED MEDIAN 5

(WRITE IN ANYTHING ELSE!)

WEST BUSCH BOULEVARD STUDY
PROPOSED IMPROVEMENTS

- Top preferred improvements:

- ≈ Lower the speed limit

- ≈ Create a pedestrian-only phase at key intersections

- ≈ Improve traffic signal timing

- ≈ Construct wide sidewalk/complete sidewalk gaps

Potential Spot Improvements – near term and within existing R/W

- SEE ROLL PLOTS for EACH ROADWAY SEGMENT

PAG Exercise: Revisit the Corridor Vision Statement

*Busch Boulevard is a corridor that offers safe
and comfortable access both through and
across the corridor for all users, regardless of
their transportation mode of choice.*

Next Steps



Next Steps for the Project Team

- Refine range of alternatives
 - ≈ Near-term improvements
 - ≈ Long-term improvements – requires further study.
- Schedule coordination meetings with agency partners: MPO, Hillsborough Co, City of Tampa
- Conduct Alternatives Public Meeting – Spring 2018

Next Steps for the Project Advisory Group

- Collect pertinent data points for the Project Team.
- Attend PAG #4
 - ≈ Summer 2018
 - ≈ Report on final near-term and long-term decisions

Next Steps - Schedule

SR 580/Busch Boulevard from North Dale Mabry Highway to North Nebraska Avenue Corridor Study

PUBLIC INVOLVEMENT PLAN SCHEDULE

ACTIVITY	2017												2018							
	Mar	Apr	May	June	July	Aug	Sept	Oct	Nov	Dec	Jan	Feb	Mar	April	May	June	July	Aug	Sept	
Notice to Proceed - Project Begins	★																			
Develop Public Involvement Plan																				
Mailing List																				
Elected Officials/Agency Kick-off Meeting					★															
Project Advisory Group (PAG) Meetings																				
Corridor Visioning Workshop																				
Alternatives Public Meeting																				
Final Corridor Alternatives & Strategies Report																				

Questions???



Appendix D



PROJECT ADVISORY GROUP (PAG) MEETING #3

West Busch Boulevard (SR 580) Corridor Study

From North Dale Mabry Highway to North Nebraska Avenue

Financial Project ID Number: 435908-1-22-01



Tuesday, January 23, 2018 • North Tampa Branch Library, 8916 North Boulevard, Tampa, Florida 33604

Please provide your comments below. If more space is needed, please use the back of this sheet or an additional sheet of paper. You may place your comments in the "Comment Box" provided at the meeting, or send to Brian Shroyer, Project Manager at the address on the back of this comment form, by February 2, 2018. Emailed comments are also acceptable to Brian.Shroyer@dot.state.fl.us

*All comments are part of the project record and are available for review by the public and the media.

→ DESIGNATION of a School Zone, esp. @ Intersections EAST & WEST OF CHAMBERLAIN H.S. @ well as at NORTH BLVD. With the school on the South East corner & a public library on the North West corner it is a highly used pedestrian & bicycling route. At least one death has occurred in the area in the past couple of years, & countless accidents #SAFETY FIRST!

→ Engaging students at Chamberlain H.S. to offer comments & suggestions on the project

→ Engage Library users in the conversation

→ Use colorful striping at crosswalks

→ Add pedestrian height lighting (in addition to existing)

CONTACT INFORMATION (PLEASE PRINT)

RETURN COMMENTS BY February 2, 2018 TO:

Name LISA J. Montelione

Brian L. Shroyer

Title Employer Relations

Multimodal Project Manager

Agency / Organization CDC of Tampa

Florida Department of Transportation

Phone Number 813-695-5030

11201 N. McKinley Drive, MS 7-500

Email LISA.Montelione@cdc of Tampa. OKG

Tampa, FL 33612

Brian.Shroyer@dot.state.fl.us

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status.



PROJECT ADVISORY GROUP (PAG) MEETING #3
 West Busch Boulevard (SR 580) Corridor Study
 From North Dale Mabry Highway to North Nebraska Avenue
 Financial Project ID Number: 435908-1-22-01



Tuesday, January 23, 2018 • North Tampa Branch Library, 8916 North Boulevard, Tampa, Florida 33604

Please provide your comments below. If more space is needed, please use the back of this sheet or an additional sheet of paper. You may place your comments in the "Comment Box" provided at the meeting, or send to Brian Shroyer, Project Manager at the address on the back of this comment form, by February 2, 2018. Emailed comments are also acceptable to Brian.Shroyer@dot.state.fl.us

*All comments are part of the project record and are available for review by the public and the media.

Near term - right-of-way items.

Reopen lanes IN DM to Aracuan

RRR: fill gaps Aracuan to Plant

mediated fence on south side of road directly adjacent to sidewalk

RRR: North South major interest in improvement for red & WSC -
 improvement north/south crossing, not just on Busch but into the neighborhood. Sidewalk widening/mid block crossings

provide cover for transit riders

RRR improve markings like uniform / turning / safety presentations

utilize "place making" projects / economic development

Chamberlin History - Presentation take film into school.

CONTACT INFORMATION (PLEASE PRINT)

RETURN COMMENTS BY February 2, 2018 TO:

Name Pedro Parra
 Title Principal Planner - Community & Land Use
 Agency / Organization Planning Commission
 Phone Number 813-625-8020
 Email pparra@planning.com, 019

Brian L. Shroyer
 Multimodal Project Manager
 Florida Department of Transportation
 11201 N. McKinley Drive, MS 7-500
 Tampa, FL 33612
 Brian.Shroyer@dot.state.fl.us

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status.

Appendix E

SEE IF TRAFFIC OPS CAN REACH OUT TO SCHOOLS

*Busch Boulevard is a corridor that offers **safe** and comfortable access both through and **across** the corridor for all users, regardless of their transportation mode of choice.*

The

*Busch Boulevard is a corridor that offers **safe** and
comfortable access both through and **across** the
corridor for all users, regardless of their
transportation mode of choice.*

Handwritten note: *Handwritten note: "A corridor"*

*Busch Boulevard is a corridor that offers **safe** and comfortable access both through and across the*

Handwritten note: *Handwritten note: "all users"*

corridor for all users, regardless of their

transportation mode of choice.

safe, comfortable
and convenient
^

Busch Boulevard is a corridor that offers **safe** and

comfortable access both through and **across** the

and all modes,

corridor for all users, regardless of their

transportation mode of choice. ?

mode 'choice'
is determined
by income

*The Busch Boulevard corridor offers **safe**, comfortable
and convenient access through and **across** the
corridor for all users and all travel modes.*

Appendix F

Project Sheet 1 of 3



Potential Corridor Wide Improvements:

- Add Pedestrian Lighting
- Add landscaping in existing opportunity areas
- Interconnect signals to increase platooning
- Add YOUR SPEED radar signs

Potential Spot Improvements:

1. Add Gunn Highway Sidewalk (County jurisdiction)
2. Add SIGNAL AHEAD sign, add rumble strips, add mast arm signals, cross walk(s), lighting, N.W. pedestrian island
3. Extend WB right turn lane



Potential Spot Improvements:

- 4. Complete sidewalks on south side of Busch Blvd (requires curb reconstruction)
- 5. Complete sidewalks on south side of Busch Blvd (requires curb reconstruction)
- 6. Add mid-block crosswalk at Willow Ave

City of Tampa
Unincorporated Hillsborough County

Begin Pavement
Resurfacing by FDOT
IDP 4370243-138-01

Proposed Armenia Avenue
Interim Pavement
Improvements
by City of Tampa
IDP 4370243-138-01

6

5

4



- Potential Spot Improvements:**
7. Leading Pedestrian Interval or Ped-only signal phase
 8. Add colored concrete pavement to intersection and crosswalks
 9. Add 20 MPH school zone around North Blvd
 10. Add refuge island in S.E. corner to shorten crosswalks
 11. Add refuge islands in N.E. and S.E. to shorten crosswalks

Section 2.4

PAG Meeting 4

Project Advisory Group Meeting #4

Date: Thursday, May 31, 2018
Time: 10:00 a.m. to 12:00 p.m.
Location: North Tampa Branch Library
8916 North Boulevard
Tampa, Florida 33604
Attendees: See Appendix (A)

Notification of Meeting

Meeting invitation letters were sent by email to the elected officials, government officials, local agencies and other interested parties. The invitation letters included the meeting date, time and location, and a summary of the project.

A Florida Administrative Register (FAR) ad was published in Volume 44/99 on May 21, 2018. The Florida Department of Transportation posted meeting information on the FDOT Public Notices website.

Copies of the invitation letter, FAR ad and FDOT Public Notice website ad are included in Appendix (B).

Meeting Handouts

A copy of the meeting agenda and PowerPoint were distributed to the attendees. PAG participants were encouraged to discuss their thoughts and concerns regarding the corridor study.

A copy of the meeting handouts and is available in Appendix (C).

Attendee Comments

Seven written comment forms were received. A copy of the comments received are included in Appendix (D).

Summary

The following provides a summary of the items discussed at the meeting:

- Consultant Project Manager (CH2M), Carol Barker, opened the meeting by welcoming attendees and thanking attendees for their participation.

- Carol guided the PAG through a PowerPoint presentation (Appendix C).
- Carol explained the six stations of boards and advised that the upcoming Alternatives Public Meeting would be set-up similar. The stations boards can be found in Appendix (E).
- Attendees participated in a “Long Term Improvements Exercise”. They were given “Happy or Not” stickers and asked to use their stickers to provide input on typical sections. Results of the exercise can be found in Appendix (F).
- Attendees participated in a “Mix & Match Puzzle Exercise” that requested additional typical section feedback based on the context classifications within the corridor. Results of the exercise can be found in Appendix (G).
- Carol advised the PAG will have an opportunity to review the Corridor Alternatives and Strategies Report before it is Final.
- Cris Schooley (CH2M) discussed the Near-Term Improvements board in detail.
- Jenn Rhodes (CH2M) explained the Evaluation Matrix and how it was scored.
- Carol discussed the upcoming Alternatives Public Meeting.
- Carol provided closing remarks. The meeting ended at 12:00 p.m.

Discussion

Project Advisory Group (PAG) questions during discussion included:

Attendee: How many people attended the Visioning Workshop and how were they notified?

Carol Barker: About 20 people attended the Visioning Workshop. Property owners and tenants were mailed a post card invitation. Fliers were delivered along the corridor and schools were notified. The meeting was advertised on the FDOT meeting website and a Florida Administrative Register ad was placed.

Attendee: People are getting hurt at the crossings. The crossing striping needs to be enhanced.

Brian Shroyer: We need to make sure we have signal spacing at Rome Circle. We also have to look at bicycle/pedestrian level of service compared to motorists.

Attendee: What are you accomplishing with the resurfacing project?

Brian Shroyer: FDOT has specific requirements for pavement rehabilitation and intersection ADA requirements. We are discussing options with FDOT to close sidewalk gaps. There is no right of way phase in RRR project and construction is anticipated in 2019.

Attendee: Any changes made to the Alternatives?



Cris Schooley: The County and City are supportive for 6-lanes in the County but not in the City. Talking to public to see if they support 6-lanes.

Attendee: Safety before speed is an important Vision Zero concept. We would like you to look at the speed.

Brian Shroyer: Traffic Ops has done a Speed Study. We are trying to lower the speed and I want to present it to the Secretary.

Attendee: In spite of data, there needs to a reduction of speed.

Brian Shroyer: We are working with Traffic Operations to get their input on this topic.

Attendee: Request these alternatives are presented to MPO committees this month.

Brian Shroyer: I am working with Gena Torres at Plan Hillsborough to schedule meetings.

Attendee: Wants electronic flier for Alternatives Public Meeting instead of a post card.

Carol Barker: We sent fliers for the Visioning Workshop via e-mail and you will receive one this before the Alternative Public Meeting. We need to have partner agencies share the flier and spread the word.

Attendee: We should take into consideration that high schools are different from elementary and middle schools. Should be viewed different during the study process. Should be a school zone where flashing lights come on and more crossings at Chamberlain High School.

Brian Shroyer: There needs to be ½ mile spacing for signals; we are looking at a crossing between signals and continue to work with Traffic Operations on this topic.

Carol Barker: We are also incorporating the roadway geometry in the evaluation of additional pedestrian crosswalks.

Attendee: Are "Signals Set for 35 mph" used in other areas? I think this would help reduce speed.

Cris Schooley: Yes, currently this type of signage is used in other states.

Attendee: Is FDOT going to talk to property owners about sidewalk gaps?

Brian Shroyer: Yes, FDOT is approaching the property owners to discuss their willingness to provide easements necessary to close sidewalk gaps.

END OF MEETING SUMMARY



West Busch Boulevard (SR 580) Corridor Study
From North Dale Mabry Highway to North Nebraska Avenue
Hillsborough County, Florida
Financial Project ID Number: 435908-1-22-01



This meeting summary was prepared by Karen Harrell, Public Involvement Coordinator. If you feel clarifications are necessary, or if this differs from your understanding, please notify Karen by telephone at 352-257-1651 or by e-mail at Karen.Harrell@qcausa.com within (5) working days upon receipt of this summary.

Appendix A



PROJECT ADVISORY GROUP MEETING #4
West Busch Boulevard (SR 580) Corridor Study
 From North Dale Mabry Highway to North Nebraska Avenue
 Financial Project ID Number: 435908-1-22-01



Thursday, May 31, 2018 from 10:00 am to 12:00 pm ■ North Tampa Branch Library, 8916 North Boulevard, Tampa, FL 33604

Project Advisory Group (PAG) Sign-In Sheet

NAME (PLEASE PRINT)	AGENCY / ORGANIZATION	TITLE	PHONE	EMAIL
Petro Parra	The Plan Hillsborough	Principal Planner SPI	813-625-8020	f
Gina Torres	Hillsborough MPO	Exec. Planner		
Calvin Thorston	CDT	Engineer	813-274-7884	Calvin.Thorston@Fla.gov, No
Alicia Smith	HCIPS	SAFETY SUP.	813-434-6314	Alicia.Smith@SDHC.FL.GOV
Bethan & Shogoro	FOOT	PROJECT MANAGER	813-975-6449	Bethan.Shogoro@DOT.STATE.FL.US
Loren Howell	Quest	Public Involvement	352-257-1651	Loren.Howell@GCAUSA.COM
Lori Morgan	Quest	Public Involvement	813-400-4563	Lori.Morgan@GUSA.COM
Jenn Rhodes	CH2M	Planner	407-497-9319	jenn.rhodes@jacobs.com
Cris Schooley	,	,	407-334-2912	CRIS.Schooley@jacobs.com
Carol Barker	CH2M	Project Manager	407-650-2173	Carol.Barker@ch2m.com
Chad Polk	CH2M	Engineer	813.281.7912	chad.polk@ch2m.com

Appendix B

Date: May 14, 2018

Subject: **Project Advisory Group (PAG) Meeting #4**
West Busch Boulevard (SR 580) Corridor Study
from North Dale Mabry Highway to North Nebraska Avenue
Hillsborough County, Florida
FDOT Financial Project ID Number: 435908-1-22-01

Dear Project Stakeholder:

The Florida Department of Transportation (FDOT), District Seven is conducting a Corridor Study on the above referenced study. This letter is an invitation for you to participate in the fourth Project Advisory Group (PAG) meeting for this project. The meeting is an opportunity for the project team to present the initial alternatives under consideration. PAG meeting #4 is scheduled for Thursday, May 31, 2018, from 10:00 AM to Noon at the **North Tampa Branch Library, 8916 North Boulevard, Tampa, Florida 33604.**

The West Busch Boulevard Corridor study limits extend from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The study area includes portions of unincorporated Hillsborough County and the City of Tampa. The purpose of the Corridor Study is to work with the community, stakeholders and a Project Advisory Group (PAG) to develop a vision and identify needs and issues in the corridor.

The PAG is an important component of the study. It serves as a special advisory resource to the FDOT and the consultant team. The PAG participants will help develop a vision for the corridor that will guide proposed short and long-term recommendations. This may include issues related to capacity, traffic operations, safety, access and egress, mobility, freight movements, transit, bicycles, and pedestrian movements.

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220 at least seven (7) working days in advance of the meeting.

If you have any questions about membership in the PAG or require further information concerning this project, please contact Brian Shroyer, Florida Department of Transportation Project Manager at 813-975-6449 or by email at Brian.Shroyer@dot.state.fl.us.

Sincerely,
Brian Shroyer
Multimodal Project Manager
Florida Department of Transportation

FDOT Public Notices Website:

WEST BUSCH BOULEVARD (SR 580) CORRIDOR STUDY PROJECT ADVISORY GROUP (PAG) MEETING #4

District: Seven

Meeting Type: Meeting

Date: Thursday, May 31, 2018

Time: 10 AM to Noon

Location Name: North Tampa Branch Library

Street Address: 8916 North Boulevard

City: Tampa, FL 33604

Purpose: The Florida Department of Transportation, District Seven, is holding the fourth Project Advisory Group (PAG) meeting for the West Busch Boulevard (SR 580) Corridor Study. The meeting is an opportunity for the project team to present the initial alternatives under consideration. The limits of the project extend from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The corridor study area includes portions of unincorporated Hillsborough County and the City of Tampa.

The purpose of the Corridor Study is to work with the community, stakeholders and a Project Advisory Group (PAG) to develop a vision and identify needs and issues in the corridor. The PAG is an important component of the study. It serves as a special advisory resource to the FDOT and the consultant team. The PAG participants will help develop a vision for the corridor that will guide proposed short and long-term recommendations. This may include issues related to capacity, traffic operations, safety, access and egress, mobility, freight movements, transit, bicycles, and pedestrian movements. A copy of the agenda may be obtained by contacting: Mr. Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us.

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220 at least seven (7) working days in advance of the meeting.

Comuniquese Con Nosotros

Nos importa mucho la opinión del público sobre el proyecto. Si usted tiene preguntas o comentarios, o si simplemente desea más información, por favor comuniquese con nuestra representante, Lilliam Escalera, al (813) 975-6403, Departamento de Transportación de Florida, 11201 N. McKinley Dr., Tampa, FL 33612, Lilliam.Escalera@dot.state.fl.us

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT.

Primary Contact: Brian Shroyer, FDOT Project Manager

Primary Phone: (813) 975-6449

Primary E-mail: Brian.Shroyer@dot.state.fl.us

Additional Contact: Christopher Speese, Public Involvement Coordinator

Additional Phone: (813) 975-6405

Additional E-mail: Christopher.Speese@dot.state.fl.us

Florida Administrative Register Ad:

The Florida Department of Transportation, District Seven announces a public meeting to which all persons are invited.

DATE AND TIME: May 31, 2018, 10:00 AM to Noon

PLACE: North Tampa Branch Library, 8916 North Boulevard, Tampa, FL 33604

GENERAL SUBJECT MATTER TO BE CONSIDERED: The Florida Department of Transportation (FDOT), District Seven, is holding the fourth Project Advisory Group (PAG) meeting for the West Busch Boulevard (SR 580) Corridor Study. The meeting is an opportunity for the project team to present the initial alternatives under consideration. The limits of the project extend from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The corridor study area includes portions of unincorporated Hillsborough County and the City of Tampa.

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Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability, or family status. Pursuant to the provisions of the Americans with Disabilities Act, any person requiring special accommodations to participate in this workshop/meeting, or persons who require translation services (free of charge) are asked to advise the agency at least 7 days before the workshop/meeting by contacting: Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220 or via email at Christopher.Speese@dot.state.fl.us

Comuniquese Con Nosotros

Nos importa mucho la opinión del público sobre el proyecto. Si usted tiene preguntas o comentarios, o si simplemente desea más información, por favor comuniquese con nuestra representante, Lilliam Escalera, al (813) 975-6403, Departamento de Transportación de Florida, 11201 N. McKinley Dr., Tampa, FL 33612, Lilliam.Escalera@dot.state.fl.us

If you are hearing or speech impaired, please contact the agency using the Florida Relay Service, 1(800)955-8771 (TDD) or 1(800)955-8770 (Voice).

For more information, you may contact: Mr. Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us

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Appendix C

Welcome: Brief Presentation

Station 1: Existing Conditions

- Study Area
- Existing Typical Sections
- Traffic Map
- Multimodal Map
- Crash Map

Station 2: Future Vision

- Developing the Corridor Vision (Prior Feedback)
- Potential Improvements (Prior Feedback)
- Corridor Vision – How do we get there?

Station 3: Near-Term Improvements

- Proposed Spot Improvements (3 Plots)
- High-level – what was considered

Station 4: Long-Term Improvements (Interactive Exercise)


- Typical section alternatives: No-Build (existing), 4-lane, 5-lane, and 6-lane.
- Plan view for all typical section alternatives
- Interactive exercise “Happy or Not” Stickers)
- Draft Evaluation Matrix: will be filled out with PAG and stakeholder feedback and documented in CASR.

Station 5: Alternatives by Segment – *Mix & Match Puzzle* (Interactive Exercise)

Station 6: Supporting Recommendations


- Additional recommendations for future study to achieve vision
- Speed limit reduction: related to design, and related to safety

Project Advisory Group Meeting #4
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from North Dale Mabry Highway to North Nebraska Avenue
Hillsborough County, Florida
FDOT Financial Project ID Number: 435908-1-22-01
North Tampa Branch Library
May 31, 2018 – 10:00 am to 12:00 pm



Agenda for Project Advisory Group Meeting #4

- Project update
- Preview Alternatives Public Meeting
- Next Steps
- Complete the interactive exercises



Project Overview



Project Study Area

- 3.3 miles
- Within the city of Tampa and unincorporated Hillsborough County
- Concurrent RRR project



Purpose of the SR 580 Corridor Study

- Define existing conditions
- Identify transportation-related needs
- Define acceptable levels of access and mobility
- Determine transportation system needs to support surrounding land uses
- Consolidate and control access points
- Identify safety concerns
- Identify operational deficiencies and promote operational efficiency



Previous PAG meetings

- PAG #1 (August 2017)
 - Preliminary existing conditions, existing context classification, and future expectations
 - Gathered feedback on User Preference Surveys
- PAG #2 (October 2017)
 - Discussed existing conditions and updated future conditions
 - Visioning Workshop Preparation
- PAG #3 (January 2018)
 - Shared Visioning Workshop results
 - Presented draft spot improvements
 - Finalized the vision statement



Since PAG #3...

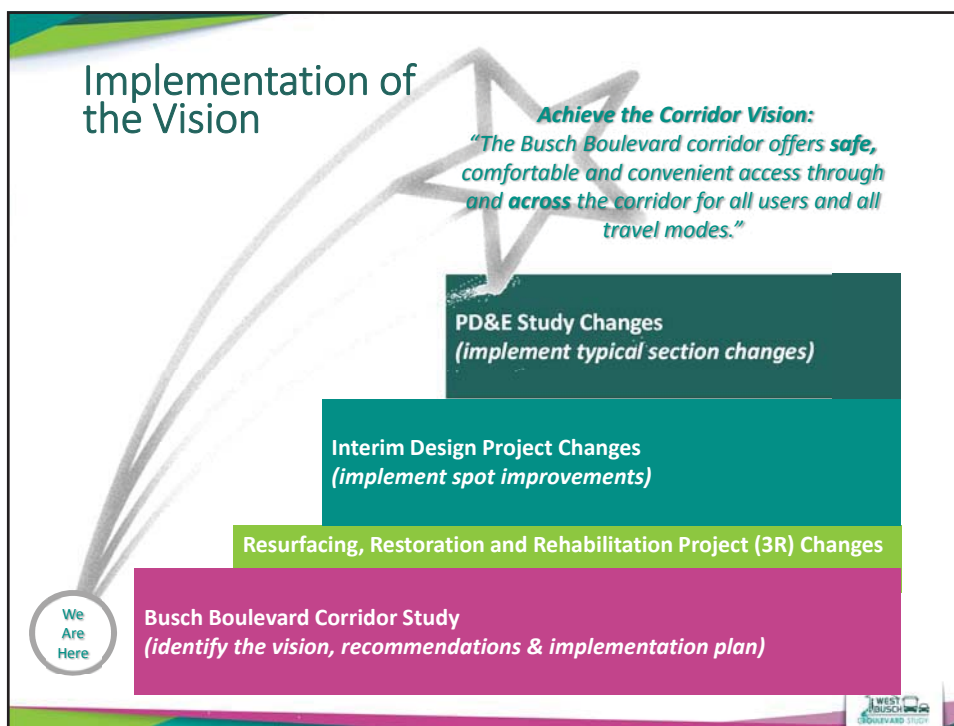
- Developed range of alternatives
- Met with local agencies to review draft alternatives and recommendations – April 12, 2018
- PAG #4 – May 31, 2018



Local Agency Coordination Meetings

- Held April 12, 2018
- MPO/Planning Commission, City of Tampa, Hillsborough County
 - ≈ Discussed draft typical section alternatives
 - ≈ Gauged agency interest in recommendations for further study which would require inter-agency coordination (JPA, maintenance agreements, etc.)





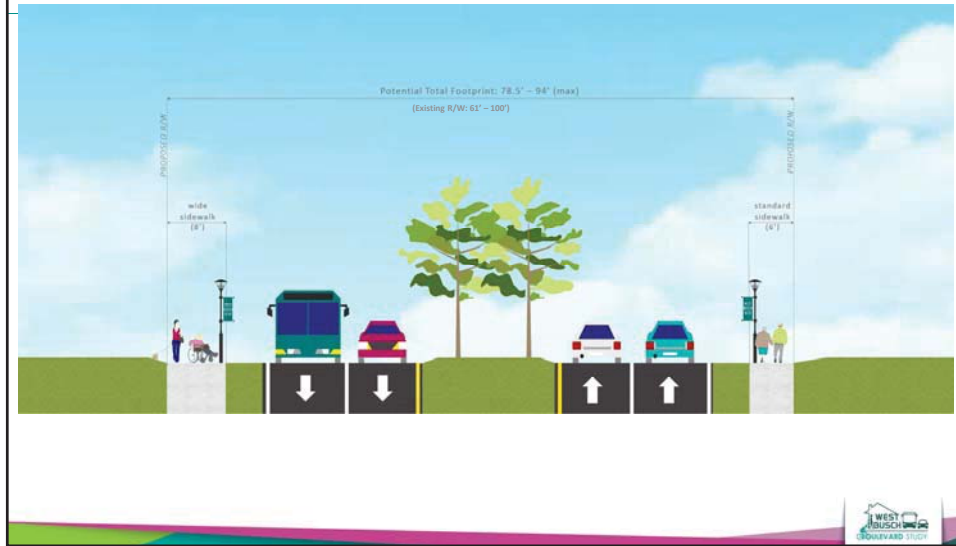
Developed the Range of Alternatives

Alternative	Description
No-Build	Existing condition
4-Lane	Construct 4-lane divided roadway without bike lanes, but with raised median, border width, and wide (8') sidewalk to accommodate additional pedestrian demand
5-Lane	Construct a 5-lane divided roadway with flush unrestricted median, bike/parking lanes, border width, 6' sidewalks
6-Lane	Construct a 6-lane divided roadway with raised median, bike lanes, border width, 6' sidewalks

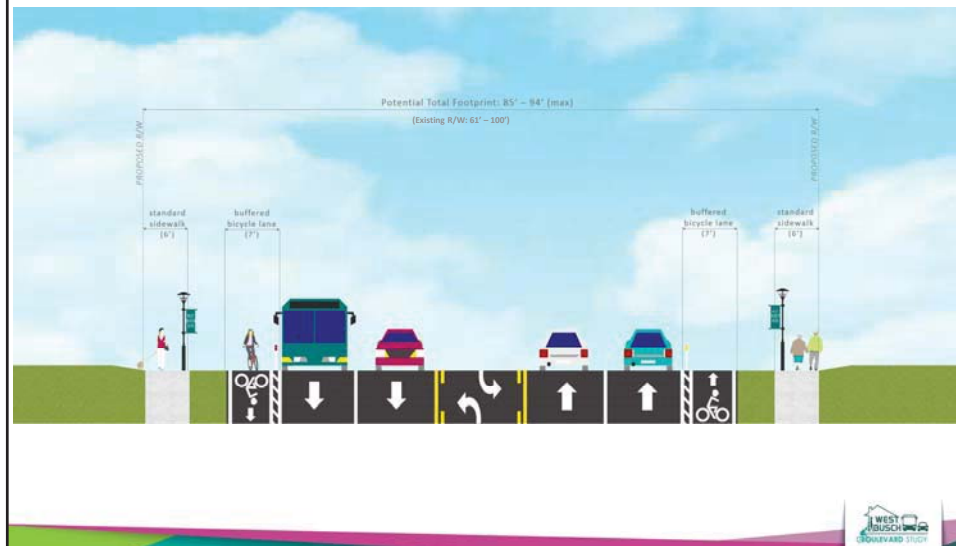
- Many variations are possible, but these alternatives provide a good range to receive feedback and further refine the concepts for the corridor vision
- To be presented at the Public Alternatives Meeting
- PD&E Alternative(s) could be all, some, or a variation of these alternatives

WEST BUSCH BOULEVARD STUDY

4-Lane Alternative: 4 Lanes with Median and Wide Sidewalk on the North Side



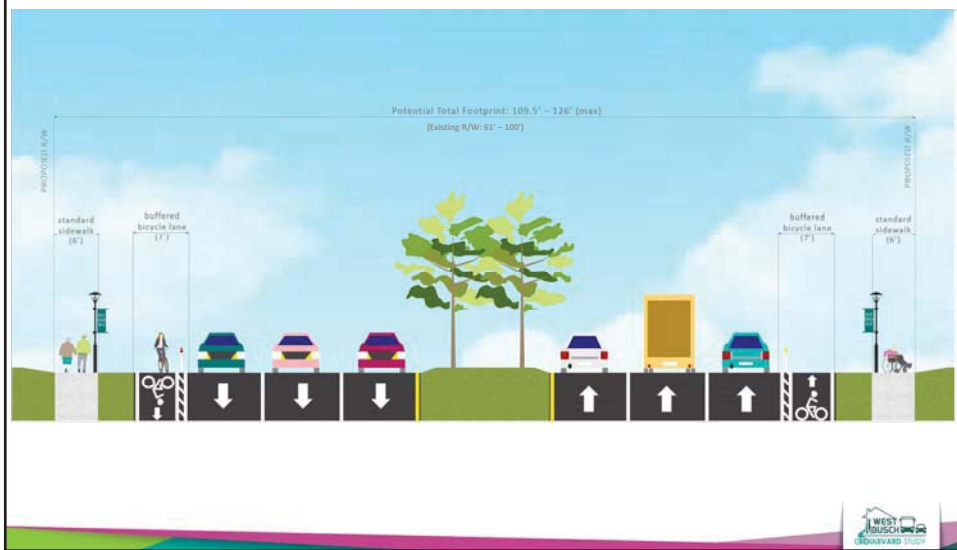
5-Lane Alternative: 4 Lanes with Center Two-Way Left Turn Lane and Buffered Bike Lanes

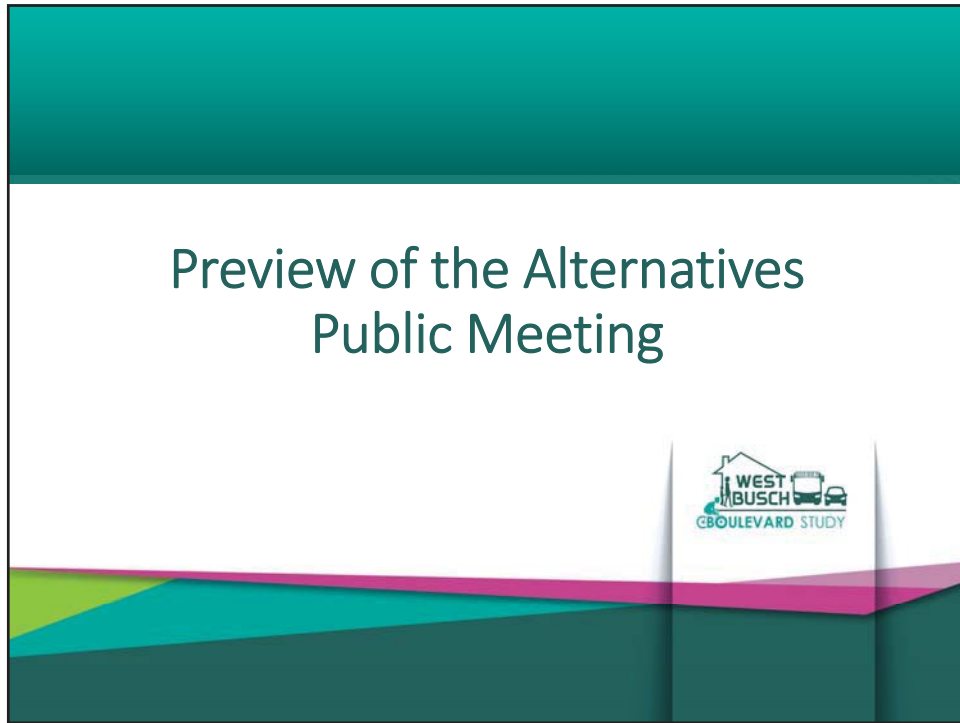


Potential Parallel Parking Option for Segment 2 (North Armenia Ave. to North Blvd.)




6-Lane Alternative: 6 Lanes with Median and Buffered Bike Lanes





Alternatives Public Workshop Preview

- Conduct workshop June/July 2018
- Informative and Interactive Stations:
 - Project overview video 
 - Station 1: **Existing Conditions** displays 
 - Study Area
 - Existing typical Sections
 - Traffic Map
 - Multimodal Map
 - Crash Map
 - Station 2: **Future Vision**
 - Developing the Corridor Vision (Prior Feedback)
 - Potential Improvements (Prior Feedback)
 - Corridor Vision and Implementation Strategy
 - Station 3: **Near-Term Improvements**
 - Proposed Spot Improvements



Alternatives Public Workshop Preview

- **Station 4: Long-Term Improvement Alternatives – Smiley Face Exercise** (Interactive Exercise)

- Typical Section Alternatives
- Concept sheets
- Future Traffic
- Evaluation Matrix
- Interactive Exercise



- **Station 5: Alternatives by Segment - Mix & Match Puzzle** (Interactive Exercise)

- Opportunity to apply different typical sections to corridor segments



- **Station 6: Supporting Recommendations**

- Recommendations for partner agencies
- Recommend further study of lowering design speed

- **Station 7: Comments** – standard comment form and comment box



Next Steps



Next Steps

- FDOT is reviewing the draft Corridor Alternatives and Strategies Report (CASR)
- Conduct the alternatives public meeting
- Additional agency meetings, as requested
- Refine and finalize the recommendations and evaluation matrix
- Agency and PAG review of the Corridor Alternatives and Strategies Report



Next Steps - Schedule

- Alternatives Public Meeting – June/July 2018
- Corridor Alternatives and Strategies Report, August 2018
- FDOT to coordinate Near-term improvement project implementation



Complete the Interactive Exercises



Appendix D



PROJECT ADVISORY GROUP (PAG) MEETING #4

West Busch Boulevard (SR 580) Corridor Study

From North Dale Mabry Highway to North Nebraska Avenue

Financial Project ID Number: 435908-1-22-01



Thursday May 31, 2018 ■ North Tampa Branch Library, 8916 North Boulevard, Tampa, Florida 33604

Please provide your comments below. If more space is needed, please use the back of this sheet or an additional sheet of paper. You may place your comments in the "Comment Box" provided at the meeting, or send to Brian Shroyer, Project Manager at the address on the back of this comment form, by June 10, 2018. Emailed comments are also acceptable to Brian.Shroyer@dot.state.fl.us

*All comments are part of the project record and are available for review by the public and the media.

Lower speeds - may require changing context classification in the one section that isn't Urban General.

Add as many refuge islands/median where the TWLT lanes are. Businesses may oppose but ~~there~~^{safety} has to come first.

Narrow lanes as much as possible. 10 1/2'?

Reference the school safety study recommendations

Ensure neighborhoods have inputs. ^{people at the} library ~~to~~ should be talked to - not necessarily walking door to door to businesses which don't live here or along road necessarily. Residents, locals are walking driving taking bus

CONTACT INFORMATION (PLEASE PRINT)

Name Gena Torres
Title Exec. Planner
Agency / Organization Hillsb MPO
Phone Number _____
Email _____

RETURN COMMENTS BY June 10, 2018 TO:

Brian L. Shroyer
Multimodal Project Manager
Florida Department of Transportation
11201 N. McKinley Drive, MS 7-500
Tampa, FL 33612
Brian.Shroyer@dot.state.fl.us

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PROJECT ADVISORY GROUP (PAG) MEETING #4

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PLEASE ADD SIGNAL AT BUSCH AND OLA

IMPROVE MPO PLAN

CONTACT INFORMATION (PLEASE PRINT)

Name ANDREW SMITH
Title FIELD SAFETY SUPERVISOR
Agency / Organization HCP5
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Email ANDREW.SMITH@SDNC.K12.FL.US

RETURN COMMENTS BY June 10, 2018 TO:

Brian L. Shroyer
Multimodal Project Manager
Florida Department of Transportation
11201 N. McKinley Drive, MS 7-500
Tampa, FL 33612
Brian.Shroyer@dot.state.fl.us

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PROJECT ADVISORY GROUP (PAG) MEETING #4

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Implement the MPO School Safety Study recommendations to the extent possible - school zone, N Blvd improvements, lighting, signal changes, etc

Implement the Community Aesthetics Feature permit crosswalks @ I-275

Send PAG an electronic flyer of Community Workshop to disseminate.

Change N. Blvd ~~to~~ to ^{Nebraska} ~~to~~ to Class of Urban General.

CONTACT INFORMATION (PLEASE PRINT)

Name Lisa Silva
Title
Agency / Organization Hills MPO
Phone Number
Email silval@plan.com.org

RETURN COMMENTS BY June 10, 2018 TO:

Brian L. Shroyer
Multimodal Project Manager
Florida Department of Transportation
11201 N. McKinley Drive, MS 7-500
Tampa, FL 33612
Brian.Shroyer@dot.state.fl.us

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Treat high school safety treatments like elementary and middle schools. Flashing lights,

Use this project as a model, treat this roadway ^{differently} especially with Chamberlain HS having a number of students crossing this high speed, high volume roadway. These students are treated as adults but they are not good at judging speed and have distractions that may not have been an issue years ago (phones, music games, etc)

Create a School Zone for High School with Lower Speed Limits, Enhanced signs, Enforcement, Crossing Guards

Put Full traffic signal at Ole & Busch for School Buses and School Traffic.

CONTACT INFORMATION (PLEASE PRINT)

Name Calvin Thayer
Title _____
Agency / Organization _____
Phone Number _____
Email _____

RETURN COMMENTS BY June 10, 2018 TO:

Brian L. Shroyer
Multimodal Project Manager
Florida Department of Transportation
11201 N. McKinley Drive, MS 7-500
Tampa, FL 33612
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↳ Love all of the near-term improvements esp patterned print and LPI
↳ can we also considered variable speed limits in addition / rather than the "signals set for..." signage?

↳ 5-lane typical section alternative is a joke => the suicide lane is a major pain point following Alexis' death. Rather than erring toward the side of danger, replacing the 2WLT w/ a landscaped median would err toward safety

↳ the school zone should be implemented => there is a study supporting this recommendation

↳ hard to believe the context classification for this corridor is suburban => it feels urban!

↳ we've been discussing design speed since the intro meeting. ~~that~~ this is a top-5 most deadly corridor within Hillsborough. Fatalities + serious injuries should warrant a critical review of the existing posted speed limit

CONTACT INFORMATION (PLEASE PRINT)

Name Johnny Wong
Title Sr. planner
Agency / Organization Hills MPO
Phone Number (813) 273-3774 x 370
Email wongj@plancm.org

RETURN COMMENTS BY June 10, 2018 TO:

Brian L. Shroyer
Multimodal Project Manager
Florida Department of Transportation
11201 N. McKinley Drive, MS 7-500
Tampa, FL 33612
Brian.Shroyer@dot.state.fl.us

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Implement MPO Plan

Signal @ Ola v Busch.

CONTACT INFORMATION (PLEASE PRINT)

Name LESTER OTTINGER
Title COMPLIANCE INSPECTOR
Agency / Organization HCPS
Phone Number 813-569-8368
Email lester.ottinger@sdho.k12.fl.us

RETURN COMMENTS BY June 10, 2018 TO:

Brian L. Shroyer
Multimodal Project Manager
Florida Department of Transportation
11201 N. McKinley Drive, MS 7-500
Tampa, FL 33612
Brian.Shroyer@dot.state.fl.us

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School Zoning light to reduce speed, this area should be reviewed and amended
The Urban Class 3 Suburban should change to Urban (between North FL & NW Blvd)
and Urban items to North Blvd.

CONTACT INFORMATION (PLEASE PRINT)

Name Pedro Pava
Title Principal Planner
Agency / Organization Pan Hillsborough
Phone Number 813-625-8020
Email ppava@hillsborough.org

RETURN COMMENTS BY June 10, 2018 TO:

Brian L. Shroyer
Multimodal Project Manager
Florida Department of Transportation
11201 N. McKinley Drive, MS 7-500
Tampa, FL 33612
Brian.Shroyer@dot.state.fl.us

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status.

Appendix E

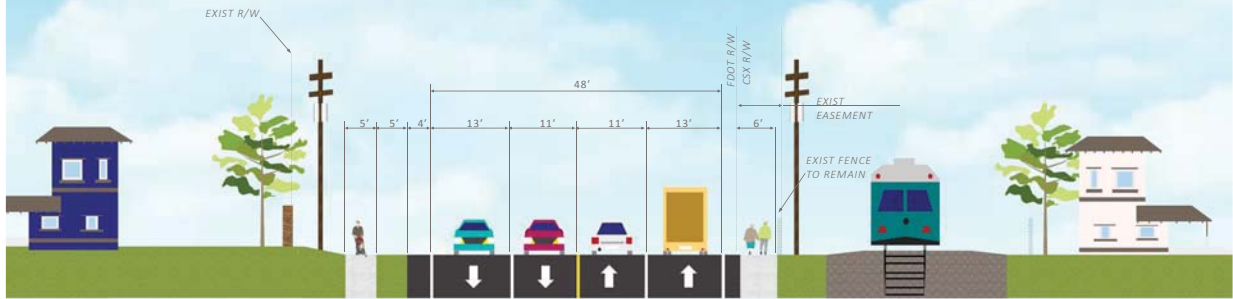
1

Existing Conditions

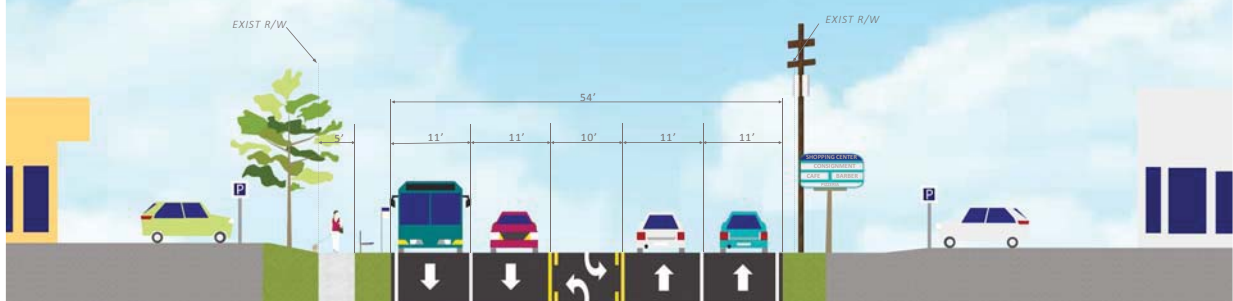
E-698

Existing Typical Sections

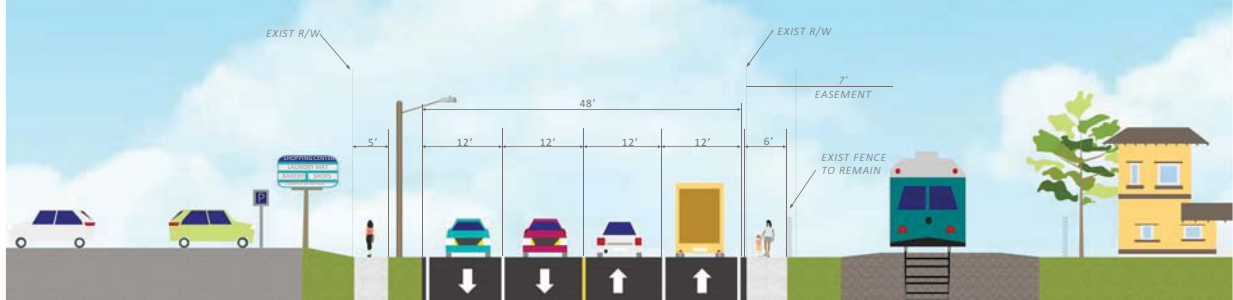
North Dale Mabry Highway to North Armenia Avenue
Suburban (Residential) Land Uses



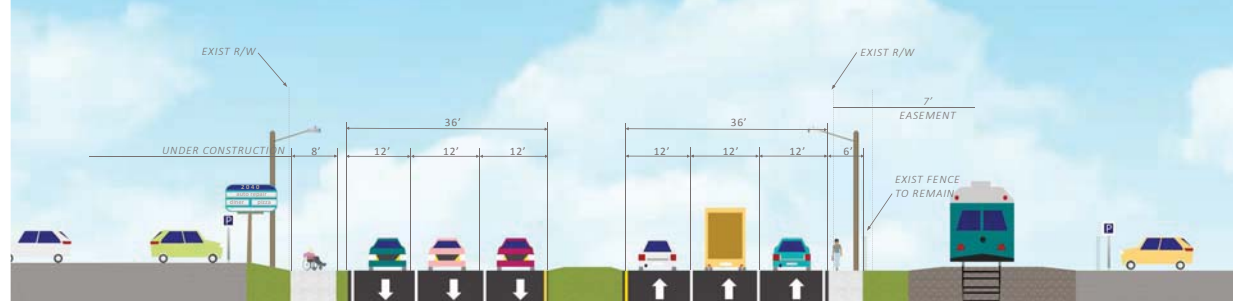
North Armenia Avenue to North Boulevard
Mixed-Use General Urban Land Uses



North Boulevard to North Florida Avenue
Suburban (Commercial) Land Uses

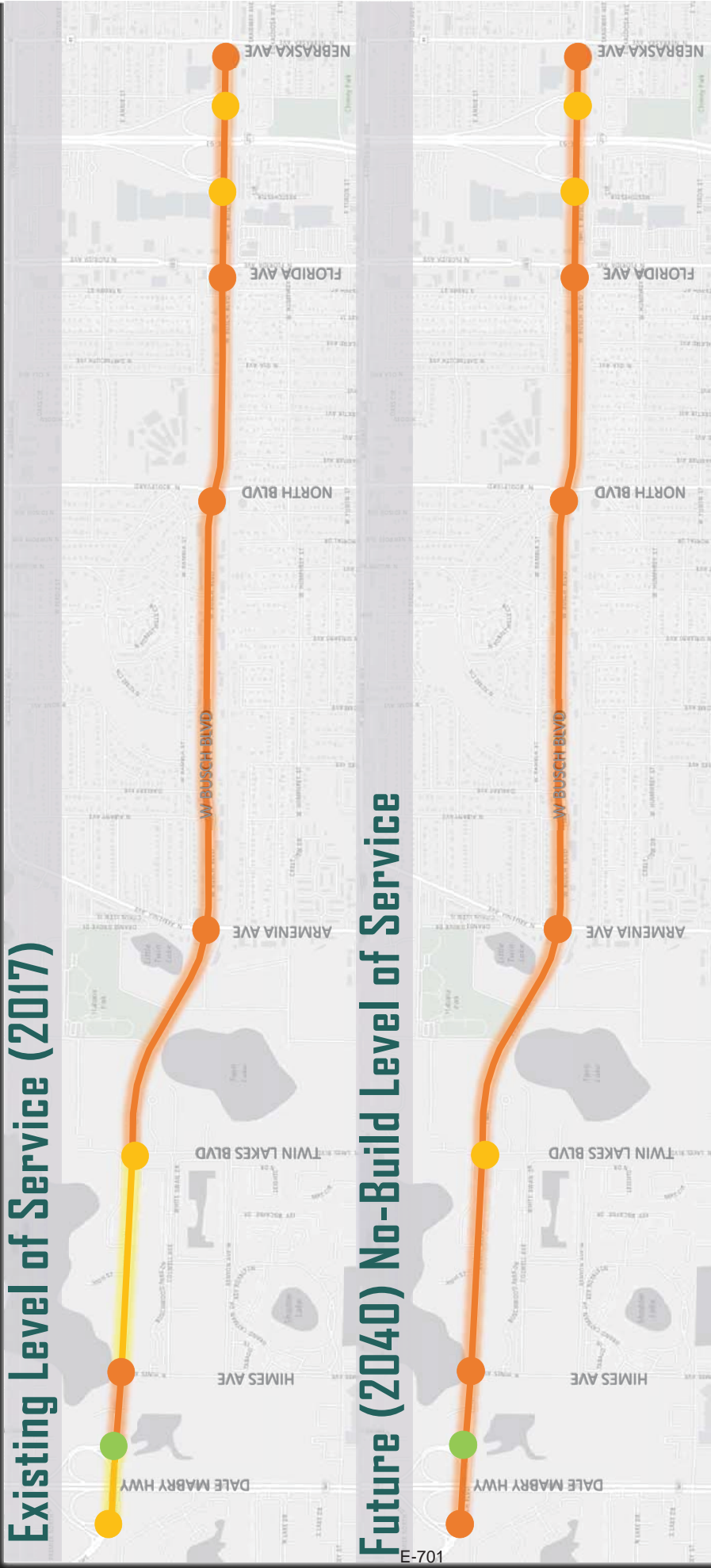


North Florida Avenue to North Nebraska Avenue
Suburban (Commercial) Land Uses

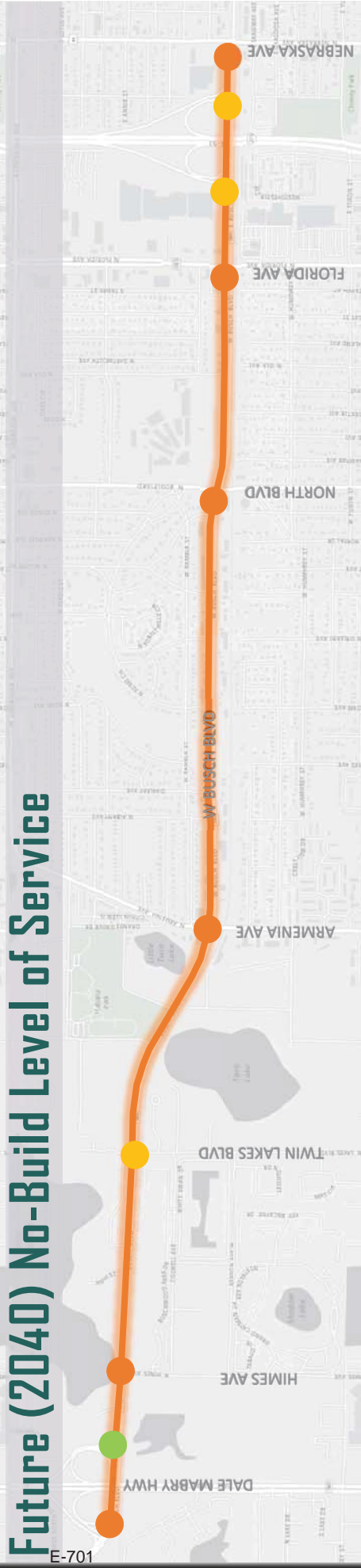


Traffic Level of Service Map

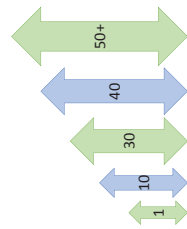
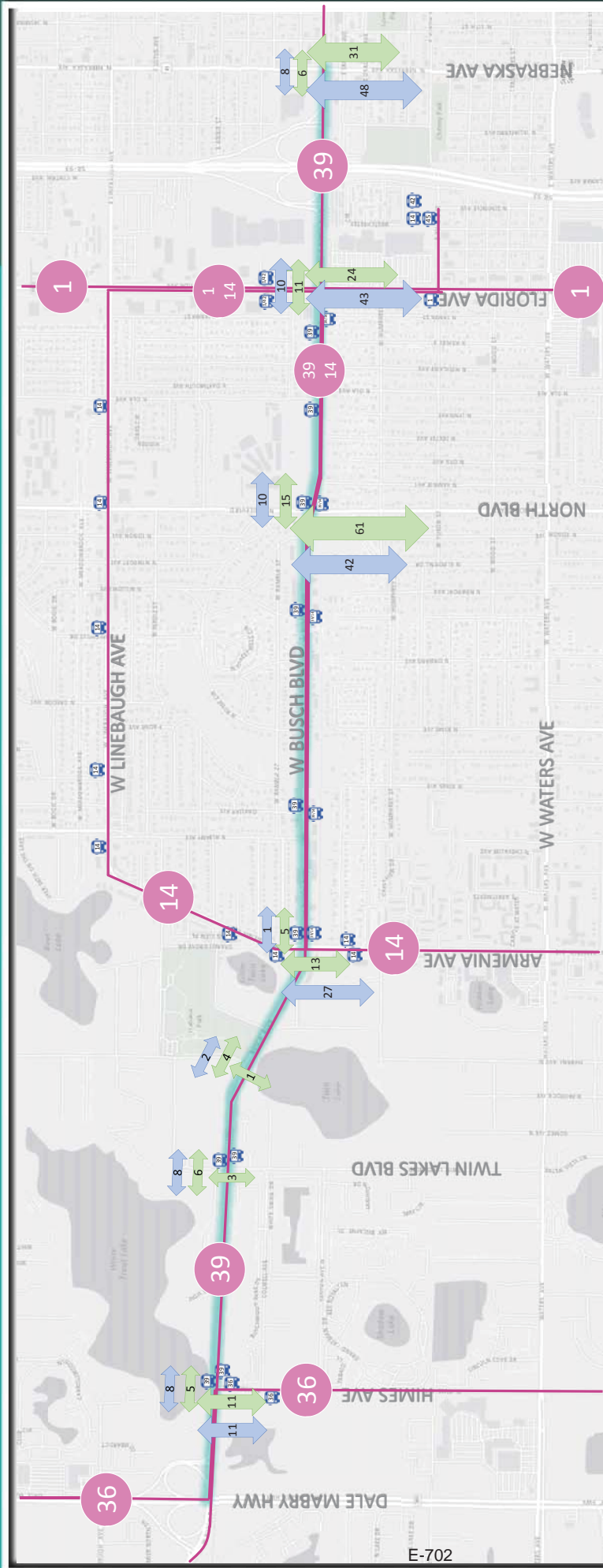
Existing Level of Service (2017)



Future (2040) No-Build Level of Service



Bicycle, Pedestrian, & Transit Map



By Cyclists
(blue)



By Pedestrians
(green)



Transit Stop



Transit Route

* Based on observations from 7 a.m. – 9 a.m. & 4 p.m. – 6 p.m. on Thursday May 4, 2017

2

Future Vision

E-703

Achieve the
Corridor Vision:
“ The Busch Boulevard
corridor offers **safe**,
comfortable, and convenient
access through and **across**
the corridor for all users and
all travel modes. ”



3

Near-Term Improvements

Near-Term Improvements

- Median islands & landscaping



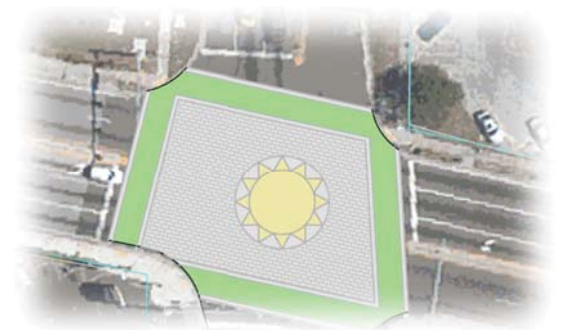
- Enhanced lighting



- Fill in sidewalk gaps, reduce crosswalk lengths, and add crosswalk at Rome Cir.



- Add patterned pavement and leading pedestrian interval crossing at North Boulevard intersection



- Add “YOUR SPEED” radar signs

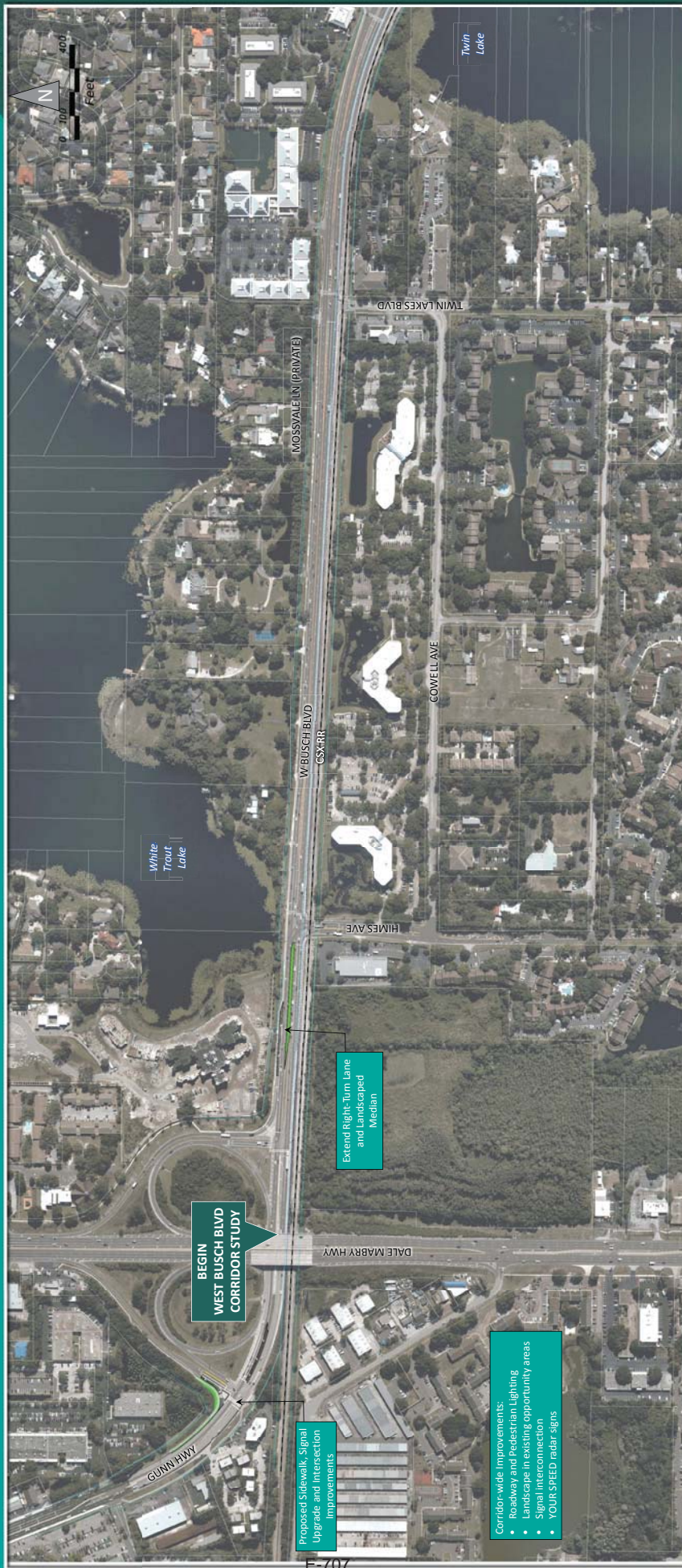


- Interconnect and time signals to increase platooning (35 mph)



Spot Improvements (Sheet 1 of 3)

DRAFT



BEGIN
WEST BUSCH BLVD
CORRIDOR STUDY

Proposed Sidewalk, Signal
Upgrade and Intersection
Improvements

Extend Right-Turn Lane
and Landscaped
Median

- Corridor-wide Improvements:
- Roadway and Pedestrian Lighting
 - Landscape in existing Opportunity areas
 - Signal interconnection
 - YOUR SPEED radar signs



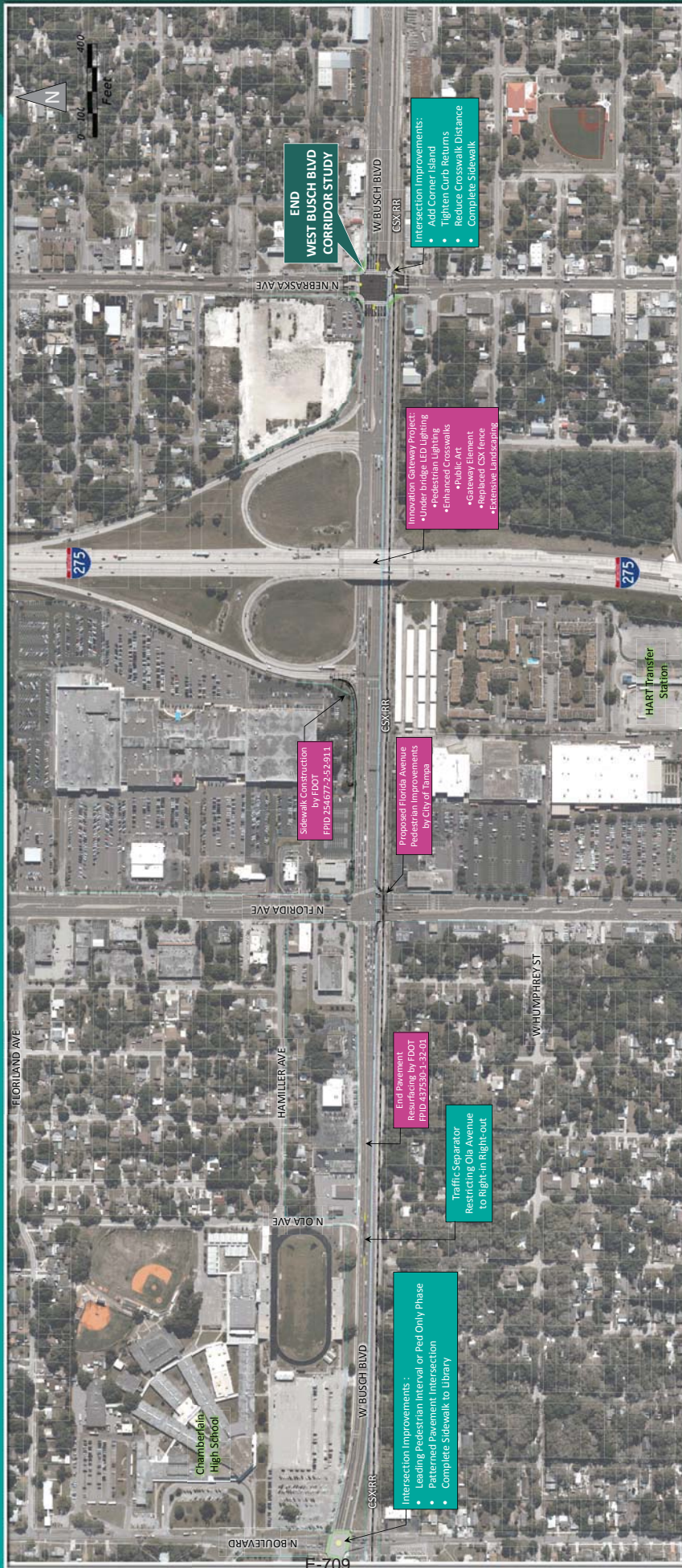
Spot Improvements (Sheet 2 of 3)

DRAFT



Spot Improvements (Sheet 3 of 3)

DRAFT

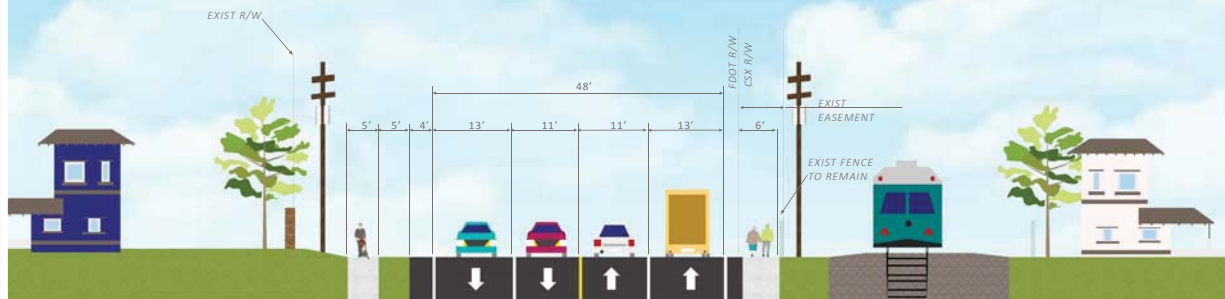


4

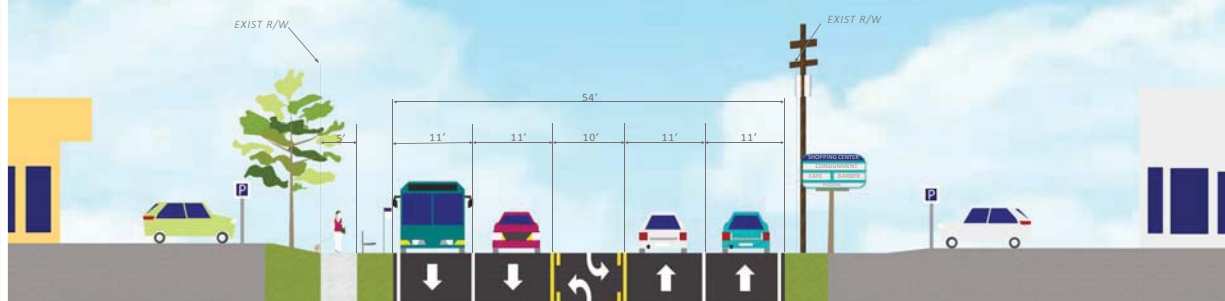
Long-term Improvement Alternatives

No-Build (Existing Typical Sections)

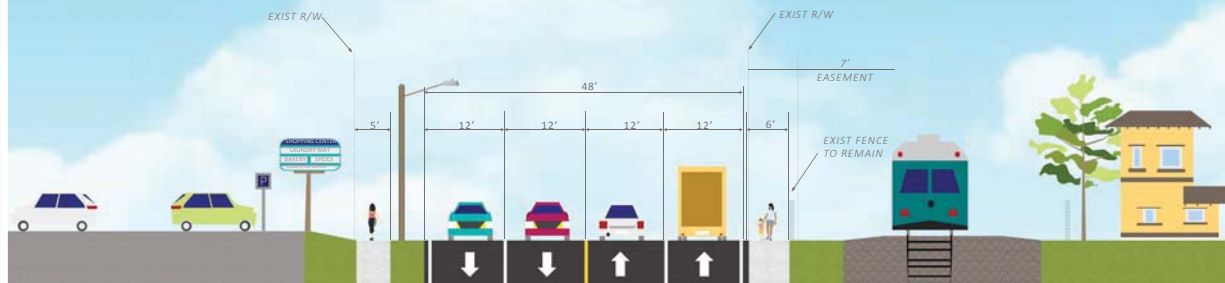
North Dale Mabry Highway to North Armenia Avenue
Suburban (Residential) Land Uses



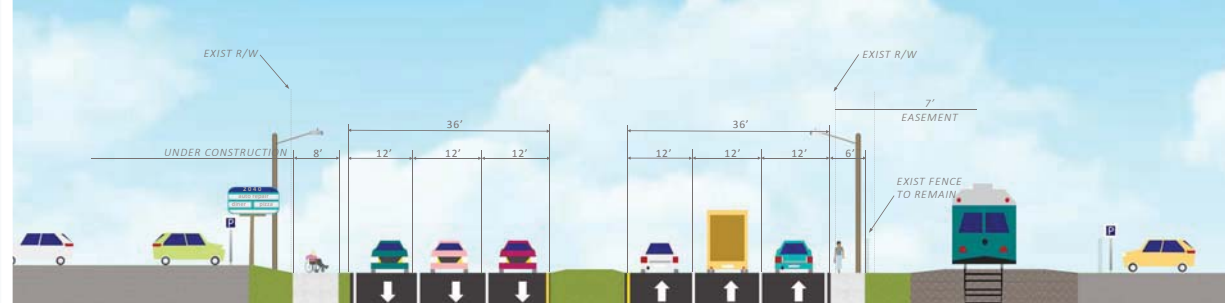
North Armenia Avenue to North Boulevard
Mixed-Use General Urban Land Uses



North Boulevard to North Florida Avenue
Suburban (Commercial) Land Uses



North Florida Avenue to North Nebraska Avenue
Suburban (Commercial) Land Uses



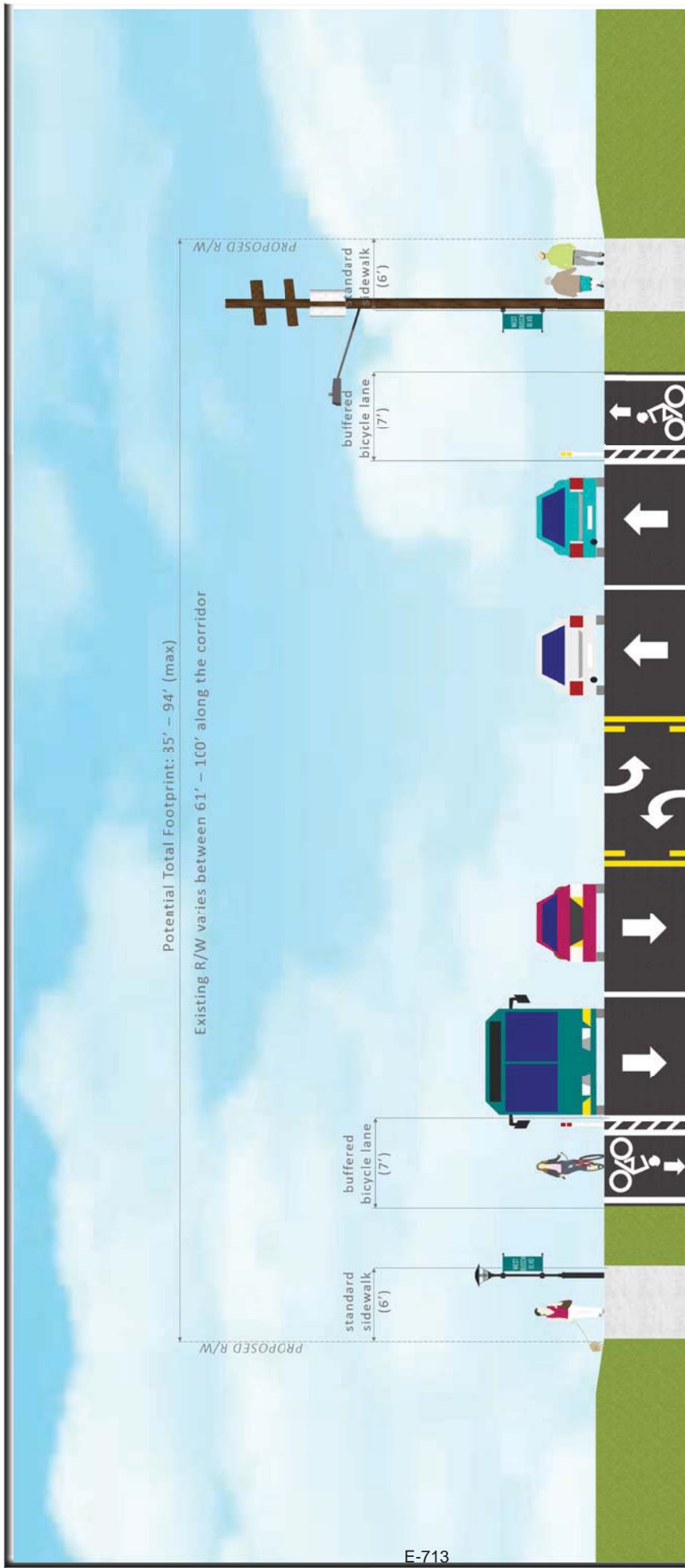
E-711

Typical Section Alternatives



4 Lanes with Median and a Wide Sidewalk

Typical Section Alternatives

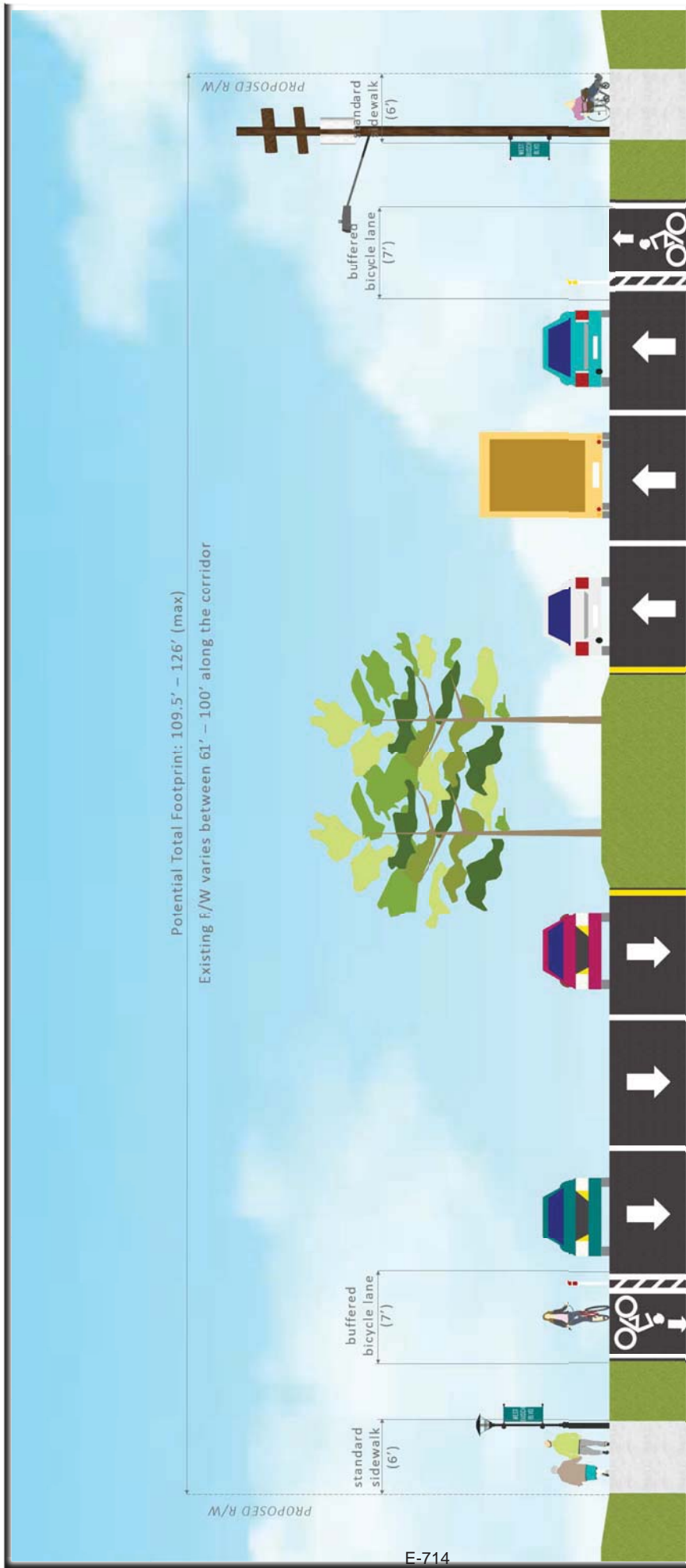


E-713

4 Lanes with Center Two-Way Left Turn Lane and Buffered Bike Lanes (5 Lanes)

DRAFT/5/2/2018

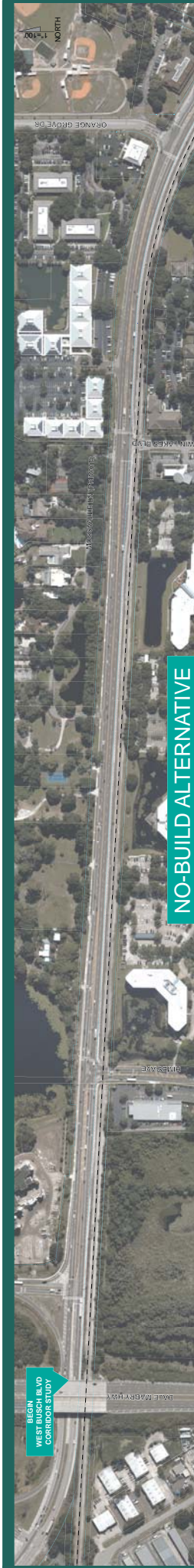
Typical Section Alternatives



E-714

6 Lanes with Median and Buffered Bike Lanes

DRAFT 5/2/2018

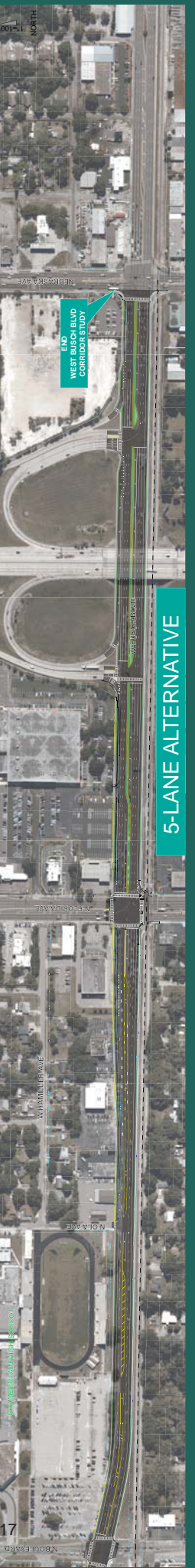




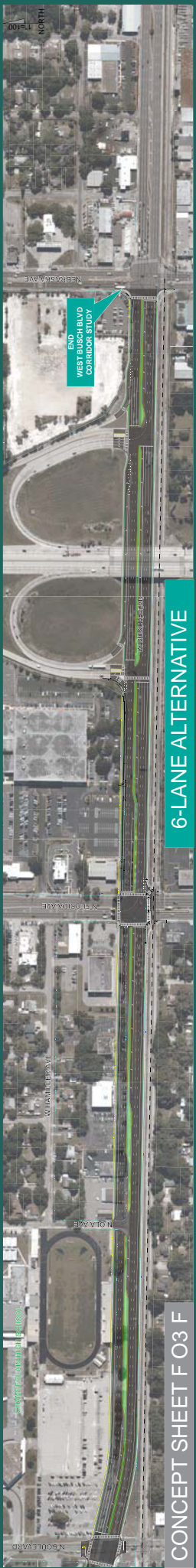
NO-BUILD ALTERNATIVE



4-LANE ALTERNATIVE



5-LANE ALTERNATIVE



6-LANE ALTERNATIVE

E-717

Traffic Level of Service Map



Future (2040) 4-Lane/5-Lane Level of Service




























Future (2040) 6-Lane Level of Service

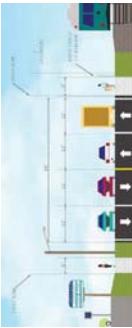





Did you know? Additional lanes won't significantly improve Level of Service but would serve over 10,000 more trips per day

Evaluation Matrix

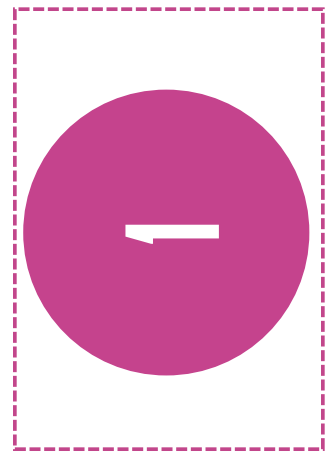
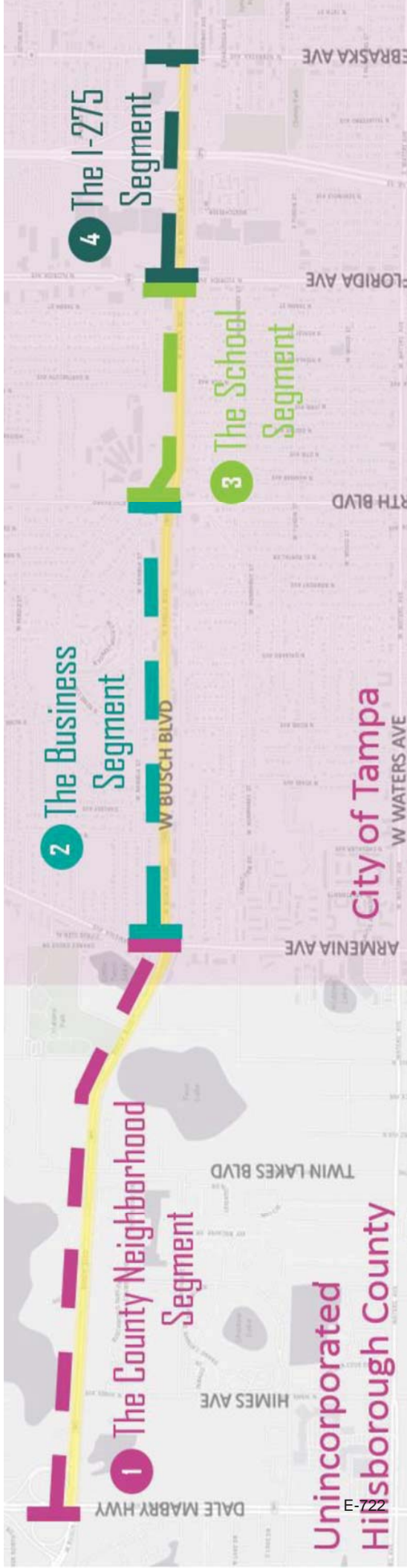
		Corridor Alternatives			
		No-Build	4-Lane	5-Lane	6-Lane
Satisfies Vision  How well does this alternative satisfy safety, comfort, and convenience of travel through and across for auto mode ?  How well does this alternative satisfy safety, comfort, and convenience of travel through and across for transit users ?  How well does this alternative satisfy safety, comfort, and convenience of travel through and across for bicyclists ?  How well does this alternative satisfy safety, comfort, and convenience of travel through and across for pedestrians ?					
					
					
					
Stakeholder Support  Relative Project Advisory Group Support  Relative Public Support	Pending PAG #4 Input	Pending PAG #4 Input	Pending PAG #4 Input	Pending PAG #4 Input	
	Pending Alternatives Public Meeting Input	Pending Alternatives Public Meeting Input	Pending Alternatives Public Meeting Input	Pending Alternatives Public Meeting Input	
Cost  Potential Residential & Business Impacts  Potential Right-of-Way Impacts  Construction Cost	-	73	80	97	
	-	2.5 acres	3 acres	12 acres	
	-	\$39 M	\$33.7 M	\$43.2 M	

Stakeholder Support

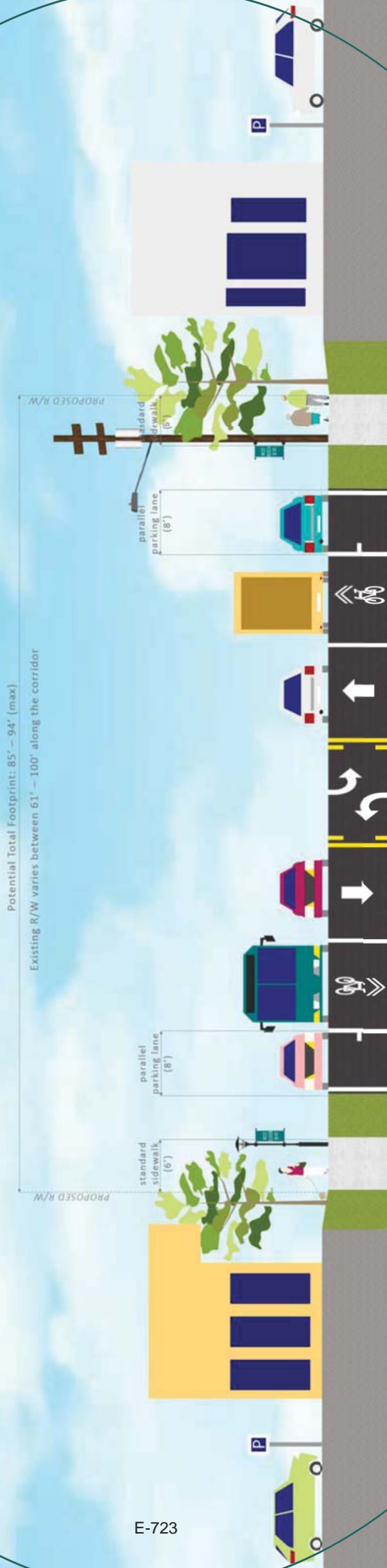
Corridor Alternatives	Relative Project Advisory Group Support		
<p>No-Build</p> 			
<p>4-Lane</p>  <p>E-720</p>			
<p>5-Lane</p> 			
<p>6-Lane</p> 			

5

Alternatives by Segment

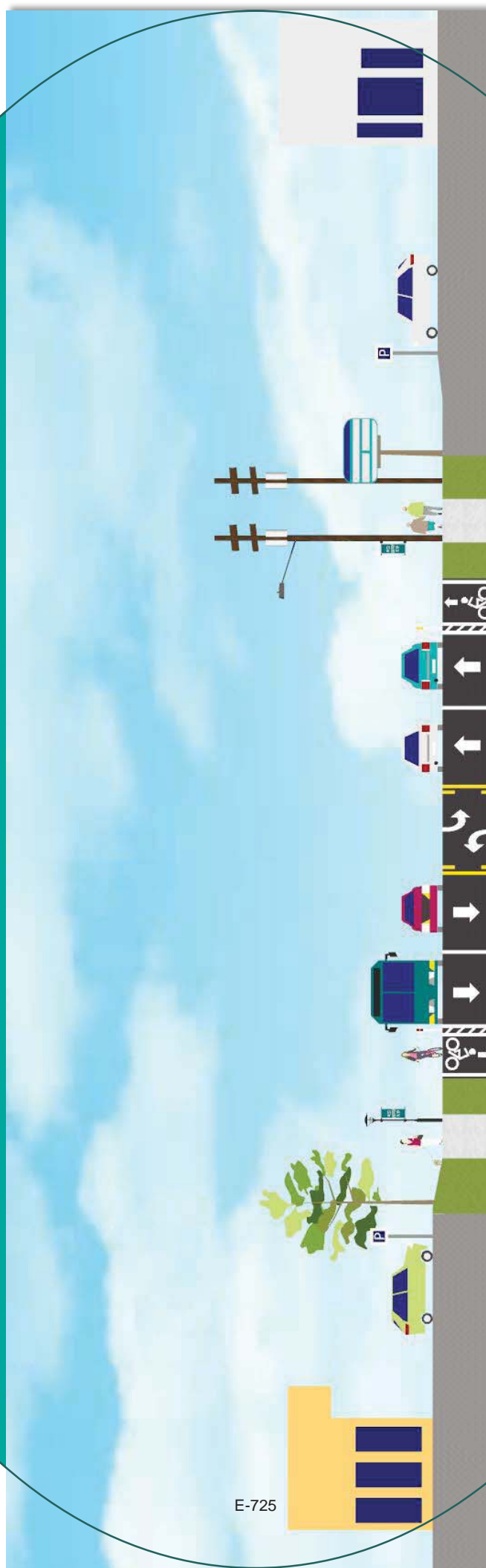


5 Lanes with Parallel Parking



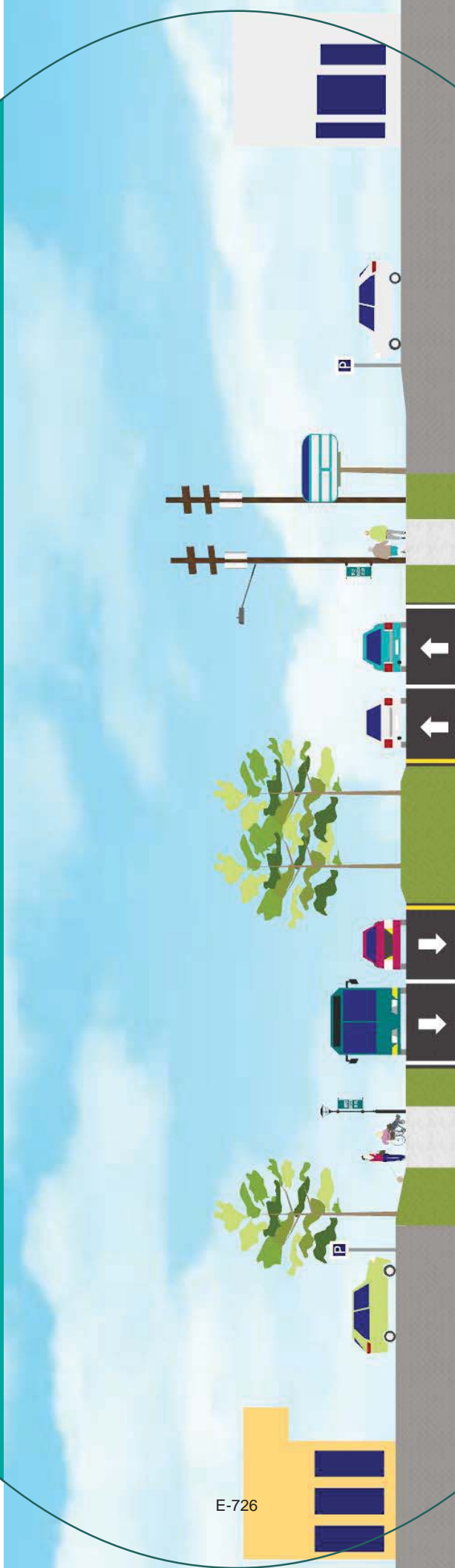
2 The Business Segment

5 Lanes with buffered bike lanes



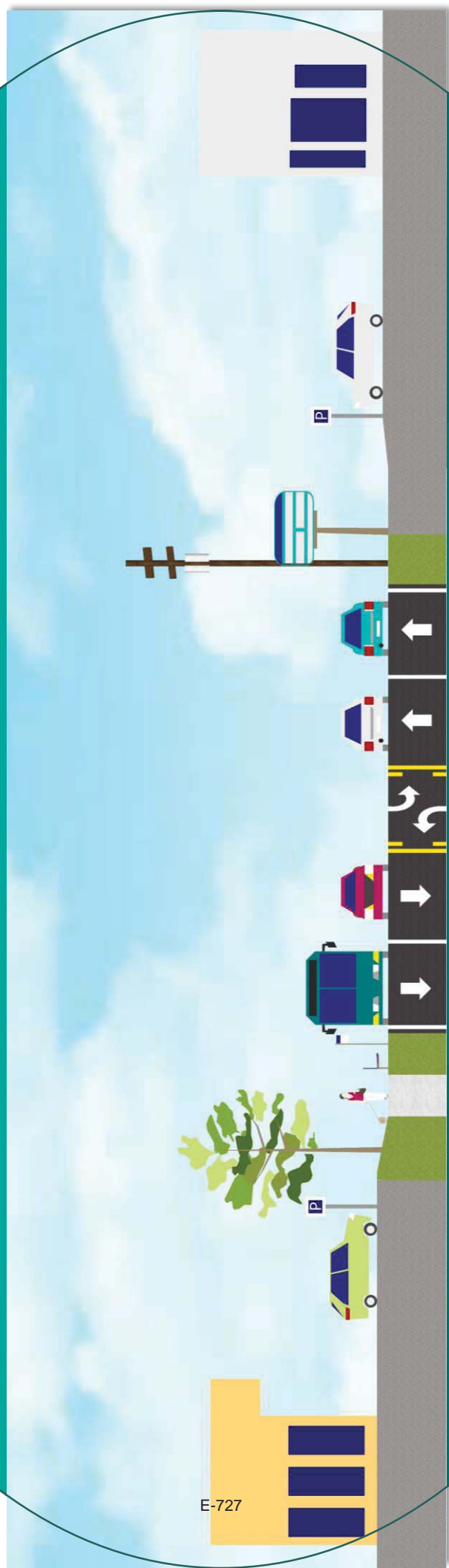
2 The Business Segment

4 Lanes with a wide sidewalk



2 The Business Segment

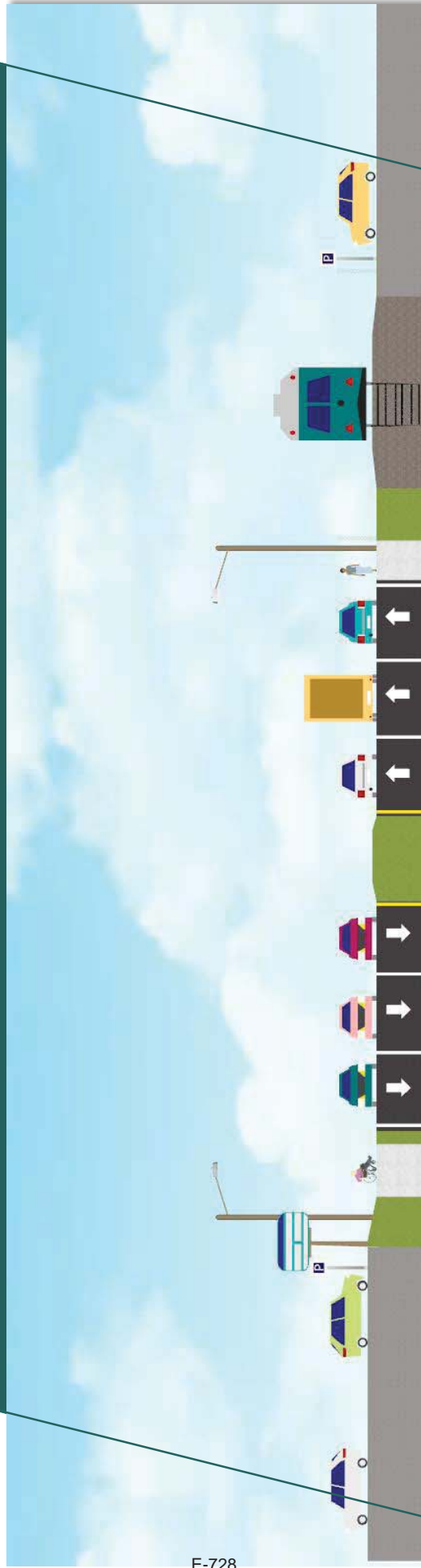
No Build (Existing Typical Section)



E-727

2 The Business Segment

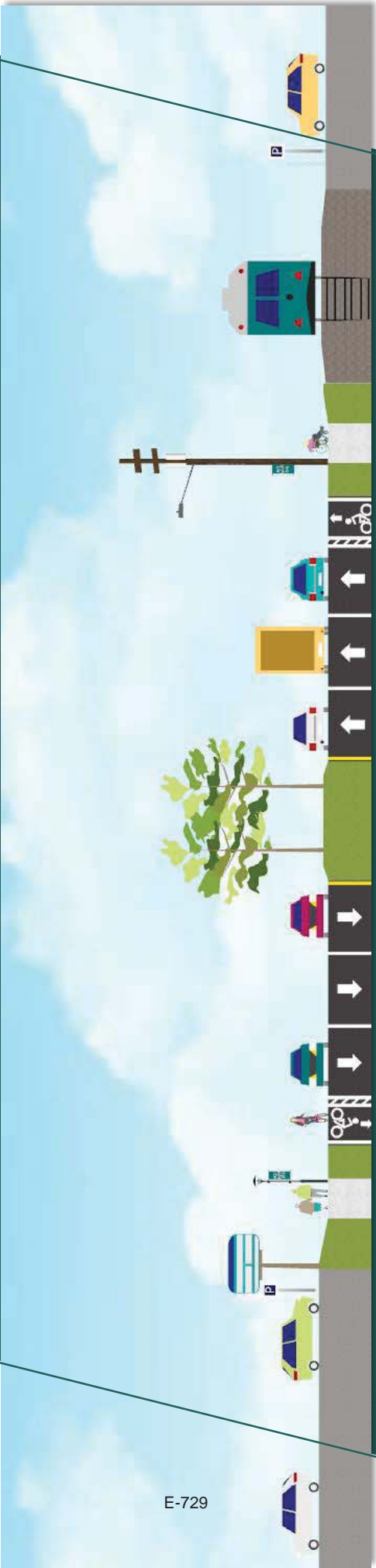
No Build (Existing Typical Section)



E-728

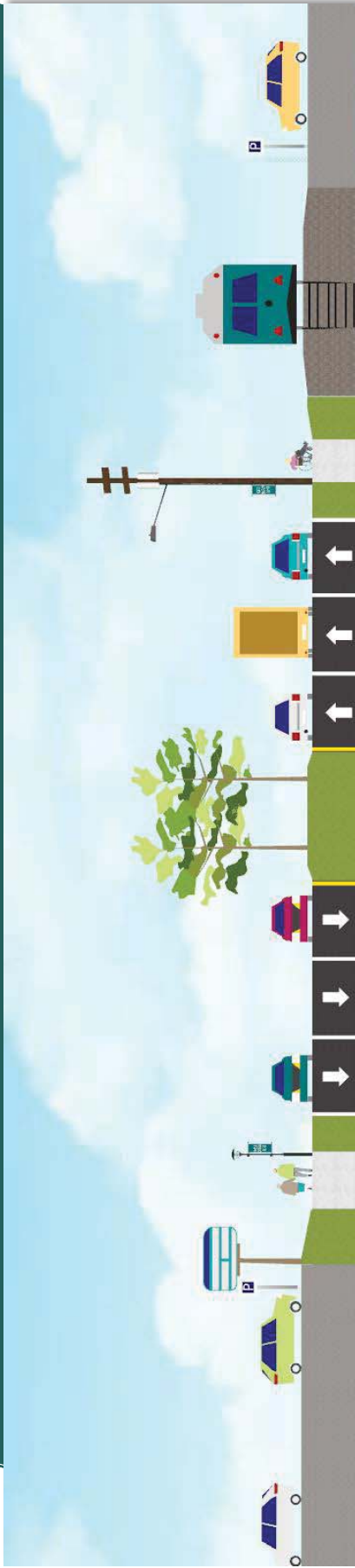
4 The I-275 Segment

6 Lanes with Buffered Bike Lanes



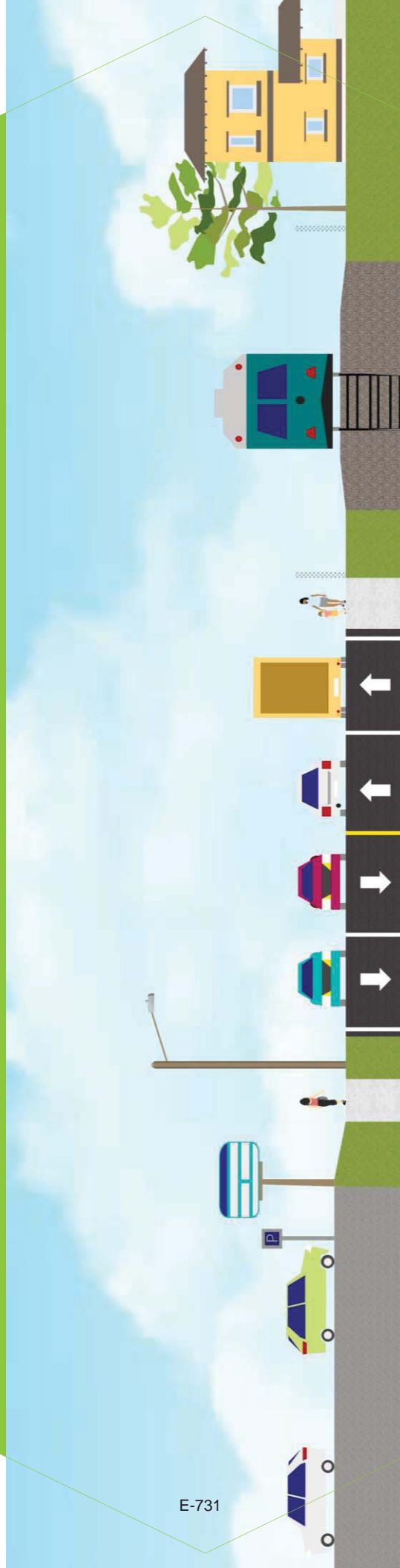
4 The I-275 Segment

6 Lanes with a wide sidewalk



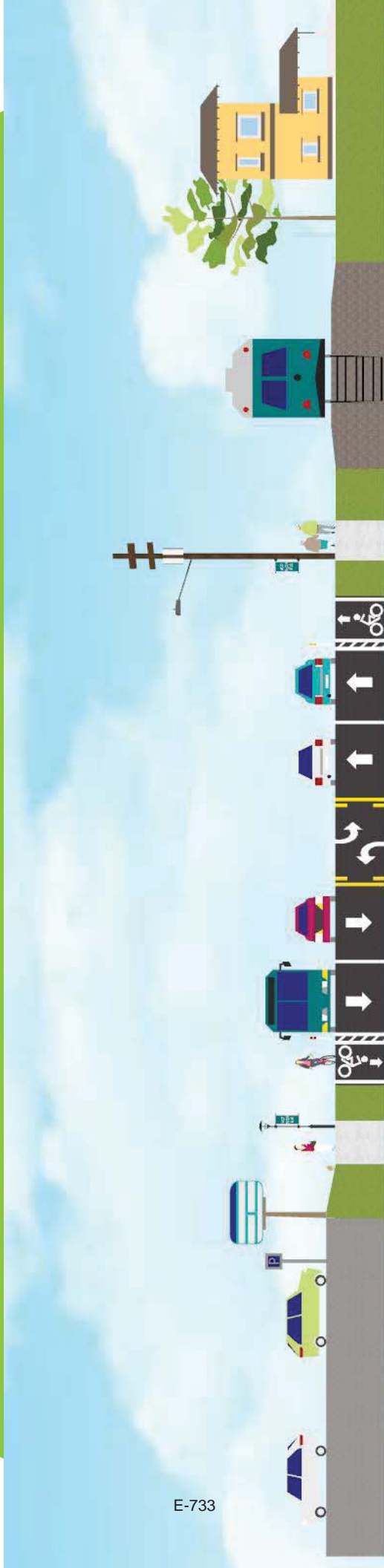
4 The I-275 Segment

No Build (Existing Typical Section)



3 The School Segment

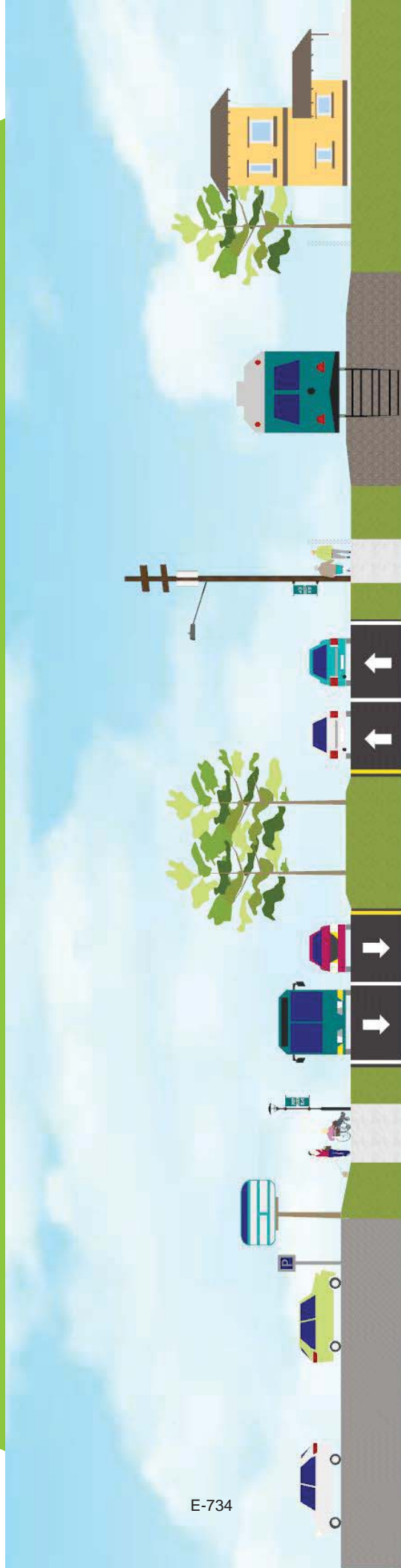
5 Lanes with Buffered Bike Lanes



E-733

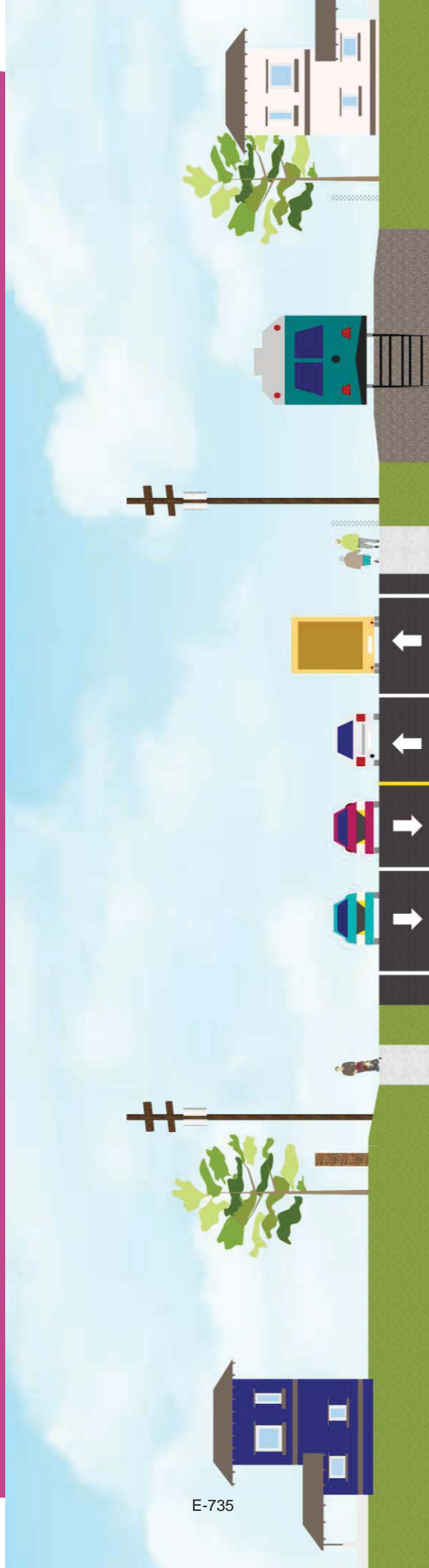
3 The School Segment

4 Lanes with a Wide Sidewalk



3 The School Segment

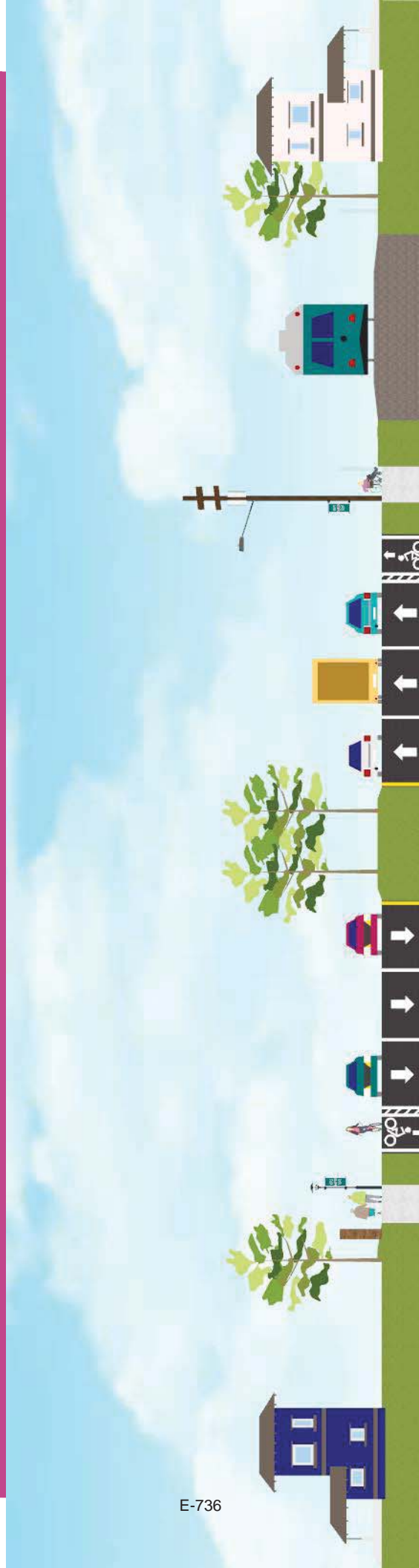
No Build (Existing Typical Section)



E-735

1 The County Neighborhood Segment

6 Lanes with Buffered Bike Lanes

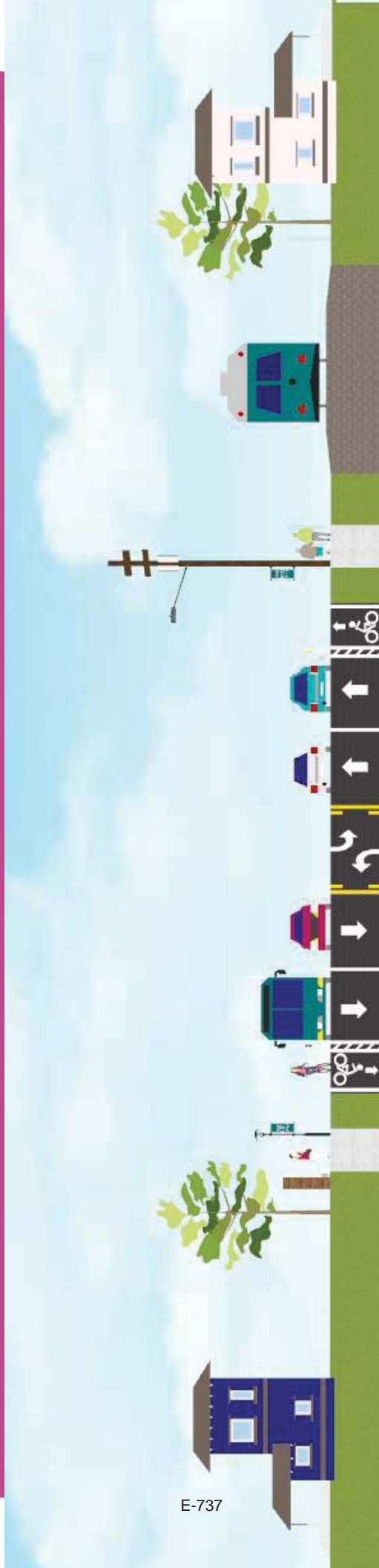


E-736

1 The County Neighborhood Segment

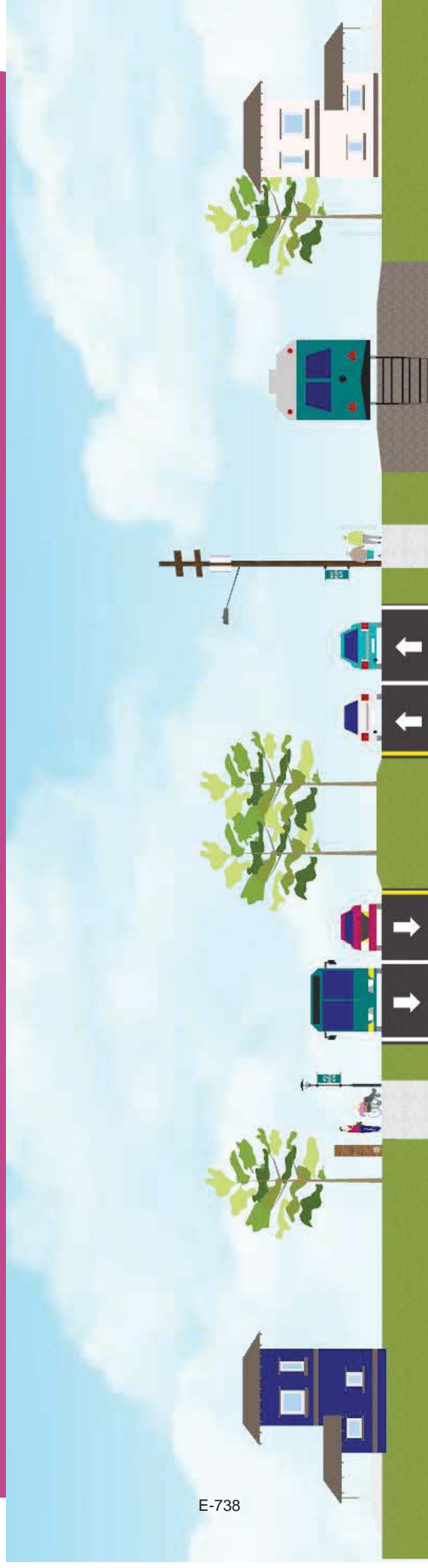
5 Lanes with Buffered Bike Lanes

E-737



1 The County Neighborhood Segment

4 Lanes with a Wide Sidewalk



E-738

1 The County Neighborhood Segment

6

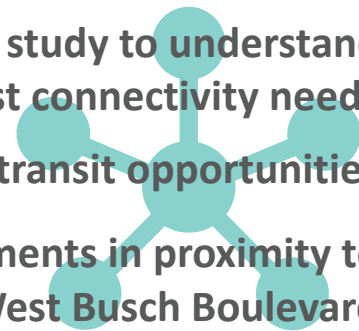
Supporting Recommendations

Partner agencies also play a role in supporting the corridor vision. The following recommendations can be implemented with their support...



- Add bus shelters to high-volume locations
- Relocate bus stops to be adjacent to right-turn lanes
- Add transit signal priority to enable buses to skip the queue

- Conduct a regional origin-destination study to understand regional east-west connectivity needs
 - Explore passenger rail transit opportunities
- Plan and prioritize additional bicycle investments in proximity to West Busch Boulevard



- Create an overlay district in the City of Tampa to ensure future development is consistent with the corridor vision



- School zone study
- High visibility police enforcement area



Benefits of a Lower Speed

Reducing the design speed along the corridor could...

- ✓ Better meet the corridor vision for pedestrians, bicyclists, and transit users
- ✓ Provide additional safety benefits including:
 - ✓ Reduce crossing distances
 - ✓ Reduce stopping sight distances
 - ✓ Reduce crash injury and mortality rates
- ✓ Allow trees, lighting and utilities to be located outside of the sidewalk
- ✓ Reduce the amount of right-of-way needed by up to 9 feet
- ✓ Lower construction cost
- ✓ Allow the opportunity for parallel parking, a two-way left turn lane and/or sharrow

The opportunity to lower the design speed / speed limit along the corridor is being recommended for further study.

Appendix F









Stakeholder Support



Corridor Alternatives

Relative Project Advisory Group Support

Corridor Alternatives	Relative Project Advisory Group Support
<p>No-Build</p> 	
<p>4-Lane</p> 	
<p>5-Lane</p> 	
<p>6-Lane</p> 	

PAG Input Results					
					Overall Score
Existing Condition	0	0	2	5	
4-Lane Alternative	2	2	3	0	
5-Lane Alternative	3	3	0	1	
6-Lane Alternative	0	1	0	6	

Appendix G



1 The County Neighborhood Segment

DALE MABRY HWY
HIMES AVE
TWIN LAKES BLVD

Unincorporated Hillsborough County



2 The Business Segment

ARMENIA AVE
WINTERS AVE
RTH BLVD
PENNOA AVE
EROCKA AVE



3 The School Segment

The 71st Segment



Section 3

Public Visioning Workshop

CORRIDOR VISIONING WORKSHOP MEETINGS SUMMARY – November 16, 2017

A corridor visioning workshop was held on Thursday, November 16, 2017 from 5:30 p.m. to 7:30 p.m. at the Christian Family Church (3457 W. Kenyon Avenue, Tampa, Florida 33614). This meeting is the first of two public meetings scheduled to take place during the corridor study. The second meeting will be an alternatives public meeting, anticipated to be scheduled in the spring of 2018.



Public Notification

Meeting invitations were sent by e-mail to 33 elected officials and 115 appointed officials/interested parties. Meeting postcard invitations were sent by mail to 10 Native American Tribes, 1255 property owners and 642 current residents. Meeting flier invitations were distributed along the project corridor to locations where a large number of people gather. The invitations included dates, times and locations for the corridor visioning workshop. Copies of the meeting invitations are included in Appendix A.

Media Notification

A corridor visioning workshop notification was placed in the Florida Administrative Register November 3, 2017 Edition. A press release was distributed by the Florida Department of Transportation (FDOT) to major local media outlets on November 9, 2017. Copies of the Florida Administrative Weekly Register and the press release are included in Appendix B.

Public Meeting Agenda

The meeting was conducted in an interactive open house format utilizing six (6) stations. The public was invited to attend at any time between 5:30 p.m. and 7:30 p.m. Parking was available to accommodate all of the attendees, including the disabled. Signs were placed at the major intersecting roads to direct attendees to the meeting location and from the parking lot to the meeting room.



Meeting attendees were asked to visit the following six (6) stations:

Station 1- PowerPoint

A PowerPoint presentation played on a loop in a separate meeting room. The presentation informed attendees of the purpose of the corridor study and existing conditions throughout the study limits.

Station 2- Existing Conditions Activity

Meeting attendees were given Post-it notes to place on a corridor map, identifying the issues they see in the corridor. A SMART board with a project map and other projects in the area overlay was also available, attendees could identify issues in the corridor on the format of their choosing.

Station 3- Information on Transportation Characteristics of the Corridor

Meeting attendees viewed boards on the following aspects: Existing Typical Sections; Traffic Level of Service Map; Bicycle, Pedestrian and Transit Map; and Crash Locations 2011-2015. Members of the project consultant team were available to answer any questions the attendees had.

Station 4- Corridor Visioning Exercise

Meeting attendees were given 5 star stickers to place on their top priorities for the future of the corridor on a Developing the Corridor board (see Appendix C). The stickers were not placed in a specific priority order (all stickers have equal weight).

Station 5- Potential Improvement Exercise

Meeting attendees were given 5 stickers numbered 1-5 with #1 being the highest priority to place on a Potential Improvements board (see Appendix C).

Station 6- Comments and User Preference Survey

Meeting attendees received a comment sheet and a user preference survey. Attendees were asked to provide feedback by filling out a comment sheet and a user preference survey.

The following display boards were available for public review (see copies of the boards in Appendix D):

- Welcome Board
- Title VI Board / English & Spanish
- Project Sheet 1-3
- Project Location Map
- Traffic Level of Service Map
- Bicycle, Pedestrian and Transit Map
- Existing Typical Sections
- Crash Locations 2011-2015



Meeting Attendance

23 persons signed the sign-in sheet at the corridor visioning workshop. A copy of the sign-in sheets is included in Appendix E.

Attending members of the project team included the Florida Department of Transportation (FDOT) District Seven Office and project consultants. Project representatives were present to discuss the project, receive public input, and answer questions.



Meeting Handouts

A meeting agenda was distributed to the attendees along with newsletter #1 and a user preference survey. Copies of the meeting handouts are included in Appendix F.

Public Comments

2 comment forms were received at the corridor visioning workshop (see attached in Appendix G). No additional comment forms were received by mail and email.



User Preference Survey

Zero user preference surveys were completed by attendees at the Corridor Visioning Workshop. Zero opinion surveys were received by mail and email.

END OF MEETING SUMMARY

This meeting summary was prepared by Karen Harrell, Public Involvement Coordinator. If you feel that clarifications are necessary, or if this differs from your understanding, please notify Karen by telephone at 352-257-1651 or by email at Karen.Harrell@qcausa.com within five (5) working days upon receipt of this summary.

APPENDICES

APPENDIX A – Public Notification

Email Invitation to Elected / Appointed Officials
Meeting Invitation Postcard to Property Owners
Meeting Invitation Flier

APPENDIX B – Media Notification

Florida Administrative Register Ad
Press Release

APPENDIX C – Pictures of Activity Boards

Corridor Visioning Exercise
Potential Improvement Exercise

APPENDIX C – Display Boards / PowerPoint Presentation

APPENDIX D – Sign-in Sheets

APPENDIX E – Handouts

Meeting Agenda
Newsletter #1
User Preference Survey

APPENDIX G – Public Comments

APPENDIX A – Public Notification

Email Invitation to Elected / Appointed Officials

Meeting Invitation Postcard to Property Owners

Meeting Invitation Flier

Date: October 25, 2017

Subject: Visioning Workshop
West Busch Boulevard (SR 580) Corridor Study
from North Dale Mabry Highway to North Nebraska Avenue
Hillsborough County, Florida
FDOT Financial Project ID Number: 435908-1-22-01

Dear Agency Partner:

The Florida Department of Transportation (FDOT), District Seven has scheduled a Visioning Workshop for a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The study area includes portions of incorporated Hillsborough County and the City of Tampa. This notice has been sent to other public officials and agency representatives interested in the project. The Visioning Workshop is scheduled for November 16, 2017, from 5:30 p.m. to 7:30 p.m. at the **Christian Family Church, 3457 W. Kenyon Avenue, Tampa, Florida 33614.**

The FDOT is looking for ways to improve West Busch Boulevard (SR 580) for pedestrians, bicyclists, transit users and motorists. The Visioning Workshop is an opportunity for the project team to report on the findings from the Project Advisory Group Meetings #1 and #2 and for the public to provide input on the vision for the corridor. Attached is a post card invitation that is being sent to property owners, organizations and individuals interested in the project.

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220 at least seven (7) working days in advance of the meeting.

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT.

If you require further information concerning this project, please contact Brian Shroyer, Florida Department of Transportation Project Manager at 813-975-6449 or by email at Brian.Shroyer@dot.state.fl.us.

Sincerely,
Brian Shroyer
Multimodal Project Manager
Florida Department of Transportation



You're invited to take part in a Florida Department of Transportation (FDOT) Visioning Workshop for a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue in Hillsborough County – a distance of approximately 3.3 miles.

The FDOT is looking for ways to improve West Busch Boulevard (SR 580) for pedestrians, bicyclists, transit users and motorists. At this workshop, we will gather your input on the existing corridor and document your ideas to resolve some of the issues that you have observed.

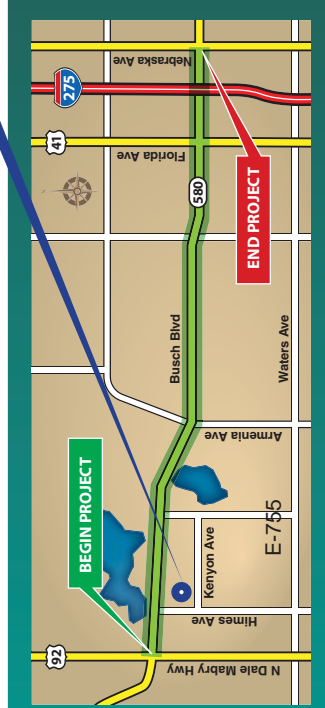
Thursday, November 16, 2017, 5:30 p.m. to 7:30 p.m.
Christian Family Church, 3457 W. Kenyon Avenue, Tampa, FL 33614

For More Information Contact:

Brian Shroyer, CPM
Project Manager
Florida Department of Transportation
11201 N. McKinley Drive
Tampa, FL 33612
813-975-6449
brian.shroyer@dot.state.fl.us

Visit Our Website

fdot7studies.com/westbuschblvd/



Brian Shroyer, CPM
Project Manager
Florida Department of Transportation
11201 North McKinley Drive
Tampa, FL 33612

West Busch Boulevard (SR 580) Corridor Study
From North Dale Mabry Highway to North Nebraska Avenue

Visit Our Website

fdot7studies.com/westbuschblvd/



Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220.

Comuníquese Con Nosotros Nos importa mucho la opinión del público sobre el proyecto. Si usted tiene preguntas o comentarios, o si simplemente desea más información, por favor comuníquese con nuestra representante, Lilliam Escalera, 813-975-6403, Departamento de Transportación de Florida, 11201 N. McKinley Dr., Tampa, FL 33612, lilliam.escalera@dot.state.fl.us

West Busch Boulevard (SR 580) Corridor Study

From North Dale Mabry Highway to North Nebraska Avenue

Hillsborough County, FL

Financial Project ID Number: 435908-1-22-01

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For More Information Contact:

Brian Shroyer, CPM, Project Manager, Florida Department of Transportation
 11201 N. McKinley Drive, Tampa, FL 33612, 813-975-6449, brian.shroyer@dot.state.fl.us

▶ Visit our website: www.fdotd7studies.com/westbuschblvd/

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220.

Comuniquese Con Nosotros Nos importa mucho la opinión del público sobre el proyecto. Si usted tiene preguntas o comentarios, o si simplemente desea más información, por favor comuniquese con nuestra representante, Lilliam Escalera, 813-975-6403, Departamento de Transportación de Florida, 11201 N. McKinley Dr., Tampa, FL 33612, lilliam.escalera@dot.state.fl.us

APPENDIX B – Media Notification

Florida Administrative Register Ad

Press Release

Notice of Meeting/Workshop Hearing

OTHER AGENCIES AND ORGANIZATIONS

OCAusa

The Florida Department of Transportation, District Seven announces a workshop to which all persons are invited.

DATE AND TIME: November 16, 2017, 5:30 PM to 7:30 PM

PLACE: Christian Family Church, 3457 W. Kenyon Avenue, Tampa, FL 33614

GENERAL SUBJECT MATTER TO BE CONSIDERED: The Florida Department of Transportation (FDOT), District Seven, is holding a Visioning Workshop for a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The study area includes portions of unincorporated Hillsborough County and the City of Tampa.

The FDOT is looking for ways to improve West Busch Boulevard (SR 580) for pedestrians, bicyclists, transit users and motorists. The Visioning Workshop is an opportunity for the project team to report on the findings from Project Advisory Group Meetings and for the public to provide input on the vision for the corridor.

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT.

Financial Project Identification Number: 435908-1-22-01

A copy of the agenda may be obtained by contacting: Mr. Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us

Pursuant to the provisions of the Americans with Disabilities Act, any person requiring special accommodations to participate in this workshop/meeting is asked to advise the agency at least 7 days before the workshop/meeting by contacting: Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220

Comuniquese Con Nosotros

Nos importa mucho la opinión del público sobre el proyecto. Si usted tiene preguntas o comentarios, o si simplemente desea más información, por favor comuniquese con nuestra representante, Lilliam Escalera, al (813) 975-6403, Departamento de Transportación de Florida, 11201 N. McKinley Dr., Tampa, FL 33612, Lilliam.Escalera@dot.state.fl.us. If you are hearing or speech impaired, please contact the agency using the Florida Relay Service, 1(800)955-8771 (TDD) or 1(800)955-8770 (Voice).

For more information, you may contact: Mr. Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us

West Busch Boulevard (SR 580) Corridor Study
Visioning Workshop Scheduled

Tampa, FL – The Florida Department of Transportation (FDOT), District Seven, is holding a Visioning Workshop for a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The study area includes portions of unincorporated Hillsborough County and the City of Tampa.

The Visioning Workshop is scheduled for November 16, 2017, from 5:30 p.m. to 7:30 p.m. at the Christian Family Church, 3457 W. Kenyon Avenue, Tampa, Florida 33614.

The FDOT is looking for ways to improve West Busch Boulevard (SR 580) for pedestrians, bicyclists, transit users and motorists. The Visioning Workshop is an opportunity for the project team to report on the findings from Project Advisory Group Meetings and for the public to provide input on the vision for the corridor. A copy of the agenda may be obtained by contacting: Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us.

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220 at least seven (7) working days in advance of the meeting.

Comuniquese Con Nosotros

Nos importa mucho la opinión del público sobre el proyecto. Si usted tiene preguntas o comentarios, o si simplemente desea más información, por favor comuniquese con nuestra representante, Lilliam Escalera, al (813) 975-6403, Departamento de Transportación de Florida, 11201 N. McKinley Dr., Tampa, FL 33612, Lilliam.Escalera@dot.state.fl.us

For more information on this project, please call the FDOT Project Manager, Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us.

###

APPENDIX C – Pictures of Activity Boards

Corridor Visioning Exercise

Potential Improvement Exercise

Developing the Corridor Vision



Now we want to know what is important to you! Help us shape the future vision of the West Busch Boulevard Corridor. Vote for the goals and objectives presented here, or add in your own!

My Vision is for a West Busch Boulevard where...

ACCESSIBILITY & CONNECTIVITY

- Transit users have shade and comfortable amenities ★
- A continuous sidewalk runs the length of the corridor ★★☆☆
- Bicycle connectivity is enhanced to adjacent homes and businesses ☆
- Safe and frequent options are provided for pedestrians and bicyclists to cross the corridor ★★☆☆

ECONOMIC DEVELOPMENT

- Aesthetic enhancements provide shade and adds beauty to the corridor ☆
- The corridor adds a unique and inviting character to the community ☆
- Existing businesses are preserved along the corridor ☆☆☆☆
- The roadway attracts reinvestment into the corridor and surrounding areas ☆

MOBILITY

- Travelers experience efficient and reliable travel times ☆☆☆
- Facilities are ADA compliant
- Congestion is reduced on the corridor ★★
- Citizens with limited mobility have enough time to cross the street

SAFETY

- Crashes occur less frequently and are not as severe ☆☆☆
- Drivers slow down to a safe speed ☆☆☆☆
- Increased lighting improves visibility for drivers and pedestrians ☆
- Children can travel to and from school safely, regardless of mode of travel ☆
- Consolidated driveways reduce conflict points for vehicles and pedestrians ★★☆☆☆☆

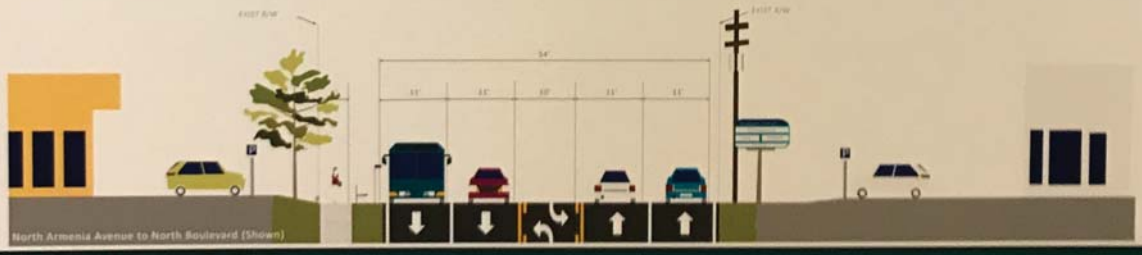
Potential Improvements

The limited available right-of-way along the corridor means that many potential improvements can not be accommodated without impacting adjacent properties. Please help prioritize the list of possible improvements below using the provided stickers to indicate **your top five** desired improvements for the corridor, with “#1” being the most preferred improvement.

- ADD ADDITIONAL TRAVEL LANES** 5 4
- CREATE A PEDESTRIAN-ONLY PHASE AT KEY INTERSECTIONS*** 3 1 2 1 5 3
- IMPROVE TRAFFIC SIGNAL TIMING*** 4 2 3 2 2 3
- ADD ON-STREET PARKING**
- ADD MID-BLOCK CROSSWALKS*** 4
- LANDSCAPING, PEDESTRIAN LIGHTING, ETC.*** 2 4 5
- LOWER SPEED LIMIT*** 2 1 3 4 4 5 5
- CREATE A WIDE SIDEWALK ALONG ENTIRE CORRIDOR** 2 4 1 3 2
- ADD ON-STREET BIKE LANES**
- ADD BUFFERED BIKE LANES** 1
- ADD A DEDICATED BUS LANE**
- ADD SCHOOL ZONE*** 1 3
- FILL IN SIDEWALK GAPS** 3 1 3 1 4
- ADD BUS SHELTERS** 2 5 2
- ADD A LANDSCAPED MEDIAN** 5

*Indicates improvements within the existing right-of-way

(WRITE IN ANYTHING ELSE!)



APPENDIX D – Display Boards / PowerPoint Presentation



Welcome

to the

West Busch Boulevard Study Corridor Visioning Workshop



TITLE VI



The proposed project is being developed in accordance with the Civil Rights Act of 1964, as amended. Under Title VI of the Civil Rights Act, public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status.

To express concern(s), please contact the following:

Christopher Speese

Title VI Coordinator | Florida Department of Transportation, District Seven
11201 N. McKinley Drive | Tampa, Florida 33612
Christopher.Speese@dot.state.fl.us
(813) 975-6405

-- or --

Jacqueline Paramore

Statewide Title VI Coordinator | Equal Opportunity Office
605 Suwannee Street, Room 260 | Mail Station 65
Tallahassee, Florida 32399-0450
Jacqueline.Paramore@dot.state.fl.us
(850) 414-4753



TITULO VI



Este proyecto se está diseñando de acuerdo con la Ley de Derechos Civiles de 1964 y 1968, según enmendada. Bajo el Título VI de la Ley de Derechos Civiles, la participación pública es solicitada sin distinción de raza, color, origen nacional, edad, sexo, incapacidad o estado familiar.

Para expresar alguna preocupación, comuníquese con:

Lilliam Escalera

Florida Department of Transportation, District Seven
11201 N. McKinley Drive | Tampa, Florida 33612
Lilliam.Escalera@dot.state.fl.us
(813) 975-6445

-- or --

Jacqueline Paramore

Statewide Title VI Coordinator | Equal Opportunity Office
605 Suwannee Street, Room 260 | Mail Station 65
Tallahassee, Florida 32399-0450
Jacqueline.Paramore@dot.state.fl.us
(850) 414-4753

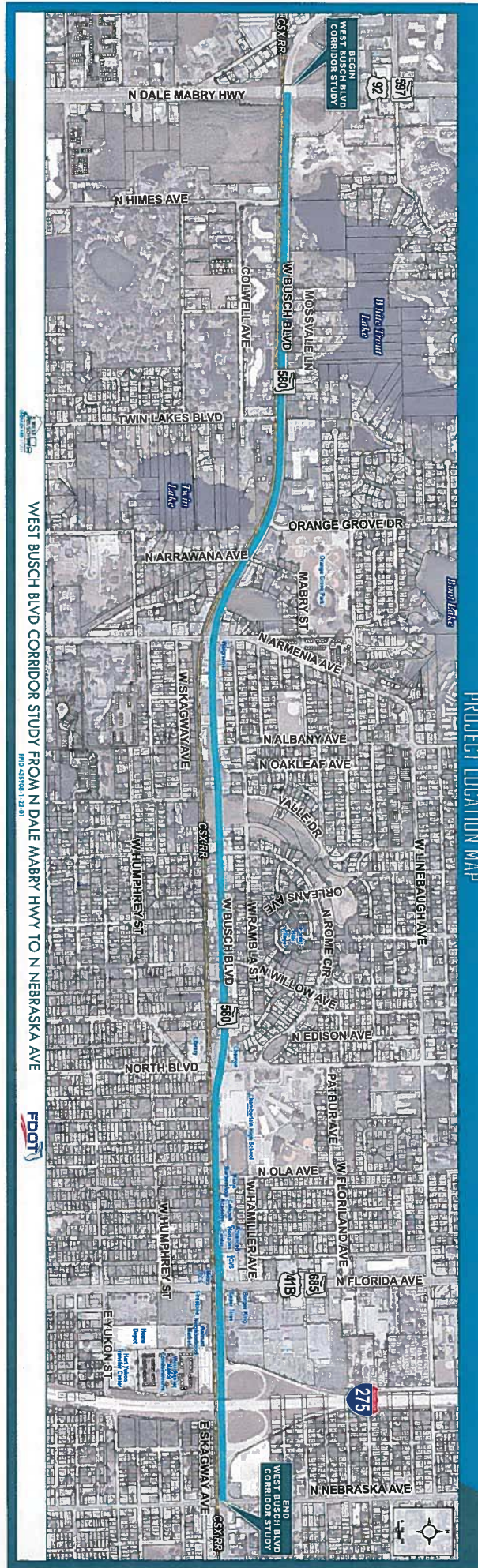
Project Sheet 1 of 3



Project Sheet 2 of 3







PROJECT LOCATION MAP

WEST BUSCH BLVD CORRIDOR STUDY FROM N DALE MABRY HWY TO N NEBRASKA AVE

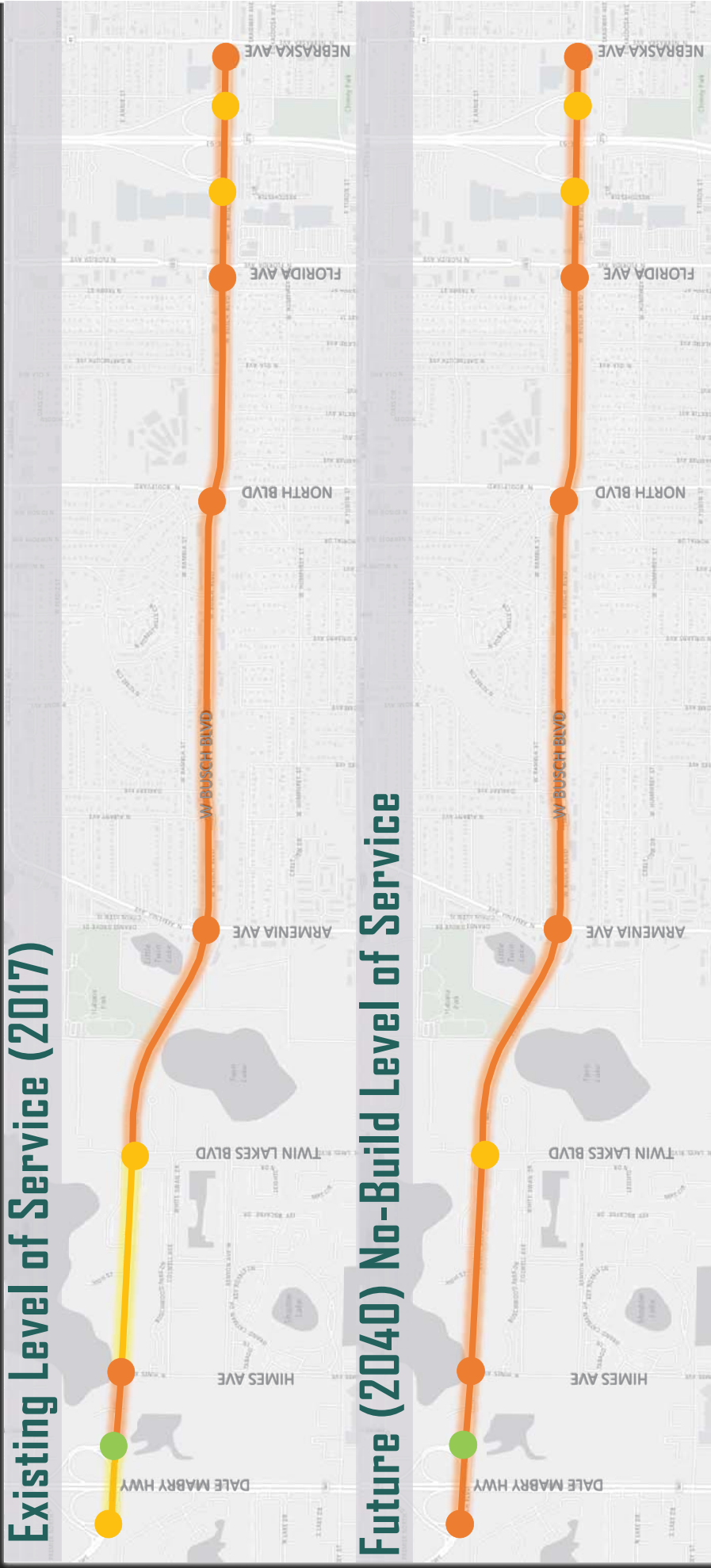


FILE NUMBER: 128.01



Traffic Level of Service Map

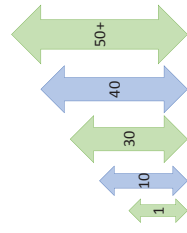
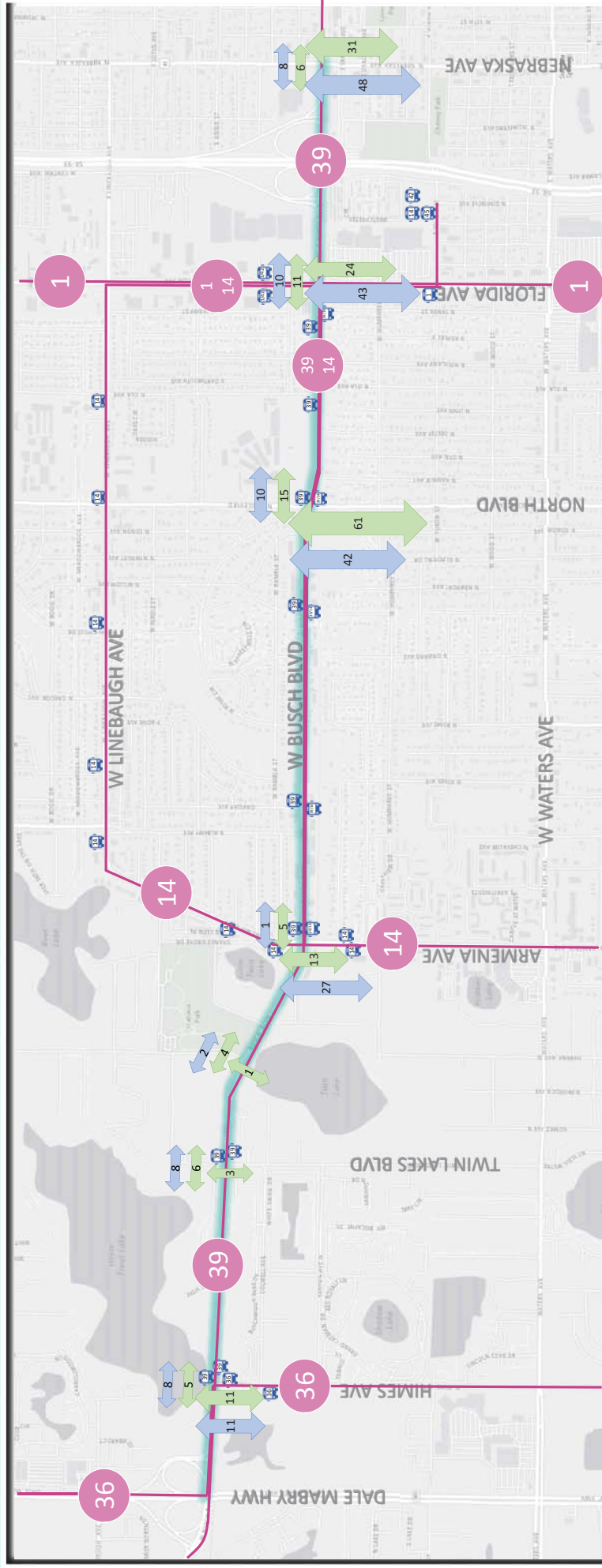
Existing Level of Service (2017)



Future (2040) No-Build Level of Service



Bicycle, Pedestrian, & Transit Map



By Cyclists
(blue)



By Pedestrians
(green)



Transit Stop

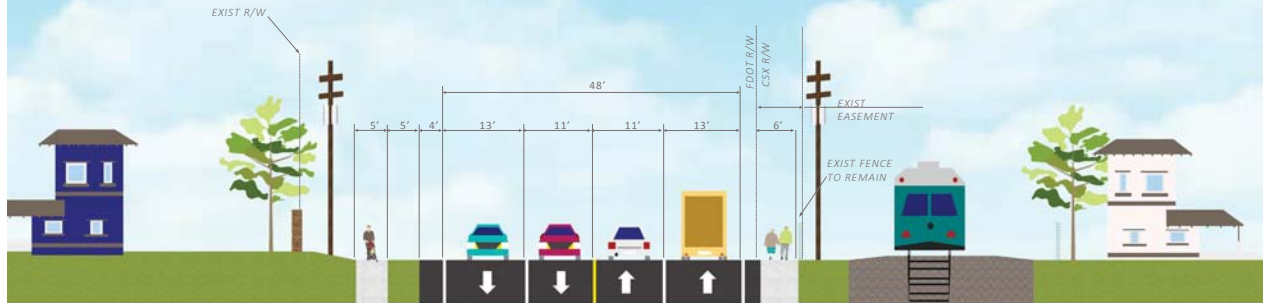


Transit Route

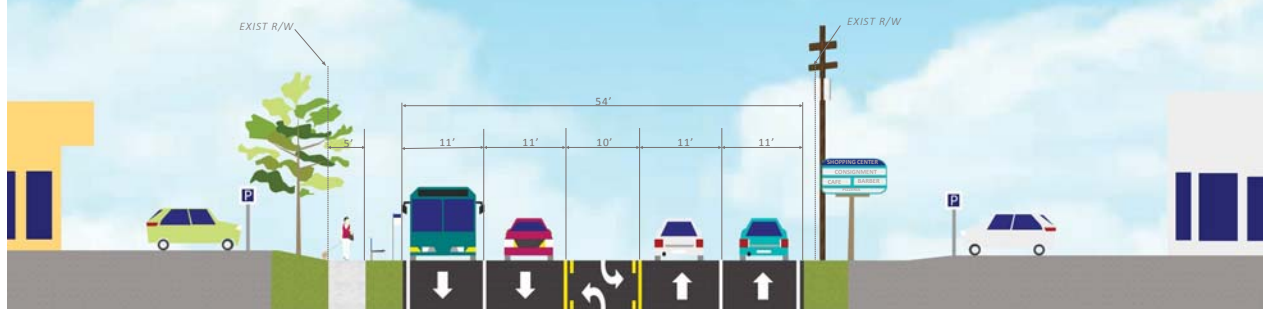
* Based on observations from 7 a.m. – 9 a.m. & 4 p.m. – 6 p.m. on Thursday May 4, 2017

Existing Typical Sections

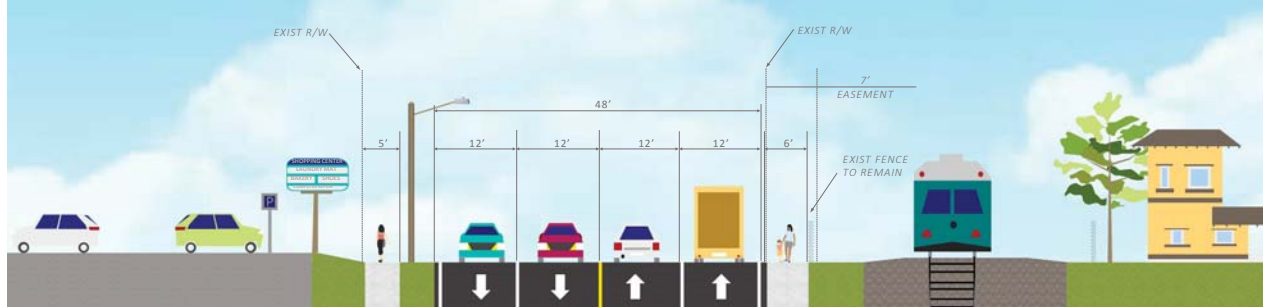
North Dale Mabry Highway to North Armenia Avenue
Suburban (Residential) Land Uses



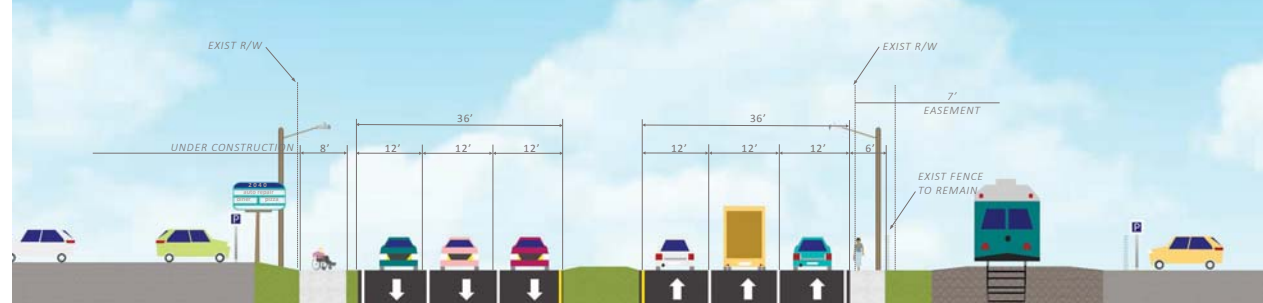
North Armenia Avenue to North Boulevard
Mixed-Use General Urban Land Uses



North Boulevard to North Florida Avenue
Suburban (Commercial) Land Uses



North Florida Avenue to North Nebraska Avenue
Suburban (Commercial) Land Uses





Crash Locations 2011-2015*



*Source: FDOT Crash Analysis Reporting System (CARS)



Developing the Corridor Vision

You helped us identify the corridor issues at Station 3. Now we want to know what is important to you! Help us shape the future vision of the West Busch Boulevard Corridor. Vote for the goals and objectives presented here, or add in your own!

My Vision is for a West Busch Boulevard where...

ACCESSIBILITY & CONNECTIVITY

Transit users have shade and comfortable amenities

A continuous sidewalk runs the length of the corridor

Bicycle connectivity is enhanced to adjacent homes and businesses

Safe and frequent options are provided for pedestrians and bicyclists to cross the corridor

ECONOMIC DEVELOPMENT

Aesthetic enhancements provide shade and adds beauty to the corridor

The corridor adds a unique and inviting character to the community

Existing businesses are preserved along the corridor

The improved roadway attracts reinvestment into the corridor and surrounding areas

MOBILITY

Travelers experience efficient and reliable travel times

Facilities are ADA compliant

Congestion is reduced on the corridor

Citizens with limited mobility have enough time to cross the street

SAFETY

Crashes occur less frequently and are not as severe

Drivers slow down to a safe speed

Increased lighting improves visibility for drivers and pedestrians


Children can travel to and from school safely, regardless of mode of travel

Consolidated driveway access reduces conflict points for vehicles and pedestrians




Potential Improvements


The limited available right-of-way along the corridor means that many potential improvements can not be accommodated without impacting adjacent properties. Please help prioritize the list of possible improvements below using the provided stickers to indicate **your top five** desired improvements for the corridor, with “#1” being the most preferred improvement.


 **ADD ADDITIONAL TRAVEL LANES**


CREATE A PEDESTRIAN-ONLY PHASE AT KEY INTERSECTIONS 

 **IMPROVE TRAFFIC SIGNAL TIMING**


ADD ON-STREET PARKING 

 **ADD MID-BLOCK CROSSWALKS**


LANDSCAPING, PEDESTRIAN LIGHTING, ETC. 

 **LOWER SPEED LIMIT**


CREATE A WIDE SIDEWALK ALONG ENTIRE CORRIDOR 


 **ADD ON-STREET BIKE LANES**


ADD BUFFERED BIKE LANES 

 **ADD A DEDICATED BUS LANE**

ADD SCHOOL ZONE 

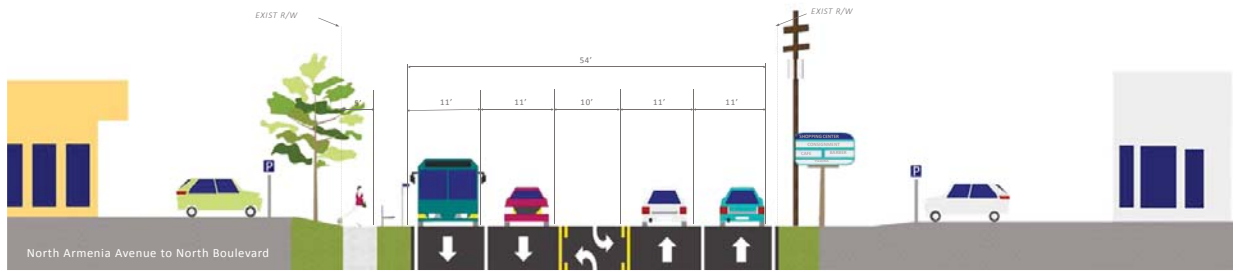
 **FILL IN SIDEWALK GAPS**

ADD BUS SHELTERS 

 **ADD A LANDSCAPED MEDIAN**

*Indicates improvements within the existing right-of-way

(WRITE IN ANYTHING ELSE!)



Welcome to the
West Busch Boulevard (SR 580) Corridor Study
from North Dale Mabry Highway to North Nebraska Avenue

Public Visioning Workshop

Hillsborough County, Florida
FDOT Financial Project ID Number: 435908-1-22-01
Christian Family Church
November 16, 2017 5:30 p.m. – 7:30 p.m.



E-778

Presentation

1

Introduction

2

Study Process

3

Develop the Vision

4

Next Steps

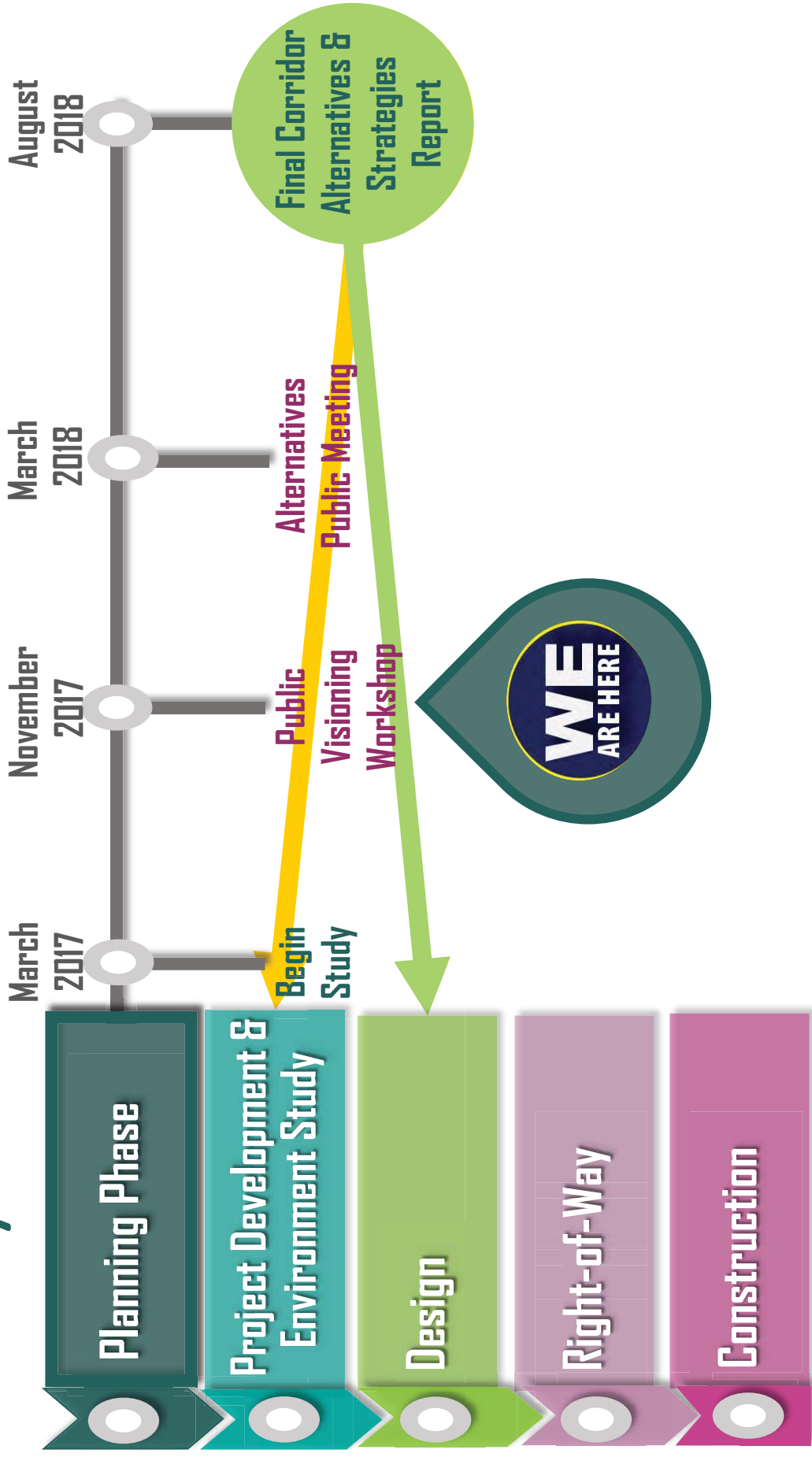


Project Study Area

- 3.3 miles
- Within the city of Tampa and unincorporated Hillsborough County

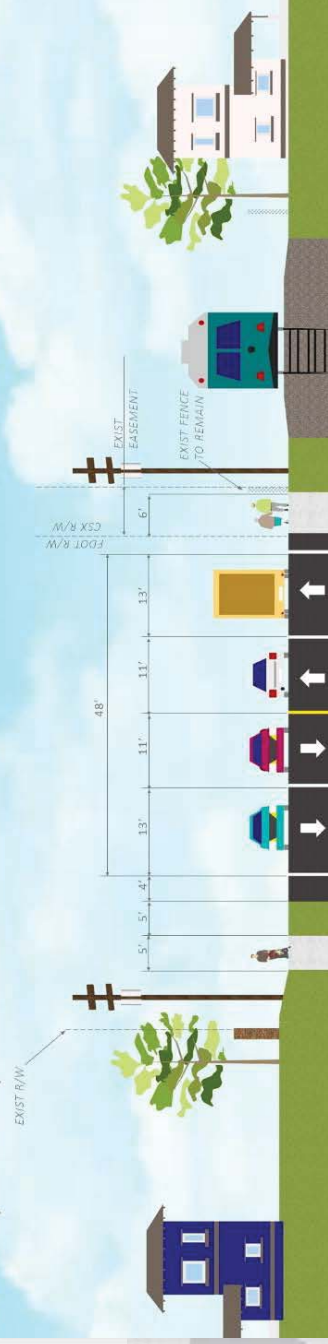


Study Process



Existing Conditions Dale Mabry Highway to North Armenia Avenue

North Dale Mabry Highway to North Armenia Avenue
Suburban (Residential) Land Uses



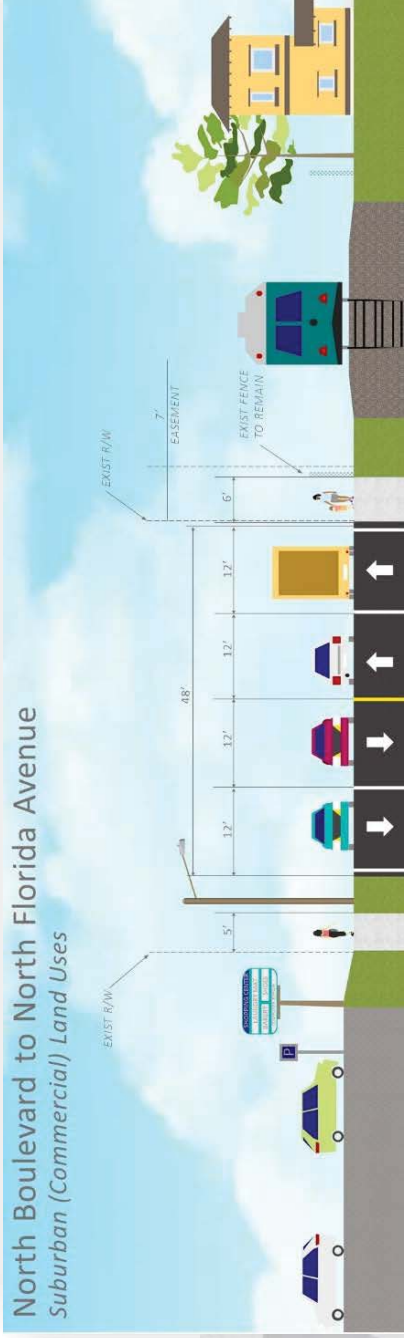
The map shows an aerial view of the project area. A yellow line highlights the project corridor from Dale Mabry Hwy in the north to North Armenia Ave in the south. Key streets shown include Dale Mabry Hwy, Himes Ave, Twin Lakes Blvd, W Busch Blvd, North Blvd, W Waters Ave, and Nebraska Ave. A dashed green line indicates a specific section of the project area.

Existing Conditions North Armenia Avenue to North Boulevard

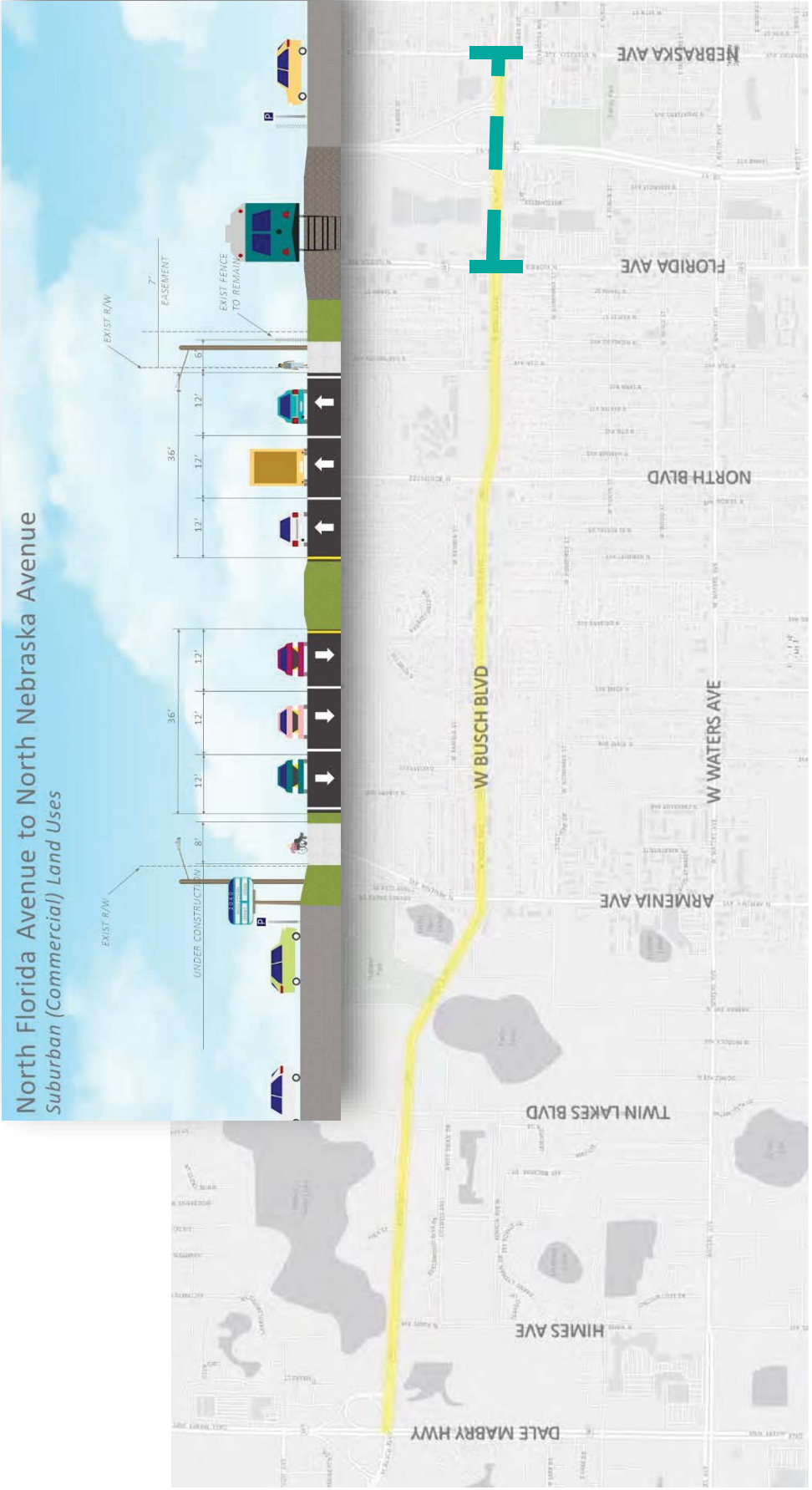


Existing Conditions North Boulevard to North Florida Avenue

North Boulevard to North Florida Avenue
Suburban (Commercial) Land Uses



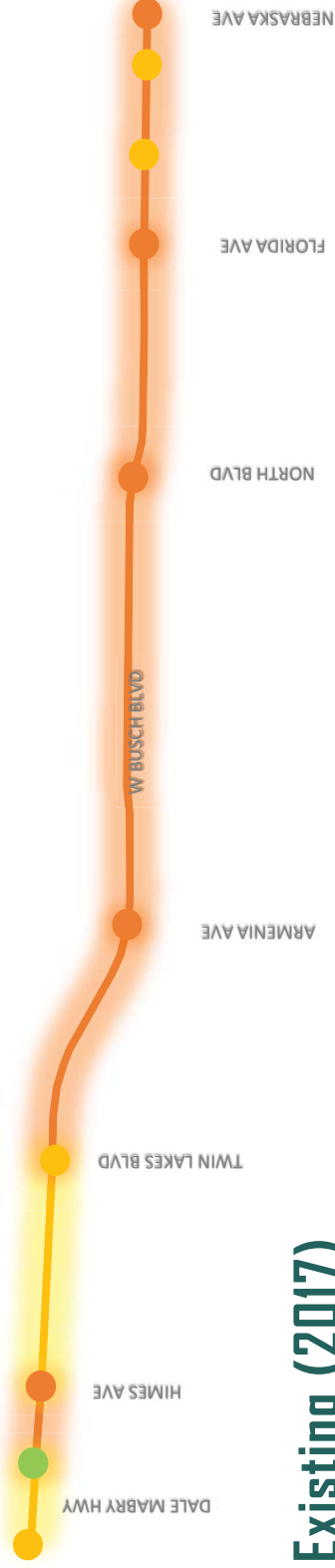
Existing Conditions North Florida Avenue to North Nebraska Avenue



Traffic Level of Service Maps

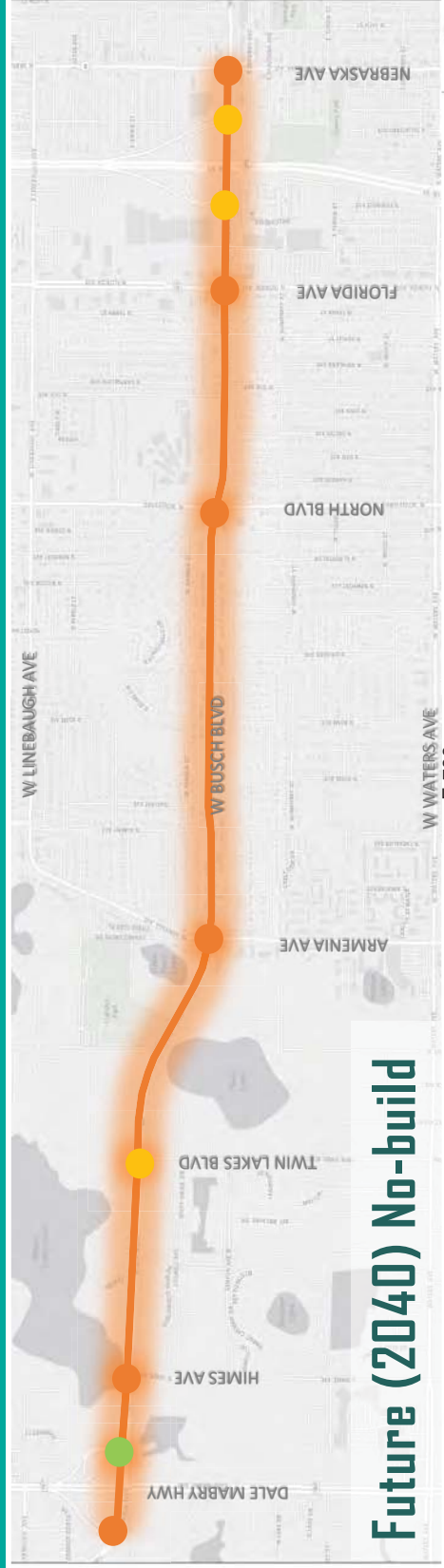


W LINEBAUGH AVE



Existing (2017)

W WATERS AVE

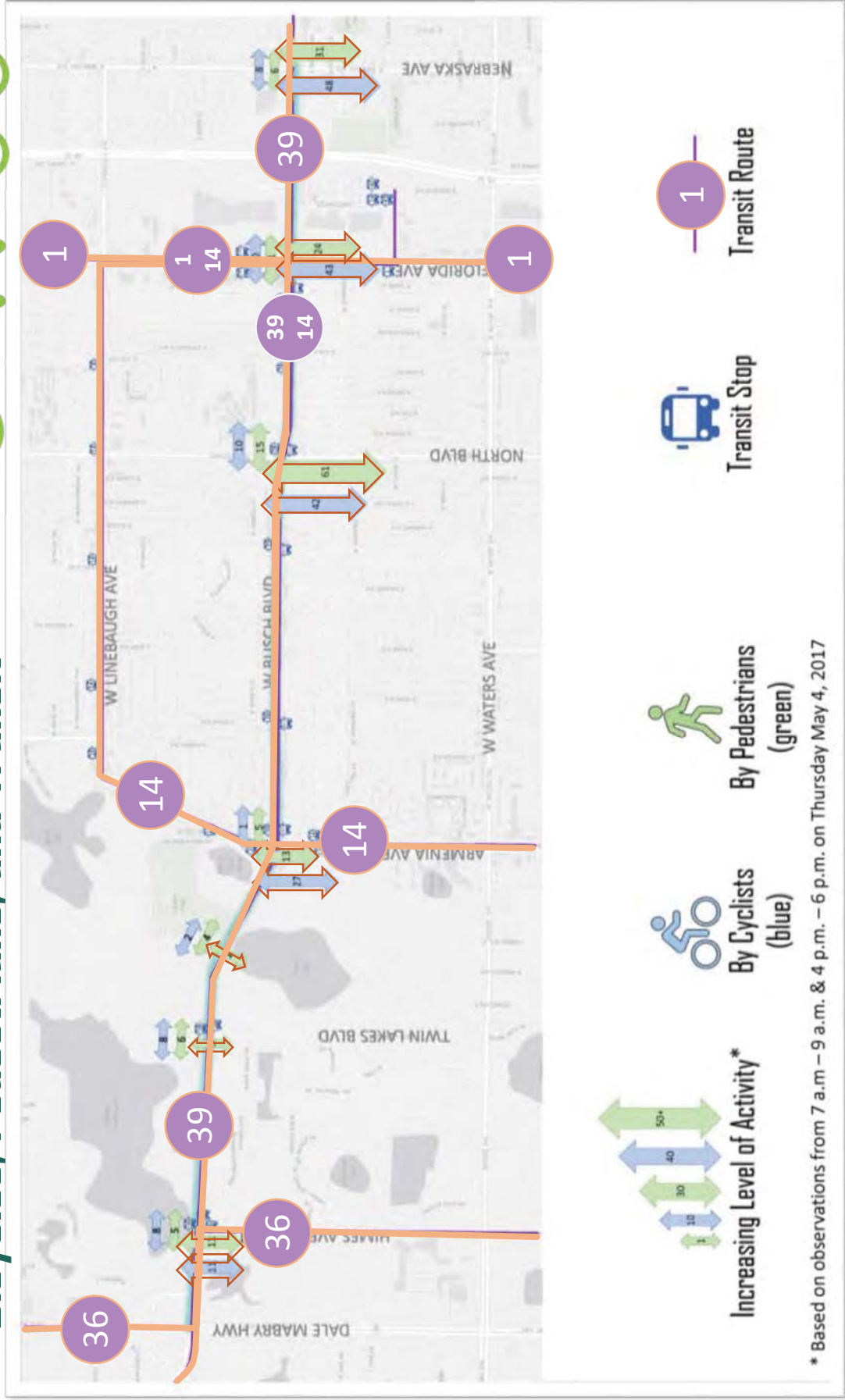


Future (2040) No-build

W WATERS AVE

E-786

Existing Conditions Bicycles, Pedestrians, and Transit



Develop the Vision

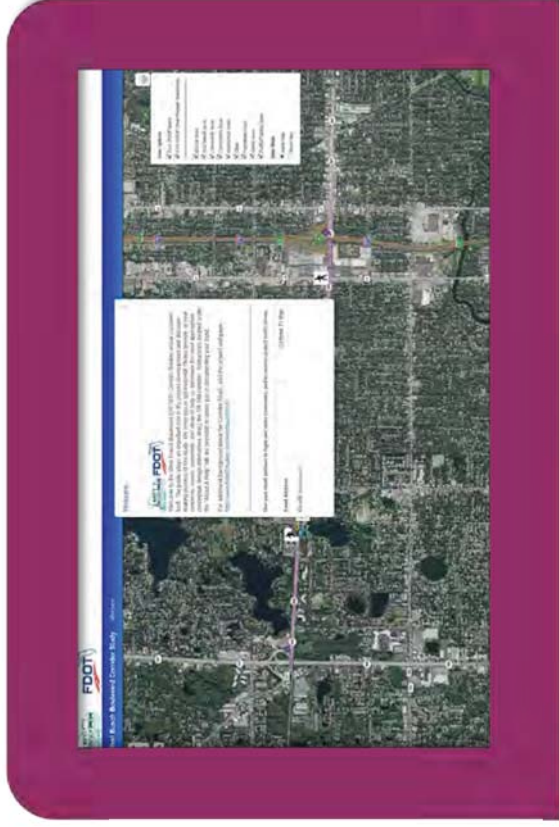


Concerns and Issues Along the Corridor A Sticky-Note Exercise



E-790

Wiki-Mapping




Use this map to share your concerns


Route I'd ride if...



Route I'd walk if...




Need bike parking 


Problem 


Facilities

 Sidewalk


 Greenway - Paved

 Greenway - Unpaved

 Bike Lane - Both sides

 Bike Lane - One side

 Sidewalk

 Wide outside shoulder

 Dirt Trails

 Bus routes

Visit: <http://wikimapping.com/wikimap/West-Busch-Boulevard-Corridor-Study.html>

E-791



Developing the Corridor Vision

You helped us identify the corridor issues at Station 3. Now we want to know what is important to you! Help us shape the future vision of the West Busch Boulevard Corridor. Vote for the goals and objectives presented here, or add in your own!

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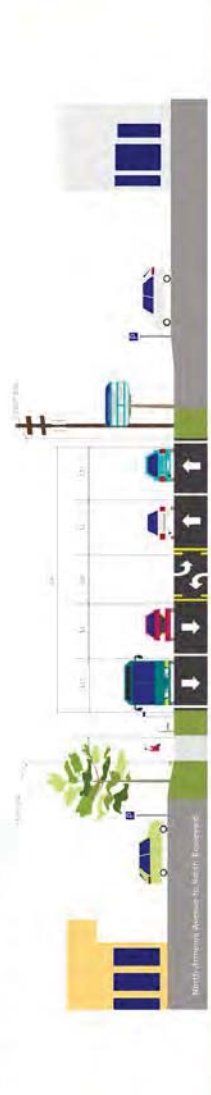
Consolidated driveways reduce conflict points for vehicles and pedestrians



Potential Improvements

The limited available right-of-way along the corridor means that many potential improvements can not be accommodated without impacting adjacent properties. Please help prioritize the list of possible improvements below using the provided stickers to indicate your top five desired improvements for the corridor, with "#1" being the most preferred improvement.

- ADD ADDITIONAL TRAVEL LANES
 - CREATE A PEDESTRIAN-ONLY PHASE AT KEY INTERSECTIONS
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 - ADD ON-STREET PARKING
 - ADD MID-BLOCK CROSSWALKS
 - LANDSCAPING, PEDESTRIAN LIGHTING, ETC.
 - LOWER SPEED LIMIT
 - CREATE A WIDE SIDEWALK ALONG ENTIRE CORRIDOR
 - ADD ON-STREET BIKE LANES
 - ADD BUFFERED BIKE LANES
 - ADD A DEDICATED BUS LANE
 - ADD SCHOOL ZONE
 - FILL IN SIDEWALK GAPS
 - ADD BUS SHELTERS
 - ADD A LANDSCAPED MEDIAN
- (WRITE IN ANYTHING ELSE!)



*Potential improvements within the existing right-of-way



User Preference Survey



DRAFT User Preference Survey
SR 580 (West Busch Boulevard) Corridor Study
 from Dale Mabry Hwy. to Nebraska Ave.

Please fill out this survey as best as you can and place in the Survey forms box.

- Home Zip code: _____
- Work/School zip code: _____
- Please circle all that apply:
 - I live on or near Busch Boulevard
 - I work on or near Busch Boulevard
 - I shop/visit businesses on or near Busch Boulevard
 - My school is on or near Busch Boulevard
 Which school? _____
 - I travel on Busch Boulevard but rarely stop
 - I don't regularly use Busch Boulevard
 - Other: _____

4. Which travel option(s) have you used on Busch Boulevard? Please circle all that apply:

- Drive
- Walk
- Bike
- HART Bus
- School Bus
- Share a ride with others (car, pool, boat, etc.)
- Other: _____

5. When driving on Busch Boulevard, rate your satisfaction on the following (please circle 1-5):

Safe	5	4	3	2	1	Unsafe	n/a
Comfortable	5	4	3	2	1	Uncomfortable	n/a
Convenient	5	4	3	2	1	Inconvenient	n/a

6. When walking/biking/riding the bus on Busch Boulevard, rate your satisfaction on the following (please circle 1-5):

Safe	5	4	3	2	1	Unsafe	n/a
Comfortable	5	4	3	2	1	Uncomfortable	n/a
Convenient	5	4	3	2	1	Inconvenient	n/a

7. How important are the following objectives in your vision for Busch Boulevard?

My Vision is for a Busch Boulevard where...	Very Important	Somewhat Important	Not Important	No Opinion
Transit users have shade and comfortable amenities.				
A continuous sidewalk runs the length of the corridor				
Bicycle connectivity is enhanced to adjacent homes and businesses.				
Safe and frequent options are provided for pedestrians and bicyclists to cross the corridor				
Aesthetic enhancements provide shade and add beauty to the corridor				
The corridor adds a unique and inviting character to the community				
Existing businesses are preserved along the corridor				
The roadway attracts reinvestment into the corridor and surrounding areas				
Travelers experience efficient and reliable travel times				
Facilities are ADA compliant				
Citizens with limited mobility have enough time to cross the street				
Congestion is reduced on the corridor				
Crashes occur less frequently and are not as severe				
Drivers slow down to a safe speed				
Increased lighting improves visibility for drivers and pedestrians				
Children can travel to and from school safely, regardless of mode of travel				
Consolidated driveways reduce conflict points for vehicles and pedestrians				

8. Which of the following potential improvements would you support?

	Strongly Agree	Agree	Neutral	Disagree	Strongly Disagree
Add additional travel lanes					
Create a pedestrian on-ramp phase at key intersections					
Improve traffic signal timing					
Add on-street lighting					
Add mid-block crosswalks					
Landscaping, pedestrian lighting, etc.					
Lower speed limit					
Create a wide sidewalk along the entire corridor					
Add on-street bike lanes					
Add buffered bike lanes					
Add dedicated bus lane					
Add school zone					
Fill in sidewalk gaps					
Add bus shelters					
Add a landscaped median					

9. Any other comments and suggestions:

Thank you for taking the time to complete our User Preference Survey. Your valuable input will help inform the identification of challenges/issues and the development of potential solutions.

Next Steps



Next Steps - Schedule

- Define a corridor vision, goals and objectives
- Identify alternative near-term and long-term solutions to meet the identified goals and objectives
- Alternatives Public Meeting Spring 2018

SR 580/Busch Boulevard from North Dale Mabry Highway to North Nebraska Avenue Corridor Study

PUBLIC INVOLVEMENT PLAN SCHEDULE

ACTIVITY	2017												2018							
	Mar	Apr	May	June	July	Aug	Sept	Oct	Nov	Dec	Jan	Feb	Mar	April	May	June	July	Aug	Sept	
Notice to Proceed - Project Begins	★																			
Develop Public Involvement Plan																				
Mailing List																				
Elected Officials/Agency Kick-off Meeting				★																
Project Advisory Group (PAG) Meetings																				
Corridor Visioning Workshop																				
Alternatives Public Meeting																				
Final Corridor Alternatives & Strategies Report																				

Next Steps – Stay Connected

- Complete a **comment form**
- Submit a comment on our **website**
- **Email** your comments
- **Mail** written comments
- Take the **survey**

Brian Shroyer
*Project Manager for
Florida Department of Transportation*
11201 N. McKinley Drive
Tampa, FL 33612
Phone: (813) 975-6444
Email: Brian.Shroyer@dot.state.fl.us

Kristen Carson
*Public Involvement Officer
Florida Department of Transportation*
11201 N. McKinley Drive
Tampa, FL 33612
Phone: (813) 975-6202 or (800) 226-7220
Email: Kristen.Carson@dot.state.fl.us

www.fdotd7studies.com/westbuschblvd/

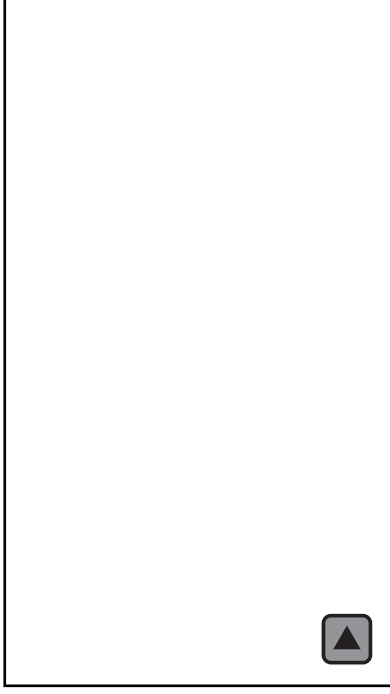
E-797



Thank you for attending!



Next Show Starts in:



APPENDIX E – Sign-in Sheets



CORRIDOR VISIONING WORKSHOP
West Busch Boulevard (SR 580) Corridor Study
 From North Dale Mabry Highway to North Nebraska Avenue
 Financial Project ID Number: 435908-1-22-01



Thursday, November 16, 2017 from 5:30 pm to 7:30 pm

Christian Family Church, 3457 W. Kenyon Avenue, Tampa, Florida 33614.

Sign-In Sheet

NAME (PLEASE PRINT)	AGENCY / ORGANIZATION	TITLE	PHONE	EMAIL
Jessica Francois	QUA		850 380 0079	Jessica.Francois@ch2m.com
Joe Belt	BEST ENGINEERING			
Bill Besseleur			813-263-5854	BESSELEUR@GARTL.COM
RAY CHIARAMONTE	TBARIA			
Karen Harrill	QCA		352-257-1651	Karen.Harrill@QCAUSA.com
Jennifer Bartlett				Jennifer.Bartlett@ch2m.com
Carol Barker	CH2M		407-650-2173	carol.barker@ch2m.com
David Hernandez	CH2M			David.Hernandez@ch2m.com
Chad Polk	CH2M		813-281-7912	chad.polk@ch2m.com
Jenn Rhodes	CH2M			jenn.rhodes@ch2m.com
Mark Jennings	CH2M			Mark.Jennings@ch2m.com
Cris Scheckley	CH2M			cris.scheckley@ch2m.com

APPENDIX F – Handouts

Meeting Agenda

Newsletter #1

User Preference Survey



PUBLIC VISIONING WORKSHOP

AGENDA

West Busch Boulevard (SR 580) Corridor Study

from North Dale Mabry Highway to North Nebraska Avenue

Hillsborough County, Florida

FDOT Financial Project ID Number: 435908-1-22-01

Christian Family Church

November 16, 2017 – 5:30 pm to 7:00 pm

This Visioning Workshop is organized as an interactive open house. The opportunities for you to provide your input are outlined below. Project team staff are available to answer your questions, just look for the staff with nametags.

1. Power Point presentation to introduce the Corridor Study
2. Existing Conditions activity – provide input on issues in the corridor
3. Information on transportation characteristics of the corridor
4. Visioning Exercise – help define the project goals and objectives
5. Potential Improvement Exercise – identify important improvement opportunities
6. Provide feedback to the study team – comments, user preference survey

THANK YOU FOR YOUR PARTICIPATION TONIGHT

West Busch Boulevard (SR 580) Corridor Study

From North Dale Mabry Highway to North Nebraska Avenue

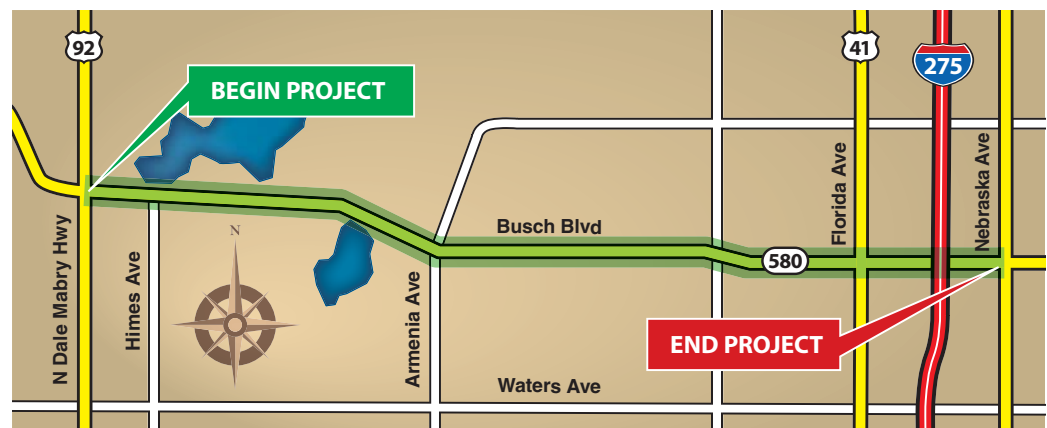
Hillsborough County, FL

Financial Project ID Number: 435908-1-22-01

Corridor Study Begins!

The Florida Department of Transportation (FDOT), District 7 is conducting a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue in Hillsborough County – a distance of approximately 3.3 miles. The West Busch Boulevard (SR 580) corridor is an east-west urban principal arterial connecting and serving local commuters and visitors between North Dale Mabry Highway and Nebraska Avenue. The corridor study area includes portions of unincorporated Hillsborough County and the City of Tampa and is the main route to the Busch Gardens Amusement Park (located approximately 2.15 miles east of the project limits at McKinley Drive).

The corridor supports a variety of land uses and demographics, including schools, churches, large and small businesses with direct access to and from SR 580, transit services, pedestrians, bicyclists and many small communities. The corridor also serves a large commuter population as a connection between Veterans Expressway to the west and I-75 to the east.



The West Busch Boulevard (SR 580) corridor study will use FDOT's Complete Streets policy to showcase how Complete Streets approaches can shape a corridor. The study, which began in March 2017, is expected to continue for approximately 18 months and will include technical work and data analysis, along with various opportunities for the public to be involved in discussing a vision for West Busch Boulevard (SR 580) that makes it accessible for all users.

What is a Corridor Study?

A corridor study is the first step in planning by defining the corridor's needs. A corridor study generally defines the relationships between a roadway and its adjacent land. Corridor studies are also used to: define acceptable levels of access and mobility; determine transportation system needs to support surrounding land uses, and identify operational deficiencies, among other things. This corridor study will include establishing a shared vision that will allow the community to help set project goals and objectives. The vision will guide the project team to develop a set of improvements that leverage local resources, match local conditions and desires, and are consistent with ongoing planning and development efforts.

Project Schedule

The West Busch Boulevard (SR 580) Corridor Study began in March 2017 and is anticipated to be completed in 18 months. Project activities will include:

- Identifying the travel related needs and issues within the West Busch Boulevard project corridor.
- Coordinating with key stakeholders to establish the vision for the corridor and identify solutions to the corridor issues and improve operational efficiency within the West Busch Boulevard corridor.
- Document the vision for all future improvements within the West Busch Boulevard corridor to maximize the “livability” for all users.

ACTIVITY	2017												2018						
	Mar	Apr	May	June	July	Aug	Sept	Oct	Nov	Dec	Jan	Feb	Mar	April	May	June	July	Aug	Sept
Notice to Proceed - Project Begins	★																		
Develop Public Involvement Plan																			
Mailing List																			
Elected Officials/Agency Kick-off Meeting				★															
Project Advisory Group (PAG) Meetings																			
Corridor Visioning Workshop									★										
Alternatives Public Meeting													★						
Final Corridor Alternatives & Strategies Report																			

Project Advisory Group (PAG)

A Project Advisory Group (PAG), composed of local government and agency representatives, are working with FDOT to identify corridor challenges along with potential solutions, assist with documenting the community goals, and develop a vision for the corridor that will guide the study development and future improvements. The PAG will meet at key points throughout the study to collaborate with the project team to ensure the vision and improvement alternatives are well-coordinated with local planning efforts, adjacent projects, and community goals.

How Can You Get Involved?

The project website is your best source for up-to-date information; you can get access at this link:

www.fdotd7studies.com/westbuschblvd/. In addition to the general study information that is already posted on the website, the project documents will be uploaded to this site as they are developed. However, you can already begin providing your input on this project using WikiMapping! This tool allows you to “pin” comments at specific locations along the corridor. You can pick the type of comment you have to help us understand the issues you observe in the corridor. Some of these issue types include: Bus/Transit, Connectivity, Intersection Issue, Pedestrian, Safety, and Traffic/Parking. If you have a comment on an issue that does not have a specific description, just use the “Other” category. The link to the WikiMapping tool is provided on the project website, but for direct access you can click on the following link:

<http://wikimapping.com/wikimap/West-Busch-Boulevard-Corridor-Study.html>. The project team will utilize this information to help inform the process and guide solutions.

Future opportunities for you to get involved include the upcoming Corridor Visioning Workshop that will be conducted in the Fall 2017. At this workshop, we will gather your input on the existing corridor and document your ideas to resolve some of the issues that you have observed. Our goal is to enlist your help to identify improvements that can be implemented in the West Busch Boulevard corridor to resolve the challenges. In advance of this workshop, a User Preference Survey will be available on the website in October; hard copies of this same survey will be available at the Corridor Visioning Workshop. These surveys will be used by the project team to understand the issues and concerns that you have regarding the corridor.

We want your input! We urge anyone interested in shaping the future of the West Busch Boulevard corridor to actively participate by visiting the website, get informed, document issues in the WikiMapping tool, attend meetings, and provide your ideas.

For more information or to comment, please contact:

Brian Shroyer, Project Manager
11201 N. McKinley Drive
Tampa, FL 33612
Office Phone: (813) 975-6449
brian.shroyer@dot.state.fl.us

Kristen Carson, Public Information Officer
11201 N. McKinley Drive
Tampa, FL 33612
Office Phone: (813) 975-6202 or 800-226-7220
kristen.carson@dot.state.fl.us

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT.

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220.

Comuníquese Con Nosotros

Nos importa mucho la opinión del público sobre el proyecto. Si usted tiene preguntas o comentarios, o simplemente desea más información, por favor comuníquese con nosotros. Nuestro representante, Señora Lilliam Escalera, 813-975-6445, Departamento de Transportación de Florida, 11201 N. McKinley Dr., Tampa, FL 33612, lilliam.escalera@dot.state.fl.us.

Please fill out this survey as best as you can and place in the Survey Forms box.

1. Home Zip code: _____

2. Work/School zip code: _____

3. Please **circle** all that apply:

- | | |
|---|--|
| a. I live on or near Busch Boulevard | e. I travel on Busch Boulevard but rarely stop |
| b. I work on or near Busch Boulevard | f. I don't regularly use Busch Boulevard |
| c. I shop/visit businesses on or near Busch Boulevard | g. Other: _____ |
| d. My school is on or near Busch Boulevard | _____ |
| Which school? _____ | |

4. Which travel option(s) have you used on Busch Boulevard? Please **circle** all that apply:

- | | |
|-------------|--|
| a. Drive | e. School Bus |
| b. Walk | f. Share a ride with others (car pool, taxi, etc.) |
| c. Bike | g. Other: _____ |
| d. HART Bus | _____ |

5. When **driving** on Busch Boulevard, rate your satisfaction on the following (**please circle 1 – 5**):

Safe	5	4	3	2	1	Unsafe	n/a
Comfortable	5	4	3	2	1	Uncomfortable	n/a
Convenient	5	4	3	2	1	Inconvenient	n/a

Additional Comments: _____

6. When **walking/biking/riding** the bus on Busch Boulevard, rate your satisfaction on the following (**please circle 1 – 5**):

Safe	5	4	3	2	1	Unsafe	n/a
Comfortable	5	4	3	2	1	Uncomfortable	n/a
Convenient	5	4	3	2	1	Inconvenient	n/a

Additional Comments: _____

7. How important are the following objectives in your vision for Busch Boulevard?

My Vision is for a Busch Boulevard where...	Very Important	Somewhat Important	Not Important	No Opinion
Transit users have shade and comfortable amenities				
A continuous sidewalk runs the length of the corridor				
Bicycle connectivity is enhanced to adjacent homes and businesses				
Safe and frequent options are provided for pedestrians and bicyclists to cross the corridor				
Aesthetic enhancements provide shade and adds beauty to the corridor				
The corridor adds a unique and inviting character to the community				
Existing businesses are preserved along the corridor				
The roadway attracts reinvestment into the corridor and surrounding areas				
Travelers experience efficient and reliable travel times				
Facilities are ADA compliant				
Citizens with limited mobility have enough time to cross the street				
Congestion is reduced on the corridor				
Crashes occur less frequently and are not as severe				
Drivers slow down to a safe speed				
Increased lighting improves visibility for drivers and pedestrians				
Children can travel to and from school safely, regardless of mode of travel				
Consolidated driveways reduce conflict points for vehicles and pedestrians				

Additional Comments:

8. Which of the following potential improvements would you support?

	Strongly Agree	Agree	Neutral	Disagree	Strongly Disagree
Add additional travel lanes					
Create a pedestrian-only phase at key intersections					
Improve traffic signal timing					
Add on-street parking					
Add mid-block crosswalks					
Landscaping, pedestrian lighting, etc.					
Lower speed limit					
Create a wide sidewalk along the entire corridor					
Add on-street bike lanes					
Add buffered bike lanes					
Add dedicated bus lane					
Add school zone					
Fill in sidewalk gaps					
Add bus shelters					
Add a landscaped median					

Additional Comments:

9. Any other comments and suggestions:

Thank you for taking the time to complete our User Preference Survey. Your valuable input will help inform the identification of challenges/issues and the development of potential solutions.

APPENDIX G – Public Comments



CORRIDOR VISIONING WORKSHOP
West Busch Boulevard (SR 580) Corridor Study
 From North Dale Mabry Highway to North Nebraska Avenue
 Financial Project ID Number: 435908-1-22-01



Thursday, November 16, 2017 Christian Family Church, 3457 W Kenyon Ave, Tampa, Florida 33614

Please provide your comments below. If more space is needed, please use the back of this sheet or an additional sheet of paper. You may place your comments in the "Comment Box" provided at the meeting, or send to Brian Shroyer, Project Manager at the address on the back of this comment form, by November 27, 2017. Emailed comments are also acceptable to Brian.Shroyer@dot.state.fl.us

*All comments are part of the project record and are available for review by the public and the media.

LEAVE PLENTY OF ROOM FOR TRUCK DELIVERY
 TO BUSINESSES ALONG BUSCH BLVD. SAFETY
 BEING MAIN CONCERN.

CONTACT INFORMATION (PLEASE PRINT)

Name COLIN GRIMES
 Title LANDOWNER
 Agency / Organization TERRACE CARPETS
 Phone Number 813/711-3169
 Email TERRACECARPETS@VERIZON.NET

PLEASE RETURN COMMENTS BY NOV 27, 2017 TO:

Brian L. Shroyer
 Multimodal Project Manager
 Florida Department of Transportation
 11201 N. McKinley Drive, MS 7-500
 Tampa, FL 33612
 Brian.Shroyer@dot.state.fl.us

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status.

If you continue to issue Licenses to people who come to this country from countries where they do not drive -- you can't solve road problems

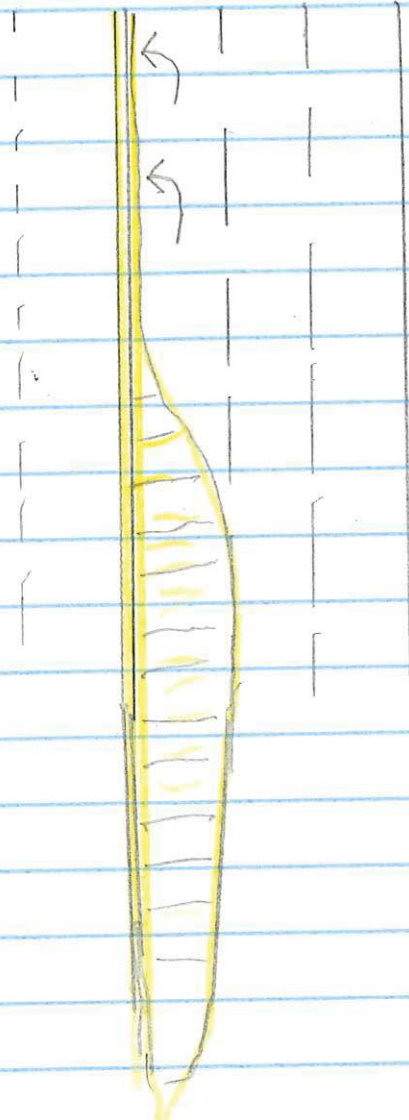
- 1) people don't use their blinkers
- 2) people don't use their mirrors
- 3) people don't know our cultures
- 4) slow drives using fast lane
- 5) temp-licenses should be issued until they come back and take another test



Thomas Malouf
3115 Mossvale Ln.
Tampa, FL 33618

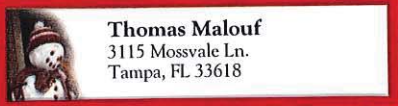
813-453-8204 E-812

Roadway



Yellow dead zone should be eliminated to make room for more cars turning

- A) Busch Blvd & Himes
- B) Florida Ave & Busch Blvd



Thomas Malouf
3115 Mossvale Ln.
Tampa, FL 33618

E-813

813. 453. 8204

Sidewalks

- 1) Complete sidewalk south-side Busch Blvd.
between N. Blvd + Armenia
- 2) make sidewalk south side of Busch Blvd
a BIKE path --
- 3) Bikes should not be on road
(Not enough room)

Rail Road Right-A-WAY

The R/R company should be responsible
for cleaning their property at least
twice @ year



Section 4 Alternatives Public Meeting

ALTERNATIVES PUBLIC MEETING MEETINGS SUMMARY – JULY 10, 2018

An alternatives public meeting was held Tuesday, July 10, 2018 from 5:30 p.m. to 7:30 p.m. at the Tampa First Seventh-Day Adventist Church (822 West Linebaugh Avenue, Tampa, Florida 33612). This meeting was the second of two public meetings scheduled to take place during the corridor study.

Public Notification

Meeting invitations were sent by e-mail to 33 elected officials and 115 appointed officials/interested parties. Meeting postcard invitations were sent by mail to 10 Native American Tribe contacts, 1259 property owners and 642 current residents. Meeting flier invitations were distributed along the project corridor to locations where a large number of people gather. The invitations included dates, times and locations for the alternatives public meeting. Copies of the meeting invitations are included in Appendix (A).



Media Notification

An alternatives public meeting notification was published in the Florida Administrative Register (FAR) on June 26, 2018 in Volume 44/124. The meeting notification was posted to the FDOT Public Notices website and the project website. A press release was distributed by the Florida Department of Transportation (FDOT) to major local media outlets on July 3, 2018. Copies of the Florida Administrative Weekly Register, FDOT Public Notices and the press release are included in Appendix (B).

Public Meeting Agenda

The meeting was conducted in an interactive open house format utilizing seven (7) stations. The public was invited to attend at any time between 5:30 p.m. and 7:30 p.m. Parking was available to accommodate all of the attendees, including the disabled. Signs were placed at the major intersecting roads to direct attendees to the meeting location and from the parking lot to the meeting room.



See copies of the display boards that were available for public review in Appendix (C). Meeting attendees were asked to watch a short project introduction video (see attached in Appendix C) and visit the following seven (7) stations:

Station 1- Existing Conditions

Meeting attendees viewed the following boards: Study Area, Existing Typical Sections, Traffic Level of Service Map, Multimodal Map, and Crash Locations 2011-2015.

Station 2- The Future Vision

Meeting attendees viewed the following boards: Developing the Corridor Vision (Prior Feedback), Potential Improvements (Prior Feedback), and Corridor Vision “How Do We Get There”.



Station 3- Mid-Term Improvements

The following Mid-Term Improvements boards were on display: Mid-Term Improvements (3 Plots). A Smart Board was available for meeting attendees to view and leave comments regarding the Mid-Term Improvements. Smart Board comments can be found in Appendix (D).

Station 4- Long-Term Improvements

The following Long-Term Improvements boards were on display: No-Build Typical Section Alternative (Existing Typical Sections), 4-Lane Typical Section, 5-lane Typical Section, and 6-Lane Typical Section. Concept plans for the Long-Term Improvement Alternatives (No-Build, 4-Lane, 5-Lane, and 6-Lane) were also on display (3 plots). This station also included the Future Traffic Level of Service Maps, and the Evaluation Matrix. A Smart Board was available for meeting attendees to view and leave comments regarding the Long-Term Improvements. Smart Board comments can be found in Appendix (D).

Station 5- Supporting Recommendations

Meeting attendees viewed the following boards: Benefits of a Lower Speed and Supporting Recommendations.

Station 6- Interactive Exercises

Attendees participated in two Long Term Improvements Exercises. First, they were given the choice of 4 emoticon stickers and asked to use their stickers to represent their support of each of the typical section options. Attendees also participated in the “Alternatives by Segment” exercise that requested additional typical section feedback based on the context classification segments within the corridor. The puzzle piece that reflected their preferred typical section option for each segment was placed on the board and recorded in a matrix. Results of the exercises can be found in Appendix (D).

Station 7- Provide Comments

Meeting attendees received a comment sheet. Attendees were asked to please provide feedback by filling out a comment sheet.

Meeting Attendance

Not including members of the project team, 20 persons signed the sign-in sheet at the corridor visioning workshop. A copy of the sign-in sheets is included in Appendix (E).

Attending were members of the project team from the Florida Department of Transportation (FDOT) District Seven Office and project consultants. Project representatives were present to discuss the project, receive public input, and answer questions.



Meeting Handouts

A meeting agenda was distributed to the attendees. A copy of the meeting handout is included in Appendix (F).

Public Comments

Five comment forms were received at the corridor visioning workshop (see attached in Appendix G). No additional comment forms were received by mail and email. Verbal feedback at the meeting stressed the importance of making improvements, especially completing sidewalk gaps.



END OF MEETING SUMMARY

This meeting summary was prepared by Karen Harrell, Public Involvement Coordinator. If you feel that clarifications are necessary, or if this differs from your understanding, please notify Karen by telephone at 352-257-1651 or by email at Karen.Harrell@qcausa.com within five (5) working days upon receipt of this summary.

APPENDICES

APPENDIX A – Public Notification

Email Invitation to Elected / Appointed Officials
Email Invitation to Agency Partners
Meeting Invitation Postcard to Property Owners
Meeting Invitation Flier

APPENDIX B – Media Notification

Florida Administrative Register Ad
FDOT Public Notices
Press Release

APPENDIX C – Display Boards / Project Video

APPENDIX D – Smart Board Comments / Interactive Exercises

APPENDIX E – Sign-in Sheets

APPENDIX F – Meeting Agenda

APPENDIX G – Public Comments

APPENDIX A – Public Notification

Email Invitation to Elected / Appointed Officials

Meeting Invitation Postcard to Property Owners

Meeting Invitation Flier

Date: June 15, 2018

Subject: Alternatives Public Meeting
West Busch Boulevard (SR 580) Corridor Study
from North Dale Mabry Highway to North Nebraska Avenue
Hillsborough County, Florida
FDOT Financial Project ID Number: 435908-1-22-01

Dear Elected / Appointed Official:

The Florida Department of Transportation (FDOT), District Seven has scheduled an Alternatives Public Meeting for a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The study area includes portions of unincorporated Hillsborough County and the City of Tampa. This notice has been sent to other public officials and agency representatives interested in the project. The Alternatives Public Meeting is scheduled for July 10, 2018, from 5:30 p.m. to 7:30 p.m. at the **Tampa First Seventh-Day Adventist Church, 822 West Linebaugh Avenue, Tampa, Florida 33612.**

The FDOT is looking for ways to improve West Busch Boulevard (SR 580) for pedestrians, bicyclists, transit users and motorists. The Alternatives Public Meeting is an opportunity for the project team to report on the findings from the Project Advisory Group Meetings, Visioning Workshop and to present the initial alternatives under consideration. Attached is an invitation that is being sent to property owners, organizations and individuals interested in the project.

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220 at least seven (7) working days in advance of the meeting.

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT.

If you require further information concerning this project, please contact Brian Shroyer, Florida Department of Transportation Project Manager at 813-975-6449 or by email at Brian.Shroyer@dot.state.fl.us.

Sincerely,
David Gwynn, P.E.
District Seven Secretary
Florida Department of Transportation

Date: June 19, 2018

Subject: Alternatives Public Meeting
West Busch Boulevard (SR 580) Corridor Study
from North Dale Mabry Highway to North Nebraska Avenue
Hillsborough County, Florida
FDOT Financial Project ID Number: 435908-1-22-01

Dear Agency Partner:

The Florida Department of Transportation (FDOT), District Seven has scheduled an Alternatives Public Meeting for a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The study area includes portions of unincorporated Hillsborough County and the City of Tampa. This notice has been sent to other public officials and agency representatives interested in the project. The Alternatives Public Meeting is scheduled for July 10, 2018, from 5:30 p.m. to 7:30 p.m. at the **Tampa First Seventh-Day Adventist Church, 822 West Linebaugh Avenue, Tampa, Florida 33612.**

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Sincerely,
Brian Shroyer
Multimodal Project Manager
Florida Department of Transportation



You're invited to take part in a Florida Department of Transportation (FDOT) Alternatives Public Meeting for a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue in Hillsborough County – a distance of approximately 3.3 miles.

The FDOT is looking for ways to improve West Busch Boulevard (SR 580) for pedestrians, bicyclists, transit users and motorists. At this meeting, we will present the initial alternatives under consideration.

Tuesday, July 10, 2018, 5:30 p.m. to 7:30 p.m.
 Tampa First Seventh-Day Adventist Church
 822 West Linebaugh Avenue, Tampa, FL 33612

For More Information Contact:
 Brian Shroyer, CPM
 Project Manager
 Florida Department of Transportation
 11201 N. McKinley Drive
 Tampa, FL 33612
 813-975-6449
 brian.shroyer@dot.state.fl.us
 Visit Our Website
fdot7studies.com/westbuschblvd/

 A map showing the project corridor for West Busch Boulevard (SR 580) in Hillsborough County, Florida. The map highlights the project area in green, starting at a 'BEGIN PROJECT' marker near Kenyon Ave and ending at an 'END PROJECT' marker near Nebraska Ave. Other streets shown include Linebaugh Ave, North Blvd, Busch Blvd, Waters Ave, Amenia Ave, Himes Ave, and N Dale Mabry Hwy. Highway markers for SR 52, SR 275, and SR 41 are also visible.

Brian Shroyer, CPM
Project Manager
Florida Department of Transportation
11201 North McKinley Drive
Tampa, FL 33612

West Busch Boulevard (SR 580) Corridor Study
From North Dale Mabry Highway to North Nebraska Avenue

Visit Our Website

fdot7studies.com/westbuschblvd/



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Comuníquese Con Nosotros Nos importa mucho la opinión del público sobre el proyecto. Si usted tiene preguntas o comentarios, o si simplemente desea más información, por favor comuníquese con nuestra representante, Lilliam Escalera, 813-975-6403, Departamento de Transportación de Florida, 11201 N. McKinley Dr., Tampa, FL 33612, lilliam.escalera@dot.state.fl.us

West Busch Boulevard (SR 580) Corridor Study

From North Dale Mabry Highway to North Nebraska Avenue

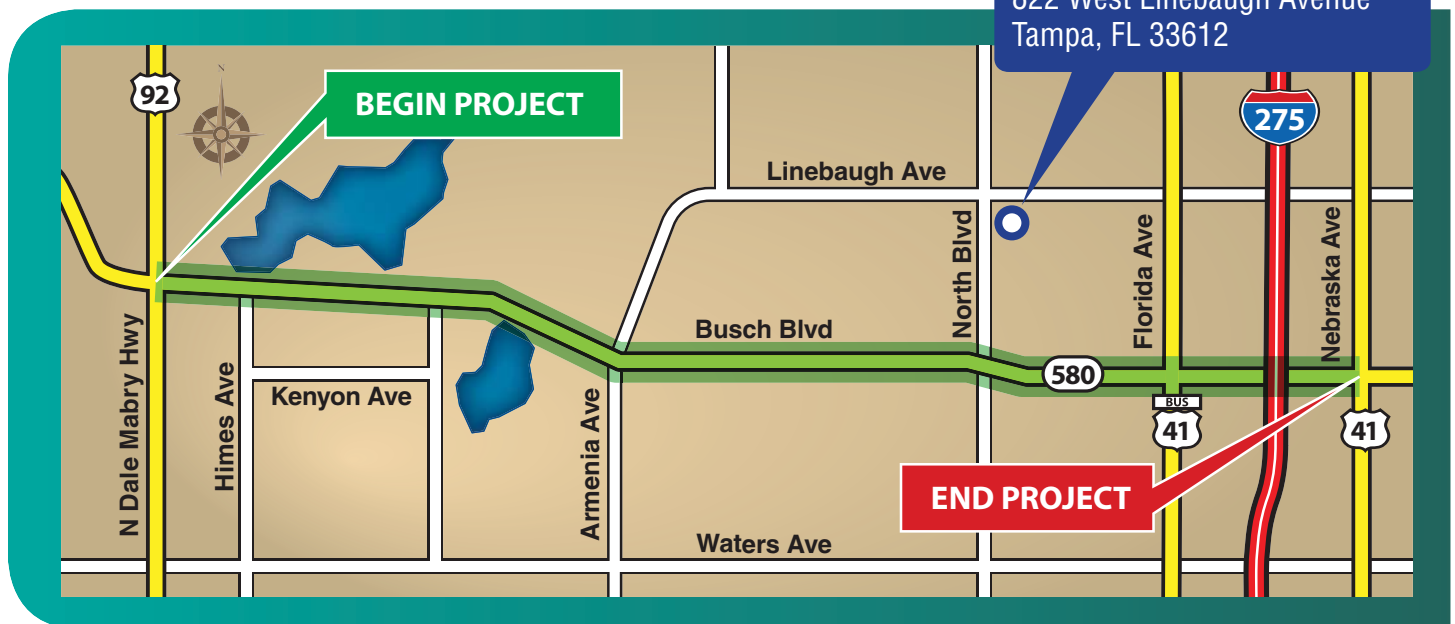
Hillsborough County, FL

Financial Project ID Number: 435908-1-22-01

You're Invited to take part in a Florida Department of Transportation (FDOT) Corridor Alternatives Public Meeting for a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue in Hillsborough County – a distance of approximately 3.3 miles.

The FDOT is looking for ways to improve West Busch Boulevard (SR 580) for pedestrians, bicyclists, transit users and motorists. At this meeting, we will present the initial alternatives under consideration.

Tuesday, July 10, 2018
5:30 p.m. to 7:30 p.m.
 Tampa First Seventh-Day
 Adventist Church
 822 West Linebaugh Avenue
 Tampa, FL 33612



For More Information Contact:

Brian Shroyer, CPM, Project Manager, Florida Department of Transportation
 11201 N. McKinley Drive, Tampa, FL 33612, 813-975-6449, brian.shroyer@dot.state.fl.us

▶ Visit our website: www.fdotd7studies.com/westbuschblvd/

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Comuníquese Con Nosotros Nos importa mucho la opinión del público sobre el proyecto. Si usted tiene preguntas o comentarios, o si simplemente desea más información, por favor comuníquese con nuestra representante, Lilliam Escalera, 813-975-6403, Departamento de Transportación de Florida, 11201 N. McKinley Dr., Tampa, FL 33612, lilliam.escalera@dot.state.fl.us

APPENDIX B – Media Notification

Florida Administrative Register Ad

FDOT Public Notices

Press Release

Notice of Meeting/Workshop

The Florida Department of Transportation, District Seven announces an Alternatives Public Meeting concerning the corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue to which all persons are invited.

DATE AND TIME: Tuesday, July 10, 2018, 5:30 PM to 7:30 PM

PLACE: Tampa First Seventh-Day Adventist Church, 822 West Linebaugh Avenue, Tampa, FL 33612

Financial Project Identification Number: 435908-1-22-01

GENERAL SUBJECT MATTER TO BE CONSIDERED: The Florida Department of Transportation (FDOT), District Seven, is holding an Alternatives Public Meeting for a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The study area includes portions of unincorporated Hillsborough County and the City of Tampa.

The FDOT is looking for ways to improve West Busch Boulevard (SR 580) for pedestrians, bicyclists, transit users and motorists. The Alternatives Public Meeting is an opportunity for the project team to report on the findings from Project Advisory Group Meetings, Visioning Workshop, and present the initial alternatives under consideration.

A copy of the agenda may be obtained by contacting: Mr. Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability, or family status. Pursuant to the provisions of the Americans with Disabilities Act, any person requiring special accommodations to participate in this workshop/meeting, or persons who require translation services (free of charge) are asked to advise the agency at least 7 days before the workshop/meeting by contacting: Stephen Benson, Public Involvement Coordinator, at (813) 975-6427 or (800) 226-7220

Comuniquese Con Nosotros

Nos importa mucho la opinión del público sobre el proyecto. Si usted tiene preguntas o comentarios, o si simplemente desea más información, por favor comuniquese con nuestra representante, Lilliam Escalera, al (813) 975-6403, Departamento de Transportación de Florida, 11201 N. McKinley Dr., Tampa, FL 33612, Lilliam.Escalera@dot.state.fl.us

If you are hearing or speech impaired, please contact the agency using the Florida Relay Service, 1(800)955-8771 (TDD) or 1(800)955-8770 (Voice).

For more information, you may contact: Mr. Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT.

FDOT Public Notices Website:

WEST BUSCH BOULEVARD (SR 580) CORRIDOR STUDY

District: Seven

Meeting Type: Meeting

Date: Tuesday, July 10, 2018

Time: 5:30 PM – 7:30 PM

Location Name: Tampa First Seventh-Day Adventist Church

Street Address: 822 West Linebaugh Avenue

City: Tampa, FL 33612

Purpose: The Florida Department of Transportation (FDOT), District Seven, is holding an Alternatives Public Meeting for a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The study area includes portions of unincorporated Hillsborough County and the City of Tampa.

The FDOT is looking for ways to improve West Busch Boulevard (SR 580) for pedestrians, bicyclists, transit users and motorists. The Alternatives Public Meeting is an opportunity for the project team to report on the findings from Project Advisory Group Meetings, Visioning Workshop and to present the initial alternatives under consideration. A copy of the agenda may be obtained by contacting: Mr. Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us.

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Primary Contact: Brian Shroyer, FDOT Project Manager

Primary Phone: (813) 975-6449

Primary E-mail: Brian.Shroyer@dot.state.fl.us

Additional Contact: Stephen Benson, Public Involvement Coordinator

Additional Phone: (813) 975-6427

Additional E-mail: Stephen.Benson@dot.state.fl.us

West Busch Boulevard (SR 580) Corridor Study **Alternatives Public Meeting Scheduled**

Tampa, FL – The Florida Department of Transportation (FDOT), District Seven, is holding an Alternatives Public Meeting for a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue, a distance of approximately 3.3 miles. The study area includes portions of unincorporated Hillsborough County and the City of Tampa.

The Alternatives Public Meeting is scheduled for July 10, 2018 from 5:30 p.m. to 7:30 p.m. at the Tampa First Seventh-Day Adventist Church, 822 West Linebaugh Avenue, Tampa, Florida 33612.

The FDOT is looking for ways to improve West Busch Boulevard (SR 580) for pedestrians, bicyclists, transit users and motorists. The Alternatives Public Meeting is an opportunity for the project team to report on the findings from Project Advisory Group Meetings, Visioning Workshop and present the initial alternatives under consideration. A copy of the agenda may be obtained by contacting: Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us.

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For more information on this project, please call the FDOT Project Manager, Brian Shroyer, Project Manager, at (813) 975-6449 or via email at Brian.Shroyer@dot.state.fl.us.

###

APPENDIX C – Display Boards / Project Video



Welcome

to the

West Busch Boulevard Study Alternatives Public Meeting



TITLE VI



The proposed project is being developed in accordance with the Civil Rights Act of 1964, as amended. Under Title VI of the Civil Rights Act, public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status.

To express concern(s), please contact the following:

Christopher Speese

Title VI Coordinator | Florida Department of Transportation, District Seven
11201 N. McKinley Drive | Tampa, Florida 33612
Christopher.Speese@dot.state.fl.us
(813) 975-6405

– or –

Jacqueline Paramore

Statewide Title VI Coordinator | Equal Opportunity Office
605 Suwannee Street, Room 260 | Mail Station 65
Tallahassee, Florida 32399-0450
Jacqueline.Paramore@dot.state.fl.us
(850) 414-4753



TITULO VI



Este proyecto se está diseñando de acuerdo con la Ley de Derechos Civiles de 1964 y 1968, según enmendada. Bajo el Título VI de la Ley de Derechos Civiles, la participación pública es solicitada sin distinción de raza, color, origen nacional, edad, sexo, incapacidad o estado familiar.

Para expresar alguna preocupación, comuníquese con:

Lilliam Escalera

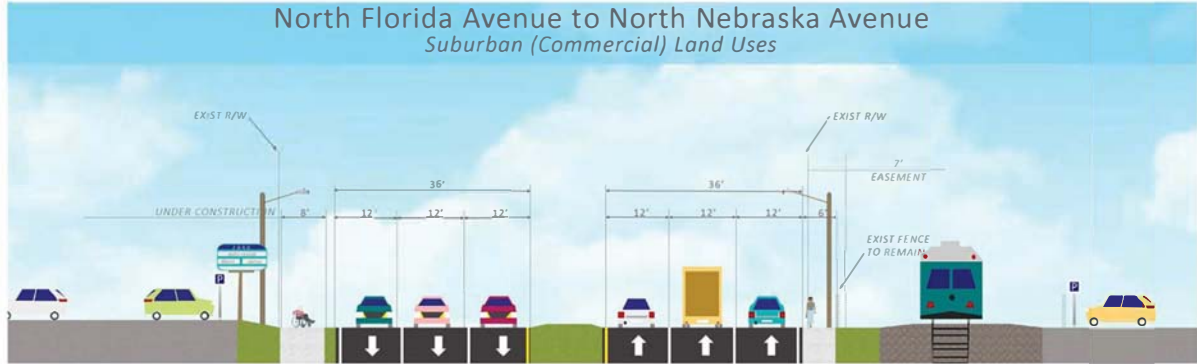
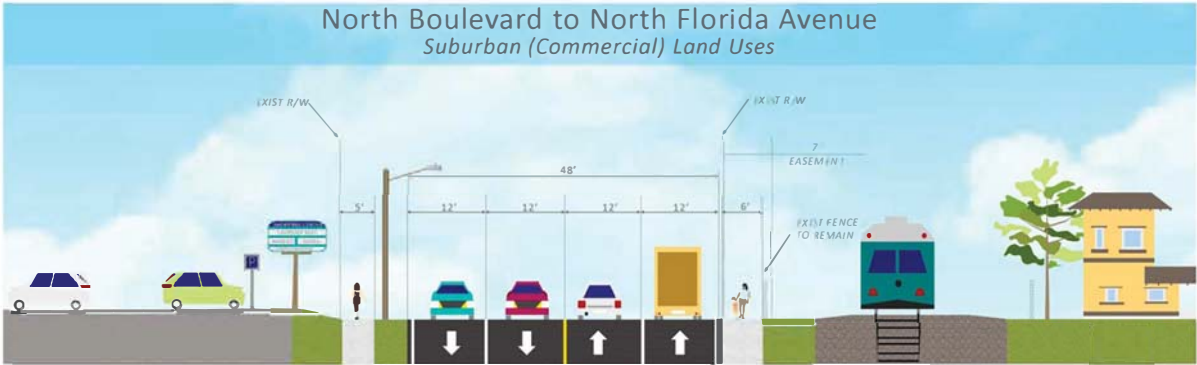
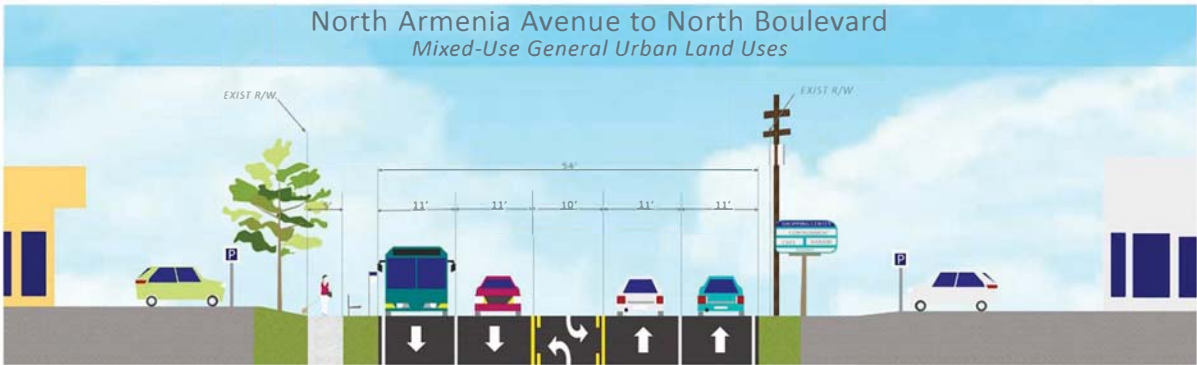
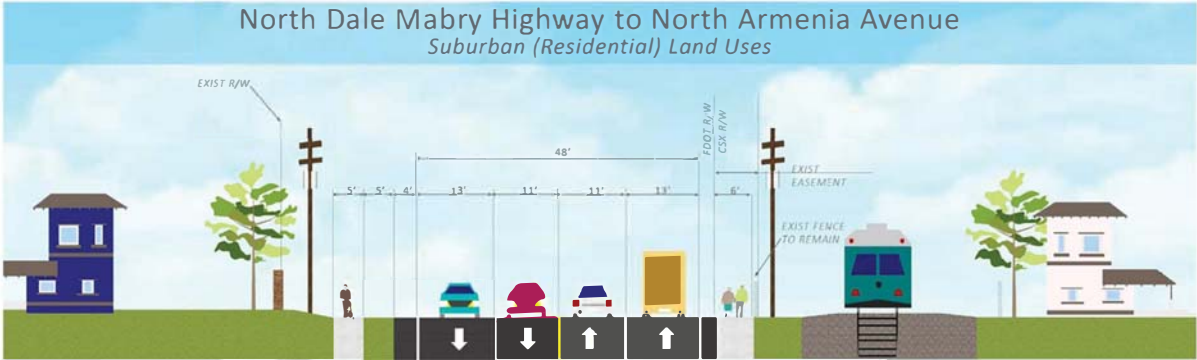
Florida Department of Transportation, District Seven
11201 N. McKinley Drive | Tampa, Florida 33612
Lilliam.Escalera@dot.state.fl.us
(813) 975-6445

– or –

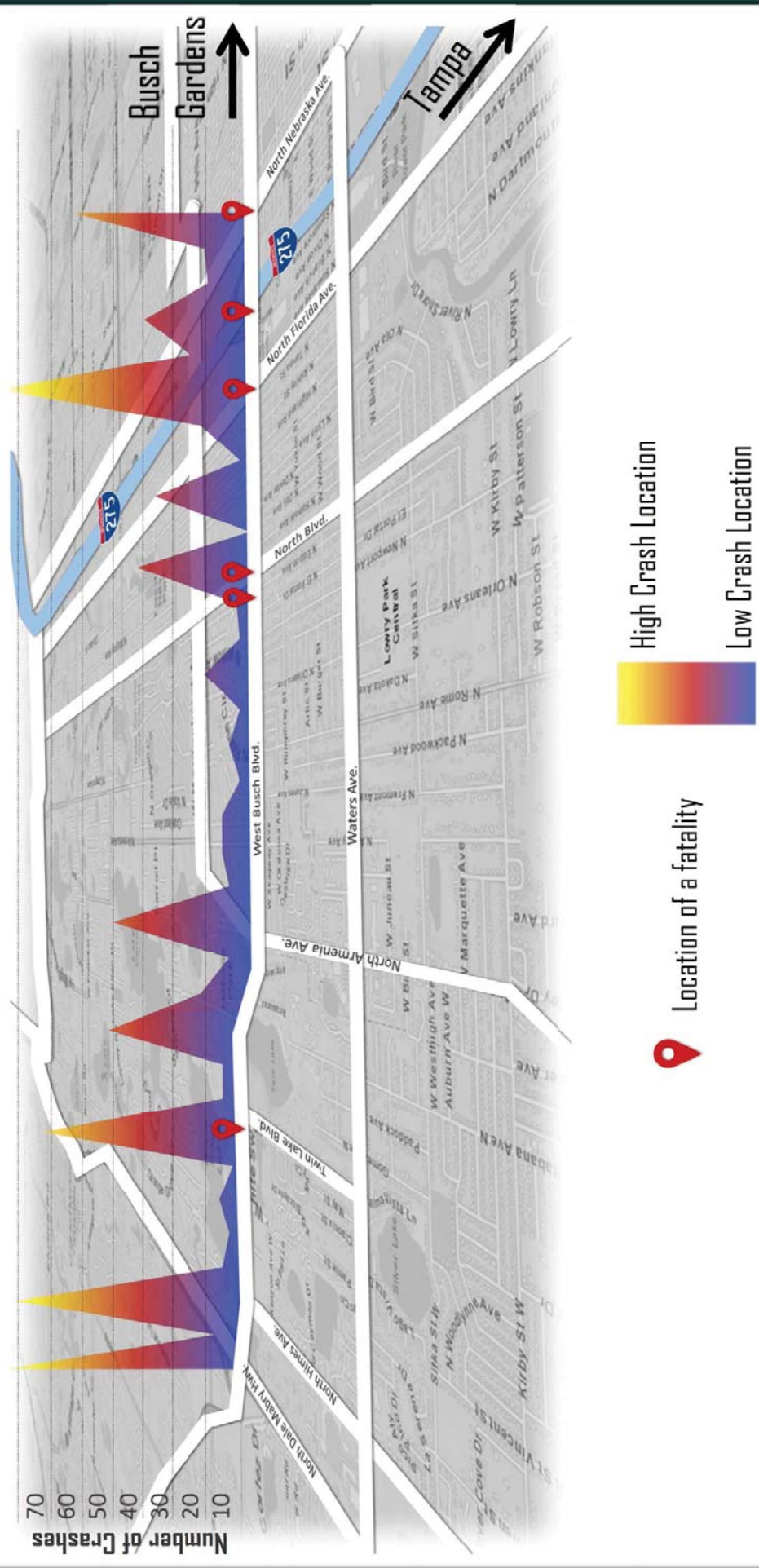
Jacqueline Paramore

Statewide Title VI Coordinator | Equal Opportunity Office
605 Suwannee Street, Room 260 | Mail Station 65
Tallahassee, Florida 32399-0450
Jacqueline.Paramore@dot.state.fl.us
(850) 414-4753

Existing Typical Sections



Crash Locations 2011 through 2015



Location of a fatality

NOTE: NOT TO SCALE
Source: FDOT Crash Analysis Reporting System (CARS)

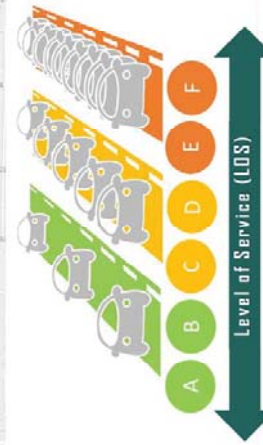
Traffic Level of Service Map



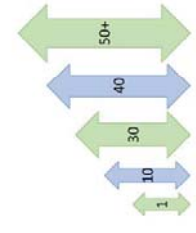
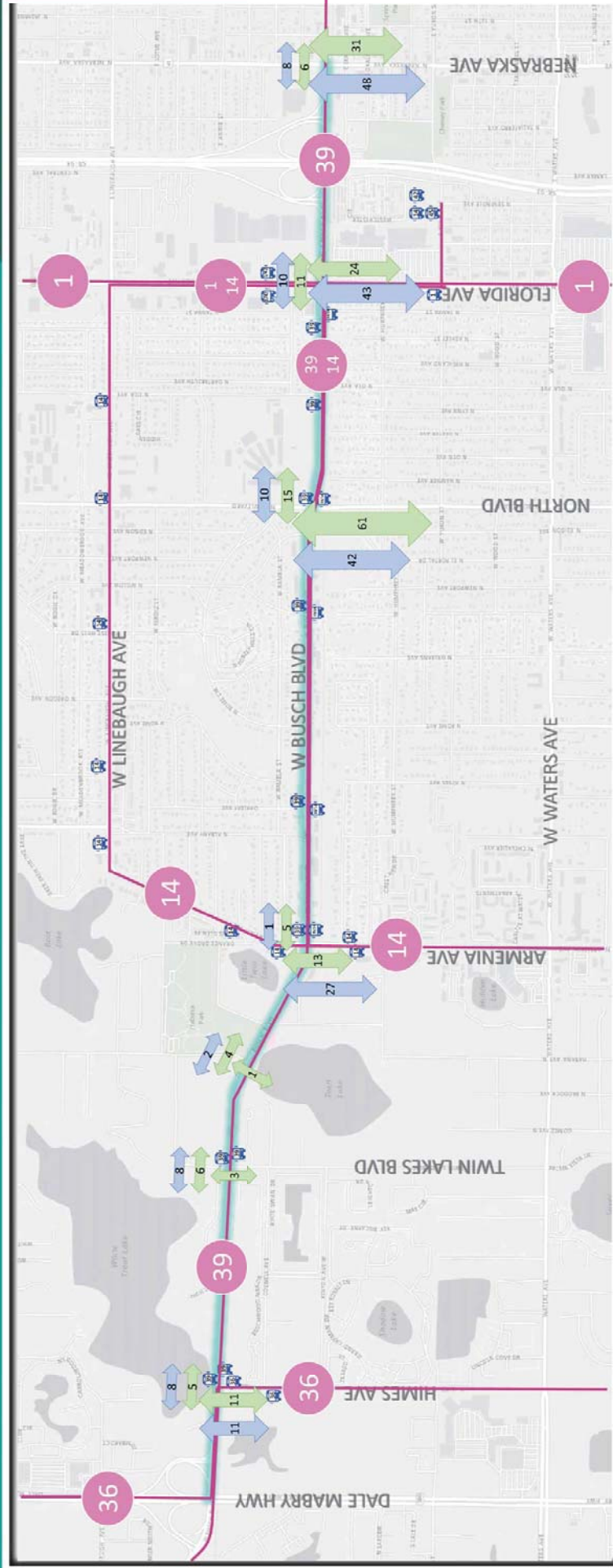
Existing Level of Service (2017)



Future (2040) No-Build Level of Service



Bicycle, Pedestrian, & Transit Map



Increasing Level of Activity*



By Cyclists
(blue)



By Pedestrians
(green)



Transit Stop



Transit Route

* Based on observations from 7 a.m. – 9 a.m. & 4 p.m. – 6 p.m. on Thursday May 4, 2017

Developing the Corridor Vision



Now we want to know what is important to you! Help us shape the future vision of the West Busch Boulevard Corridor. Vote for the goals and objectives presented here, or add in your own!

My Vision is for a West Busch Boulevard where...

ACCESSIBILITY & CONNECTIVITY

- Transit users have shade and comfortable amenities ★
- A continuous sidewalk runs the length of the corridor ★★☆☆
- Bicycle connectivity is enhanced to adjacent homes and businesses ☆
- Safe and frequent options are provided for pedestrians and bicyclists to cross the corridor ★★★☆☆

ECONOMIC DEVELOPMENT

- Aesthetic enhancements provide shade and adds beauty to the corridor ☆
- The corridor adds a unique and inviting character to the community ☆
- Existing businesses are preserved along the corridor ☆★☆☆☆
- The roadway attracts reinvestment into the corridor and surrounding areas ☆☆

MOBILITY

- Travelers experience efficient and reliable travel times: ☆☆☆
- Facilities are ADA compliant
- Congestion is reduced on the corridor ☆☆☆
- Citizens with limited mobility have enough time to cross the street

SAFETY

- Crashes occur less frequently and are not as severe ☆☆☆☆☆
- Drivers slow down to a safe speed ☆☆☆☆☆
- Increased lighting improves visibility for drivers and pedestrians ☆
- Children can travel to and from school safely, regardless of mode of travel ☆
- Consolidated driveways reduce conflict points for vehicles and pedestrians ★★☆☆☆☆

Potential Improvements



The limited available right-of-way along the corridor means that many potential improvements can not be accommodated without impacting adjacent properties. Please help prioritize the list of possible improvements below using the provided stickers to indicate **your top five** desired improvements for the corridor, with **"#1"** being the most preferred improvement.

ADD ADDITIONAL TRAVEL LANES 5 4

CREATE A PEDESTRIAN-ONLY PHASE AT KEY INTERSECTIONS 3 1 2 1 5 3

IMPROVE TRAFFIC SIGNAL TIMING 4 2 3 2 2 3

ADD ON-STREET PARKING

ADD MID-BLOCK CROSSWALKS 4

LANDSCAPING, PEDESTRIAN LIGHTING, ETC. 2 4 5

LOWER SPEED LIMIT 2 1 3 4 4 5 5

CREATE A WIDE SIDEWALK ALONG ENTIRE CORRIDOR 2 4 1 3 2

ADD ON-STREET BIKE LANES

ADD BUFFERED BIKE LANES 1

ADD A DEDICATED BUS LANE

ADD SCHOOL ZONE 1 3

FILL IN SIDEWALK GAPS 3 1 3 1 4

ADD BUS SHELTERS 2 5 2

ADD A LANDSCAPED MEDIAN 5

(WRITE IN ANYTHING ELSE!)

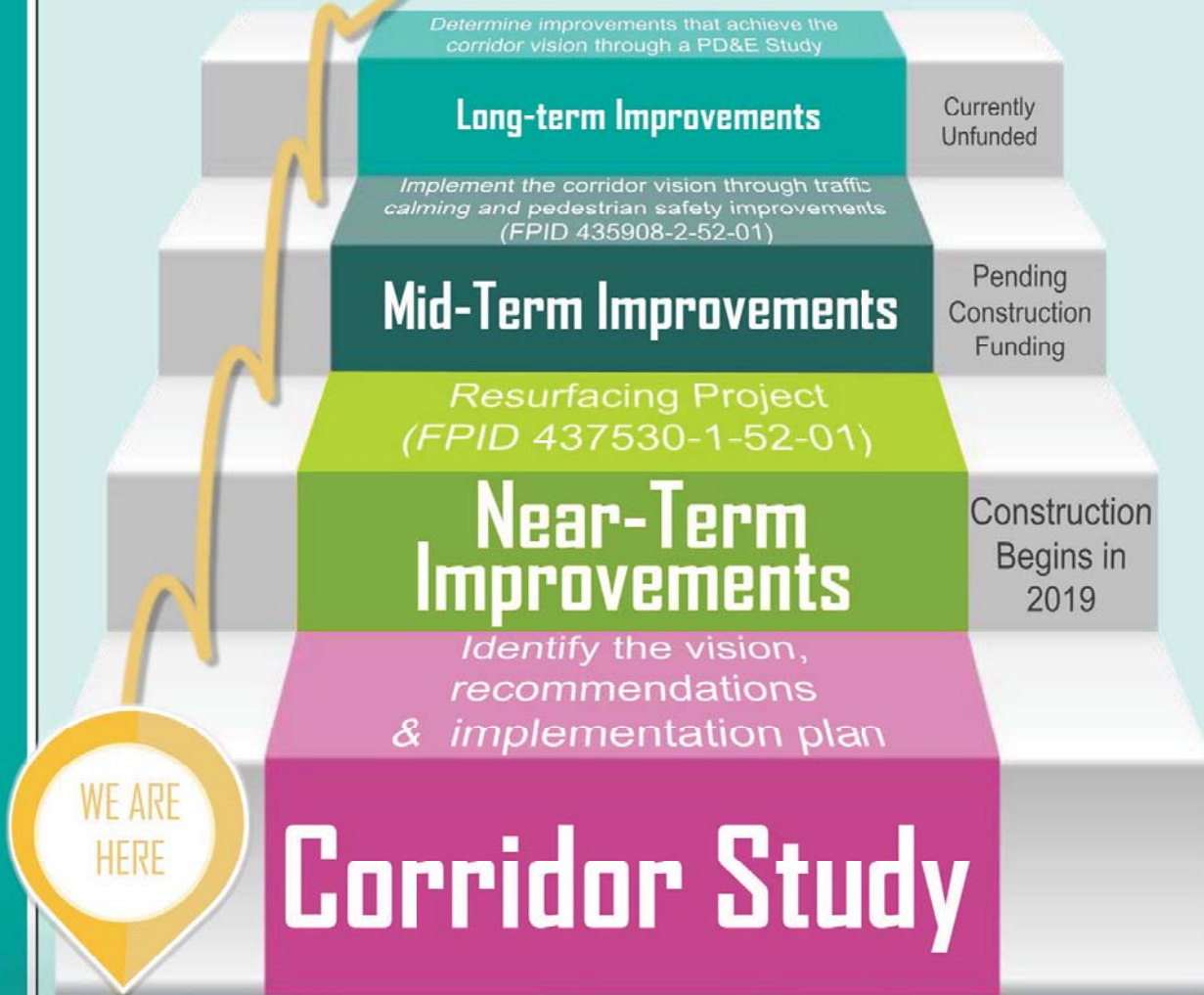
*Indicates improvements within the existing right-of-way



Corridor Vision: How Do We Get There?

“ The Busch Boulevard corridor offers **safe**, comfortable, and convenient access through and **across** the corridor for all users and all travel modes. ”

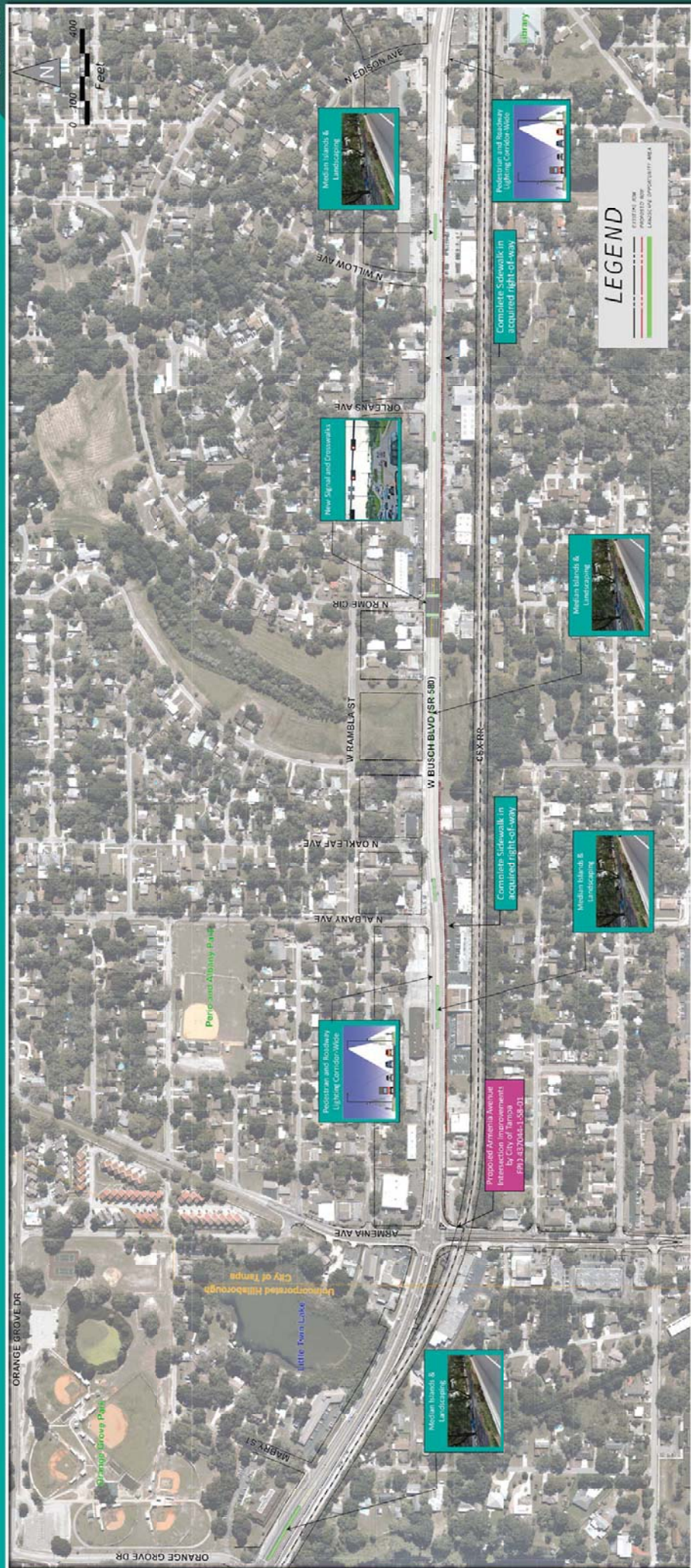
- Corridor Vision Statement



Mid-Term Improvements (1 of 3)

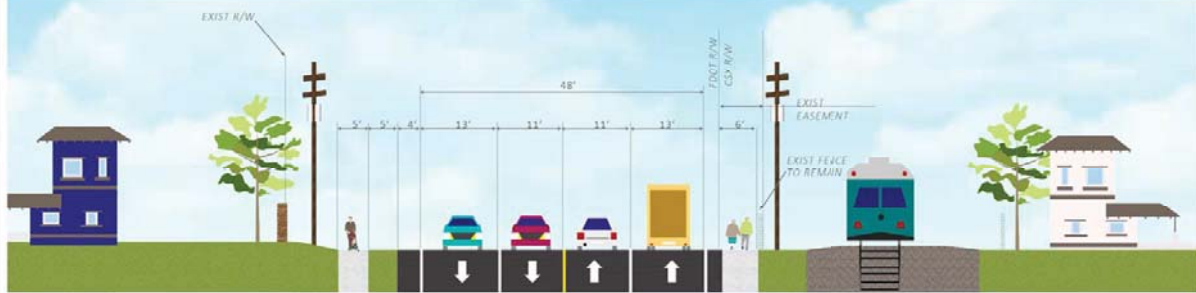


Mid-Term Improvements (2 of 3)

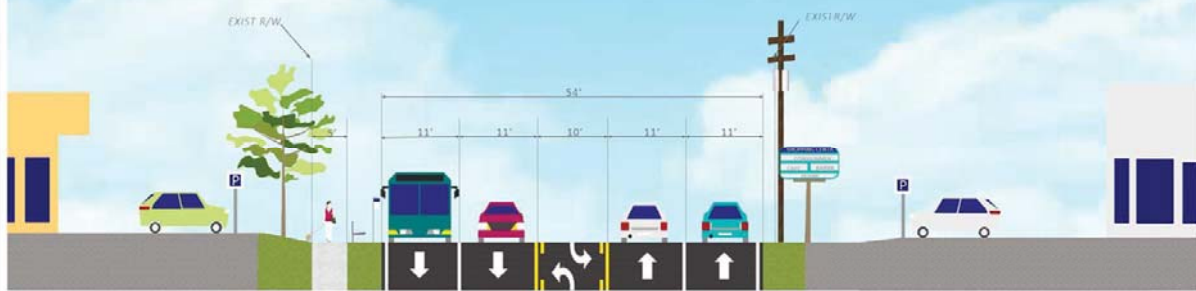


No-Build Alternative (Existing Typical Sections*)

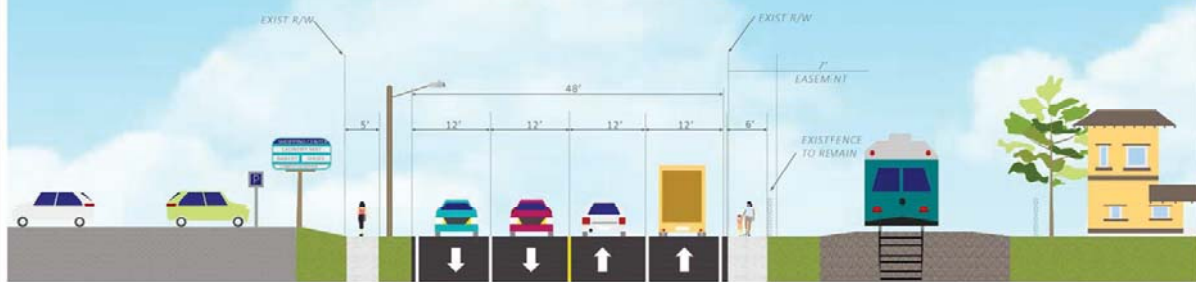
Segment 1: North Dale Mabry Highway to North Armenia Avenue
Suburban (Residential) Land Uses



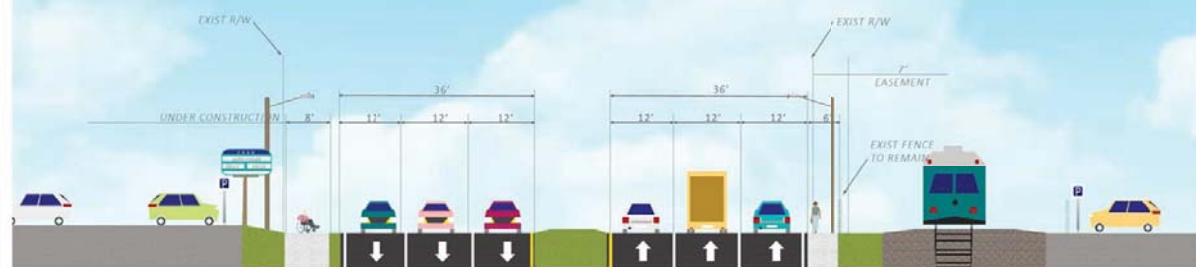
Segment 2: North Armenia Avenue to North Boulevard
Mixed-Use General Urban Land Uses



Segment 3: North Boulevard to North Florida Avenue
Suburban (Commercial) Land Uses



Segment 4: North Florida Avenue to North Nebraska Avenue
Suburban (Commercial) Land Uses



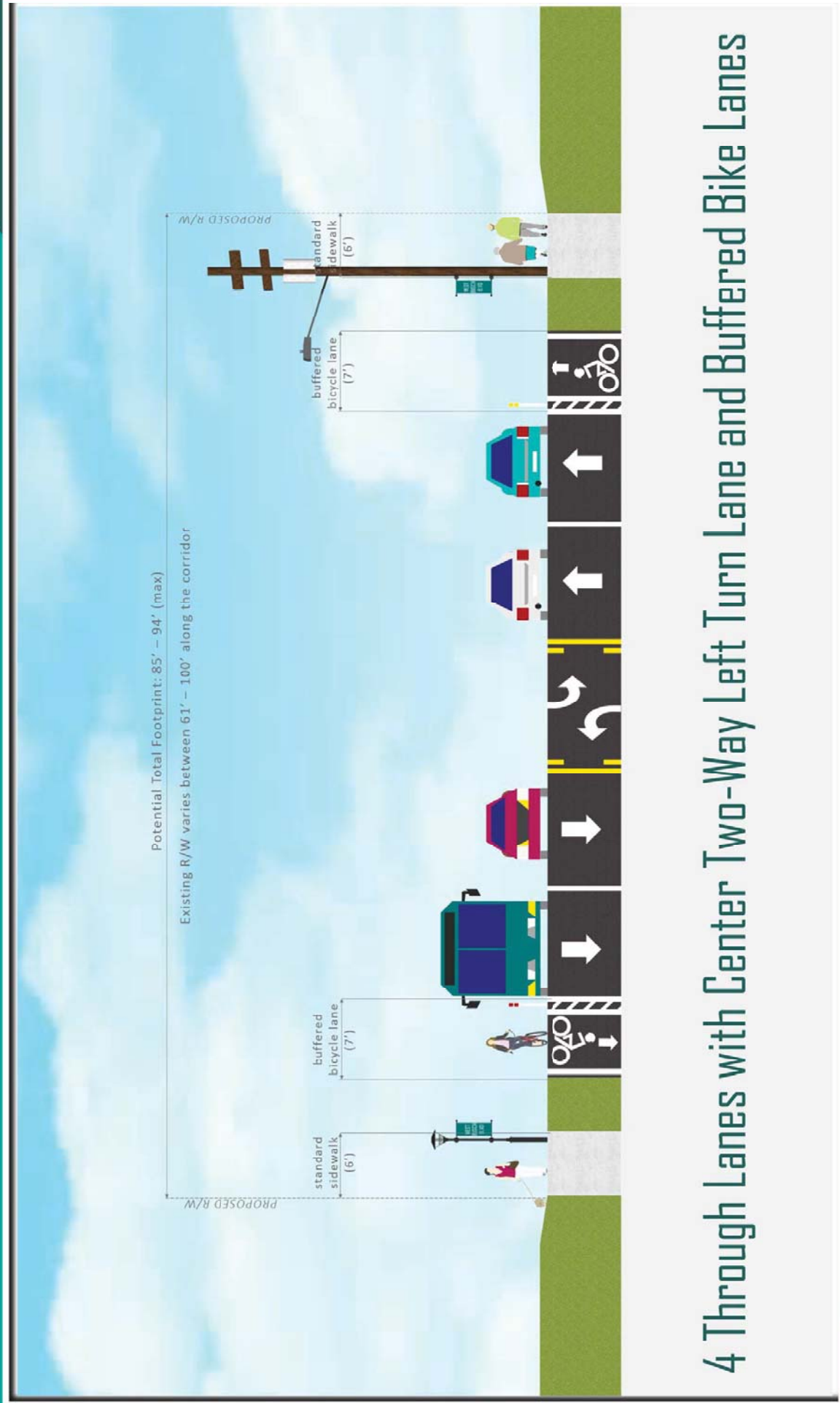
* The No-Build Alternative could include the identified near-term and mid-term improvements

4-Lane Typical Section Alternative



4 Through Lanes with Median and a Wide Sidewalk on the North Side

5-Lane Typical Section Alternative



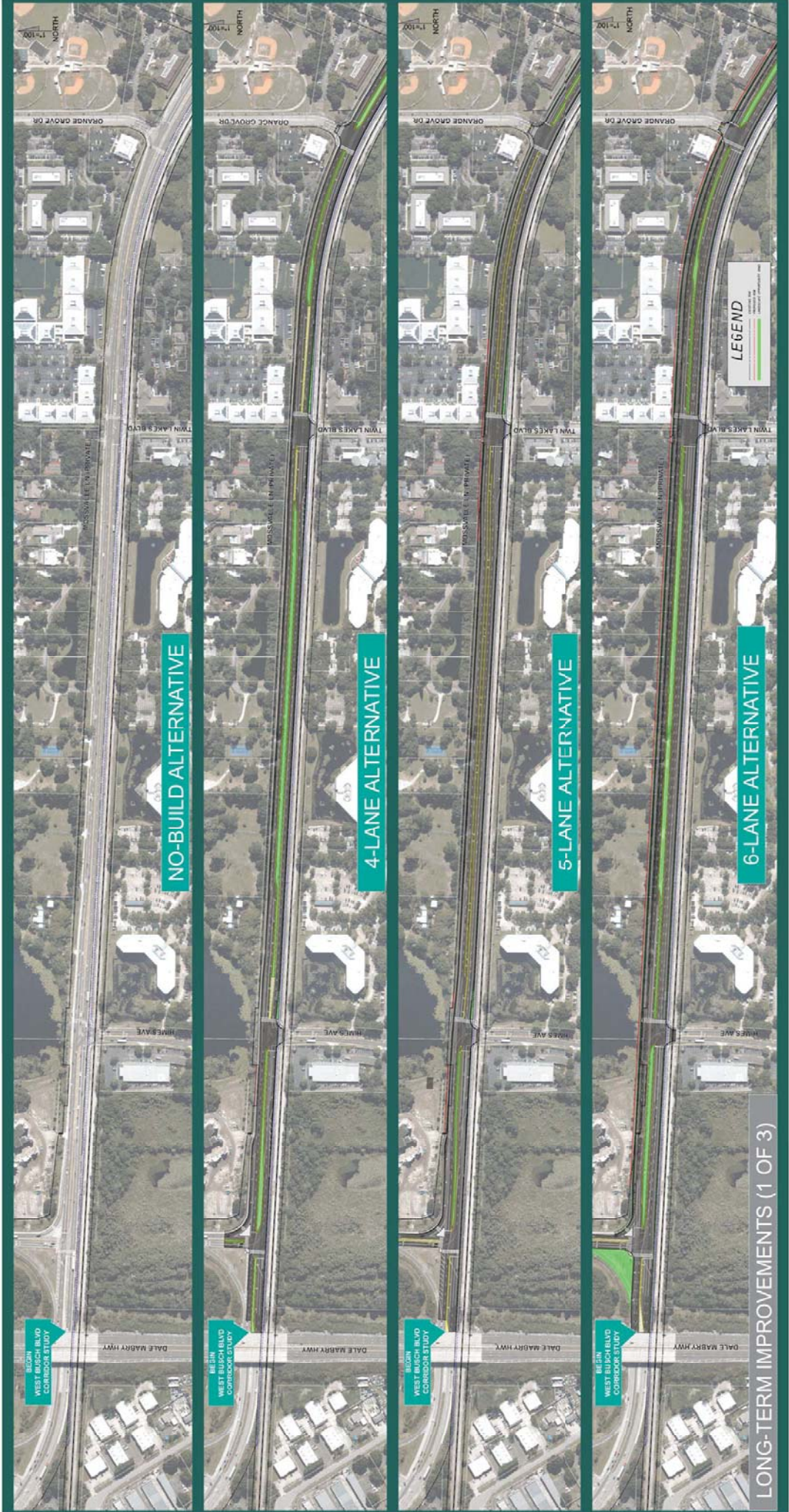
4 Through Lanes with Center Two-Way Left Turn Lane and Buffered Bike Lanes

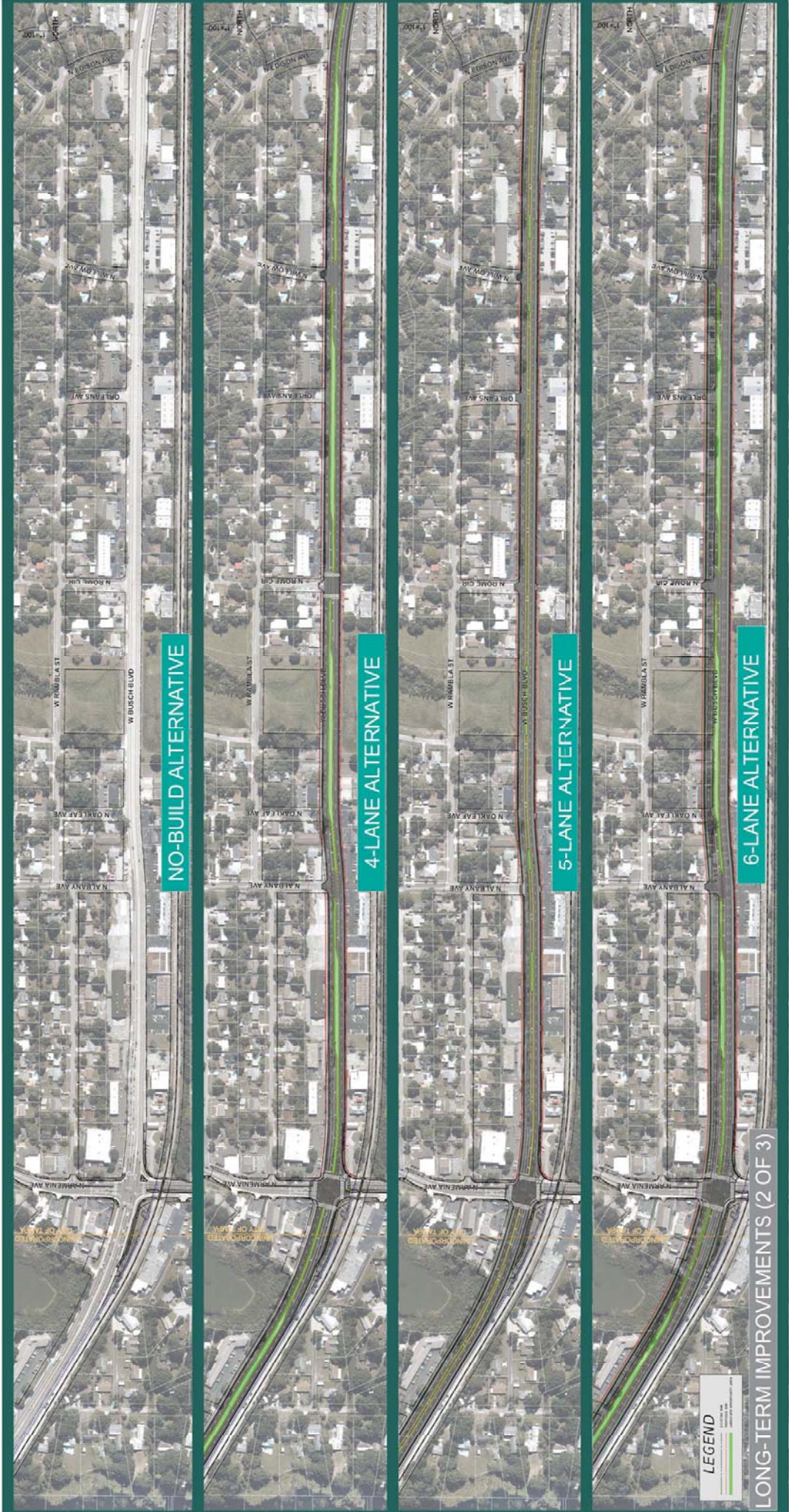
6-Lane Typical Section Alternative

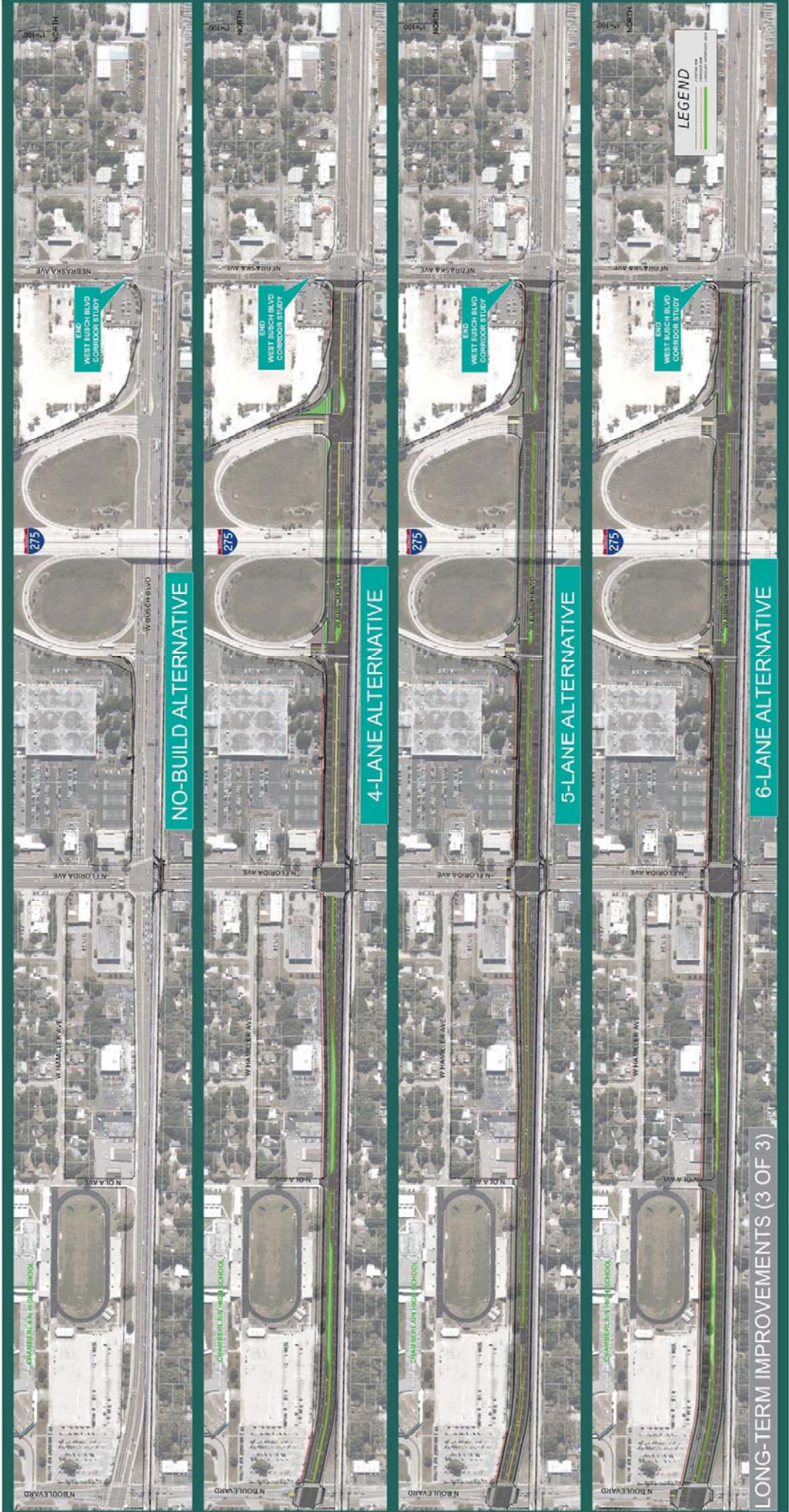


6 Through Lanes with Median and Buffered Bike Lanes*

*Unless the additional lanes are designated as transit-only lanes, a 6-lane section through segment 2 would require an amendment to Tampa's Comprehensive Plan.







Traffic Level of Service Map



Future (2040) 4-Lane/5-Lane Level of Service






























Future (2040) 6-Lane Level of Service



The 6-lane alternative won't significantly improve Level of Service, but it could serve over 10,000 more trips per day.

Projected Average Annual Daily Traffic (AADT)

Evaluation Matrix

		Alternatives			
		No-Build	4-Lane	5-Lane	6-Lane
Satisfies Vision	 How well does this alternative satisfy safety, comfort, and convenience of travel through and across for auto mode ?				
	 How well does this alternative satisfy safety, comfort, and convenience of travel through and across for transit users ?				
	 How well does this alternative satisfy safety, comfort, and convenience of travel through and across for bicyclists ?				
	 How well does this alternative satisfy safety, comfort, and convenience of travel through and across for pedestrians ?				
Stakeholder Support	Relative Project Advisory Group Support				
	Relative Public Support	Pending Input from Today's Meeting	Pending Input from Today's Meeting	Pending Input from Today's Meeting	Pending Input from Today's Meeting
Cost	 Potential Residential & Business Impacts*	-	73	80	97
	 Potential Right-of-Way Impacts*	-	2.5 acres	3 acres	12 acres
	 Construction Cost**	-	\$39 M	\$33.7 M	\$43.2 M

*Estimated impacts based on widest potential footprint
 **Excludes right-of-way cost

Supporting Recommendations

PARTNER AGENCIES ALSO PLAY A ROLE IN SUPPORTING THE CORRIDOR VISION.

THE FOLLOWING RECOMMENDATIONS CAN BE IMPLEMENTED WITH THEIR SUPPORT:

Add bus shelters to high-volume locations

Relocate bus stops to be adjacent to right-turn lanes

Implement transit signal priority to enable buses to skip the queue

Conduct a regional original-destination study to understand regional east-west connectivity needs

Explore passenger rail transit opportunities

Plan and prioritize additional bicycle investments in proximity to West Busch Boulevard

Create an overlay district in the City of Tampa to ensure future development is consistent with the corridor vision

Complete a School Zone study

Establish a high visibility police enforcement area

Benefits of a Lower Speed

THE FEASIBILITY OF LOWERING THE DESIGN SPEED / SPEED LIMIT ALONG THE CORRIDOR IS RECOMMENDED FOR FURTHER STUDY. A LOWER SPEED:

BETTER SATISFIES THE CORRIDOR VISION

- Improves the comfort, and convenience of travel through and across the corridor for pedestrians, bicyclists, and transit users

Trees, lighting and utilities can be moved out of the sidewalk

More design options could be implemented, including parallel parking lanes, two-way left turn lanes and sharrows

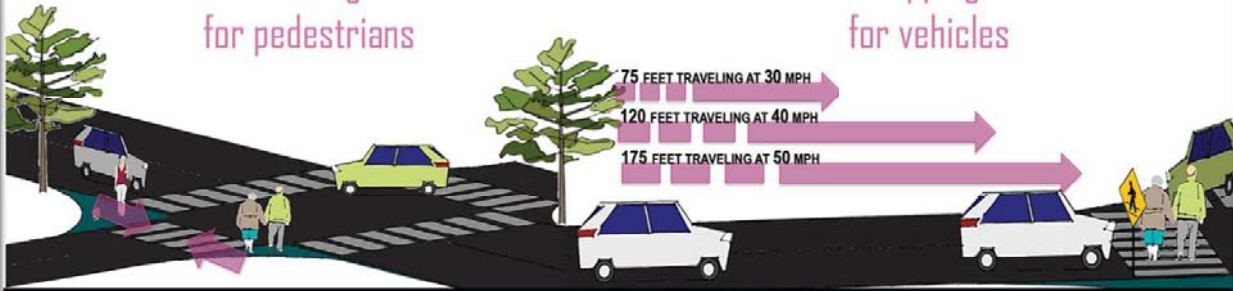
- Lowers right-of-way impacts and construction cost



AND IMPROVES SAFETY

Reduces crossing distances for pedestrians

Reduces stopping distances for vehicles



Reduces crash fatalities and severe injuries



80% LIKELIHOOD OF FATALITY OR SEVERE INJURY

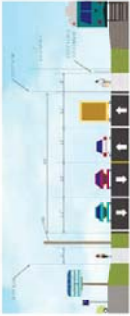





40% LIKELIHOOD OF FATALITY OR SEVERE INJURY

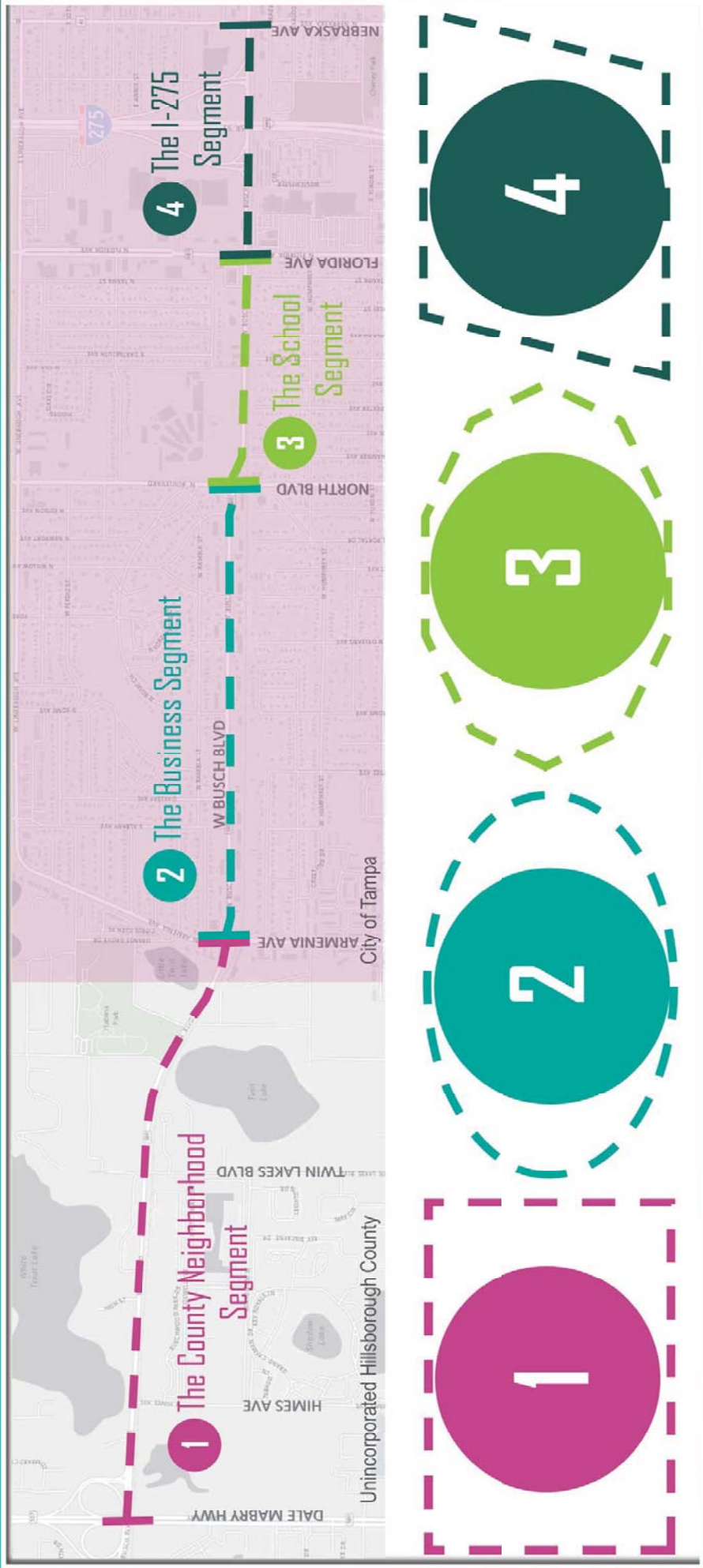


10% LIKELIHOOD OF FATALITY OR SEVERE INJURY

Stakeholder Support

Alternatives	Relative Public Support
<p>No-Build</p> 	
<p>4-Lane</p> 	
<p>5-Lane</p> 	
<p>6-Lane</p> 	

Alternatives by Segment



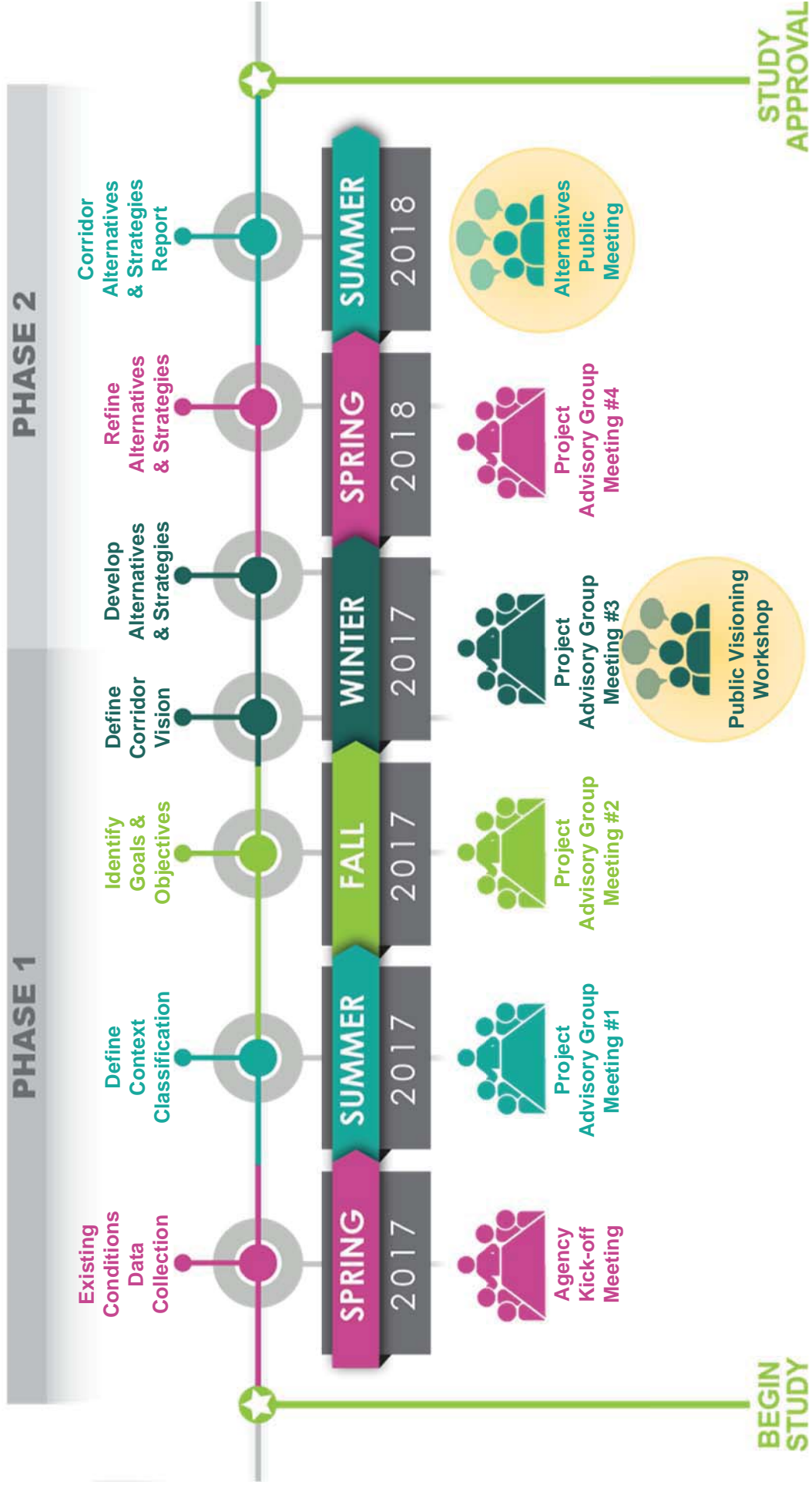
Welcome to the
West Busch Boulevard (SR 580) Corridor Study
from North Dale Mabry Highway to North Nebraska Avenue

Alternatives Public Meeting

Hillsborough County, Florida
FDOT Financial Project ID Number: 435908-1-22-01
Tampa First Seventh-Day Adventist Church
July 10, 2018 5:30 p.m. – 7:30 p.m.



Study Schedule



Today's Presentation

1 Study Overview

2 Preliminary Recommendations

3 Next Steps



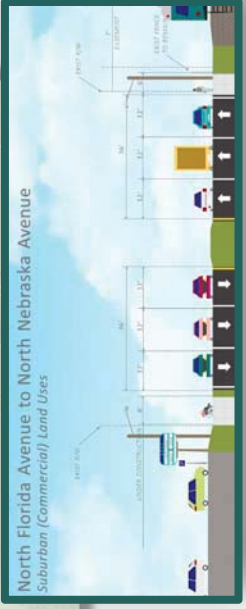
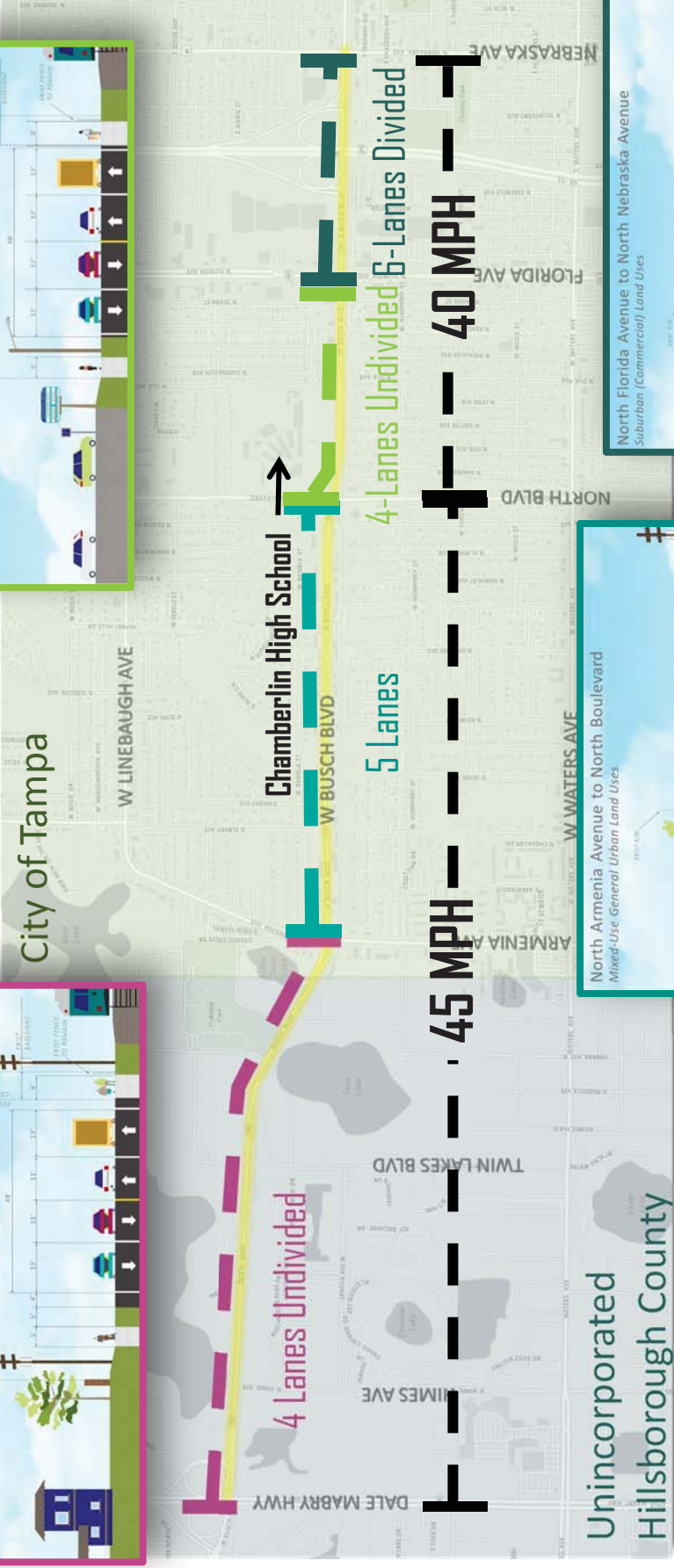
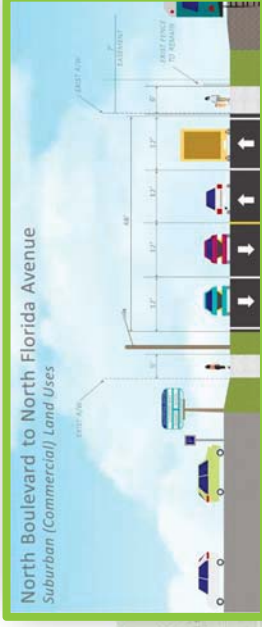
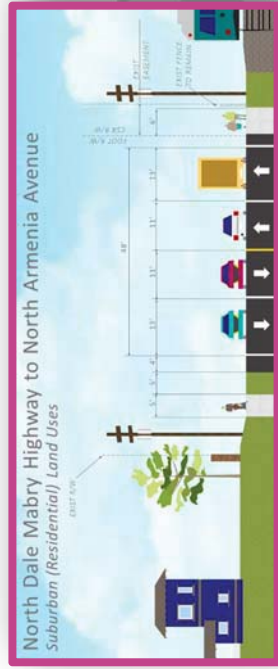
Study Overview



Project Study Area



Existing Conditions



Why West Busch Boulevard?

- Provides direct access to important community features
- Important east-west regional roadway
- Need to improve congestion, access, and operational efficiency
- Need to reduce vehicular, pedestrian and bicyclist crashes and provide safe access to transit

How Do We Get There?

“ The Busch Boulevard corridor offers **safe**, comfortable, and convenient access through and **across** the corridor for all users and all travel modes. ”

Corridor Vision Statement



Preliminary Recommendations: Near-Term and Mid-Term Improvements

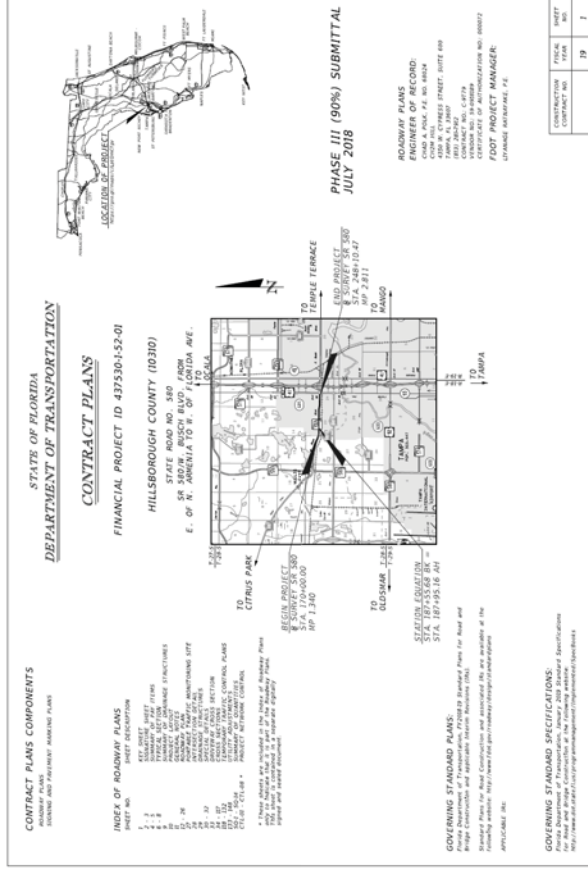


Identifying Concerns and Issues Along the Corridor



Near-Term Improvements

- Resurfacing and restriping the pavement as part of on-going maintenance
- ADA improvements
- Improvements to the sidewalk condition



Mid-Term Improvements

YOUR SPEED Radar Signs

Time Signals to Increase Platooning (35 MPH)

SIGNALS SET FOR **35** MPH

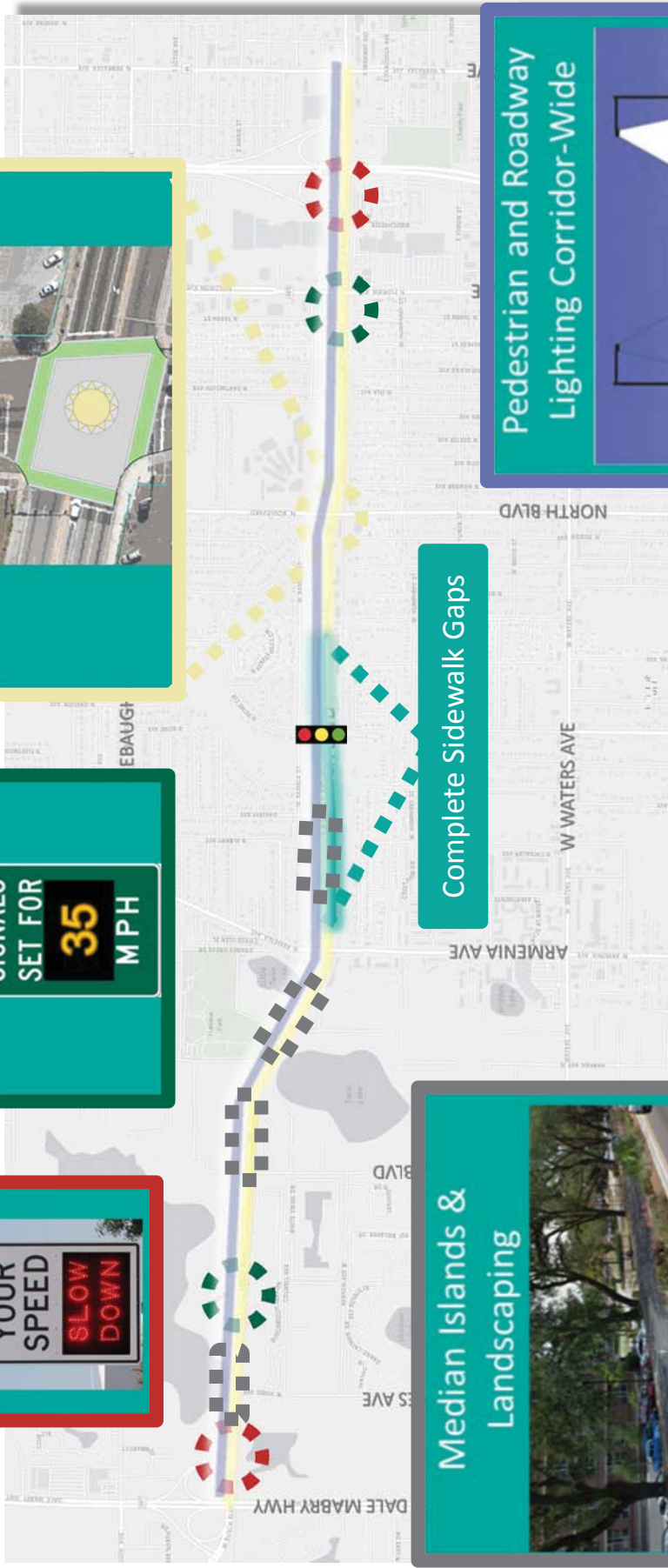
Intersection Improvements :

- Leading Pedestrian Interval or Ped Only Phase
- Patterned Pavement Intersection
- Complete Sidewalk to Library

Median Islands & Landscaping

Complete Sidewalk Gaps

Pedestrian and Roadway Lighting Corridor-Wide

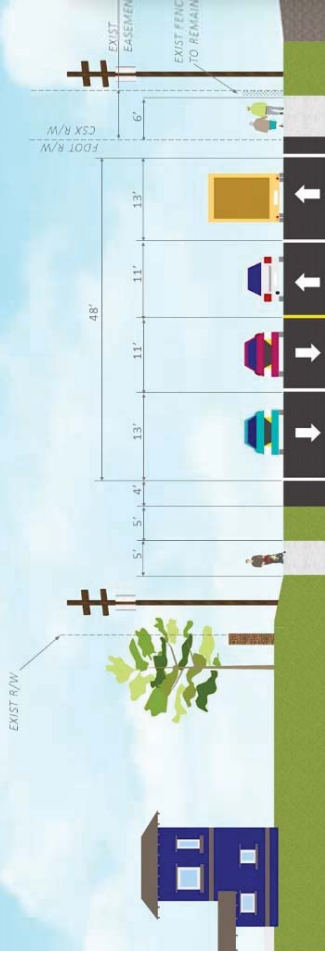


Preliminary Recommendations: Long-Term Improvements

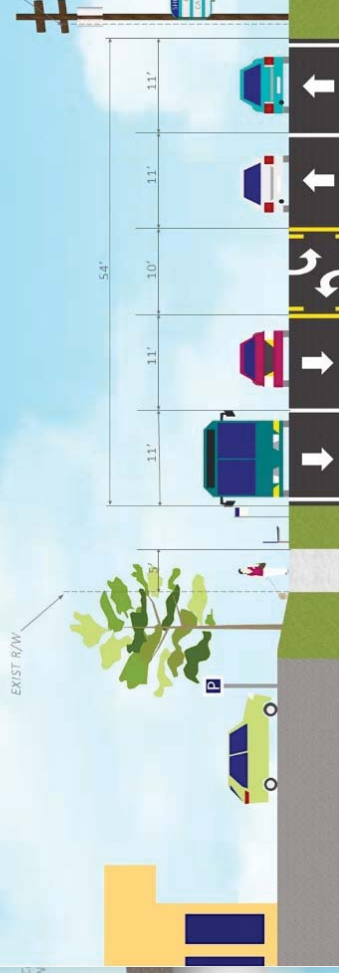


No-Build Alternative

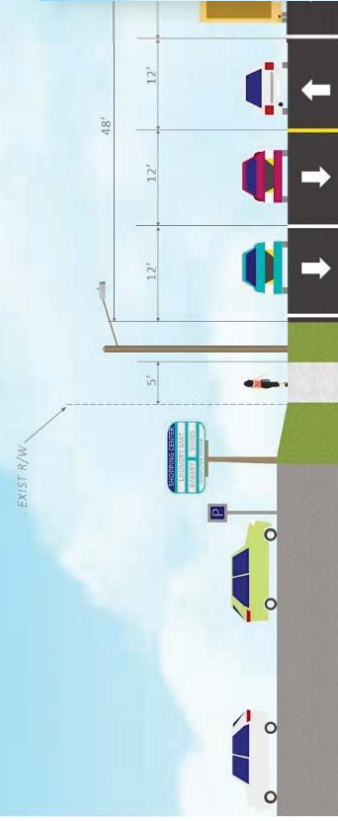
North Dale Mabry Highway to North Armenia Avenue
Suburban (Residential) Land Uses



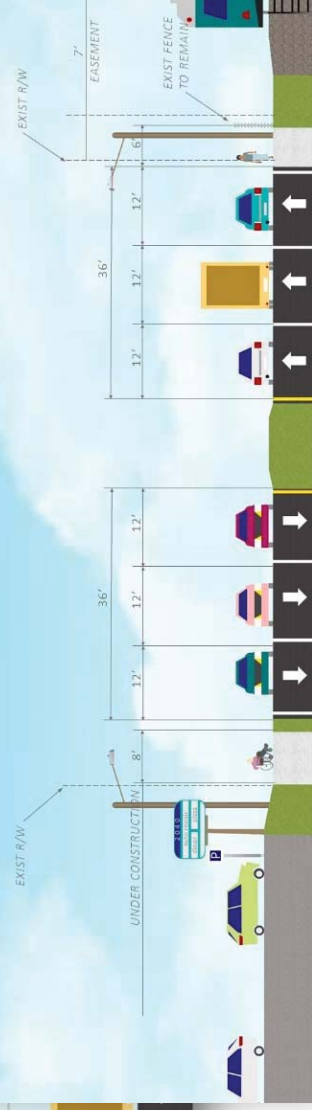
North Armenia Avenue to North Boulevard
Mixed-Use General Urban Land Uses



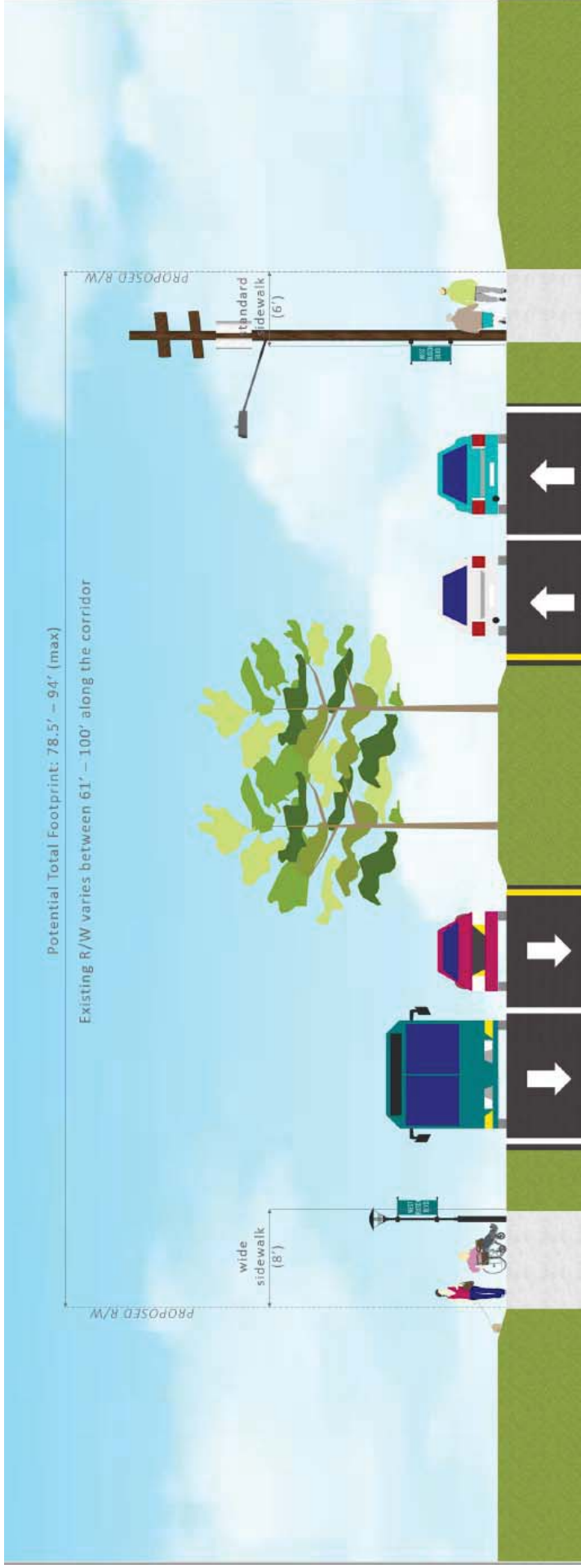
North Boulevard to North Florida Avenue
Suburban (Commercial) Land Uses



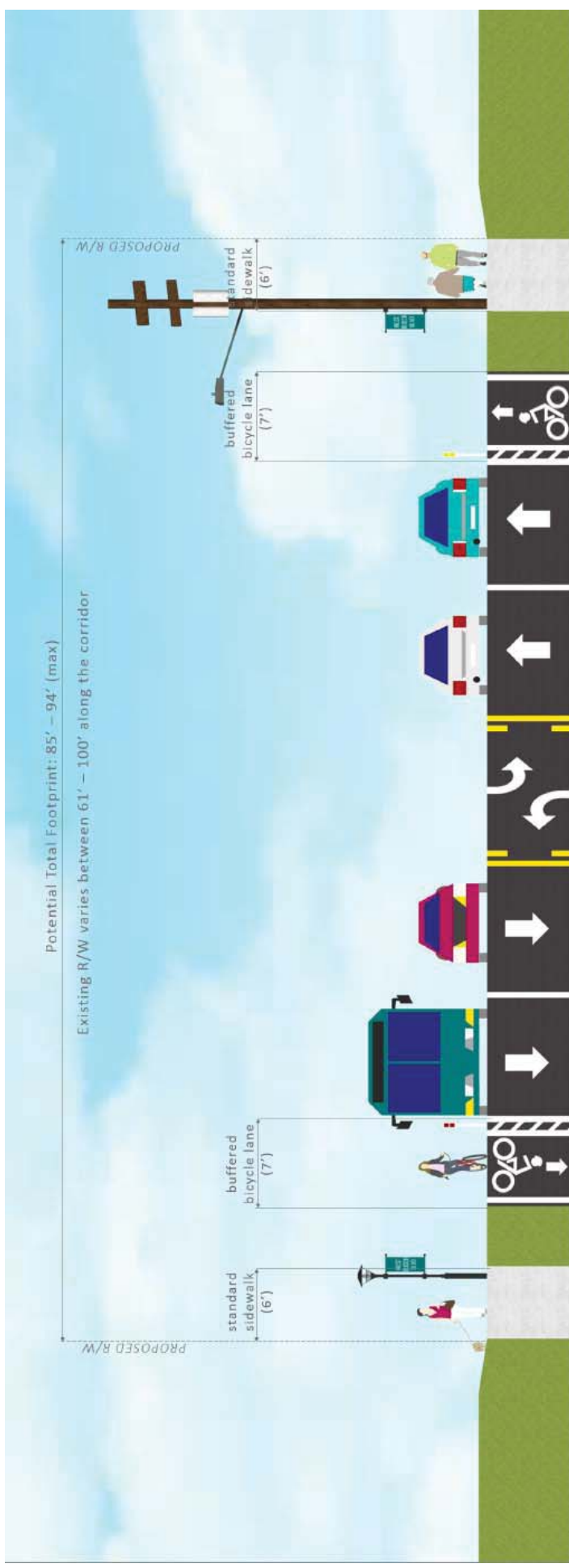
North Florida Avenue to North Nebraska Avenue
Suburban (Commercial) Land Uses



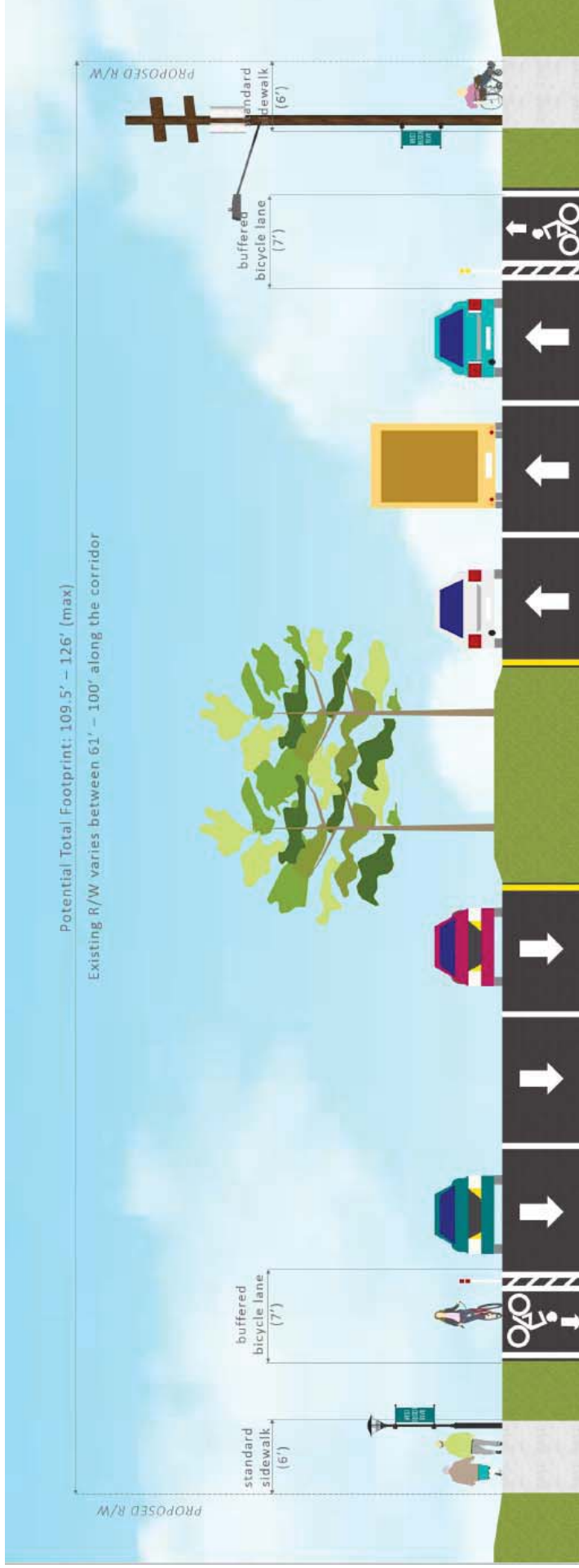
Long-Term 4-Lane Alternative: Median and Wide Sidewalk on the North Side



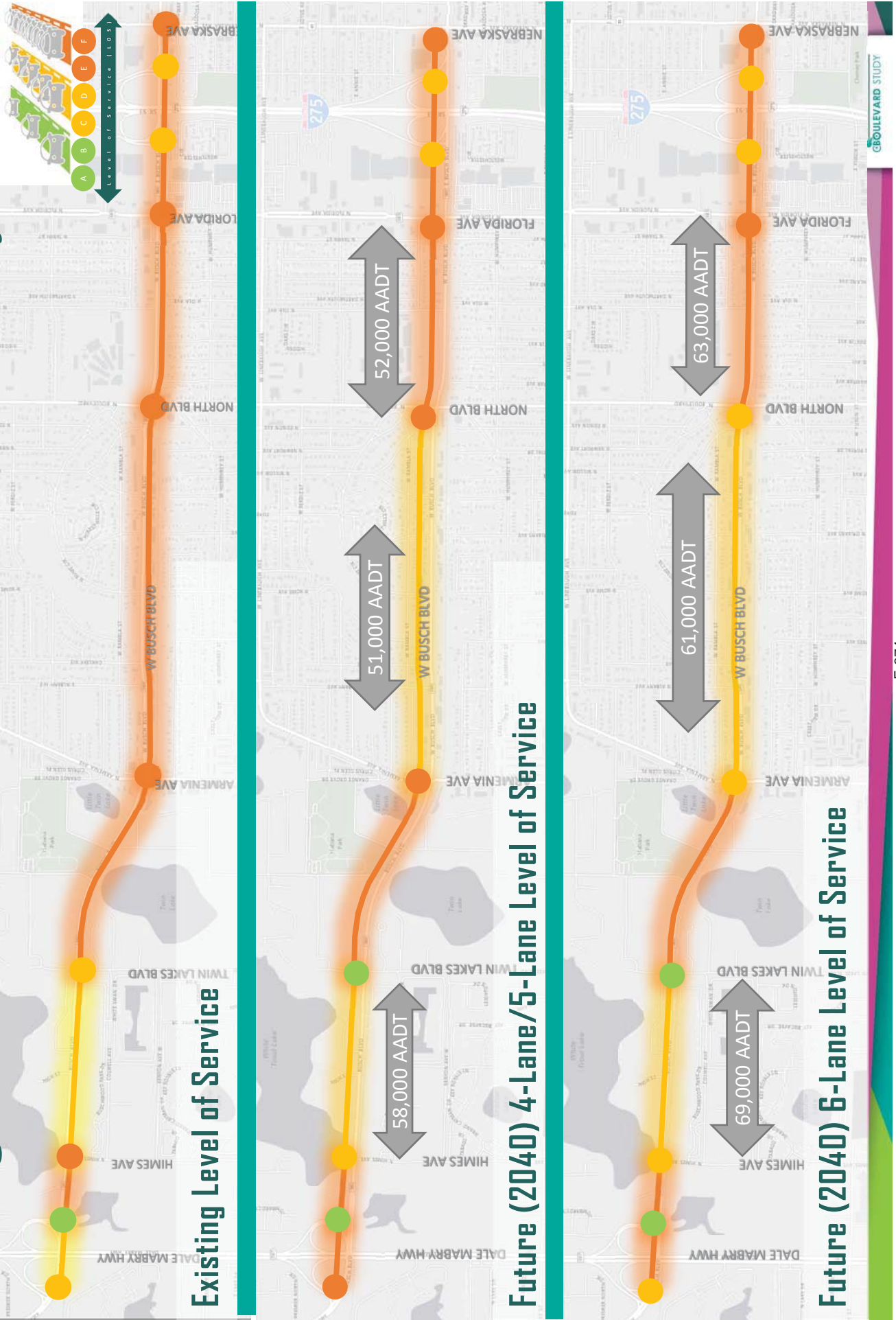
Long-Term 5-Lane Alternative: Two-Way Left Turn Lane and Buffered Bike Lanes



Long-Term 6-Lane Alternative: Additional Lanes, Median, and Buffered Bike Lanes



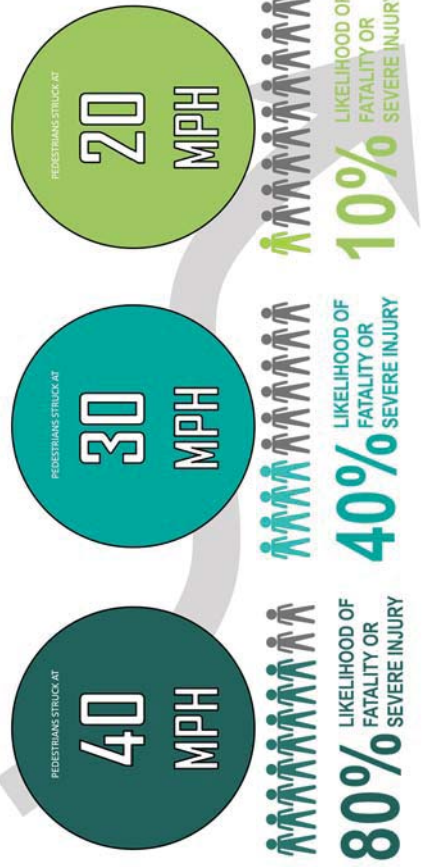
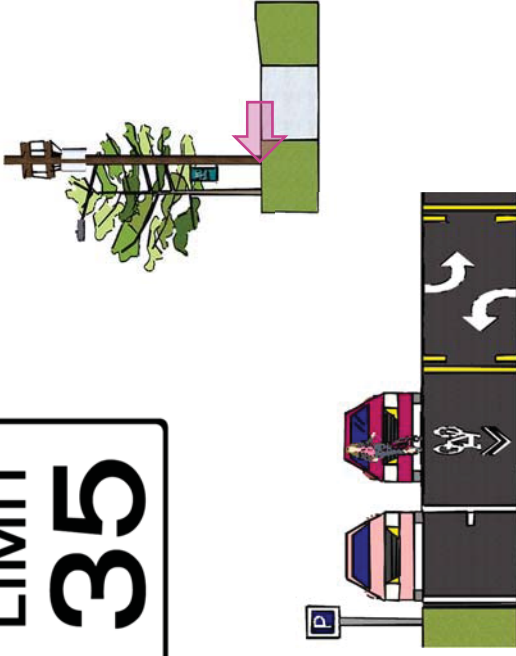
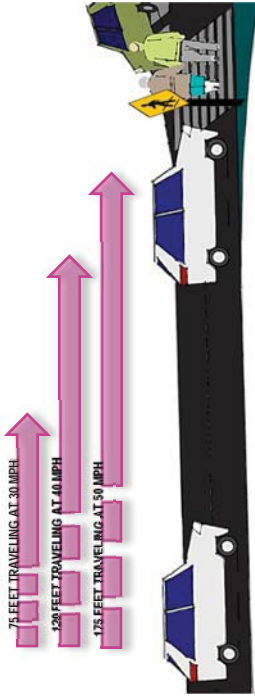
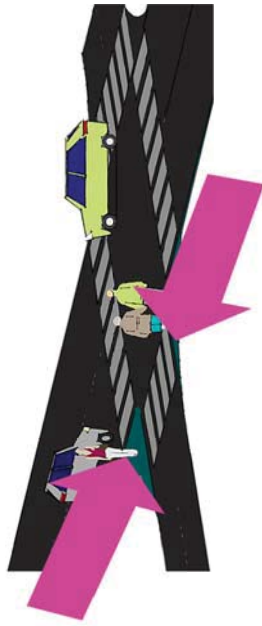
Long-Term Alternatives: Future Traffic Projections



Long-Term Alternatives: Evaluation Matrix

		Alternatives			
		No-Build	4-Lane	5-Lane	8-Lane
Satisfies Vision	 How well does this alternative satisfy safety, comfort, and convenience of travel through and across for auto mode ?				
	 How well does this alternative satisfy safety, comfort, and convenience of travel through and across for transit users ?				
	 How well does this alternative satisfy safety, comfort, and convenience of travel through and across for bicyclists ?				
	 How well does this alternative satisfy safety, comfort, and convenience of travel through and across for pedestrians ?				
Stakeholder Support	Relative Project Advisory Group Support				
	Relative Public Support				
Cost	Potential Residential & Business Impacts*	-	73	80	97
	Potential Right-of-Way Impacts*	-	2.5 acres	3 acres	12 acres
	Construction Cost**	-	\$39 M	\$33.7 M	\$43.2 M

Potential to Reduce the Design Speed



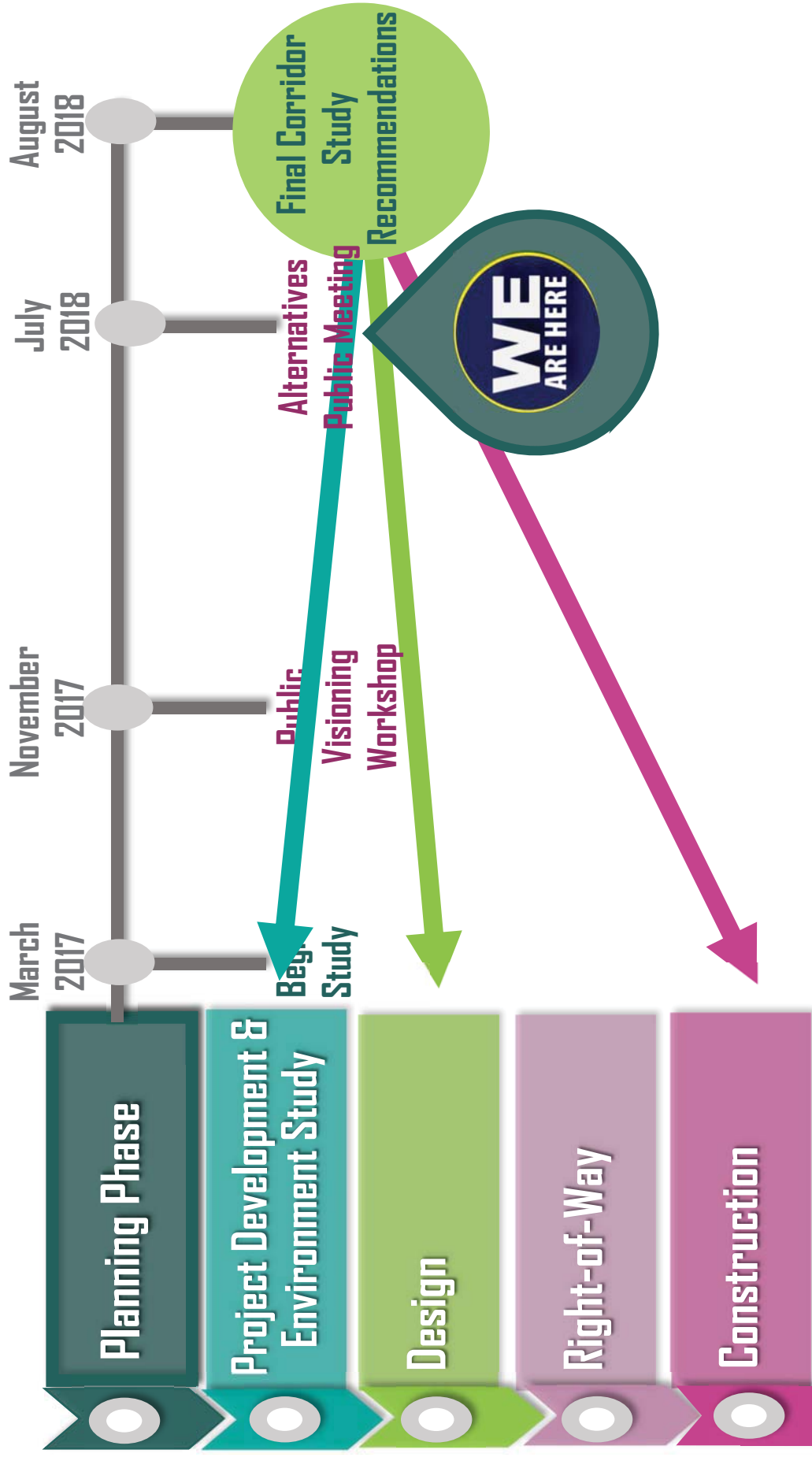
	No-Build	4-Lane	5-Lane	6-Lane
How well does this alternative satisfy safety, comfort, and convenience of travel through and across for transit users?	🔴	🟡	🟢	🟢
How well does this alternative satisfy safety, comfort, and convenience of travel through and across for bicyclists?	🔴	🟡	🟢	🟢
How well does this alternative satisfy safety, comfort, and convenience of travel through and across for pedestrians?	🔴	🟡	🟢	🟢
Relative Project Advisory Group Support	🟡	🟢	🟢	🟢
Relative Public Support	🟡	🟢	🟢	🟢
Potential Residential & Business Impacts*	-	73	80	97
Potential Right-of-Way Impacts*	-	2.5 acres	3 acres	12 acres
Construction Cost**	-	\$39 M	\$33.7 M	\$43.2 M



Next Steps



Study Process





Overview of the Meeting Stations

Station 1: Existing Conditions

Station 5: Supporting Recommendations

Station 2: The Future Vision

Station 6: Interactive Exercises

Station 3: Mid-Term Improvements

Station 7: Provide Comments

Station 4: Long-Term Improvements

Stay Connected

www.fdotd7.com | fdot@fdot.com | **ACTIVE STUDY**

West Busch Boulevard (SR 580) Corridor Study

From North Dale Mabry Highway to North Nebraska Avenue | Financial Project ID Number: 435698-122201 | Hillsborough County, Florida

Home | What is a Corridor Study? | Project Details | Public Involvement | Project Documents | Project Schedule | Contact Information | Send Us Your Comments

Project Overview

An Alternatives Public Meeting for the West Busch Boulevard (SR 580) Corridor Study will be held on **Tuesday, July 10, 2018** from 6:30 p.m. to 7:30 p.m. For more information on the meeting, please [click here](#).

The Florida Department of Transportation (FDOT), District Seven, is conducting a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue in Hillsborough County – a distance of approximately 3.3 miles. The SR 580 corridor is an east-west urban principal arterial that connects local commuters to regionally significant corridors, including the Veterans Expressway to the west and I-275 and I-75 to the east. The corridor study area includes portions of unincorporated Hillsborough County and the City of Tampa and is one of the primary links to Busch Gardens Amusement Park (located approximately 2 miles east of the project limit).



The corridor supports a variety of land uses and demographics, including schools, churches, and businesses with direct access to and from Busch Blvd, transit services, pedestrians, bicyclists and many small communities.

The West Busch Boulevard (SR 580) corridor study will identify transportation-related problems the area faces and develop alternatives.

Michael Shroyer, FDOT, District Manager

Send us your Comments

We invite you to get involved! The public plays an important role in the project development and decision-making process of this study. Send us your comments and ideas to help us determine the most appropriate conceptual design alternatives along the West Busch Boulevard (SR 580) corridor.

[>> Read more](#)

Project Schedule

The West Busch Boulevard (SR 580) study began in March 2017 and is anticipated to be completed in 18 months. The project activities include collecting information, defining a vision for the corridor, and developing roadway improvement alternatives.

[>> Read more](#)

Contact Information

For more information or to comment, please contact:

Michael Shroyer, FDOT, District Manager

Search here: [Go](#)

Brian Shroyer
Project Manager for
Florida Department of Transportation
11201 N. McKinley Drive
Tampa, FL 33612
Phone: (813) 975-6444
Email: Brian.Shroyer@dot.state.fl.us

Kristen Carson
Public Involvement Officer
Florida Department of Transportation
11201 N. McKinley Drive
Tampa, FL 33612
Phone: (813) 975-6202 or (800) 226-7220
Email: Kristen.Carson@dot.state.fl.us

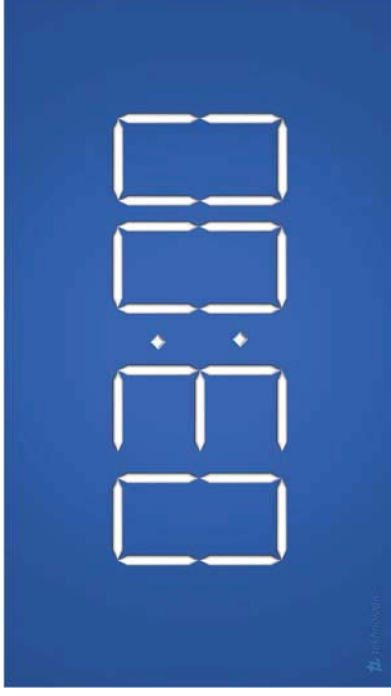
www.fdotd7studies.com/westbuschblvd/



Thank you for attending!



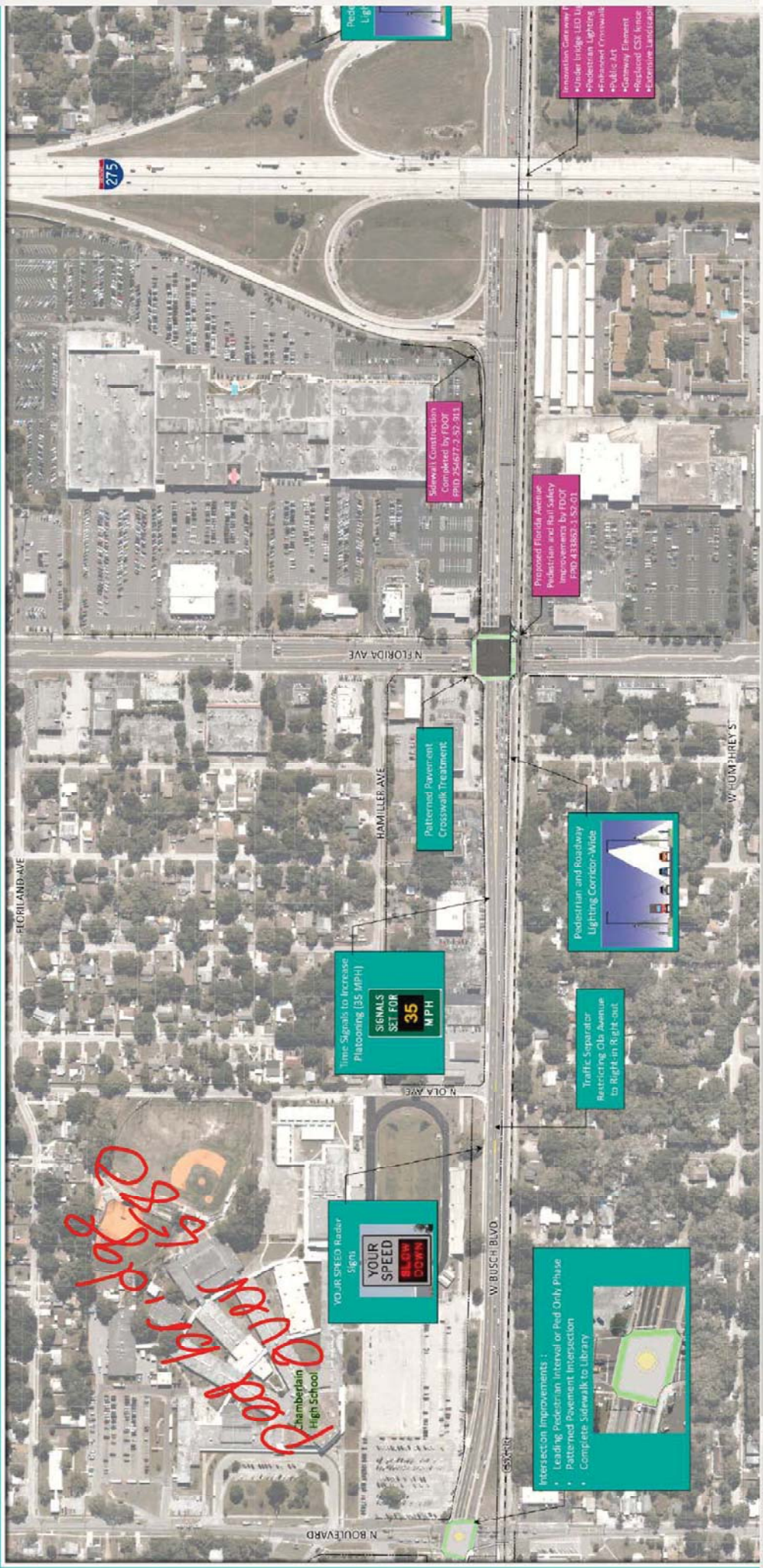
Next Show Starts in:



APPENDIX D – Smart Board Comments / Interactive Exercises









Mid-Term Improvements (3 of 3)

*Redesign
 Over
 2875*

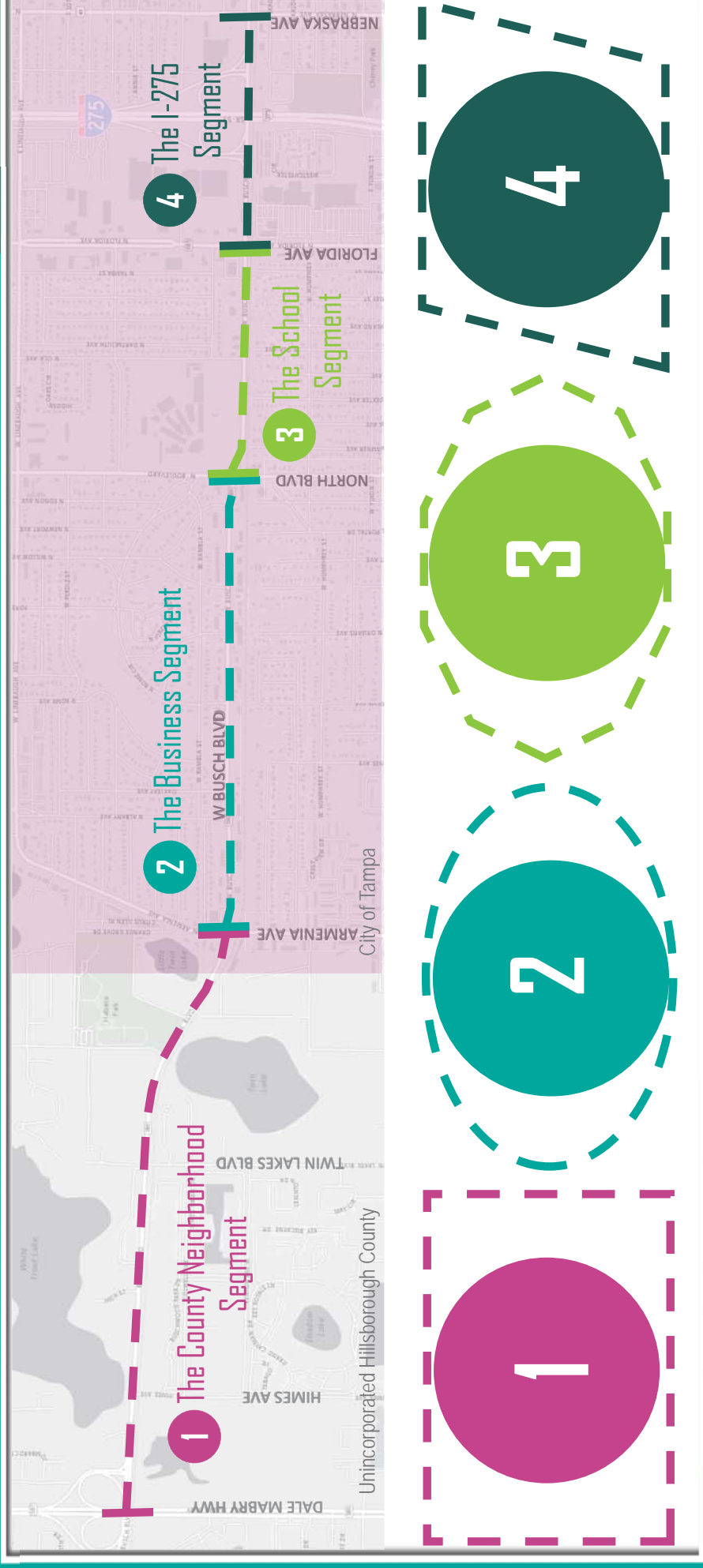




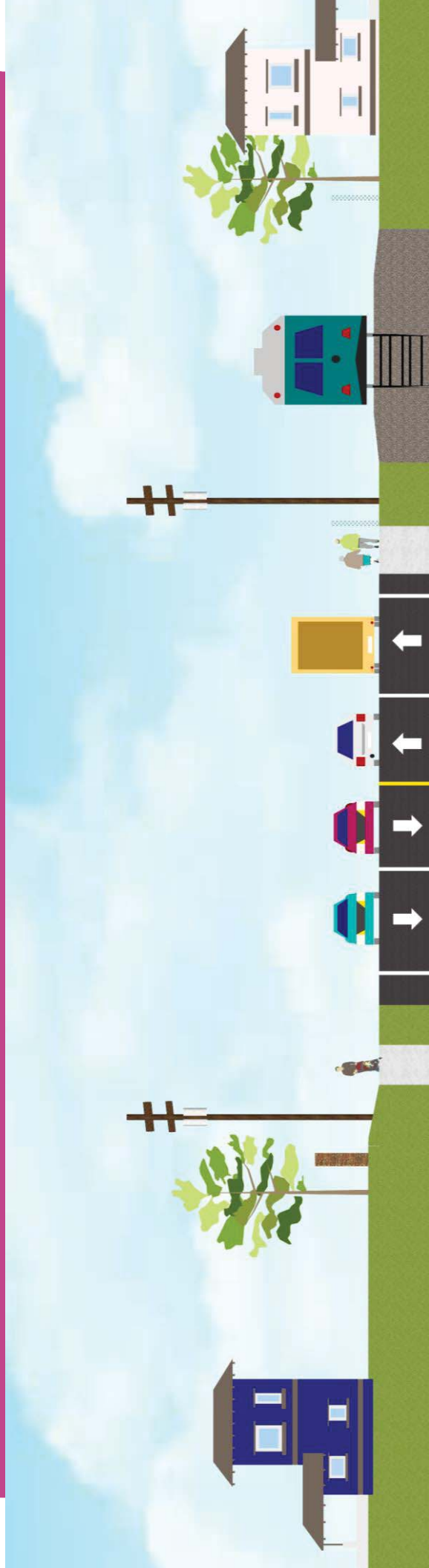
Stakeholder Support

Alternatives	Relative Public Support
<p>No-Build</p> 	
<p>4-Lane</p> 	
<p>5-Lane</p> 	
<p>6-Lane</p> 	

Alternatives by Segment

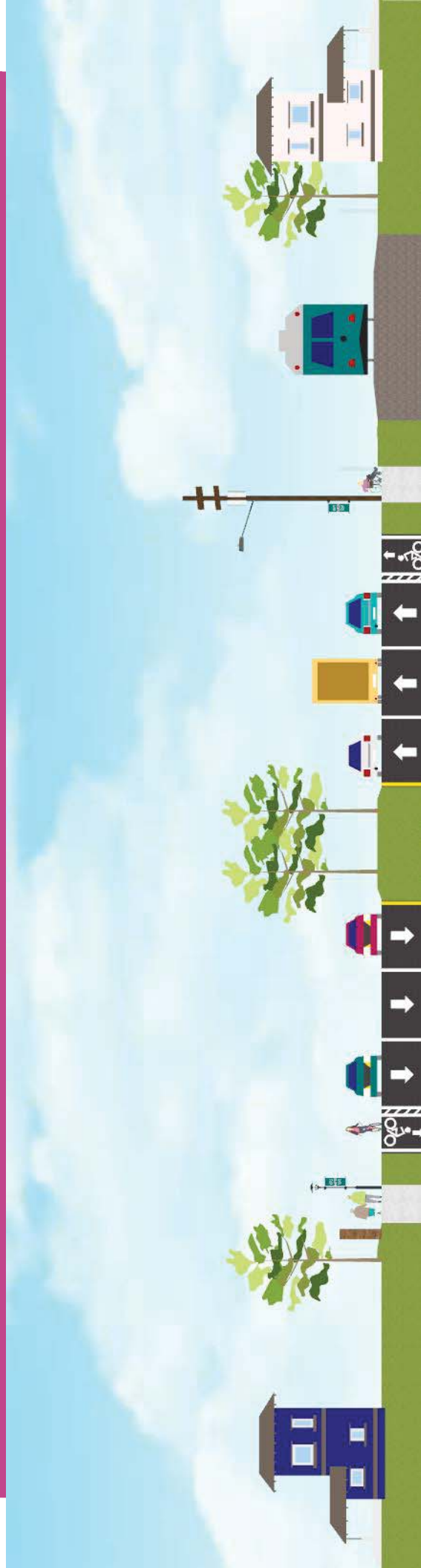


No Build (Existing Typical Section)



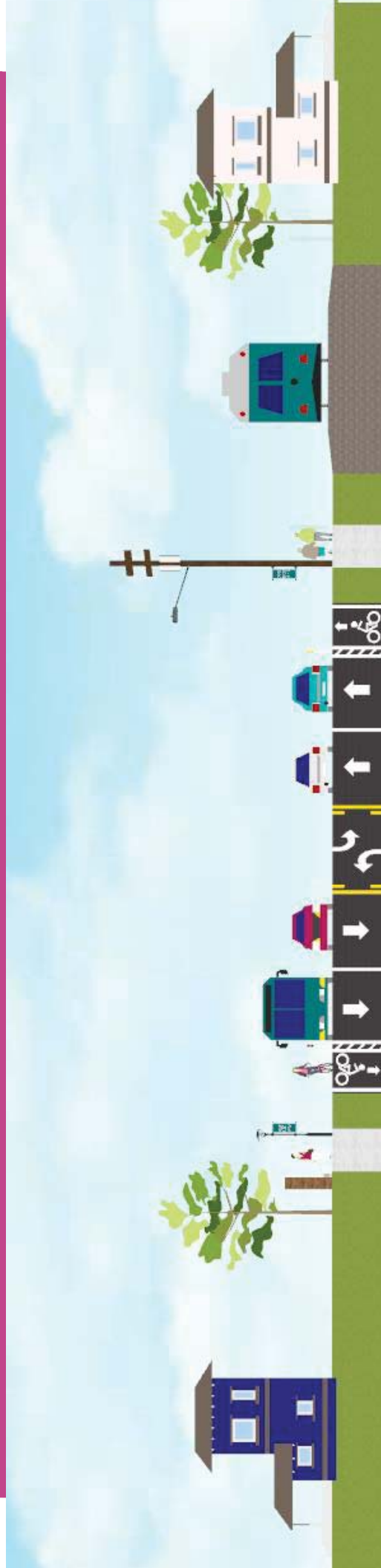
1 The County Neighborhood Segment

6 Lanes with Buffered Bike Lanes



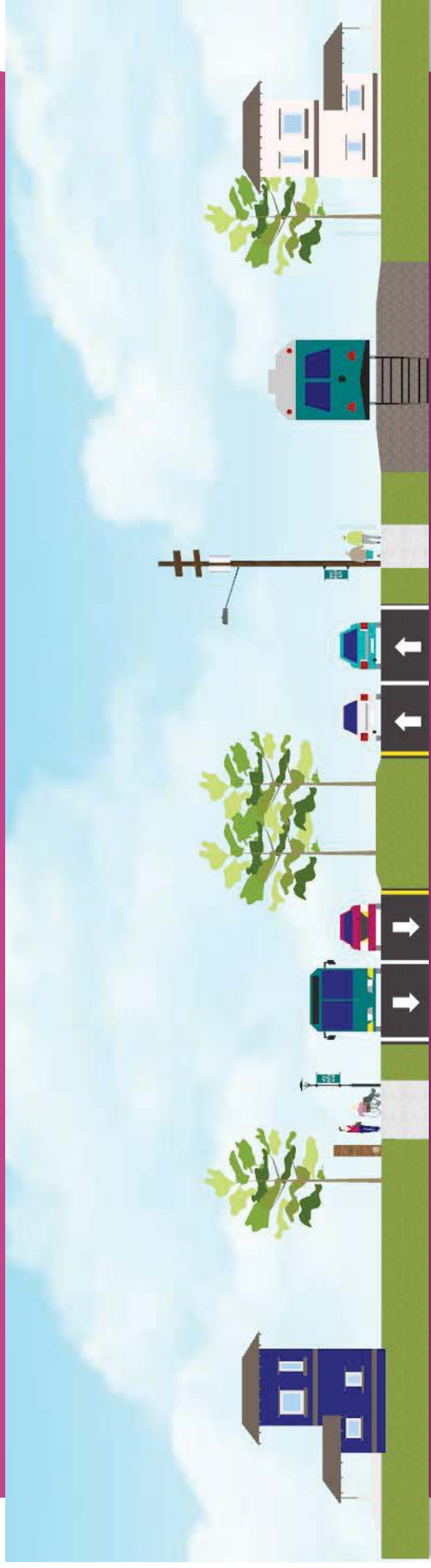
1 The County Neighborhood Segment

5 Lanes with Buffered Bike Lanes



1 The County Neighborhood Segment

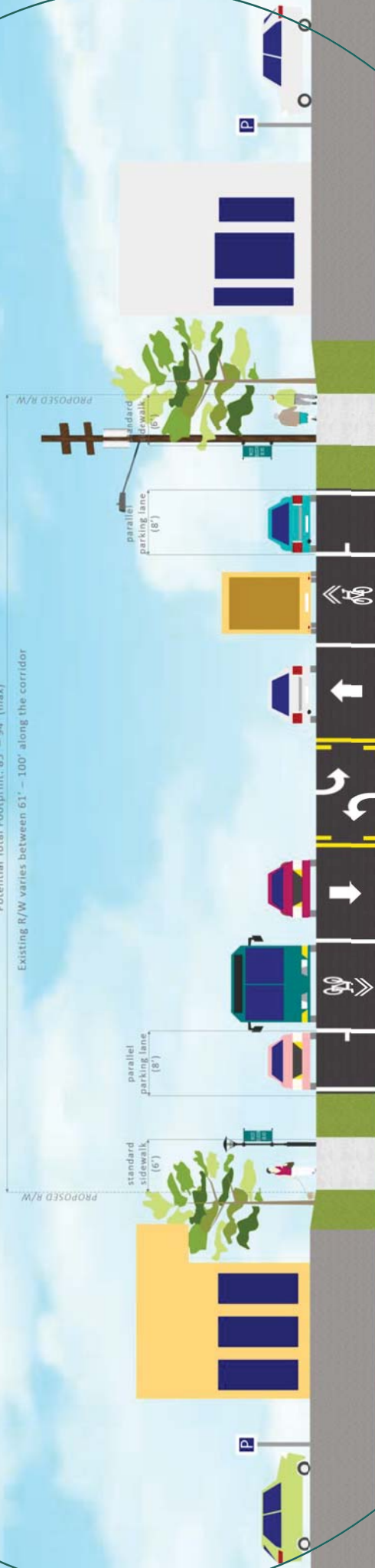
4 Lanes with a Wide Sidewalk



1 The County Neighborhood Segment

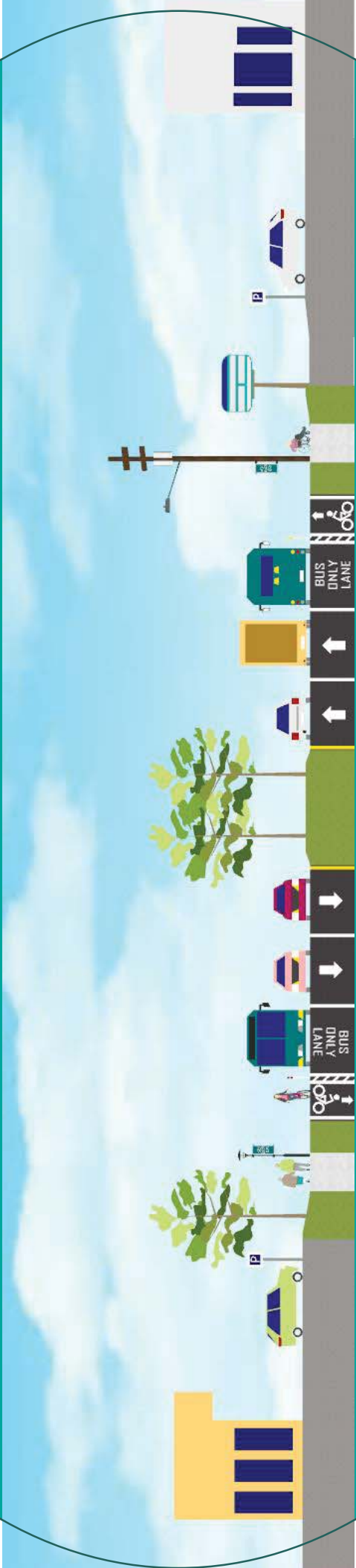
5 Lanes with Parallel Parking

Potential Total Footprint: 85' – 94' (max)
Existing R/W varies between 61' – 100' along the corridor



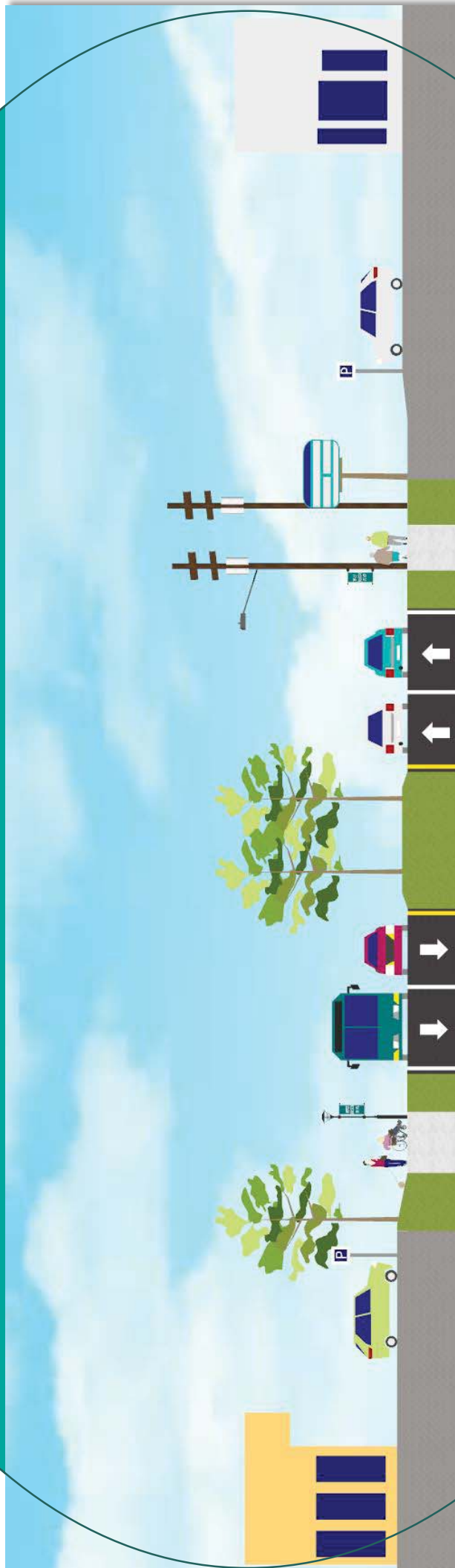
2 The Business Segment

6 Lanes with bus-only lanes and buffered bike lanes



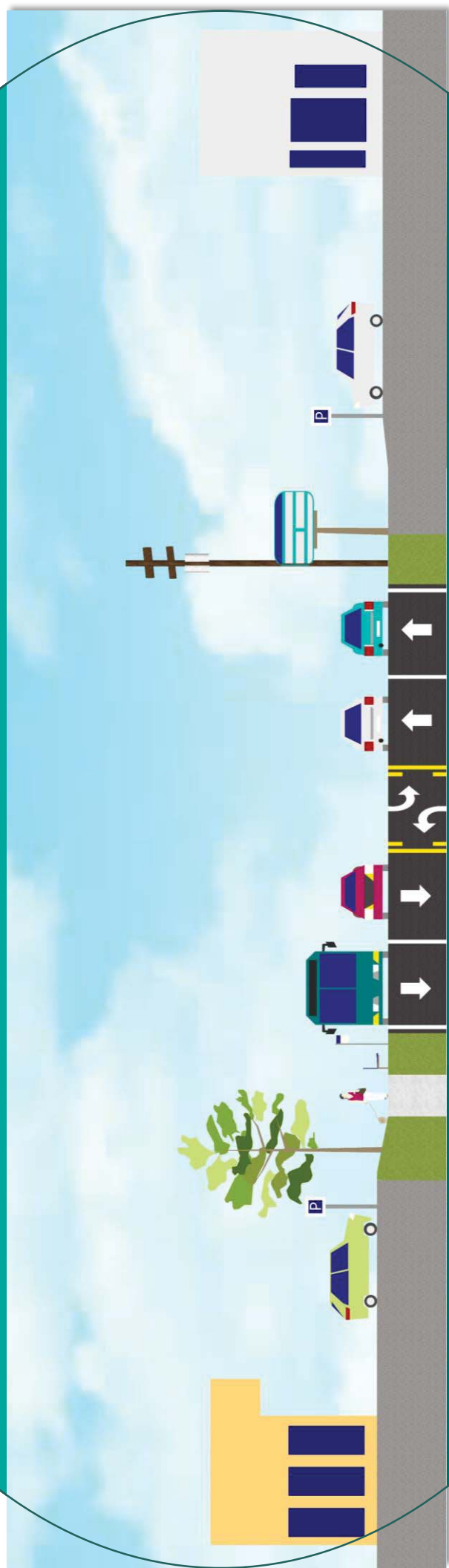
2 The Business Segment

4 Lanes with a wide sidewalk



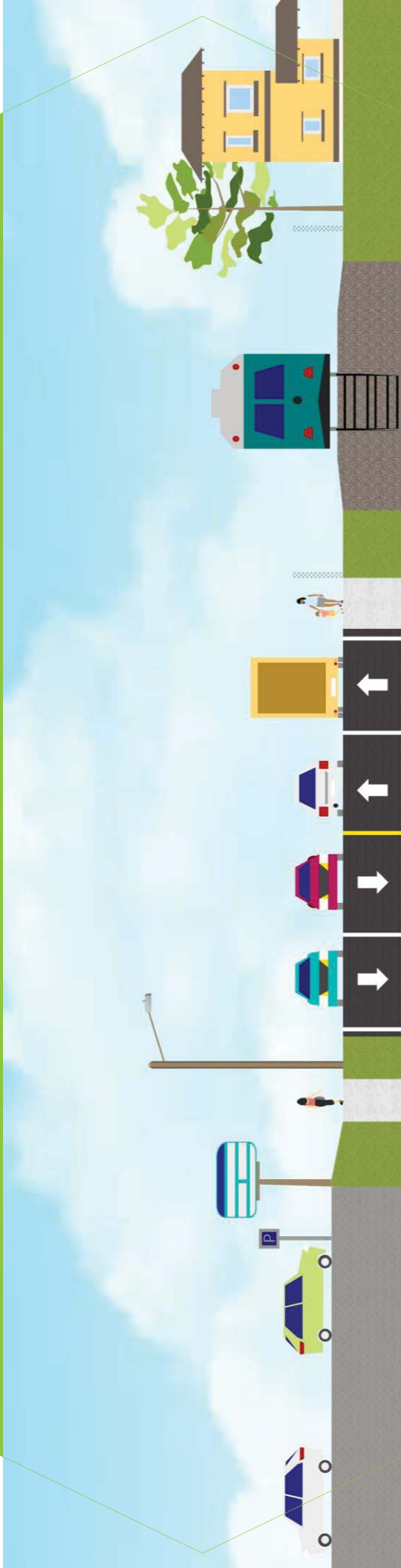
2 The Business Segment

No Build (Existing Typical Section)



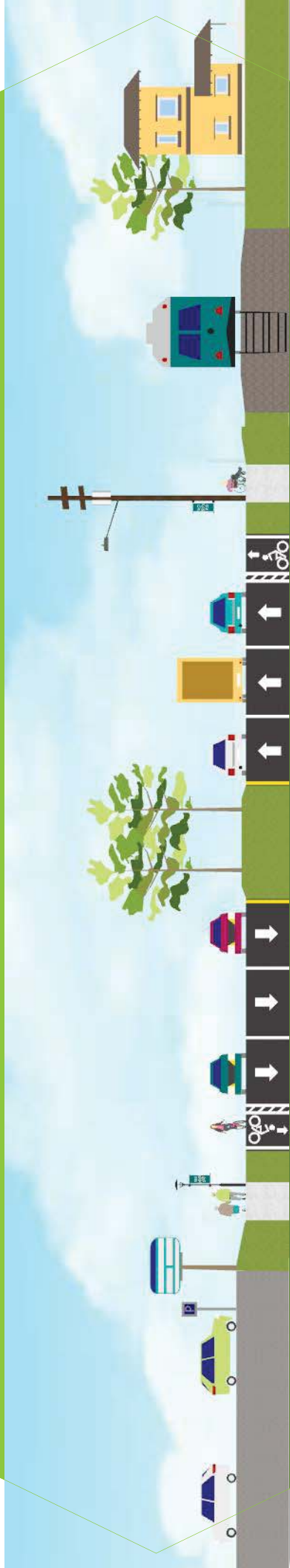
2 The Business Segment

No Build (Existing Typical Section)



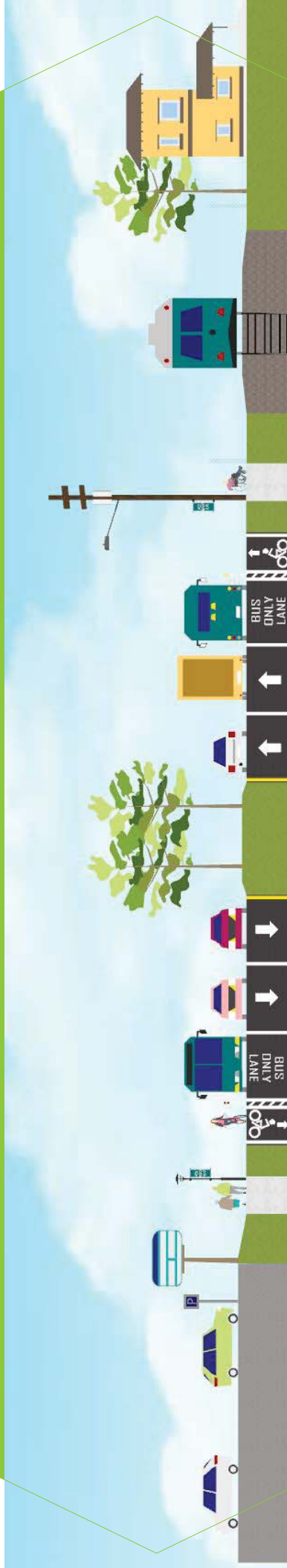
3 The School Segment

6 Lanes with Buffered Bike Lanes



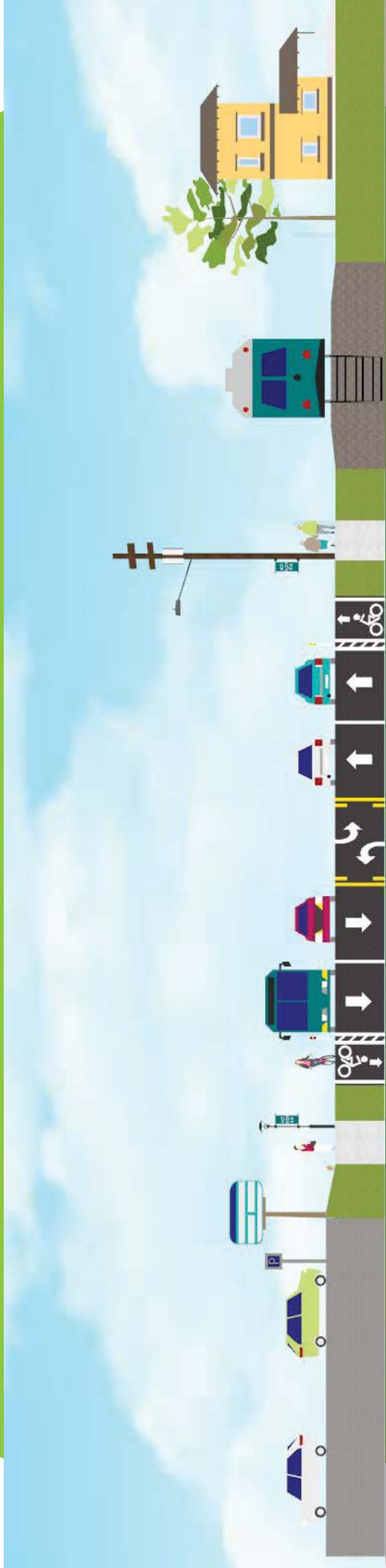
3 The School Segment

6 Lanes with Bus-Only Lanes and Buffered Bike Lanes



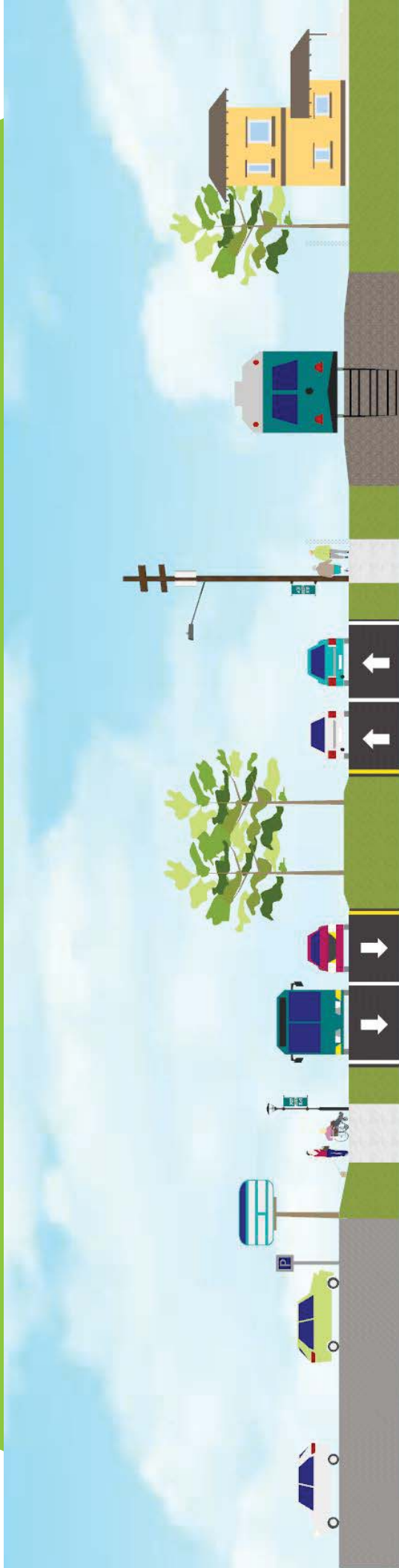
3 The School Segment

5 Lanes with Buffered Bike Lanes



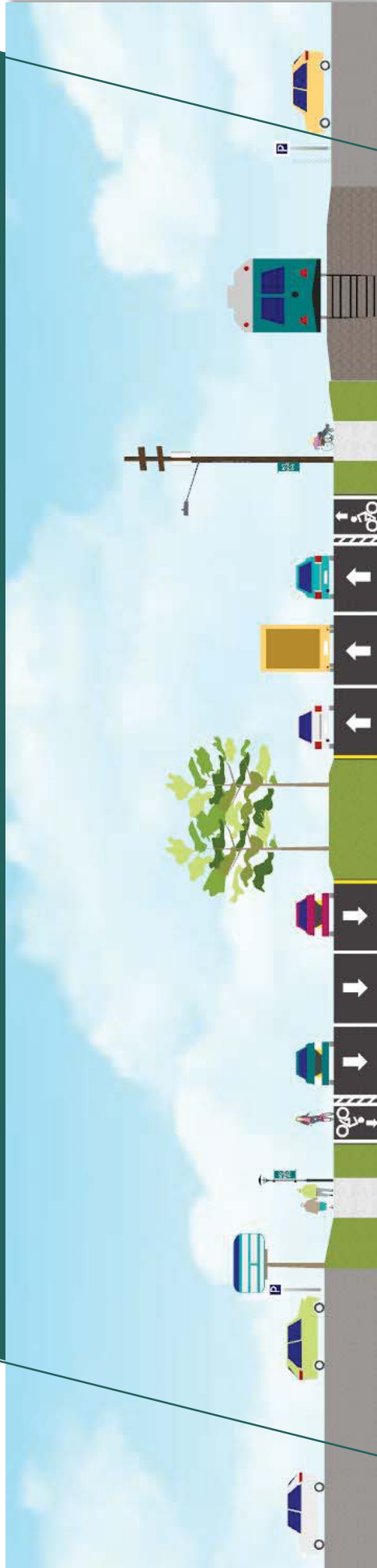
3 The School Segment

4 Lanes with a Wide Sidewalk



3 The School Segment

6 Lanes with Buffered Bike Lanes



4 The I-275 Segment



Alternatives by Segment

#	Segment 1	Segment 2	Segment 3	Segment 4	NAME
1	No-build (NB) 4-lane (4) 5-lane (5) 6-lane (6)	No-build (NB) 4-lane (4) 5-lane (5) 6-lane (6) 5-lane with parallel parking (SPP) 6-lane with bus only lane (6B)	No-build (NB) 4-lane (4) 5-lane (5) 6-lane (6) 6-lane with bus only lane (6B)	No-build (NB) 6-lane without bike lanes (6 w/o) 6-lane (6)	
2	4	5	4	6 w/b	
3	S	6B	6B	6	
4	6	5	6	6	
5	6	6	6	6	
6	S	5	5	6	
7	4	4	4	6 w/o	
8	S	5	5	6	
9	6	6B	6B	6	
10	S	6	5	6	
11					
12					
13					
14					
15					
16					
17					
18					
19					
20					

APPENDIX E – Sign-in Sheets



ALTERNATIVES PUBLIC MEETING
West Busch Boulevard (SR 580) Corridor Study
 From North Dale Mabry Highway to North Nebraska Avenue
 Financial Project ID Number: 435908-1-22-01



Tuesday, July 10, 2018 from 5:30 pm to 7:30 pm ■ Tampa First Seventh-Day Adventist Church, 822 West Linebaugh Ave, Tampa, FL 33612

Alternatives Public Meeting Sign-In Sheet

NAME (PLEASE PRINT)	AGENCY / ORGANIZATION	TITLE	PHONE	EMAIL
Carol Barker	CH2M	Project Manager	407.650.2173	Carol.barker@ch2m.com
David Hernandez	CH2M	Consultant		david.hernandez@ch2m.com
Henry Spells			813-531-5943	HenrySpells@gmail.com
Daniel Edger	Bullington Insurance Group	VP	813-428-8186	daniel.edger-fl@gmail.com
Estrella Rodriguez				
OLIVIA STEPHANIDES	9906 LOVE TREE LN		813-546-4711	stephanides-olivia@gmail.com
DR MARIS STEPHANIDES	HOVORNWOOD TAMPA 33618			
HEGLEY HAZEL	8920 ALPACONIA AVE		813-731-3682	← TEXT
RAY CLARK	4404 SUMMER OAK DR			
Gregory Colombo	9403 N. Edison Ave.		813-244-3223	GraphicGreg2009@yahoo.com
Melissa Marison (and Kevin)	9610 Forest Hills Dr 33612		813-892-6095	MARISSON@hotmail.com
Gena Torres	Hillsborough L MPO		813.273.5774 x357	
Jenn Rhodes	CH2M	Planner	407-497-9319	JENN.RHODES@JACOBS.COM
Cris Schooley			407-334-2912	Cris.Schooley@JACOBS.COM
Alex Harris	FDOT	PA (not this project)	813-323-1140	Alex.Harris@dot.state.fl.us
Lewis Carlisi	Self		(813) 951-5134	l.garlisi@garlisi.com
Chad Polk	CH2M	Consultant	813-281-7912	chad.polk@ch2m.com
Ura Bouck	Lochner		813.494.8101	LBouck@Hwloch.net.com
BRYAN L. SHROYER	FDOT	PROJECT MANAGER	813975-6449	BRYAN.SHROYER@DOT.STATE.FL.US

*

APPENDIX F – Meeting Agenda









ALTERNATIVES PUBLIC MEETING

Florida Department of Transportation Alternatives Public Meeting

5:30 p.m. – 7:30 p.m. • Tuesday, July 10, 2018

Tampa First Seventh-Day Adventist Church • 822 West Linebaugh Avenue • Tampa, FL 33612

We invite you to learn more about the **West Busch Boulevard Corridor Study** by completing the following steps:

-  **INTRO VIDEO** Before reviewing the exhibits in the next room, please watch a short project introduction video
-  **STATION 1** Existing Conditions
-  **STATION 2** The Future Vision
-  **STATION 3** Mid-Term Improvements
-  **STATION 4** Long-Term Improvements
-  **STATION 5** Supporting Recommendations
-  **STATION 6** Interactive Exercises
-  **STATION 7** Provide Comments

APPENDIX G – Public Comments



ALTERNATIVES PUBLIC MEETING
West Busch Boulevard (SR 580) Corridor Study
 From North Dale Mabry Highway to North Nebraska Avenue
 Financial Project ID Number: 435908-1-22-01



Tuesday, July 10, 2018 ■ Seventh-Day Adventist Church, 822 W. Linebaugh Ave. Tampa, Florida 33612

Please provide your comments below. If more space is needed, please use the back of this sheet or an additional sheet of paper. You may place your comments in the "Comment Box" provided at the meeting, or send to Brian Shroyer, Project Manager at the address on the back of this comment form, by July 23, 2018. Emailed comments are also acceptable to Brian.Shroyer@dot.state.fl.us

*All comments are part of the project record and are available for review by the public and the media.

There needs a sidewalk between Armenia Ave and ~~Boyd~~^{Blvd}
 so that I don't have to ride the Center Area
 I would also help for other people who uses wheelchairs

CONTACT INFORMATION (PLEASE PRINT)

Name _____
 Title _____
 Agency / Organization _____
 Phone Number _____
 Email _____

RETURN COMMENTS BY July 23, 2018 TO:

Brian L. Shroyer
 Multimodal Project Manager
 Florida Department of Transportation
 11201 N. McKinley Drive, MS 7-500
 Tampa, FL 33612
 Brian.Shroyer@dot.state.fl.us

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status.



ALTERNATIVES PUBLIC MEETING
West Busch Boulevard (SR 580) Corridor Study
 From North Dale Mabry Highway to North Nebraska Avenue
 Financial Project ID Number: 435908-1-22-01



Tuesday, July 10, 2018 ■ Seventh-Day Adventist Church, 822 W. Linebaugh Ave. Tampa, Florida 33612

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*All comments are part of the project record and are available for review by the public and the media.

Traffic light at Rome + Busch - Dedicated Left Turn Please!

Low cut shrubs for plants - No trees! - Block view

Love the medium Idea! We need a divider!

Love better Lighting! Needed!

Great Presentation! Very Informative!

CONTACT INFORMATION (PLEASE PRINT)

RETURN COMMENTS BY July 23, 2018 TO:

Name _____
 Title _____
 Agency / Organization _____
 Phone Number _____
 Email _____

Brian L. Shroyer
 Multimodal Project Manager
 Florida Department of Transportation
 11201 N. McKinley Drive, MS 7-500
 Tampa, FL 33612
 Brian.Shroyer@dot.state.fl.us

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ALTERNATIVES PUBLIC MEETING
West Busch Boulevard (SR 580) Corridor Study
 From North Dale Mabry Highway to North Nebraska Avenue
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*All comments are part of the project record and are available for review by the public and the media.

Thank you for the opportunity to comment. As Tampa grows, our neighborhood suffers with infrastructure woes. It would be shameful to not go full force with a 6-lane alternative, and prep Forest Hills for the foreseeable future. A 4 or 5 lane alternative will be outgrown in short order and this effort wasted.

For neighbors, it will be the most painful to endure the construction of, but it is the best option for our community. - 6 Lanes!

As:

**NO PARALLEL PARKING ...
EVER!**

CONTACT INFORMATION (PLEASE PRINT)

Name Melissa Marison
 Title Resident
 Agency / Organization _____
 Phone Number _____
 Email marisons@hotmail.com

RETURN COMMENTS BY July 23, 2018 TO:

Brian L. Shroyer
 Multimodal Project Manager
 Florida Department of Transportation
 11201 N. McKinley Drive, MS 7-500
 Tampa, FL 33612
 Brian.Shroyer@dot.state.fl.us

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status.

Section 5

Local Coordination Meeting Summaries

Section 5.1

City Summary



Local Agency Coordination- City of Tampa

PREPARED BY: Cris Schooley (CH2M)
PROJECT: SR 580/Busch Blvd. from N. Dale Mabry Hwy. to N. Nebraska Ave. Hillsborough County
FPID 435908-1-22-01 (PD&E) and 435908-2-32-01 (Corridor Design)
MEETING DATE: April 12, 2018
MEETING TIME: 2:30 PM to 4:00 PM
LOCATION: 306 E. Jackson Street 6th Floor North Wing
ATTENDEES: See Sign-in

1. Introductions

Jenn Rhodes/CH2M began the meeting. Cris Schooley/CH2M, RRR PM Indike Ratnayake/FDOT, and Calvin Thornton/Tampa introduced themselves.

2. Presentation

Jenn and Cris presented the attached slides. Jenn, Cris, and Indike answered questions and responded to comments.

3. Discussion Items

- a. School access is a critical concern for the City. **Agree, multiple recommendations propose improving safe access to the schools along North Boulevard that are accessed by crossing Busch Blvd.**
- b. City is not necessarily opposed to showing on-street parking as an option between Armenia Ave and North Boulevard, but parking enforcement would be a concern. **We can discuss the possibility of on-street parking in the report and note that City controlled development changes would be needed to implement.**
- c. The future transit-rail corridor could potentially lead to Transit Oriented Development (TOD) along Busch Blvd. **We will review the documentation and note TOD potential as appropriate.**
- d. Area lighting, as opposed to just street lighting, may be better for Crime Prevention Through Environmental Design. That way there are less shadows between the sidewalk and adjacent buildings. Police prefer this as well. The City typically uses TECO to design, install, and maintain their lighting. TECO does not maintain lighting they don't design. City might prefer to lead the lighting project as a LAP.
- e. An additional signalized intersection at Rome is preferable to a pedestrian crossing with RFBs or a mid-block crossing.
- f. Signals were connected with copper communication lines within city limits. This may or may not still be the case. **Upcoming project is proposing fiber optic interconnection cables. Discussed that it is common for the City and County to trade signals so that logical corridors are interconnected.**

- g. A city overlay district might not be seen as feasible here in the short-term, but is being used in other parts of the city to improve the pedestrian environment in the right of way and in the adjacent development. In the long-term, can see the benefit of an overlay district to achieve the vision. Calvin stated he is interested in the Planning Commission staff's input on this idea. Ultimately, this recommendation would have to be coordinated with the City of Tampa planning staff at the City and Planning Commission.
- h. The CSX railroad creates a neighborhood barrier issue. Discussed children jumping the fence to cross the corridor. ***This is true but options are limited. City could explore a pedestrian underpass at Jones Ave.***
- i. MPO has a walk/bike improvement map with more detail. Specifically, MPO Bike/Walk Plan #2 on MPO website. ***MPO has indicated they will send us an updated bike plan.***
- j. Calvin stated that he is good with the FDOT showing all of the recommended improvements discussed during the meeting at the Public Alternatives Meeting and will take the input gathered back to the city. Final determination of which improvements the city will partner on implementing depend on the overall feedback and interest of the community.

4. Action Items

Task	Description	Responsible Person	Status	Comments

5. Next Scheduled Meeting: Project Advisory Group meeting #4 May 24, 2018. Time TBA.



LOCAL AGENCY COORDINATION MEETING

DRAFT AGENDA

West Busch Boulevard (SR 580) Corridor Study

from North Dale Mabry Highway to North Nebraska Avenue
Hillsborough County, Florida

FDOT Financial Project ID Number: 435908-1-22-01

601 E Kennedy Blvd, 18th floor, Tampa, FL, 33602

April 12, 2018

1. Introductions
2. Project Overview
3. Typical Section Alternatives
4. Inter-agency recommendations (Interim Design)
5. Other Recommendations

West Busch Boulevard (SR 580) Corridor Study

from North Dale Mabry Highway to North Nebraska Avenue

Hillsborough County, Florida

FDOT Financial Project ID Number: 435908-1-22-01

Local Agency Coordination Meeting



Purpose of this meeting

1. Discuss draft typical section alternatives
2. Gauge agency interest in recommendations for further study which would require inter-agency coordination (JPA, maintenance agreements, etc.)



Agenda

1. Introductions
2. Project Overview
3. Typical Section Alternatives
4. Inter-agency Recommendations (Interim Design)
5. Other Recommendations



Introductions

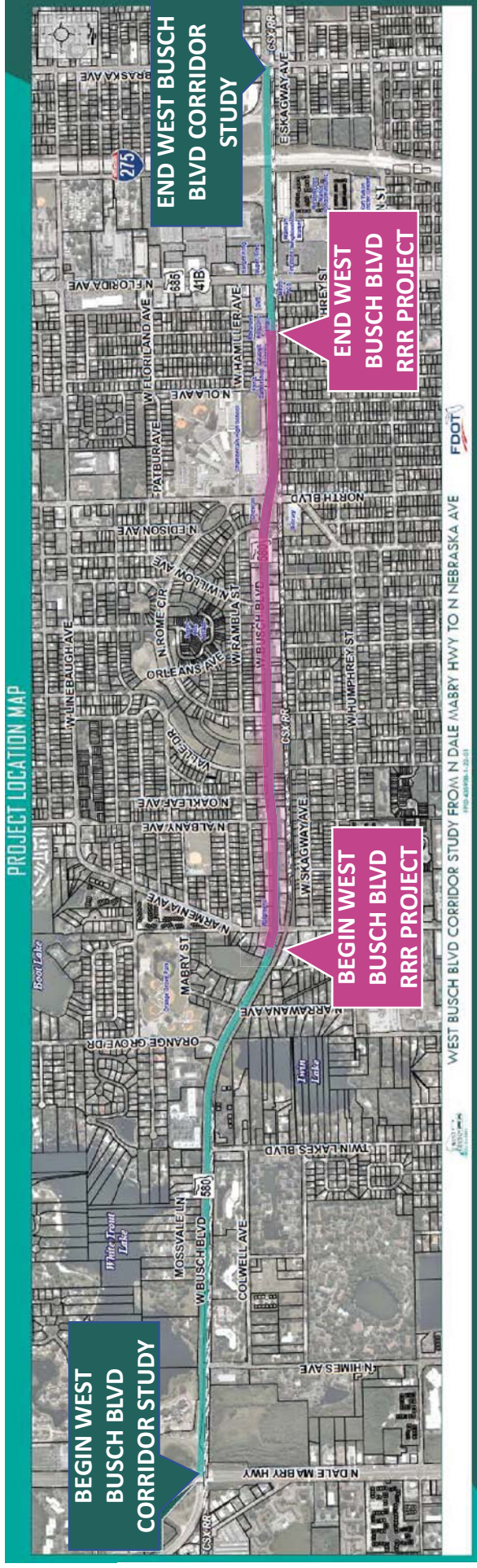


Project Overview



Project Study Area

- 3.3 miles
- Within the city of Tampa and unincorporated Hillsborough County
- Concurrent RRR project



Study Purpose

Achieve the Corridor Vision:

*“The Busch Boulevard corridor offers **safe, comfortable and convenient access through and across the corridor for all users and all travel modes.**”*

PD&E Study Changes
(implement typical section changes)

Interim Design Project Changes
(implement spot improvements)

Resurfacing, Restoration and Rehabilitation Project (3R) Changes

Busch Boulevard Corridor Study
(identify the vision, recommendations & implementation plan)

**We
Are
Here**

Updated Study Schedule

- Began in March 2017
- Held Agency Project Kick-off Meeting, 3 Project Advisory Group Meetings, & Public Visioning Workshop
- **Meet with local agencies to review draft alternatives and recommendations**
- PAG #4 – May 24, 2018
- Alternatives Public Meeting – June 2018
- Final Corridor Alternatives and Strategies Report – August 2018

Long Term Recommendations: Typical Section Alternatives

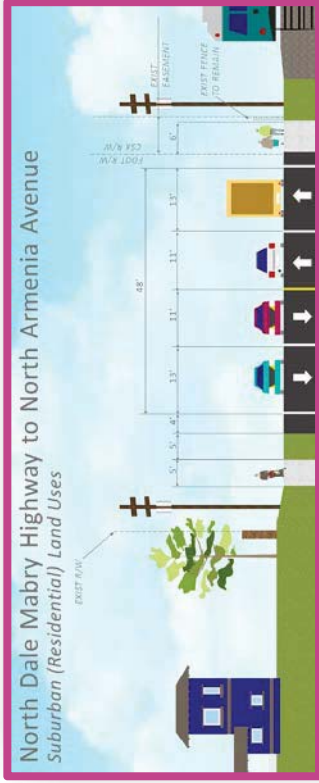


Currently Developing Range of Alternatives

Alternative	Description
No-Build	Existing condition
4-Lane	Construct 4-lane divided roadway without bike lanes, but with raised median, border width, and wide (8') sidewalk to accommodate additional pedestrian demand
5-Lane	Construct a 5-lane divided roadway with flush unrestricted median, bike/parking lanes, border width, 6' sidewalks
6-Lane	Construct a 6-lane divided roadway with raised median, bike lanes, border width, 6' sidewalks

- Many variations are possible, but these alternatives provide a good range to receive feedback and further refine the concepts for the corridor vision
- To be presented at the Public Alternatives Meeting
- PD&E Alternative(s) could be all, some, or a variation of these alternatives

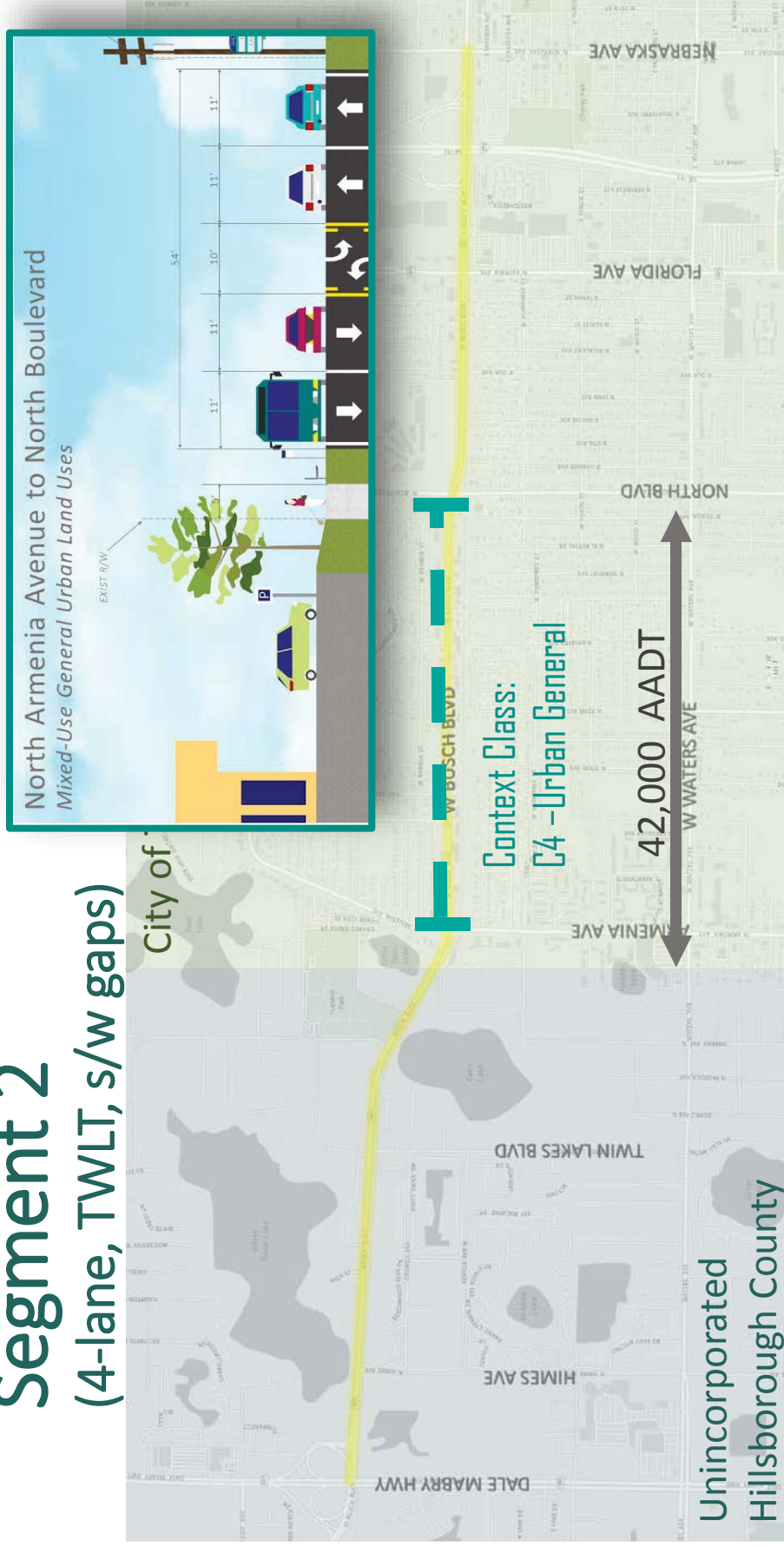
Existing Conditions (no build) Segment 1 (4-lane undivided w/sidewalk)



Existing Conditions (no build)

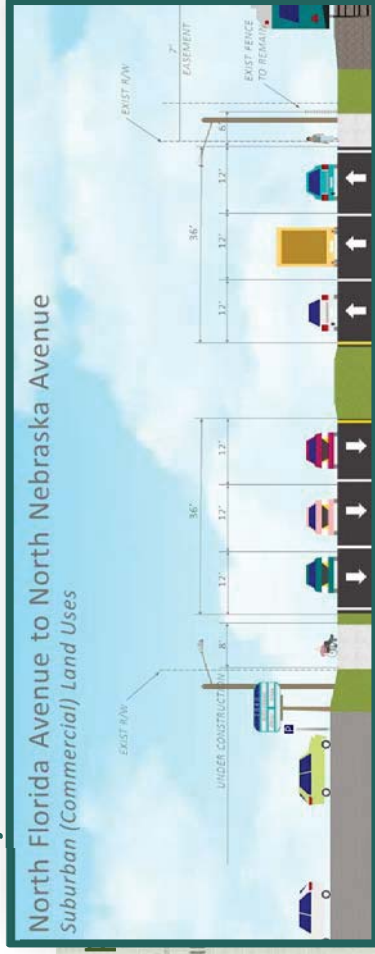
Segment 2

(4-lane, TWLI, s/w gaps)



Existing Conditions (no build)

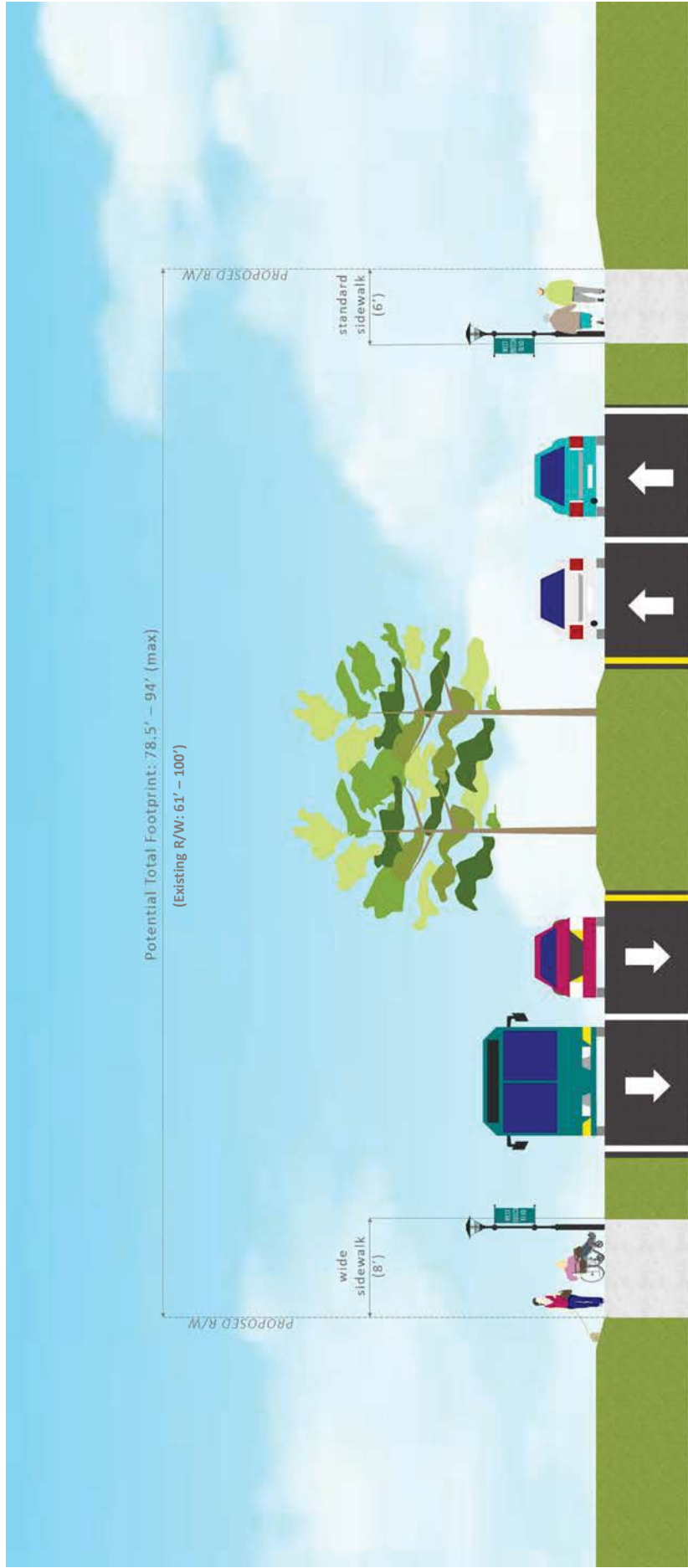
Segment 4 (6-lane, median)



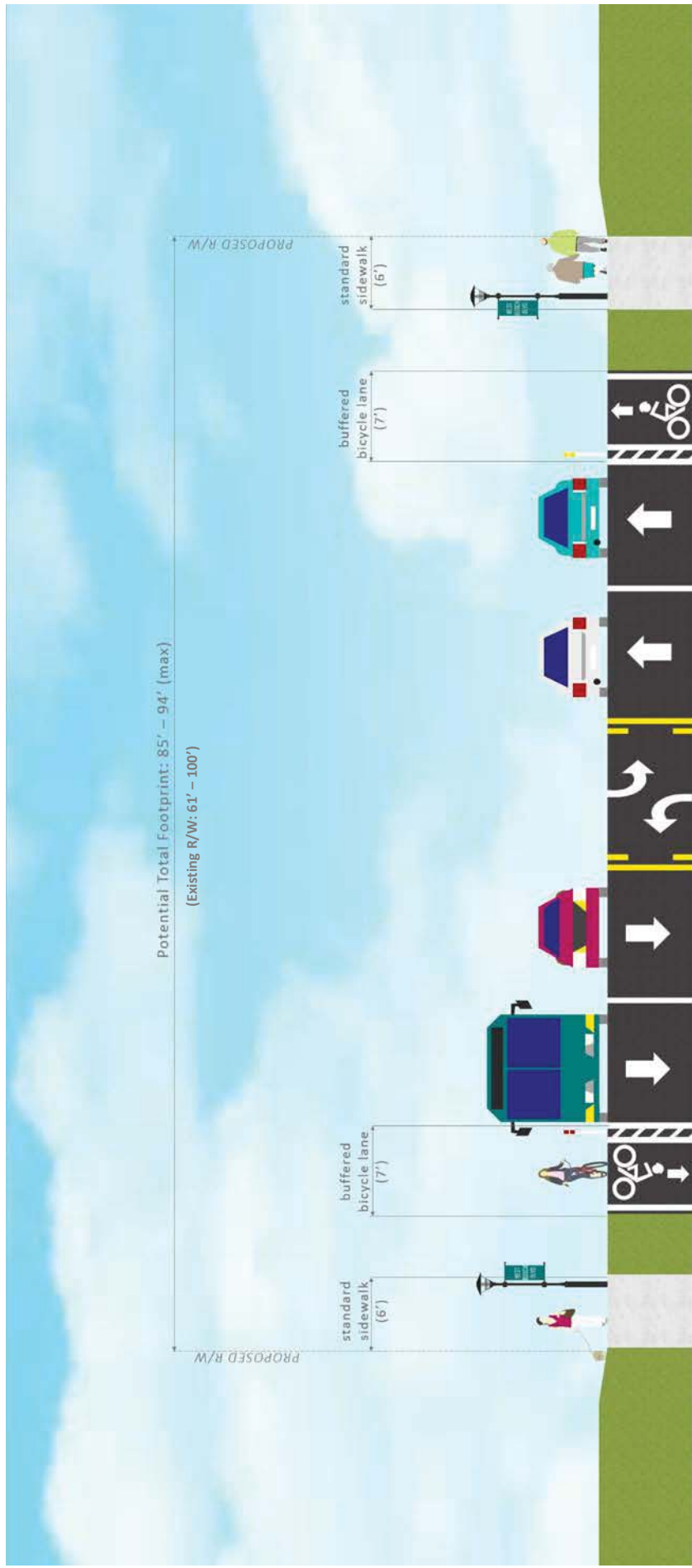
Context Class:
C3C -
Suburban
Commercial

49,000
AADT

Alternative 1: 4 Lanes with Median and Wide Sidewalk on the North Side



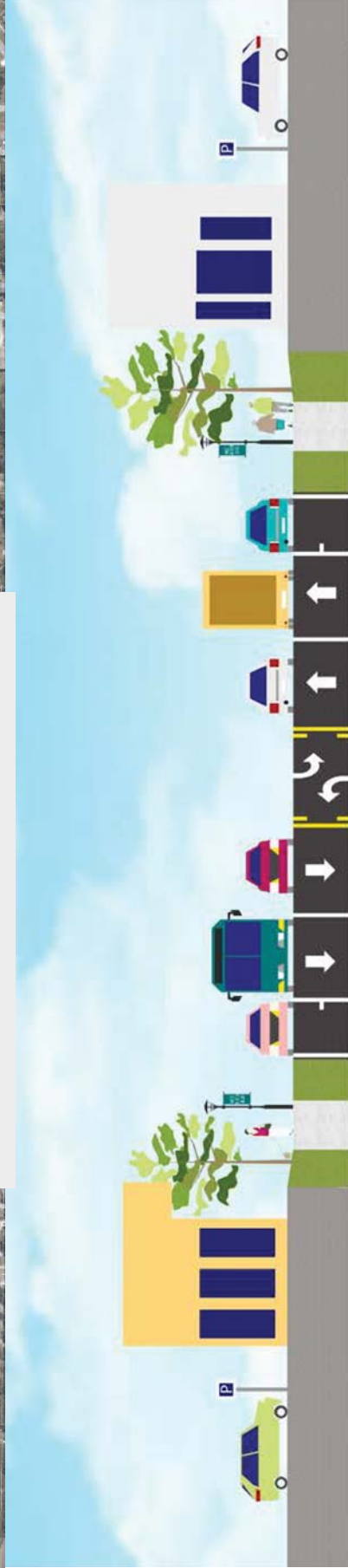
Alternative 2: 4 Lanes with Center Two-Way Left Turn Lane and Buffered Bike Lanes



Potential Parallel Parking Option for Segment 2 (North Armenia Ave. to North Blvd.)

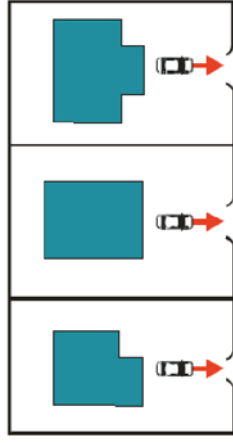


South side of Busch Boulevard
 466 off-street parking spaces existing
 332 on & off-street proposed
 29% reduction

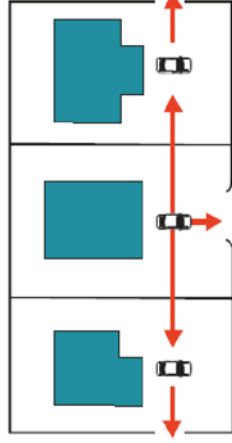


Width of parallel parking = buffered bike lane

City of Tampa code does not currently address cross/joint access and discourages commercial access from non-arterial streets (Sec. 27-283.12.(j)(1)).



Avoid



Promote

- Cross Access
- Joint Access
- Complete on-site circulation

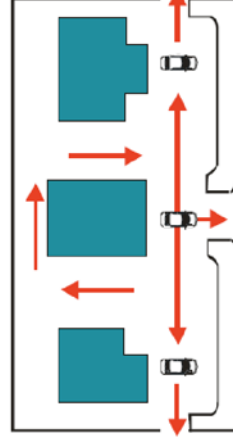


Image: Driveway Information Guide (FDOT 2008) p. 85

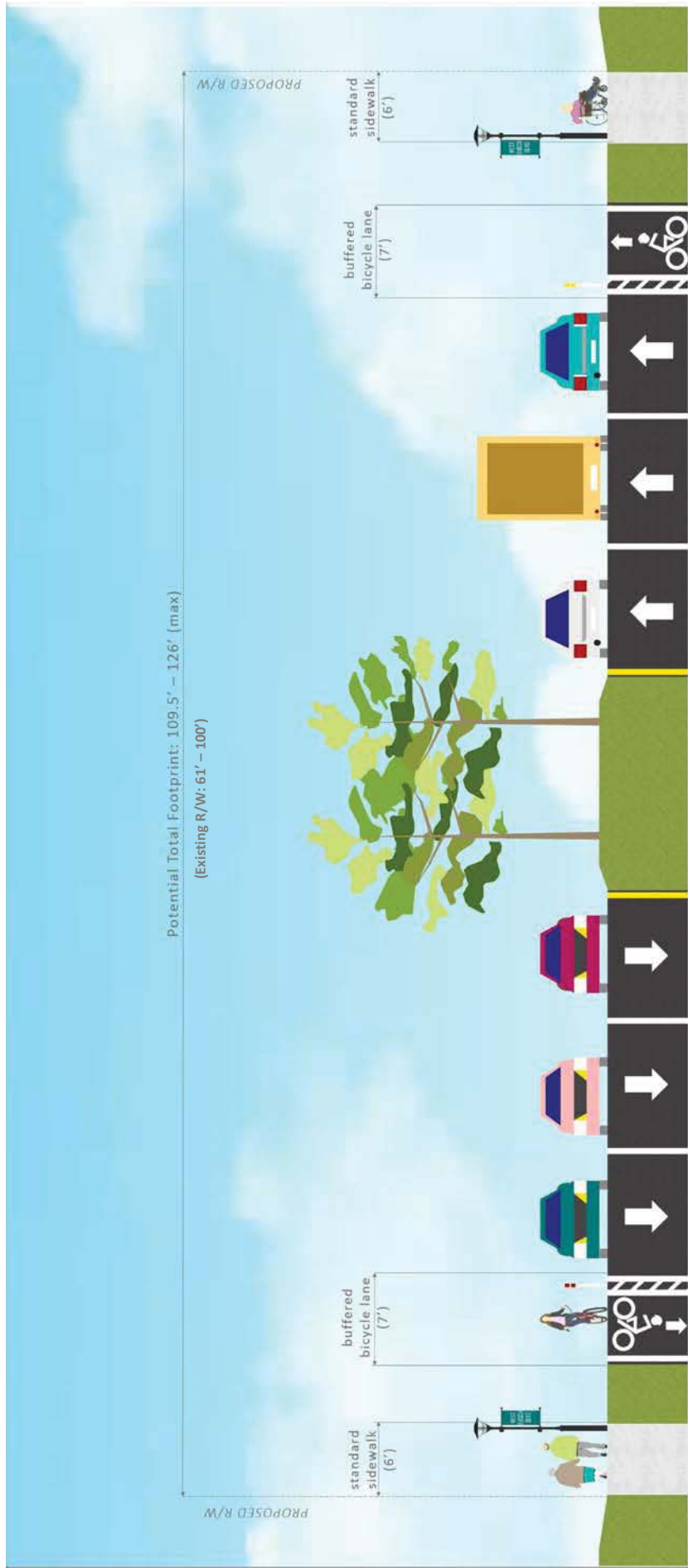
To address the public interest in these matters, land development regulations include conditions or circumstances where nonconforming access features may be brought into conformance. Such conditions may include:

- when new driveway permits are requested;
- an increase in land use intensity;
- substantial enlargements or improvements;
- significant change in trip generation; and
- as changes to roadway design allow. (p.1-8)

Overlay requirements may address any issues of concern, such as joint access, parking lot cross access, reverse frontage, driveway spacing, and limitations on new driveways. (p. 1-9)

Source: "Land Development and Subdivision Regulations that Support Access Management", Center for Urban Transportation Research, University of South Florida, January 1994. Williams, Kristine M. <https://www.cutr.usf.edu/oldpubs/pcm/files/Model%20Land%20Development%20and%20Subdivision%20Regulations%20that%20Support%20Access%20Management.pdf>

Alternative 3: 6 Lanes with Median and Buffered Bike Lanes

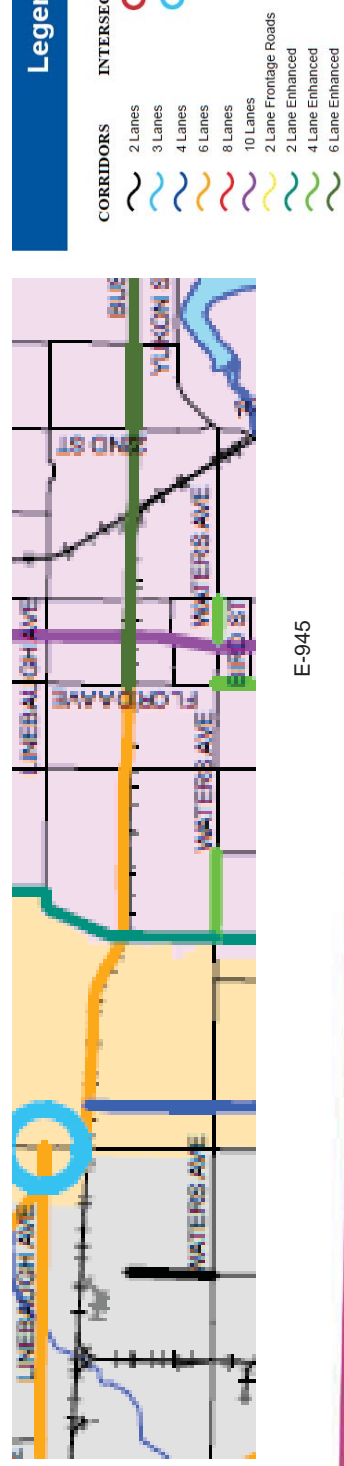


Planning Consistency – 6 Lane Alternative

- 2040 LRTP
 - ≈ Need for 6/7 lanes on “Beyond 2040” Map (pg. 98) from Armenia Blvd. to North Blvd.



- Tampa Comprehensive Plan (effective date February 20, 2016)
 - Constrained Roadways (physical constraints)
 - ≈ Busch Blvd. from Dale Mabry Blvd. to North Blvd. constrained to 4 lanes
 - ≈ “Page 151; MBY (Mobility) Policy 3.2.2: Constrained roadways... shall be eligible for... Dedicated transit lanes...”
- Hillsborough County Corridor Preservation Plan (Map 25) shows preserved for 6 lanes/ 6 lanes enhanced through study limit



Inter-agency Recommendations (Interim Design)



Interim Design Project Recommendations

- Safety improvements at Dale Mabry off-ramp
- Turn lane extensions as feasible
- Complete sidewalk gaps
- Add signal and/or mid-block crossing at Rome Circle or Willow Ave.
- Leading pedestrian interval or ped-only phase at North Blvd.
- Add refuge islands or tighten curb radius at North Florida Ave. and North Nebraska Ave.
- Add YOUR SPEED radar sign(s)

Potential additional improvements with local agreements include...

Corridorwide Recommendations



- Add pedestrian lighting

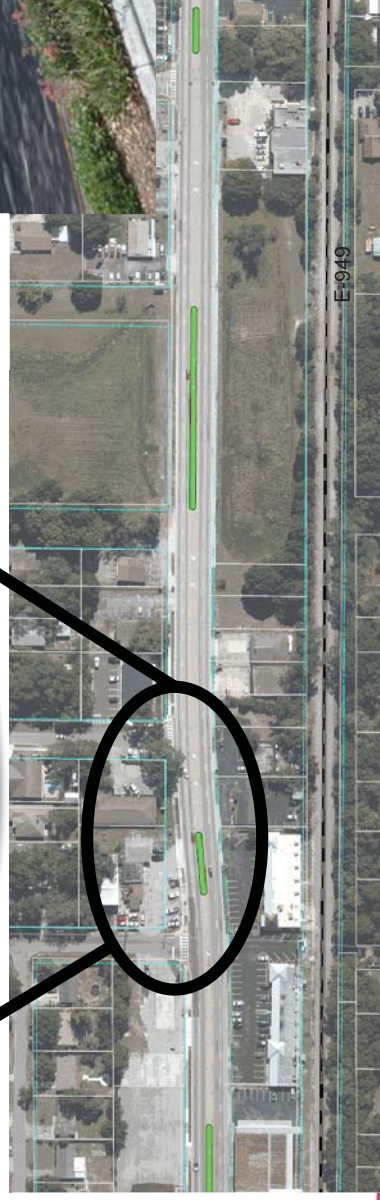
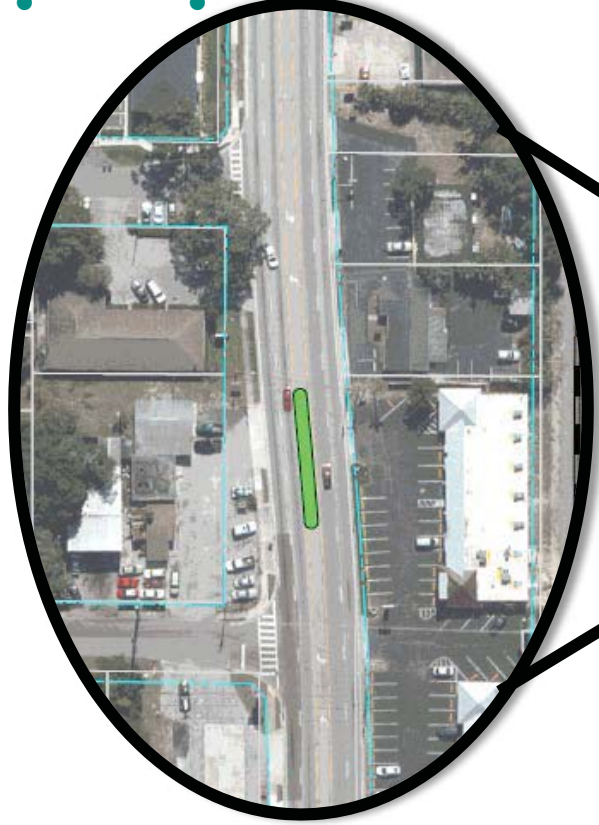
- Interconnect signals to increase platooning
 - ≈ Traffic signal speed signs
 - ≈ Explore potential of dynamic signal timing and/or setting signals to a 35 mph target speed
 - ≈ City and county signals are not interconnected



*Traffic Signal Speed Sign
MUTCD, Section 2H.03 Traffic Signal Speed
Sign (11-1)*

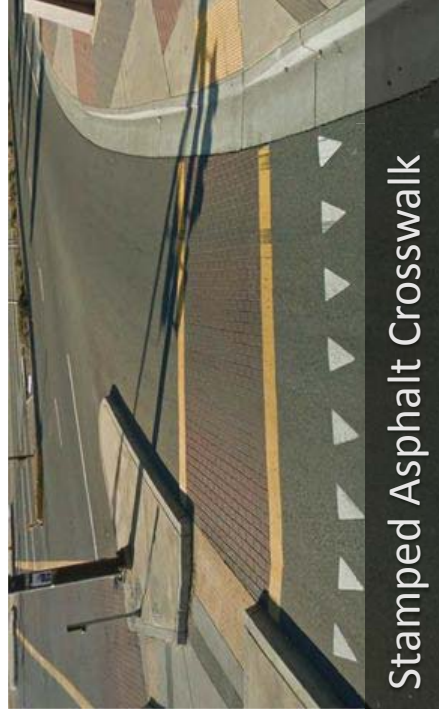
Florida Friendly/ Pedestrian Deterrent Landscaping in Opportunity Areas

- Opportunity areas defined where:
 - **At least one existing driveway per property accessible via left turn**
 - **At least 50-ft. of turn lane** provided in advance of the driveway



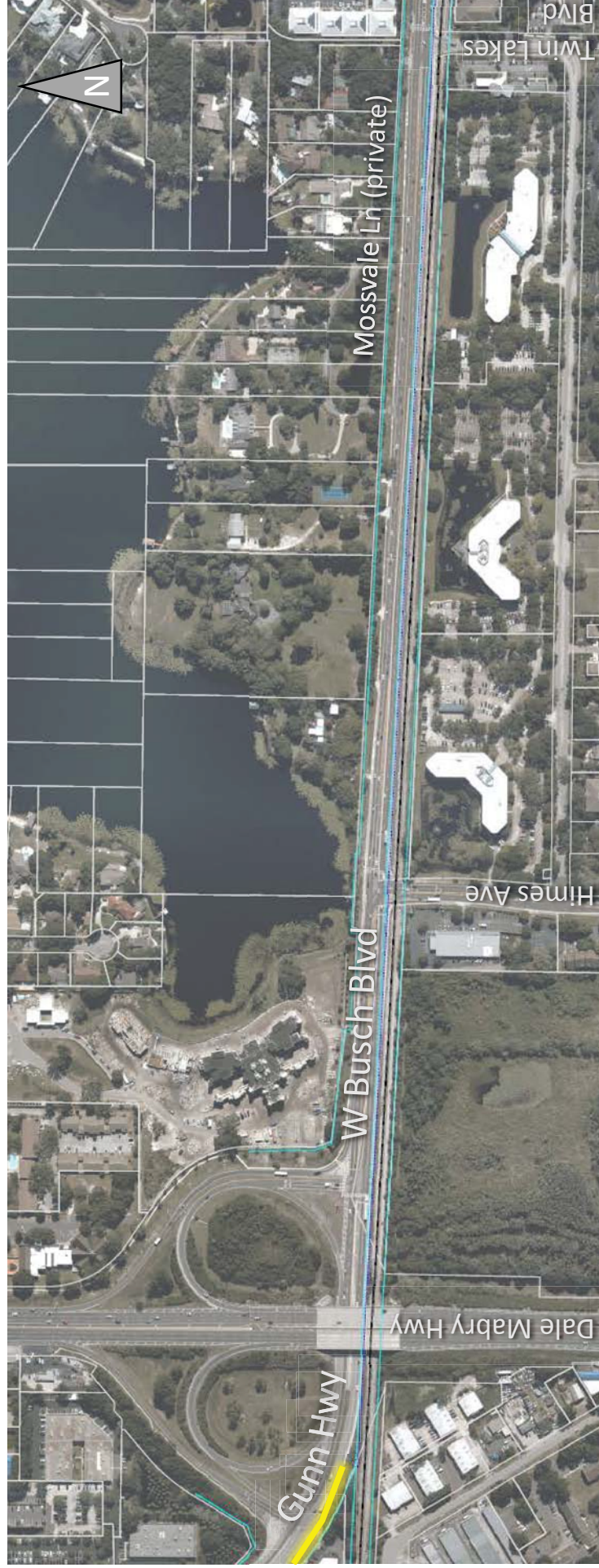
Example: 13th Street, Gainesville, FL

High-emphasis pedestrian crossings / hardscape intersections



Gunn Highway Sidewalk Gap

- County jurisdiction



Mossvale Lane Extension + Intersection

- County land-use jurisdiction

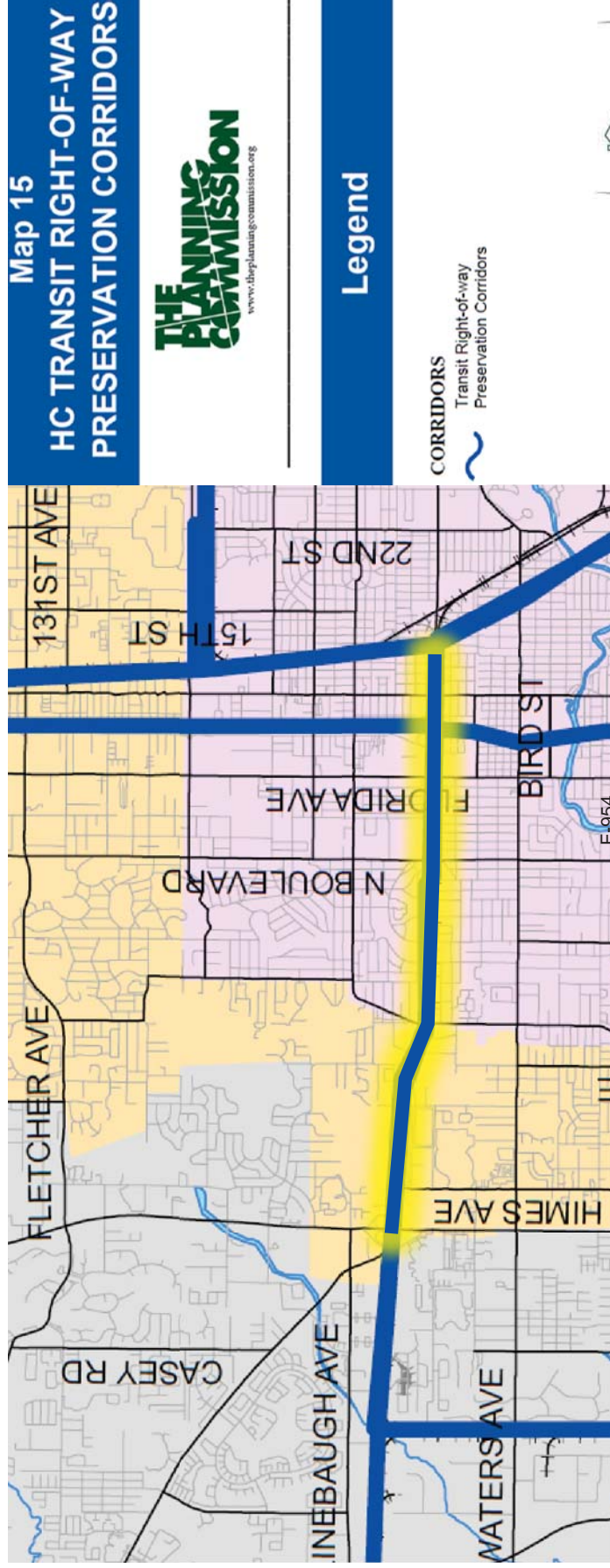


Other Recommendations



Recommendations related to future transit/rail plans

- The CSX line runs parallel to the corridor along the south for most of the study limits
- **Are there any plans related to transit or the rail corridor that should be documented in this report?**



Bicycle Master Plan/ Bicycle Network

- Overall regional bicycle connectivity benefits are limited
- Bicycle routes defined at a regional level would help support the potential future investment of bicycle lanes on Busch Boulevard
- Parallel route to Busch Blvd. may be preferable
- MPO/County should lead

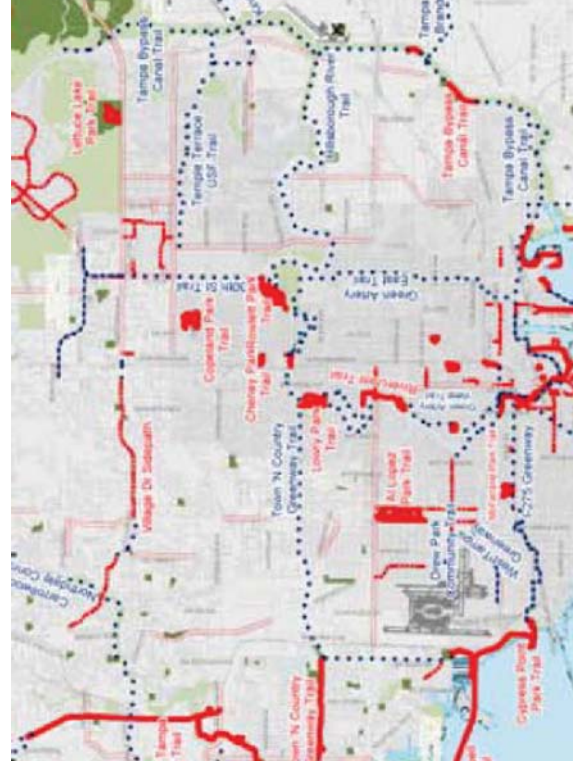


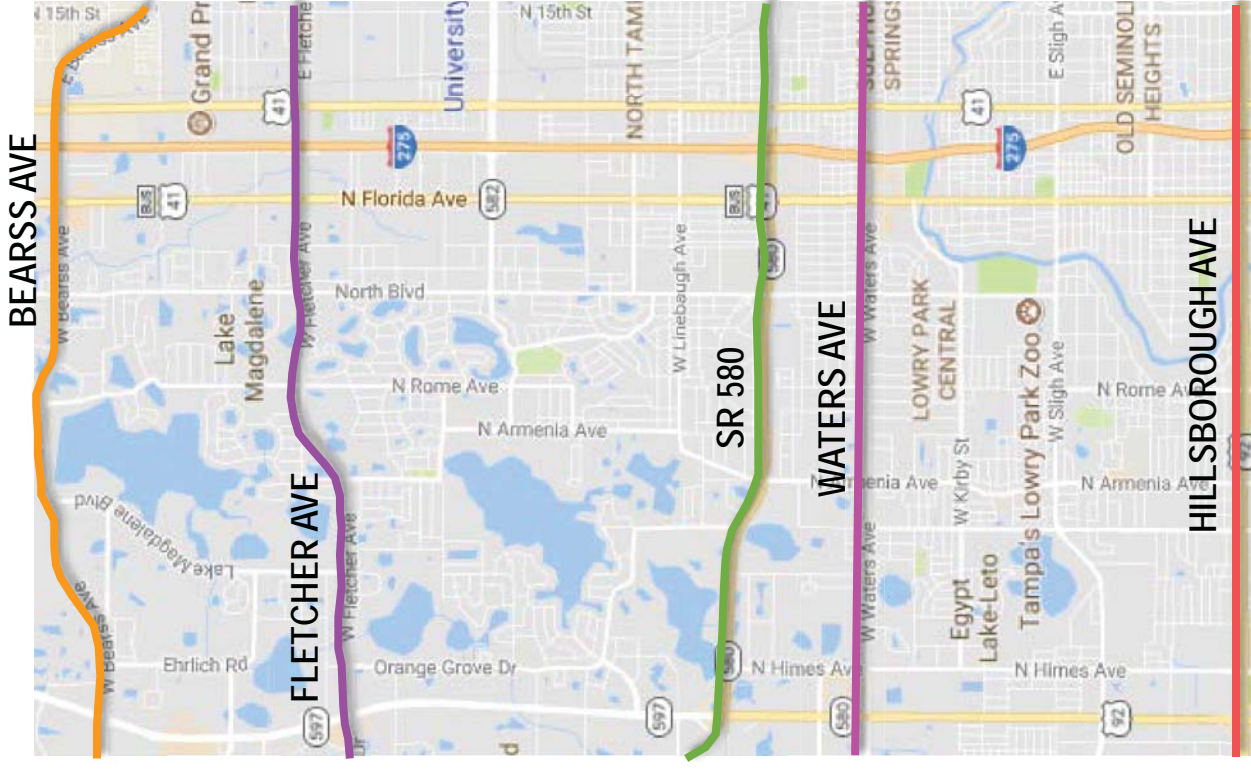
Figure 3-23
Trails/Sidepaths Planned and Potential Corridors Map



E-957

Regional origin/destination planning study

- To study the perceived "through traffic" along the corridor between Dale Mabry Hwy and I-275
- Limited east-west corridors and existing congestion suggest the opportunity to address regional travel flows at a more regional level than within the limits of this corridor study
- MPO or FDOT should lead



Corridor	Lanes	AADT	Notes
Bearss Avenue	4-lane	54,000	3.8 miles north
Fletcher Avenue	4-lane	23,500	Limited western connectivity
SR 580	4-lane	50,000	
Waters Avenue	4-lane	27,000	Limited eastern connectivity
Hillsborough Avenue	6-lane	51,500	2.6 miles south

Section 5.2

County Summary



Local Agency Coordination- Hillsborough County

PREPARED BY: Cris Schooley (CH2M)

PROJECT: SR 580/Busch Blvd. from N. Dale Mabry Hwy. to N. Nebraska Ave. Hillsborough County
FPID 435908-1-22-01 (PD&E) and 435908-2-32-01 (Corridor Design)

MEETING DATE: April 12, 2018

MEETING TIME: 1:20 PM to 2:10 PM

LOCATION: 22FL-MtgRm-2202E-Lobby-30cap (County Center - 601 East Kennedy Blvd.)

ATTENDEES: See Sign-in

1. Introductions

Jenn Rhodes/CH2M began the meeting. Cris Schooley/CH2M, RRR PM Indike Ratnayake/FDOT, and the other attendees introduced themselves.

2. Presentation

Jenn and Cris presented the attached slides. Jenn, Cris, and Indike answered questions and responded to comments.

3. Discussion Items

- a. Why is the wide sidewalk on the north side and not the south? ***The north side has more destinations and higher use because of the railroad adjacent to the south for most of the project length.***
- b. The county would be supportive of corridor lighting for roadway, intersection, and pedestrians if it addresses a needed reduction in crashes. ***Crashes will be documented along with potential benefits of the proposed improvements.***
- c. The county would be supportive of corridor landscaping if it addresses a needed reduction in crashes. ***Crashes will be documented along with potential benefits of the proposed improvements.***
- d. The county would be interested in a joint project agreement/local agency program to complete the Gunn Highway sidewalks (in County jurisdiction) as part of the upcoming FDOT design project.
- e. The Mossvale lane extension proposal, though a good idea, may be too late. Plats for the subdivision have already been approved.

4. Action Items

Task	Description	Responsible Person	Status	Comments

5. **Next Scheduled Meeting:** Project Advisory Group meeting #4 May 24, 2018. Time TBA.



LOCAL AGENCY COORDINATION MEETING

DRAFT AGENDA

West Busch Boulevard (SR 580) Corridor Study

from North Dale Mabry Highway to North Nebraska Avenue
Hillsborough County, Florida

FDOT Financial Project ID Number: 435908-1-22-01

601 E Kennedy Blvd, 18th floor, Tampa, FL, 33602

April 12, 2018

1. Introductions
2. Project Overview
3. Typical Section Alternatives
4. Inter-agency recommendations (Interim Design)
5. Other Recommendations

West Busch Boulevard (SR 580) Corridor Study

from North Dale Mabry Highway to North Nebraska Avenue

Hillsborough County, Florida

FDOT Financial Project ID Number: 435908-1-22-01

Local Agency Coordination Meeting



Purpose of this meeting

1. Discuss draft typical section alternatives
2. Gauge agency interest in recommendations for further study which would require inter-agency coordination (JPA, maintenance agreements, etc.)



Agenda

1. Introductions
2. Project Overview
3. Typical Section Alternatives
4. Inter-agency Recommendations (Interim Design)
5. Other Recommendations



Introductions

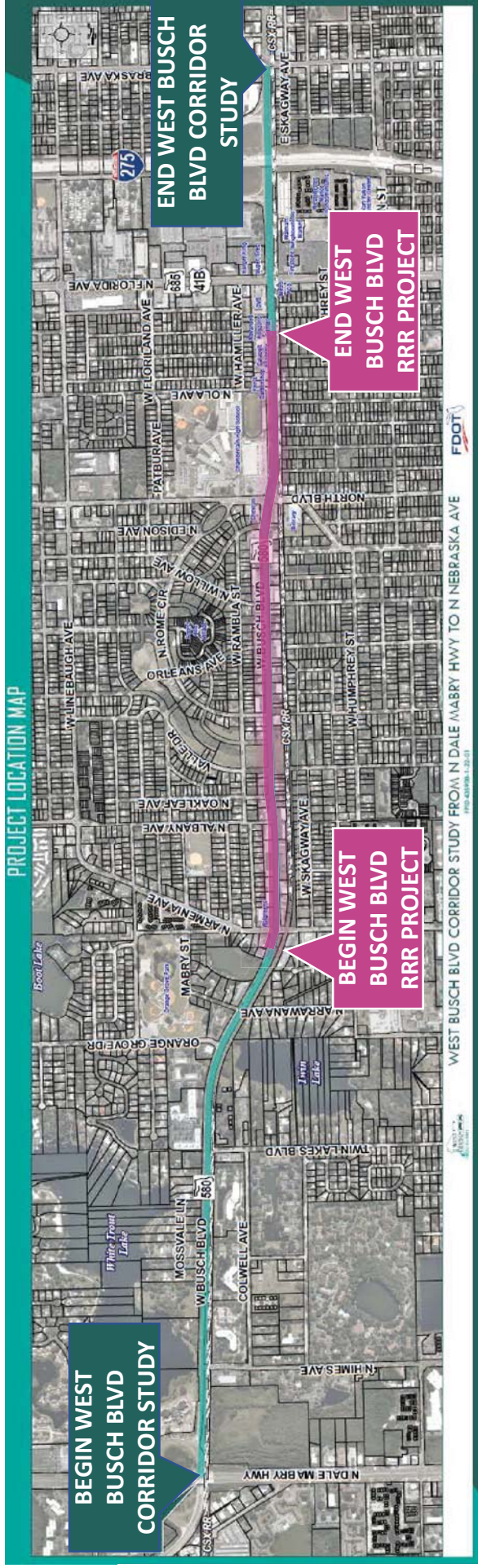


Project Overview



Project Study Area

- 3.3 miles
- Within the city of Tampa and unincorporated Hillsborough County
- Concurrent RRR project



Study Purpose

Achieve the Corridor Vision:

*“The Busch Boulevard corridor offers **safe, comfortable and convenient access through and across the corridor for all users and all travel modes.**”*

PD&E Study Changes
(implement typical section changes)

Interim Design Project Changes
(implement spot improvements)

Resurfacing, Restoration and Rehabilitation Project (3R) Changes

Busch Boulevard Corridor Study
(identify the vision, recommendations & implementation plan)

**We
Are
Here**

Updated Study Schedule

- Began in March 2017
- Held Agency Project Kick-off Meeting, 3 Project Advisory Group Meetings, & Public Visioning Workshop
- **Meet with local agencies to review draft alternatives and recommendations**
- PAG #4 – May 24, 2018
- Alternatives Public Meeting – June 2018
- Final Corridor Alternatives and Strategies Report – August 2018

Long Term Recommendations: Typical Section Alternatives



Currently Developing Range of Alternatives

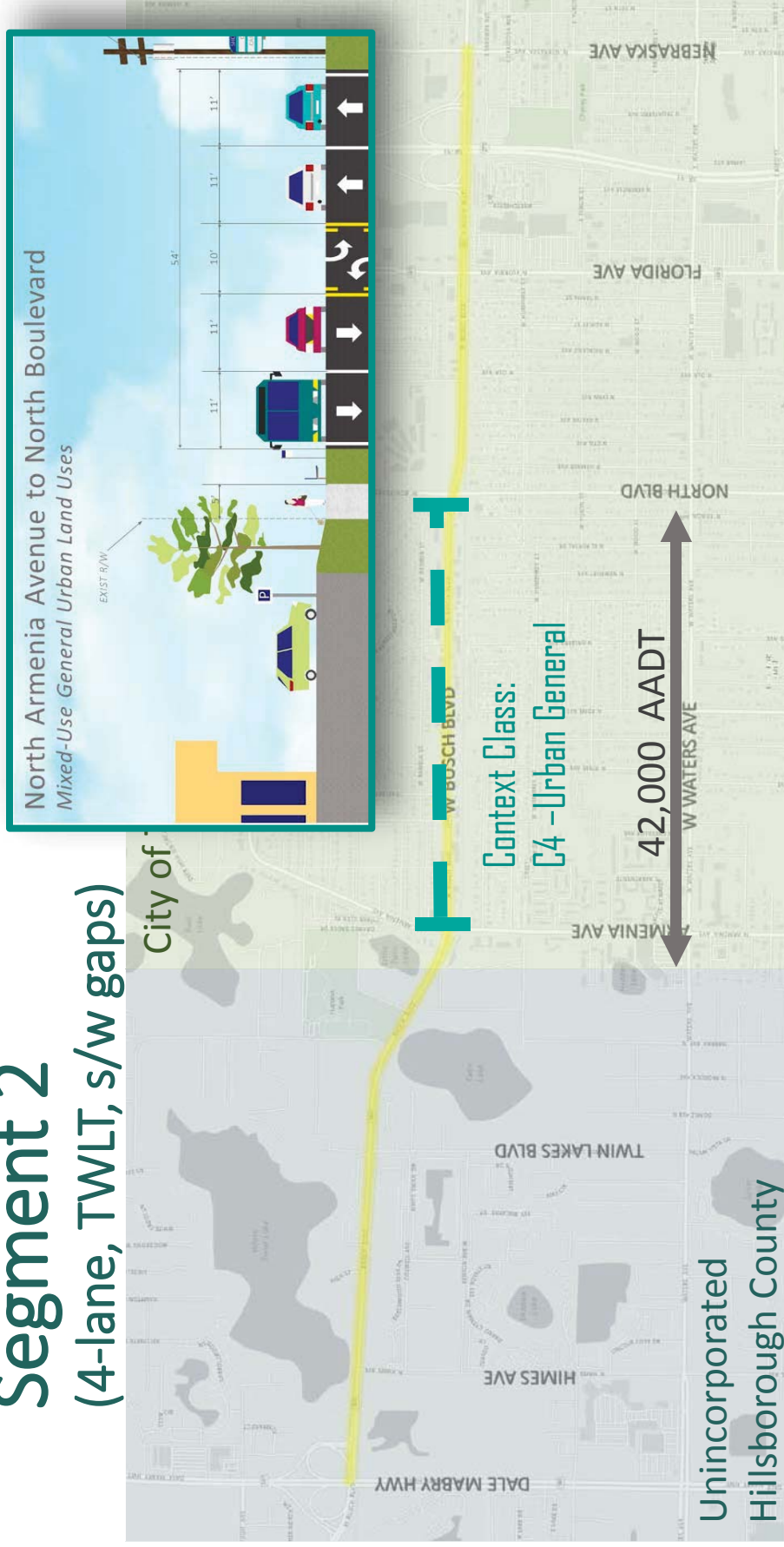
Alternative	Description
No-Build	Existing condition
4-Lane	Construct 4-lane divided roadway without bike lanes, but with raised median, border width, and wide (8') sidewalk to accommodate additional pedestrian demand
5-Lane	Construct a 5-lane divided roadway with flush unrestricted median, bike/parking lanes, border width, 6' sidewalks
6-Lane	Construct a 6-lane divided roadway with raised median, bike lanes, border width, 6' sidewalks

- Many variations are possible, but these alternatives provide a good range to receive feedback and further refine the concepts for the corridor vision
- To be presented at the Public Alternatives Meeting
- PD&E Alternative(s) could be all, some, or a variation of these alternatives

Existing Conditions (no build)

Segment 2

(4-lane, TWLI, s/w gaps)

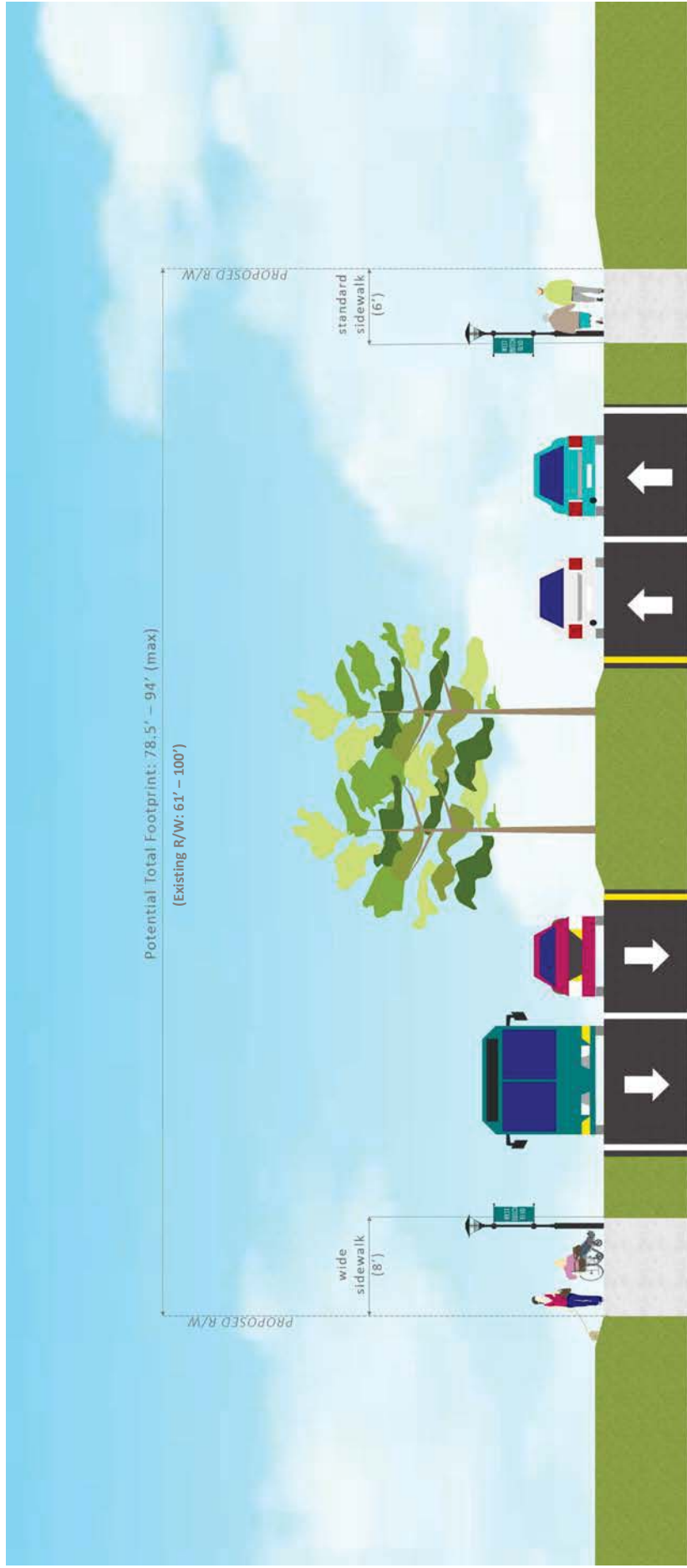


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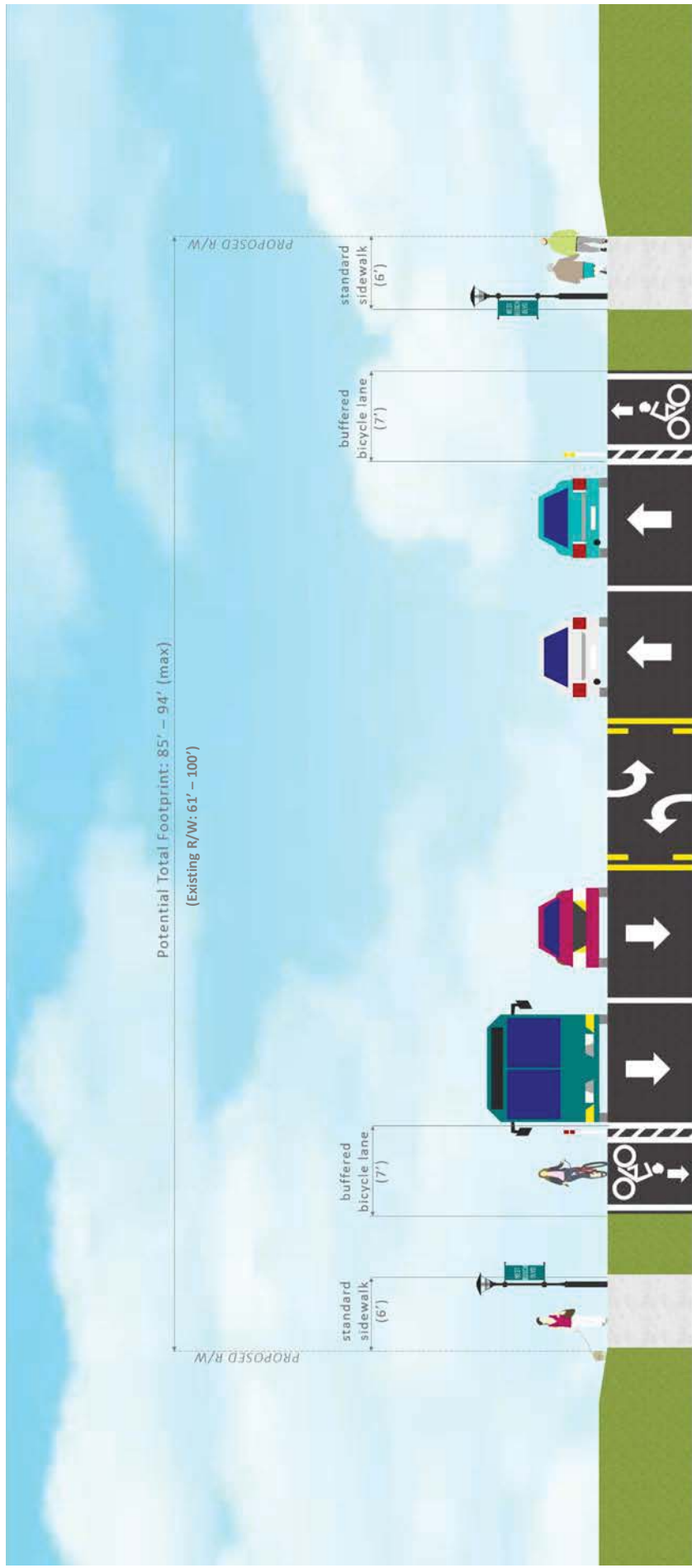
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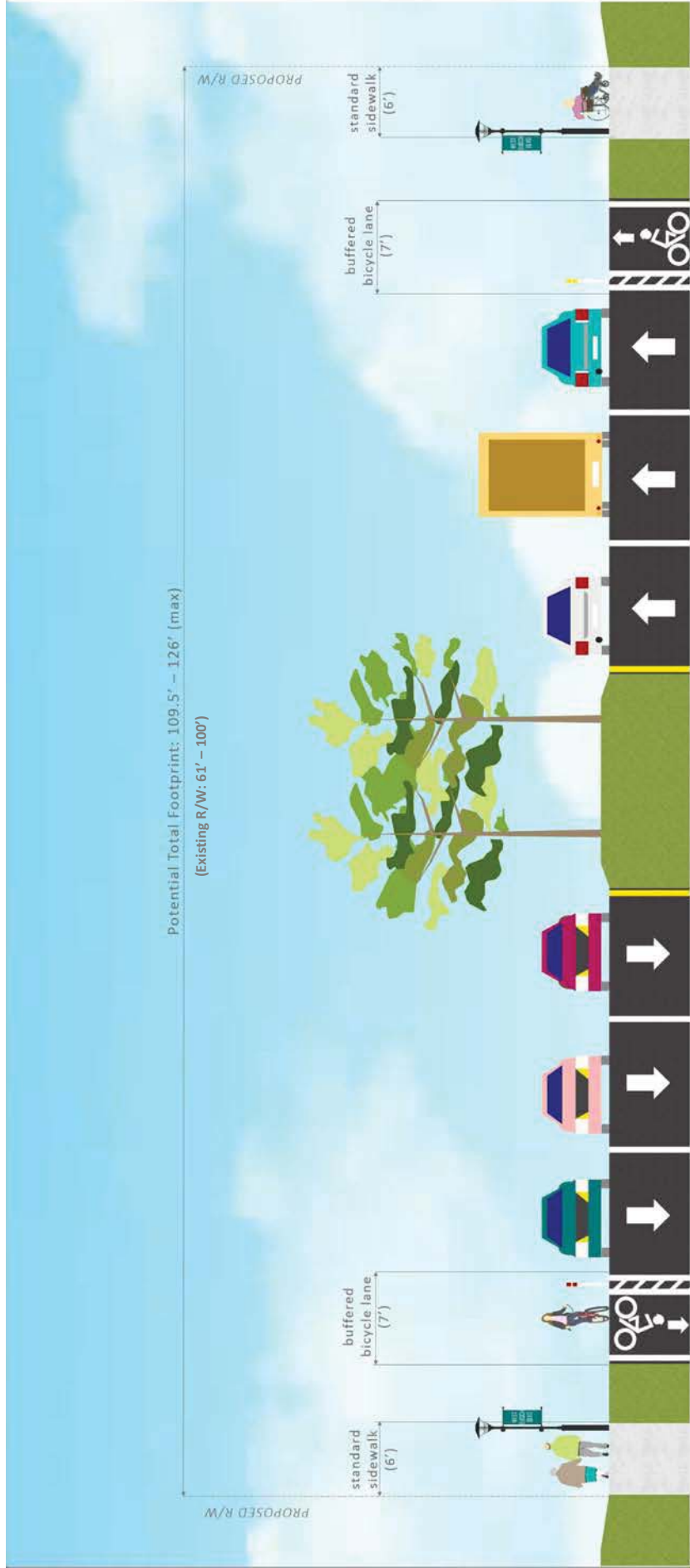
Alternative 1: 4 Lanes with Median and Wide Sidewalk on the North Side



Alternative 2: 4 Lanes with Center Two-Way Left Turn Lane and Buffered Bike Lanes



Alternative 3: 6 Lanes with Median and Buffered Bike Lanes



Inter-agency Recommendations (Interim Design)



Interim Design Project Recommendations

- Safety improvements at Dale Mabry off-ramp
- Turn lane extensions as feasible
- Complete sidewalk gaps
- Add signal and/or mid-block crossing at Rome Circle or Willow Ave.
- Leading pedestrian interval or ped-only phase at North Blvd.
- Add refuge islands or tighten curb radius at North Florida Ave. and North Nebraska Ave.
- Add YOUR SPEED radar sign(s)

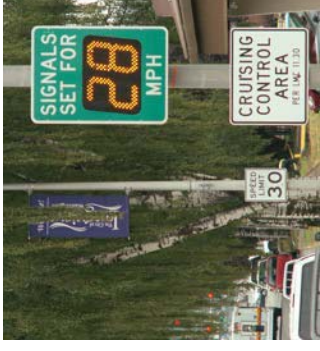
Potential additional improvements with local agreements include...

Corridorwide Recommendations



- Add pedestrian lighting

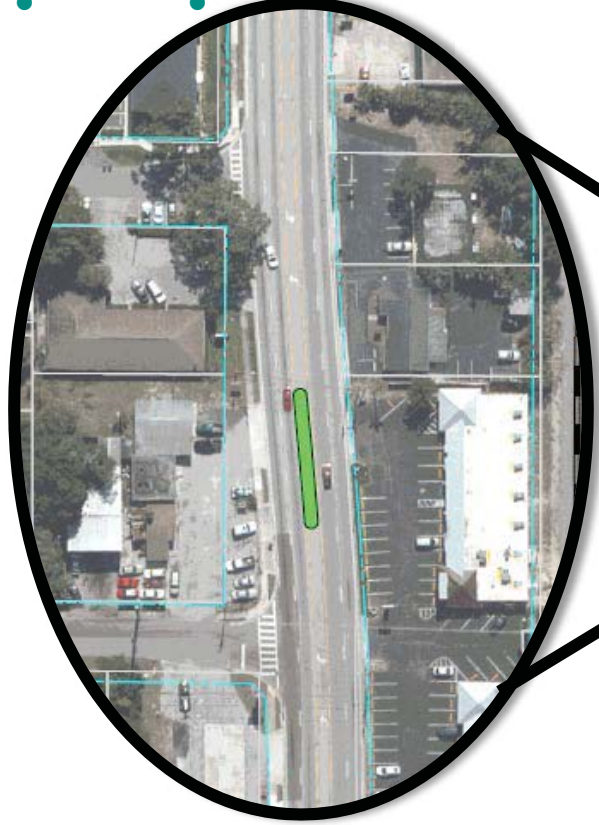
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 - ≈ Traffic signal speed signs
 - ≈ Explore potential of dynamic signal timing and/or setting signals to a 35 mph target speed
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*Traffic Signal Speed Sign
MUTCD, Section 2H.03 Traffic Signal Speed
Sign (11-1)*

Florida Friendly/ Pedestrian Deterrent Landscaping in Opportunity Areas

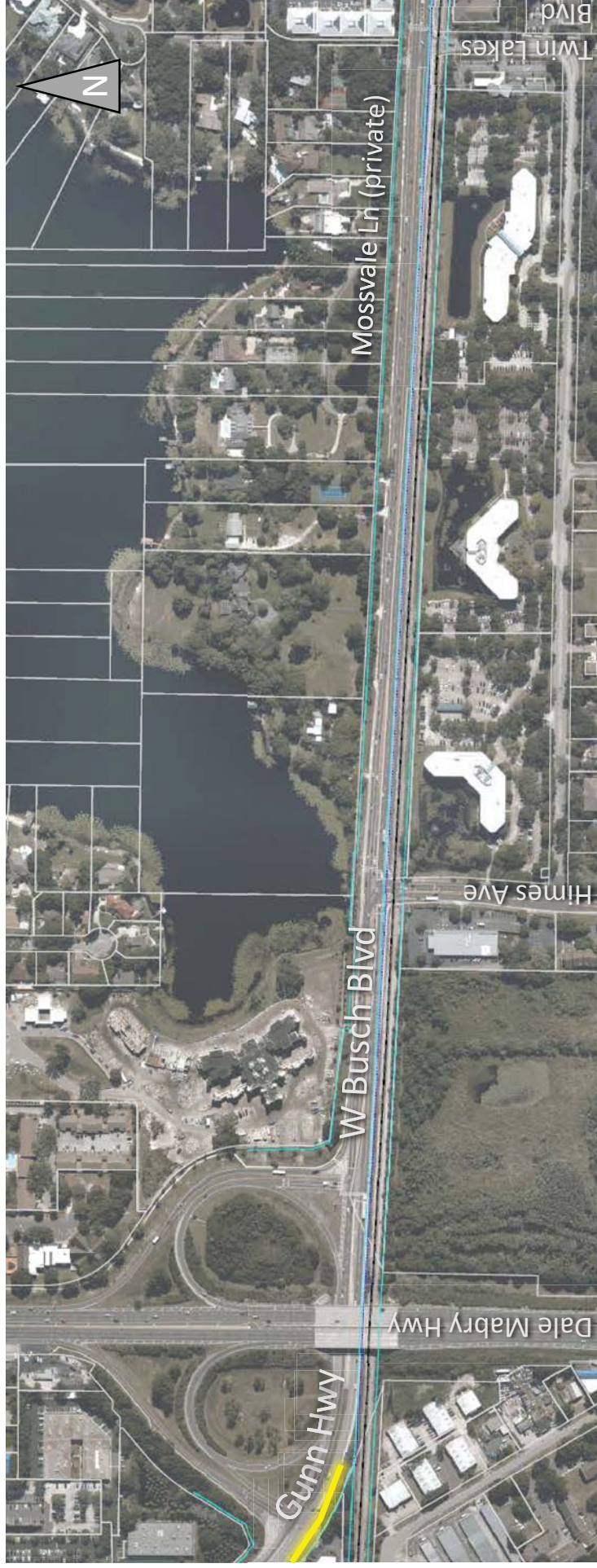
- Opportunity areas defined where:
 - **At least one existing driveway per property accessible via left turn**
 - **At least 50-ft. of turn lane** provided in advance of the driveway



Example: 13th Street, Gainesville, FL

Gunn Highway Sidewalk Gap

- County jurisdiction



Mossvale Lane Extension + Intersection

- County land-use jurisdiction

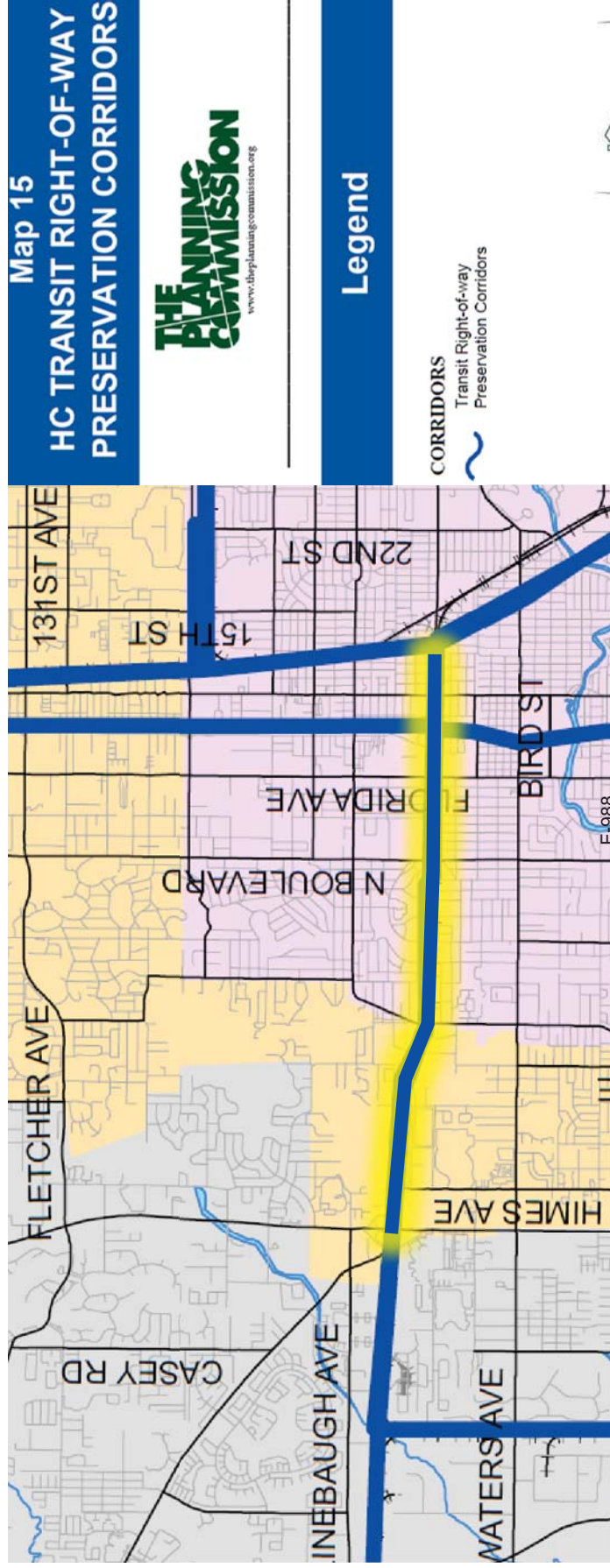


Other Recommendations



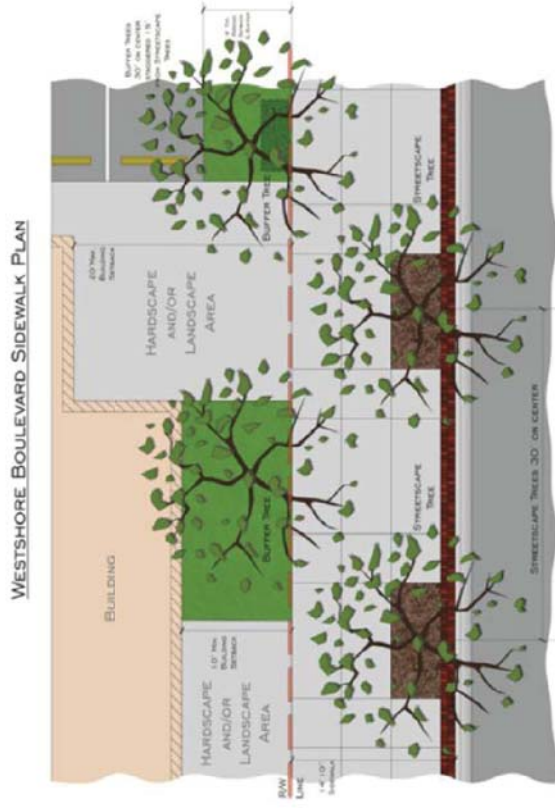
Recommendations related to future transit/rail plans

- The CSX line runs parallel to the corridor along the south for most of the study limits
- **Are there any plans related to transit or the rail corridor that should be documented in this report?**



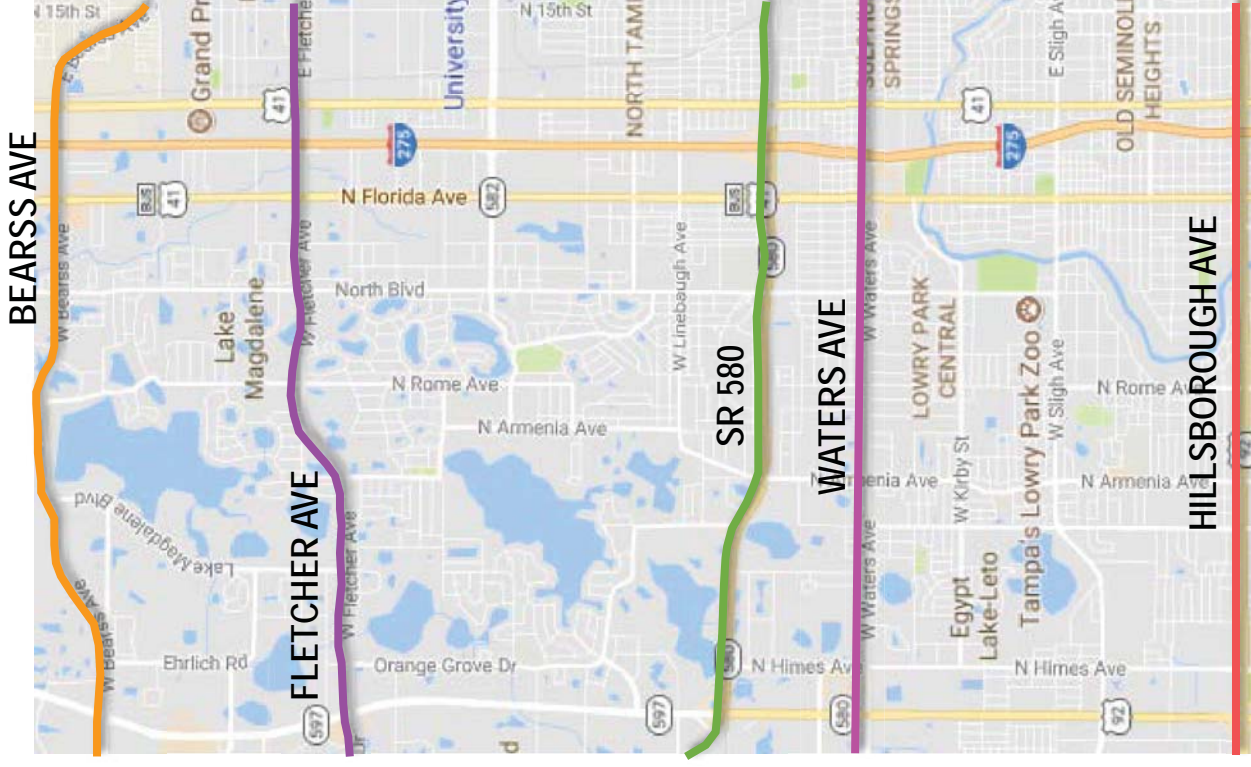
Speed Management along Corridor

- Recommending further study of lowering the speed limit
- Recommend school safety study to explore school zone designation
- Speed reduction can be bolstered by:
 - ≈ Urban form changes
 - ≈ Increasing police enforcement
 - ≈ Parallel parking (requires speed limit of 35 mph or less)
 - ≈ Landscaping
- Development/redevelopment requirement to provide landscaping along sidewalk can calm traffic and improve pedestrian environment



Regional origin/destination planning study

- To study the perceived "through traffic" along the corridor between Dale Mabry Hwy and I-275
- Limited east-west corridors and existing congestion suggest the opportunity to address regional travel flows at a more regional level than within the limits of this corridor study
- MPO or FDOT should lead



Corridor	Lanes	AADT	Notes
Bearss Avenue	4-lane	54,000	3.8 miles north
Fletcher Avenue	4-lane	23,500	Limited western connectivity
SR 580	4-lane	50,000	
Waters Avenue	4-lane	27,000	Limited eastern connectivity
Hillsborough Avenue	6-lane	51,500	2.6 miles south

Section 5.3

MPO Summary



Local Agency Coordination- Hillsborough Planning Commission and MPO

PREPARED BY: Cris Schooley (CH2M)
PROJECT: SR 580/Busch Blvd. from N. Dale Mabry Hwy. to N. Nebraska Ave. Hillsborough County
FPID 435908-1-22-01 (PD&E) and 435908-2-32-01 (Corridor Design)
MEETING DATE: April 12, 2018
MEETING TIME: 11:00 AM to 12:00 PM
LOCATION: Plan Hillsborough Room (County Center - 601 East Kennedy Blvd., 18th Floor)
ATTENDEES: See Sign-in

1. Introductions

Jenn Rhodes/CH2M began the meeting, introduced Cris Schooley/CH2M and RRR PM Indike Ratnayake/FDOT. Other attendees introduced themselves. Attendees represented planning staff for the city of Tampa, planning staff for Hillsborough County, and MPO staff.

2. Presentation

Jenn and Cris presented the attached slides. Jenn, Cris, and Indike answered questions and responded to comments.

3. Discussion Items

- a. The MPO committee schedule is available. Multiple committees would like to have a presentation on the study recommendations.
- b. What is a RRR? **Resurfacing, Restoration, and Rehabilitation project. Mostly maintenance items with limited other improvements.**
- c. Will the access changes (median) be shown to the public? **Yes, the alternatives will be drawn out in plan view for the Alternatives Public Meeting. Any access changes would require more study and a public hearing.**
- d. Is the heat island effect from adding more pavement being evaluated? **Heat is not being directly evaluated, but indirectly the benefits of reducing crossing distance and adding landscaping will also help reduce heat. We will make a note of this in the report.**
- e. Do the alternatives include relocating utilities underground? **Utility relocations have not been evaluated at this time. While relocating overhead utilities underground increases the potential for tree canopy, the poles for street lighting would remain. The future typical sections will show utility/light poles at the alternatives public meeting.**
- f. The CVS at Florida Ave. and Busch Blvd has a significant number of HART boardings; consider adding a shelter and/or bus pull-out. **HART coordination is ongoing.**
- g. Are you considering new pedestrian crossings? **Yes, an additional signalized or unsignalized crossing between Armenia Ave. and North Boulevard is a potential spot improvement proceeding to design.**

h. Is there a recommendation for the left turns at Ola Ave. being considered? **Yes, as part of the design improvement recommendations, a small raised median with a “No-Left Turn” at Ola Ave. is included.**

i. What does the Tampa Comprehensive Plan roadway constraint mean? **The MBY Table: Constrained Roadways on page 159 of the Imagine 2040: Tampa Comprehensive Plan lists Busch Blvd from Dale Mabry to Boulevard as 4 Lane Undivided with physical constraints. However, MBY Policy 3.3.2 states:**

“Constrained roadways, though ineligible for two or more general use thru-lane additions, shall be eligible for transit system enhancements including those described above [Bus turn-outs and transit station areas, Queue-jump lanes, Dedicated transit thru-lanes/rail facilities]. Dedicated transit lanes constructed pursuant to this policy shall not be retrofitted for use by automobile or truck traffic so long as the subject roadway remains constrained.”

This means that the construction of transit lanes is consistent with the Plan so long as they are Bus Only.

j. Fletcher Avenue is an example of what the City is likely to support:



Image of Fletcher Ave (above) not shown in meeting.

- k. The MPO’s school safety study is complete with specific recommendations that may apply to the Busch Blvd. Interim Design Project. Lisa to send latest.
- l. Linebaugh and Busch intersection lighting is very good. Suggest replicating through the corridor. MPO supports lighting the corridor.
- m. Suggest noting *why* we are doing the improvements in the report and to the public. Especially benefits to disabled and aging population.
- n. The I-275 interchange with Busch Blvd innovation way gateway project is working through pedestrian improvements with FDOT which include a high-emphasis pedestrian crossing/hardscape intersection. These recommendations should be noted in the Report. These are in addition to what this study should recommend at North Boulevard.
- o. There are transit plans effecting the CSX ROW and more updated bike plans. MPO will provide.
- p. Some confusion regarding the long-term potential for a change in design speed after PD&E and the near-term speed management recommendations to be included as part of

the design project. ***Will clarify in report and public materials: The Interim Design (Spot improvements) will implement Speed Management best practices. The Ultimate typical section (PD&E Study) will evaluate a reduction in design speed/speed limit; not yet approved. Benefits of a lower design speed will be presented at the public alternatives meeting.***

4. Action Items

Task	Description	Responsible Person	Status	Comments
<i>PM</i>	<i>Schedule MPO committee presentations</i>	<i>Brian Shroyer/FDOT</i>		
	<i>Send Study team the latest MPO School safety study for Chamberlain HS</i>	<i>Lisa Silva/MPO</i>		
	<i>Send Study team the latest Rail transit and Bike plans</i>	<i>MPO</i>		
	<i>Incorporate comments into Corridor Alternatives and Strategies Report</i>	<i>Cris Schooley/CH2M</i>		

5. Next Scheduled Meeting: Project Advisory Group meeting #4 May 24, 2018. Time TBA.



LOCAL AGENCY COORDINATION MEETING

SIGN-IN

West Busch Boulevard (SR 580) Corridor Study
 from North Dale Mabry Highway to North Nebraska Avenue
 Hillsborough County, Florida

FDOT Financial Project ID Number: 435908-1-22-01

601 E Kennedy Blvd, 18th floor, Tampa, FL, 33602

April 12, 2018

Name/Agency	Email
Beth Alden	aldenb@plancom.org
Cris Schooley/CH2M	Cris.Schooley@ch2m.com
David Hey	heyd@plancom.org
Gena Torres	torresg@plancom.org
Indike Ratnayake/FDOT	Liyanage.Ratnayake@dot.state.fl.us
Jenn Rhodes/CH2M	Carol.Barker@CH2M.com
Johnny Wong	wongj@plancom.org
Krista Kelly	kellyk@plancom.org
Lisa Silva	silval@plancom.org
Melissa Zornitta	zornittam@plancom.org
Michele Ogilvie	ogilviem@plancom.org
Pedro Parra	parrap@plancom.org
Rich Clarendon	clarendonr@plancom.org
Sandy Mortellaro	sandy@plancom.org
Tony Garcia	garciat@plancom.org
Wade Reynolds	reynoldsw@plancom.org
Wanda West	WestW@plancom.org
<i>BUOWHITSAEAA</i>	<i>BUOWHITSAEAA@PLANCOM.ORG</i>

Comp Planner ✓

Comp Planner ✓

Comp Planner ✓



LOCAL AGENCY COORDINATION MEETING

DRAFT AGENDA

West Busch Boulevard (SR 580) Corridor Study

from North Dale Mabry Highway to North Nebraska Avenue
Hillsborough County, Florida

FDOT Financial Project ID Number: 435908-1-22-01

601 E Kennedy Blvd, 18th floor, Tampa, FL, 33602

April 12, 2018

1. Introductions
2. Project Overview
3. Typical Section Alternatives
4. Inter-agency recommendations (Interim Design)
5. Other Recommendations

April 27th or in June

if you can come in April

Hillsborough MPO Upcoming Board and Committee Agenda Items - Current Status

May Committees (June MPO)																
Item	Presenter	Staff Responsible	Presenter Confirmed?	Meetings Malicious OA Review	9-May 2-May 25-Apr	CAC	BPAC	TAC	ITS	Policy	LRC	TDCEB	STWG	MPO	Notes	
ACTION ITEMS																
TP Adoption (Public Hearing at MPO meeting)	Sarah McKinley	Sarah McKinley			Yes			Yes						12-Jun 5-Jun 29-May	Yes	
PPP Update	Johnny Wong	Brandon Berry			Yes			Yes						23-May 16-May 9-May	Yes	Rich at CAC, BPAC, Consent at MPO Board (note: 45 day Consent at Board)
MPO School Safety Study and Top 10 Report	Wade Weaver	Lisa Silva	Yes	Y	Yes			Yes						N/A	No	
BPAC request to MPO Board	Patrick Thorpe	Wade Reynolds			Yes			Yes						N/A	Yes	
TBRPC Interlocal Agreement	Allison Yeh	Allison Yeh			Yes			Yes						N/A	Yes	Consent at MPO Board
STATUS REPORTS																
Tampa Bay Next Quarterly Update	J. Patrick	Allison Yeh	Yes		Yes			Yes								RPG to study extending 46th to BBD, per JP's scope
Innovation District Subarea (42nd/46th Street) Study	Tony LaColla	Lisa Silva	Yes		Yes			Yes								And/or full board in August
City of Tampa Parklets	Michael Case	Beth Alden			Yes			Yes								
MPO Regional Planning Best Practices Study	Brian Shroyer	Allison Yeh			Yes			Yes							Yes	Schedule after walking tour
Resiliency Study for Tampa Bay TMA: Overview & Approach	Jim Hudock	Gena Torres	No		Yes			Yes							Yes	requested by Vance Arnett - check w/ Jan Washington
W. Busch Blvd Corridor Study	Brian Shroyer	Gena Torres			Yes			Yes							Yes	Jay thinks it should be ready and a good topic
Hillsborough County Design Standards	Jim Hudock	Gena Torres			Yes			Yes							Yes	confirm w/ David Cook @ Turnpike
Tampa Sidewalk Requirements	Jay Collins	Rich Clarendon			Yes			Yes								
Planning Commission Alexander Street Marketing Corridor Study	Jay Collins	Rich Clarendon			Yes			Yes								
Toll Lane Before/After Studies	Jay Collins	Rich Clarendon			Yes			Yes								
Channel Drive Design Workshop	Jay Collins	Lisa Silva			Yes			Yes								
OLD/NEW BUSINESS																
ADDENDUM ITEMS																
SPECIAL PRESENTATIONS																

West Busch Boulevard (SR 580) Corridor Study

from North Dale Mabry Highway to North Nebraska Avenue

Hillsborough County, Florida

FDOT Financial Project ID Number: 435908-1-22-01

Local Agency Coordination Meeting



E-1000



Purpose of this meeting

1. Discuss draft typical section alternatives
2. Gauge agency interest in recommendations for further study which would require inter-agency coordination (JPA, maintenance agreements, etc.)



Agenda

1. Introductions
2. Project Overview
3. Typical Section Alternatives
4. Inter-agency Recommendations (Interim Design)
5. Other Recommendations



Introductions

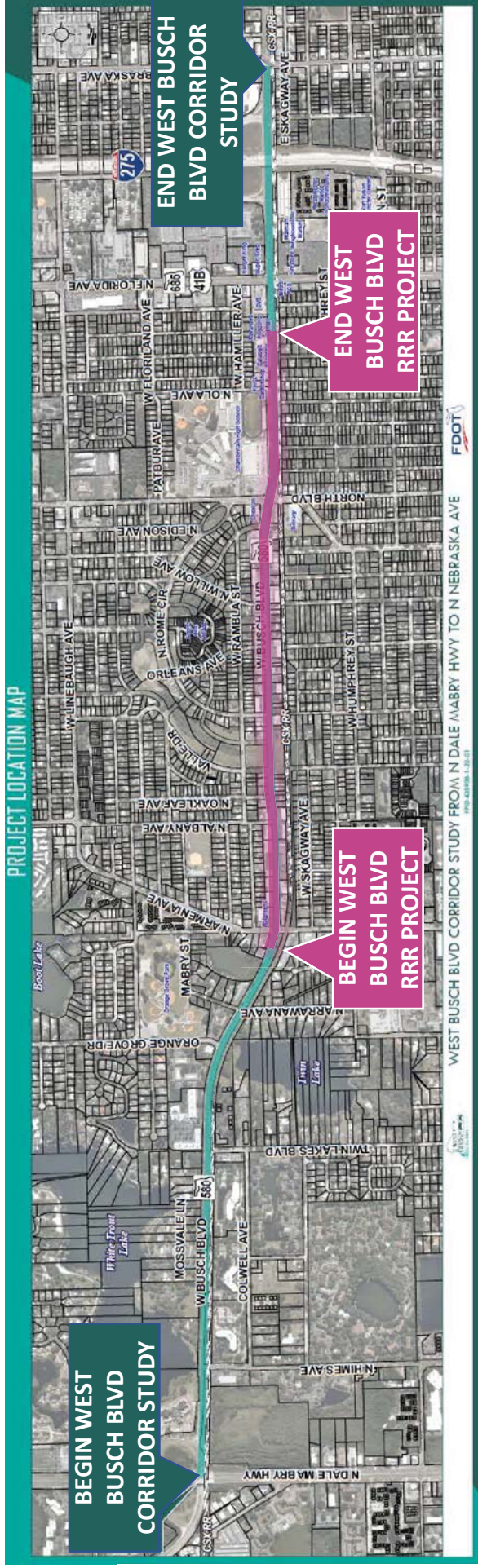


Project Overview



Project Study Area

- 3.3 miles
- Within the city of Tampa and unincorporated Hillsborough County
- Concurrent RRR project



Study Purpose

Achieve the Corridor Vision:

*“The Busch Boulevard corridor offers **safe, comfortable and convenient access through and across the corridor for all users and all travel modes.**”*

PD&E Study Changes
(implement typical section changes)

Interim Design Project Changes
(implement spot improvements)

Resurfacing, Restoration and Rehabilitation Project (3R) Changes

Busch Boulevard Corridor Study
(identify the vision, recommendations & implementation plan)

**We
Are
Here**

E-1006

Updated Study Schedule

- Began in March 2017
- Held Agency Project Kick-off Meeting, 3 Project Advisory Group Meetings, & Public Visioning Workshop
- **Meet with local agencies to review draft alternatives and recommendations**
- PAG #4 – May 24, 2018
- Alternatives Public Meeting – June 2018
- Final Corridor Alternatives and Strategies Report – August 2018

Long Term Recommendations: Typical Section Alternatives

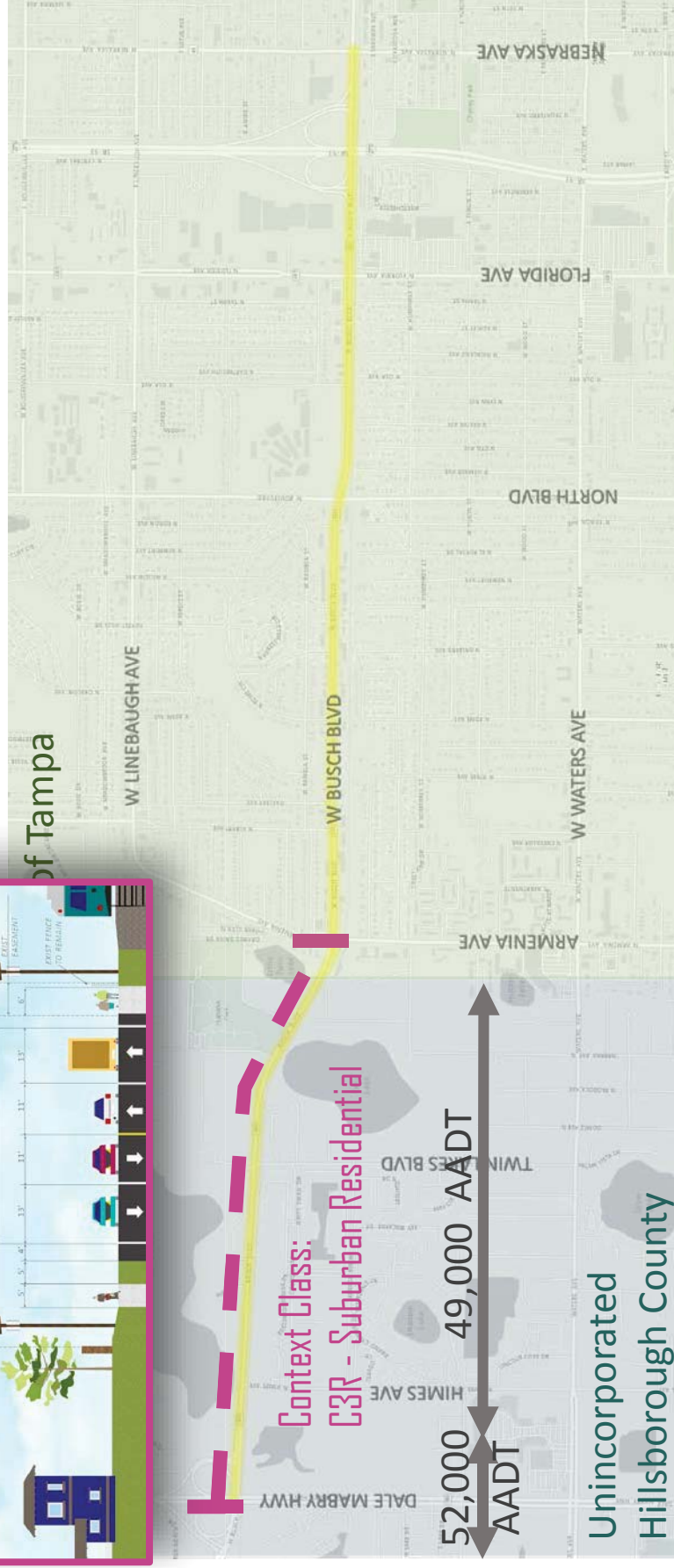
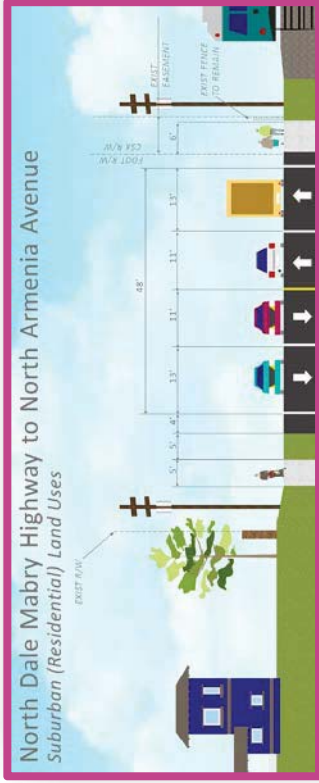


Currently Developing Range of Alternatives

Alternative	Description
No-Build	Existing condition
4-Lane	Construct 4-lane divided roadway without bike lanes, but with raised median, border width, and wide (8') sidewalk to accommodate additional pedestrian demand
5-Lane	Construct a 5-lane divided roadway with flush unrestricted median, bike/parking lanes, border width, 6' sidewalks
6-Lane	Construct a 6-lane divided roadway with raised median, bike lanes, border width, 6' sidewalks

- Many variations are possible, but these alternatives provide a good range to receive feedback and further refine the concepts for the corridor vision
- To be presented at the Public Alternatives Meeting
- PD&E Alternative(s) could be all, some, or a variation of these alternatives

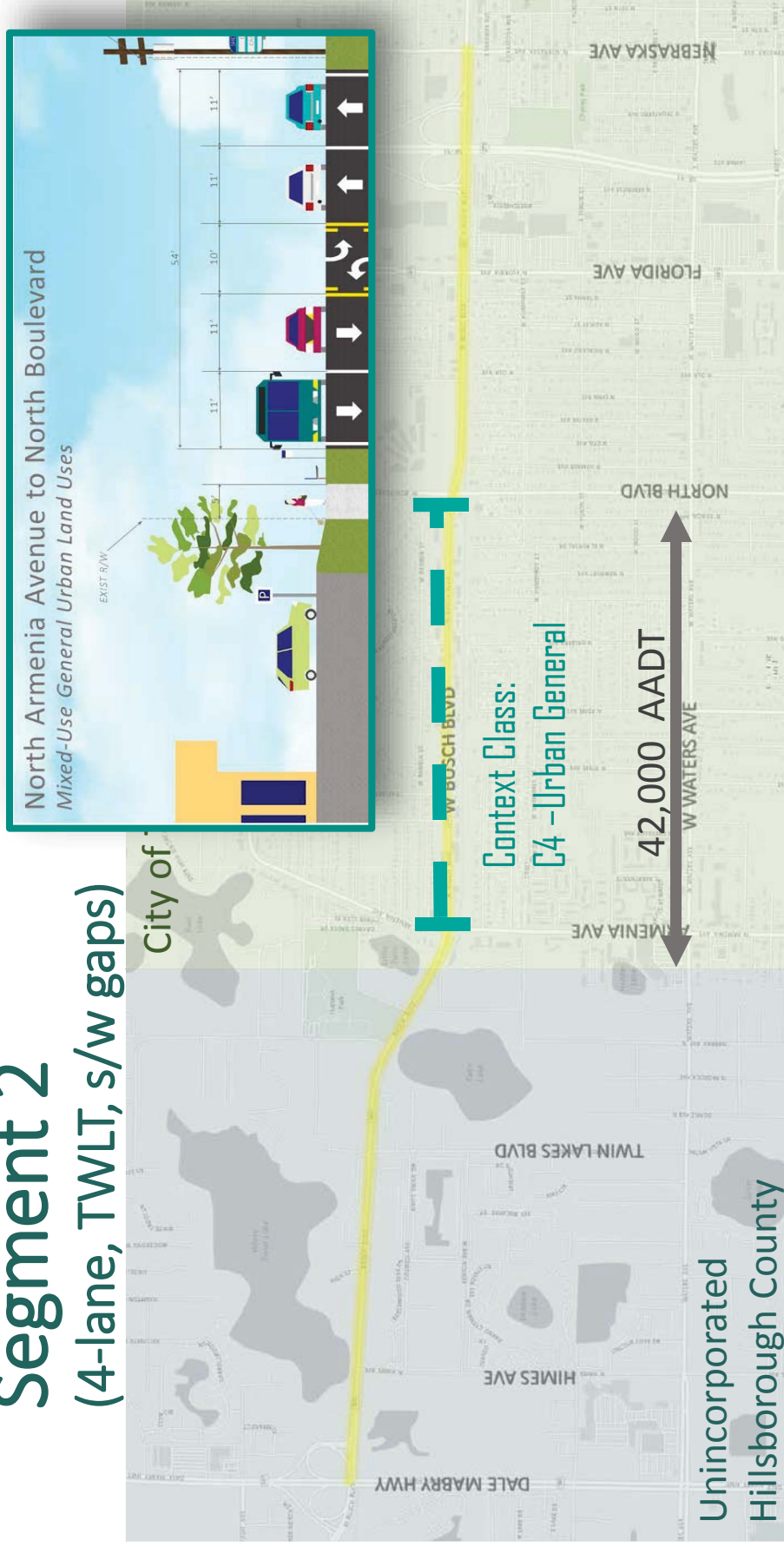
Existing Conditions (no build) Segment 1 (4-lane undivided w/sidewalk)



Existing Conditions (no build)

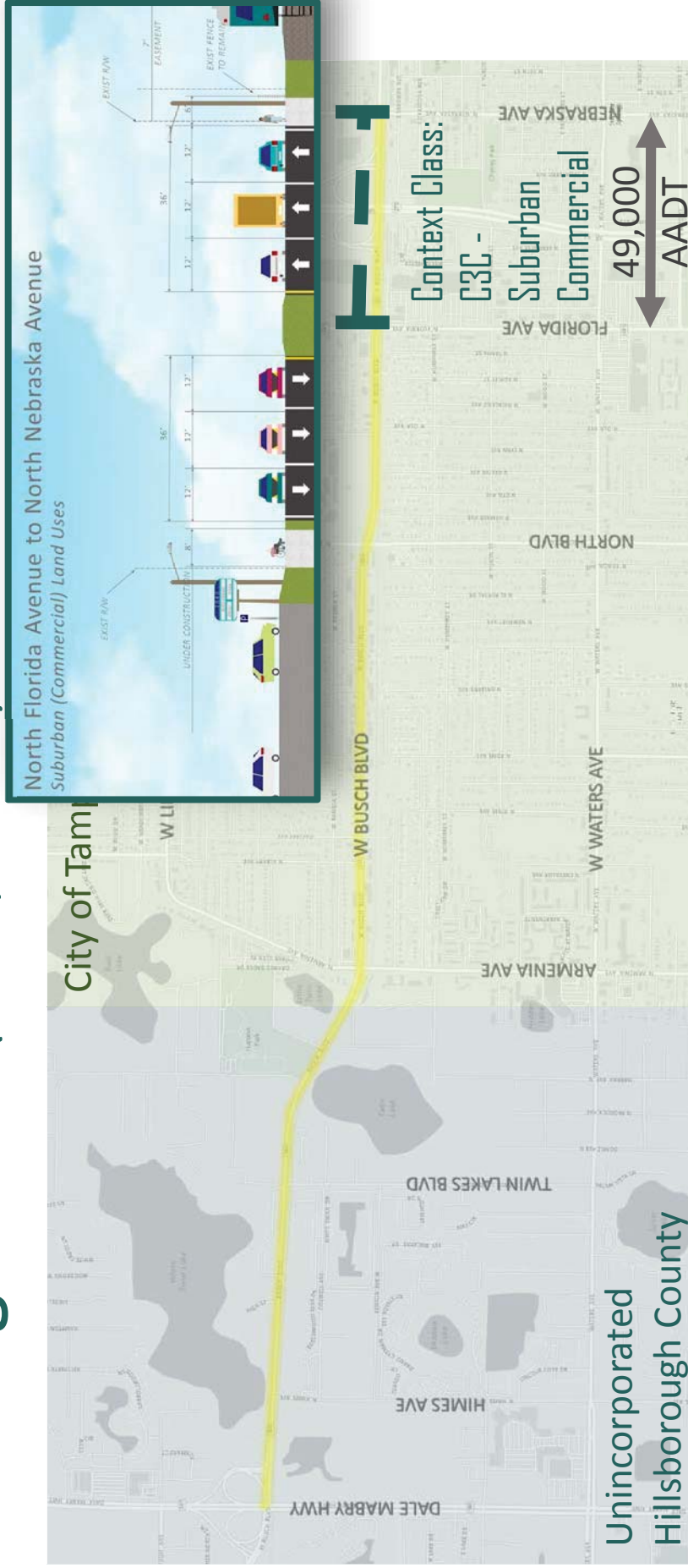
Segment 2

(4-lane, TWLI, s/w gaps)

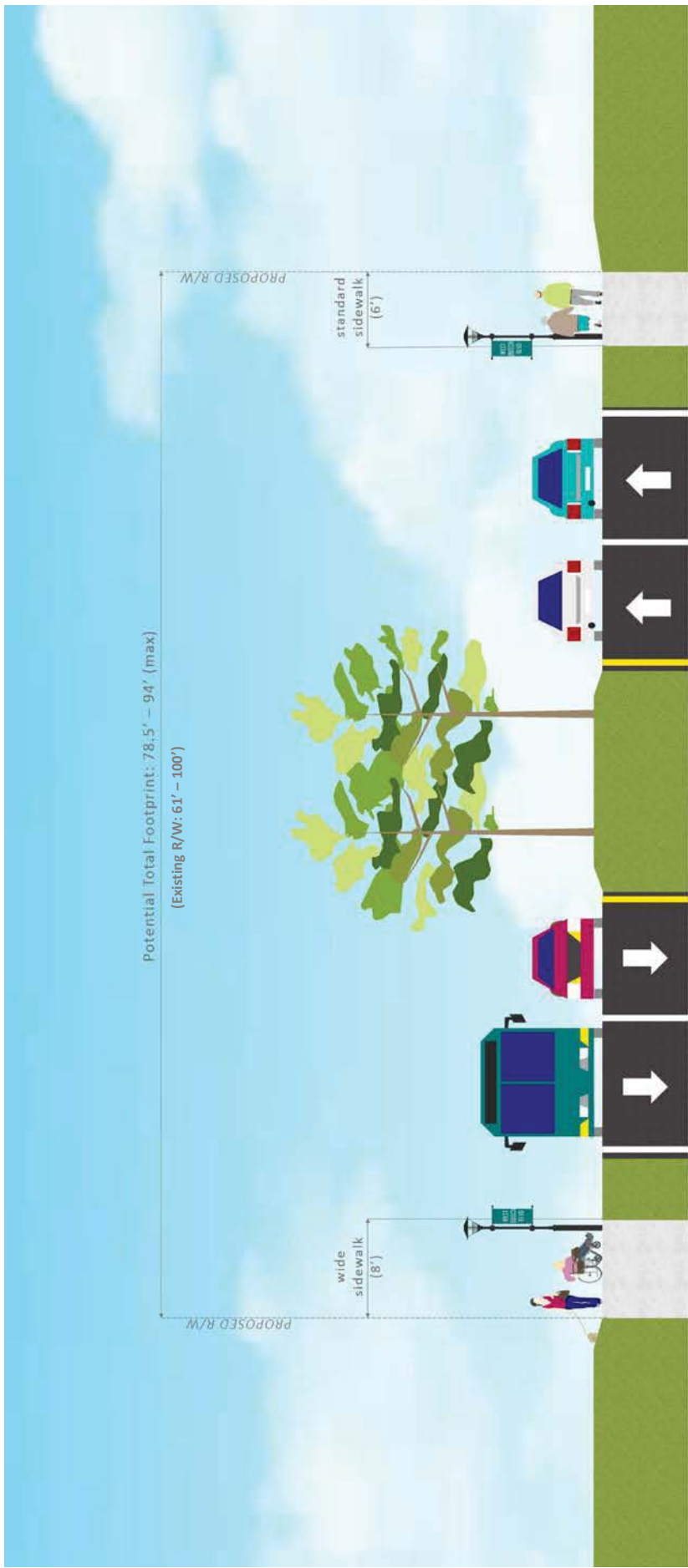


Existing Conditions (no build)

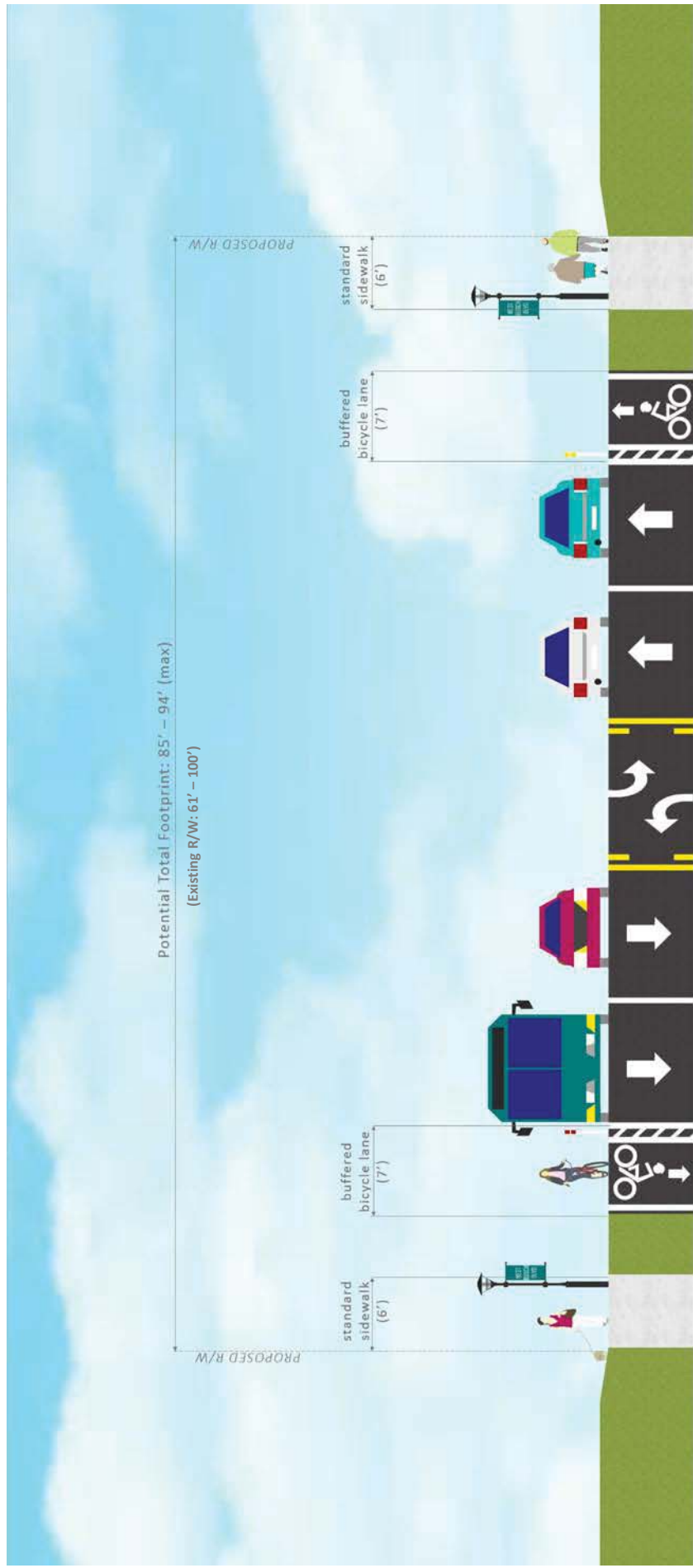
Segment 4 (6-lane, median)



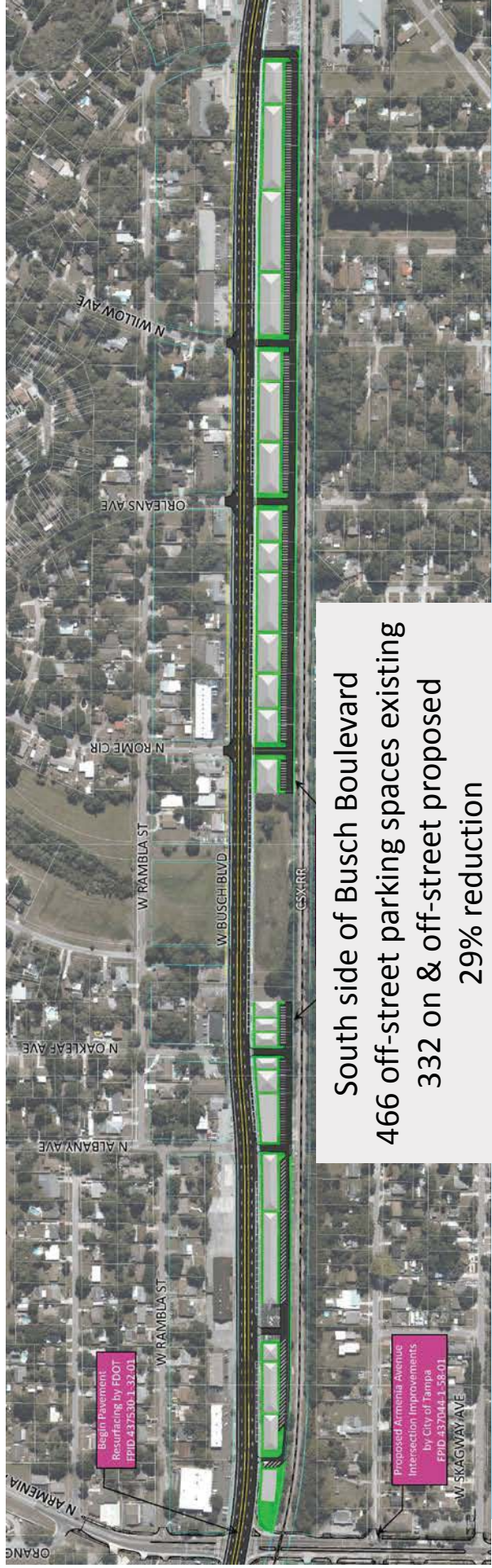
Alternative 1: 4 Lanes with Median and Wide Sidewalk on the North Side



Alternative 2: 4 Lanes with Center Two-Way Left Turn Lane and Buffered Bike Lanes

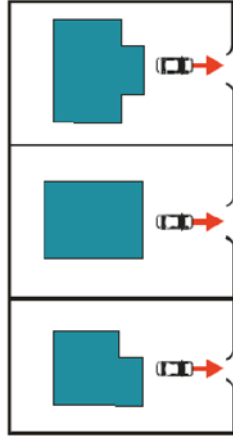


Potential Parallel Parking Option for Segment 2 (North Armenia Ave. to North Blvd.)

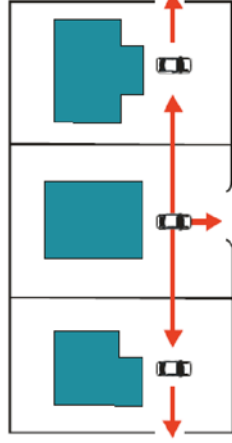


Width of parallel parking = buffered bike lane

City of Tampa code does not currently address cross/joint access and discourages commercial access from non-arterial streets (Sec. 27-283.12.(j)(1)).



Avoid



Promote

- ➔ Cross Access
- ➔ Joint Access
- ➔ Complete on-site circulation

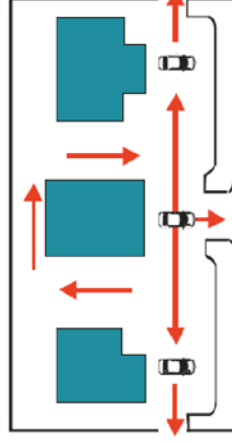


Image: Driveway Information Guide (FDOT 2008) p. 85

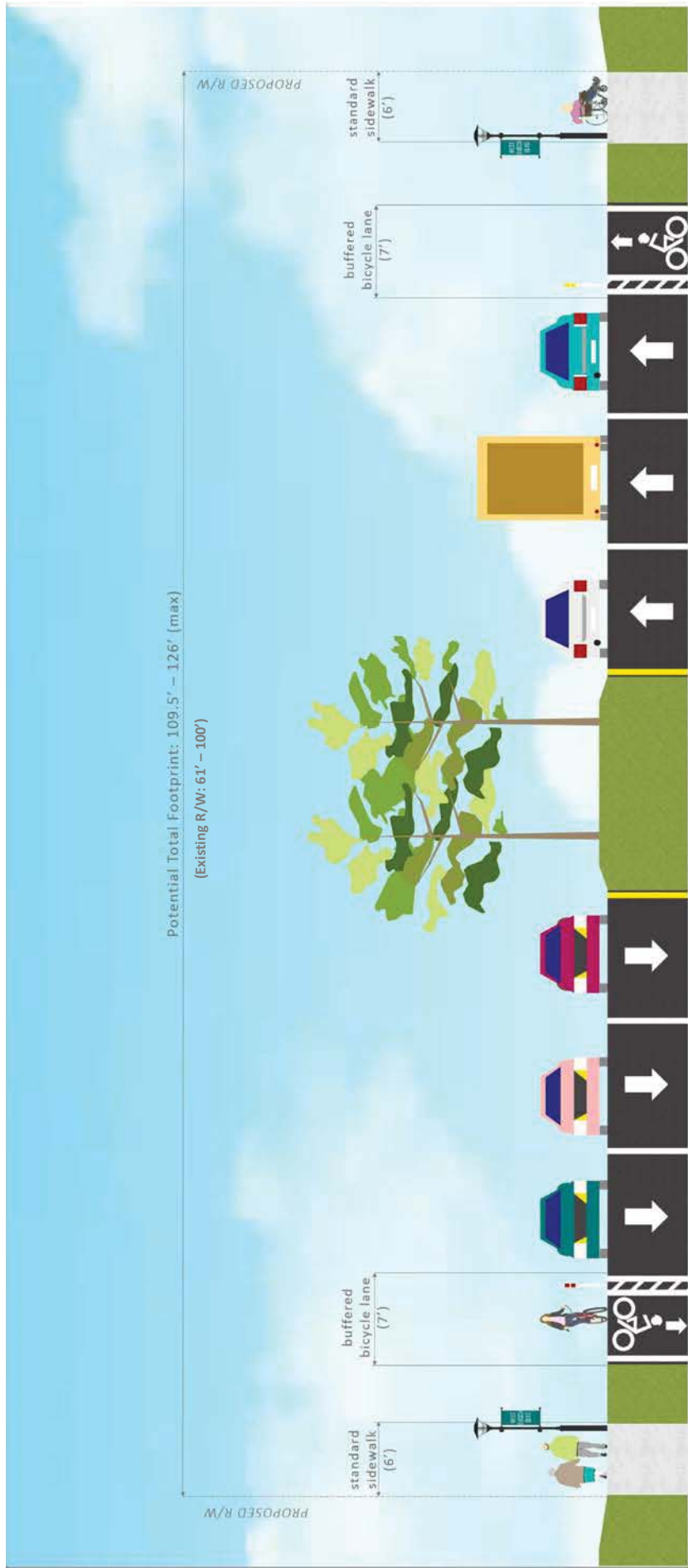
To address the public interest in these matters, land development regulations include conditions or circumstances where nonconforming access features may be brought into conformance. Such conditions may include:

- when new driveway permits are requested;
- an increase in land use intensity;
- substantial enlargements or improvements;
- significant change in trip generation; and
- as changes to roadway design allow. (p.1-8)

Overlay requirements may address any issues of concern, such as joint access, parking lot cross access, reverse frontage, driveway spacing, and limitations on new driveways. (p. 1-9)

Source: "Land Development and Subdivision Regulations that Support Access Management", Center for Urban Transportation Research, University of South Florida, January 1994. Williams, Kristine M. <https://www.cutr.usf.edu/oldpubs/pcm/files/Model%20Land%20Development%20and%20Subdivision%20Regulations%20that%20Support%20Access%20Management.pdf>

Alternative 3: 6 Lanes with Median and Buffered Bike Lanes

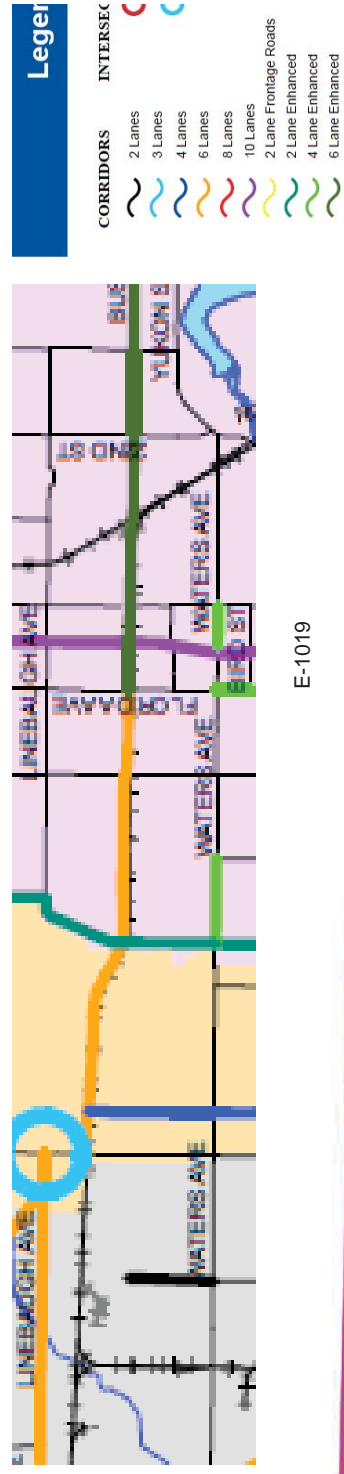


Planning Consistency – 6 Lane Alternative

- 2040 LRTP
 - ≈ Need for 6/7 lanes on “Beyond 2040” Map (pg. 98) from Armenia Blvd. to North Blvd.



- Tampa Comprehensive Plan (effective date February 20, 2016)
 - Constrained Roadways (physical constraints)
 - ≈ Busch Blvd. from Dale Mabry Blvd. to North Blvd. constrained to 4 lanes
 - ≈ “Page 151; MBY (Mobility) Policy 3.2.2: Constrained roadways... shall be eligible for... Dedicated transit lanes...”
- Hillsborough County Corridor Preservation Plan (Map 25) shows preserved for 6 lanes/ 6 lanes enhanced through study limit



Inter-agency Recommendations (Interim Design)



Interim Design Project Recommendations

- Safety improvements at Dale Mabry off-ramp
- Turn lane extensions as feasible
- Complete sidewalk gaps
- Add signal and/or mid-block crossing at Rome Circle or Willow Ave.
- Leading pedestrian interval or ped-only phase at North Blvd.
- Add refuge islands or tighten curb radius at North Florida Ave. and North Nebraska Ave.
- Add YOUR SPEED radar sign(s)

Potential additional improvements with local agreements include...

Corridorwide Recommendations



- Add pedestrian lighting

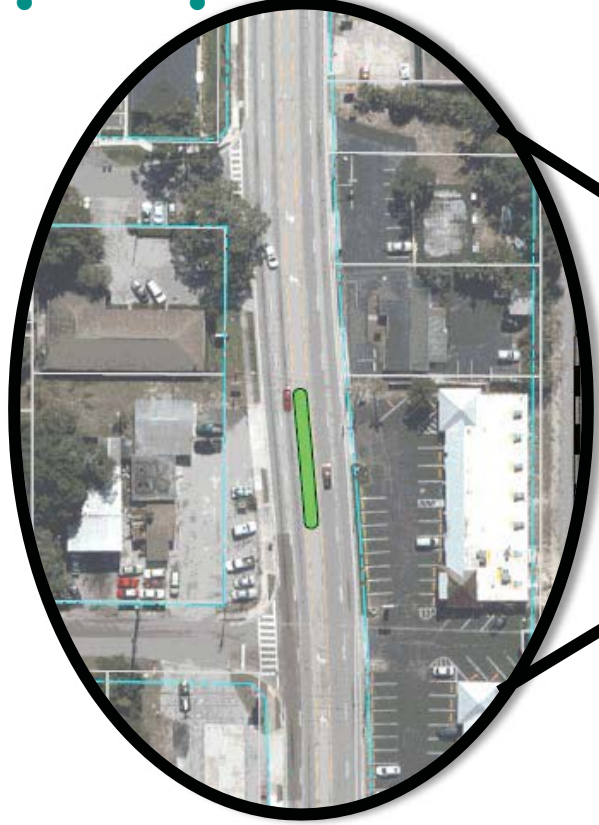
- Interconnect signals to increase platooning
 - ≈ Traffic signal speed signs
 - ≈ Explore potential of dynamic signal timing and/or setting signals to a 35 mph target speed
 - ≈ City and county signals are not interconnected



*Traffic Signal Speed Sign
MUTCD, Section 2H.03 Traffic Signal Speed
Sign (11-1)*

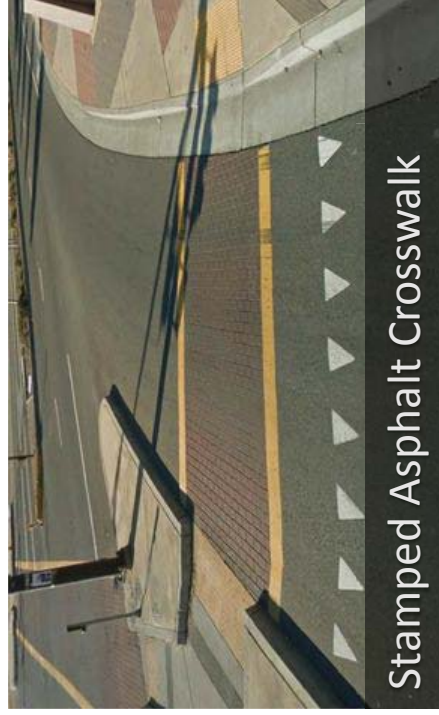
Florida Friendly/ Pedestrian Deterrent Landscaping in Opportunity Areas

- Opportunity areas defined where:
 - **At least one existing driveway per property accessible via left turn**
 - **At least 50-ft. of turn lane** provided in advance of the driveway



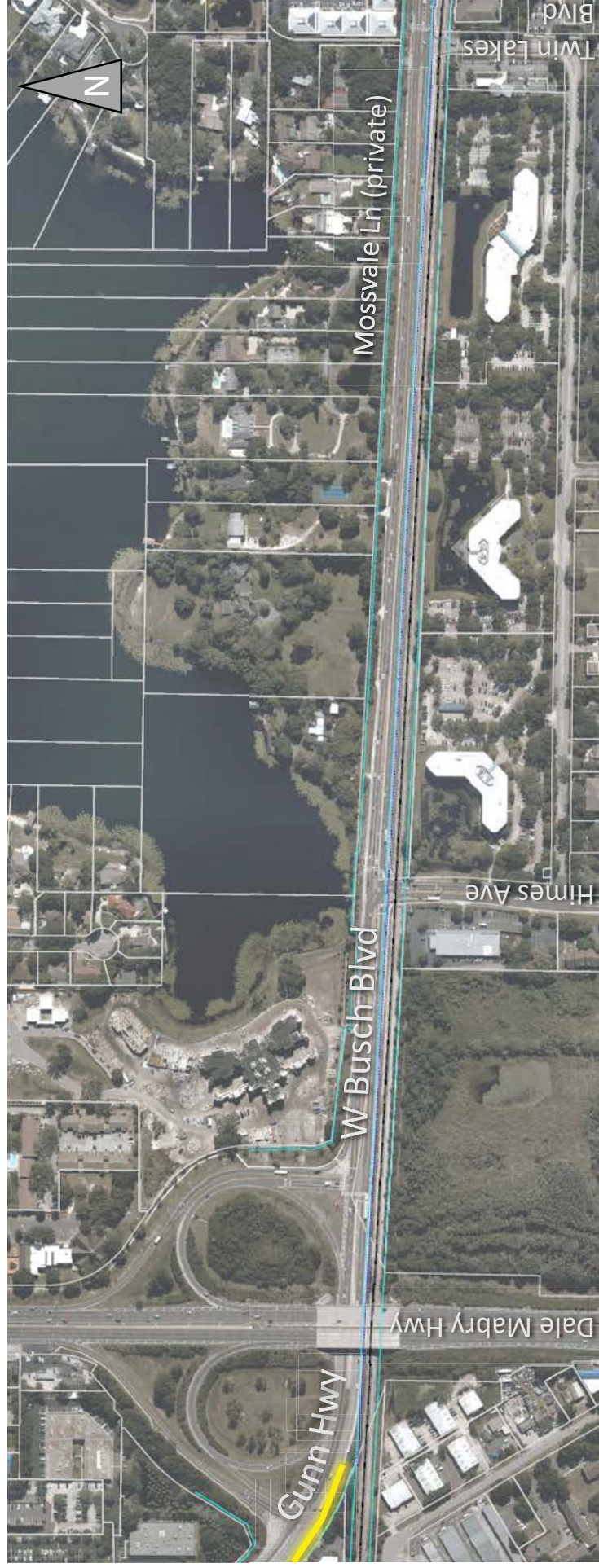
Example: 13th Street, Gainesville, FL

High-emphasis pedestrian crossings / hardscape intersections



Gunn Highway Sidewalk Gap

- County jurisdiction



Mossvale Lane Extension + Intersection

- County land-use jurisdiction

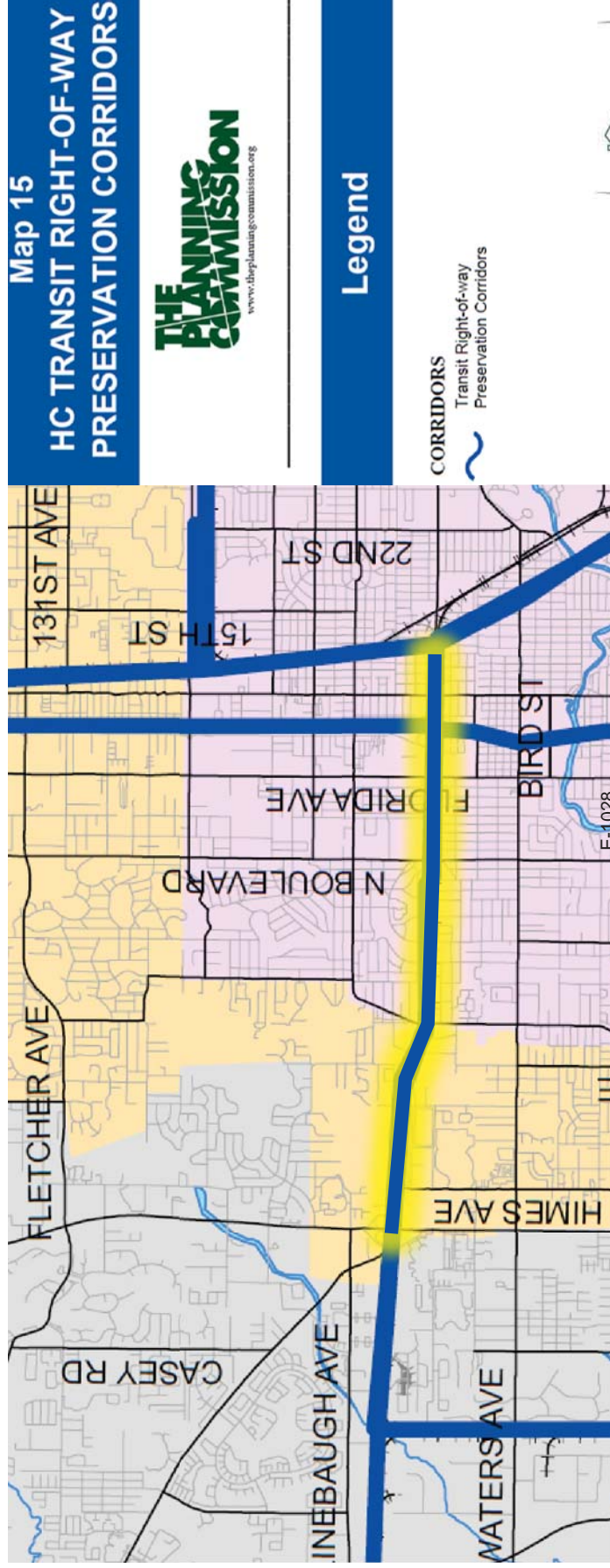


Other Recommendations



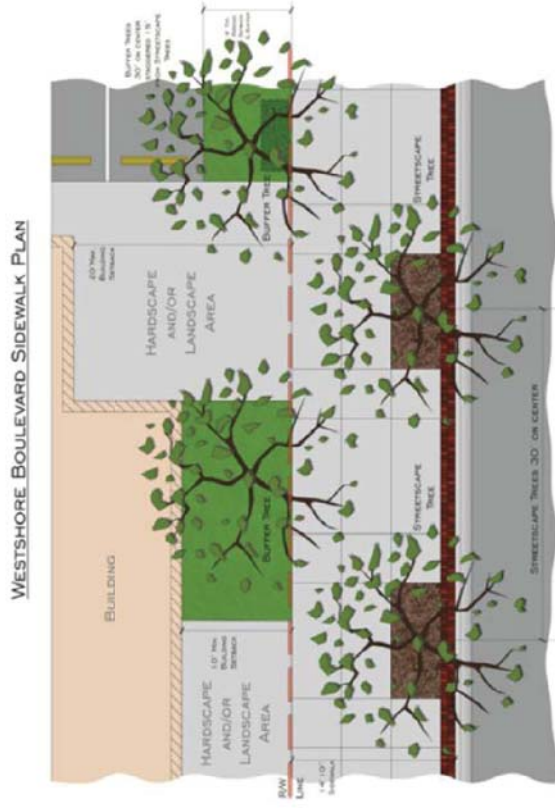
Recommendations related to future transit/rail plans

- The CSX line runs parallel to the corridor along the south for most of the study limits
- **Are there any plans related to transit or the rail corridor that should be documented in this report?**



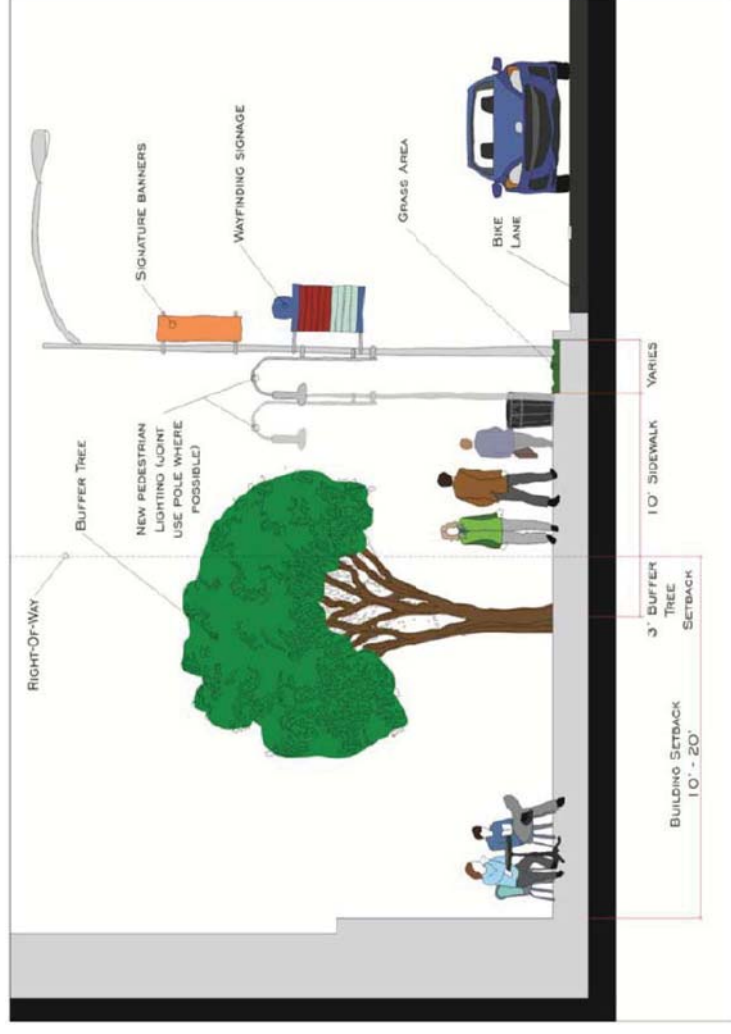
Speed Management along Corridor

- Recommending further study of lowering the speed limit
- Recommend school safety study to explore school zone designation
- Speed reduction can be bolstered by:
 - ≈ Urban form changes
 - ≈ Increasing police enforcement
 - ≈ Parallel parking (requires speed limit of 35 mph or less)
 - ≈ Landscaping
- Development/redevelopment requirement to provide landscaping along sidewalk can calm traffic and improve pedestrian environment



Adopt an overlay district to support corridor vision

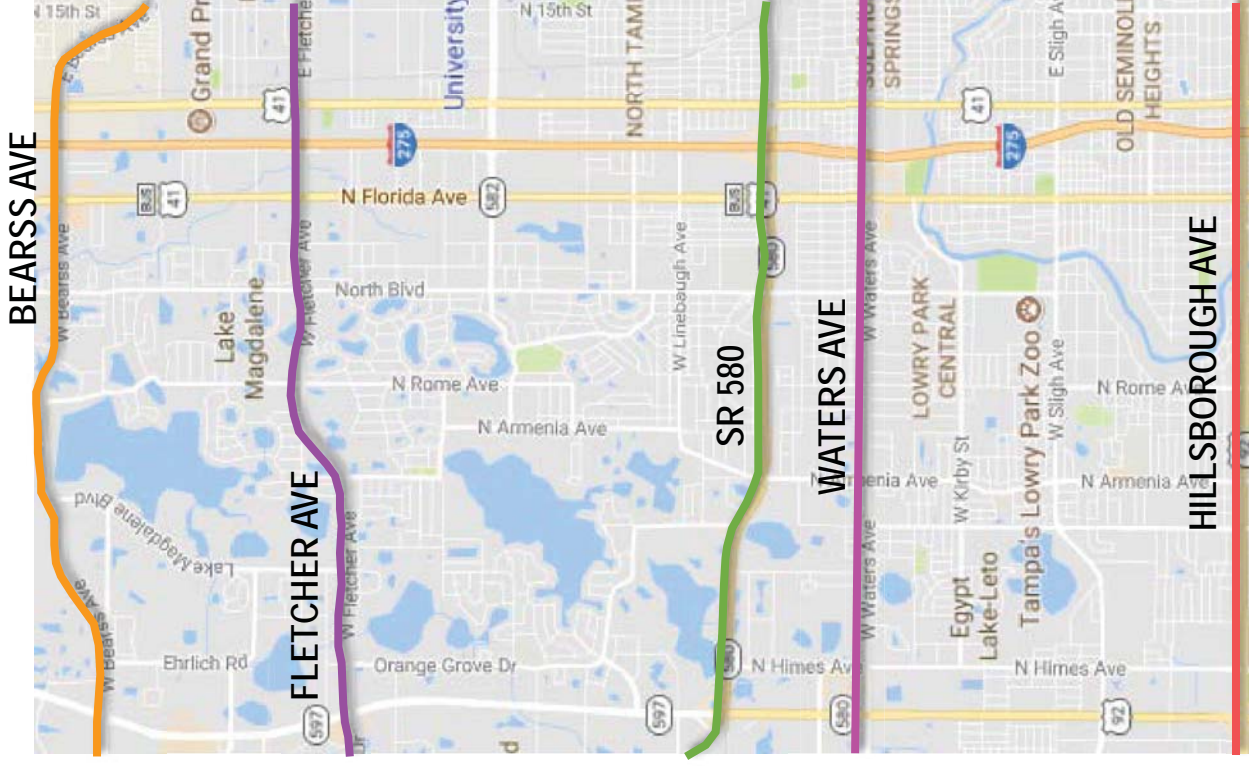
- Coordinate landscaping opportunities with development code
- Set-back for R/W preservation based on recommended alternative(s)
- Reduced/revised parking requirements
- Form-based code to promote pedestrian environment



City of Tampa Code Sec. 27-238 Westshore Overlay District Table 238.c

Regional origin/destination planning study

- To study the perceived "through traffic" along the corridor between Dale Mabry Hwy and I-275
- Limited east-west corridors and existing congestion suggest the opportunity to address regional travel flows at a more regional level than within the limits of this corridor study
- MPO or FDOT should lead



Corridor	Lanes	AADT	Notes
Bearss Avenue	4-lane	54,000	3.8 miles north
Fletcher Avenue	4-lane	23,500	Limited western connectivity
SR 580	4-lane	50,000	
Waters Avenue	4-lane	27,000	Limited eastern connectivity
Hillsborough Avenue	6-lane	51,500	2.6 miles south

Section 6

Newsletters

Section 6.1

Newsletter #1

West Busch Boulevard (SR 580) Corridor Study

From North Dale Mabry Highway to North Nebraska Avenue

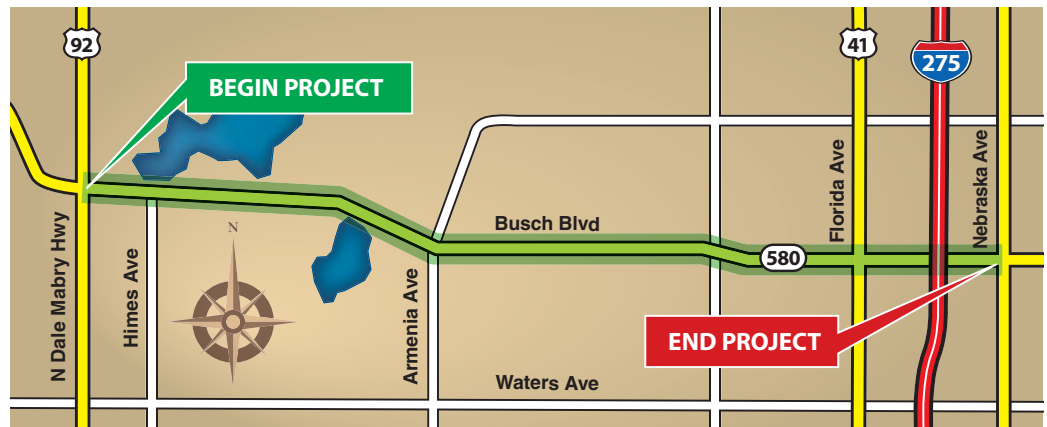
Hillsborough County, FL

Financial Project ID Number: 435908-1-22-01

Corridor Study Begins!

The Florida Department of Transportation (FDOT), District 7 is conducting a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue in Hillsborough County – a distance of approximately 3.3 miles. The West Busch Boulevard (SR 580) corridor is an east-west urban principal arterial connecting and serving local commuters and visitors between North Dale Mabry Highway and Nebraska Avenue. The corridor study area includes portions of unincorporated Hillsborough County and the City of Tampa and is the main route to the Busch Gardens Amusement Park (located approximately 2.15 miles east of the project limits at McKinley Drive).

The corridor supports a variety of land uses and demographics, including schools, churches, large and small businesses with direct access to and from SR 580, transit services, pedestrians, bicyclists and many small communities. The corridor also serves a large commuter population as a connection between Veterans Expressway to the west and I-75 to the east.



The West Busch Boulevard (SR 580) corridor study will use FDOT's Complete Streets policy to showcase how Complete Streets approaches can shape a corridor. The study, which began in March 2017, is expected to continue for approximately 18 months and will include technical work and data analysis, along with various opportunities for the public to be involved in discussing a vision for West Busch Boulevard (SR 580) that makes it accessible for all users.

What is a Corridor Study?

A corridor study is the first step in planning by defining the corridor's needs. A corridor study generally defines the relationships between a roadway and its adjacent land. Corridor studies are also used to: define acceptable levels of access and mobility; determine transportation system needs to support surrounding land uses, and identify operational deficiencies, among other things. This corridor study will include establishing a shared vision that will allow the community to help set project goals and objectives. The vision will guide the project team to develop a set of improvements that leverage local resources, match local conditions and desires, and are consistent with ongoing planning and development efforts.

Project Schedule

The West Busch Boulevard (SR 580) Corridor Study began in March 2017 and is anticipated to be completed in 18 months. Project activities will include:

- Identifying the travel related needs and issues within the West Busch Boulevard project corridor.
- Coordinating with key stakeholders to establish the vision for the corridor and identify solutions to the corridor issues and improve operational efficiency within the West Busch Boulevard corridor.
- Document the vision for all future improvements within the West Busch Boulevard corridor to maximize the “livability” for all users.

ACTIVITY	2017												2018						
	Mar	Apr	May	June	July	Aug	Sept	Oct	Nov	Dec	Jan	Feb	Mar	April	May	June	July	Aug	Sept
Notice to Proceed - Project Begins	★																		
Develop Public Involvement Plan																			
Mailing List																			
Elected Officials/Agency Kick-off Meeting				★															
Project Advisory Group (PAG) Meetings																			
Corridor Visioning Workshop										★									
Alternatives Public Meeting													★						
Final Corridor Alternatives & Strategies Report																			

Project Advisory Group (PAG)

A Project Advisory Group (PAG), composed of local government and agency representatives, are working with FDOT to identify corridor challenges along with potential solutions, assist with documenting the community goals, and develop a vision for the corridor that will guide the study development and future improvements. The PAG will meet at key points throughout the study to collaborate with the project team to ensure the vision and improvement alternatives are well-coordinated with local planning efforts, adjacent projects, and community goals.

How Can You Get Involved?

The project website is your best source for up-to-date information; you can get access at this link:

www.fdotd7studies.com/westbuschblvd/. In addition to the general study information that is already posted on the website, the project documents will be uploaded to this site as they are developed. However, you can already begin providing your input on this project using WikiMapping! This tool allows you to “pin” comments at specific locations along the corridor. You can pick the type of comment you have to help us understand the issues you observe in the corridor. Some of these issue types include: Bus/Transit, Connectivity, Intersection Issue, Pedestrian, Safety, and Traffic/Parking. If you have a comment on an issue that does not have a specific description, just use the “Other” category. The link to the WikiMapping tool is provided on the project website, but for direct access you can type the following address into your web browser:

<http://wikimapping.com/wikimap/West-Busch-Boulevard-Corridor-Study.html>. The project team will utilize this information to help inform the process and guide solutions.

Future opportunities for you to get involved include the upcoming Corridor Visioning Workshop that will be conducted in the Fall 2017. At this workshop, we will gather your input on the existing corridor and document your ideas to resolve some of the issues that you have observed. Our goal is to enlist your help to identify improvements that can be implemented in the West Busch Boulevard corridor to resolve the challenges. In advance of this workshop, a User Preference Survey will be available on the website in October; hard copies of this same survey will be available at the Corridor Visioning Workshop. These surveys will be used by the project team to understand the issues and concerns that you have regarding the corridor.

We want your input! We urge anyone interested in shaping the future of the West Busch Boulevard corridor to actively participate by visiting the website, get informed, document issues in the WikiMapping tool, attend meetings, and provide your ideas.

For more information or to comment, please contact:

Brian Shroyer, Project Manager
11201 N. McKinley Drive
Tampa, FL 33612
Office Phone: (813) 975-6449
brian.shroyer@dot.state.fl.us

Kristen Carson, Public Information Officer
11201 N. McKinley Drive
Tampa, FL 33612
Office Phone: (813) 975-6202 or 800-226-7220
kristen.carson@dot.state.fl.us

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by the Florida Department of Transportation (FDOT) pursuant to 23 U.S.C. §327 and a Memorandum of Understanding dated December 14, 2016 and executed by the Federal Highway Administration and FDOT.

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status. Persons who require special accommodations under the Americans with Disabilities Act or persons who require translation service (free of charge) should contact Christopher Speese, Public Involvement Coordinator, at (813) 975-6405 or (800) 226-7220.

Comuníquese Con Nosotros

Nos importa mucho la opinión del público sobre el proyecto. Si usted tiene preguntas o comentarios, o simplemente desea más información, por favor comuníquese con nosotros. Nuestro representante, Señora Lilliam Escalera, 813-975-6445, Departamento de Transportación de Florida, 11201 N. McKinley Dr., Tampa, FL 33612, lilliam.escalera@dot.state.fl.us.

Section 6.2

Newsletter #2

West Busch Boulevard (SR 580) Corridor Study

From North Dale Mabry Highway to North Nebraska Avenue

Hillsborough County, FL

Financial Project ID Number: 435908-1-22-01

Corridor Study Update

The Florida Department of Transportation (FDOT), District 7 is currently conducting a corridor planning study along West Busch Boulevard (SR 580) from North Dale Mabry Highway to North Nebraska Avenue in Hillsborough County – a distance of approximately 3.3 miles.



The study began in March 2017 and will conclude in late fall of 2018. The outcome of the study will be a Corridor Alternatives and Strategies Report which will document the vision for short-term and long-term improvements within the corridor to meet the mobility and connectivity needs of all users, regardless of their transportation mode of choice.

To date, the study team has:

- Evaluated existing conditions, including existing and future traffic levels, crash history, existing bicycle, pedestrian and transit activity and infrastructure, and the context of the surrounding land uses;
- Developed potential goals and objectives for the future of West Busch Boulevard related to accessibility and connectivity, economic development, mobility, and safety;
- Began developing a range of potential improvement alternatives that address the anticipated traffic needs, multi-modal considerations (bicycle, pedestrian, transit), address safety concerns, incorporate suggestions based on comments received so far (from agencies, elected officials, Wiki-Mapping interactive tool, and public meetings). These improvements will be separated into two categories: those that can be implemented within the existing right-of-way (near term) and those that may require additional right-of-way and require further study;
- Held three Project Advisory Group (PAG) meetings, where local government and agency representatives have worked with FDOT to identify corridor challenges along with potential solutions, assisted in documenting community goals, and helped develop a vision for the corridor that will guide the study development and future improvements;

About the Corridor

The West Busch Boulevard (SR 580) corridor is an east-west urban principal arterial connecting and serving local commuters and visitors between North Dale Mabry Highway and Nebraska Avenue. The corridor study area includes portions of unincorporated Hillsborough County and the City of Tampa and is the main route to the Busch Gardens Amusement Park (located approximately 2.15 miles east of the project limits at McKinley Drive).

The corridor supports a variety of land uses and demographics, including schools, churches, large and small businesses with direct access to and from SR 580, transit services, pedestrians, bicyclists and many small communities. The corridor also serves a large commuter population as a connection between Veterans Expressway to the west and I-275 to the east.

What is a Corridor Study?

A corridor study is the first step in planning by defining the corridor's needs. A corridor study generally defines the relationships between a roadway and its adjacent land. Corridor studies are also used to: define acceptable levels of access and mobility; determine transportation system needs to support surrounding land uses, and identify operational deficiencies, among other things. This corridor study will include establishing a shared vision that will allow the community to help set project goals and objectives. The vision will guide the project team to develop a set of improvements that leverage local resources, match local conditions and desires, and are consistent with ongoing planning and development efforts.

- Conducted a Public Visioning Workshop, where members of the community were invited to provide input on the current conditions and concerns along the corridor, prioritize goals and objectives of the study, and identify potential improvements that are most important to them; and
- Solicited input online through the project website, Wikimapping tool, and a Community Preference Survey.

▶ Learn more about the project here: www.fdot7studies.com/westbuschblvd/

Next Steps

Based on the technical work, data analysis, and input received to date from the Project Advisory Group (PAG) and the public, the goals and objectives and desired potential improvements will be further defined in early 2018 into options for future typical sections and alignment alternatives.

These draft recommendations will then be presented to the public for additional input at the Alternatives Public Meeting, anticipated to be held in Spring 2018. One additional PAG meeting is also scheduled before the conclusion of the study.

The Final Corridor Alternatives and Strategies Report will be published in fall of 2018. In addition to documenting the corridor vision, the report will document improvements that can be advanced to the design phase; these are improvements within the existing right-of-way. The report will also document long-term improvements that must be further refined through a more detailed Project Development and Environment (PD&E) Study.

ACTIVITY	2017												2018						
	Mar	Apr	May	June	July	Aug	Sept	Oct	Nov	Dec	Jan	Feb	Mar	April	May	June	July	Aug	Sept
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Alternatives Public Meeting																			
Final Corridor Alternatives & Strategies Report																			

How Can You Stay Involved?

We want your input! We urge anyone interested in shaping the future of the West Busch Boulevard corridor to actively participate by visiting the website, get informed, document issues in the WikiMapping tool, attend meetings, and provide your ideas.

The project website is your best source for up-to-date information; you can get access at this link:

www.fdot7studies.com/westbuschblvd/. The website not only houses general study information, but also all project documents that have been developed to date, including presentations and meeting materials presented at the PAG meetings and the Public Visioning Workshop. The website will continue to be updated throughout the study with the latest documents and information.

You also have the continued opportunity to provide input using WikiMapping! This tool allows you to “pin” comments at specific locations along the corridor. The link to the WikiMapping tool is provided on the project website, but for direct access you can click on the following link: <http://wikimapping.com/wikimap/West-Busch-Boulevard-Corridor-Study.html>. The project team will continue to utilize this information to help inform the process and guide solutions.

Future opportunities for you to get involved include the upcoming Alternatives Public Meeting, anticipated to be held in Spring 2018. Please keep a look out on the project website for additional information regarding the Alternatives Public Meeting!

For more information or to comment, please contact:

Brian Shroyer, Project Manager
11201 N. McKinley Drive
Tampa, FL 33612
Office Phone: (813) 975-6449
brian.shroyer@dot.state.fl.us

Kristen Carson, Public Information Officer
11201 N. McKinley Drive
Tampa, FL 33612
Office Phone: (813) 975-6202 or 800-226-7220
kristen.carson@dot.state.fl.us

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Section 7

Additional Public Comments

Section 7.1

Emailed Comments

Barker, Rachel/ORL

From: Barker, Carol/ORL
Sent: Wednesday, June 20, 2018 12:24 PM
To: Rhodes, Jenn/ORL; Schooley, Cris/ORL; Polk, Chad/TPA; Karen Harrell
Subject: Fw: [EXTERNAL] 435908-1-22-01: West Busch Boulevard (SR 580) Corridor Study

FYI

Sent from my Verizon LG Smartphone

----- Original message-----

From: Jim Rowe
Date: Wed, Jun 20, 2018 12:14 PM
To: brian.shroyer@dot.state.fl.us;
Cc: Barker, Carol/ORL;Koffler, Melanie/ORL;
Subject:[EXTERNAL] 435908-1-22-01: West Busch Boulevard (SR 580) Corridor Study

To:
Brian Shroyer

Name:
Jim Rowe

Agency/Organization:
none

E-Mail:
jrowe12486@aol.com

Comments:
I have a solution...not just for Busch, but ALL. The STATE needs to BAN and text/phone use while driving. Maybe handsfree. Not just a secondary offense....and outright BAN and ENFORCEMENT. I drive this road daily....drivers constantly moving from lane, stayed stopped at green lights....slow down/speed up, when sending/reading texts.

Address:
8627 White Swan Dr

City:
Tampa

State:
FL

Zip:
33614

Please add me to the mailing list to receive project updates:

no

Sent from (ip address): 65.32.54.235

(653254hfc235.tampabay.res.rr.com)

Date/Time: June 20, 2018 12:14 pm

Coming from (referer):

https://urldefense.proofpoint.com/v2/url?u=http-3A_www.fdotd7studies.com_westbuschblvd_send-2Dus-2Dyour-2Dcomments_&d=DwlBaQ&c=OgZOSER8c1RLeytEexU279Q2qk0jVwkrOdYe5iSi-kk&r=loBEJ7vft77GHRWDirJ2otBldtwXGzDxq8JZRDSjRQw&m=ledSOcdzBuC3luMUURvNlkwxRZ_2ljEsfUz10-kLzYE&s=3wKLMqe0pflxZHqn60JUs0CY4LH_cla35LqKMsY7Vio&e=

Using (user agent): Mozilla/5.0 (Macintosh; Intel Mac OS X 10_13_5)

AppleWebKit/605.1.15 (KHTML, like Gecko) Version/11.1.1

Safari/605.1.15

Barker, Rachel/ORL

From: Barker, Carol/ORL
Sent: Wednesday, June 20, 2018 8:58 AM
To: Schooley, Cris/ORL; Rhodes, Jenn/ORL
Subject: Fw: [EXTERNAL] 435908-1-22-01: West Busch Boulevard (SR 580) Corridor Study

FYI

Sent from my Verizon LG Smartphone

----- Original message-----

From: J.E. Marsh
Date: Wed, Jun 20, 2018 8:36 AM
To: brian.shroyer@dot.state.fl.us;
Cc: Barker, Carol/ORL;Koffler, Melanie/ORL;
Subject:[EXTERNAL] 435908-1-22-01: West Busch Boulevard (SR 580) Corridor Study

To:
Brian Shroyer

Name:
J.E. Marsh

Agency/Organization:
ARCO Inc.

E-Mail:
Jem61@gmail.com

Comments:
We DON'T need more crosswalks! We simply need pedestrians to cross in the current designated crosswalk areas & follow the rules.

Address:
10820 N. Florida Ave.

City:
Tampa

State:
Florida

Zip:
33612

Please add me to the mailing list to receive project updates:
no

Sent from (ip address): 47.200.114.62 (47.200.114.62)

Date/Time: June 20, 2018 8:36 am

Coming from (referer):

https://urldefense.proofpoint.com/v2/url?u=http-3A_www.fdotd7studies.com_westbuschblvd_send-2Dus-2Dyour-2Dcomments_&d=DwlBaQ&c=OgZOSER8c1RLeytEexU279Q2qk0jVwkrOdYe5iSi-kk&r=loBEJ7vft77GhrWDirJ2otBldtwXGzDxq8JZRDSjRQw&m=xLijZ0Se_DEG_IB2_iur9r23Hv2ITrgSg0oOHlol6fg&s=6y18-19yNgJnrETRJRaW1yA3vvP4BokuZrZ7vFOM9_8&e=

Using (user agent): Mozilla/5.0 (Linux; Android 7.0; SAMSUNG SM-T810

Build/NRD90M) AppleWebKit/537.36 (KHTML, like Gecko)

SamsungBrowser/7.2 Chrome/59.0.3071.125 Safari/537.36

Barker, Rachel/ORL

From: Barker, Carol/ORL
Sent: Wednesday, June 20, 2018 6:47 PM
To: Polk, Chad/TPA; Schooley, Cris/ORL; Rhodes, Jenn/ORL; Karen Harrell
Subject: Fw: [EXTERNAL] 435908-1-22-01: West Busch Boulevard (SR 580) Corridor Study

Follow Up Flag: Follow up
Flag Status: Completed

FYI

Sent from my Verizon LG Smartphone

----- Original message-----

From: Stan Stroz
Date: Wed, Jun 20, 2018 6:01 PM
To: brian.shroyer@dot.state.fl.us;
Cc: Barker, Carol/ORL;Koffler, Melanie/ORL;
Subject:[EXTERNAL] 435908-1-22-01: West Busch Boulevard (SR 580) Corridor Study

To:
Brian Shroyer

Name:
Stan Stroz

Agency/Organization:
Citizen

E-Mail:
stormirma732@gmail.com

Comments:
One easy solution is to have TPD enforce the actual speed limit.

I take this roadway 5 days per week and fail to see any enforcement of the speed limit. I do however observe TOD and TTPD as well as HCSO vehicles both marked and unmarked surpassing the speed limit and waving from lane to lane passing slower vehicles without responding to a call.

Let us see our LEO'S observe enforce the law by example and then perhaps people will follow.

Next enforce the jay walking law as many pedestrians refuse to use the crosswalk but rather cross whenever and wherever they desire causing pedestrian accidents.

Additional lighting of crosswalk is a waste of money if the crosswalks are not even being used.

All goes back to same issue. Enforce the current laws and see the improvements without spending a dime.

Address:
5399 Corvette Dr

City:
Tampa

State:
Fl

Zip:
33624

Sent from (ip address): 47.196.172.68 (47.196.172.68)

Date/Time: June 20, 2018 6:01 pm

Coming from (referer):

https://urldefense.proofpoint.com/v2/url?u=http-3A_www.fdotd7studies.com_westbuschblvd_send-2Dus-2Dyour-2Dcomments_&d=DwIBaQ&c=OgZOSER8c1RLeytEexU279Q2qk0jVwkrOdYe5iSi-kk&r=loBEJ7vfT77GhrWDirJ2otBldtwXGzDxq8JZRDSjRQw&m=mujrPTc4ui1eGQ85vRv7dYE2G-EeWOGM7Yv1kwJh0U0&s=E43X-vpMfXUY-4gcAMt9F_NPIka-IgFvC_UzpBVisH0&e=

Using (user agent): Mozilla/5.0 (Linux; Android 7.0; SAMSUNG-SM-J727A Build/NRD90M; wv) AppleWebKit/537.36 (KHTML, like Gecko) Version/4.0 Chrome/67.0.3396.87 Mobile Safari/537.36

Barker, Rachel/ORL

From: Karen Harrell <Karen.Harrell@qcausa.com>
Sent: Wednesday, August 30, 2017 8:49 AM
To: Barker, Carol/ORL
Cc: Polk, Chad/TPA; Lori Buck
Subject: West Busch Blvd -PAG documents [EXTERNAL]
Attachments: Busch Blvd PAG comment.pdf; Busch Blvd PAG sign-in.pdf

Follow Up Flag: Follow up
Flag Status: Completed

Carol,

Please see the attached scanned sign in sheets and comment sheet. The comment sheet is from Amy Clark. Ms. Clark was visiting the library and she is a HART bus rider. She started asking questions about the project. Chad spoke with her for a while. She sounded very interested in the PAG and may be a good fit for citizen /pedestrian / transit user representation.

Can you please share her comments with Brian and ask him if he would like us to add her for future PAG invites?

Can you please e-mail me the power point and draft user survey for the meeting summary? Thank you.



Quest Corporation of America, Inc.

Karen Harrell

Senior Associate

c 352.257.1651 | o 866.662.6273 | f 813.926.2962

Karen.Harrell@QCAusa.com | QCAusa.com

Transportation Communicators | DBE/SBE Certified



PROJECT ADVISORY GROUP (PAG) MEETING #1

West Busch Boulevard (SR 580) Corridor Study

From North Dale Mabry Highway to North Nebraska Avenue

Financial Project ID Number: 435908-1-22-01



Tuesday, August 29, 2017 ▪ North Tampa Branch Library, 8916 North Boulevard, Tampa, Florida 33604

Please provide your comments below. If more space is needed, please use the back of this sheet or an additional sheet of paper. You may place your comments in the "Comment Box" provided at the meeting, or send to Brian Shroyer, Project Manager at the address on the back of this comment form, by September 8, 2017. Emailed comments are also acceptable to Brian.Shroyer@dot.state.fl.us

*All comments are part of the project record and are available for review by the public and the media.

Any decisions/plans about changes along West Busch Blvd., from N. Dale Mabry Hwy to N. Nebraska Ave. in Hillsborough Cnty, need to consider the highest ease of use for patrons of the H. A. R. T. bus number 391 that runs on Busch Blvd. I live north of Busch Blvd on N. Nebraska Ave. at Bougainvillea St. I use the bus transfer point at N. Nebraska Ave and Busch Blvd. and get off the bus at Busch Blvd and North Ave. so I can use the North Tampa library at 8916 North Blvd. Remember, bus patrons, like myself are on foot trying to avoid getting injured by inattentive motorists. I am a retired teacher. I would be delighted to serve as a senior citizen and H.A. R. T. bus patron in any focus group or public advisory council. Please contact me. This project is very important. I can help recruit members of the public for your effort to hear the voices of pedestrians and senior/disabled students who do not drive.

CONTACT INFORMATION (PLEASE PRINT)

RETURN COMMENTS BY SEPTEMBER 8, 2017 TO:

Name Amy E. Clark
Title patron user of HART buses
Agency/Organization North Tampa Library
Phone Number 774-273-1544
Email quaker.lady@yahoo.com

Brian L. Shroyer
Multimodal Project Manager
Florida Department of Transportation
11201 N. McKinley Drive, MS 7-500
Tampa, FL 33612
Brian.Shroyer@dot.state.fl.us

Public participation is solicited without regard to race, color, national origin, age, sex, religion, disability or family status.

Barker, Rachel/ORL

From: Barker, Carol/ORL
Sent: Friday, February 02, 2018 2:37 PM
To: Karen Harrell; Schooley, Cris/ORL; Rhodes, Jenn/ORL; Polk, Chad/TPA
Subject: FW: 435908-1 survey comments [EXTERNAL]
Attachments: 435908.pdf

Follow Up Flag: Follow up
Flag Status: Completed

Received these user preference survey's from Brian. So interesting.

Thanks everyone.

Carol Barker, PE
Project Manager

D 407 650 2173
C 407 491 1874
carol.barker@ch2m.com

CH2M is now **JACOBS**
www.ch2m.com | [LinkedIn](#) | [Twitter](#) | [Facebook](#)

From: Shroyer, Brian [mailto:Brian.Shroyer@dot.state.fl.us]
Sent: Tuesday, January 30, 2018 7:41 AM
To: Barker, Carol/ORL <Carol.Barker@CH2M.com>
Subject: 435908-1 survey comments [EXTERNAL]

Carol,

Here are some survey sheets I received in the mail. They are from the following address.

Anke Alberts-Garcia
3409 W Busch Blvd
Tampa, FL 33618

Brian L Shroyer, CPM, FCCM

Multimodal Project Manager
11201 North McKinley Drive
Tampa, FL 33612
(813) 975-6449
Brian.Shroyer@dot.state.fl.us

USER PREFERENCE SURVEY



Please fill out this survey as best as you can and place in the Survey Forms box.

1. Home Zip code: 33618

2. Work/School zip code: 33618

3. Please circle all that apply:

- a. I live on or near Busch Boulevard
- b. I work on or near Busch Boulevard
- c. I shop/visit businesses on or near Busch Boulevard
- d. My school is on or near Busch Boulevard
Which school? _____
- e. I travel on Busch Boulevard but rarely stop
- f. I don't regularly use Busch Boulevard
- g. Other: _____

4. Which travel option(s) have you used on Busch Boulevard? Please circle all that apply:

- a. Drive
- b. Walk
- c. Bike
- d. HART Bus
- e. School Bus
- f. Share a ride with others (car pool, taxi, etc.)
- g. Other: _____

5. When driving on Busch Boulevard, rate your satisfaction on the following (please circle 1 - 5):

Safe	5	4	3	2	1	<u>Unsafe</u>	n/a
Comfortable	5	4	3	2	<u>1</u>	Uncomfortable	n/a
Convenient	5	4	<u>3</u>	2	1	Inconvenient	n/a

Additional Comments: Too Much traffic. Too Fast. No improvements in years. Very UNSAFE.

6. When walking/biking/riding the bus on Busch Boulevard, rate your satisfaction on the following (please circle 1 - 5):

Safe	5	4	3	<u>2</u>	1	Unsafe	n/a
Comfortable	5	4	3	<u>2</u>	1	Uncomfortable	n/a
Convenient	5	4	<u>3</u>	2	1	Inconvenient	n/a

Additional Comments: Does Not Apply fo Bus. Walking Very dangerous

USER PREFERENCE SURVEY



BOULEVARD STUDY

7. How important are the following objectives in your vision for Busch Boulevard?

My Vision is for a Busch Boulevard where...	Very Important	Somewhat Important	Not Important	No Opinion
Transit users have shade and comfortable amenities	✓			
A continuous sidewalk runs the length of the corridor	✓			
Bicycle connectivity is enhanced to adjacent homes and businesses	✓			
Safe and frequent options are provided for pedestrians and bicyclists to cross the corridor	✓			
Aesthetic enhancements provide shade and adds beauty to the corridor	✓			
The corridor adds a unique and inviting character to the community	✓			
Existing businesses are preserved along the corridor			✓	
The roadway attracts reinvestment into the corridor and surrounding areas		✓		
Travelers experience efficient and reliable travel times	✓			
Facilities are ADA compliant		✓		
Citizens with limited mobility have enough time to cross the street	✓			
Congestion is reduced on the corridor	✓	✓		
Crashes occur less frequently and are not as severe	✓			
Drivers slow down to a safe speed	✓	✓		
Increased lighting improves visibility for drivers and pedestrians	✓			
Children can travel to and from school safely, regardless of mode of travel	✓			
Consolidated driveways reduce conflict points for vehicles and pedestrians	✓			

Additional Comments:

Cycle lights and Reduce speeds

USER PREFERENCE SURVEY



BOULEVARD STUDY

8. Which of the following potential improvements would you support?

	Strongly Agree	Agree	Neutral	Disagree	Strongly Disagree
Add additional travel lanes			✓		
Create a pedestrian-only phase at key intersections	✓				
Improve traffic signal timing	✓				
Add on-street parking			✓		
Add mid-block crosswalks	✓				
Landscaping, pedestrian lighting, etc.	✓				
Lower speed limit	✓				
Create a wide sidewalk along the entire corridor		✓			
Add on-street bike lanes		✓			
Add buffered bike lanes		✓			
Add dedicated bus lane		✓			
Add school zone	✓				
Fill in sidewalk gaps	✓				
Add bus shelters		✓			
Add a landscaped median	✓				

Additional Comments:

A new East-West High Capacity Road is needed in North Hills borough. Elevated and Widen Bearrs & Rich from Xpressway to 275

9. Any other comments and suggestions:

New overpass at Dale Mabry & Waters and Dale Mabry & Busch

Thank you for taking the time to complete our User Preference Survey. Your valuable input will help inform the identification of challenges/issues and the development of potential solutions.

USER PREFERENCE SURVEY



Please fill out this survey as best as you can and place in the Survey Forms box.

1. Home Zip code: 33618

2. Work/School zip code: 33606

3. Please circle all that apply:

- | | |
|--|--|
| <input checked="" type="checkbox"/> a. I live on or near Busch Boulevard | <input checked="" type="checkbox"/> e. I travel on Busch Boulevard but rarely stop |
| b. I work on or near Busch Boulevard | f. I don't regularly use Busch Boulevard |
| c. I shop/visit businesses on or near Busch Boulevard | g. Other: _____ |
| d. My school is on or near Busch Boulevard | _____ |
| Which school? _____ | |

4. Which travel option(s) have you used on Busch Boulevard? Please circle all that apply:

- | | |
|--|--|
| <input checked="" type="checkbox"/> a. Drive | e. School Bus |
| b. Walk | f. Share a ride with others (car pool, taxi, etc.) |
| c. Bike | g. Other: _____ |
| d. HART Bus | _____ |

5. When driving on Busch Boulevard, rate your satisfaction on the following (please circle 1 – 5):

Safe	5	4	<input checked="" type="radio"/> 3	2	1	Unsafe	n/a
Comfortable	5	4	<input checked="" type="radio"/> 3	2	1	Uncomfortable	n/a
Convenient	5	4	<input checked="" type="radio"/> 3	2	1	Inconvenient	n/a

Additional Comments: _____

6. When walking/biking/riding the bus on Busch Boulevard, rate your satisfaction on the following (please circle 1 – 5):

Safe	5	4	3	2	1	Unsafe	n/a
Comfortable	5	4	3	2	1	Uncomfortable	n/a
Convenient	5	4	3	2	1	Inconvenient	n/a

Additional Comments: don't bike there

USER PREFERENCE SURVEY



BOULEVARD STUDY

7. How important are the following objectives in your vision for Busch Boulevard?

My Vision is for a Busch Boulevard where...	Very Important	Somewhat Important	Not Important	No Opinion
Transit users have shade and comfortable amenities	✓			
A continuous sidewalk runs the length of the corridor	✓			
Bicycle connectivity is enhanced to adjacent homes and businesses	✓			
Safe and frequent options are provided for pedestrians and bicyclists to cross the corridor	✓			
Aesthetic enhancements provide shade and adds beauty to the corridor	✓			
The corridor adds a unique and inviting character to the community		✓		
Existing businesses are preserved along the corridor		✓		
The roadway attracts reinvestment into the corridor and surrounding areas	✓			
Travelers experience efficient and reliable travel times	✓			
Facilities are ADA compliant				
Citizens with limited mobility have enough time to cross the street	✓			
Congestion is reduced on the corridor		✓		
Crashes occur less frequently and are not as severe	✓			
Drivers slow down to a safe speed	✓			
Increased lighting improves visibility for drivers and pedestrians		✓		
Children can travel to and from school safely, regardless of mode of travel	✓			
Consolidated driveways reduce conflict points for vehicles and pedestrians	✓			

Additional Comments:

USER PREFERENCE SURVEY



8. Which of the following potential improvements would you support?

	Strongly Agree	Agree	Neutral	Disagree	Strongly Disagree
Add additional travel lanes				✓	
Create a pedestrian-only phase at key intersections		✓			
Improve traffic signal timing			✓		
Add on-street parking				✓	
Add mid-block crosswalks		✓			
Landscaping, pedestrian lighting, etc.	✓				
Lower speed limit		✓			
Create a wide sidewalk along the entire corridor		✓			
Add on-street bike lanes				✓	
Add buffered bike lanes		✓			
Add dedicated bus lane		✓			
Add school zone	✓				
Fill in sidewalk gaps	✓				
Add bus shelters	✓				
Add a landscaped median			✓		

Additional Comments:

9. Any other comments and suggestions:

Thank you for taking the time to complete our User Preference Survey. Your valuable input will help inform the identification of challenges/issues and the development of potential solutions.

USER PREFERENCE SURVEY



Please fill out this survey as best as you can and place in the Survey Forms box.

1. Home Zip code: 33618

2. Work/School zip code: 33604

3. Please circle all that apply:

- a. I live on or near Busch Boulevard
- b. I work on or near Busch Boulevard
- c. I shop/visit businesses on or near Busch Boulevard
- d. My school is on or near Busch Boulevard
Which school? _____
- e. I travel on Busch Boulevard but rarely stop
- f. I don't regularly use Busch Boulevard
- g. Other: I Live on Busch Blvd

4. Which travel option(s) have you used on Busch Boulevard? Please circle all that apply:

- a. Drive
- b. Walk
- c. Bike
- d. HART Bus
- e. School Bus
- f. Share a ride with others (car pool, taxi, etc.)
- g. Other: _____

5. When driving on Busch Boulevard, rate your satisfaction on the following (please circle 1 – 5):

Safe	5	4	3	2	1	<u>Unsafe</u>	n/a
Comfortable	5	4	3	2	1	<u>Uncomfortable</u>	n/a
Convenient	5	4	3	2	1	<u>Inconvenient</u>	n/a

Additional Comments: Make a Turn Lane!!

6. When walking/biking/riding the bus on Busch Boulevard, rate your satisfaction on the following (please circle 1 – 5):

Safe	5	4	3	2	1	<u>Unsafe</u>	n/a
Comfortable	5	4	3	2	1	<u>Uncomfortable</u>	n/a
Convenient	5	4	3	2	1	<u>Inconvenient</u>	n/a

Additional Comments: _____

USER PREFERENCE SURVEY



BOULEVARD SURVEY

7. How important are the following objectives in your vision for Busch Boulevard?

My Vision is for a Busch Boulevard where...	Very Important	Somewhat Important	Not Important	No Opinion
Transit users have shade and comfortable amenities				✓
A continuous sidewalk runs the length of the corridor	✓			
Bicycle connectivity is enhanced to adjacent homes and businesses				✓
Safe and frequent options are provided for pedestrians and bicyclists to cross the corridor	✓			
Aesthetic enhancements provide shade and adds beauty to the corridor		✓		
The corridor adds a unique and inviting character to the community		✓		
Existing businesses are preserved along the corridor		✓		
The roadway attracts reinvestment into the corridor and surrounding areas			✓	
Travelers experience efficient and reliable travel times	✓			
Facilities are ADA compliant				✓
Citizens with limited mobility have enough time to cross the street				✓
Congestion is reduced on the corridor	✓			
Crashes occur less frequently and are not as severe	✓			
Drivers slow down to a safe speed	✓			
Increased lighting improves visibility for drivers and pedestrians		✓		
Children can travel to and from school safely, regardless of mode of travel	✓			
Consolidated driveways reduce conflict points for vehicles and pedestrians	✓			

Additional Comments:

we need a turn lane

USER PREFERENCE SURVEY



CELEBRATING 100 YEARS OF WEST BUSCH

8. Which of the following potential improvements would you support?

	Strongly Agree	Agree	Neutral	Disagree	Strongly Disagree
Add additional travel lanes (turn lane)	✓				
Create a pedestrian-only phase at key intersections	✓				
Improve traffic signal timing		✓			
Add on-street parking					✓
Add mid-block crosswalks					✓
Landscaping, pedestrian lighting, etc.					✓
Lower speed limit	✓	Agree			
Create a wide sidewalk along the entire corridor					✓
Add on-street bike lanes					✓
Add buffered bike lanes			✓		
Add dedicated bus lane					✓
Add school zone			✓		
Fill in sidewalk gaps			✓		
Add bus shelters					✓
Add a landscaped median					✓

Additional Comments:

create a turn lane for those of us that live on Busch Blvd.

9. Any other comments and suggestions:

turn lane & reduce speeds so those of us that live on busch don't get rear ended or can't even get out of our driveways

Thank you for taking the time to complete our User Preference Survey. Your valuable input will help inform the identification of challenges/issues and the development of potential solutions.

USER PREFERENCE SURVEY



Please fill out this survey as best as you can and place in the Survey Forms box.

1. Home Zip code: 33618

2. Work/School zip code: 33602

3. Please circle all that apply:

- a. I live on or near Busch Boulevard
- b. I work on or near Busch Boulevard
- c. I stop/visit ~~businesses~~ ~~on or near~~ Busch Boulevard
- d. My school is on or near Busch Boulevard
Which school? _____
- e. I travel on Busch Boulevard but rarely stop
- f. I don't regularly use Busch Boulevard
- g. Other: _____

4. Which travel option(s) have you used on Busch Boulevard? Please circle all that apply:

- a. Drive
- b. Walk
- c. Bike
- d. HART Bus
- e. School Bus
- f. Share a ride with others (car pool, taxi, etc.)
- g. Other: _____

5. When driving on Busch Boulevard, rate your satisfaction on the following (please circle 1 - 5):

Safe	5	4	3	2	<input checked="" type="radio"/> 1	Unsafe	n/a
Comfortable	5	4	3	2	<input checked="" type="radio"/> 1	Uncomfortable	n/a
Convenient	5	4	<input checked="" type="radio"/> 3	2	1	Inconvenient	n/a

Additional Comments: Impossible to get out of driveway at certain times

6. When walking/biking/riding the bus on Busch Boulevard, rate your satisfaction on the following (please circle 1 - 5):

Safe	5	4	3	2	<input checked="" type="radio"/> 1	Unsafe	n/a
Comfortable	5	4	3	2	<input checked="" type="radio"/> 1	Uncomfortable	n/a
Convenient	5	4	3	2	<input checked="" type="radio"/> 1	Inconvenient	n/a

Additional Comments: _____

USER PREFERENCE SURVEY



7. How important are the following objectives in your vision for Busch Boulevard?

My Vision is for a Busch Boulevard where...	Very Important	Somewhat Important	Not Important	No Opinion
Transit users have shade and comfortable amenities			X	
A continuous sidewalk runs the length of the corridor	X			
Bicycle connectivity is enhanced to adjacent homes and businesses		X		
Safe and frequent options are provided for pedestrians and bicyclists to cross the corridor	X			
Aesthetic enhancements provide shade and adds beauty to the corridor			X	
The corridor adds a unique and inviting character to the community				X
Existing businesses are preserved along the corridor			X	
The roadway attracts reinvestment into the corridor and surrounding areas			X	
Travelers experience efficient and reliable travel times	X			
Facilities are ADA compliant			X	
Citizens with limited mobility have enough time to cross the street	X			
Congestion is reduced on the corridor	X			
Crashes occur less frequently and are not as severe	X			
Drivers slow down to a safe speed	X			
Increased lighting improves visibility for drivers and pedestrians	X			
Children can travel to and from school safely, regardless of mode of travel	X			
Consolidated driveways reduce conflict points for vehicles and pedestrians	X			

Additional Comments:

USER PREFERENCE SURVEY



BOULEVARD STUDY

8. Which of the following potential improvements would you support?

	Strongly Agree	Agree	Neutral	Disagree	Strongly Disagree
Add additional travel lanes		X			
Create a pedestrian-only phase at key intersections		X			
Improve traffic signal timing	X				
Add on-street parking				X	
Add mid-block crosswalks			X		
Landscaping, pedestrian lighting, etc.		X			
Lower speed limit		X			
Create a wide sidewalk along the entire corridor		X			
Add on-street bike lanes		X			
Add buffered bike lanes		X			
Add dedicated bus lane				X	
Add school zone			X		
Fill in sidewalk gaps		X			
Add bus shelters			X		
Add a landscaped median			X		

Additional Comments:

9. Any other comments and suggestions:

Too congested, unsafe to turn into or out of driveway

Thank you for taking the time to complete our User Preference Survey. Your valuable input will help inform the identification of challenges/issues and the development of potential solutions.

Barker, Rachel/ORL

From: Barker, Carol/ORL
Sent: Thursday, March 01, 2018 7:35 AM
To: Polk, Chad/TPA; Hernandez, David/TPA; Schooley, Cris/ORL
Subject: Fw: 435908-1 Survey response [EXTERNAL]
Attachments: Survey.pdf

Follow Up Flag: Follow up
Flag Status: Completed

FYI.

Sent from my Verizon LG Smartphone

----- Original message-----

From: Shroyer, Brian
Date: Thu, Mar 1, 2018 7:17 AM
To: Barker, Carol/ORL;
Cc:
Subject: 435908-1 Survey response [EXTERNAL]

Carol,

I received this the other day. Here is a copy for your records.

Thanks,

Brian L Shroyer, CPM, FCCM

Multimodal Project Manager
11201 North McKinley Drive
Tampa, FL 33612
(813) 975-6449
Brian.Shroyer@dot.state.fl.us

USER PREFERENCE SURVEY



Please fill out this survey as best as you can and place in the Survey Forms box.

1. Home Zip code: 33625 - Citrus Park

2. Work/School zip code: 33612 - FDOT D7

3. Please circle all that apply:

- a. I live on or near Busch Boulevard
- b. I work on or near Busch Boulevard
- c. I shop/visit businesses on or near Busch Boulevard
- d. My school is on or near Busch Boulevard
Which school? _____
- e. I travel on Busch Boulevard but rarely stop
- f. I don't regularly use Busch Boulevard
- g. Other: _____

4. Which travel option(s) have you used on Busch Boulevard? Please circle all that apply:

- a. Drive
- b. Walk
- c. Bike
- d. HART Bus (Bus is inconvenient - takes too long to destination)
- e. School Bus
- f. Share a ride with others (car pool, taxi, etc.)
- g. Other: _____

5. When driving on Busch Boulevard, rate your satisfaction on the following (please circle 1 – 5):

Safe	5	4	3	2	1	<u>Unsafe</u>	n/a
Comfortable	5	4	3	2	1	<u>Uncomfortable</u>	n/a
Convenient	5	4	3	2	1	<u>Inconvenient</u>	n/a

Additional Comments: Feels unsafe with double line and no median or turn lane from Dale Mabry to Armenia, cars travelling in left thru lane of opposite direction often drift (cell phone use) into oncoming traffic. Students/parents perform unsafe left turns from SB Ola onto EB Busch next to Chamberlain HS - drivers creeping through stopped WB traffic lanes try to turn left in front of oncoming traffic in EB lanes of Busch. Left EB Busch travel lane turning left/NB to Ola.

6. When walking/biking/riding the bus on Busch Boulevard, rate your satisfaction on the following (please circle 1 – 5):

Safe	5	4	3	2	1	<u>Unsafe</u>	n/a
Comfortable	5	4	3	2	1	<u>Uncomfortable</u>	n/a
Convenient	5	4	3	2	1	<u>Inconvenient</u>	n/a

Additional Comments: Pedestrians from temp employment office cross Busch midblock to get to bus stop or convenience store on north side. Students in particular practice unsafe crossing habits when running late for school.

USER PREFERENCE SURVEY



7. How important are the following objectives in your vision for Busch Boulevard?

My Vision is for a Busch Boulevard where...	Very Important	Somewhat Important	Not Important	No Opinion
Transit users have shade and comfortable amenities		X		
A continuous sidewalk runs the length of the corridor		X		
Bicycle connectivity is enhanced to adjacent homes and businesses		X		
Safe and frequent options are provided for pedestrians and bicyclists to cross the corridor - emphasis on student safety		X		
Aesthetic enhancements provide shade and adds beauty to the corridor			X	
The corridor adds a unique and inviting character to the community			X	
Existing businesses are preserved along the corridor			X (ROW needed)	
The roadway attracts reinvestment into the corridor and surrounding areas			X	
Travelers experience efficient and reliable travel times	**X**			
Facilities are ADA compliant		X		
Citizens with limited mobility have enough time to cross the street		X		
Congestion is reduced on the corridor	**X**			
Crashes occur less frequently and are not as severe	X			
Drivers slow down to a safe speed		X		
Increased lighting improves visibility for drivers and pedestrians		X		
Children can travel to and from school safely, regardless of mode of travel		X		
Consolidated driveways reduce conflict points for vehicles and pedestrians		X		

Additional Comments:

Chamberlain students have been observed running/dodging through traffic to cross Busch heading south, then deliberately racing to cross the CSX ROW ahead of oncoming train.

USER PREFERENCE SURVEY



8. Which of the following potential improvements would you support?

	Strongly Agree	Agree	Neutral	Disagree	Strongly Disagree
Add additional travel lanes	XXX				
Create a pedestrian-only phase at key intersections		X			
Improve traffic signal timing	XXX				
Add on-street parking					X
Add mid-block crosswalks					X
Landscaping, pedestrian lighting, etc.				X	
Lower speed limit					X
Create a wide sidewalk along the entire corridor	X				
Add on-street bike lanes			X		
Add buffered bike lanes			X		
Add dedicated bus lane (consider bus bays if possible)	X				
Add school zone			X		
Fill in sidewalk gaps		X			
Add bus shelters		X			
Add a landscaped median (consider pedestrian refuge)		X			

Additional Comments:

The traffic signal timing along this corridor negatively impacts congestion. Improvements to the signal timing will allow traffic to flow in a continuous manner. Synchronizing the signals to turn green or red in a sequenced pattern would smooth out traffic flow.

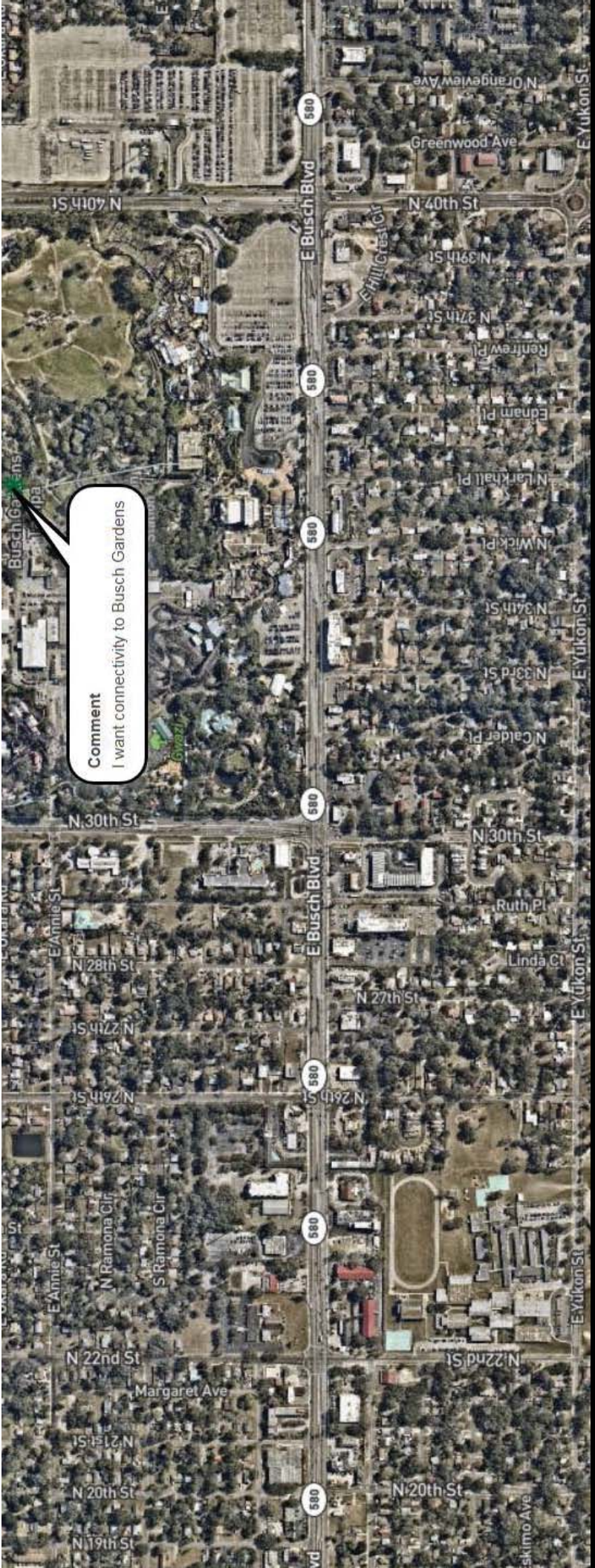
9. Any other comments and suggestions:

The signal at Busch and Armenia is among the worst in the corridor. However, inbound and outbound Chamberlain traffic attempting to make left turns onto Ola from Busch or left turns from Ola onto Busch clog traffic flow also. A center median preventing left turns would improve safety at this intersection. Students from Chamberlain HS have been observed dodging traffic mid block crossing or at the North Blvd crosswalk without waiting for the pedestrian crossing signal. They have also been observed deliberately racing to cross the CSX tracks before an oncoming train reaches the intersection at North Blvd - this risk taking behavior poses a major safety hazard to the students.

Thank you for taking the time to complete our User Preference Survey. Your valuable input will help inform the identification of challenges/issues and the development of potential solutions.

Section 7.2

WikiMapping Comments

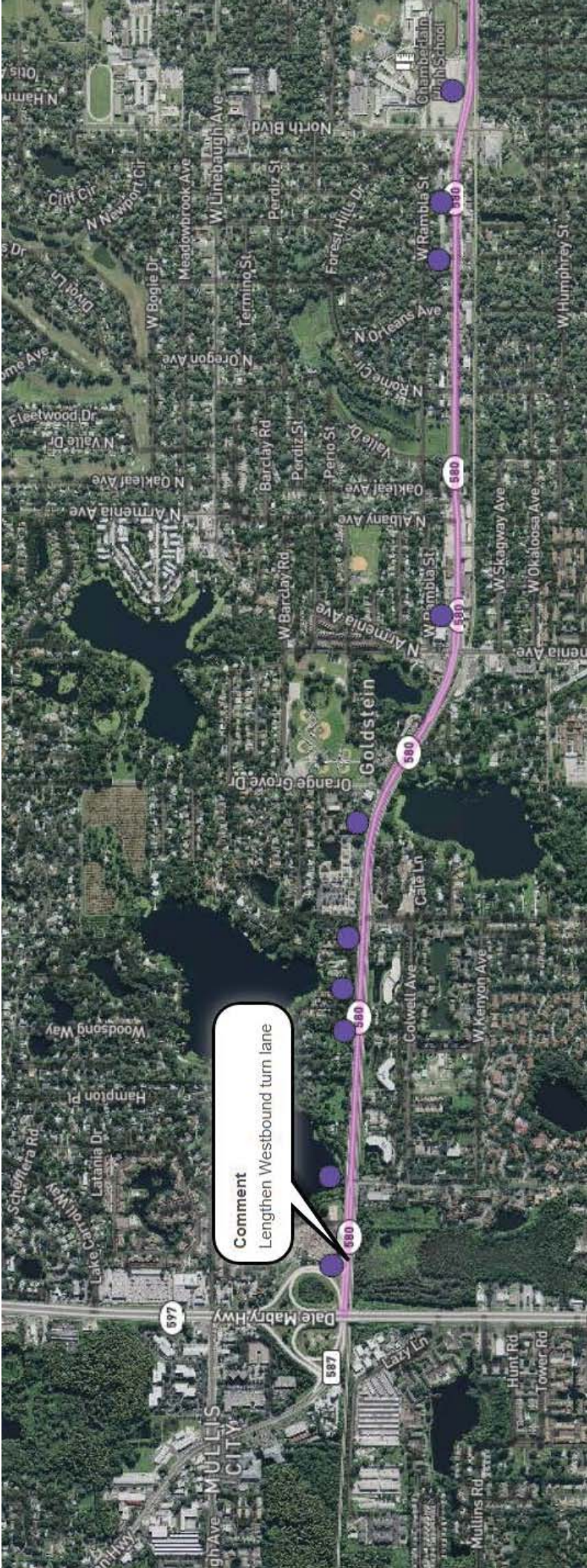


Comment
I want connectivity to Busch Gardens

Connectivity | 20-Jun-17

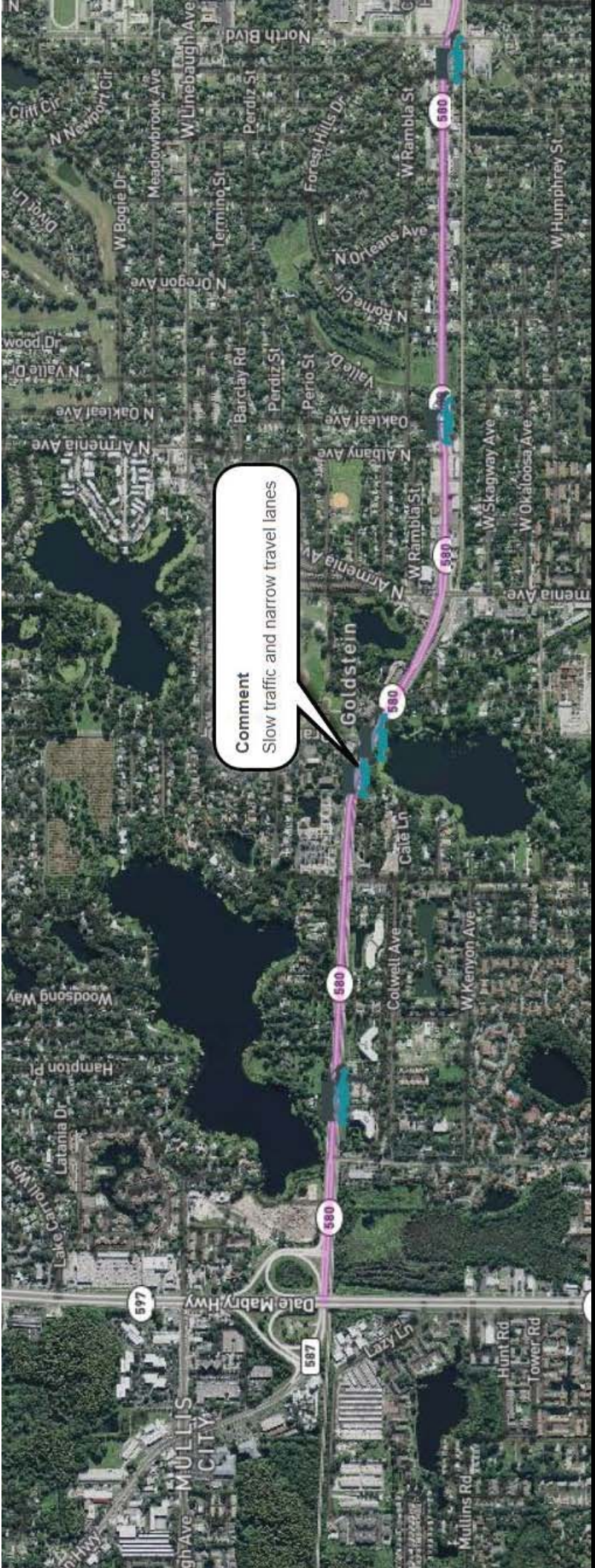


Community | 20-Jun-17



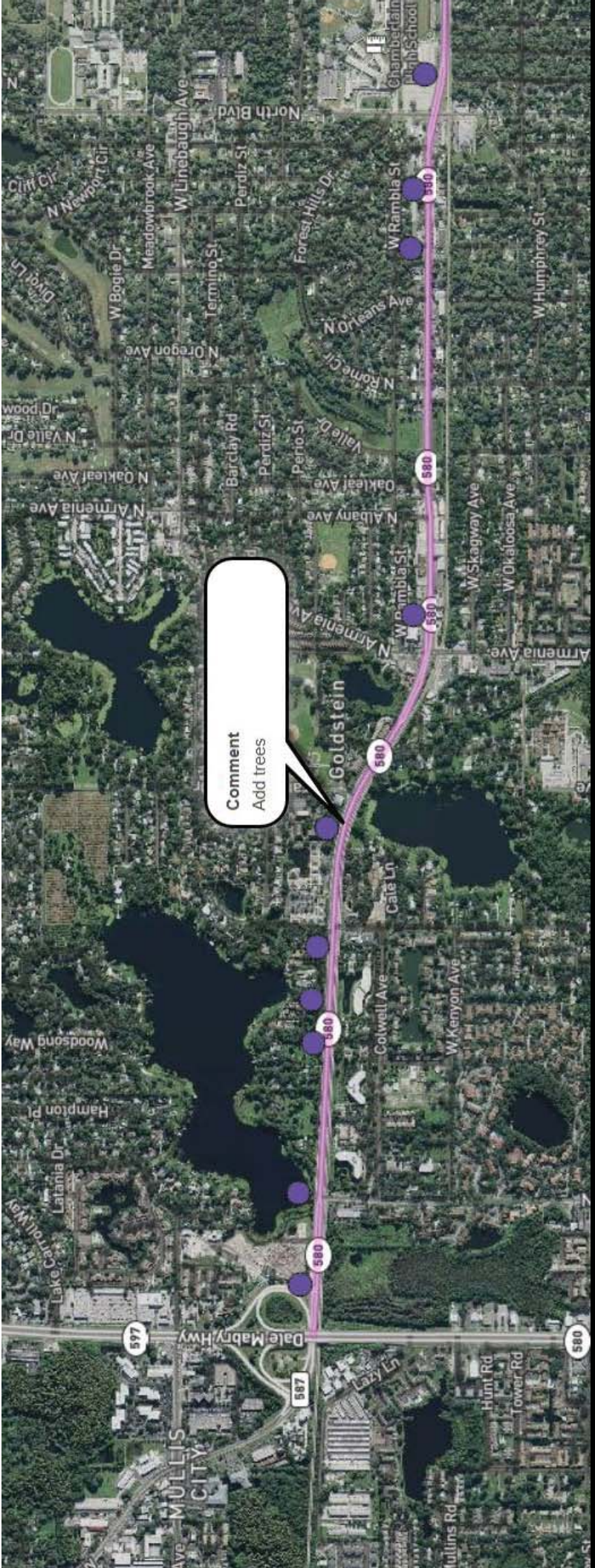
Comment
Lengthen Westbound turn lane

Other | 29-Jun-17



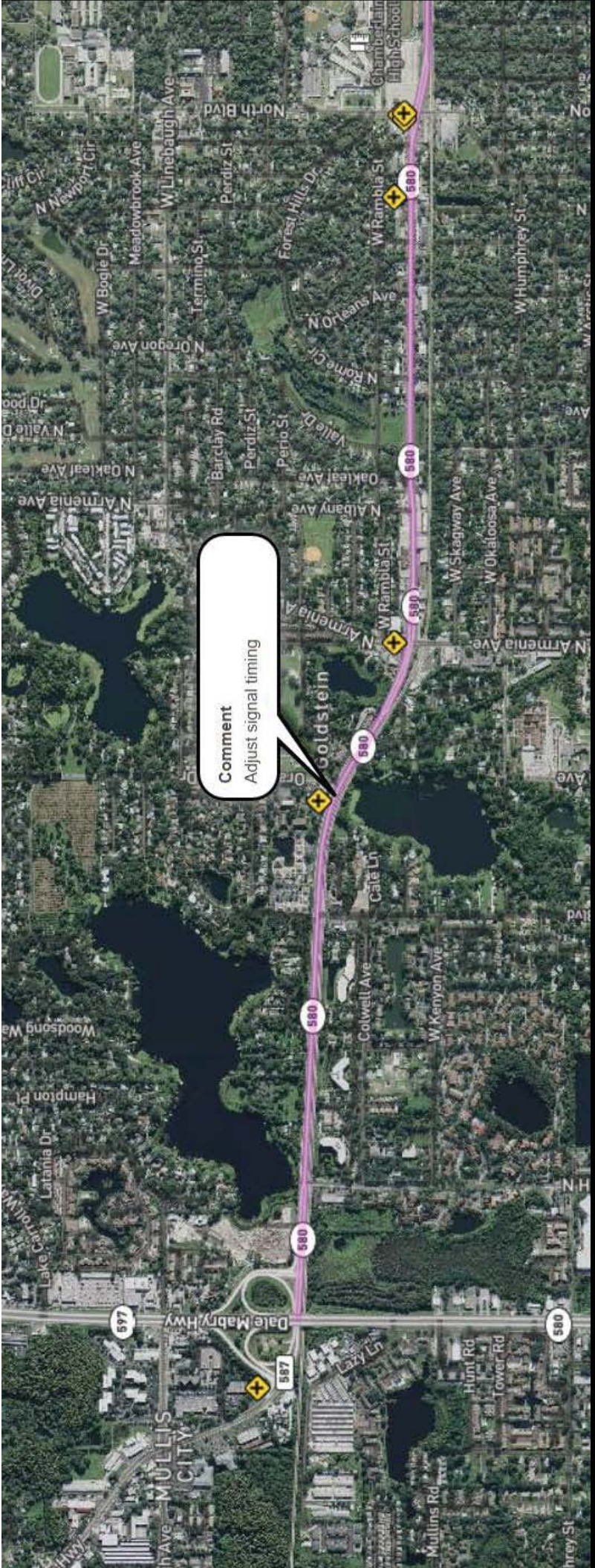
Traffic / Parking

29-Jun-17



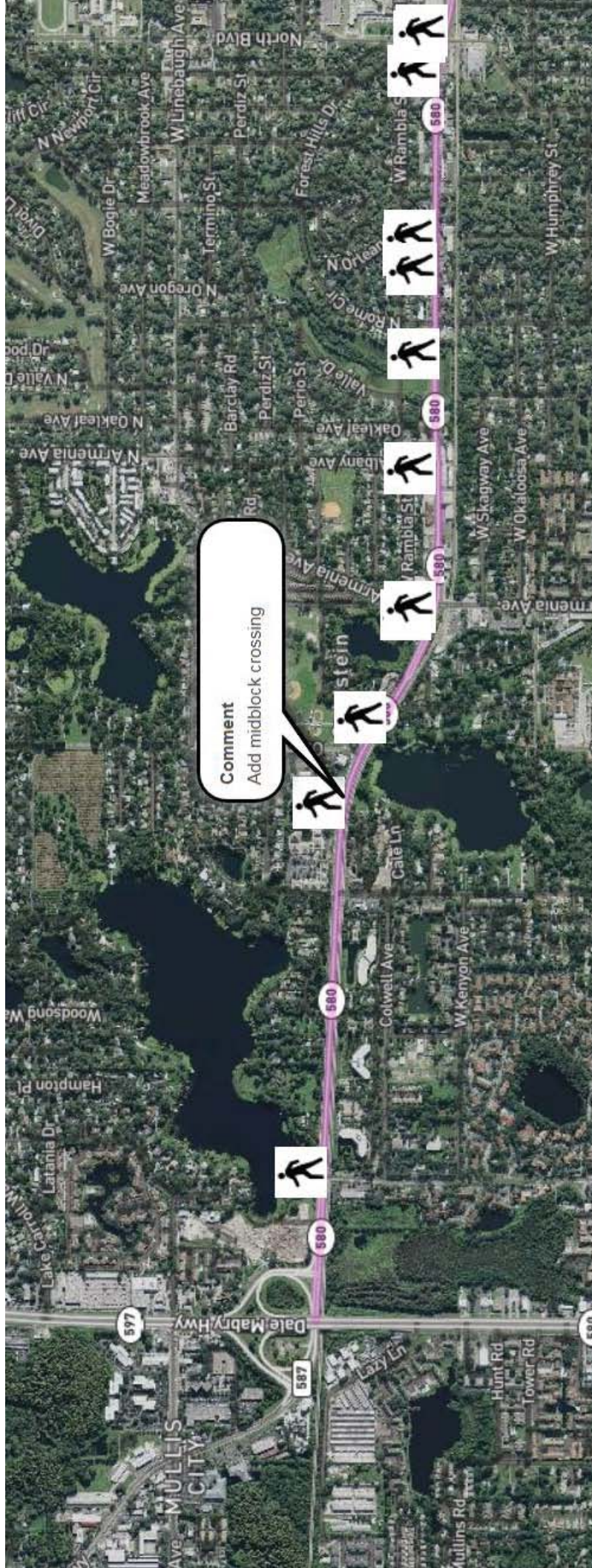
Comment
Add trees

Other | 29-Jun-17



Comment
Adjust signal timing

Intersection | 29-Jun-17



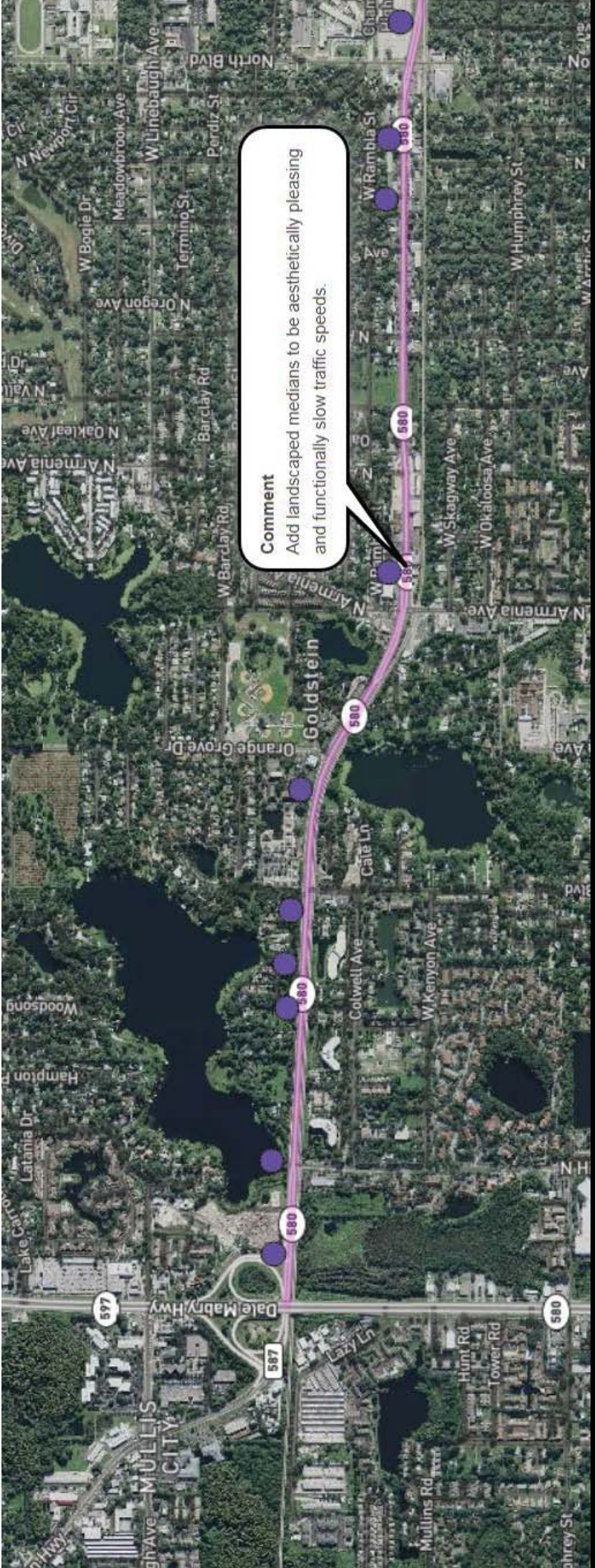
Comment
Add midblock crossing

Pedestrian | 29-Jun-17



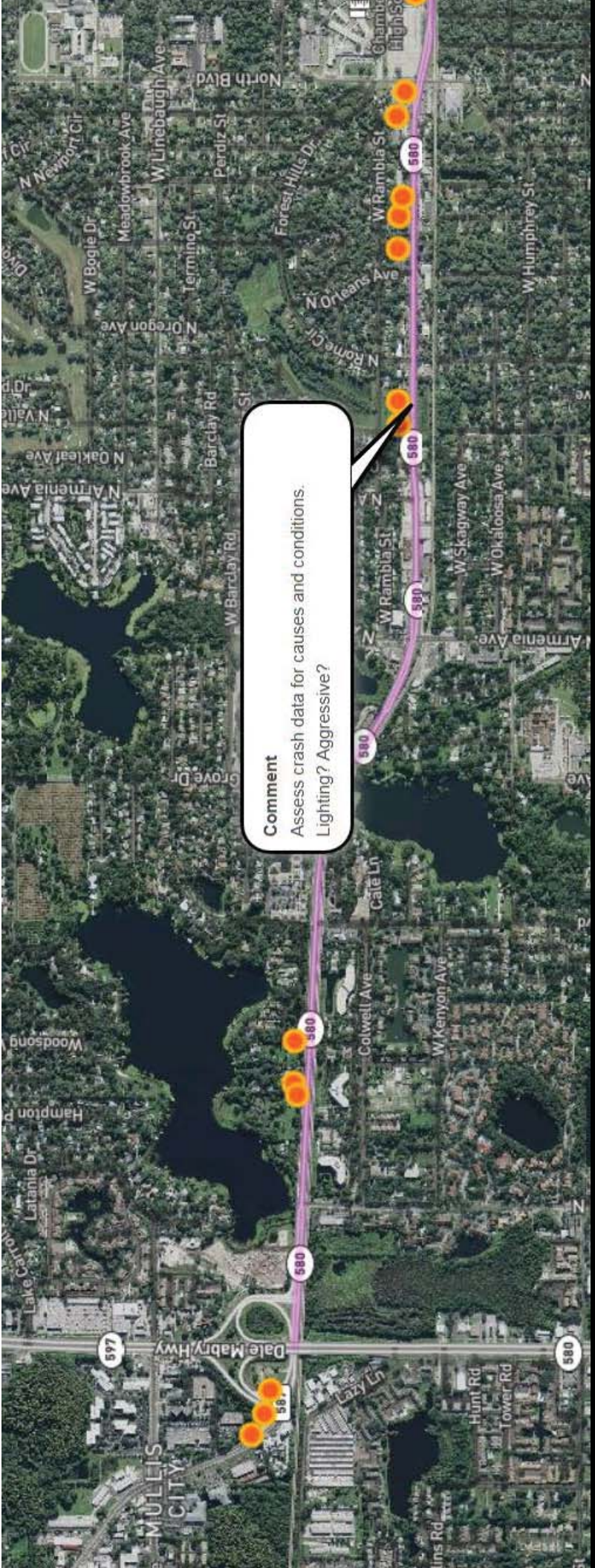
Comment
Flat concrete medians need to be eliminated. They are being used as sidewalk.

Pedestrian | 29-Jun-17



Comment
Add landscaped medians to be aesthetically pleasing and functionally slow traffic speeds.

Other | 29-Jun-17



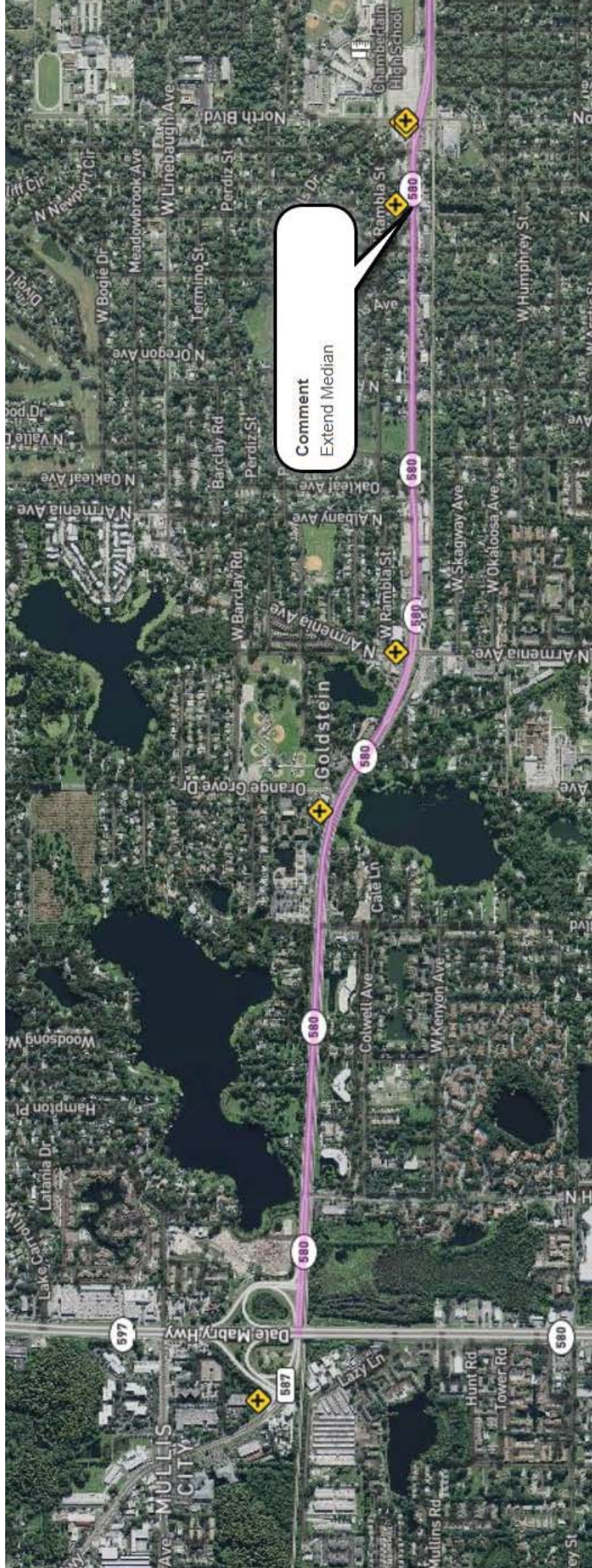
Comment
Assess crash data for causes and conditions.
Lighting? Aggressive?

Safety | 29-Jun-17



Comment
Add sidewalk on south side of Busch.

Pedestrian | 29-Jun-17



Comment
Extend Median

Intersection | 29-Jun-17



Comment
Lighting

Pedestrian | 29-Jun-17



Comment
Pedestrian lighting

Pedestrian | 29-Jun-17

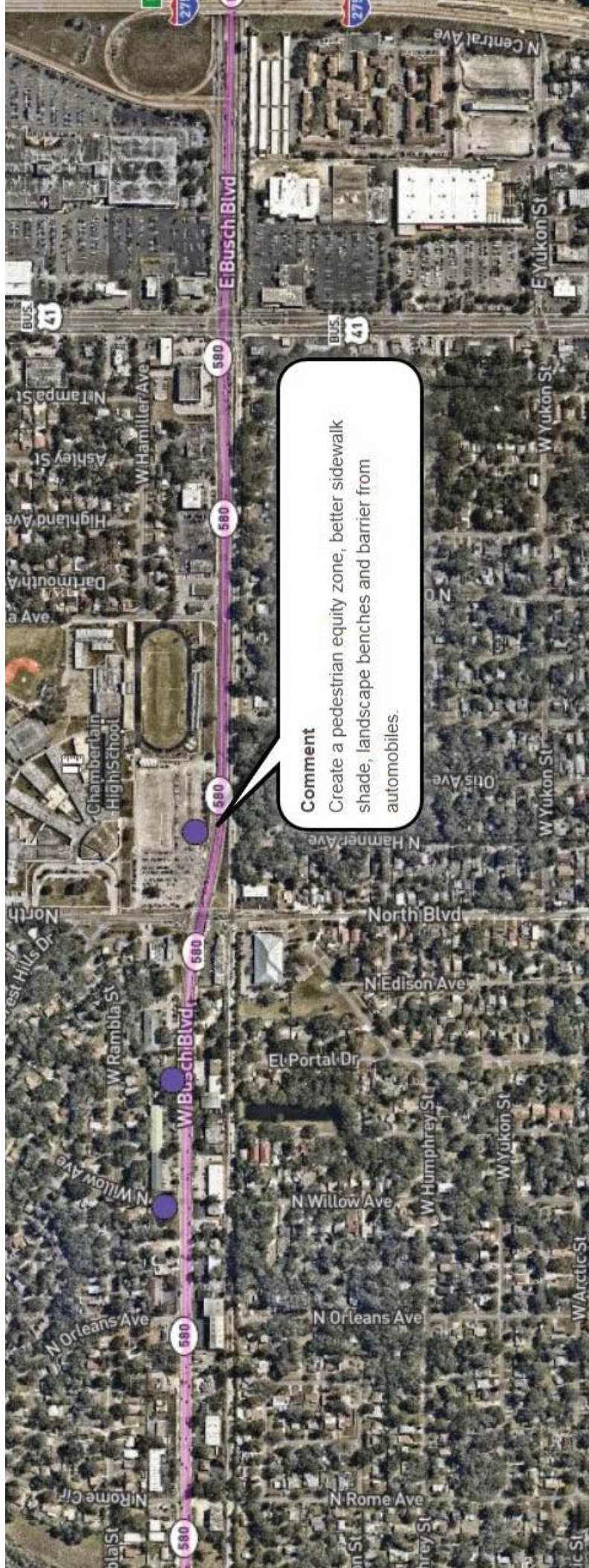


Comment
Needs to be a very visible crosswalk and narrower turning radii

Pedestrian | 29-Jun-17

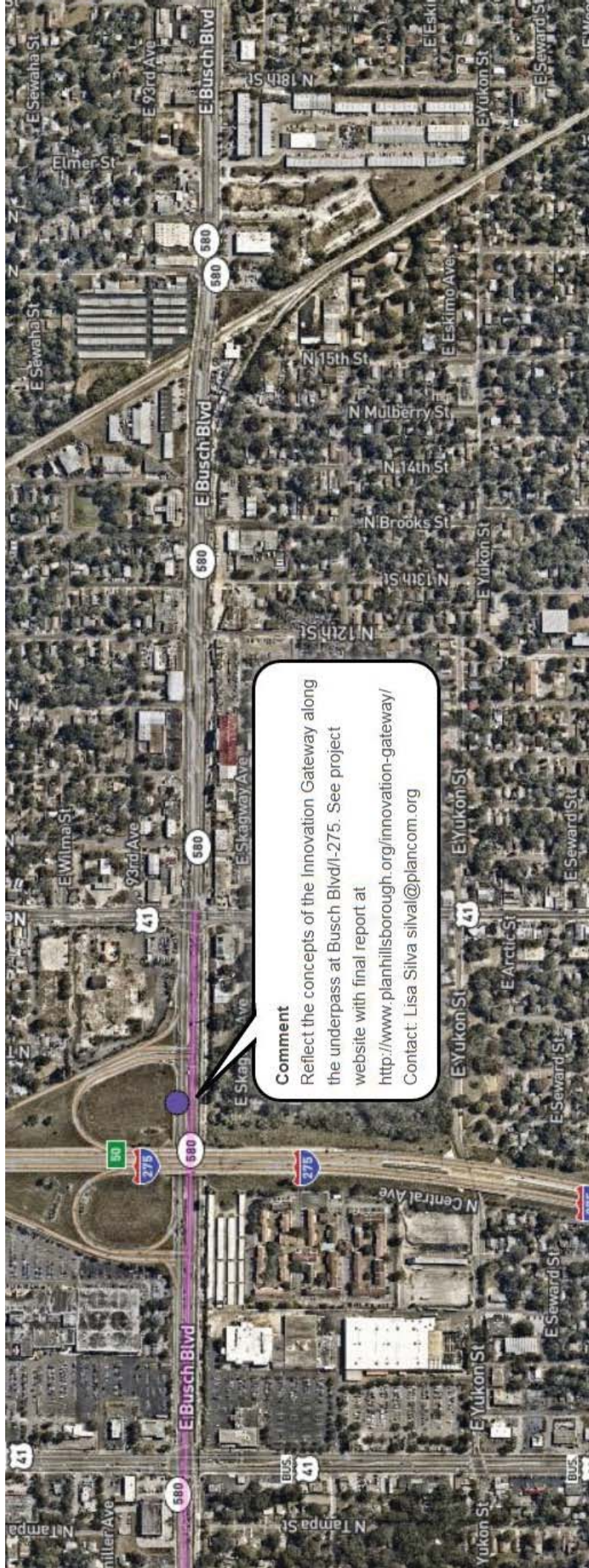


Safety | 29-Jun-17



Comment
Create a pedestrian equity zone, better sidewalk shade, landscape benches and barrier from automobiles.

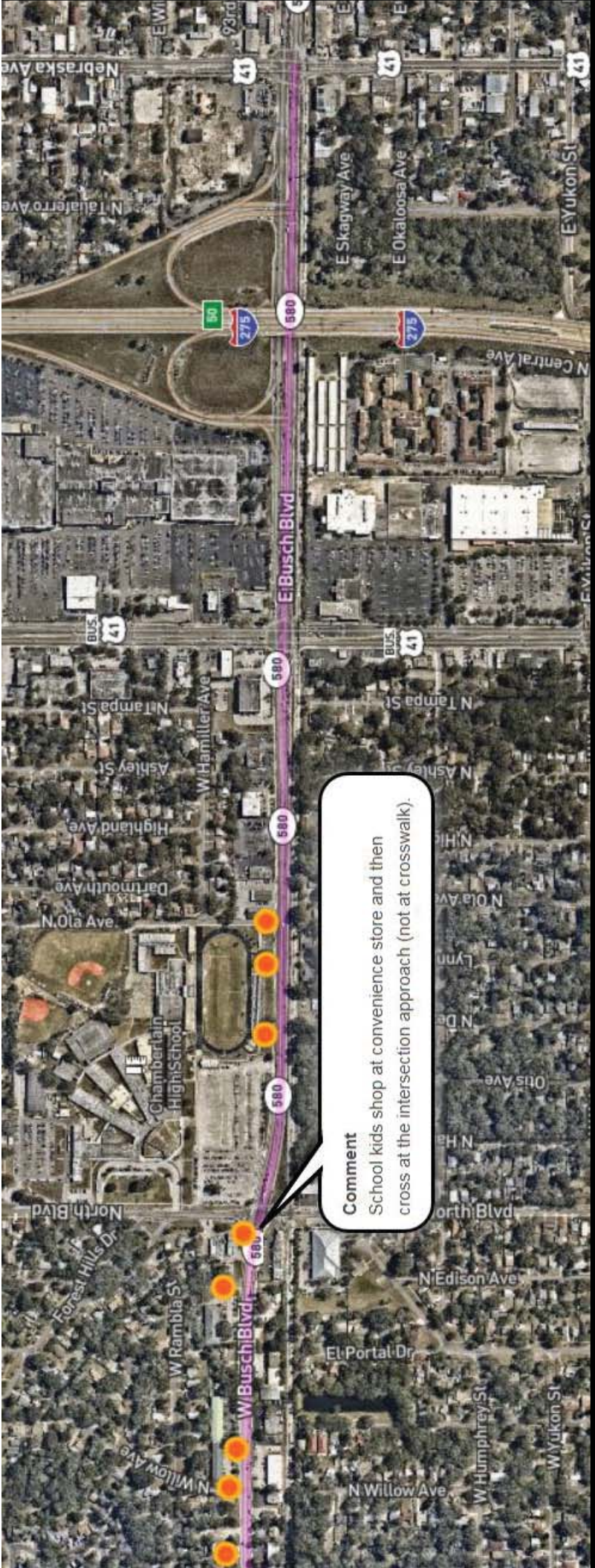
Other | 29-Jun-17



Comment

Reflect the concepts of the Innovation Gateway along the underpass at Busch Blvd/I-275. See project website with final report at <http://www.planhillsborough.org/innovation-gateway/>
Contact: Lisa Silva silval@plancom.org

Other | 8-Aug-17



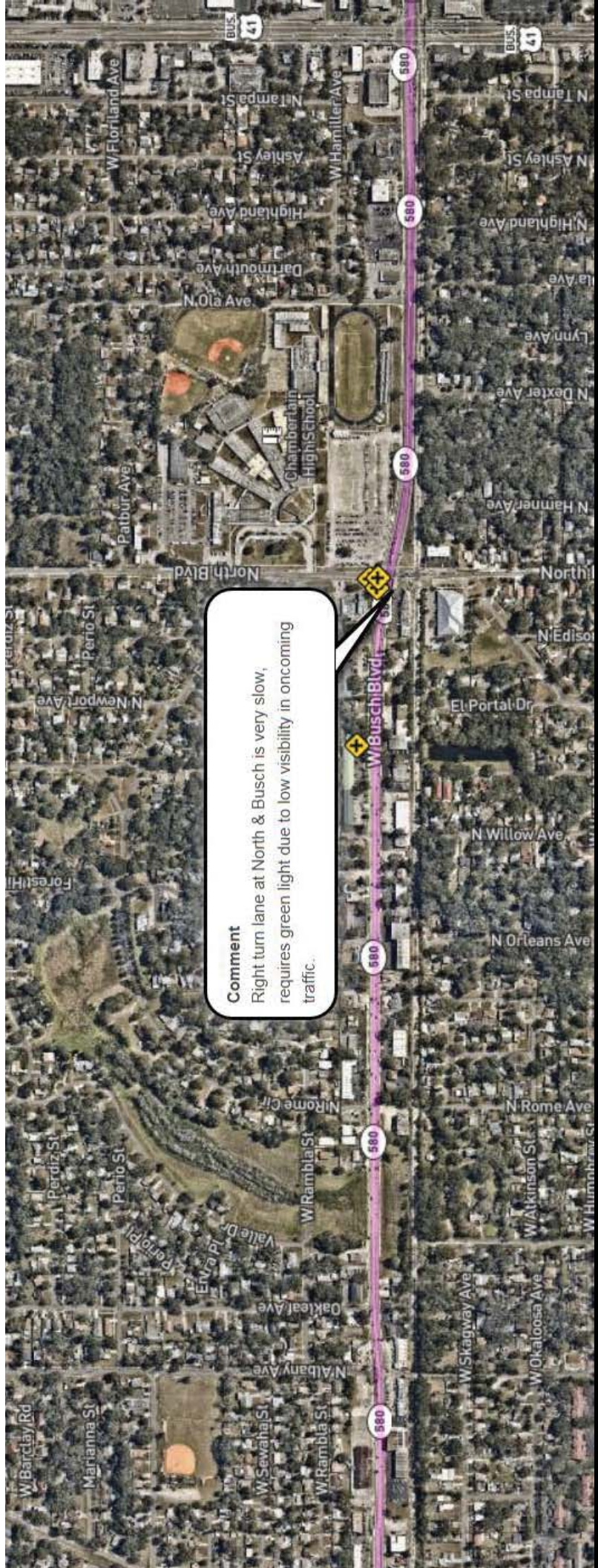
Safety | 3-oct-17

E-1068



Comment
Provide median refuge islands proximate to side street access between Armenia and N. Blvd.

Safety | 26-Oct-17



Comment
Right turn lane at North & Busch is very slow,
requires green light due to low visibility in oncoming
traffic.

Intersection | 13-Dec-17



Comment
North Blvd. & Busch Intersection in the AM is bad.

Intersection | 13-Dec-17



Comment
I would like not to get run over at the crosswalk.

Pedestrian | 13-Dec-17

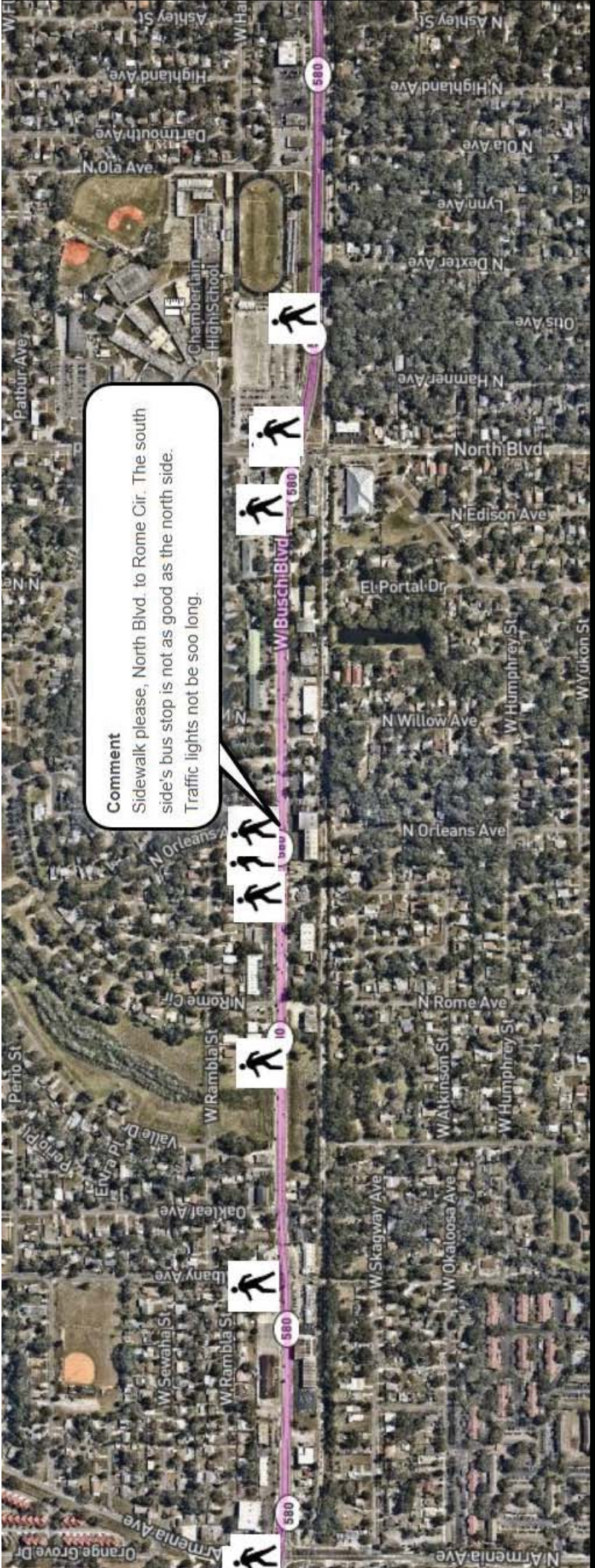


Pedestrian | 13-Dec-17



Comment
Complete S/W gaps. E/W movement by Peds/Biker may be limited because of lack of sidewalk.

Pedestrian | 13-Dec-17

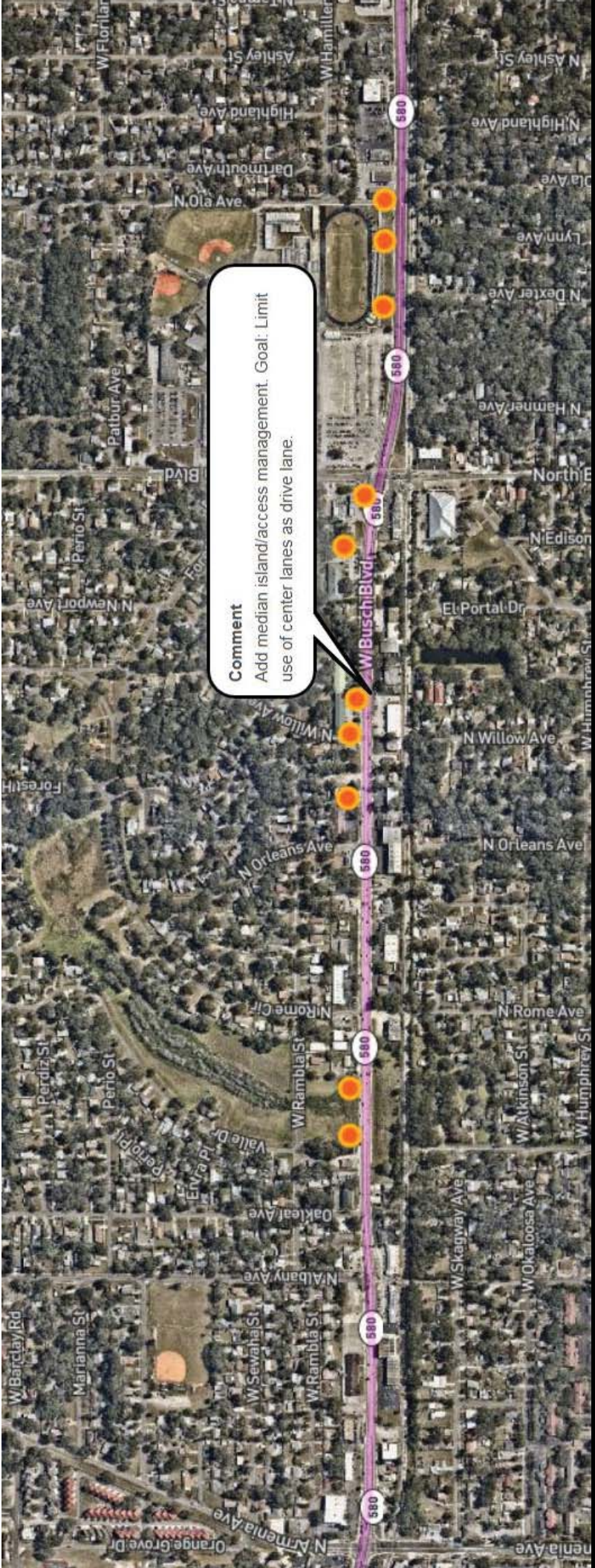


Comment
Sidewalk please, North Blvd. to Rome Cir. The south side's bus stop is not as good as the north side. Traffic lights not be soo long.

Pedestrian | 13-Dec-17



Other | 13-Dec-17



Comment
Add median island/access management. Goal: Limit use of center lanes as drive lane.

Safety | 13-Dec-17

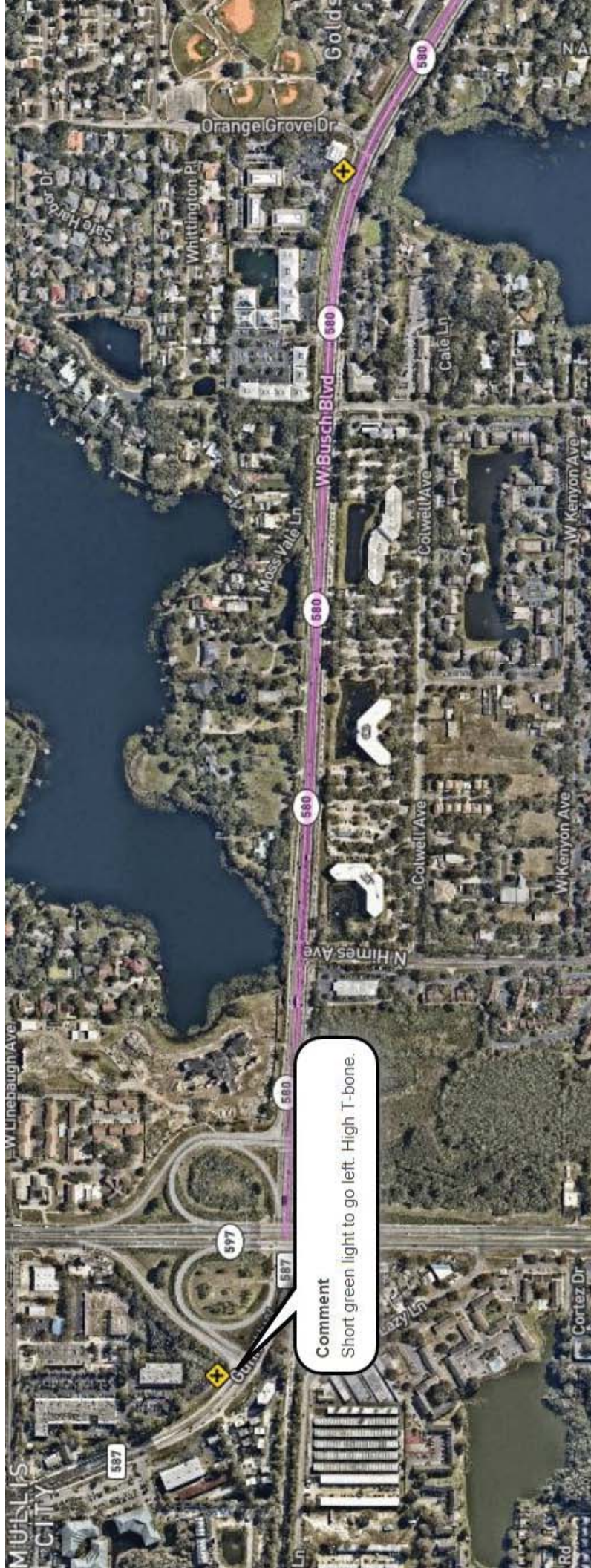


Comment
Light and landscape entire corridor.

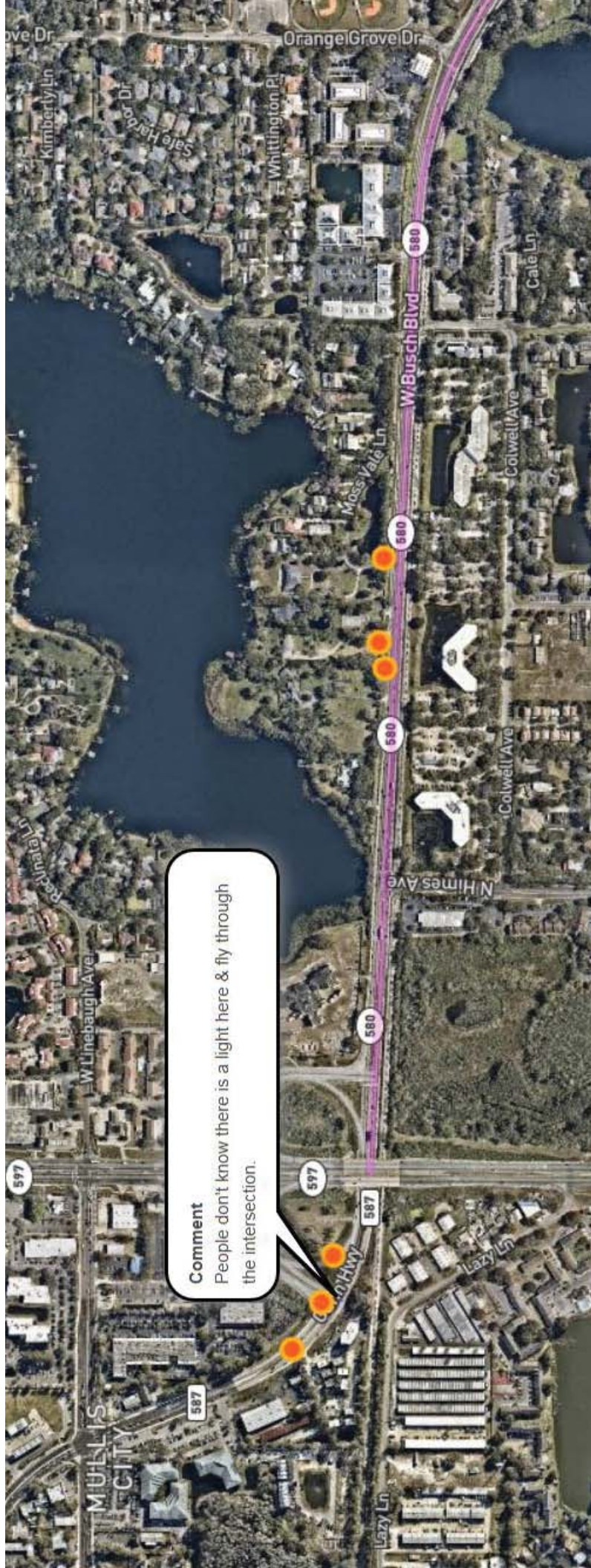
Other | 13-Dec-17



Safety | 13-Dec-17



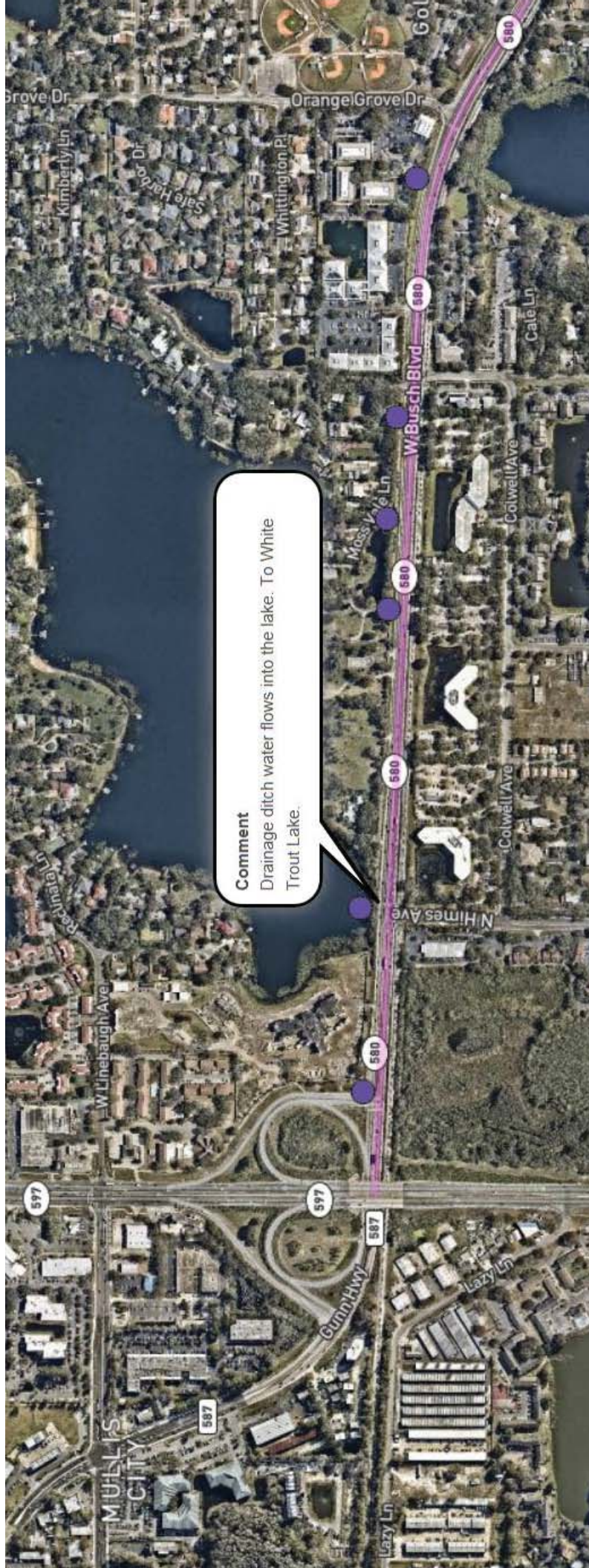
Intersection | 13-Dec-17



Safety | 13-Dec-17



Safety | 13-Dec-17



Other | 13-Dec-17



Bus / Transit | 13-Dec-17



Pedestrian | 13-Dec-17



Traffic / Parking | 13-Dec-17



Comment
Lower speed limit.

Traffic / Parking | 13-Dec-17



Safety | 13-Dec-17

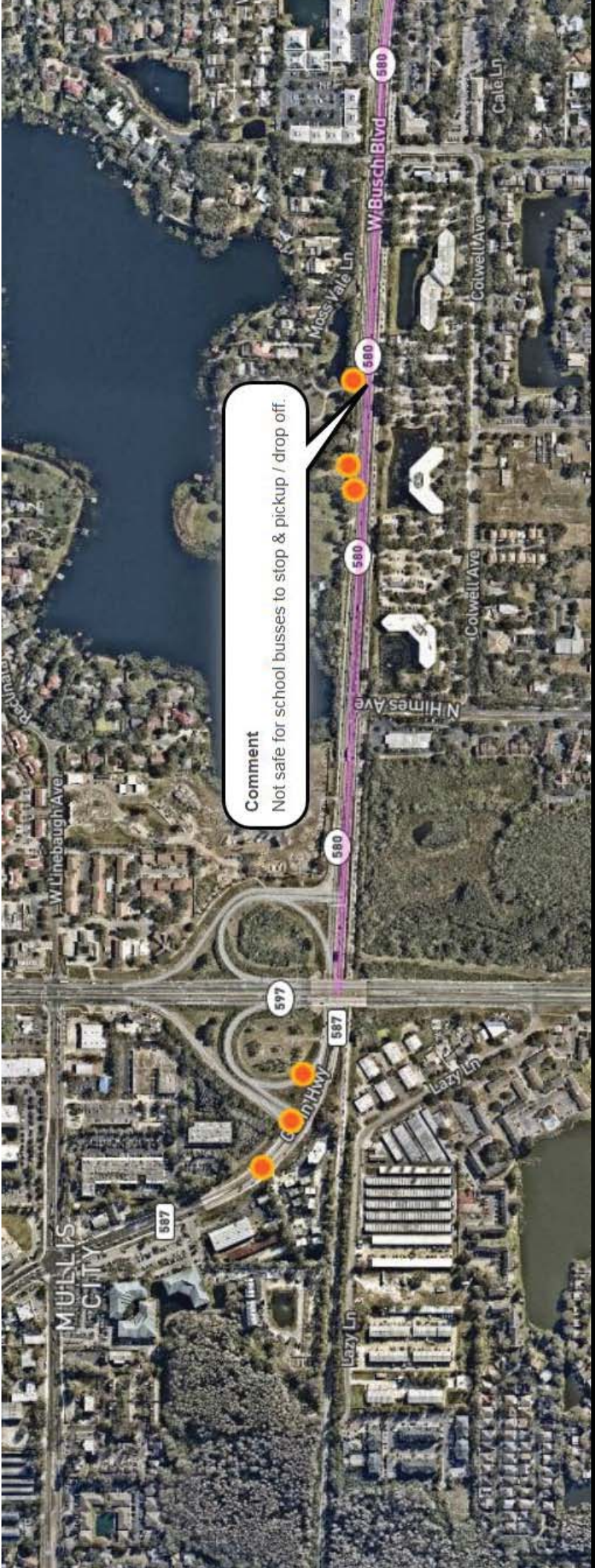


Connectivity | 13-Dec-17



Comment
More police enforcement.

Safety | 13-Dec-17



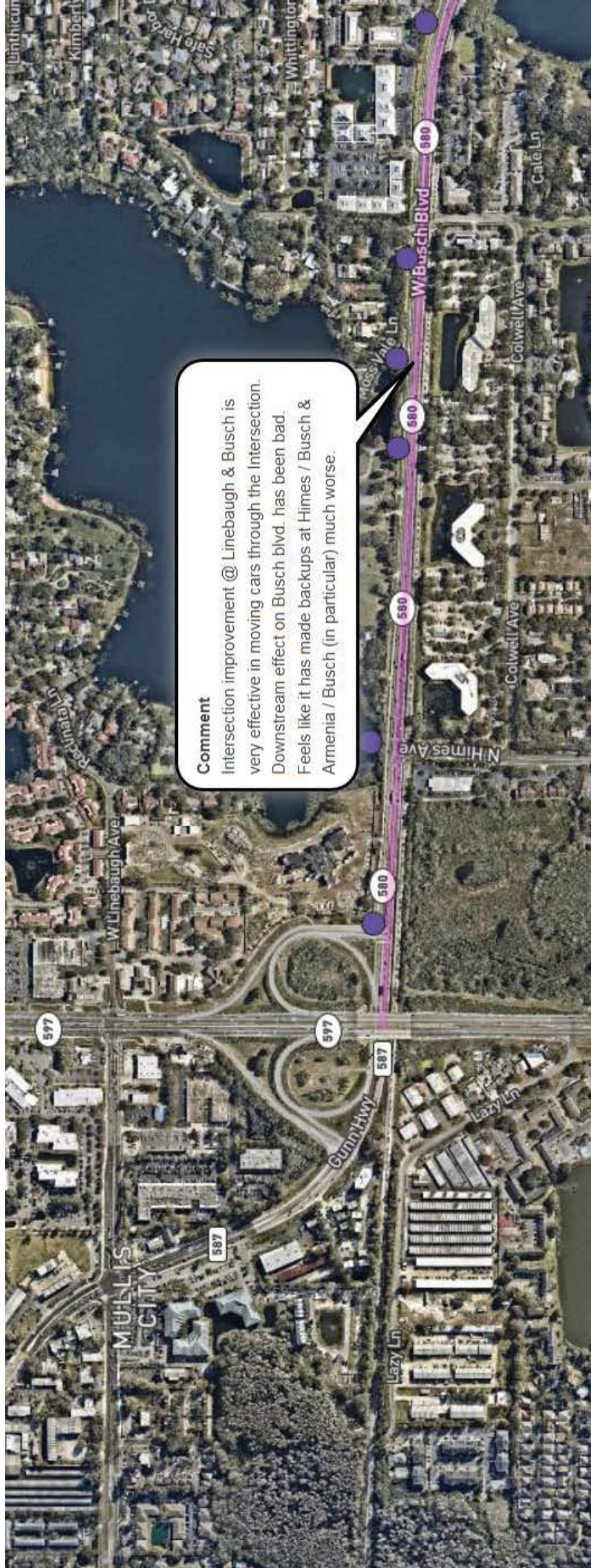
Comment
Not safe for school busses to stop & pickup / drop off.

Safety | 13-Dec-17



Comment
timing of lights to allow us to get out of my driveway.
3409 W. Busch Blvd.

Other | 13-Dec-17



Other | 13-Dec-17



Comment
Raise medians where possible with landscaping.

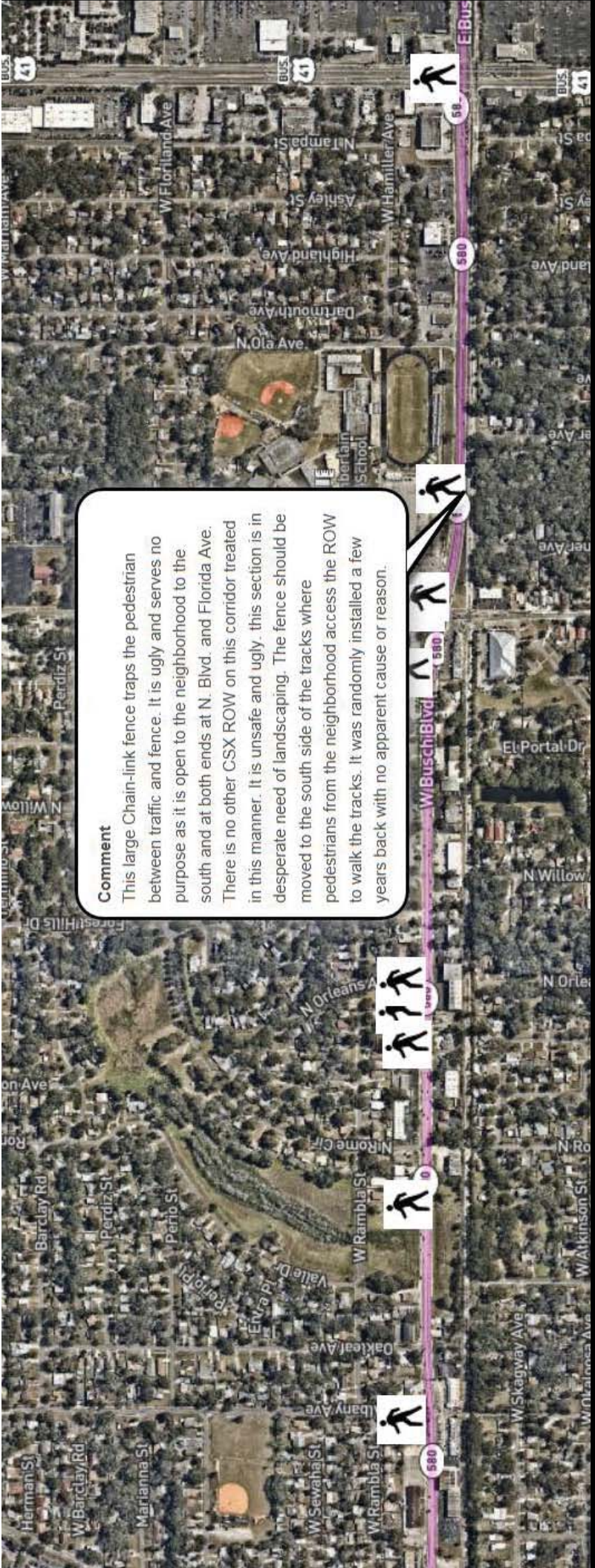
Other | 13-Dec-17



Comment

Blind spot because of hedges for vehicles puts sidewalk users at severe risk. The hedge blinds drivers from approaching bikes and pedestrians at the N. Edison @ Busch stop sign.

Safety | 25-Jan-18

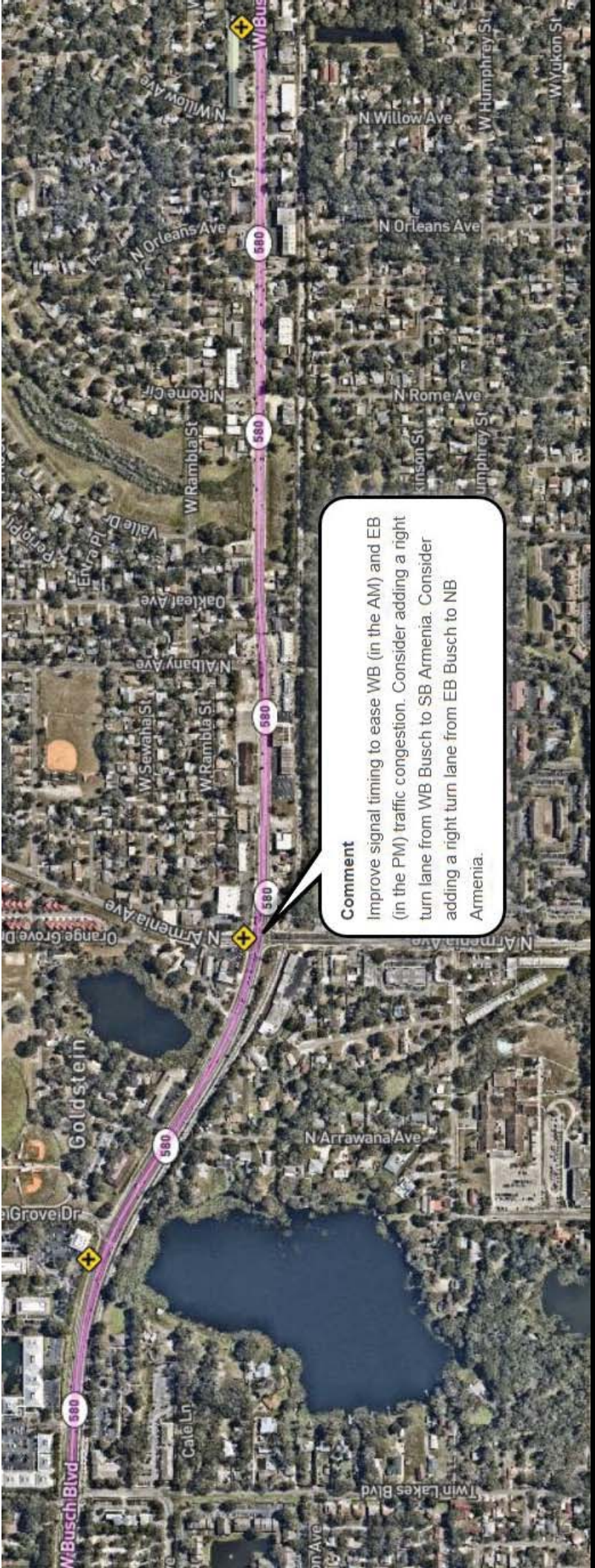


Comment

This large Chain-link fence traps the pedestrian between traffic and fence. It is ugly and serves no purpose as it is open to the neighborhood to the south and at both ends at N. Blvd. and Florida Ave. There is no other CSX ROW on this corridor treated in this manner. It is unsafe and ugly. this section is in desperate need of landscaping. The fence should be moved to the south side of the tracks where pedestrians from the neighborhood access the ROW to walk the tracks. It was randomly installed a few years back with no apparent cause or reason.

Pedestrian

25-Jan-18



Comment

Improve signal timing to ease WB (in the AM) and EB (in the PM) traffic congestion. Consider adding a right turn lane from WB Busch to SB Armenia. Consider adding a right turn lane from EB Busch to NB Armenia.

Intersection | 7-Mar-18



Bus / Transit | 7-Mar-18



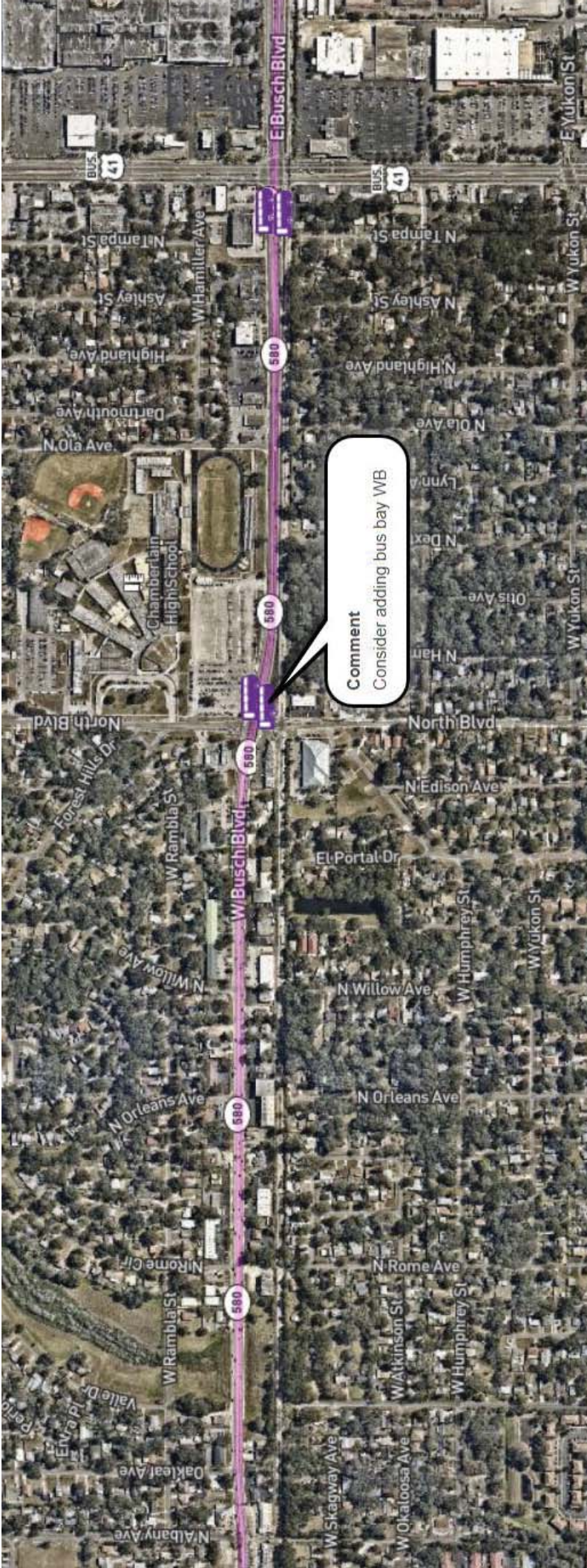
Comment
 WB congestion in the morning rush hour period.
 Traffic gets backed up through several signal cycles.

Traffic / Parking | 7-Mar-18



Comment
Heavy EB Congestion in the evening rush hour period.

Traffic / Parking | 7-Mar-18



Comment
Consider adding bus bay WB

Bus / Transit | 7-Mar-18



Comment
Consider adding bus bay EB

Bus / Transit | 7-Mar-18

